



The Council of the City of Cockburn

Ordinary Council Meeting  
**Agenda Paper**

For Thursday, 13 April 2023



City of Cockburn  
PO Box 1215, Bibra Lake  
Western Australia 6965

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Coleville Crescent, Spearwood

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### **NOTICE OF MEETING**

Pursuant to Clause 2.4 of Council's Standing Orders, an Ordinary Meeting of Council has been called for Thursday 13 April 2023. The meeting is to be conducted at 7pm in the City of Cockburn Council Chambers, Administration Building, Coleville Crescent, Spearwood.

The Agenda will be made available on the City's website on the Friday prior to the Council Meeting.

A handwritten signature in black ink, appearing to be 'D. Arndt', is positioned above the name of the signatory.

Daniel Arndt  
**Acting Chief Executive Officer**

## The Council of the City of Cockburn

### Ordinary Council Meeting Thursday, 13 April 2023 at 7pm

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## **The Council of the City of Cockburn**

**Ordinary Council Meeting  
Thursday, 13 April 2023 at 7pm**

### **Agenda**

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- 1. Declaration of Meeting**
  
- 2. Appointment of Presiding Member (If required)**
  
- 3. Disclaimer (To be read aloud by Presiding Member)**

Members of the public, who attend Council Meetings, should not act immediately on anything they hear at the Meetings, without first seeking clarification of Council's position.

Persons are advised to wait for written advice from the Council prior to taking action on any matter that they may have before Council.

- 4. Acknowledgement of Receipt of Written Declarations of Financial Interests and Conflict of Interest (by Presiding Member)**

- 5. Apologies & Leave of Absence**

Cr P Eva, Apology

- 6. Response to Previous Public Questions Taken on Notice**

Nil

- 7. Written Requests for Leave of Absence**

Nil

**8. Public Question Time**

**9. Confirmation of Minutes**

**9.1 Minutes of the Special Council Meeting – 2/3/2023**

**Recommendation**

That Council confirms the Minutes of the Special Council Meeting held on Thursday, 2 March 2023 as a true and accurate record.

**9.2 Minutes of the Ordinary Council Meeting - 9/3/2023**

**Recommendation**

That Council confirms the Minutes of the Ordinary Council Meeting held on Thursday, 9 March 2023 as a true and accurate record.

**9.3 Minutes of the Special Council Meeting - 23/3/2023**

**Recommendation**

That Council confirms the Minutes of the Special Council Meeting held on Thursday, 23 March 2023 as a true and accurate record.

**9.4 Minutes of the Special Council Meeting - 28/3/2023**

**Recommendation**

That Council confirms the Minutes of the Special Council Meeting held on Tuesday, 28 March 2023 as a true and accurate record.

**10. Deputations**

**Mr Steve Hall, College of Electrical Training**

Subject: Delegation visit to Jakarta and Surabaya, Indonesia

**11. Business Left Over from Previous Meeting (if adjourned)**

Nil

**12. Declaration by Members who have Not Given Due Consideration to Matters Contained in the Business Paper Presented before the Meeting**

**13. Decisions Made at Electors Meeting**

Nil

## 14 Reports - CEO (and Delegates)

### 14.1 Built and Natural Environment

#### 14.1.1 Proposed Structure Plan - Lots 117, 200, 803, 805, 816, 1000, 1001, 9001, 9002 & 9003 Wattleup Road (Hammond Quarter)

<b>Responsible Executive</b>	A/Chief of Built and Natural Environment
<b>Author</b>	Senior Strategic Planner
<b>Attachments</b>	<ol style="list-style-type: none"> <li>1. Southern Suburbs Stage 3 DSP Map <a href="#">↓</a></li> <li>2. WAPC Endorsed Concept Plan (Ministerial Subdivision Approval) <a href="#">↓</a></li> <li>3. WAPC Endorsed Density Plan (Ministerial Subdivision Approval) <a href="#">↓</a></li> <li>4. Proposed Structure Plan Map <a href="#">↓</a></li> <li>5. Subdivision Concept and Staging Plan <a href="#">↓</a></li> <li>6. Landscape Masterplan <a href="#">↓</a></li> <li>7. Preliminary Earthworks Plan <a href="#">↓</a></li> <li>8. Proposed Noise Mitigation - Option 3 <a href="#">↓</a></li> <li>9. Schedule of Submissions <a href="#">↓</a></li> <li>10. Recommended Modifications <a href="#">↓</a></li> </ol>
<b>Location</b>	Lots 117, 200, 803, 805, 816, 817, 1000, 1001, 9002 and 9003 Wattleup Road, Hammond Park
<b>Owner</b>	Qube Wattleup Development Pty Ltd (Lots 803, 816, 1000 & 9002); Qube Hammond Quarter Pty Ltd (Lot 809); Primewest (Wattleup) Pty Ltd (Lot 817 & 9001), Nunzio Foti (Lot 200), Dale, Desmond & Deborah Alfrevich (Lot 9001)
<b>Applicant</b>	Hatch Roberts Day, on behalf of Qube
<b>Application Reference</b>	110/241

#### RECOMMENDATION

That Council:

- (1) ADOPTS the Schedule of Submissions prepared in respect of the proposed Structure Plan as set out in Attachment 9;
- (2) RECOMMENDS, pursuant to Schedule 2, Part 4, Clause 20 of the *Planning and Development (Local Planning Schemes Regulations) 2015*, the Western Australian Planning Commission APPROVE the proposed Hammond Quarter Structure Plan, subject to the modifications outlined in Attachment 10;
- (3) ENDORSES the Bushfire Management Plan, prepared by MBS Environmental (Revision 1, dated 29 November 2022); and
- (4) ADVISES those who made a submission of Council's decision accordingly.

## Background

The proposed Structure Plan is presented for a recommendation on final determination by the Western Australian Planning Commission (WAPC).

### Previous Structure Plans

Two structure plans were previously prepared for part of the area now covered by the proposed Hammond Quarter Structure Plan, being:

- the Lots 121, 122 and 801 Wattleup Road, Wattleup Structure Plan
- the Lots 117 and 805 Wattleup Road, Wattleup Road Structure Plan.

At the 11 December 2008 Ordinary Council Meeting, Council resolved to recommend the WAPC approve both structure plans, subject to minor modifications.

On 25 August 2009, the WAPC deferred determination of both structure plans, pending the Kwinana Buffer Review Committee considering monitoring and modelling information related to Alcoa's Residue Drying Areas (RDAs) in Mandogalup.

The WAPC consequently did not endorse either structure plan.

### Southern Suburbs Stage 3 District Structure Plan

The land has been zoned 'Development' since gazettal of Town Planning Scheme No.3 (TPS 3) in December 2002.

In 2008, the City approved Southern Suburbs Stage 3 District Structure Plan, which covers the area of Hammond Park within the 'Development' zone to the south of Gaebler Road (refer Attachment 1).

The DSP was used to inform Amendment 28 to TPS 3, which introduced two Special Control Areas (Development Area 26 – Hammond Park and Development Area 27 – Wattleup), and two Development Contribution Areas (DCAs). Amendment 28 was gazetted in December 2014.

In recognition of the two earlier structure plans not being endorsed by the WAPC, the City amended the DSP in 2012.

The 2012 DSP (the current approved DSP) deferred planning for the future development cell south of Wattleup Road as 'Subject to Future Structure Planning', informed by the following note on the DSP map:

*"Future structure planning of the Cell south of Wattleup Road must provide an appropriate interface with residential development north of Wattleup Road.*

*This is to have particular regard to the position of the Western Australian Planning Commission in respect of its position on the acceptability (or otherwise) of residential development south of Wattleup Road, and alternative (non-residential) land uses that may be required. Any impacts associated within the future development of non-*



*residential land uses within the Cell south of Wattleup Road must be fully contained within the Cell boundaries.”*

### Ministerial Subdivision Approvals

Following approval of the two structure plans by the City in 2008, two subdivision applications were submitted to the WAPC in May 2013 at Lot 805 and 117 Wattleup Road (WAPC Ref: 147987), and Lots 809, 811 and 9002 Wattleup Road (WAPC Ref: 148010), which proposed a total of 240 residential lots.

In late 2013, the applicant for the subdivision applications triggered a deemed refusal, due to the WAPC not issuing a decision within the statutory timeframe.

Both applications were consequently refused by the State Administrative Tribunal in 2014, following an application for review.

The refused subdivision applications were ‘called in’ by the Minister for Planning (then the Hon. John Day), pursuant to Section 246 of the *Planning and Development Act 2005* (PD Act).

On 18 November 2020, the Minister for Planning (the Hon. Rita Saffioti) approved, subject to Section 247 of the PD Act, the two subdivision applications. These subdivision approvals remain valid until November 2024.

On 18 May 2021, the WAPC approved a ‘Concept Plan’ (refer Attachment 2) covering the area subject to the two Ministerial approvals, which modified the approved lot design, location of road reserves, road entries onto Wattleup Road and location of public open space (POS) to reflect the conditions of approval.

The WAPC also approved a ‘Density Plan’ (refer Attachment 3), which applied a residential coding over the approved lots.

### Health and Amenity Issues

The delay in approving the two subdivision applications was in part due to the need for scientific investigations to determine whether dust generated from industrial activities in Mandogalup (particularly the Alcoa RDAs) posed an unacceptable health and amenity risk to future residents.

In making her decision with respect to the two subdivision approvals, the Minister was satisfied that dust related issues are an amenity impact, but not a public health impact.

On this basis, the Minister considered it reasonable to allow the subdivisions to proceed on the basis that a notification be applied on the title of future lots warning landowners of the amenity impacts from industrial activities within the locality.

Whilst the two existing subdivision approvals do not extend to four of the lots within the Structure Plan area (being Lots 805, 801, 1000 and 1001), it is considered that the Minister’s decision to accept the impact of dust as an amenity impact extends to the overall Structure Plan area.

## Submission

N/A

## Report

### Structure Plan Overview

The proposed Structure Plan provides for the coordination of future subdivision and development, including:

- residential codings of 'R20', 'R30' and 'R40', delivering an estimated 296 lots;
- two public open space (POS) reserves of 4,930m<sup>2</sup> and 19,350m<sup>2</sup>; and
- an internal road network, including three access roads from Wattleup Road.

A copy of the proposed Structure Plan has been included (refer Attachment 4).

A copy of the proposed Subdivision Concept and Staging Plan is included (refer Attachment 5).

### Appropriateness of Residential Land Use

Submissions received from Alcoa and Planning Solutions (on behalf of Alcoa) question the appropriateness of the Structure Plan area being used for residential development, due to the proximity of the site to Alcoa's RDAs in Mandogalup.

Whilst it is noted that the WAPC did not approve previous structure plans for the site due to the absence of modelling information related to the RDAs, the Minister for Planning's approval of the two subdivisions within the Structure Plan area (which cover most of the Structure Plan area) is considered the final decision on the site's acceptability for residential land use.

Irrespective of whether a structure plan was prepared for the site, residential subdivision would be permitted to occur within the validity period of the current subdivision approvals.

### Residential Density

Notwithstanding the WAPC's endorsement of a Density Plan, residential codings still need to be enabled via an approved structure plan, including the three lots not subject to the earlier Ministerial subdivision approvals.

#### *R30*

A residential coding of 'R30' is proposed across the majority of the Structure Plan area. Under the DSP, an 'R30' coding is accepted as the base coding except where proximity to public transport and areas of high amenity (such as neighbourhood centres and educational facilities) supports higher density.

*R40*

A small pocket of 'R40' is located central to the Structure Plan, which proposes park frontage lots accessed by a rear laneway.

The lots provide surveillance of the central POS, consistent with the Ministerial subdivision approval.

Due to the site's proximity to POS, the 'R40' coding is considered acceptable.

*R20*

The residential areas of Hammond Park south of Gaebler Road are typically characterised by a minimum density coding of 'R30', consistent with the DSP.

An area between Access Road 2 and 3 (abutting Wattleup Road), is proposed with an 'R20' coding.

The proposed coding recognises that a significant portion of each lot is likely to contain battering to manage the significant level difference between the developable portion of the lot and Wattleup Road (discussed below).

To ensure lots are capable of residential development and are not constrained further through consequent subdivision, an 'R20' coding is considered acceptable in this instance.

Access to/from Wattleup Road

Access to the Structure Plan is proposed via three entry roads intersecting with Wattleup Road. The three access roads are consistent with the location shown on the Concept Plan endorsed by the WAPC.

Except for the easternmost access road (which is proposed to be limited to left in/left out, the remaining intersections are proposed as a full-movement intersections.

A 70km/h speed limit currently applies to Wattleup Road where it abuts the Structure Plan area.

Due to the rapid urbanisation of land abutting Wattleup Road in Hammond Park, the City's preference is for the speed limit to be reduced.

A reduction in the speed limit needs to be considered by Main Roads WA (MRWA) and may be impacted by the timing of the proposed Rowley Road extension.

In the absence of Rowley Road being extended, Wattleup Road is utilised as an important connection for heavy vehicles between the Kwinana Freeway and Rockingham Road, as well as for traffic generated by surrounding residential development.

Adjacent to the Structure Plan area the existing level of the Wattleup Road carriageway varies by 18m between Access Road 1 (west) and Access Road 3.

Upgrades to Wattleup Road are required to bring the road up to the standard of an urban road, however these upgrades will not extend to modifying existing levels.

Whilst the approval of the three access roads under the existing subdivision approvals is not in dispute, the City is concerned that the roads may lead to increased safety concerns for vehicle users and pedestrians, including:

- the ability for drivers to safely turn right onto Wattleup Road for the full-movement intersection (Access Road 2), due to visual sightlines of eastbound traffic on Wattleup Road travelling the posted speed (70km/h)
- the ability for drivers to slow down to make a left turn into Access Road 2 from Wattleup Road without a deceleration lane, due to the significant drop in levels along Wattleup Road
- the absence of a permanent median treatment in Wattleup Road adjacent to Access Road 3, to limit the intersection to a 'left in/left out' (LILO) treatment as proposed by the Structure Plan.

It is acknowledged that the design of the access roads is consistent with the conditions of the Ministerial subdivision approvals and Concept Plan endorsed by the WAPC, however these approvals were not informed by a Traffic Impact Assessment and Road Safety Audit, as necessary to determine any widening requirements to Wattleup Road (including median treatments and deceleration lanes).

In the longer term, the State Government has proposed the extension of Rowley Road to provide a major freight link to service Westport and surrounding industrial areas, however there is no planned timeframe for the completion of these works.

Due to this uncertainty, it is considered necessary for any road upgrades to be undertaken based on the current road environment of Wattleup Road.

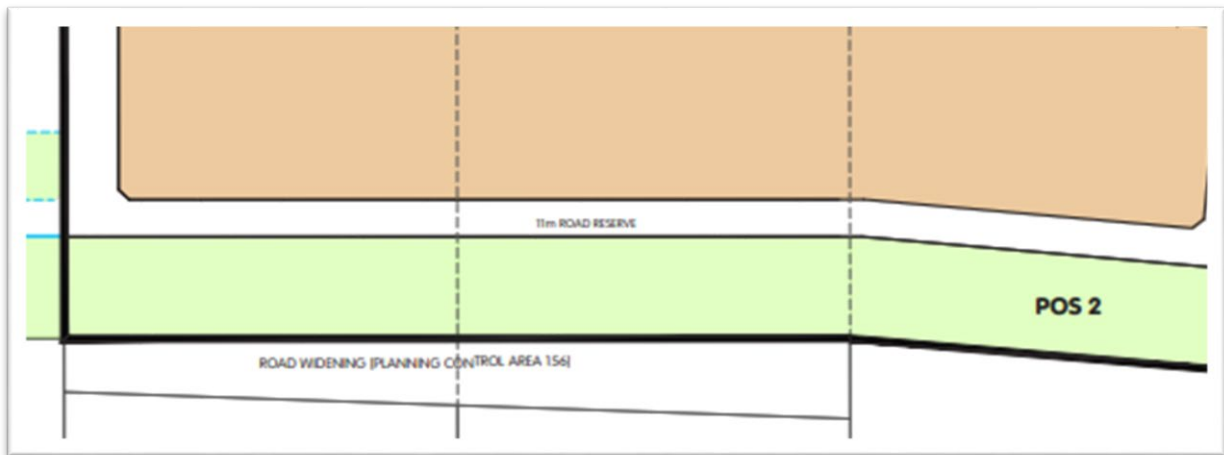
To provide greater certainty on the appropriateness of the access arrangements to the Structure Plan area, it is recommended that the TIA is modified to:

- include turn warrant assessments to all three intersections, to assess whether deceleration lanes are required and to determine whether widening is required on the southern side of Wattleup Road (within Structure Plan area)
- detail a permanent solid median intersection treatment within Wattleup Road (at the entrance to Access Road 3) to restrict this intersection to LILO only and detail any indicative road widening required.

### Internal Subdivision Roads

The design of the roads used to inform the Structure Plan design are generally consistent with the width requirements of the WAPC's *Liveable Neighbourhoods* policy, except for an 11m reserve which runs parallel with the southern linear public open space (POS).

This road reserve is consistent with Condition 3(a) of one of the Ministerial subdivision approvals (WAPC Ref: 148010 & 147987), which require a public open space area at least 30m in width and local road reserve at least 11m in width.



**Figure 1 – Portion of proposed 11m road reserve between the proposed Public Open Space and Residential 'R30'**

Notwithstanding the Minister's approval, the applicant has not provided an indicative cross-section within the Transport Impact Assessment (TIA) which demonstrates how a footpath, services (underground power, reticulated water, reticulated sewer, gas and NBN, street lighting and embayed parking (on the POS side) can be accommodated within an 11m road reserve.

In the absence of a cross-section to demonstrate that an 11m wide road reserve can be constructed without compromised street tree provision or servicing arrangements and a minimum 1m verge on the POS side, it is the City's position that a minimum 11.5m wide road reserve is required, with localised widenings to 13m to allow for the provision of parking embayments near the most usable portions of POS.

Such an arrangement would be more consistent with the minimum reserve width for an 'Access Street D' abutting POS under *Liveable Neighbourhoods*, the City's most recent consideration of other subdivision applications and the reserve widths proposed around the Central POS.

This change may require a slight adjustment to the depth of lots on the other side of the road to ensure it does not result in an under-provision of creditable POS (discussed further below).

Should the WAPC resolve to support a 11m road reserve under the Structure Plan and consequent subdivision approvals, this may set an undesirable precedent for a non-compliant road reserve width to be supported for other applications.

### Public Open Space

The structure plan includes two areas of public open space (POS), totalling 23,120m<sup>2</sup> (2.132ha) in area, consistent with the area shown on the WAPC endorsed Concept Plan. A copy of the Landscape Masterplan is included (refer Attachment 6).

Due to the level difference between the future Rowley Road reserve and future residential development, the southern ('Linear') POS includes a substantial sloped area, most prominent at its eastern end. This slope is a partial 1:3 and 1:4 gradient.

In the case of the 1:3 gradient, this means that there is a 1 metre difference in height for every 3 metres of horizontal surface.

A 1:3 gradient is the steepest slope that the City typically accepts, as any steeper adversely impacts its ease of maintenance (note: anything beyond a 1:6 gradient is typically planted as it cannot be safely mowed without specialist equipment).

The Structure Plan provides a total POS area of 2.1068ha, of which approximately 0.437ha is non-creditable, due to accommodating drainage (up to the 1:1 ARI) and surplus 'Restricted' POS (due to the large batter discussed above).

WAPC Operation Policy *Liveable Neighbourhoods* requires 10% of the gross subdivisional area is to be developed and ceded as POS.

Based on the reduced gross subdivisible area (GSA) of 19.067ha, this equates to a minimum (creditable) POS requirement of approximately 1.9ha, of which 1.53ha (or 80% of the minimum requirement) shall be 'Unrestricted' POS and a maximum of 0.38ha can be 'Restricted' POS.

The total creditable area equates to an area of 1.99ha, or 10.4% of the GSA.

This achieves the minimum requirements of Liveable Neighbourhoods.

#### Interface with Wattleup Road

Consistent with the WAPC's endorsed Concept Plan, the Structure Plan proposes residential lots which directly abut the existing Wattleup Road reserve.

To achieve the lot levels capable of residential development (through the provision of a water and sewer service by the Water Corporation), a significant cut and fill approach is required, involving a large volume of fill being removed from the eastern (and northern edge) of the site.

This results in the 'developable' portion of the lots which abut Wattleup Road (i.e., the part of the site which can accommodate residential development), being up to five metres below the adjacent road reserve.

The Preliminary Earthworks Plan (refer Attachment 7) prepared for the Structure Plan is premised upon the difference in levels being managed by retaining walls, a batter within the residential lots, or a combination of retaining walls and battering.

Deeper lots are proposed between Access Road 2 and 3 to accommodate a '1:3' batter.

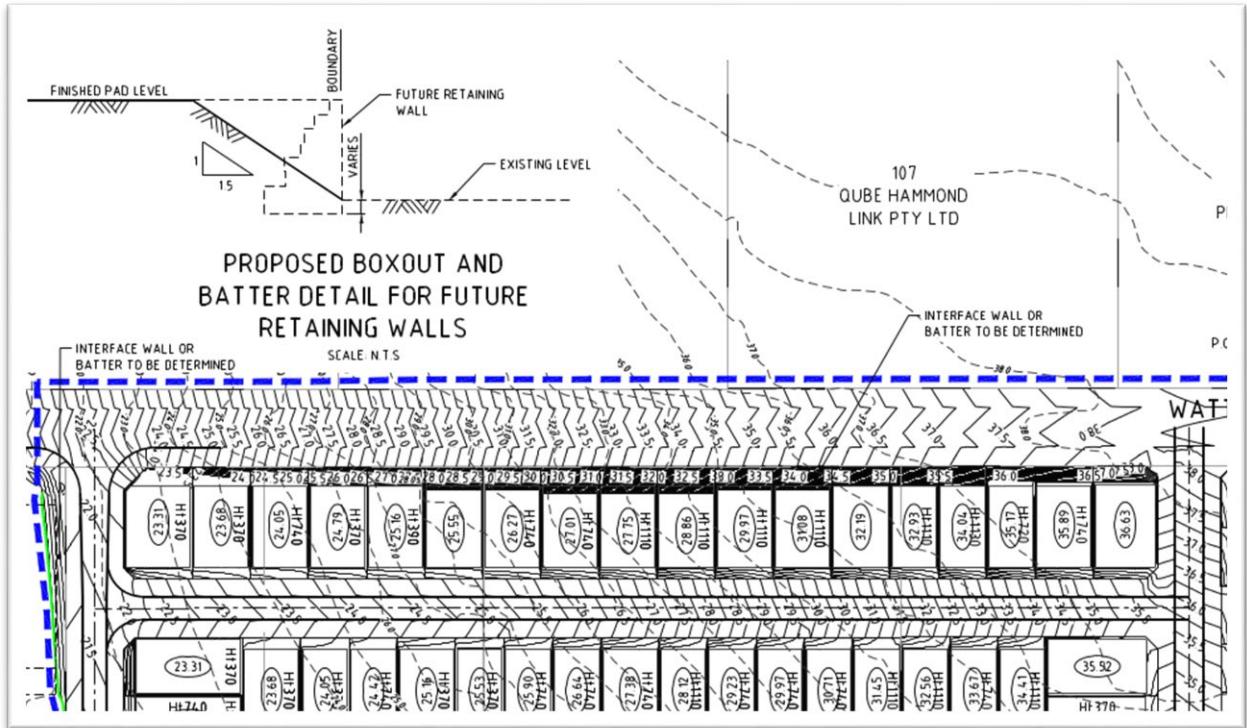


Figure 2 – Preliminary Earthworks Plan Extract – Lots Adjacent to Wattleup Road between Access Road 1 and 2

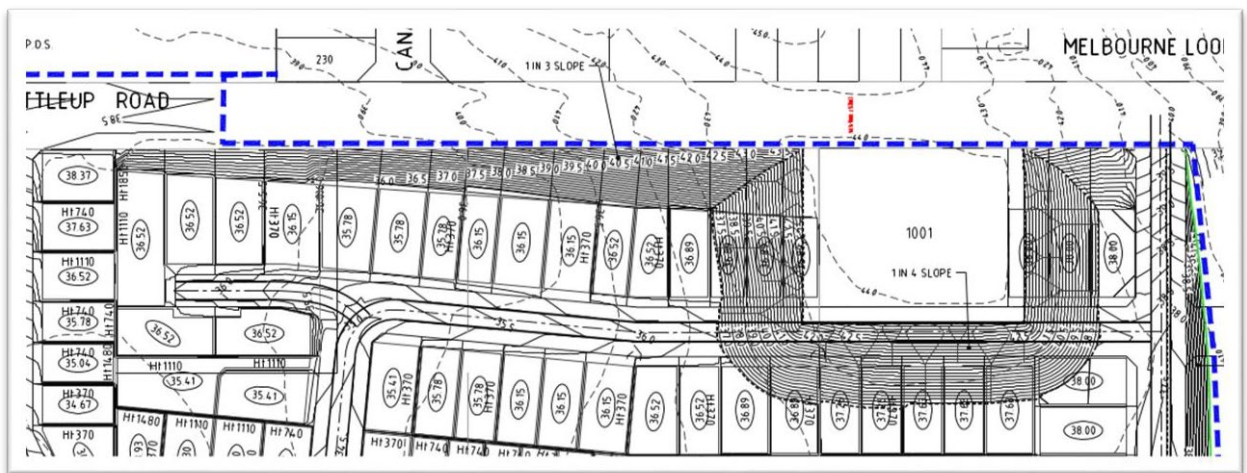


Figure 3 – Preliminary Earthworks Plan Extract – Lots Adjacent to Wattleup Road between Access Road 2 and 3

Of note, the batter depicted in Figure 3 around Lot 1001 is required to retain two existing dwellings fronting Wattleup Road, with surrounding lots likely to be held back until such time as those dwellings are removed and the site re-subdivided in a similar manner to adjacent lots, that front towards the new internal road.

Due to the Minister's subdivision approval involving a shared reserve arrangement along the eastern edge of Lot 800, construction of Access Road 3 is also contingent upon significant earthworks within the adjoining property (outside the structure plan area).

This means that lots in this area will also need to be held back, until either an agreement is reached with the owner of Lot 115 or (depending on timing), the road is pulled back entirely within the structure plan area (inclusive of any necessary batter).

Battering within freehold lots is not typically supported within residential subdivisions of the scale proposed, as this constrains the landowner or occupant's use and enjoyment of the land.

As the existing Ministerial approvals are at present capable of being implemented, there is limited opportunity for an alternative design to be considered, unless consequent subdivision approvals from the WAPC are sought.

The City accepts that battering into lots may be necessary to manage the level difference based on the proposed Structure Plan design, however, the restrictions it imposes on the use and development potential of the lot will need to be considered in further detail at the subdivision and/or civil drawing stages of development.

It is recommended that Part One (Implementation Section) of the Structure Plan is modified to insert a provision requiring this matter to be addressed via a preliminary civil design plan, ideally submitted upon lodgement of a subdivision application.

The provision of such a plan will inform the City's recommendation to the WAPC (or its approval of subsequent civil drawings where a new subdivision has not been triggered), allowing it to consider whether battering or retaining will unreasonably encumber reasonable use and development of affected lots.

It will also allow it to consider the need for an encumbrance to be registered on the title ensuring that Wattleup Road is adequately protected from future disturbance or subsidence.

Such an encumbrance, typically a restrictive covenant or easement to the benefit of the City, is normally imposed via a condition of subdivision approval.

### Noise Attenuation

The Structure Plan is located directly to the north of Planning Control Area (PCA) 153. Land within PCA 153 is being progressively acquired by the WAPC for the purposes of the future Rowley Road extension, ahead of this land being reserved as a 'Primary Regional Road' under the Metropolitan Region Scheme (MRS).

Where future residential development is located within 200 metres of an existing or proposed regional road reserve, the future noise impact of the road is to be assessed in accordance with *State Planning Policy 5.4 – Road and Rail Noise (SPP 5.4)*.



Under SPP 5.4, the developer or future landowners are typically responsible for the cost of mitigating the impact of road noise, which may include Quiet House Design (QHD) measures in individual dwellings, noise walls (at, or close to the noise source), increasing separation (such as with POS), or a combination of these measures.

To achieve the levels required for development of the Structure Plan area and to tie in with the future levels of Rowley Road, the level of Rowley Road will be between 1m and 6m above the finished level of lots.

Depending on the specific location of the Rowley Road extension and the POS located between residential lots and PCA 156, the road will be approximately 60-70m from future residential lots.

The Transportation Noise Assessment (TNA) prepared for the Structure Plan has considered noise mitigation measure in three different scenarios. The applicant and Main Roads WA have accepted 'Option 3 – Shared Responsibility', which broadly includes:

- MRWA being responsible for constructing a 4m high noise wall along the western interface with Rowley Road, adjacent to the road carriageway. The noise wall will be constructed at Main Roads' expense when the Rowley Road extension is constructed.
- The developer being responsible for the construction of a noise bund (to a maximum height of 40 RL, which sits partially within City managed POS and PCA 153. The noise bund will be constructed at the developer's expense.
- Future landowners of approximately 30 lots being responsible for implementing QHDs into the design of individual dwellings. The cost of QHD measures will be borne by the individual landowners in the cost of the constructing the dwelling (but are often discounted from the lot purchase price).

A copy of the proposed noise mitigation requirements based on 'Option 3', as per Figure 5.4 of the TNA is included (refer Attachment 8).

The City accepts that 'Option 3' provides for appropriate noise mitigation.

A modification to the TNA is recommended to reference 'Option 3' as the preferred option, to be implemented as part of the future subdivision and title creation works.

The recommended noise mitigation measures in 'Option 3' provides for future lots identified as requiring QHD for the ground floor, but do not detail the extent of QHD where a second storey is proposed.

A modification to the TNA is recommended to provide an assessment of the noise impacts for the second storey of residential development, to inform the application of QHD requirements through a future local development plan.

#### Nearby Extractive Industry

The Structure Plan area is located approximately 140m from existing sand mining activities located on Lot 10 (No.2) Rowley Road, Mandogalup.

These activities have a current development approval as an ‘Extractive Industry’ from the City of Kwinana (under its Town Planning Scheme No.2) and the WAPC (under the Metropolitan Region Scheme). Kwinana’s approval is due to expire in February 2026.

A significant area directly to the south of the Structure Plan area is classified as ‘Significant Geological Supplies’ (SGS), which are identified by the Department of Mines, Industry and Safety (DMIRS) as a long-term supply of basic raw materials (BRM) which require protection.

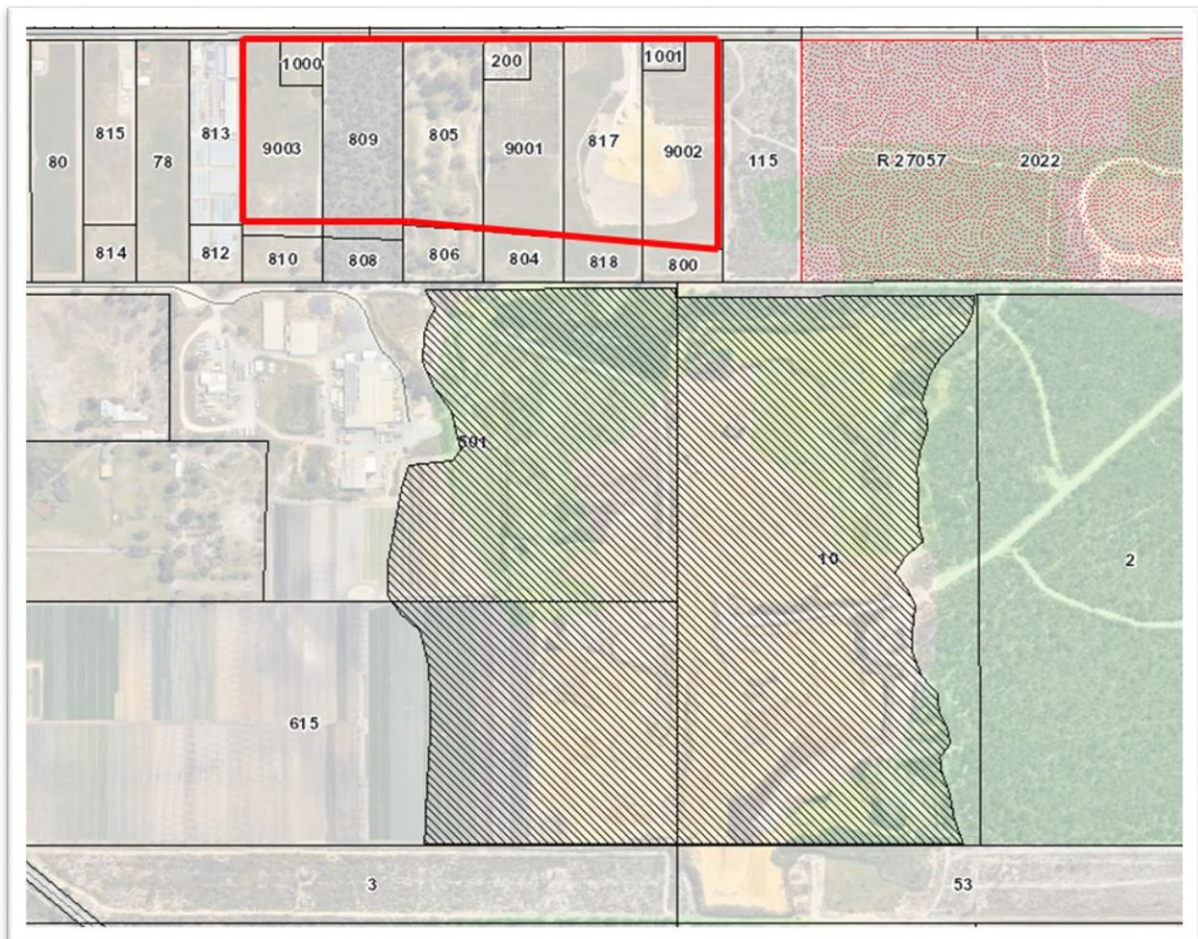


Figure 4 – Proximity of Structure Plan Area to identified Significant Geological Supplies (shown in hatched area) - Source: DMIRS Geoview.WA Mapping

Under the Environmental Protection Authority’s (EPA) *Guidance Statement No.3 – Separation Distances between Industrial and Sensitive Land Uses (GS 3)*, an indicative buffer of between 300m and 500m applies between an Extractive Industry (sand and limestone extraction) land uses and sensitive land uses (including residential), to provide adequate separation from potential noise and dust impacts.

The indicative buffer under GS 3 applies unless there is an accepted site-specific study which demonstrates a lesser separation between a sensitive land use and an industrial land use should be applied.

Under *State Planning Policy 2.4 – Planning for Basic Raw Materials* (SPP 2.4), structure plans should identify SGS areas and appropriate separation distances, to avoid any health risks or amenity implications for adjacent sensitive land uses.

The Structure Plan submission does not provide a site-specific study to support a reduction in the buffer under GS 3.

Considering the proposed location of POS and subdivisional roads, depending on the timing of their creation, future residential lots could be located as close to 180m from existing sand mining activities.

In the absence of a technical study that considers the amenity impacts of dust and noise from nearby sand mining activities, it is unknown what the impact will be to future residents within the Structure Plan area.

Notwithstanding this, by virtue of the Ministerial subdivision approvals, residential development is still permitted to occur whilst the sand mining activities continue.

On this basis, it is considered that specific studies to consider the amenity impacts is unnecessary, particularly in this instance where the Ministerial subdivision approval already requires a memorial for similar purposes.

Notwithstanding this, it is recommended that Part One (Implementation) of the Structure Plan be modified to reflect this consideration and allow a notification on title to be imposed as a condition of any consequent subdivision approval, also advising of the potential amenity impacts of nearby sand mining activities.

In the absence of further detail investigation, it is recommended that the precautionary approach be applied, and that notifications on title be applied to the creation of any lot within 500m of active (or incomplete) sand mining activities.

#### Native Vegetation

Lot 803 (formerly Lot 809) Wattleup Road contain 3.03 hectares of Banksia Woodlands, a Threatened Ecological Community (TEC).

Clearing of TEC vegetation requires approval from the Commonwealth Department of Climate Change, Energy, the Environment and Water (DCCEEW) under the Commonwealth *Environmental Protection and Biodiversity Conservation Act 1999* (EPBC Act).

DCCEEW approved the clearing of all 3.03 hectares of Banksia TEC in September 2022, which permits the proponent to clear all native vegetation on-site 'as of right' without approval from the State Government, providing the land is subject to a valid subdivision approval.

The need to undertake major earthworks to provide reticulated water and sewer to the site makes it difficult to retain existing vegetation, however the proposed finished levels of POS and road reserve on Lot 803 provide some opportunity for the retention of existing Banksia vegetation.

It is the City's expectation that the proponent makes reasonable efforts to retain some of the existing Banksia vegetation, where possible.

It is recommended that the Part Two (Explanatory section) is modified to clarify that existing vegetation should be retained within road reserves and POS, where finished levels allow.

### Aboriginal Cultural Heritage

On 1 July 2023, new legislation relating to Aboriginal cultural heritage, the *Aboriginal Cultural Heritage Act 2021* (ACH Act) is due to be proclaimed, which will apply to activities proposing ground disturbance, unless a specific activity is exempted under the Act or subsidiary regulations.

Under the proposed legislation and related guidelines, subdivisional works (which typically follow the approval of a Structure Plan (once the WAPC has approved a subdivision) are proposed as a 'Tier 3' activity, which relates to highest tier of ground disturbance works.

Under Part Two (Explanatory Section) of the Structure Plan, it is acknowledged that there are no Aboriginal heritage sites listed under the current *Aboriginal Heritage Act 1972*, with the closest site located approximately 500 metres from the boundary.

Under the *Aboriginal Cultural Heritage Act 2021*, there is a presumption that Aboriginal Cultural Heritage (ACH) may be present, which may trigger the need for a new assessment under the Act, which may include a management plan where culturally significant sites are identified

It should be noted that approvals obtained under other legislation (such as the *Planning and Development Act 2005* and *Building Act 2012*) does not negate a proponent's obligation to comply with the ACH Act.

To acknowledge that the proponent will be required to comply with the ACH Act as of 1 July 2023, a modification to Part Two of the Structure Plan is recommended to clarify the proponent's ongoing obligations.

### **Strategic Plans/Policy Implications**

#### Local Economy

A sustainable and diverse local economy that attracts increased investment and provides local employment.

- Increased Investment, economic growth and local employment.

#### Environmental Responsibility

A leader in environmental management that enhances and sustainably manages our local natural areas and resources.

- Protection and enhancement of our natural areas, bushland, parks and open spaces.

### City Growth & Moving Around

A growing City that is easy to move around and provides great places to live.

- An attractive, socially connected and diverse built environment.

### **Budget/Financial Implications**

The cost of processing the Structure Plan was calculated in accordance with the *Planning and Development Regulations 2009* and has been paid by the proponent.

### **Legal Implications**

N/A

### **Community Consultation**

The proposal was advertised for a period of 42 days, in accordance with Regulation 18 (3A) of the *Planning and Development (Local Planning Schemes) Regulations 2015*, between 5 January 2023 and 16 February 2023.

Advertising consisted of an advertisement in the Perth Now (Cockburn) newspaper, notice on the City's 'Comment on Cockburn' website, letters to surrounding landowners and relevant State Government agencies and servicing authorities.

Hard copies of the documentation were also available to view during work hours at the City's Administration Building, Spearwood for the duration of the advertising period.

As per the requirements of *Local Planning Policy 5.19 – Structure Plans & Telecommunications Infrastructure*, the proposal was forwarded to telecommunication providers.

The City received 18 submissions, including 11 from State Government agencies and servicing authorities and 7 submissions from members of the public.

In summary:

- three (3) submissions supported the proposal
- 11 submissions provided comment or no objection
- four (4) submissions objected to the proposal.

A copy of the Schedule of Submissions, inclusive of more detailed officer commentary in response to the individual issues raised is included (refer Attachment 9).

### **Risk Management Implications**

The officer recommendation considers the relevant planning matters associated with the proposal. It is considered that the officer recommendation is appropriate.

**Advice to Proponent(s)/Submitters**

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 13 April 2023 Ordinary Council Meeting.

**Implications of Section 3.18(3) *Local Government Act 1995***

Nil

**PLAN NOTES**

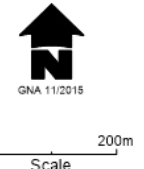
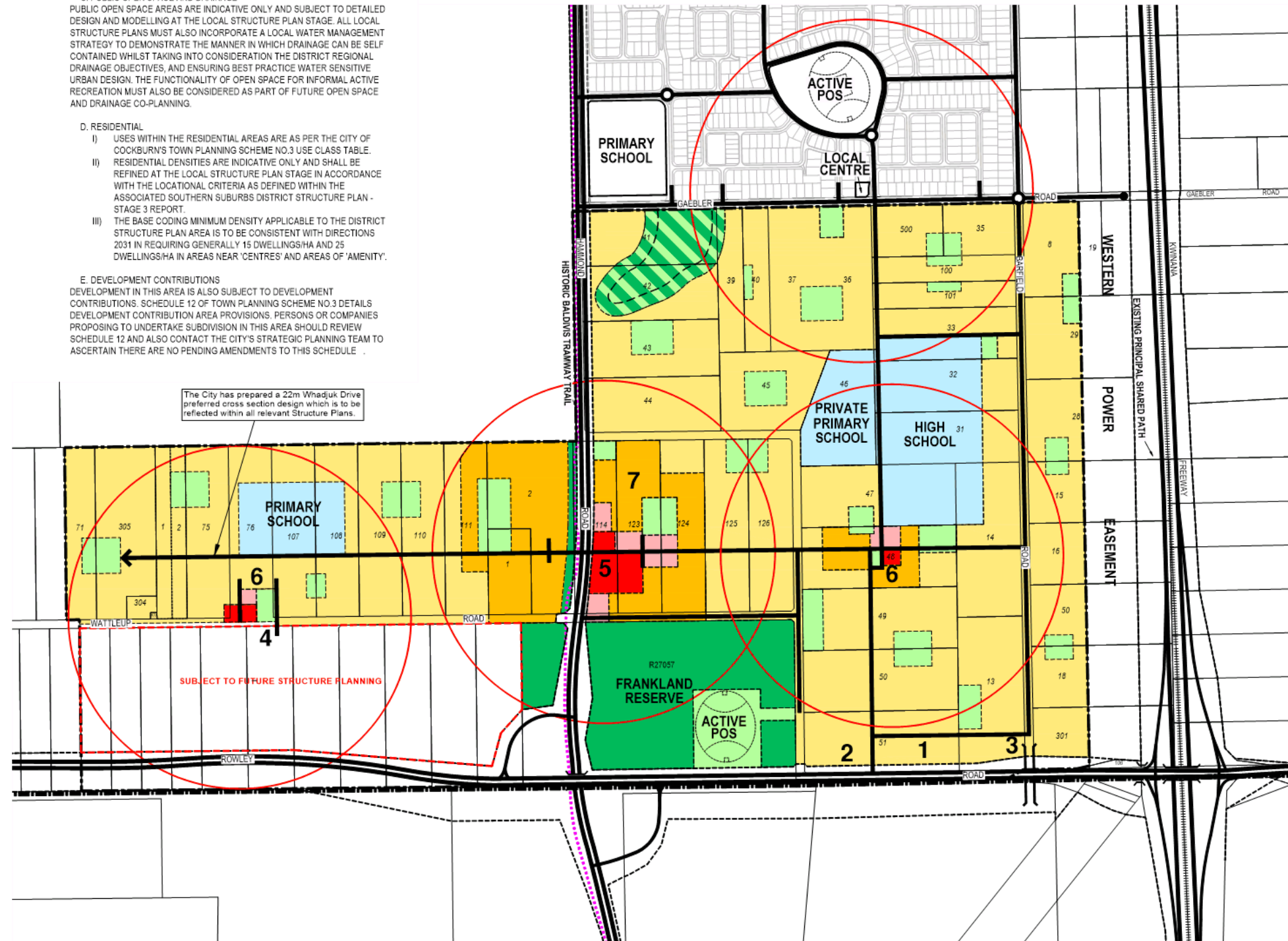
1. FUTURE RESIDENTIAL DEVELOPMENT SHALL NOT DIRECTLY ABUT ROWLEY ROAD. FUTURE LOCAL STRUCTURE PLANNING IS TO DEMONSTRATE A SUITABLE INTERFACE TREATMENT (E.G. ENLARGED SERVICE ROAD DESIGN WITH FRONTING RESIDENTIAL DEVELOPMENT AS A MINIMUM) BEING PROVIDED TO THE FUTURE ROWLEY ROAD FREIGHT ACCESS ROUTE.
2. FUTURE ACCESS ROAD TO BE PROVIDED AS A FULL INTERSECTION UNTIL ROWLEY ROAD IS UPGRADED AND CONSTRUCTED TO A REGIONAL ROAD AT WHICH TIME THE INTERSECTION WILL BE CONVERTED AND MAINTAINED AS LEFT INLEFT OUT ACCESS ONLY. (SUBJECT TO MAIN ROADS WA APPROVAL).
3. AS PART OF THE UPGRADE OF ROWLEY ROAD, GRADE SEPARATED PEDESTRIAN AND VEHICULAR ACCESS IS TO BE PROVIDED AS A CONTINUATION OF BARFIELD ROAD, IN ORDER TO MAINTAIN CONNECTIVITY BETWEEN FUTURE DEVELOPMENT TO THE SOUTH OF ROWLEY ROAD. THIS MAY BE FURTHER RATIONALISED THROUGH SUBSEQUENT LOCAL STRUCTURE PLANNING TO DETERMINE HOW THIS SPECIFIC ACCESS IS CREATED.
4. FUTURE STRUCTURE PLANNING OF THE CELL SOUTH OF WATTLEUP ROAD MUST PROVIDE AN APPROPRIATE INTERFACE WITH RESIDENTIAL DEVELOPMENT NORTH OF WATTLEUP ROAD. THIS IS TO HAVE PARTICULAR REGARD TO THE POSITION OF THE WESTERN AUSTRALIAN PLANNING COMMISSION IN RESPECT OF ITS POSITION ON THE ACCEPTABILITY (OR OTHERWISE) OF RESIDENTIAL DEVELOPMENT SOUTH OF WATTLEUP ROAD, AND ALTERNATIVE (NON-RESIDENTIAL) LAND USES THAT MAY BE REQUIRED. ANY IMPACTS ASSOCIATED WITH THE FUTURE DEVELOPMENT OF NON-RESIDENTIAL LAND USES IN THE CELL SOUTH OF WATTLEUP ROAD MUST BE FULLY CONTAINED WITHIN THE CELL BOUNDARIES.
5. NEIGHBOURHOOD CENTRE - THE DESIGN AND FUNCTION OF THE PROPOSED NEIGHBOURHOOD CENTRE SHALL BE BASED ON 'MAIN STREET' PRINCIPLES AND RELEVANT PROVISIONS OF LIVEABLE NEIGHBOURHOODS. ANY ASSOCIATED LOCAL STRUCTURE PLAN MUST ADEQUATELY DEMONSTRATE THROUGH CONCEPT PLANS AND OR DETAILED AREA PLANS THE MANNER IN WHICH THE CENTRE ADDRESSES THE REQUIREMENTS OF THE RELEVANT SECTIONS OF THE SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN - STAGE 3 REPORT, AND PARTICULARLY THE NEIGHBOURHOOD CENTRE CONCEPT PLAN PROVIDED WITHIN THE REPORT DOCUMENT.
6. NEIGHBOURHOOD NODES - THE DESIGN AND FUNCTIONALITY OF THE NEIGHBOURHOOD NODES SHALL BE DISTINCTLY DIFFERENT TO THE PRIMACY OF THE NEIGHBOURHOOD CENTRE. THESE LOCATIONS ARE TO PROVIDE FOR ARRANGE OF MORE LOCALLY FOCUSED ACTIVITIES AND FUNCTIONS. WHERE RETAIL IS PROPOSED, THESE ARE TO NOT EXCEED A MAXIMUM RETAIL FUNCTION OF 300SQM, WITH SUCH FUNCTION BASED UPON A 'CONVENIENCE STORE' TYPE USE.
7. CENTRAL PRECINCT - A COMPREHENSIVE LOCAL STRUCTURE PLAN WILL BE REQUIRED FOR THE CENTRAL NEIGHBOURHOOD CENTRE AND ADJACENT DEVELOPMENT. THE CITY WILL NOT CONSIDER INDIVIDUAL STRUCTURE PLANS IN THIS LOCATION DUE TO THE NEED TO ENSURE THE SUITABLE INTEGRATION OF DEVELOPMENT.

**GENERAL NOTES**

- A. ALL LOCAL STRUCTURE PLANS MUST INCLUDE AND BE INFORMED BY:**
- I) DETAILED LVMS BASED UPON REGIONAL DRAINAGE STUDY,
  - II) DETAILED NOISE MANAGEMENT STRATEGY WHERE LSPAD JOINS ROWLEY ROAD,
  - III) FIRE MANAGEMENT PLAN WHERE LSP IS LOCATED NEAR ROS OR SIGNIFICANT POS,
  - IV) FLORA AND FAUNA MANAGEMENT PLAN,
  - V) TRAFFIC MANAGEMENT PLAN,
  - VI) CONTAMINATED SITES & ACID SULPHATE SOILS MANAGEMENT PLAN WHERE REQUIRED,
  - VII) HERITAGE STUDY WHERE LSP INCLUDES FORMER HISTORIC TRAMWAY,
  - VIII) TRANSITION AND/OR INTERFACE STRATEGY IN RESPECT OF EXISTING RURAL USES,
  - IX) NEIGHBOURHOOD CENTRE CONCEPT PLAN AND DETAILED AREA PLAN WHERE INCLUDED WITHIN LSP AREA,
  - X) NEIGHBOURHOOD NODE CONCEPT PLAN AND DETAILED AREA PLAN.
- B. LOCAL STRUCTURE PLANS**  
SUBDIVISION AND DEVELOPMENT OF THE SUBJECT AREA IS TO BE IN ACCORDANCE WITH AN ENDORSED APPLICABLE LOCAL STRUCTURE PLAN.

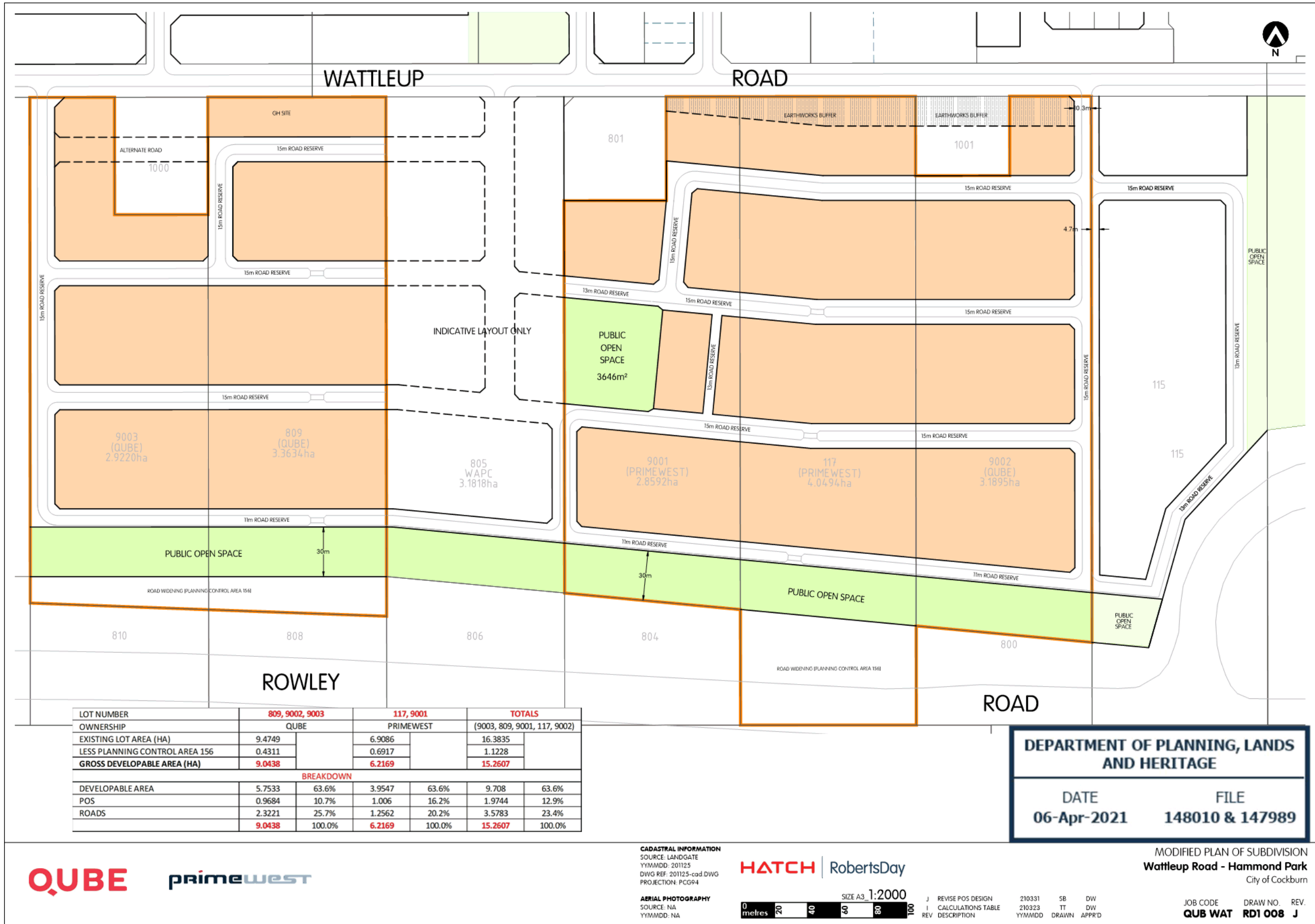
- C. PUBLIC OPEN SPACE AND DRAINAGE**  
PUBLIC OPEN SPACE AREAS ARE INDICATIVE ONLY AND SUBJECT TO DETAILED DESIGN AND MODELLING AT THE LOCAL STRUCTURE PLAN STAGE. ALL LOCAL STRUCTURE PLANS MUST ALSO INCORPORATE A LOCAL WATER MANAGEMENT STRATEGY TO DEMONSTRATE THE MANNER IN WHICH DRAINAGE CAN BE SELF CONTAINED WHILST TAKING INTO CONSIDERATION THE DISTRICT REGIONAL DRAINAGE OBJECTIVES, AND ENSURING BEST PRACTICE WATER SENSITIVE URBAN DESIGN. THE FUNCTIONALITY OF OPEN SPACE FOR INFORMAL ACTIVE RECREATION MUST ALSO BE CONSIDERED AS PART OF FUTURE OPEN SPACE AND DRAINAGE CO-PLANNING.
- D. RESIDENTIAL**
- I) USES WITHIN THE RESIDENTIAL AREAS ARE AS PER THE CITY OF COCKBURN'S TOWN PLANNING SCHEME NO.3 USE CLASS TABLE.
  - II) RESIDENTIAL DENSITIES ARE INDICATIVE ONLY AND SHALL BE REFINED AT THE LOCAL STRUCTURE PLAN STAGE IN ACCORDANCE WITH THE LOCALITY CRITERIA AS DEFINED WITHIN THE ASSOCIATED SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN - STAGE 3 REPORT.
  - III) THE BASE CODING MINIMUM DENSITY APPLICABLE TO THE DISTRICT STRUCTURE PLAN AREA IS TO BE CONSISTENT WITH DIRECTIONS 2031 IN REQUIRING GENERALLY 15 DWELLINGS/HA AND 25 DWELLINGS/HA IN AREAS NEAR 'CENTRES' AND AREAS OF 'AMENITY'.
- E. DEVELOPMENT CONTRIBUTIONS**  
DEVELOPMENT IN THIS AREA IS ALSO SUBJECT TO DEVELOPMENT CONTRIBUTIONS. SCHEDULE 12 OF TOWN PLANNING SCHEME NO.3 DETAILS DEVELOPMENT CONTRIBUTION AREA PROVISIONS. PERSONS OR COMPANIES PROPOSING TO UNDERTAKE SUBDIVISION IN THIS AREA SHOULD REVIEW SCHEDULE 12 AND ALSO CONTACT THE CITY'S STRATEGIC PLANNING TEAM TO ASCERTAIN THERE ARE NO PENDING AMENDMENTS TO THIS SCHEDULE.

The City has prepared a 22m Whadjuk Drive preferred cross section design which is to be reflected within all relevant Structure Plans.



<p><b>Residential Density</b></p> <ul style="list-style-type: none"> <li><span style="display: inline-block; width: 15px; height: 15px; background-color: yellow; border: 1px solid black;"></span> High</li> <li><span style="display: inline-block; width: 15px; height: 15px; background-color: orange; border: 1px solid black;"></span> Medium</li> </ul>	<p><b>Local Centre (Pedestrian Based Retail)</b></p> <ul style="list-style-type: none"> <li><span style="display: inline-block; width: 15px; height: 15px; background-color: red; border: 1px solid black;"></span> Local Centre (Pedestrian Based Retail)</li> <li><span style="display: inline-block; width: 15px; height: 15px; background-color: pink; border: 1px solid black;"></span> Mixed Business/Commercial/ Home Based Business</li> <li><span style="display: inline-block; width: 15px; height: 15px; background-color: lightblue; border: 1px solid black;"></span> Community Facilities/ Schools etc</li> <li><span style="display: inline-block; width: 15px; height: 15px; background-color: lightgreen; border: 1px solid black;"></span> Conservation Category Wetland* <small>*in accordance with DEC Geomorphic Wetlands Swan Coastal Plain dataset</small></li> </ul>	<p><b>Key Open Space Areas:</b></p> <ul style="list-style-type: none"> <li><span style="display: inline-block; width: 15px; height: 15px; background-color: lightgreen; border: 1px solid black;"></span> Public Open Space</li> <li><span style="display: inline-block; width: 15px; height: 15px; background-color: green; border: 1px solid black;"></span> Regional Open Space</li> </ul>	<p><b>Key Road Structure (Existing and Proposed)</b></p> <ul style="list-style-type: none"> <li><span style="display: inline-block; width: 15px; border-bottom: 2px solid black;"></span> Major Shared Path</li> <li><span style="display: inline-block; width: 15px; border-bottom: 2px dashed black;"></span> Historic Baldvis Tramway Trail</li> <li><span style="display: inline-block; width: 15px; border-bottom: 2px solid red;"></span> 400m Walkable Catchment</li> <li><span style="display: inline-block; width: 15px; border-bottom: 2px solid black;"></span> Central Precinct</li> </ul>
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**SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN**  
- STAGE 3, HAMMOND PARK / WATTLEUP



LOT NUMBER	809, 9002, 9003		117, 9001		TOTALS	
OWNERSHIP	QUBE		PRIMEWEST		(9003, 809, 9001, 117, 9002)	
EXISTING LOT AREA (HA)	9.4749		6.9086		16.3835	
LESS PLANNING CONTROL AREA 156	0.4311		0.6917		1.1228	
<b>GROSS DEVELOPABLE AREA (HA)</b>	<b>9.0438</b>		<b>6.2169</b>		<b>15.2607</b>	
	<b>BREAKDOWN</b>					
DEVELOPABLE AREA	5.7533	63.6%	3.9547	63.6%	9.708	63.6%
POS	0.9684	10.7%	1.006	16.2%	1.9744	12.9%
ROADS	2.3221	25.7%	1.2562	20.2%	3.5783	23.4%
	<b>9.0438</b>	<b>100.0%</b>	<b>6.2169</b>	<b>100.0%</b>	<b>15.2607</b>	<b>100.0%</b>

**DEPARTMENT OF PLANNING, LANDS AND HERITAGE**

---

DATE: **06-Apr-2021** FILE: **148010 & 147989**



**CADASTRAL INFORMATION**  
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 YYMMDD: 201125  
 DWG REF: 201125-cad.DWG  
 PROJECTION: PCG94



**AERIAL PHOTOGRAPHY**  
 SOURCE: NA  
 YYMMDD: NA



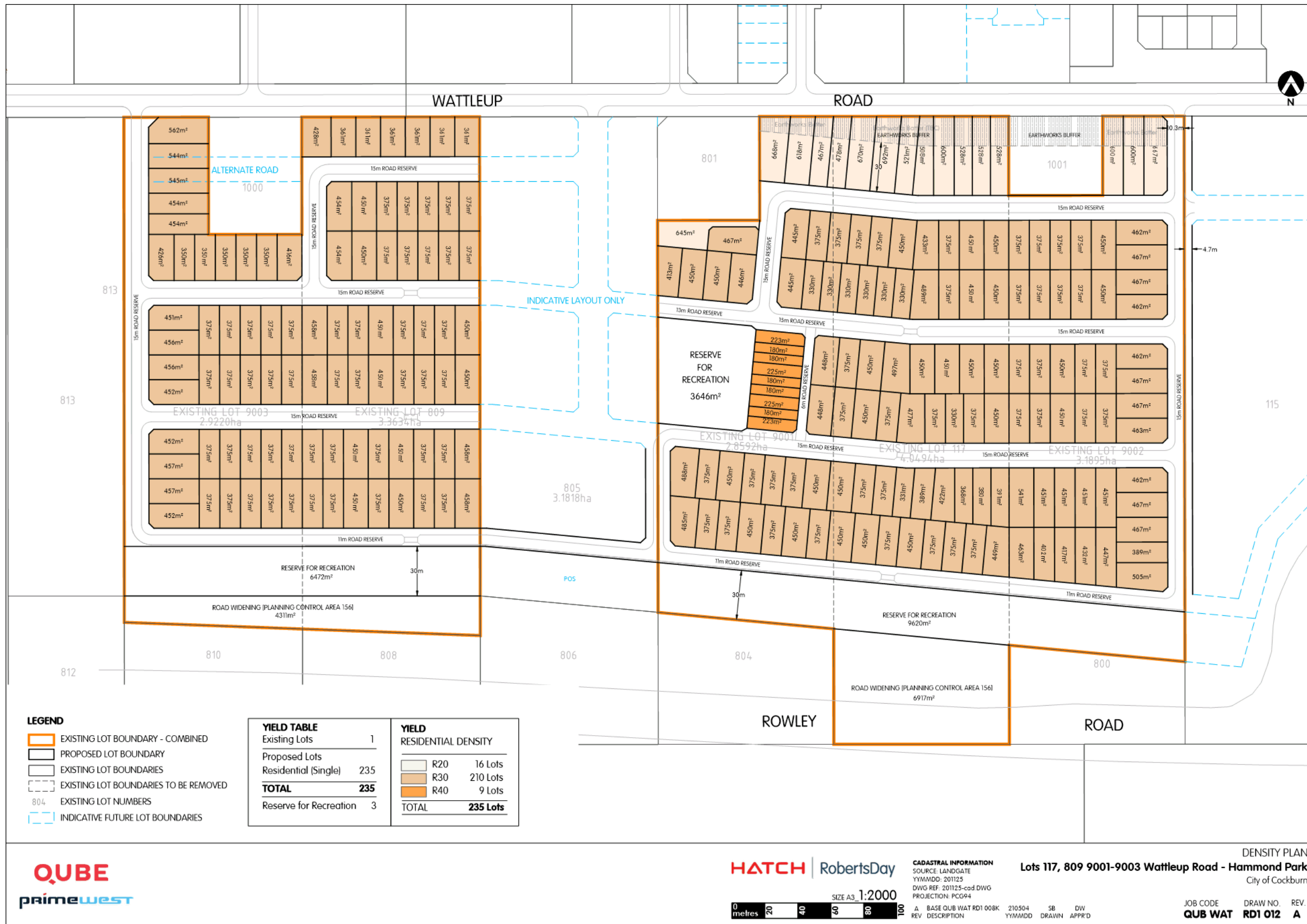
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I	CALCULATIONS TABLE	210323	TT	DW
REV	DESCRIPTION	YYMMDD	DRAWN	APPRD

MODIFIED PLAN OF SUBDIVISION  
**Wattleup Road - Hammond Park**  
 City of Cockburn

JOB CODE	DRAW NO.	REV.
<b>QUB WAT</b>	<b>RD1 008</b>	<b>J</b>

DISCLAIMER: ISSUED FOR DESIGN INTENT ONLY. ALL AREAS AND DIMENSIONS ARE SUBJECT TO DETAIL DESIGN AND SURVEY





HATCH | RobertsDay

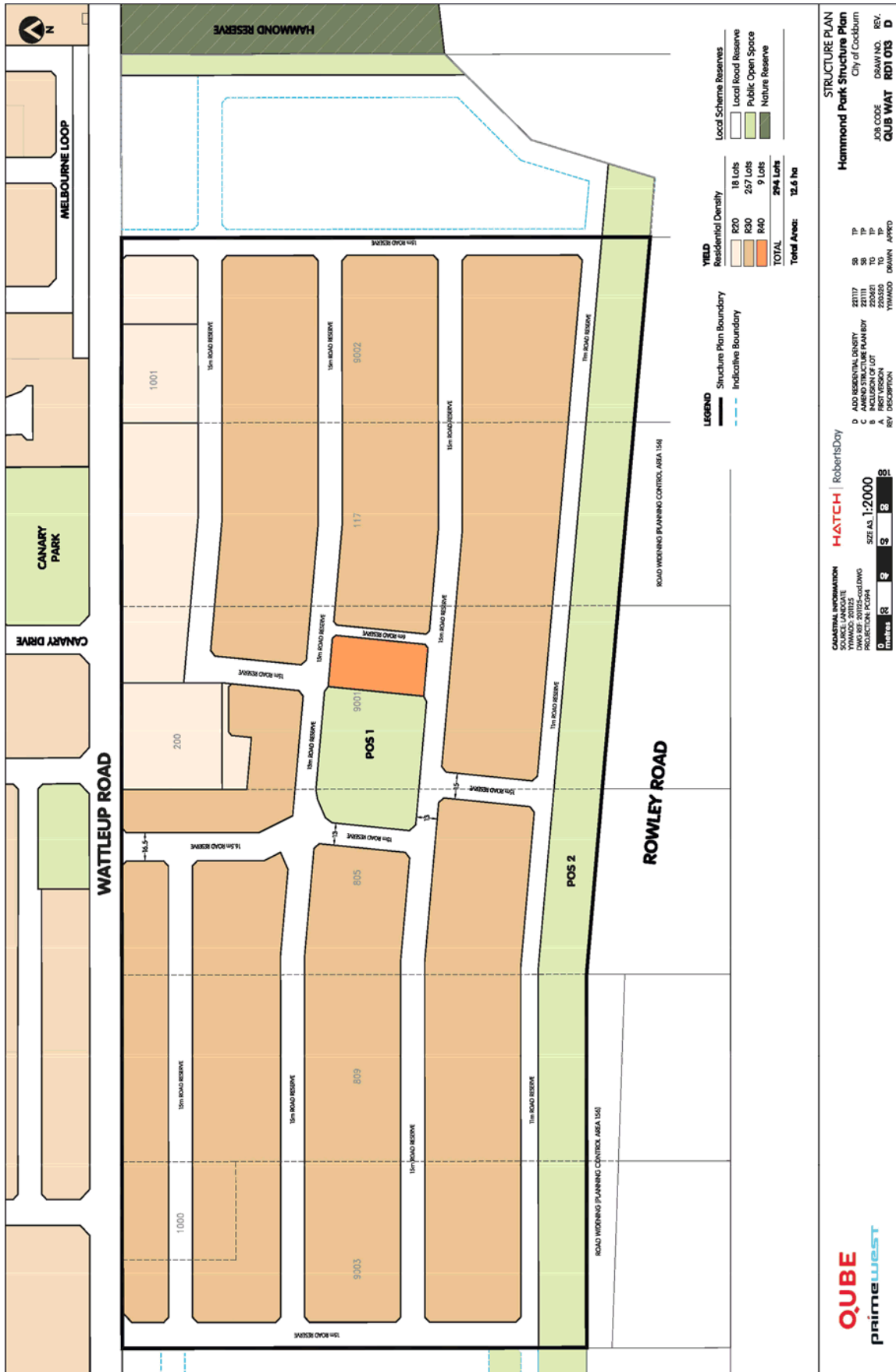
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 REV DESCRIPTION YYMMDD DRAWN APPR'D

**DENSITY PLAN**  
**Lots 117, 809 9001-9003 Wattleup Road - Hammond Park**  
 City of Cockburn

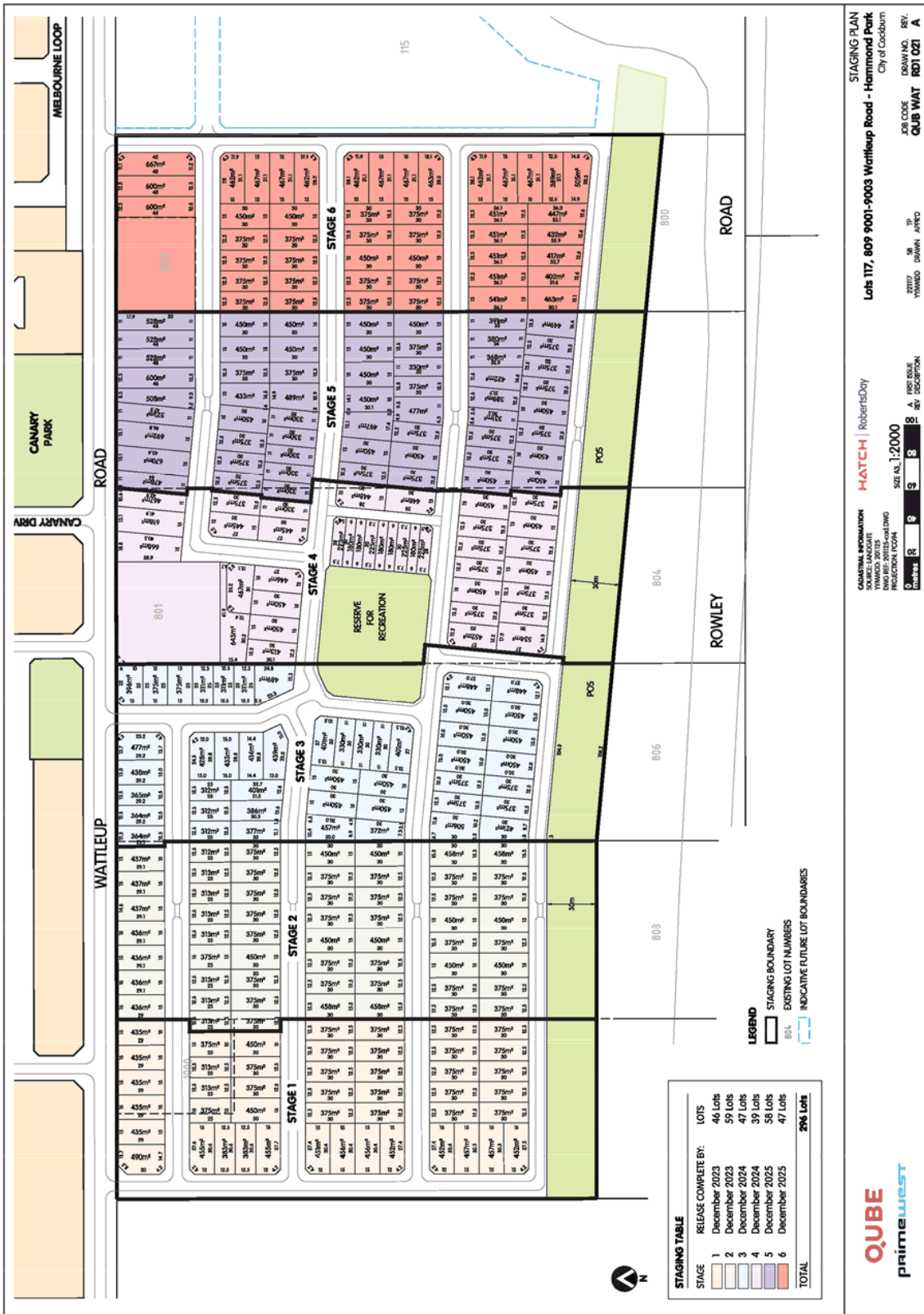


JOB CODE: QUB WAT  
 DRAW NO: RD1 012  
 REV: A

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Plan 1 - Structure Plan



STAGING PLAN  
 Lots 117, 809 9001-9003 Wattleup Road - Hammond Park  
 City of Cockburn  
 DRAIN NO. 151  
 SUB CODE QUB WAT  
 RDI 021 A

Geospatial Information  
 SOURCE: LANDSAT  
 DATE: 2015-01-08  
 PROJECT: COCKBURN  
 SCALE: 1:2000  
 DATE: 13/04/2023  
 DRAWN BY: [Signature]  
 CHECKED BY: [Signature]  
 APPROVED BY: [Signature]

**QUBE**  
 prime**west**  
 Plan 5 - Staging Plan

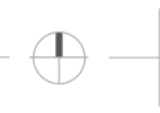



HAMMOND SOUTH  
 PREPARED FOR QUBE MANDOGALUP PTY LTD AND PRIMEWEST  
 LANDSCAPE MASTERPLAN  
 OCTOBER 2022

JOB NO. 2102804  
 1:1000 @ A1

M1.101  
 0 10 20 40 60 80 100m  
 REV G

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PLAN  
E

**LANDSCAPE ARCHITECTS**  
 414 ROKEBY RD SUBIACO WA 6008  
 T. (08) 9388 9566 E. mail@plane.com.au



- LEGEND**
- 01 CONCRETE FOOTPATH
  - 02 PLANTED BIORETENTION BASIN WITH WINTER WET NATIVE TREES AND PLANT SPECIES. 1:1 ARI, BASE AREA: 740 M2, VOLUME: 376 M3
  - 03 PLANTED DETENTION BASIN - 1:100 ARI, BASE AREA: 1640 M2, VOLUME: 2157 M3
  - 04 PASSIVE TURF AREA
  - 05 KICKABOUT TURF AREA
  - 06 INFORMAL PLAYGROUND EQUIPMENT IN MULCH
  - 07 SHELTER WITH TABLE SEATINGS & BBQ.
  - 08 NATIVE PLANTING WITH GROVES OF TREES
  - 09 ALL-AGES FITNESS EQUIPMENT NODE
  - 10 ACTIVE ZONE WITH HIT UP WALL, BASKETBALL HOOP, SOCCER AND COLOURED LINEMARKING
  - 11 SEATING NODE
  - 12 REFLECTIVE NODE - TERRACED TIMBER SEATING.
  - 13 TERRACED SEATING WITH CLIMBING OPPORTUNITIES
  - 14 TERRACED RETAINING WALL
  - 15 EARTHWORKS BATTER
  - 16 3M MAINTENANCE ACCESS (OFF SITE)
  - 17 OVERFLOW SPILLWAY FROM 1:1 TO 1:100 BASIN



FUTURE ROWLEY ROAD



HAMMOND SOUTH  
PREPARED FOR QUBE MANDOGALUP PTY LTD AND PRIMEWEST

LINEAR POS  
OCTOBER 2022

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1:800 @ A1



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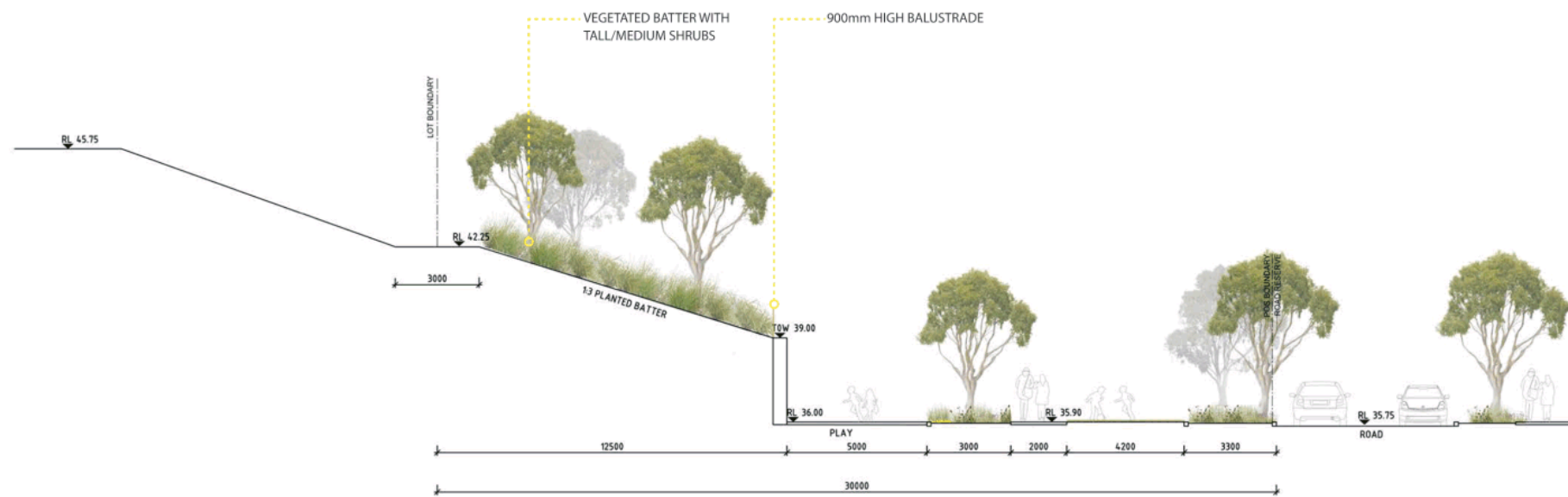
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SECTION A



SECTION B



HAMMOND SOUTH  
 PREPARED FOR QUBE MANDOGALUP PTY LTD AND PRIMEWEST  
 LINEAR POS - SECTIONS  
 OCTOBER 2022

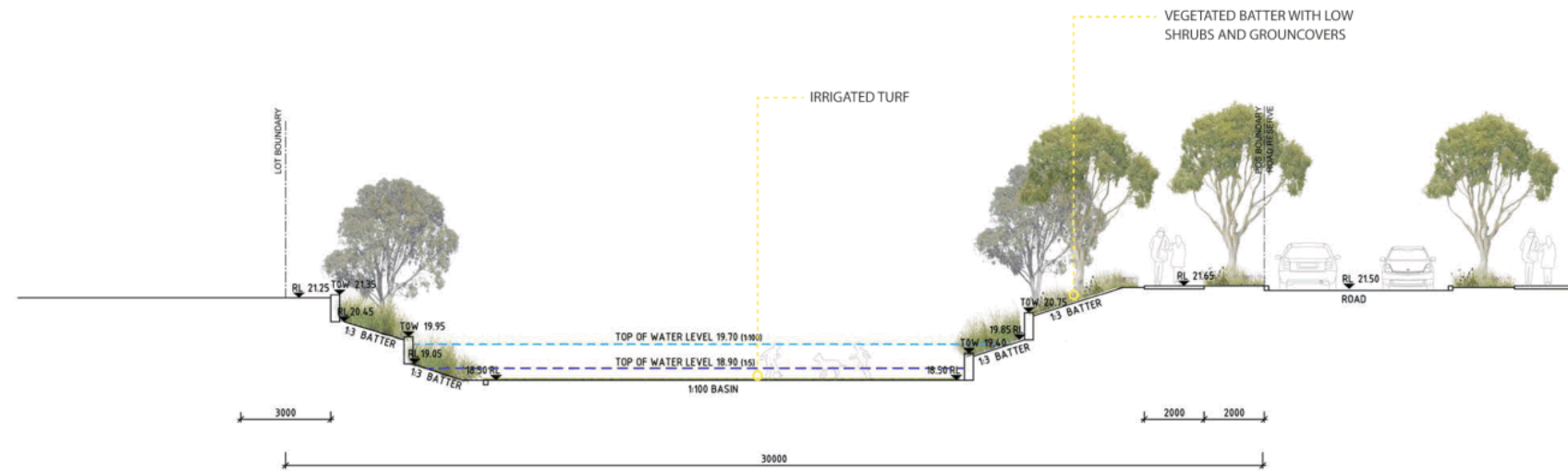
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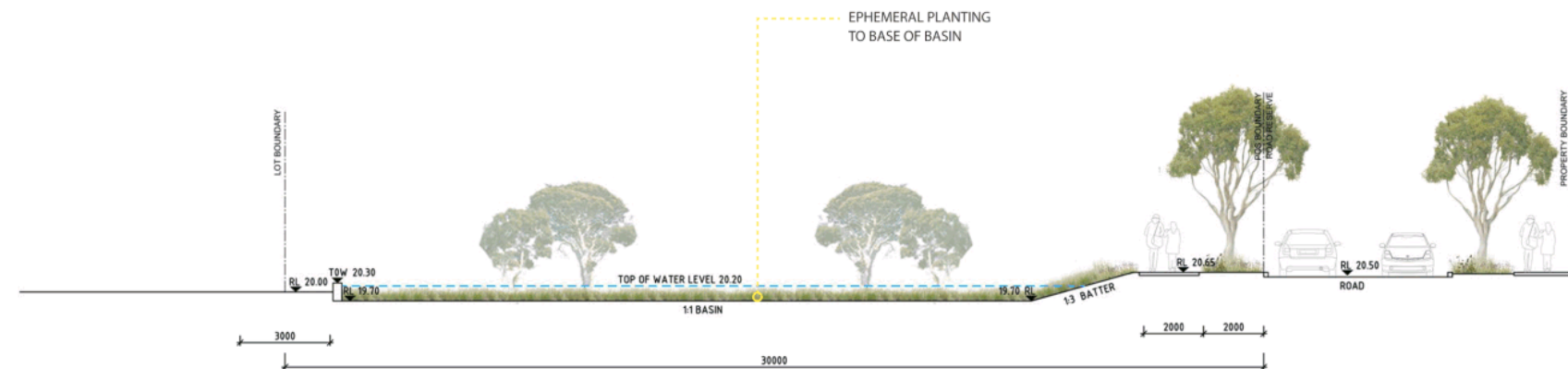
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### SECTION C



### SECTION D



HAMMOND SOUTH  
 PREPARED FOR QUBE MANDOGALUP PTY LTD AND PRIMEWEST  
 LINEAR POS - SECTIONS  
 OCTOBER 2022

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PLAN E

LANDSCAPE ARCHITECTS  
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- LEGEND**
- PROPOSED TREE PLANTING.
  - 01 FEATURE TREE PLANTING / VISUAL GREEN CONNECTION FROM ENTRY ROAD
  - 02 OPPORTUNITY FOR ENTRY / WAYFINDING ARTWORK
  - 03 TURF KICKABOUT AREA
  - 04 MOUNDED TURF
  - 05 SHADE STRUCTURE WITH SEATING, BBQ AND AMENITY
  - 06 PLAYGROUND EQUIPMENT IN SAND PIT
  - 07 TIERED RETAINING WALLS WITH PLANTING
  - 08 1:100 ARI DETENTION BASIN
  - 09 PLANTED 1:1 ARI BIORETENTION BASIN WITH WINTER-WET NATIVE TREES AND PLANT SPECIES.
  - 10 CULVERT AND HEADWALL TO CONNECT 1:1 AND 1:100 BASINS
  - 11 COMMUNITY ORCHARD
  - 12 CONCRETE FOOTPATH
  - 13 FEATURE CONCRETE PAVING
  - 14 PATH CONNECTION TO ADJACENT LOTS
  - 15 TREES AND PLANTING TO INTERFACE WITH OVERWATCH PRODUCT
  - 16 POS PARKING BAYS

HAMMOND SOUTH  
 PREPARED FOR QUBE MANDOGALUP PTY LTD AND PRIMEWEST  
 CENTRAL POS - CONCEPT PLAN  
 OCTOBER 2022

JOB NO. 2102804  
 1:200 @ A1

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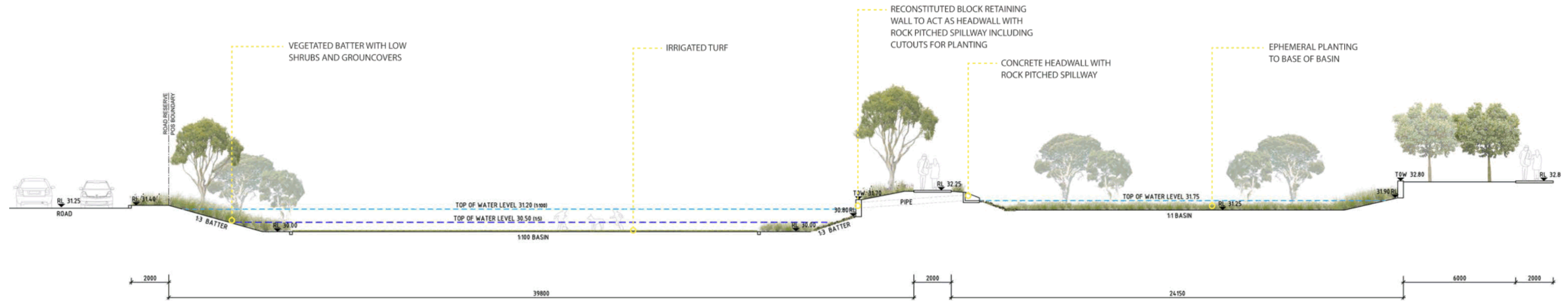
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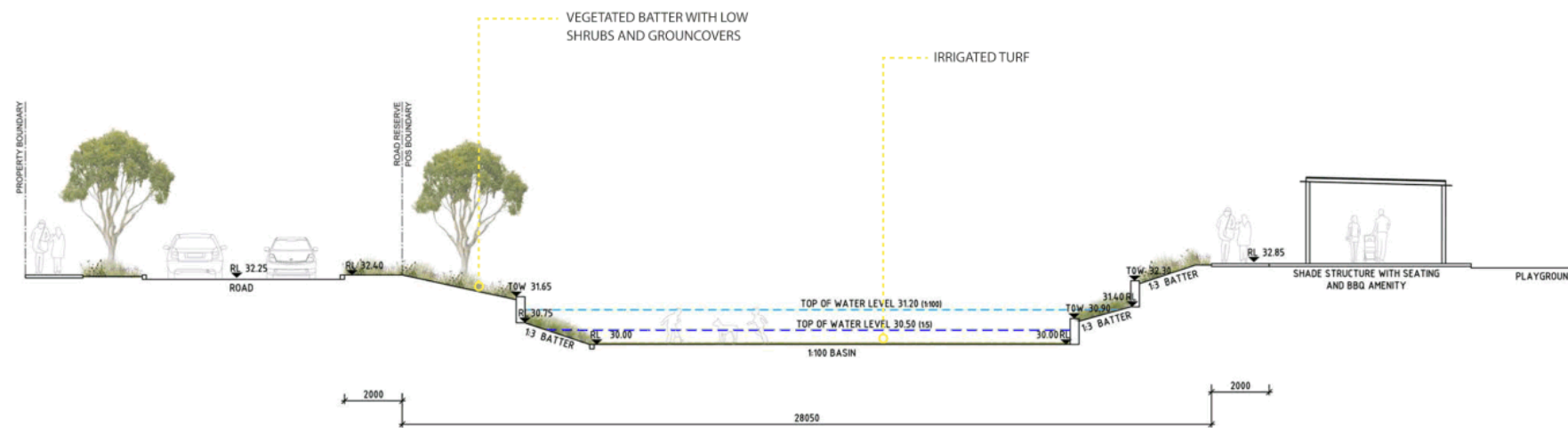
**LANDSCAPE ARCHITECTS**  
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 T: (08) 9388 9566 E: mail@plene.com.au



SECTION E



SECTION F



HAMMOND SOUTH  
 PREPARED FOR QUBE MANDOGALUP PTY LTD AND PRIMEWEST  
 CENTRAL POS - SECTIONS  
 OCTOBER 2022

JOB NO. 2102804  
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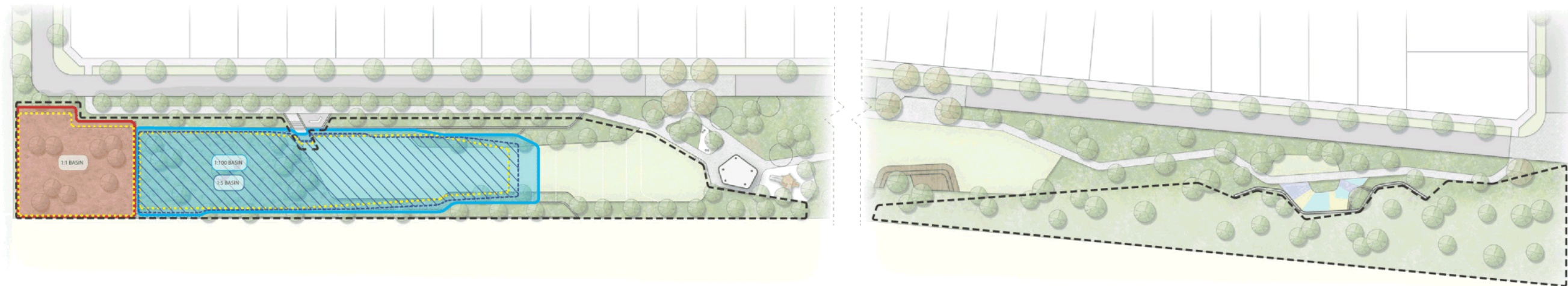
CENTRAL POS



POS CALCULATIONS

POS	TOTAL AREA (ha)	1:1 BASE AREA (ha)	1:100 BASE AREA (ha)	RESTRICTED 1:1 YEAR EVENT (ha)	RESTRICTED 1:5 YEAR EVENT (ha)	RESTRICTED 1:100 YEAR EVENT (ha)	EXTENT OF BATTERED AREA WITH SLOPE GREATER THAN 1:6 (ha)	USEABLE TURF AREA (HA) INCLUSIVE OF 1:6
Central POS	0.4935 (ex. Verge)	0.0315	0.0734	0.0422	0.0870	0.1165	0.0714	0.1738
Linear POS	1.9347 (ex. Verge)	0.0740	0.1640	0.0791	0.1828	0.2050	0.3625	0.2580

LINEAR POS



**LEGEND**

- BASIN BASE AREA
- RESTRICTED 1:1 YEAR EVENT
- RESTRICTED 1:5 YEAR EVENT
- RESTRICTED 1:100 YEAR EVENT
- BATTERED AREA WITH SLOPE GRATER THAN 1:6

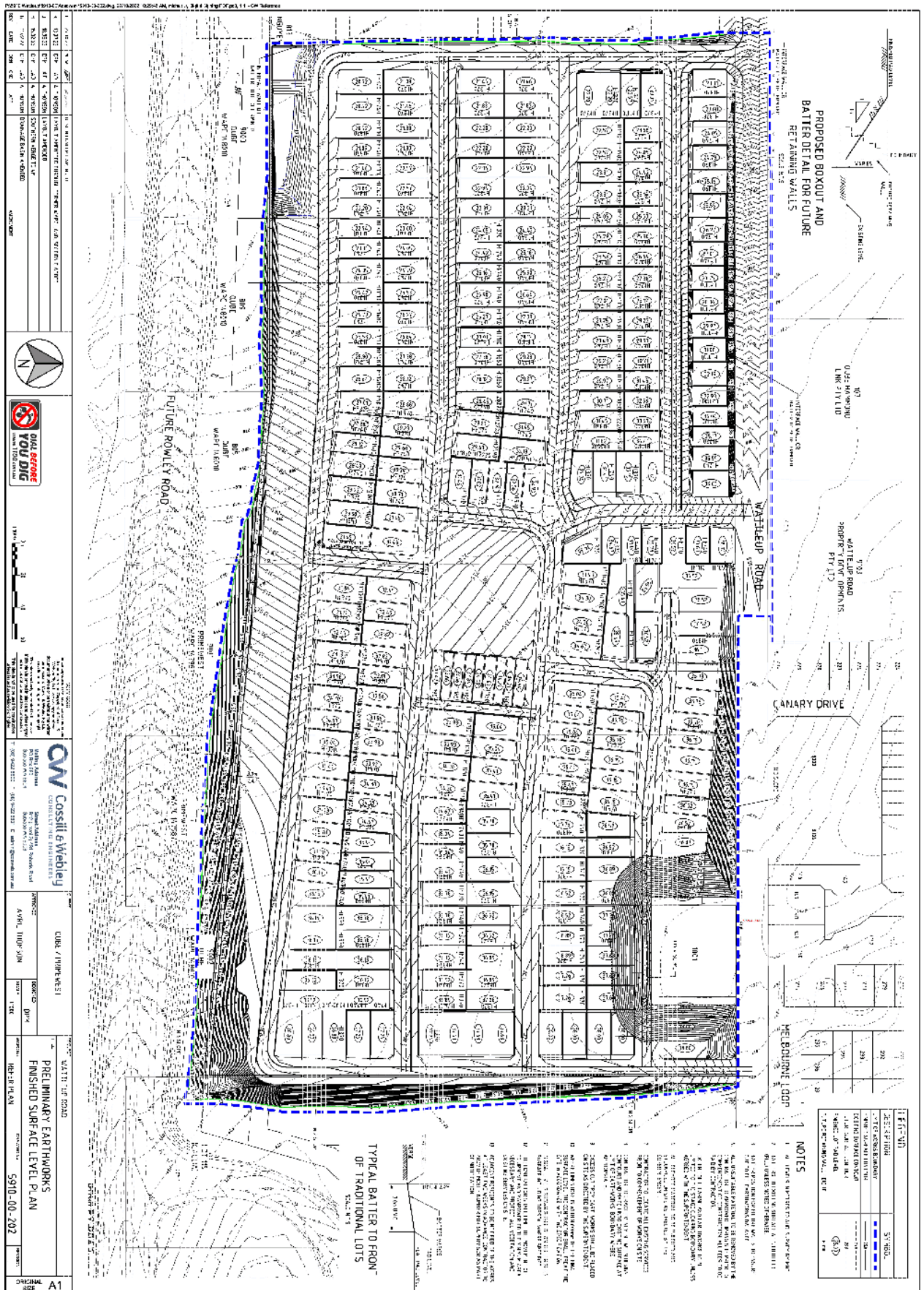
HAMMOND SOUTH  
 PREPARED FOR QUBE MANDOGALUP PTY LTD AND PRIMEWEST  
 POS RESTRICTED AREAS  
 OCTOBER 2022

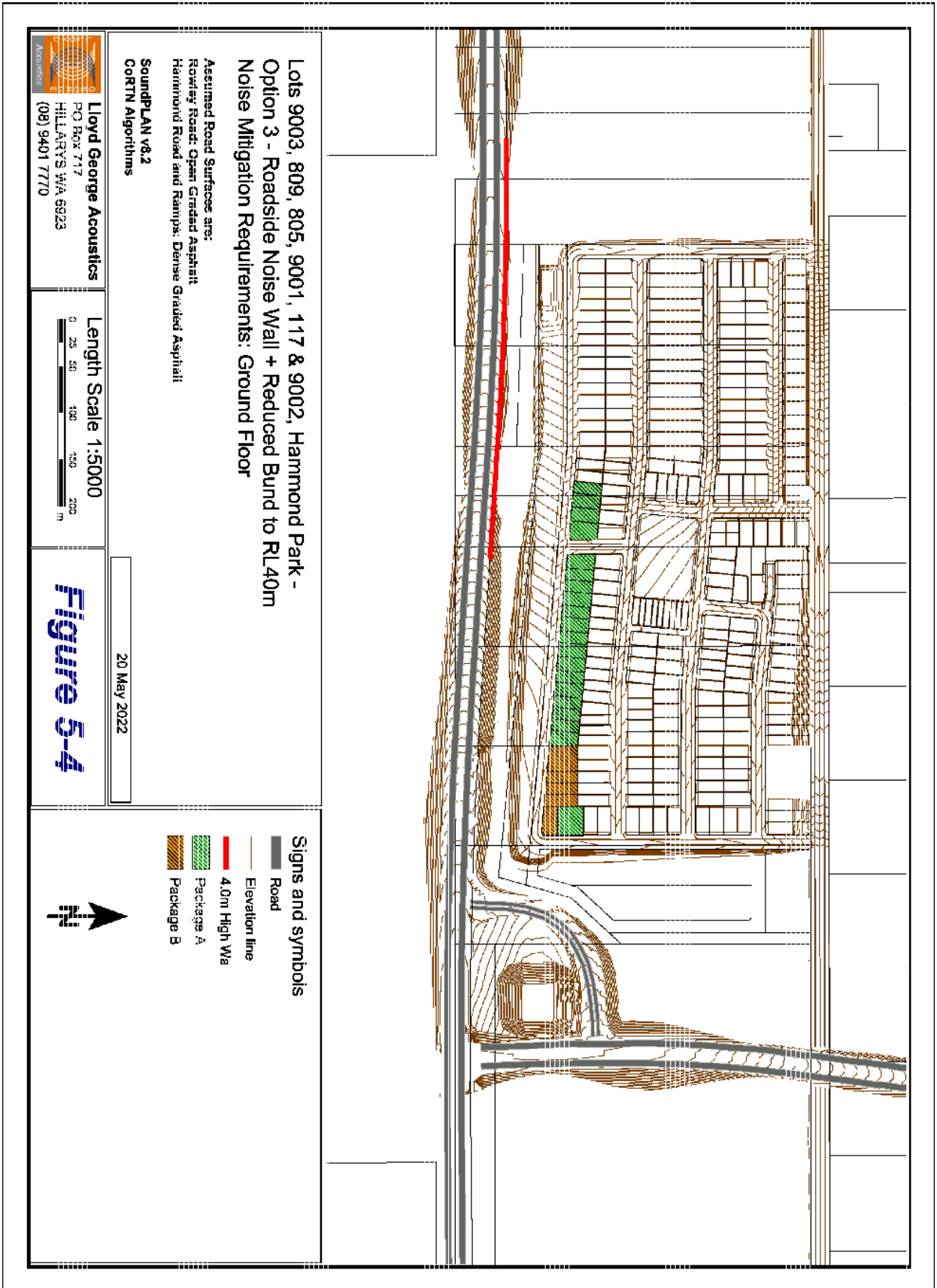
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File No. 110/241

**SCHEDULE OF SUBMISSIONS**  
**PROPOSED STRUCTURE PLAN: Hammond Quarter - Wattleup Rd, Hammond Park**

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
1	ATCO Gas Australia, Prinsep Rd, Jandakot	<p><b>NO OBJECTION:</b> ATCO Gas Australia (ATCO) has no objection to the proposed application, based on the information and plan provided.</p> <p>Advice notes:</p> <ol style="list-style-type: none"> <li>1. Anyone proposing to carry out construction or excavation works must contact 'Before You Dig Australia' (<a href="http://www.byda.com.au">www.byda.com.au</a>) to determine the location of buried gas infrastructure. Refer to ATCO document AGA-O&amp;M-PR24-Additional Information for Working Around Gas Infrastructure <a href="https://www.atco.com/en-au/for-home/natural-gas/wa-gas-network/working-around-gas.html">https://www.atco.com/en-au/for-home/natural-gas/wa-gas-network/working-around-gas.html</a></li> <li>2. Proposed construction and excavation works need to be managed in accordance with the ATCO document Additional Information for Working Around Gas Infrastructure - AGA-O&amp;M-PR24 <a href="https://www.atco.com/en-au/for-home/natural-gas/wa-gas-network/working-around-gas.html">https://www.atco.com/en-au/for-home/natural-gas/wa-gas-network/working-around-gas.html</a></li> </ol>	1. Noted.
2	Westport, William St Perth	<p><b>COMMENT:</b> Rowley Road is identified as a Primary Freight Route in the South Metropolitan Peel Sub-Regional Planning Framework and is intended to function as the substitute heavy vehicle access road for the future container port in Kwinana, providing resilience in the road freight network and maintaining accessibility during future Anketell Road construction works.</p> <p>Should the City and WAPC elect to support the Structure Plan, noise mitigation measures (including a noise wall, quiet house design measures and/or notifications on title as outlined in the</p>	<p>1. Main Roads WA has advised that the noise mitigation options in 'Option 3' of the Transportation Noise Assessment are considered acceptable. Option 3 relates to a 'shared responsibility' approach, whereby:</p> <ul style="list-style-type: none"> <li>• the developer will be responsible for constructing an earthworked bund on the south-east boundary of the site;</li> </ul>


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		<p>submitted Transportation Noise Assessment) should be required to be implemented by the proponent in accordance with State Planning Policy 5.4 via standard conditions as subdivision and development progresses.</p> <p>In relation to the technical requirements regarding noise mitigation, the Westport Office recommends that the suitability of the acoustic assessment be confirmed by MRWA prior to the determination of the Structure Plan.</p>	<ul style="list-style-type: none"> <li>• Main Roads constructing a 4m high acoustic wall to south-west of the site, adjacent to eastbound carriageway of the future Rowley Road extension;</li> <li>• Individual landowners implement quiet house design measures, where lots are forecast to exceed noise targets under <i>State Planning Policy 5.4 – Road and Rail Noise</i>; and</li> <li>• Notifications are placed on the certificate of title of lots, to advise landowners of the proximity to a future regional road and the likely noise impact.</li> </ul> <p>The TNA uses three modelling options based on different mitigation measures and responsibilities. To reflect that 'Option 3' (Shared Responsibility) is the approach accepted by Main Roads, it is recommended the TNA is modified based on the Option 3 modelling and mitigation measures only.</p>
3	Carl, Wattleup Rd, Hammond Park	<p><b>OBJECT:</b> Our main objection is based on vehicle traffic on Wattleup Road currently. This has now turned into a main road with all the industrial estate underway and expanding Eg , sand pits , earthworks disposal pits , truck holding yards , storage yards ,and the new Latitude 32 stage being built on Russel Road, also Recreational reserve Franklin road. Traffic has quadrupled ++ over the last 10 years (we have been here for over 25 years).</p> <p>When we moved here there was promise that Rowley Road extension would be in within 10 years. Speed is a major</p>	<ol style="list-style-type: none"> <li>1. The extension of Rowley Road is a State Government project under the management of Main Roads WA. It is understood Main Roads WA has not determined a date for the commencement of the Rowley Road extension.</li> <li>2. The City is aware of community concerns regarding the speed limit on Wattleup Road, particularly given the proximity to residential development. The City has previously</li> </ol>

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		<p>concern, although new 70km signs have been placed at the new Hammond west estate this doesn't slow anyone down. You can drive on Wattleup Road now and visually see the amount of skid marks at every new entry to estates. Over our time here there has been 7 deaths on Wattleup Road that we know of. Our cctv footage shows the dangers on this road (channel 7 has footage). With all the new entries to Wattleup Road this will create more deaths to come. So in the safety of everyone, we object to this plan. We would like to see plans put forward for Rowley road extension to be first in place before estates go into works The safety of peoples lives need to be accountable by someone, either the council / developers/ or main roads. We will set up a public petition for Rowley Road extension to be put in before anymore developments to Wattleup Road.</p>	<p>approached Main Roads WA to reduce the speed limit, where adjacent to existing residential development, but has been unsuccessful to date.</p> <p>Speeding beyond the posted limit is an individual behaviour that the City has no control over. Where speed exceeds permitted limits, this falls within the jurisdiction of WA Police under the applicable road safety legislation.</p>
4	Department of Mines, Industry Regulation and Safety, Plain St, East Perth	<p><b>NO OBJECTION:</b> The Department of Mines, Industry Regulation and Safety (DMIRS) has determined that this proposal raises no significant issues with respect to mineral and petroleum resources, geothermal energy, and basic raw materials.</p> <p>DMIRS lodges no objections to the above structure plan.</p>	1. Noted.
5	Dept of Water and Environmental Regulation, Mandurah	<p><b>COMMENT:</b> The Department has reviewed the proposed local structure plan and provides the following advice.</p> <p><b>Issue</b></p> <p>Better Urban Water Management</p> <p><b>Advice</b></p> <p>Consistent with <i>Better Urban Water Management (WAPC, 2008)</i> and policy measures outlined in <i>State Planning Policy 2.9: Water Resources</i>, the proposed structure plan should be supported by</p>	<p>1. It is noted that Part Two of the Structure Plan does not include commentary on <i>State Planning Policy 2.9 – Water Resources</i> (SPP 2.9). It is recommended that the Structure Plan is modified to include detail on how the urban water management plan (UWMP) prepared for the proposal demonstrates compliance with SPP 2.9.</p> <p>2. It is noted that Section 2.1 in Part Two of the Structure Plan refers to the application having been made under the</p>

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		<p>an approved Local Water Management Strategy (LWMS) prior to finalisation.</p> <p>The Department recently reviewed the <i>Wattleup Road, Hammond Park: Lots 9003, 805, 809, 9001, 117 and 9002 Urban Water Management Plan (Rev 3)</i> (Environnivate, 15 November 2022) in lieu of a Local Water Management Strategy. This report was considered adequate, subject to minor amendments, to inform the structure plan. Please refer to the Department's comments, in correspondence dated 12 January 2023, provided in Attachment 1.</p> <p>The Department has no objection to the proposed structure plan proceeding, subject to the recommended amendments in Attachment 2.</p> <p>In the event there are modifications to the structure plan that may have implications on aspects of environment and/or water management, the Department should be notified to enable the implications to be assessed.</p> <p><b>Attachment 2:</b></p> <table border="1" data-bbox="624 842 1330 1230"> <thead> <tr> <th data-bbox="624 842 750 895">Item No.</th> <th data-bbox="750 842 936 895">Reference</th> <th data-bbox="936 842 1330 895">Reviewer comment/advice</th> </tr> </thead> <tbody> <tr> <td data-bbox="624 895 750 1230">1</td> <td data-bbox="750 895 936 1230">s1.4.4 (Pg 19)</td> <td data-bbox="936 895 1330 1230"> <p><b>Issue:</b> Planning Policies and Statements</p> <p><b>Recommendation/Advice:</b> Please include reference to State Planning Policy 2.9 Water Resources.</p> <p><b>Discussion:</b> In accordance with SPP2.9, water is to be considered in all stages of planning. The proponent's UWMP has been</p> </td> </tr> </tbody> </table>	Item No.	Reference	Reviewer comment/advice	1	s1.4.4 (Pg 19)	<p><b>Issue:</b> Planning Policies and Statements</p> <p><b>Recommendation/Advice:</b> Please include reference to State Planning Policy 2.9 Water Resources.</p> <p><b>Discussion:</b> In accordance with SPP2.9, water is to be considered in all stages of planning. The proponent's UWMP has been</p>	<p>Commonwealth <i>Environmental Protection and Biodiversity Conservation Act 1999</i> to clear native vegetation on Lot 809 Wattleup Road (now Lot 803). The Commonwealth has now approved the clearing of 3.03 hectares of Banksia Woodland on the site. It is recommended that Section 2.1 is modified to note the proponent has obtained approval to clear this vegetation.</p> <p>It is noted that the EPBC Act approval for clearing of vegetation on Lot 803 does not exempt the need to obtain a clearing permit under the (State) <i>Environmental Protection Act 1986</i>. It is anticipated that a clearing permit exemption will be obtained under Schedule 6, Clause 9 of the EP Act once a subdivision approval is obtained (noting Lot 803 is not subject to the current Ministerial approval). It is recommended that Section 2.1 is also modified to note that a State clearing permit or exemption will still be required.</p> <p>3. The references to 'Table 5' and 'Figure 6' are in reference to the proposed UWMP.</p> <p>Cross-sections of the proposed drainage basins are included in Appendix C – Landscape Masterplan. Inclusion of the cross-sections in Part Two of the Structure Plan is not considered necessary.</p>
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1	s1.4.4 (Pg 19)	<p><b>Issue:</b> Planning Policies and Statements</p> <p><b>Recommendation/Advice:</b> Please include reference to State Planning Policy 2.9 Water Resources.</p> <p><b>Discussion:</b> In accordance with SPP2.9, water is to be considered in all stages of planning. The proponent's UWMP has been</p>							



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			included within the Structure Plan for this purpose.	
		2	<p>s2.1 (Pg 23)</p> <p><b>Issue:</b> Native Vegetation</p> <p><b>Recommendation/Advice:</b> Please include reference to the Notification of Approval under the Environment Protection and Biodiversity Conservation Act (EPBC) 1999 from the Department of Climate Change, Energy, the Environment and Water, as per Appendix A of the UWMP.</p> <p>As subdivision approval has been received under Ministerial appeal, exemptions apply under the Environmental Protection Act 1986.</p> <p><b>Discussion:</b> Please note that an approval under the EPBC Act does not preclude the requirement for approvals under the Environmental Protection Act 1986. Schedule 6 exemptions apply as subdivision approval has already been granted.</p>	
		3	<p>s3.5 (Pg 29)</p> <p><b>Issue:</b> Water Management</p> <p><b>Recommendation/Advice:</b> Please include summary details of the proposed water storage or reference the UWMP.</p>	

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			<p><b>Discussion:</b> The text references Table 5, which has been omitted from the report. Please include a figure of the drainage basins. Figure 6 is incorrectly referenced.</p>									
6	Indara Digital Infrastructure (Axicom), Eight Mile Plains, Qld	<p><b>COMMENT: Existing Infrastructure</b></p> <p>There are four existing telecommunications facilities within approximately 3km of the structure plan area. Information is sourced from the RFNSA archive.</p>  <p><i>Figure 1: Location of Existing Telecommunications Facilities in vicinity of Hammond Quarter (RFNSA &amp; Google Earth)</i></p> <table border="1" data-bbox="631 1054 1314 1233"> <thead> <tr> <th>RFNSA ID</th> <th>Address</th> <th>Tower Owner</th> <th>Carriers</th> </tr> </thead> <tbody> <tr> <td>6167007</td> <td>10 Rowley Rd. Mandogalup</td> <td>Amplitel</td> <td>Telstra (4G &amp; 5G) Optus and TPG Vodafone not currently present but</td> </tr> </tbody> </table>		RFNSA ID	Address	Tower Owner	Carriers	6167007	10 Rowley Rd. Mandogalup	Amplitel	Telstra (4G & 5G) Optus and TPG Vodafone not currently present but	<p>1. It is acknowledged that Indara Digital Infrastructure (Indara) has identified future mobile telecommunications infrastructure upgrades, as a result of residential development in Hammond Park and the surrounding localities.</p> <p>Whilst there is no planning or legislative requirement for telecommunication providers to provide an adequate level of service, it is encouraging that future upgrades are foreshadowed.</p> <p>Any new infrastructure or upgrades to mobile telecommunications infrastructure may require future development approval, except where works are otherwise exempted under the Commonwealth <i>Telecommunications Act 1997</i>.</p> <p>Further to Indara's comments, Cossill and Webley (acting on behalf of Hatch Roberts Day) have received additional comment from Indara. These comments advise that Indara has no plans to install mobile telecommunications infrastructure within the Structure Plan area.</p>
RFNSA ID	Address	Tower Owner	Carriers									
6167007	10 Rowley Rd. Mandogalup	Amplitel	Telstra (4G & 5G) Optus and TPG Vodafone not currently present but									

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				showing as 'proposed' on RFNSA		
		6166002	Lot 30 Wattleup Rd, Wattleup	Indara	Optus (4G & 5G) Telstra (4G &5G)	
		6166009	Lot 30 Wattleup Rd, Wattleup	Wavecon n	TPG Vodafone (4G & 5G)	
		6164004	Gaebler Rd, Aubin Grove	Indara	Optus (4G & 5G) Telstra (4G & 5G) TPG Vodafone (4G&5G)	
<p><b><u>Indara Deployment Activities</u></b></p> <p>Indara does not have any current proposals in the vicinity of Hammond Quarter. However, Indara is intending to deploy several new sites within the wider district.</p> <ul style="list-style-type: none"> <li>• The closest new Indara facility is located at 158 Tindal Avenue, Beeliar (our reference “3400950 Yangebup South”). This facility is being rolled out in conjunction with both Optus and TPG Vodafone. A DA has been submitted and is currently being assessed by City of Cockburn (DA22/0860). This facility will be approximately 5km northwest of Hammond Quarter and will not directly service this development.</li> <li>• Indara, working with Optus, will shortly commence scoping activities for a potential new facility along the Kwinana Freeway, south of Rowley Road. This site is in its early stages and no candidates have been identified.</li> <li>• Indara, working with TPG Vodafone, will also shortly commence scoping for a potential new facility in the large commercial area around Success. Again, this site is in its early stages and no candidates have been identified.</li> </ul>						

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		<p>Having assessed demand in the Wattleup Road area, Indara also intends to further investigate the feasibility of a new site specifically servicing new development in this area. Further detail is below.</p> <p><b><u>Future Need</u></b></p> <p>The applicant reports that Telstra and Optus have no immediate need for new facilities in this area (Optus intending to co-locate on RFNSA 6167007 in March 2024). TPG Vodafone did not provide comment.</p> <p>Indara takes a long-term view toward mobile infrastructure provision. Our network planning team has considered population growth along Wattleup Road, and are of the view that additional mobile infrastructure should be considered to future proof service for local residents.</p> <p>The Hammond Quarter development will result in approximately 296 new residences. While this development alone will increase local mobile network traffic, it should also be considered in the context of other developments in the area including:</p> <ul style="list-style-type: none"> <li>• Large scale developments either complete, or currently underway, north of Wattleup Road and east of Frankland Avenue (forming part of the Southern Suburbs District Structure Plan area).</li> <li>• The recently approved Hammond Park West Structure Plan (260 residential lots).</li> </ul> <p>Per Figure 1, the existing base stations at Lot 30 Wattleup Road and 10 Rowley Road are approximately 3.4km apart. These are the only existing base stations in the area, with no existing facilities directly north or south of Wattleup Road.</p> <p>In suburban areas, mobile base stations generally only provide a reliable coverage footprint of 1-2 kilometres, depending on a</p>	

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		<p>variety of factors including population density, number and type of users, and physical factors such as the height of facility and local terrain.</p> <p>Outside of this 1-2km radius the facility will still provide service, but it may be lower quality and less reliable. Users may experience call dropouts, slow data speeds, or be unable to use their phone indoors.</p> <p>While service may now be adequate, Indara’s assessment is that existing facilities will not be able to fully service the community as the area develops and demand increases – especially at the outer fringe of the existing base stations’ coverage footprint. It is our view that an additional site will be needed within the next 5 years to accommodate network traffic increases.</p> <p>As City of Cockburn will be aware, it can be difficult to ‘retrofit’ a mobile base station into an established community once it has been developed. It is much easier to deploy a mobile base station before an area is developed.</p> <p>Noting the growth expected in the Wattleup Road area, we are of the view that an additional facility is justified. Indara intend to further assess the need for service, and explore potential opportunities for an additional site, in this area.</p> <p><b><u>Recommendations &amp; Next Steps</u></b></p> <p>Indara make the following comments in respect of the Hammond Quarter plan, and development in the Wattleup Road area more generally.</p> <ul style="list-style-type: none"> <li>• In general, we encourage Council to consider future need for telecommunications where approving structure plans in this area.</li> </ul>	

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		<ul style="list-style-type: none"> <li>• Indara consider that there will be a future need for mobile infrastructure in this area, to handle increasing mobile traffic and ensure adequate coverage.</li> <li>• Indara will further assess need for an additional facility in this area. We also intend to scope the Wattleup Road area for potential sites capable of servicing future residential development. Scoping activities will commence shortly.</li> <li>• We would like to partner with City of Cockburn in servicing this community. We would be particularly interested in discussing the availability of Council owned land in this area where a mobile facility may be suitable.</li> </ul> <p>Indara own and manage over 4300 mobile base stations across Australia. Our core mission is to accelerate a digitised future for Australian communities. Our facilities are designed specifically to support co-location, and our network boasts a higher than industry standard co-location rate compared with other infrastructure providers. By encouraging co-location, we reduce the number of towers that are needed in a specific area, minimising amenity impacts for local communities.</p> <p>Indara can also offer significant expertise in smart pole and small cell deployment. Indara recently acquired ENE-HUB, a major provider of smart poles and other small telecommunications infrastructure. These solutions integrate particularly well in parks and open space because of their lower profile design. These facilities cannot replicate the service of a full-size base station, but they can augment and bolster local network capacity whilst presenting a smaller visual impact.</p> <p>Noting the significant development occurring in City of Cockburn, we would welcome the opportunity to work more closely with Council in delivering service to your communities.</p>	

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		<p>We would be interested in discussing whether there is Council owned land available for telecommunications infrastructure, and whether there is capability for the City to reserve space on new parks for small cell infrastructure – especially where parkland or open space is being vested to Council upon completion of the development.</p>	
7	Water Corporation, Leederville, WA	<p><b>COMMENT: Water</b></p> <p>As noted in the servicing report, the water network is constrained to a limit of supply of 38mAHD. Any lots located above this level will experience low pressure and would be unable to be serviced. Earthworks will be required to ensure all lots meet this limit. If this is not able to be achieved the applicant should contact us to discuss alternative options.</p> <p><b>Sewer</b></p> <p>The Eastern portion of the subject area can be connected into the existing sewer network in Jennings St via extensions of the reticulation network. The Western Portion will require the delivery of Thomsons Lake PS G, this is currently scheduled to be completed by December 2023.</p> <p><b>Drainage</b></p> <p>The subject area is located outside the Water Corporations drainage catchment.</p> <p>The developer is expected to provide all water and sewerage reticulation. A contribution for Water, Sewerage and Drainage headworks will also be required.</p> <p>The information provided above is subject to review and may change. If the proposal has not proceeded within the next 6 months, please contact us to confirm that this information is still valid.</p>	<ol style="list-style-type: none"> <li>1. It is acknowledged that future lots will be limited to a finished level of 38m AHD in order for Water Corporation to provide a reticulated water supply to lots. The Preliminary Earthworks Plan provided for the Structure Plan demonstrates no lots will exceed 38m AHD.</li> <li>2. The Water Corporation has confirmed that the reticulated sewer network to the entire Structure Plan area will be dependent upon the delivery of the Thomsons Lake Pump Station F, due for completion by December 2023. It is anticipated that titles for new residential lots, including those covered by the existing Ministerial subdivision approvals, will not be issued until this pump station is operational.</li> </ol> <p>As the Water Corporaton advise that the planned Pump Station F includes the entire Structure Plan area, it is recommended that Figure 7 in Appendix B – Engineering Report is replaced with Water Corporation’s current sewer catchment plan.</p>

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8	Telstra Australia	<p><b>COMMENT:</b> The subject area is covered by our Mandogalup mobiles site, therefore this development of 296 dwellings is able to be absorbed by our existing Wireless network.</p> <p>However, additional development work in this area of this size will necessitate a review.</p>	1. Noted.
9	Department of Fire and Emergency Services, Perth	<p><b>COMMENT:</b> This advice relates only to State Planning Policy 3.7 Planning in Bushfire Prone Areas (SPP 3.7) and the Guidelines for Planning in Bushfire Prone Areas (Guidelines). It is the responsibility of the proponent to ensure the proposal complies with all other relevant planning policies and building regulations where necessary. This advice does not exempt the applicant/proponent from obtaining necessary approvals that may apply to the proposal including planning, building, health or any other approvals required by a relevant authority under other written laws.</p> <p><b>Recommendation – compliant application</b></p> <p>At the structure planning stage, consideration should be given to the intensification of land use and how this relates to identified bushfire hazards. DFES is satisfied that the bushfire hazard level assessment has adequately identified the bushfire risk and considered how compliance with the bushfire protection criteria can be achieved at subsequent planning stages.</p>	<p>1. Noted.</p> <p>Notwithstanding the advice provided by the Department of Fire and Emergency Services, the City accepts the Bushfire Management Plan prepared for the Structure Plan. On this basis, endorsement of the BMP is recommended without modification.</p>
10	Department of Health, Perth WA	<p><b>COMMENT:</b> The DoH provides the following comment:</p> <p><b>1. Water Supply and Wastewater Disposal</b></p> <p>The development is required to connect to scheme water and reticulated sewerage and be in accordance with the Government Sewerage Policy 2019.</p> <p>Potable water must be of the quality as specified under the Australian Drinking Water Quality Guidelines 2011.</p>	<p>1. Noted. Residential lots will be required to be connected to scheme water and reticulated sewerage through future conditions of subdivision approval.</p> <p>2. Whilst it is acknowledged that the lots within the Structure Plan area have been subject to previous agricultural uses, the site is not a registered Contaminated Site under the <i>Contaminated Sites Act 2003</i>. Should</p>



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		<p><b>2. Public Health Impacts</b></p> <p>The site (20ha) comprises the northern parts of five former market gardening lots used for intensive agriculture (3), plantation/orchards (1) and intensive livestock (chicken) production (1) and one Lot retained as remnant bushland. The site is to be rezoned and redeveloped for residential uses. Similar market garden land to the north and west along Wattleup Road have been rezoned and subdivided for residential redevelopment in recent years. The DoH is aware that market gardening/intensive agricultural land uses are a potentially contaminative land uses and associated with soil and groundwater contamination including agrochemicals, pesticides, fuel oils and asbestos containing materials and the previously rezoned Lots have been subject to extensive investigation, assessment and remediation works prior to residential redevelopment. Therefore, the DoH recommends that an appropriate condition be applied to planning/subdivision approval of the current site requiring the investigation, risk assessment and remediation of soil and groundwater contamination, conducted by a suitable competent consultant and supervised by an independent contaminated sites auditor.</p> <p>The DoH is aware that the site lies immediately adjacent to the east of “Best Buy Steel” which produces steel sheeting, pipework and fascia, and fibreglass and polycarbonate sheets for the construction industry. It is not clear whether this facility uses or stores hazardous chemicals (solvents, paints, resins etc), and the proponent should assess whether any emissions from this facility pose a risk to public health.</p> <p>In addition, the site lies 180m north of a significant sand quarry (Mandogalup Sand Pit) producing 150,000m<sup>3</sup> pa of building sand. The quarry is regulated under Part IV of the Environmental Protection Act 1986 and subject to a dust monitoring and management plan. EPA2005 ‘Separation</p>	<p>asbestos containing materials be found during subdivisional works, it is expected that such materials will be notified to the relevant authorities and the site remediated in accordance with the relevant legislative requirements.</p> <p>3. The ‘Best Buy Steel’ facility, located on Lot 810 Wattleup Road, has a current temporary development approval for a Storage use, which will lapsed on 30 June 2023. Given the proximity of the site to future residential development, the City is unlikely to consider any further extensions to this approval.</p> <p>It is anticipated that this use will cease prior to residential development being occupied in the Structure Plan area, therefore these activities are not considered to pose a public health risk.</p> <p>4. The City of Kwinana has confirmed that that there are two current Extractive Industry development approvals which cover Lot 10 Rowley Road and Lots 501 &amp; 615 Mandogalup Road, Mandogalup, which also have required approval under the Metropolitan Region Scheme from the Western Australian Planning Commission.</p> <p>The City acknowledges that current mining activities on Lot 10 Rowley Road, Mandogalup are located within 500 metres from future lots within the Hammond Quarter Structure Plan, with it being likely</p>

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		<p>Distances between Industrial and Sensitive Land Uses' recommends a minimum separation distance for sand extraction of 300-500m depending on size to mitigate dust and noise impacts. The current submission does not include any assessment of the impacts and public health risks from dust or noise emissions from this facility.</p> <p>The DoH advises that development should not proceed until the site is demonstrably suitable for that use.</p> <p><b>3. Increased Density – Public Health Impacts</b></p> <p>The City of Cockburn should also use this opportunity to minimise potential negative impacts of the increased density development such as noise, odour, light and other lifestyle activities. Public health impacts draw attention to those issues, and they should be appropriately and adequately addressed at this stage.</p> <p>To minimise adverse impacts on the residential component, the City of Cockburn could consider incorporation of additional sound proofing/insulation, double glazing on windows, or design aspects related to location of air conditioning units and other appropriate building/construction measures such as ensuring adequate ventilation requirements for wet areas.</p> <p><b>4. Medical Entomology</b></p> <p>The subject land is in a region that regularly experiences significant problems with nuisance and disease carrying mosquitoes. These mosquitoes can disperse several kilometres from breeding sites and are known carriers of Ross River Virus (RRV) and Barmah Forest Virus (BFV) v. Human cases of RRV and BFV diseases occur annually in this general locality.</p> <p>The subject land is also within 3km of mosquito dispersal distance from mosquito breeding sites at Thomson's Lake Reserve and Harry Waring Marsupial Reserve. Mosquitoes will</p>	<p>that subdivision occurs whilst sand mining is still occurring.</p> <p>It is acknowledged that the Structure Plan is located within an area zoned for residential development and is partially subject to an existing (Ministerial) subdivision approval which approval residential subdivision.</p> <p>In accordance with <i>State Planning Policy 5.3 – Basic Raw Materials</i>, it is considered that a notification on title to advise landowners of potential amenity impacts (dust and noise) should be imposed on future subdivision.</p> <p>5. Amenity issues relating to noise, odour and dust can occur in all residential areas, regardless of the residential density.</p> <p>Whilst the City encourages the use of innovative construction and design solutions to reduce the impact of noise and improve visual amenity, the City is unable to compel landowners to go beyond minimum planning and building requirements.</p> <p>6. The City is not aware of any mosquito issues experienced within the Structure Plan area (or surrounding areas), as a consequence of the site's proximity to potential breeding sites at the Thomson's Lake Reserve and Harry Waring Marsupial Reserve.</p> <p>The proposed POS within the Structure Plan area will include permanent drainage to detain stormwater from the road network</p>

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		<p>disperse from these sites to the subject land under favourable environmental conditions. There may also be seasonal freshwater mosquito breeding habitat within proximity to the subject land. Additionally, there is the potential for mosquitoes to breed in on-site infrastructure and constructed water bodies if they are poorly designed.</p> <p>The above disease risks, as well as the lifestyle impacts of nuisance mosquitoes, will inevitably result in demands for the application of chemicals to control larval and/or adult mosquitoes. Environmental agencies may not automatically approve the use of such measures in and around environmentally significant wetlands. Therefore, it will be important that in-principle approval for effective mosquito control measures in and around these wetlands is obtained from the relevant environmental agencies before planning decisions are finalised.</p> <p>Prior to development, the DoH recommends a mosquito management plan be developed and approved by both the DoH and the local government in which the proposal is based to ensure the risk to the community of exposure to nuisance and/or disease carrying mosquitoes is considered. This Mosquito Management Plan (MMP) is to be approved by the local government and the DoH prior to any subdivision.</p> <p>The DoH has provided guides and templates for the development of suitable MMPs to assist land developers meet these requirements.</p> <p>Please see: Mosquito management (<a href="http://health.wa.gov.au">health.wa.gov.au</a>) for additional support.</p> <p>In addition, due to the high-risk nature of the proposed development, the DoH requires the following wording to be placed on all land title documents:</p>	<p>up to 1:1 year event, however, these drainage reserves are not anticipated to support mosquito breeding.</p> <p>The City is not supportive of a Mosquito Management Plan being prepared as a condition of subdivision approval, as there is no evidence to suggest ongoing management is required.</p> <p>7. The availability of the 'Land Use Planning for Disaster Resilient Communities' document, published by the Australian Institute for Disaster Resilience, is noted.</p>

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		<p>"This lot is located near extensive mosquito breeding habitat and can experience substantial numbers of nuisance mosquitoes after certain environmental conditions. The mosquito species in the region are known vectors of RRV and other mosquito-borne diseases and the region is subject to annual outbreaks of these diseases."</p> <p><b>5. Land Use Planning for Natural Hazards</b></p> <p>A document 'Land Use Planning for Natural Hazards' can also guide the use of land to effectively reduce risk and enhance sustainability for areas prone to hazards such as flooding (including storm surge), fire, landslide, earthquake, strong wind and erosion. Available for downloading from:</p> <p><a href="https://knowledge.aidr.org.au/resources/land-use-planning-for-natural-hazards-handbook/">https://knowledge.aidr.org.au/resources/land-use-planning-for-natural-hazards-handbook/</a></p>	
11	Dept of Education, Perth WA	<p><b>COMMENT:</b> The Department has reviewed the information in support of the Structure Plan and notes the subject site is proposed to be located south of Wattelup Road, Hammond Park with an anticipated lot yield of 296. Preliminary analyses undertaken by the Department indicates the subject proposal will unlikely pose any adverse impacts on planned public schooling within the vicinity.</p> <p>In view of the above, the Department does not have any in principle objection to the proposed Structure Plan. However, if there are any changes to residential density coding and / or dwelling numbers in the area which may result in an increase to the projected student yield, the Department requests prior consultation.</p>	<p>1. It is noted that the residential lot yield of 296 lots is not considered to have an adverse impact on planned public schooling in the locality.</p> <p>Should the City receive any future amendments to the Structure Plan which contemplate an increase in residential lots, the amendment will be advertised to the Department of Education in accordance with draft <i>State Planning Policy 2.4 – Planning for School Sites</i>.</p>
12	Name & Address withheld, Kardinya	<p><b>OBJECT:</b> We would like to comment on the proposed structure plan submitted to council.</p>	<p>1. It is not clear whether the referenced 'embankment is the area on the south-eastern side of the Structure Plan, (located within the proposed public open space and</p>

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		<p>This plan impacts not only on our property and the quiet enjoyment of our land, but also the amenities of the area and the neighbourhood.</p> <p>The major points of concern are listed below:</p> <ul style="list-style-type: none"> <li>- The proposed embankment has a negative impact on our land which is unacceptable, as there will be constant run off requiring regular maintenance. Furthermore, it alters the road at grade to drive in. Not to mention the visual aesthetics of the large embankment affecting the amenities of the neighbouring area</li> <li>- We refer to the proposed road (running through Lot 115). As this is a QUBE/Primewest development, we would advocate that this road be moved to sit entirely in the developer's land, which will safeguard the quiet enjoyment of our land. The said road can be re-directed, by pulling it back on the border edge adjacent to the current road, (consequently QUBE would have to give up 12 lots). This option is very reasonable and possible, taking into consideration our rights as landowners over profits. We consider this to be a fair proposition.</li> </ul>	<p>Planning Control Area 153), or on the north-eastern side of the Structure Plan (adjacent to Wattleup Road). Irrespective of which area is being referenced, these embankments are required to manage the level difference between developed lots/road and existing (Wattleup) or future (Rowley) roads.</p> <p>Future subdivision works within the Structure Plan area will not impact access to Lot 115 Wattleup Road from Wattleup Road.</p> <p>2. The City notes the Ministerial Subdivision included a condition requiring supporting a 10.3 metre road reserve along the eastern boundary of Lot 9002 Wattleup Road (6m carriageway, 4.3m verge), as a full road reserve 15m is unable to be provided, as the remaining 4.7m of road reserve would need to be ceded from Lot 115.</p> <p>The City previously queried whether the road, as approved, is capable of being constructed, as the road carriageway would border the boundary of Lot 115, which sits approximately 4 metres higher at the boundary.</p> <p>The applicant provided a revised Preliminary Earthworks Plan (as advertised), which contemplates earthworks encroaching into Lot 115 to manage the level difference. Such works would be reliant upon the landowner consenting</p>


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			<p>either through the development application or subdivision process.</p> <p>As the Ministerial Subdivision approvals preceded preparation of a Structure Plan detailing preliminary earthworks levels, it is likely that the difference in levels between the developed Lot and existing Lot 115 was not considered.</p> <p>To ensure the eastern entry road is capable of completion and not reliant upon subdivision of Lot 115, it is recommended that a full 15 metre road reserve is provided within Lot 9002 Wattleup Road.</p>
13	Planning Solutions (on behalf of Alcoa), Perth	<p><b>OBJECT:</b> Planning Solutions has prepared this submission objecting to the proposed Structure Plan on behalf of Alcoa of Australia (Alcoa). In summary, we consider the Structure Plan should be refused for the following reasons:</p> <ol style="list-style-type: none"> <li>1. The Minister for Planning's decision to approve subdivision of the land is not relevant to the assessment of a proposal for a Structure Plan.</li> <li>2. The Structure Plan has failed to give due regard to State Planning Policy 4.1 Industrial Interface (SPP4.1).</li> <li>3. The Structure Plan fails to give regard to the Kwinana Air Quality Buffer (KAQB) transition zone that was endorsed by the Western Australian Planning Commission (WAPC) in September 2010 and remains in place today.</li> <li>4. There being unresolved potential impacts of risk which warrant application of the precautionary principle.</li> </ol>	<ol style="list-style-type: none"> <li>1. <b>State Planning Policy 4.1 Industrial Interface</b>  <i>State Planning Policy 4.1 – Industrial Interface</i> was gazetted on 29 July 2022, following the Minister for Planning's approval of subdivision approvals, and considerable period after a Scheme Amendment was gazetted to facilitate residential development of the site (subject to the preparation and approval of a Structure Plan.</li> <li>2. <b>Risk (Individual and Societal)</b>            It is acknowledged that the WAPC's <i>Development Control Policy 4.2 – Planning for Hazards and Safety</i> seeks the avoidance of development which exposes people to risk, however, it is considered that risks have been considered by virtue of the</li> </ol>

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		<p>5. Dust modelling studies were limited in scope and does not demonstrate the Structure Plan area to be suitable for sensitive land uses.</p> <p>6. The Structure Plan fails to provide transitional land uses as required by SPP4.1.</p> <p>7. The Structure Plan’s proposed memorial on title does not sufficiently identify the hazard.</p> <p><b>INTRODUCTION</b></p> <p>The land subject to the Structure Plan is situated between 1.1km and 1.6km to the north of Alcoa's Kwinana Alumina Refinery bauxite residue storage areas (RDAs). Alcoa commenced operations at the Kwinana Refinery in July 1963 and has progressively opened and operated the RDAs in their current location since 1972. Alcoa's State Agreement, the Alumina Refinery Agreement Act 1961 (WA), allows Alcoa to operate the RDAs until at least 2045, with a subsequent period of renewal for 21 years, conditional on WA Government approval.</p> <p>The primary purpose of this Structure Plan is to update and consolidate previously approved structure plans and reflect subdivisional layout approved by the Minister for Planning in 2020 on appeal, and to integrate other lots into the Structure Plan. The proposed Structure Plan covers an area of 20.6ha of which 12.6ha is proposed for residential comprising a yield of 296 dwellings.</p> <p><b>DECISION OF THE MINISTER FOR PLANNING TO APPROVE SUBDIVISION NOT RELEVANT</b></p> <p>In November 2020 the Minister for Planning granted approval for subdivision of portion of the land the subject of this Structure Plan.</p>	<p>site’s zoning and consequent Ministerial subdivision approvals.</p> <p><b>3. Dust and Amenity</b></p> <p>Refer to Submission No.14, Recommendation No.2</p> <p><b>4. Lack of Transitional Land Uses</b></p> <p>The City notes that the WAPC is currently in the process of preparing an Improvement Scheme for the area covered by Improvement Plan No.47 (IP 47), which will guide development of appropriate land uses within proximity to industrial uses in Mandogalup.</p> <p>By virtue of the Minister for Planning’s decision to approve two residential subdivision and the consequent endorsement of a Subdivision Concept Plan by the WAPC, a ‘Transition Zone’ is not required within the Structure Plan area.</p> <p><b>5. Notifications on Title is Deficient in Information</b></p> <p>As noted above, in approving the two subdivision applications within the Structure Plan area, the Minister for Planning accepted that the broader Kwinana Industrial Area (including Alcoa’s RDAs) posed only amenity impacts, rather than health impacts.</p> <p>The imposition of a Notification on Title on future subdivision approvals which alludes</p>

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		<p>The proponent relies on this decision as the rationale for the Structure Plan area intersecting with the KAQB. We submit the decision to grant subdivision approval is not relevant to this Structure Plan.</p> <p>The situation is analogous to the circumstances in <i>The Bethanie Group Pty Ltd and City of Stirling [2016] WASAT 6</i> (Bethanie), where the State Administrative Tribunal upheld the refusal of an application to amend a local development plan to make it consistent with a development approval that had been granted. The Tribunal commented at [32], “<i>the circumstances under consideration in each decision are not the same. They do not involve a ‘virtual duplication of circumstances and conditions’ [citation omitted]. The principle of consistency in decision-making does not apply to the circumstances of this application.</i>” [our emphasis added]</p> <p>Adopting the reasoning of the Tribunal in Bethanie, the Structure Plan performs a different role to that of the subdivision approval. The Structure Plan coordinates future subdivision and zoning of the land, whereas the subdivision approval provides authority for the works necessary to construct the lots, parklands, and roads and sets the conditions by which the subdivider must abide in undertaking the works. A structure plan to be made consistent with a subdivision approval is, as the Tribunal put it in Bethanie at [35], “<i>seeking to have the tail wag the dog</i>”.</p> <p>We submit this Structure Plan must be determined on individual merit, and the subdivision approval is not relevant to the assessment and determination of the Structure Plan.</p> <p><b>STATE PLANNING POLICY 4.1 INDUSTRIAL INTERFACE</b></p> <p>SPP4.1 was published in the Government Gazette on 29 July 2022. Its purpose is to seek to prevent conflict and encroachment between industrial and sensitive land uses.</p>	<p>to susceptibility of health issues is inconsistent with the Minister’s decision.</p>



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		<p>SPP4.1 expressly applies to proposed structure plans. Per clause 6.1.5 of SPP4.1:</p> <p><i>Structure planning over impact areas that are zoned urban and are related to SIAs and strategic proposals more generally, should not permit sensitive land uses, unless it has been adequately demonstrated that the impacts on these uses can be appropriately avoided, mitigated or managed.</i></p> <p>On 21 September 2010 the WAPC resolved to:</p> <ol style="list-style-type: none"> <li>1. <i>Endorse the revised KAQB line of 1km north, north east and east from the boundary of the Alcoa residue disposal area land holdings.</i></li> <li>2. <i>Endorse an additional 0.5km extension of the 1km buffer as a non-residential 'transition zone' to be referenced in planning documents and memorials on lot tiles.</i></li> </ol> <p>The WAPC affirmed the above decision on 24 May 2011. There has not been any formal recission of the KAQB by the WAPC.</p> <p>The KAQB transition zone extends across most of the Structure Plan area (refer Figure 1). Despite this, the proposed Structure Plan fails to give consideration to the provisions of SPP4.1, save for a single sentence relying solely on the Minister for Planning's decision to approve subdivision of the land (which we say is not relevant to the determination of this Structure Plan).</p> <p>It would be wholly inconsistent with the principles of orderly and proper planning to approve a structure plan that proposes to introduce residential development in an impact area without adequately demonstrating the impacts on these uses can be appropriately avoided, mitigated or managed. The proponent has failed to submit any documentation demonstrating how the KAQB impact area will be dealt with. We submit the Structure</p>	

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		<p>Plan should be refused as it has not addressed the requirements of SPP4.1, including failing to give adequate consideration to risk and dust (refer below).</p>  <p>Figure 1: Aerial photography showing the proposed Structure Plan in relation to the Alcoa RDAs and KAQB area</p> <p><b>RISK (INDIVIDUAL AND SOCIETAL)</b></p> <p>Risk (both individual and societal) is a relevant consideration in considering areas impacted by industry under SPP4.1. The WAPC's <i>Policy DC 4.2 Planning for Hazards and Safety</i> (DC4.2) states any development introducing a substantial number of people into an area where the individual risks are significant should be avoided.</p> <p>Alcoa wishes to draw attention to the potential critical consequence of a tailings dam failure, which the global resources industry has mobilised to address in the past three years. In the past decade, the global resources industry has experienced the tragic consequences of catastrophic wet tailings dam failures, which have inundated surrounding areas and local communities, such as:</p> <ul style="list-style-type: none"> <li>the red mud wall collapse at the Ajka alumina refinery, Hungary, in October 2010, which led to 10 deaths;</li> </ul>	

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		<ul style="list-style-type: none"> <li>• the Fundão dam collapse (Samarco) in Mariana, Brazil, in November 2015, which led to 19 deaths; and</li> <li>• the tailings dam failure at the Córrego do Feijão mining facility (Vale) in Brumadinho, Brazil, in January 2019, which led to 270 deaths.</li> <li>• the tailings dam collapse in Jagersfontein, South Africa, in September 2022, which led to one death and another two people missing.</li> </ul> <p>Risk from a tailings dam failure is not, to our knowledge, a factor considered in previous planning proposals applicable to the Structure Plan area. It is a new consideration not resolved by planning decision-makers.</p> <p>Following the tailings dam failures, resource companies are publishing more information about their tailings' facilities including the consequence classification of these facilities based on the consequence of failure. In all cases, as the population increases the potential loss of life should an event occur increases and the consequence is higher.</p> <p>Governments around the world are actively seeking to limit residential, commercial, and industrial development downstream to tailings facilities to mitigate any consequences. Companies are moving to alternative technologies such as residue filtration, introduced by Alcoa at the Kwinana RDA in 2016. As such, Alcoa strongly urges consideration to be given to the consequence of tailings dam failure when determining the Structure Plan.</p> <p>Consistent with the Global Industry Standard on Tailings Management (GISTM August 2020), Alcoa is a member of the International Council on Mining and Metals (ICMM) and have agreed to meet the requirements of the GISTM. Alcoa designed all RDAs to the relevant standards when they were built and has led industry with implementation of mandated</p>	

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		<p>tailings management design standards for over 25 years. Alcoa continuously improves design, controls, procedures, and operating measures at the Kwinana RDAs to prevent potential catastrophic failure. To compliment and support these measures, a precautionary approach should be applied to achieve the objective of separating the RDAs from inconsistent land uses and urban encroachment, which increase the population at risk.</p> <p>SPP4.1 requires planning authorities to take a precautionary approach to proposals involving off-site impacts or risks where;</p> <p><i>inadequate information is known or available about the impacts, and where these impacts are difficult to avoid, mitigate or manage, or where the cost to do so is not commensurate with the risk. This will be particularly important if there are potential significant adverse health and amenity impacts on existing or proposed sensitive land uses within impact areas, where consequences and mitigation measures need to be weighed more carefully.</i></p> <p>This is the case for the Structure Plan, where the risk is undefined but higher density development would be at real risk should a low-likelihood catastrophic failure ever occur. Simply put, it would be premature to proceed with the Structure Plan at the current time. The proposed Structure Plan should not proceed until it has been demonstrated the risk has been comprehensively investigated and addressed to ensure that no significant residential development will be placed in a position of risk.</p> <p><b>DUST AND AMENITY</b></p> <p>In June 2017 the Environmental Protection Authority (EPA) provided advice on the size of a land use planning buffer relating to health and amenity impacts of dust, now and into the future, in respect of potential urban development in the Mandogalup area, titled <i>Consideration of potential health and</i></p>	

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		<p><i>amenity impacts of dust in determining the size of a buffer for urban development in the Mandogalup area. The EPA recommended further investigation to determine the principal sources of dust and contributing to the exceedances of the NEPM dust standard in the area to the north and north-east of Alcoa's RDAs and for corrective measures to be undertaken if practicable. These findings were endorsed by the State Government. The Structure Plan area lies immediately north of Area D of the EPA's guidance, being:</i></p> <p><i>Air quality does not appear to meet the revised NEPM goal for PM10 in this area under the current or planned future RDA operation. Also, dust events from the RDA may cause occasional amenity impacts in the area under current and future operations.</i></p> <p><i>- There is a need for further investigation to determine the sources of dust contributing to the exceedances of the NEPM goal in this area and for corrective measures to be undertaken if practicable to achieve the NEPM air quality goal in this area.</i></p> <p>Further air quality monitoring was undertaken and the results published in the Department of Water and Environmental Regulation's (DWER's) <i>Mapping dust plumes at Mandogalup using a LiDAR Report of air quality monitoring conducted between 1 December 2017 and 31 March 2018 (LiDAR Report)</i>. This report concluded the RDA was one of several contributors to dust levels in the area. While the Monitoring Report made various findings on NEPM PM10 targets, it did not redefine the KAQB.</p> <p>The Minister for Planning's 2020 decision to approve subdivision in the Structure Plan area selectively relied on the DWER's LiDAR Report to assess the dust and amenity impacts associated with Alcoa's RDAs and other dust sources in the area. Proper consideration of meteorological conditions is</p>	

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		<p>needed to understand the full potential for dust and amenity impacts and consequent urban conflict. A precautionary approach should be taken to the dust and amenity issues consistent with clause 6.5 of SPP4.1 (published 2022).</p> <p><b>LACK OF TRANSITIONAL LAND USES</b></p> <p>SPP4.1 contemplates light industry zoning and other appropriate zones and reserves as a compatible land use transition at the interface of general and strategic industrial areas.</p> <p>In this case, the subject land is located within reasonable proximity of Alcoa's bauxite RDAs and is adjacent to the Improvement Plan 47 (IP47) area which the WAPC's South Metropolitan Peel Sub-regional Planning Framework has designated for industrial investigation. On 23 November 2022 the WAPC adopted a land use plan for the IP47 area identifying the land to the south of the Structure Plan area for general industry and light industry.</p> <p>The KAQB endorsed by WAPC in 2010 included a 500m-wide 'transition zone'. No information has been provided with the Structure Plan demonstrating how the transition zone will be implemented.</p> <p>Should the Structure Plan proceed, there should be controls inserted into the proposal text and any subsequent local development plans to require development in the Structure Plan area to be of a suitably low scale so as to minimise the potential risk (as discussed above) and to provide a suitable transition between residential and industry. This is an outcome consistent with the WAPC's September 2010 decision establishing the KAQB transition zone and required by clause 6.1.5 of SPP4.1.</p> <p><b>NOTIFICATION ON TITLE IS DEFICIENT IN INFORMATION</b></p>	

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		<p>Clause 4.2 of the Structure Plan proposes lots within 500m of the KAQB have a memorial on title advising:</p> <p><i>“This lot is located in close proximity to existing industrial development, freight routes and semi-rural activities and has the potential to be affected by transport noise, vibration and dust which may be associated with the ongoing operation of those nearby activities.”</i></p> <p>This proposed notification lacks sufficient specificity to enable prospective purchasers to identify the hazard. A notification such as that proposed fails to provide purchasers with information on the specific source of the hazard, being Alcoa’s RDAs and other heavy industrial land uses in the Kwinana Industrial Area, or the potential health implications of dust. The draft condition submitted to the Minister for Planning by the State Administrative Tribunal quoted in <i>Wattleup Road Development Co Pty Ltd -v- State Administrative Tribunal [No 2] [2016] WASC 279</i> at [65] provided a level of information which gave prospective purchasers more guidance on the hazard and possible consequences:</p> <p><i>‘This lot is located in proximity to industrial operations within and associated with the broader Kwinana industrial area and therefore may be affected by dust from that area. The Department of Health advises that dust may adversely impact amenity and well-being. Individuals who may be more susceptible to the adverse health effect of fine and coarse dust particulars include the elderly, children and people with pre-existing respiratory and cardiovascular health conditions’.</i></p> <p>If the WAPC were minded to approve this Structure Plan (not withstanding our objections) a notification worded as above should be applied.</p> <p><b>CONCLUSION</b></p>	

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		<p>For the reasons listed above, we submit the Structure Plan should not proceed.</p> <p>We respectfully request we are informed of any committee and Council meetings at which this matter is considered and are invited to attend the meeting.</p>	
14	Alcoa of Australia Ltd, Applecross WA	<p><b>OBJECT:</b> Please find enclosed a submission on the Structure Plan by Planning Solutions, which has been prepared on behalf of Alcoa.</p> <p>In addition to the submission, Alcoa wishes to draw the City and WAPC's attention to two specific matters.</p> <p><b>1. Consideration of potential critical consequence of tailings dam failure</b></p> <p>Alcoa has since 1972 operated residue storage areas, which are a type of tailings dam, in their current location to the south of the land the subject of the Structure Plan.</p> <p>The Minister for Planning's decision in 2020 (the <b>Hammond Park decision</b>) to grant approval for subdivision of a portion of the land the subject of the Structure Plan did not consider the potential critical consequence of tailings dam failure.</p> <p>The global resources industry has in the past four years mobilised to address the potential critical consequence of tailings dam failure. Alcoa draws the City and WAPC's attention to the fact that Governments around the world are actively seeking to limit residential, commercial, and industrial development downstream to tailings facilities to mitigate any consequences.</p> <p>Due to the proximity of the residue storage areas to the land the subject of the Structure Plan and the lack of consideration of tailings dam risk in the Hammond Park decision, Alcoa submits that that decision to approve the subdivision should not be relevant to the assessment and determination of the</p>	<p>1. <b>Consideration of potential critical consequence of tailings dam failure</b></p> <p>Alcoa's comments in respect of the consequences of a tailing dam failure are noted.</p> <p>It should be noted that the City, WAPC and State Government has accepted the suitability of the Structure Plan area for residential development, notably through the below decisions:</p> <ul style="list-style-type: none"> <li>• the lifting of Urban Deferment (to an 'Urban') zoning under the Metropolitan Region Scheme (gazetted 31 October 2008);</li> <li>• the introduction of a 'Development' zone over the subject site upon gazettal of the City of Cockburn Town Planning Scheme No.3 on 20 December 2002, and Amendment No.28 (gazetted 14 December 2014) which introduced the requirement for a structure plan to be prepared in support of residential development; and</li> <li>• the City's adoption of structure plans for the then Lots 117 &amp; 805 Wattleup Road, Wattleup and Lots 121, 122 and 801</li> </ul>



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		<p>Structure Plan. The City and WAPC should apply a precautionary approach to the Structure Plan to achieve the objective of separating the residue storage areas from inconsistent land uses and urban encroachment, which increase the population at risk.</p> <p><b>2. Previous decision to approve subdivision involved inadequate consideration of dust and amenity impacts</b></p> <p>Alcoa has maintained its position that the amenity impacts of dust from Alcoa's residue storage areas were not properly considered in the Hammond Park decision. Alcoa notes the following:</p> <ul style="list-style-type: none"> <li>• The State Administrative Tribunal explored the impacts of dust arising from Alcoa's residue storage areas during their consideration of the proposed subdivisions at Wattleup Road, Hammond Park.<sup>1</sup> In these proceedings, the Tribunal recommended that the subdivision approval be refused. However, this subdivision was ultimately approved in November 2020.</li> <li>• The Hammond Park decision should be given limited weight by the City and WAPC in its assessment of the Structure Plan because it did not adequately reflect the advice issued to the Minister for Environment under section 16E of the <i>Environmental Protection Act 1986</i> (WA).<sup>2</sup> In June 2016, the Minister for Environment requested the EPA provide advice on the size of a land use planning buffer relating to amenity impacts of dust, now and into the future, in respect of potential urban development in the Mandogalup area.</li> <li>• The Hammond Park decision did not adequately reflect the EPA's advice on the Mandogalup sub-areas {B, C and D of the EPA's Advice<sup>3</sup>} or the studies by consultant, GHD, which informed the EPA's advice.</li> </ul>	<p>Wattleup Road, Wattleup on 11 December 2008;</p> <ul style="list-style-type: none"> <li>• the decision of the Minister for Planning to conditionally approve two subdivision applications in November 2020, and the WAPC's consequent endorsement of a Concept Plan in May 2021.</li> </ul> <p><b>2. Previous decision to approve subdivision involved inadequate consideration of dust and amenity impacts</b></p> <p>In approving the two subdivision applications with the Structure Plan area, the Minister for Planning accepted the LiDAR report was sufficient to determine the impact of dust, consequently determining the observed levels of dust represented an amenity impact, rather than a public health risk.</p> <p>The Minister's determination in relation to the LiDAR report noted that the monitoring period represented a 'worst case scenario' over the four months of monitoring, which included the summer months.</p> <p>By virtue of the Minister's acceptance of the LiDAR report in approving the subdivisions, it is not considered necessary for new modelling (which factors in meteorological conditions) to be undertaken to determine the impact amenity through the structure planning process.</p>

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<ul style="list-style-type: none"> <li>The Hammond Park decision selectively relied on the Department of Water and Environment Regulation's <i>Mapping dust plumes at Mandogalup using a UDAR report - January 2019</i><sup>3</sup> to assess the dust and amenity impacts associated with Alcoa's residue storage areas and other dust sources in the area. The LiDAR Report was not sufficient by itself to inform planning for the land the subject of the Structure Plan because that report did not consider meteorological conditions and the amenity impacts were not determined.</li> </ul> <p>Alcoa maintains that the City and WAPC must have proper consideration of meteorological conditions to understand the full potential for dust and amenity impacts and consequent urban conflict. The studies by GHD, which informed the EPA's advice, remain the authoritative studies to be used in the City and WAPC's assessment of the proposed Structure Plan. Accordingly, Alcoa sees the current process as the appropriate time for the City and WAPC to uphold a precautionary approach to the dust and amenity issues, as they apply to the area covered by the Structure Plan.</p> <p><sup>1</sup> <i>Wattleup Road Development Company Pty Ltd and Western Australian Planning Commission [2014] WASAT 159; and Wattleup Road Development Co Pty Ltd v State Administrative Tribunal [No 2] [2016] WASC 279</i></p> <p><sup>2</sup> <i>EPA's Advice: June 2017 Consideration of potential health and amenity impacts of dust in determining the size of a buffer for urban development in the Mandogalup area; Advice of the Environmental Protection Authority to the Minister for Environment under Section 16(e) of the Environmental Protection Act 1986. And the Supplementary information; Mandogalup area buffer zone s16e advice, June 2017</i></p>	

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p><sup>3</sup> January 2019 Department of Water and Environmental Regulation, Mapping dust plumes at Mandogalup using LiDAR Report of air quality monitoring conducted between 1 December 2017 and 31 March 2018</p>	
15	Main Roads WA, East Perth	<p><b>COMMENT:</b> In response to your correspondence received on 5 January 2023 Main Roads provides the following comments regarding the above structure plan:</p> <ul style="list-style-type: none"> <li>• The structure plan area does not front a state road, however Planning Control Area 156 (PCA 156) abuts the site. The intent of the PCA is to protect the proposed Rowley Road alignment between Kwinana Freeway and the Coast. The PCA is created to ensure that no further development occurs on this land, which might compromise this purpose until it becomes reserved in the Metropolitan Region Scheme.</li> </ul> <p>The following comments are provided for consideration in relation to the submitted Transport Impact Assessment (TIA):</p> <ul style="list-style-type: none"> <li>• The development yield used within the TIA does not reflect the scale of development proposed by the structure plan. Trips have been estimated based upon lots rather than using development/dwelling yield. The number of dwellings is required to determine the trip rate which in turn informs necessary warrant assessments that determines upgrades required to facilitate the structure plan.</li> <li>• 2.2 Proposed Land Use It is recommended the internal road alignment is reconsidered. Direct road connectivity from the structure plan area should be provided to the residential area on the north side of Wattleup Road. This could be achieved by aligning a north-south road in the structure plan to link to one of the roads on the north side of Wattleup Road that</li> </ul>	<ol style="list-style-type: none"> <li>1. The trip generation within the Transport Impact Assessment (TIA) being based on the number of proposed lots is considered acceptable, as this is consistent with the number of dwellings anticipated.</li> <li>2. The proposed internal road network is largely consistent with the existing Ministerial subdivisions, with some changes required to accommodate additional that were not part of these previous approvals.  It is acknowledged that a roundabout could be an option adjacent to the westernmost access road onto Wattleup Road, however, it is restricted by the location of existing public open space and residential lots on the northern side of Wattleup Road.</li> <li>3. The City accepts that additional information is required regarding additional widenings for the three access roads, including turning treatments. A modification to the TIA is recommended to review the proposed intersection treatments and any further widening required to Wattleup Road.</li> <li>4. The City considers that the assumptions made regarding trip generation in the TIA are acceptable.</li> <li>5. Agreed - it is recommended that the TIA be modified to include further justification on</li> </ol>

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>have a frontage to the future primary school, being a key local trip attractor/ generator. The four-way intersection at Wattleup Road could be constructed as a roundabout which may assist short to medium term traffic management.</p> <ul style="list-style-type: none"> <li>• 4.1.4.2 Road Upgrades The Council should consider obtaining additional information regarding the need for the proposed localised widenings provided for Access Road 1, 2, and 3. Specifically, the treatment type at these intersections should be in accordance with the warrants for BA, AU and CH turn treatments set out in Clause A.8 of MRWA Supplement to Austroads Guide to Road Design - Part 4. Consider further investigation to identify any left/right turn lane treatments to meet operational/safety requirements.</li> <li>• 6.3 Traffic Distribution Council to consider obtaining more details with regards to the assumptions used to determine traffic distribution. The proportion of structure plan area generated trips attributed to the west appear high, considering the location of nearby trip generators and the Kwinana Freeway. It is unclear if the trip generation has validated an assessment of existing peak hour movements from existing residential areas in close proximity along Wattleup Road.</li> <li>• 6.4 Background Traffic Further Justification for the adopted 1% growth rate should be provided. Details of the ROM data used needs to be provided. Please quote job number and date on which the information was obtained.</li> </ul>	<p>the 1% adopted growth rate and ROM data used.</p> <p>6. The TIA prepared for the Structure Plan is not required to model traffic generation from existing residential development and proposed commercial development in the locality.</p>

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<ul style="list-style-type: none"> <li>6.4.2 Other Development Not Included</li> </ul> <p>Details should be provided showing the modelling assessment has taken into consideration traffic generation from surrounding proposed residential developments. New surrounding proposals include a neighbourhood shopping centre (including a supermarket), fast food outlets and specialty retail near the corner of Wattleup Road and Frankland Avenue.</p> <p>Should the City disagree with the above advice, Main Roads requests an opportunity to meet and discuss the proposal further, prior to a final recommendation being made.</p>	
16	Name and Address withheld, Attadale	<b>SUPPORT:</b> We are satisfied with the proposed structure plan.	1. Noted.
17	Department of Transport, Perth	<p><b>COMMENT:</b> The Urban Mobility (UM) division of Department of Transport (DoT) has reviewed the structure plan documents and I can advise that the DoT generally supports the proposed Structure Plan however provides the following advice in relation to cycling, pedestrian and public transport connectivity:</p> <p><b>Long Term Cycle Network and Shared and Pedestrian Path Network</b></p> <p>The structure plan should be updated to reflect DoT's Long Term Cycle Network (LTCN), including:</p> <ul style="list-style-type: none"> <li>Future Rowley Rd extension is identified as a primary route in the LTCN</li> <li>Identification of cycling route connections from the locality to the LTCN</li> </ul>	<p>1. The location of pedestrian footpaths will be determined at the detailed design stage, to satisfy a condition of subdivision approval. As required under the WAPC's <i>Liveable Neighbourhoods</i> policy, the proposed access streets will require a footpath on a least one side of the road.</p> <p>The City acknowledges that longer-term traffic volumes on Wattleup Road are consistent with a Neighbourhood Connector classification, which will require pedestrian footpaths on both sides of the road. It is anticipated that at detailed design stage, pedestrian crossings are provided along Wattleup Road to provide safe crossing points to north, which will facilitate safe</p>

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<ul style="list-style-type: none"> <li>• LTCN route designs are to be in accordance with the Western Australian Cycling Network Hierarchy and LTCN for Perth and Peel; and</li> <li>• Ensure signage within the locality exists to guide cyclists to abutting LTCN routes</li> </ul> <p>The City of Cockburn's Integrated Transport Policy 2020-2030 encourages the transition to sustainable transport mode choices such as cycling, walking, and public transport. DoT considers that there are opportunities for this Structure Plan area to more proactively ensure that future residents are better connected and integrated with the surrounding network that as is proposed in the Structure Plan documents.</p> <p>The Structure plan provides basic cycle and pedestrian infrastructure through the provision of footpaths. It does not clearly demonstrate how these would extend over Wattleup Road to access local amenities including shops and the primary and secondary schools. Whadjuk Drive to the north of Wattleup Rd provides a good cycling connection however links to this area are also not clear.</p> <p>Documentation notes that a safe crossing of Wattleup Road should be provided however there is a lack of existing infrastructure on Wattleup Road, which currently has no footpath, and it is not clear if/when or by whom such infrastructure will be provided.</p> <p>As the long term future of Wattleup Road is to be a lower volume road it would appear to be more appropriately suited to being a "Neighbourhood Connector" than a local access road. It is noted that very few residences to the north or south of Wattleup Road will front Wattleup Road and it would make a suitable active transport connector.</p> <p><b>Public Transport</b></p>	<p>pedestrian access to the nearby Primary School and other public amenities.</p> <p>2. It is acknowledged that the planning for future bus routes will be dependent upon the timing for future residential development and population growth. The future footpath network within, and adjacent to, the Structure Plan area is expected to facilitate a walkable catchment to bus services on Wattleup Road and/or Whadjuk Drive.</p>

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>There is currently no walkable access to public transportation, with the nearest bus routes being the Route 535 bus service operating between Aubin Grove Station and Hammond Park Primary School and the Route 536 bus service operating between Aubin Grove Station and Hammond Park Secondary College. The area surrounding the proposed structure plan site is still largely being developed, with public transport catchment constraints due to the current surrounding land uses being incongruent with demand for public transport services.</p> <p>The Transport Impact Assessment (TIA) clause 4.5 Public Transport Network and Figure 4-16 states “the provision of a future bus service potentially along Whadjuk Drive and Wattleup Road which may also link to the Latitude 32 Industrial area and terminate at the Fremantle Rockingham transit system”. We note that there is no government commitment to delivery of a Fremantle – Rockingham Transit system. The Transperth Service Development Plan is currently in the process of being reviewed in this area. It is anticipated that this review would identify the extension of Route 535 along Wattleup Road would be the preferred option with terminus to be confirmed depending on ultimate development plans. The future operation of a bus route in the area is subject to the provision of funding and resources to operate the service.</p> <p>Transperth prefers to operate bus routes centrally through the catchment area, close to higher density, and with the aim of creating a 500 metre walkable catchment area either side of the route to ensure well supported patronage. The structure plan area should ensure excellent pedestrian permeability to the preferred alignment and safe crossing locations to support walkable access to a potential future bus service. There is limited pedestrian permeability along the northern boundary of the structure plan area for north-south pedestrian travel. This will negatively impact on walkability to a potential future bus service and may ultimately impact on the viability of a future</p>	

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>bus service and encourage greater dependence on alternative modes of travel.</p> <p>Transperth also recommends that to ensure the ability to provide public transport access in the early stages of development, infrastructure should be provided to allow buses to terminate at the north-west extent of the structure plan site. This includes the provision of an appropriately located embayment, that allows the use of a roundabout, turning circle or appropriate road network infrastructure to allow buses to turn around efficiently.</p> <p>It is strongly encouraged that the City engage with the Public Transport Authority to discuss provision of services to the Hammond Quarter.</p>	
18	Department of Biodiversity, Conservation and Attractions, Crawley	<p><b>COMMENT:</b> The Parks and Wildlife Service of the Department of Biodiversity, Conservation and Attractions (the department) provide the following comments.</p> <p>The proposal may result in the loss of habitat for threatened black cockatoo species. Carnaby's Black Cockatoo and the Forest Red-tailed Black Cockatoo are listed as 'Endangered' and 'Vulnerable' respectively under the Commonwealth's Environmental Protection and Biodiversity Conservation (EPBC) Act 1999. These species are also listed as 'Specially Protected' under the Western Australian Biodiversity Conservation Act 2016. The proposal will also impact a remnant of the EPBC Act listed Banksia woodlands of the Swan Coastal Plain Threatened Ecological community.</p> <p>It is understood that the Federal Department of Climate Change, Energy, the Environment and Water has determined that the proposed clearing of Lot 809 Wattleup Rd is a controlled action under the EPBC Act, and that they are currently undertaking an assessment of the proposal.</p>	<ol style="list-style-type: none"> <li>1. The proponent as obtained approval in September 2022 from the Commonwealth Department of Climate Change, Energy, the Environment and Water (DCCEEW) under the Commonwealth Environmental Protection and Biodiversity Conservation Act 1999 (EPBC Act) to clear all 3.03 hectares of Banksia Threatened Ecological Community on Lot 809 Wattleup Road.</li> <li>2. Whilst the Commonwealth approval permits the clearance of all Banksia TEC present on Lot 809, the City encourages the proponent to retain any remnant vegetation within road reserves and POS, where the finished levels allow.</li> </ol>



### City of Cockburn Recommended Modifications - Hammond Quarter Structure Plan

#### Part One (Implementation)

1. Clause 4.2 (Notifications on Title) – Update to reference that the affected lots have been identified in accordance with the Transportation Noise Assessment prepared for the Structure Plan, as per the requirements of *State Planning Policy 5.3 – Road and Rail Noise* and remove reference the existing subdivision approvals.
2. Clause 4.2 (Notifications on Title) – Remove ‘*Consistent with the conditions of subdivision approval of WAPC Ref 148010 and WAPC Ref 147987 that preceded the preparation of this SP*’.
3. Clause 4.2 (Notifications on Title) – Update to include an additional notification on title being required as a condition of subdivision approval for lots located within 500m of current (or incomplete) Extractive Industry (sand mining) activities in Mandogalup. The notification is to state:
 

*“This lot is located in close proximity to approved Extractive Industry (sand mining) activities in Mandogalup and has the potential to be affected by dust and noise that are associated with the continued operation of a sand mine.”*
4. Clause 4.6 (Wattleup Road) – Update to specify that subdivision will trigger the need to upgrade Wattleup Road to an ‘Urban’ standard, where it abuts future subdivision, which may include additional road widening to accommodate median treatments and deceleration lanes within the developer’s land.
5. Clause 4.11 (Residential Design Codes Variations) – Delete in recognition of the recent release of the updated Residential Design Codes – Vol.1.
6. Clause 4.0 (Subdivision and Development Requirements) – Insert an additional sub-clause called ‘Lots Abutting Wattleup Road’, which states:
 

*Where a proposed subdivision includes lots which abut Wattleup Road, preliminary engineering plans are to be prepared and submitted to the Western Australian Planning Commission detailing the proposed treatment to manage the level difference between the Wattleup Road reserve and the developable portion of the residential lot.*

*The preparation of preliminary engineering plans will inform whether the proposed interface treatment needs to be protected via restrictive covenants or easements. Such encumbrances will need to be imposed via a condition of subdivision approval.*
7. Clause 4.0 (Subdivision and Development Requirements) – Insert an additional sub-clause called ‘Road Safety Audit’, which states:
 

*At the subdivision stage, a Road Safety Audit is to be prepared by an accredited road safety auditor to assess any safety implications arising from the access road design and broader impacts on Wattleup Road.*
8. Clause 6.0 (Local Development Plans) – Insert an additional site attribute for the preparation of a LDPs for lots subject to quiet house design requirements under the Transportation Noise Assessment prepared for the Structure Plan.

#### Structure Plan Map

9. Modify the residential density yield from 294 dwellings to 296 dwellings, consistent with the Executive Summary and Part Two.

**City of Cockburn Recommended Modifications - Hammond Quarter Structure Plan**

10. Remove the Residential Density and Local Scheme Reserves designated for lots outside of the Structure Plan area.
11. Delineate Lots 200 and 1001 with a black dashed line, consistent with the remaining lots in the Structure Plan area.
12. Correct the x-y scale.
13. Amend the 11m wide road reserve (adjacent to the Linear Public Open Space) to a 11.5m road reserve and include localised widenings to a minimum width of 13m to accommodate embayed parking adjacent high-use POS areas within the road reserve. The additional road widening must not compromise continued compliance with the 10% minimum creditable public open space requirement.

**Part Two (Explanatory)**

14. Section 1.4.2.1 (Southern Suburbs District Structure Plan Stage 3) – Amend Figure 4 to delineate the location of the Structure Plan.
15. Section 1.4.4.4 (State Planning Policy 2.8 – Bushland Policy for the Perth Metropolitan Region) – Delete entirely.
16. Section 1.4 (Planning Framework) – Insert an additional sub-section in relation to *State Planning Policy 2.9 – Water Resources* (SPP 2.9), which details how the proposed UWMP demonstrates compliance with SPP 2.9.
17. Section 1.4 (Planning Framework) – Insert an additional sub-section in relation to Local Planning Policy 5.7 – Uniform Fencing, which acknowledges the requirement for fencing abutting public open space and road reserves. This is to include reference to the boundary fencing between residential lots and Wattleup Road being constructed of solid (masonry) materials.
18. Section 2.1 (Biodiversity and Natural Area Assets) – Amend to:
  - reference approval having been obtained under the Commonwealth *Environmental Biodiversity and Conservation Act 1999* to clear native vegetation on Lot 803 (formerly Lot 809) Wattleup Road;
  - reference that a State clearing permit under the *Environmental Protection Act 1986* may be required, if an exemption has not been obtained through a subdivision approval; and
  - note that reasonable efforts should be made to retain existing Banksia vegetation within the proposed road reserves and public open space, where the finished levels of the subdivision allow.
19. Section 2.5 (Heritage) - Amend to reference the *Aboriginal Cultural Heritage Act 2021* and to acknowledge that any ground disturbance works that occur after 1 July 2023 may require assessment under this legislation.
20. Section 3.2 (Open Space) – Amend to stipulate that the 10% of the gross subdivisional area is a requirement under the WAPC's *Liveable Neighbourhoods* policy and *Development Control Policy 2.3 – Public Open Space in Residential Areas*, rather than the requirements under the two existing Ministerial subdivision approvals.
21. Section 3.5 (Water Management) – Amend to reference that an Urban Water Management Plan (UWMP) is required to be prepared in accordance with *draft State*

**City of Cockburn Recommended Modifications - Hammond Quarter Structure Plan**

*Planning Policy 2.9 – Planning for Water and the WAPC's Better Urban Water Management Policy.*

22. Section 3.5 (Water Management) – Amend to acknowledge that the Department of Water and Environmental Regulation (DWER) has accepted the preparation of a UWMP, in lieu of a Local Water Management Strategy.

Appendix B – Engineering Report

23. Under Section 7 Sewerage Reticulation, replace 'Figure 7: Sewer Reticulation Planning (Water Corporation 24 Aug 2021)' with the updated Sewer Catchment Plan (MZ70-1-2, Issue B), dated 9 March 2022.

Appendix F- Transport Impact Assessment

24. Amend the Transport Impact Assessment (TIA) to include turn warrant assessments for all three intersections onto Wattleup Road to assess whether there is a need to provide deceleration lanes and whether additional widening is required on the southern side of Wattleup Road.
25. Amend 'Figure 4-5 Wattleup Road/ Access Road 3 Intersection Concept' to detail a permanent solid median treatment to remove reference to a Flat Hazard Panel median treatment, and where this results in widening to the southern side of Wattleup Road, detail the indicative widening required.

Appendix G – Transportation Noise Assessment

26. Revise the Transportation Noise Assessment to reflect modelling and recommended noise mitigation options for 'Option 3 – Roadside Noise Wall + Reduced Noise Bund to RL40m' and specify quiet house design packages, as per *State Planning Policy 5.4 – Road and Rail Noise*. This is to include additional modelling to determine the extent of Quiet House Design (QHD) packages required where a second storey is proposed to a dwelling, and the specific QHD packages to be applied.

## 14.1.2 Consideration of Draft Local Planning Strategy - Outcomes of Consultation

<b>Responsible Executive</b>	A/Chief of Built and Natural Environment
<b>Author</b>	Senior Strategic Planner
<b>Attachments</b>	<ol style="list-style-type: none"> <li>1. Proposed Schedule of Modifications <a href="#">↓</a></li> <li>2. WAPC Required Modifications (for Advertising) <a href="#">↓</a></li> <li>3. Draft Local Planning Strategy - As Advertised (circulated under separate cover) <a href="#">↓</a></li> <li>4. Schedule of Submissions (circulated under separate cover) <a href="#">↓</a></li> <li>5. Large Attachments to Submissions <a href="#">↓</a></li> <li>6. Draft Planning Area K: Kinley/Lyon Road Area <a href="#">↓</a></li> <li>7. Draft Planning Area L: Bibra Lake Recreational Area <a href="#">↓</a></li> <li>8. Proposed Appendix B: Banjup Principles for Preservation <a href="#">↓</a></li> <li>9. Proposed Appendix C: Strategic Activity Centre Comparison <a href="#">↓</a></li> </ol>
<b>Location</b>	City of Cockburn
<b>Owner</b>	N/A
<b>Applicant</b>	City of Cockburn
<b>Application Reference</b>	197/002

### RECOMMENDATION

That Council:

- (1) SUPPORTS the draft Local Planning Strategy as shown in Attachment 3 with modifications outlined in Attachment 1 to address issues raised in the submissions, pursuant to Part 3 clause 14 of the *Planning and Development (Local Planning Schemes) Regulations 2015*.
- (2) DELEGATES submission of the following to the Western Australian Planning Commission, pursuant to Part 3 Clause 14(3) of the *Planning and Development (Local Planning Schemes) Regulations 2015*:
  - (a) a copy of the advertised local planning strategy;
  - (b) a schedule of the submissions received; and
  - (c) particulars of any modifications to the advertised local planning strategy proposed by the local government.
- (3) ADVISES those parties that made a submission of Council's decision accordingly.

## Background

Council at its meeting of 8 October 2020 resolved to request that the Western Australian Planning Commission (WAPC) certify the draft Local Planning Strategy for advertising.

This is a requirement pursuant to clause 12, Part 3 - local planning strategies of the *Planning and Development (Local Planning Schemes) Regulation 2015* (Regulations) prior to advertising a Local Planning Strategy (Strategy).

The draft Strategy was forwarded to the WAPC on 10 October 2020 for their certification. At their meeting on 14 June 2022, the WAPC Statutory Planning Committee (SPC) certified the draft City of Cockburn Local Planning Strategy for advertising, subject to modifications.

The required modifications are listed (refer Attachment 2), with the key changes being:

- Addition of 7 new 'Planning Areas':
  - Jandakot Industrial Investigation Area
  - Glen Iris Golf Course site
  - Rural Living Precinct
  - Wattleup Industrial Investigation Area
  - Cockburn Central Activity Centre
  - Phoenix/Spearwood District Centre
  - Cockburn Coast District Centre
- Inclusion of references to the Industrial Investigation Areas (Wattleup and Lake Coogee/Munster) and planning directions that are more high-level
- Removal of some of the detail around future zonings to ensure the Strategy remains at a high level
- Removing some advocacy items that are not intrinsically linked to planning processes, instruments, or mechanisms
- Changes to align with the State Planning Framework (Perth and Peel @3.5 million)
- Neighbourhood Plans being renamed 'Local Area Plans'
- Draft Strategy Map updates, in consideration of cartographic convention, and to show Activity Centres and proposed 'Planning Areas' and reflect what is shown in Perth and Peel@3.5 million.

Following confirmation from the WAPC that the modifications had been suitably undertaken, the draft Strategy was formally advertised for public comment for 60-days from 22 September 2022 to 21 November 2022, with the full details of the engagement outlined under the 'Consultation' section of this report.

Draft Strategy as advertised for public comment, incorporating the WAPC required changes (refer Attachment 3).

### Purpose of a Strategy

The Strategy is a high-level, long-term plan to guide City growth and future land use change over the next 15 years.

Importantly, the Strategy will inform preparation of the City's new Town Planning Scheme (Scheme), being the primary statutory instrument used by the City for making planning decisions.

Preparation of the new Scheme will commence as the Strategy progresses towards finalisation.

Together, the Strategy and Scheme will guide the preparation and adoption of other planning instruments, including local planning policies, activity centre and local structure plans to create a logical and interconnected local planning framework that makes the intent behind every decision clear.

### Format of a Strategy

The Strategy format consists of two parts: Part One contains Planning Directions and actions and sets out 'Planning Areas'.

Part Two provides background supporting information and planning context.

The content of the Strategy and matters to be addressed are defined in clause 11(2) of the Regulations.

The draft Strategy was prepared prior to the preparation of the WAPC Local Planning Strategy Guidelines (October 2021) however it generally aligns with that format.

### Process for Preparing a New Strategy

The Regulations define the steps to be followed in preparing and finalising a Strategy. These are summarised as follows:

1. Local Government (LG) prepare a "Report of Review" to decide if the existing Strategy is satisfactory, should be reviewed, or a new Strategy prepared
2. LG make a recommendation to the WAPC
3. WAPC make a decision on "Report of Review"
4. LG define the scope, undertake background analysis and preliminary stakeholder consultation and prepare Draft Strategy
5. LG endorse the Draft Strategy and request WAPC Certification for advertising
6. WAPC certify that the Draft Strategy is suitable for advertising
7. LG formally advertise the Draft Strategy
8. LG consider submissions and determines whether to support Draft Strategy as is or in a modified format (we are currently at this stage in the process)
9. WAPC endorse the Strategy
10. WAPC and LG publish the finalised Strategy.

In respect to points 1 to 3, the City started this process prior to the Regulations being in place and completed tasks consistent with the requirements in place at that time.

### Submission

N/A

### Report

The purpose of this report is for Council to consider the draft Strategy having regard to submissions raised and decide whether to support the Strategy with or without modifications, in accordance with Part 3 clause 14 of the *Planning and Development (Local Planning Schemes) Regulations 2015*.

### Overview of the Strategy

Part One of the draft Strategy leads with the vision:

*“To create a sustainable, healthy, connected and prosperous Cockburn community”.*

This vision is derived from the outcomes of the Strategic Community Plan (2020–2030), the principles of the State Planning Strategy and is intended to capture their intent with a land use focus.

The vision is to be achieved through the following key themes and their respective objective:

Key Theme	Objective
Environment	Our City will protect and enhance environmental values and the diverse natural landscape, promote sustainability, and respond to a changing climate.
Urban Growth and Settlement	Our neighbourhoods will have a distinctive local character that is valued by the community, offer high levels of amenity, and housing that responds to the needs of residents, to optimise their health and wellbeing.
Economy and Employment	Our economy will mature and evolve to capitalise on emerging industries, attract investment and create broader local employment opportunities so that we prosper.
Infrastructure	Our City will be connected and easy to move around, safely for all users, with high amenity pedestrian and cycling connections, making it easier to make sustainable travel choices.
Governance	Our City will be able to adapt and respond to change to efficiently manage the challenges of growth and respond to community needs while working towards our vision.

Each theme is supported by a set of Planning Directions, Actions and Evaluation Measures within Part 1 of the Strategy.

Part One also includes ten 'Planning Areas' where it is considered specific analysis and guidance is required. For each 'Planning Area' there are either relevant principles, or specific 'Planning Directions' and actions identified.

#### Outcomes of Community Consultation

A total of 241 submissions were received during the advertising period.

All submissions have been addressed in the Schedule of Submissions (refer Attachment 4), with large attachments that were included with submissions (that could not be included in the table) (refer Attachment 5).

The key issues raised are discussed under the headings that follow.

#### *Lake Coogee 'Urban Deferred' Area*

Letters were sent to landowners in the 'Lake Coogee Urban Deferred Area' inviting comment on the draft Strategy.

Consultation confirmed the City's earlier investigations that the community do not support use of the area for industrial purposes and requested that the City advocate for removal of its 'Industrial Investigation Area' designation in *the 'Perth and Peel @3.5million'* and underlying *'South Metropolitan Peel Sub-Regional Planning Framework'*.

There was general support for proposed 'Planning Area E: Lake Coogee Urban Deferred Area', which proposes investigation into whether residential development is appropriate for the area, subject to appropriate consideration of odour impacts from the Woodman Point Wastewater Treatment Plant.

Whilst the draft Strategy identifies this as an action for the next 0-5 years a number of landowners wanted greater commitment for this to be prioritised.

It is recommended that an appropriate timeframe for this project be determined upon WAPC approval of the Strategy (noting that the Water Corporation and Kwinana Industries Council remain steadfastly opposed to the introduction of further sensitive uses in this area).

#### *Rural Water Protection zone*

Letters were sent to landowners in the 'Rural Water Protection' zone both outside and within the *Perth and Peel @3.5million* 'Planning Investigation Area' (PIA), inviting comment on the draft Strategy.

A number of submissions expressed a desire to subdivide, with stated reasons including that the lots were too large to maintain; that rural amenity had been significantly degraded; and that the land is ideally located to accommodate the urgent need for further housing within the Perth metropolitan area.

Within the PIA many submissions expressed frustration regarding the length of time the investigation has taken, and a desire for certainty regarding the area's future.



Until such time as the WAPC make a decision regarding the PIA (and the broader groundwater protection area), it is recommended that the land retain its existing 'Rural Water Protection' designation under the local planning framework, consistent with State Planning Policy 2.3 - Jandakot Groundwater Protection (SPP 2.3).

Should there be a change at the State Planning framework level the City's local planning framework will respond, with the draft Strategy outlining the need for a district structure plan (DSP) to ensure coordinated development occurs.

This is particularly important given the constraints of the area, the highly fragmented landownership and existing road network.

In response to the added complexity and risk associated with suggestions that the PIA be broken into smaller DSP precincts, it is recommended the Strategy be amended to make clear the City's expectation that the plan encompass the entire PIA, to ensure a fair and equitable planning outcome.

The draft Strategy suggests the new Scheme includes the 'Rural Water Protection' zone, inclusive of a set of approvable uses in the land use table. This will provide an improved framework and greater certainty for considering proposals in the area.

#### *Banjup 'Rural Water Protection' Area*

The Banjup Residents Group and a number of other submissions requested that the Banjup Preservation principles (adopted by Council in October 2022) be inserted into the Strategy.

Whilst these principles were not subject to community consultation, and a number of submissions expressed a contrary view, it is recommended they be included as an Appendix to the Strategy (refer Attachment 8), as a starting point for future investigation/consultation into the desired future character of the area.

A key concern raised for Banjup 'Rural Water Protection' area relates to the protection of rural amenity in the event that some or all of the area is rezoned to 'Urban' under the Metropolitan Region Scheme (MRS).

In the first instance, it is important to note that at present, no proposal has been received to rezone any of the area, and it is not identified as an area of change under Perth and Peel @3.5million.

Should this change, similar to Lake Coogee and the Jandakot/Treeby PIA areas, the Governance section of the draft Strategy sets out the City's expectation that a DSP accompany any 'major' MRS Amendment proposal that seeks to change land use, development and/or subdivision outcomes (across the City).

A standard requirement of DSP's is to consider the logical staging of development, including appropriate interfaces between different zones (such as rural and urban or industrial zoned land).

However, it should be noted that maintaining rural amenity within an area rezoned 'urban' may not be achievable. For instance, if the entire area were rezoned 'urban', then retention of a 'rural' character would no longer be applicable.

#### *Kinley/Lyon Road 'Rural Water Protection' area*

The boundary of the MRS 'Rural – Water Protection' zone south of Armadale Road has a spatially regular western interface with the 'urban' zone, except for the area bounded by properties on Kinley, Lyon and Rowley Roads.

This area projects further westward, interfacing with the 'urban' zone to the north and west as shown below.



These spatial characteristics, and identification within various State Government strategic planning documents of the 1990s as 'future' or 'potential' urban, has precipitated ongoing requests from landowners for an MRS rezoning to 'Urban'.

The draft advertised Perth and Peel @3.5million defined urban expansion as *"land that has been identified for potential urban development in preceding planning studies, or represents the logical expansion of an existing urban area"*.

In recognition of the land's previous identification, the City's 2015 submission on Perth and Peel @3.5million advocated for this area to be identified as 'Urban Expansion'.

This recommendation was not adopted by the State Government and the land was not identified in the final adopted version of Perth and Peel @3.5million as either an 'Urban Expansion' or 'Urban Investigation Area'.

Whilst the City is obligated to ensure its local planning framework is consistent with the sub-regional planning framework, in recognition of this background and significant community support for such a change, it is recommended that a new 'Planning Area' be identified in the Strategy for this area, to provide a framework to consider future MRS amendment requests for this area (as discussed under the 'Key recommended modifications' section that follows).

#### *Rural and Rural Living zones*

A number of submissions from landowners in the 'Rural' and 'Rural Living' zone expressed the desire to be able to subdivide, primarily to create lots that are more manageable for landowners.

In the 'Rural Living' zone a number of submissions expressed the desire to be able to subdivide into two lots to create 2,000m<sup>2</sup> lots.

It is acknowledged that many landowners would like the opportunity to subdivide their land, however the City's rural and rural living zones are largely constrained by the *Environmental Protection (Kwinana) (Atmospheric Wastes) Policy 1992* buffer area ("EPP buffer"), within which the creation of additional dwelling rights and/or sensitive land use is not accepted by the WAPC (the determining agency for both Local Planning Strategies and subdivision applications). It is for this reason that these areas are identified as remaining rural in Perth and Peel @3.5million.

Of note, the City also received a number of submissions requesting no further subdivision be permitted and supporting the general vision for the area as outlined in the draft Strategy.

Many landowners recognised the importance of these areas for their conservation and landscape values, with the draft Strategy including actions to further document the character of this area and identify measures to protect it.

#### *Former Glen Iris Golf Course*

The Jandakot Residents and Ratepayers Association made a submission on the draft Strategy, requesting deletion of 'Planning Area D: Glen Iris Golf Course Site'.

A number of other submissions also expressed concern regarding the future of the site.

'Planning Area D: Glen Iris Golf Course Site' has been included in the draft Strategy at the request of the Department of Planning, Lands and Heritage (DPLH).

It does not advocate for any particular outcome and includes only one 'Planning Direction' being: *'Land use and development that achieves an appropriate interface with existing residential land uses.'*

The associated Action is: *'Consider any proposed land use or zoning changes against the State and local planning framework, including consideration of the following (among other things):*

- *An appropriate interface to surrounding landholdings*
- *Impact on environmental values*
- *Facilitating good connectivity for pedestrians and cyclists*
- *Consideration of bushfire risk*
- *Traffic.'*

The former golf course forms the subject of rezoning and draft structure plan proposals that (at the time of writing this report) are under consideration by the State Government.

In the absence of a final decision by the Minister (on the rezoning) and WAPC (on the structure plan), it is considered that the proposed high-level 'Planning Direction' and Action of 'Planning Area D' (that do not preclude either a retained golf course or residential outcome) remain appropriate in this context.

The broader need for golf facilities within the City will be further evaluated as part of its forthcoming review of its Community Sport and Recreation Facilities Plan.

#### *Traffic and transport matters*

A number of submissions raised specific traffic and transport issues.

The draft Strategy is primarily a land use planning tool and cannot specifically address these issues, however it does set the high-level objectives to ensure these matters are considered wherever possible.

This includes an Integrated Transport Strategy (ITS) approach and a shift towards active and sustainable transport that will assist with addressing traffic congestion.

#### *Pedestrian and cycling infrastructure*

A number of submissions requested improvements to footpaths and cycle paths throughout the City, making it clear that this is a priority for the community.

While this level of detail is again beyond the scope of the Strategy, it does include high-level objectives to improve connectivity and walkability for bike riders and pedestrians.

### *Key community priorities*

The consultation identified key priorities and areas for improvement, including:

- Protection of the environment
- Footpath improvements
- Streetscape and verge improvements and street tree planting.

This valuable feedback will be used to inform the future scoping and preparation of Local Area Plans for which there was broad community support.

### Key recommended modifications

A wide range of modifications are recommended in response to submissions received from the community and government agencies. Given the amount of time that has passed since the Strategy was drafted in 2020 a number are also recommended in response to changes that have occurred to ensure it is up to date.

All recommended modifications are set out and explained (refer Attachment 1), with the most notable modifications discussed below:

#### *Inclusion of additional 'Planning Area K: Kinley/Lyon Road area*

As discussed in the previous section, the MRS 'Rural – Water Protection' zone area bounded by properties on Kinley, Lyon and Rowley Roads has been the subject of numerous MRS amendment requests.

This is due to its location adjacent to the 'urban' zone, and identification within various State Government strategic planning documents (dating back to the 1990's) as 'future' or 'potential' urban.

To ensure the appropriate preparation and consideration of likely future MRS amendment requests for the area, it is recommended that the Strategy include this area within a new 'Planning Area' ('Planning Area K: Kinley/Lyon Road Area' (refer Attachment 6).

#### *Inclusion of additional 'Planning Area L: Bibra Lake Recreational Area'*

Most recently updated in 2015, the Bibra Lake Management Plan identifies a distinct Planning Area (shown below), that encompasses a variety of privately owned land and public reserves that surround and have an important relationship to the environmental and landscape characteristics and values of Bibra Lake.



Whilst both the Bibra Lake and broader Beeliiar Regional Park Management Plans provide suitable guidance for the reserved land, they lack the planning rigour necessary to suitably consider future expansion and/or redevelopment of the privately owned MRS 'Private Recreation' zoned lots located along Gwilliam Drive and North Lake Road that include uses such as Adventure World and the Perth Waldorf School.

At present these lots form the subject of two 'Special Use' zones under the local scheme, which allow for a limited range of uses including education, private recreation, restaurant, health studio, club premises and fast-food outlets.

The vision for this precinct is to facilitate appropriate uses that respect and complement the regionally significant recreational, conservation and cultural values of Bibra Lake and support and enhance the visitor experience to the precinct.

Inclusion within a new 'Planning Area' in the Strategy provides the opportunity to articulate this, and to identify 'Planning Directions' and 'Actions' for this precinct.

Attachment 7, Draft 'Planning Area L: Bibra Lake Recreational Area' includes actions such as a review of the 'Special Use' zones to identify a potentially broader range of uses that deliver (and not compromise) this vision.

It also recommends consideration be given to amending the MRS to include the privately owned 'Parks and Recreation' reserved lots (such as the southern-most Adventure World landholdings and Cockburn Ice) to the 'Private Recreation' zone.

#### *Inclusion of 'Banjup Principles of Preservation'*

As discussed earlier in the report, it is considered appropriate to include the Banjup Preservation Principles adopted by Council on 13 October 2022 within the Strategy.

However, given they were not subject to consultation and do not necessarily reflect the view of the entire Banjup community, it is recommended that they be referred to and included as an Appendix to the Part 2 (Background Information and Planning Context) portion of the Strategy.

This should be entitled 'Principles for Preservation of Banjup (as adopted by Council 13 October 2022)' (refer Attachment 8).

#### *Elevation of Cockburn Central Activity Centre*

Cockburn Central is currently a Secondary Centre. Council's vision is for it to be positioned as a Strategic Metropolitan Centre and the most influential Activity Centre in the South West Metropolitan Sub-Region by 2031.

Elevation to a Strategic Centre will recognise its emerging function and aspirations of the City.

The draft Strategy includes Cockburn Central Activity Centre within 'Planning Area H: Cockburn Central Activity Centre'.

This includes an action to investigate the opportunity for it to be reclassified to a strategic metropolitan centre based on the function of the centre.

At the time of preparing the draft Strategy there was no mechanism to change the State's adopted activity centre hierarchy.

In August 2020, the WAPC released a draft update to 'State Planning Policy 4.2 – Activity Centres' and associated 'Implementation Guidelines' which include a pathway to pursue such a change.

The draft states that "*change to the activity centre hierarchy can occur:*

- If identified in an endorsed local planning strategy; and
- Based on a determination by the Western Australia Planning Commission."

It is therefore considered an opportune time to include the elevation of Cockburn Central to a Strategic Centre, and it is recommended that the relevant 'Planning Direction 1.0' be amended as such, to state: *'Cockburn Central Activity Centre being reclassified as a Strategic Centre.'*

With the associated action: *'Prepare and lodge a request with the WAPC to elevate Cockburn Central to a Strategic Centre.'*

It is also recommended that Part 2 be amended to reflect this change, and that additional information be included in a new appendix (Appendix C: refer Attachment 9), which demonstrates that Cockburn Central Activity Centre is well-placed in comparison to the existing ten Strategic Centres, especially in terms of area, catchment population, future population and employment.

It is recommended that a summary of the key areas of comparison be included in Part 2 (Section 5.2).

#### *Future identification of Coastal Nodes*

*State Planning Policy 2.6 'State Coastal Planning Policy'* recognises the need for the provision of coastal nodes within the foreshore reserve that provide for a range of facilities to benefit the broader public.

The associated *'State Coastal Planning Policy Guidelines'* identify a coastal node hierarchy (regional, district and local), and give examples of the scale and types of facilities expected for each type of coastal node.

The City has not formally identified coastal nodes, and it is considered that identifying a hierarchy of nodes along the coast will assist in guiding planning, community expectations and funding opportunities for the types of facilities and infrastructure to be provided. It is therefore recommended that this be included as an additional 'Planning Direction' as follows:

*'Planning for coastal zone land that balances competing needs and aspirations in a way that takes into account the values of the coastal zone, which include scenic, aesthetic and ecological qualities; recreational opportunities; and social, indigenous, cultural and economic importance.'*

With the following associated Action (9.1): *'Identification of a coastal node hierarchy through a coastal planning strategy and/or foreshore management plan(s) to ensure that coastal nodes are appropriately developed to provide connectivity, access, amenity and recreational opportunities'*.

#### *Regional Open Space (Active Planning Fields)*

The Perth and Peel @3.5 million sub-regional framework identifies provision of a site of 20ha for 'Open Space-Sport' within the City of Cockburn as a strategic direction/priority.



However, the potential to deliver such a facility is constrained by a lack of suitable sites. A potentially viable alternative is the creation of a facility within the City of Kwinana, south of Hammond Park.

It is therefore recommended that an action be included supporting the City of Kwinana's continued investigation into the need and feasibility for a Regional Open Space facility (active planning fields), as a viable alternative.

#### *Woodman Point Marine Services Precinct*

A submission was received from the Department of Transport (DoT) Maritime regarding the proposed Woodman Point Marine Services Precinct.

DoT is steadily progressing planning for the precinct to include an expansion of the existing recreational boat launching facility and associated boat storage facilities located immediately adjacent to the site (Cockburn Pleasure Boat Storage and Cockburn Power Boat Club), as well as a range of other marine-related land uses.

In recognition of this it is recommended that Part 2 of the Strategy include reference to this precinct, its future development as a strategic boating precinct, and the benefit of the boating precinct to future employment, tourism, and recreation.

#### Conclusion

It is recommended that Council support the draft Local Planning Strategy (refer Attachment 3), modified in the manner outlined in Attachment 1, to address the many and varied issues raised in the submissions, and that the draft Strategy be forwarded to the WAPC for their review and final endorsement.

### **Strategic Plans/Policy Implications**

#### Local Economy

A sustainable and diverse local economy that attracts increased investment and provides local employment.

- Plan for and facilitate opportunities for local business (including home business and sole traders), local activity centres and industry to thrive.
- Ensure the City is 'Easy to do Business with' through reduction in red tape and improved business focused processes.

#### Environmental Responsibility

A leader in environmental management that enhances and sustainably manages our local natural areas and resources.

- Sustainably manage our environment by protecting and enhancing our unique natural coastal, bushland, wetlands areas and native wildlife.

#### Community, Lifestyle and Security

A vibrant healthy, safe, inclusive and connected community.

- Foster local community identity and connection through social inclusion, community development, and volunteering opportunities.

### City Growth and Moving Around

A growing City that is easy to move around and provides great places to live.

- Plan to provide residents with great places to live, activated social connections and high quality open spaces.
- Develop Cockburn Central as our City centre and strengthen local area localities through planning and activation.

### Listening and Leading

A community focused, sustainable, accountable and progressive organisation.

- Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.
- Listen to, communicate, consult and engage with our residents, businesses and community in a timely, open and collaborative manner.

### **Budget/Financial Implications**

Barring any request for further significant investigation or justification, no additional budget or resourcing is required to finalise the Strategy.

Ultimately, the endorsed Strategy will play an important role in guiding the prioritisation and use of future resources towards achieving the agreed vision, planning directions and actions as outlined in the document.

### **Legal Implications**

Section 11 of the *Planning and Development (Local Planning Schemes) Regulations 2015* (Regulations) mandates the preparation of a Local Planning Strategy to accompany a new Local Planning Scheme.

Section 11. (2) requires a Local Planning strategy to:

- “(a) set out the long-term planning directions for the local government; and*
- (b) apply any State or regional planning policy that is relevant to the strategy; and*
- (c) provide the rationale for any zoning or classification of land under the local planning scheme.”*

Section 12 defines the certification process for approval to advertise the Strategy.

Section 13 details the statutory obligations for advertising which includes a 21-day public submission period.

Section 14 defines the process as assessing submissions received and Section 15 details timeframes and options for the WAPC when considering finalisation.

## Community Consultation

In preparing the Draft Strategy, significant community consultation was undertaken, well beyond the City's statutory obligations, including:

- preparing local profile papers and seeking preliminary feedback
- undertaking six focus group workshops to establish objectives and aspirations for key areas including the:
  - Australian Marine Complex (AMC)
  - Rural Zone (Henderson and Wattleup)
  - Rural Living Zone (Beeliar)
  - Resource Zone (Banjup)
  - Resource Zone (Treeby and Jandakot)
  - Lake Coogee (land within the wastewater treatment plant buffer).

The outcomes of this preliminary consultation heavily informed the direction and content of the draft Strategy that was subsequently advertised for public comment.

As outlined above, the Statutory requirement is a minimum 21-day public submission period.

However, to enable comprehensive consultation and assist in achieving the best quality feedback, WAPC permission was obtained for an extended 60-day period running from the 22 September to 21 November 2022.

The City then advertised the strategy in the following ways:

- Comment on Cockburn website
- City of Cockburn website
- E-newsletter to Comment on Cockburn subscribers
- E-newsletter to resident groups
- E-newsletter to a random sample of the ratepayer database
- E-newsletter to business news subscribers
- Flyer dropped to all residential letterboxes
- Graphic in staff email signatures
- Social media posts (organic and paid)
- Displays at City libraries and administration building
- Community newspaper (PerthNow Cockburn) advertisements
- Letters to local authorities potentially interested in the strategy
- Letters to major developers in Cockburn.

Letters were also sent to residents in the following areas:

- Lake Coogee 'Urban Deferred' area (wastewater treatment plant buffer)
- 'Rural' and 'Rural Living' zones
- Metropolitan Region Scheme Water Protection Zone (both inside and outside the Planning Investigation Area).

To provide the community and stakeholders with an opportunity to ask questions and better understand the strategy, officers ran the following drop-in information sessions:

- Phoenix Shopping Centre (3-6pm on Thursday, 20 October)
- Online session (9-11am on Saturday, 22 October)
- Success Library (4:30-6:30pm on Tuesday, 8 November)
- Beeliar Village Shopping Centre (10am-1pm on Thursday, 10 November).

The City offered one-on-one meetings with residents and businesses on request (11 requests were received).

The City also invited several resident groups to a presentation on the strategy based on the anticipated level of impact the strategy would have on their area. City officers attended the following meetings in October or November 2022:

- Bibra Lake Resident Association
- Coolbellup Resident Association
- Connecting South Lake
- Hamilton Hill Resident Association
- Jandakot Residents and Ratepayers Association
- South Coogee Community Association
- Spearwood Progress Association
- Yangebup Process Association.

The City presented relevant aspects of the strategy to the following reference groups:

- Aboriginal Reference Group
- Disability Reference Group
- Age Friendly Reference Group.

### **Risk Management Implications**

Supporting the resolution will ensure that the planning framework is contemporary and reflects the needs and aspirations of the community.

Not supporting the resolution will stop progression of downstream planning instruments (Scheme and Local Planning Policy reviews), inhibiting the City's ability to modernise its local planning framework to guide future land use and development proposals in more appropriate manner.

Furthermore, the City would still be obligated to review its Strategy under the Regulations and request/justify the associated ongoing timeframes.

### **Advice to Proponent(s)/Submitters**

N/A

### **Implications of Section 3.18(3) *Local Government Act 1995***

Nil

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## City of Cockburn Draft Local Planning Strategy – Schedule of Modifications (April 2023)

No.	RECOMMENDED MODIFICATION	REASON
<b>PART 1 REPORT MODIFICATIONS</b>		
1	<p>Modify Part 1 – Rural Water Protection (Page 16) to include the following:</p> <p><i>'The Jandakot Groundwater Mound contributes a significant volume of high-quality water to Perth's integrated water supply scheme. Rural Water Protection zones are within the assigned Priority 2 Public Drinking Water Source Areas where low level rural land uses are considered appropriate (generally with conditions) and the risk to the water resource is minimised. Further guidance found at State Planning Policy 2.3 – Jandakot Groundwater Protection and Water Quality Protection Note 25: Land use compatibility tables for public drinking water source areas (DWER, 2021).'</i></p>	<p>The modifications are recommended in response to the Department of Water &amp; Environmental Regulation feedback to provide further clarification regarding the contribution to Perth's integrated water supply scheme.</p>
2	<p>Update Part 1 to:</p> <p>Refer to the environmental values of rural zones.</p> <p>Reword references to the area being used primarily for agricultural purposes, to clarify that some areas of land were not cleared and remained bushland.</p> <p>Clarify that 'most' of the City's 'Rural' zone is within the EPP buffer (i.e. there are <u>not</u> large areas outside the buffer within the 'Rural' zone, those areas outside the buffer are within the 'Rural Water Protection' zone).</p>	<p>To provide clarification regarding the values of the rural zone including environmental values, and to clarify references to the EPP buffer as being within the 'Rural' zone (not 'Rural Water Protection' zone).</p>
<b>PLANNING DIRECTION / ACTION MODIFICATIONS</b>		
3	<p>Modify 1.0 Environment - 'Planning Direction 3.0'to include reference to rural land as follows:</p> <p><i>'Maximise all opportunities to increase and retain tree canopy cover to reduce the heat island effect, provide habitat, enhance amenity, protect local character and improve the health and well-being of residents, including:</i></p> <p><i>Tree canopy within the public realm that is maintained, protected and increased over time.</i></p> <p><i>More trees and gardens within existing and proposed urban <b>and rural</b> environments, including private properties.'</i></p> <p><i>Commercial and industrial developments to contribute to tree canopy cover.</i></p>	<p>To reflect the intention to investigate measures to retain vegetation on rural land, and in recognition of the environmental value of vegetation on rural land, and the significant contribution rural land makes to ecological corridors.</p>
4	<p>Modify 2.0 Urban Growth and Housing - Action 1.3 to specify the rural water protection zone as –</p> <p><i>'Identify ways to support and strengthen the unique character, identity and amenity of rural,</i></p>	<p>To make it clear that the character, identity and amenity will be defined for each of the different rural zoned areas. There will be some differences between the character of the different rural zoned areas (for example the 'Rural' zone will have</p>

## City of Cockburn Draft Local Planning Strategy – Schedule of Modifications (April 2023)

No.	RECOMMENDED MODIFICATION	REASON
	<i>rural living and rural water protection areas, including placemaking opportunities.'</i>	productive agricultural uses whereas the 'Rural Water Protection' zone will not, and this changes the expected character of the areas).
5	Amend the scope of 2.0 Urban Growth and Housing - Action 8.1 to: <i>'Identify the various activity centres within our community and articulate their role, function and growth expectations within the Local Commercial and Activity Centres Strategy' from 'low' to 'medium'.</i>	It is considered that the scale of the project should be 'medium', as it is an action that will affect several stakeholders and business units within the City. It is therefore recommended that this be amended.
6	Under 2.0 Urban Growth and Housing, include an additional Action (14.4 - 'Infrastructure that keeps pace with growth to support Cockburn as a resilient and liveable place to work and live.') as follows: <i>'Ensure the early planning of new school sites through the preparation of structure plans (district, local and precinct) and subdivisions where residential development is proposed.'</i>	In response to the submission by Department of Education, and in recognition of the updated Operational Policy 2.4 Planning for school sites (OP 2.4), recognising that the location and planning of school sites requires early planning to ensure they are accessible and well-connected to meet the needs of the community.
7	Under 2.0 Urban Growth and Housing, replace Action 10.5 ('Continue to investigate options for 20 hectares of regional open space in accordance with the South Metropolitan Peel Sub-Regional Framework'), with the following: <i>'Support the City of Kwinana's continued investigation into the feasibility and need for Regional Open Space (active playing fields).'</i>	Perth and Peel @3.5 million sub-regional framework identifies provision of a site of 20ha for 'Open Space-Sport' within the City of Cockburn as a strategic direction/priority. The potential for such a facility is constrained by a lack of suitable sites. A potentially viable alternative is the creation of a facility within the City of Kwinana. Therefore recommended that an action be included supporting the City of Kwinana's continued investigation into the need and feasibility for a Regional Open Space facility (active planning fields), as a viable alternative.
8	Modify 4.0 Infrastructure – Action 1.1 to specify the City as the 'Lead' for this project.	Action 1.1 sets out for the City to work with State Government to develop an integrated movement plan, and identifies the City as a partner. The development of an Integrated Transport (or movement) Plan is typically led by the local government, with State Government agencies being key stakeholders.
9	Reword 5.0 Governance – Action 2.1 to state: <i>'Create a transparent local planning framework that provides clarity around how and why planning decisions are made, including when and why we will use discretion and depart from policy using an evidence-based approach.'</i>	To include reference to 'why' decisions are made to improve transparency around decision-making.
10	Clarify 5.0 Governance – Action 4.6 by rewording as follows: <i>'Identify an appropriate range of land uses in the City's TPS3 'rural' and 'rural living' zones (to be zoned 'rural') to provide for rural pursuits while protecting rural lifestyle amenity and character.'</i>	This action is intended to refer to the City's rural and rural living zones under TPS3, not the 'Resource' zone, given the Rural Water Protection area land uses are outlined in SPP 2.3.

## City of Cockburn Draft Local Planning Strategy – Schedule of Modifications (April 2023)

No.	RECOMMENDED MODIFICATION	REASON
11	<p>Modify 5.0 Governance – to include an additional ‘Planning Direction’ (9) under as follows:</p> <p><i>‘Planning for coastal zone land that balances competing needs and aspirations in a way that takes into account the values of the coastal zone, which include scenic, aesthetic and ecological qualities; recreational opportunities; and social, indigenous, cultural and economic importance.’</i></p> <p>Include the following associated Action (9.1):</p> <p><i>‘Identification of a coastal node hierarchy through a coastal planning strategy and/or foreshore management plan(s) to ensure that coastal nodes are appropriately developed to provide connectivity, access, amenity and recreational opportunities.’</i></p>	To respond to SPP 2.6 ‘State Coastal Planning Policy’ and the associated guidelines which identify a coastal node hierarchy (Regional, district and local). Identifying the type of node will assist in guiding planning, community expectations and funding opportunities for the types of facilities and infrastructure to be provided.
<b>PLANNING AREA MODIFICATIONS</b>		
12	<p>Modify ‘Planning Area B: Jandakot/Treeby Planning Investigation Area’ 2.0 Action 1 to state:</p> <p><i>‘Identification of remnant bushland, ecological corridors and areas of conservation value (including Carnaby Cockatoo habitat) to be protected, and measures to protect them, prior to any land use or MRS zoning change.’</i></p>	In response to comments from the Department of Water and Environmental Regulation (DWER), and to reflect the importance of Carnaby Cockatoo habitat in the area.
13	<p>Modify ‘Planning Area B - Jandakot / Treeby Planning Investigation Area’ 5.0 to include an additional Action (3) as follows:</p> <p><i>‘Future development of the area is to include investigation into the provision of a new road connection between Dean Road and Lakes Way for local connectivity, bushfire safety and public transportation improvement purposes.’</i></p>	To reflect the desire to achieve this connection that was identified through consideration of the Scheme Amendment and Structure Plan for the former Glen Iris Golf Course site for improved local connectivity, traffic management, and fire safety.
14	<p>Modify ‘Planning Area B - Jandakot / Treeby Planning Investigation Area’ 6.0 ‘Planning Direction’ specifying that in the event of an MRS Amendment, a district structure planning approach is to include the <u>entire</u> PIA.</p>	To reflect the intent of the district structure planning approach which is to ensure comprehensive planning for the whole area, including road network, bushfire planning, environmental issues, efficient servicing and community and educational infrastructure etc.
15	<p>Modify ‘Planning Area B: Jandakot/Treeby Planning Investigation Area’ 6.0 Action 1 to include a Retail Needs Assessment as follows:</p> <p><i>‘Appropriately sized and located centre to meet the needs of the community, based on a Needs Assessment.’</i></p>	In line with the requirements of SPP4.2 ‘Activity Centres for Perth and Peel’, and to ensure the City’s hierarchy of centres is protected whilst ensuring the needs of the community are being met.
16	<p>Modify ‘Planning Area C: Jandakot Industrial Investigation Area’ 3.0 Action 1 to state:</p> <p><i>‘Requirement for an appropriate vegetated buffer to the eastern interface of the Planning Area in the local planning scheme and framework.’</i></p>	To reflect the adopted Local Development Plan (LDP) and approved subdivision for the subject land.

## City of Cockburn Draft Local Planning Strategy – Schedule of Modifications (April 2023)

No.	RECOMMENDED MODIFICATION	REASON
17	Modify 'Planning Area F: Rural Living Precinct' 1.0 to include an additional Action (2) that states: <i>'City to advocate for an appropriate interface to the south that respects the rural lifestyle amenity of the precinct'.</i>	In response to the anticipated redevelopment of the Cockburn Cement site to the south. Whilst this area is subject to the Hope Valley Wattleup Act, the City can advocate for an appropriate interface.
18	Replace 'Planning Area H: Cockburn 'Planning Direction 1.0' which states <i>'Investigate the opportunity for it to be reclassified to a strategic metropolitan centre based on the function of the centre.'</i> With the following: <i>'Cockburn Central Activity Centre being reclassified as a Strategic Centre.'</i> Replacing Action 1 which states: <i>'Investigate the opportunity for it to be reclassified to a strategic metropolitan centre based on the function of the centre.'</i> with the following: <i>'Prepare and lodge a request with the WAPC to elevate Cockburn Central to a Strategic Centre.'</i>	The draft Strategy includes Cockburn Central Activity Centre within 'Planning Area H: Cockburn Central Activity Centre' and includes an action to investigate the opportunity for it to be reclassified to a strategic metropolitan centre based on the function of the centre. At the time of preparing the draft Strategy there was no mechanism for changes to the activity centre hierarchy, however since then the WAPC have released draft Activity Centres Strategy & Implementation Guidelines (November 2021) which include new provisions for changes to the hierarchy and new activity centres. This provides the opportunity for the City to seek reclassification, and it is recommended that the wording be changed accordingly.
<b>PROPOSED NEW PLANNING AREAS</b>		
19	Inclusion of an additional Planning Area - 'Planning Area K: Kinley/Lyon Road Area'	This area of the 'Water Protection' zone interfaces with the 'urban' zone to the north (Aubin Grove) and west (Hammond Park). This spatial characteristic, and identification within State Government strategic planning documents of the 1990s as 'future' or 'potential' urban, has precipitated ongoing requests from landowners for rezoning to 'urban'.  Whilst this recommendation was not adopted by the State Government (the land was not identified in the final adopted Perth and Peel @3.5million as an urban expansion or investigation area), and the City is obligated to ensure its local planning framework is consistent with the sub-regional planning framework, inclusion of the land within 'Planning Area K' is proposed to ensure an appropriate framework is in place to ensure the appropriate preparation and consideration of future MRS amendment requests for the area.
20	Inclusion of an additional Planning Area - 'Planning Area L: Bibra Lake Recreational Area'	To identify high-level planning direction for the 'Planning Area' identified in the Bibra Lake Management Plan, which includes lots zoned 'Private Recreation' under the MRS that lack guidance as to how they can support the strategic intent of the precinct, and to inform the local planning scheme zoning for the privately owned lots.
<b>PART 2 MODIFICATIONS</b>		



## City of Cockburn Draft Local Planning Strategy – Schedule of Modifications (April 2023)

No.	RECOMMENDED MODIFICATION	REASON
21	Update Part 2, Section 2 to include references to State Planning Policy 2.8 Bushland Policy for the Perth Metropolitan Region (SPP 2.8).	SPP 2.8 is an important policy that applies to Bush Forever areas and local bushland within the Perth Metropolitan Region.
22	Modify Part 2, Section 2.1.2 (Rural land and linkages) to remove reference to land becoming 'highly' fragmented.	While this description has come from 'Towards a Green Network' the current reference to vegetation being 'highly' fragmented is subjective.
23	Update Part 2 (Background Profile and Analysis) Section 2.5.2 (State Planning Policy), 2.6 (State Coastal Policy) and 6.3 (Coastal Areas) to reference the proposed additional Part 1, 5.0 Governance 'Planning Direction' (9) and the associated Action regarding coastal planning and nodes.	To reflect the proposed additional 'Planning Direction' (9) and action and provide further context to SPP 2.6 'State Coastal Planning Policy'.
24	Replace the 3 <sup>rd</sup> paragraph of Part 2, Section 2.6.3 (Extent of Groundwater Protection Area) with the following: <i>'The Western Australian Planning Commission (WAPC) identified the Treeby development site through a strategic government planning process as the site was already substantially cleared due its past use as a quarry as well as other economic and community factors. This resulted in the preferred strategic outcome of urban development with the acceptance of risk and potential future loss of water resources supplying the Perth integrated water supply scheme. Once the WAPC identified the site as future urban, DWER was requested to review the Treeby District Water Management Strategy within the context of a P3 PDWSA. However the DWMS did not determine the acceptability of the rezoning from Rural Water Protection to Urban, this was a strategic planning decision of the WAPC.'</i>	The modifications are recommended in response to the Department of Water & Environmental Regulation feedback to accurately outline the background to the development of Treeby.
25	Modify Part 2, Section 4 (Economy and Employment) to include a high-level brief commentary on the key industrial areas of the Western Trade Coast, Kwinana Industrial Area, Australian Marine Complex, Latitude 32 and the proposed new container port, as well as their importance in relation to employment opportunities and broader connectivity to the Perth Metropolitan road and rail network.	In recognition of the importance of employment areas and their connectivity.
26	Rename Part 2, Section 5.1.1 'Potential additional neighbourhood centre - north east' to 'Potential additional local/neighbourhood centre - north east'; and addition of the following commentary: <i>'An additional centre in the north-east will need to be justified through a Needs Assessment, demonstrating whether a new centre is required and whether this be designated as a Local or</i>	The draft Strategy flags a potential need for an additional centre in the north-east future to be explored through the review of the Local Commercial and Activity Centre Strategy (LCACS). It is agreed that this need would require justification through a retail Needs Assessment, and to determine whether this should be a local or neighbourhood centre.

## City of Cockburn Draft Local Planning Strategy – Schedule of Modifications (April 2023)

No.	RECOMMENDED MODIFICATION	REASON																																																																																
	<i>Neighbourhood Centre. Any additional centre will need to respond to the needs of the community without negatively impacting the City's hierarchy of centres.'</i>																																																																																	
27	<p>Modify Section 5.2 (Secondary Centres) to refer to 'Planning Area H: Cockburn Central Activity Centre' to include the following text:</p> <p><i>'Appendix C demonstrates that Cockburn Central Activity Centre is well-placed in comparison to the existing ten Strategic Centres terms of area, catchment population, future population and employment. The tables below summarise this comparison:</i></p> <table border="1"> <thead> <tr> <th colspan="2">Area</th> <th colspan="2">Future Population</th> </tr> </thead> <tbody> <tr><td>1</td><td>Mandurah 800 ha</td><td>1</td><td>Rockingham 36,000</td></tr> <tr><td>2</td><td>Rockingham 600 ha</td><td>2</td><td>Stirling 25,000</td></tr> <tr><td>3</td><td>Joondalup 475 ha</td><td>3</td><td>Cannington 25,000</td></tr> <tr><td>4</td><td>Stirling 360 ha</td><td>4</td><td>Joondalup 18,000</td></tr> <tr><td>5</td><td><b>Cockburn 360 ha</b></td><td>5</td><td>Fremantle ~15,000</td></tr> <tr><td>6</td><td>Morley 220 ha</td><td>6</td><td>Midland 14,000</td></tr> <tr><td>7</td><td>Cannington 200 ha</td><td>7</td><td><b>Cockburn 13,000</b></td></tr> <tr><td>8</td><td>Midland 180 ha</td><td>8</td><td>Morley 12,000</td></tr> <tr><td>9</td><td>Yanchep 106 ha</td><td>9</td><td>Mandurah ~8,000</td></tr> <tr><td>10</td><td>Fremantle 100 ha</td><td>10</td><td>Armadale 6,000</td></tr> <tr><td>11</td><td>Armadale 85 ha</td><td>11</td><td>Yanchep 5,500</td></tr> </tbody> </table> <table border="1"> <thead> <tr> <th colspan="2">Catchment Population</th> <th colspan="2">Employment (jobs)</th> </tr> </thead> <tbody> <tr><td>1</td><td>Joondalup 350,000</td><td>1</td><td>Joondalup 17,000</td></tr> <tr><td>2</td><td>Cannington 280,000</td><td>2</td><td>Fremantle 8,500</td></tr> <tr><td>3</td><td><b>Cockburn 280,000</b></td><td>3</td><td><b>Cockburn 8,500</b></td></tr> <tr><td>4</td><td>Midland 270,000</td><td>4</td><td>Morley 5,000</td></tr> <tr><td>5</td><td>Rockingham 130,000</td><td>5</td><td>Cannington 5,000</td></tr> <tr><td>6</td><td>Armadale 120,000</td><td>6</td><td>Armadale 3,000</td></tr> <tr><td>7</td><td>Mandurah 110,000</td><td>7</td><td>Yanchep 0</td></tr> </tbody> </table>	Area		Future Population		1	Mandurah 800 ha	1	Rockingham 36,000	2	Rockingham 600 ha	2	Stirling 25,000	3	Joondalup 475 ha	3	Cannington 25,000	4	Stirling 360 ha	4	Joondalup 18,000	5	<b>Cockburn 360 ha</b>	5	Fremantle ~15,000	6	Morley 220 ha	6	Midland 14,000	7	Cannington 200 ha	7	<b>Cockburn 13,000</b>	8	Midland 180 ha	8	Morley 12,000	9	Yanchep 106 ha	9	Mandurah ~8,000	10	Fremantle 100 ha	10	Armadale 6,000	11	Armadale 85 ha	11	Yanchep 5,500	Catchment Population		Employment (jobs)		1	Joondalup 350,000	1	Joondalup 17,000	2	Cannington 280,000	2	Fremantle 8,500	3	<b>Cockburn 280,000</b>	3	<b>Cockburn 8,500</b>	4	Midland 270,000	4	Morley 5,000	5	Rockingham 130,000	5	Cannington 5,000	6	Armadale 120,000	6	Armadale 3,000	7	Mandurah 110,000	7	Yanchep 0	To support the proposal for Cockburn Central Activity Centre to be elevated to a Strategic Centre.
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28	<p>Modify Part 2, Section 6.1 (Identifying tourism aspirations and opportunities) to include the following additional key tourism themes:</p> <p>Adventure tourism activities (beyond just bike riding and trails) to diversify experience offerings in the City.</p> <p>Proposed Surf Park in Jandakot and the associated opportunities.</p> <p>Protection of the 'Private Recreation' uses associated with Bibra Lake.</p>	In response to Tourism WA submission and recognising the diversity of adventure tourism opportunities within the City, and in recognition of the future Surf Park which is a significant tourism opportunity.																																																																																
29	Modify Part 2, Section 6.3 (Coastal Areas) to include reference to the Woodman Point Recreational Boating Precinct and its future development as a strategic boating precinct, and the benefit of the boating precinct to future employment, tourism, and recreation.	The Woodman Point Recreational Boating Precinct is one of the largest and busiest recreational boating hubs in the Perth Metropolitan area. DoT Maritime Planning has established a concept design for the precinct which will see it transform into the largest recreational boating facility in Western Australia, increasing public amenity in the area, creating																																																																																

## City of Cockburn Draft Local Planning Strategy – Schedule of Modifications (April 2023)

No.	RECOMMENDED MODIFICATION	REASON
		employment, potential tourist activities and contributing to the State's economy. The current vision is to establish a Marine Business Centre facilitating a range of complimentary maritime services and business opportunities. When fully developed, it is intended that the precinct will include boat building and maintenance facilities, showrooms, and boat stacking facilities, as well as commercial leases that support recreational boating activities. It is considered that this is significant enough to require recognition in the Local Planning Strategy to inform planning for the precinct and surrounding area.
30	Modify Part 2, Section 7.1 (Open space in new areas) to include the following: <i>'It is imperative that the water source to irrigate the POS is secured early in the planning process and water efficiency measures are incorporated into its design to ensure water resources are available to maintain the active spaces. The retention of native vegetation and the planting of green spaces to provide shade and urban cooling will also benefit the community as well as reducing the requirement for large volumes of irrigation.'</i>	The modifications are recommended in response to the Department of Water & Environmental Regulation feedback to highlight the importance of considering water source for POS early in the process to inform the design, and to ensure the design of POS minimises requirements for water while meeting the needs of the community.
31	Update Part 2, Section 8.3 (Education facilities) to include reference to the Western Australian Planning Commission's (WAPC) Operational Policy – Planning for School Sites (OP 2.4), and to identify the potential need for additional schools should there be a MRS zoning change(s) that increase 'urban' zoned land, and that district structure planning will be critical to appropriately plan for this.	In response to the submission by Department of Education, and in recognition of the Western Australian Planning Commission's updated Operational Policy 2.4 - Planning for school sites (OP 2.4), recognising that the location and planning of school sites requires early planning to ensure they are accessible and well-connected to meet the needs of the community.
32	Update Part 2, Section 9.1.1 ('Rural' zone, Beeliiar and Munster) to: Remove reference to good access to transport services in the rural zone. Add locally native species planted as roadside vegetation. Add references under 'rural placemaking' to post and wire rural fencing being used to create a unique rural identity. Reword section regarding the 'market gardening history' to refer to a 'rural history' which includes market gardening and bushland conservation.	To more accurately reflect the issues, character and values of the rural zone, in response to submissions received on the draft Strategy.
33	Amend Part 2 Section 9.1.1 (Other constraints) to move information regarding Aboriginal heritage	To ensure that Aboriginal heritage sites are recognised as an opportunity and not necessarily a 'constraint'.

## City of Cockburn Draft Local Planning Strategy – Schedule of Modifications (April 2023)

No.	RECOMMENDED MODIFICATION	REASON
	significance to a new heading 'Aboriginal Heritage sites'.	
34	Modify Part 2, Section 9.2.1 (Jandakot / Treeby Planning Investigation Area) to include reference to Carnaby Cockatoo habitat where high conservation values are described.	The modifications are recommended in response to the Department of Water & Environmental Regulation feedback to highlight the importance of Cockatoo habitat in the area.
35	Modify Part 2, Section 9.3.3 ('Water Protection' zone character snapshot) to reference the Council adopted principles for Banjup and insert them as Appendix C: 'Principles for Preservation of Banjup (as adopted by 13 Council October 2022)', noting that they have not been subject to consultation but will be used as a starting point when identifying the character and amenity for Banjup.	To reflect the Banjup Preservation Principles adopted by Council subsequent to the drafting of the Strategy, whilst noting that these proposed principles were not subject to community consultation and are not necessarily considered to reflect the view of the entire Banjup community. These principles will be a starting point when identifying the character and amenity for Banjup.
36	Modify Part 2, Section 9.3.5 (Rural character and identity) to include the following: <i>'There is concern from some of the community in the 'Rural Water Protection' zone as to how to manage the interface of surrounding areas, to achieve a level of quiet enjoyment and amenity which is considered congruent with the 'Rural Water Protection' zone.'</i>	To reflect feedback received on the draft Strategy, and previous consultation undertaken in the 'Rural Water Protection' zone.
37	Update Part 2, Section 10.2.7 (Significant Trees) to reflect the City's proposed updates to the framework for 'Significant Trees' and introduction of Tree Preservation Orders.	Since the drafting of the Strategy Council have adopted updated criteria for 'Significant Trees' and has initiated a Scheme Amendment to introduce provisions for 'Tree Preservation Orders'.
38	Update Part 2, Section 12.1 (Road Network) as follows: Include in-text references to Figures 54, 55 and 56. Include reference in Section 12.1.1 (Regional Road Network) to the Main Roads Future Road Project and those roads that have been identified to potentially become State Roads. Modify references to Cockburn Coast Drive (which Main Roads WA have advised they no longer intend to deliver) and recognise the impact that will have on planning for the parallel section of Cockburn Road (which will likely need to be upgraded to provide for a 4 lane dual carriageway road in perpetuity). Update Section 12.1.1 (Regional Road Network) to reflect that the Armadale Road to North Lake Road Bridge Project has been completed. Update Section 12.1.3 (Roe Highway) to reflect that Government has passed the Beeliam Bill removing the PRR reservation through Beeliam Park and that DPLH has commenced a planning review for the	To update the section since it has been drafted, and in response to comments from Main Roads WA.

## City of Cockburn Draft Local Planning Strategy – Schedule of Modifications (April 2023)

No.	RECOMMENDED MODIFICATION	REASON
	remainder of the Roe 8/9 corridor (Roe 8 (West) and Roe 9 Corridor Planning Study).	
39	Reword Part 2, Section 12.1.1 (Regional Road Network) - East-West Links – Preserve and enhance’ to include reference to ‘rural areas’ as follows: <i>‘These roads need to be preserved and enhanced to provide improved road safety and active transport amenity, whilst minimising impacts on the surrounding environment and land use, including the City’s urban and rural areas.’</i>	To recognise the impact of roads on both urban and rural areas.
40	Modify Part 2, Section 12.1.2 (Freight Network), to include reference to the importance of movement of freight and the regional road freight network of primary / secondary freight roads as outlined in the in Perth and Peel @3.5m Transport Network.	To highlight the relevance and importance of Perth and Peel @3.5m Transport Network.
41	Update Part 2, Section 12.4 (Cycling and Walking) to include reference to the WA Long-Term Cycle Network (LTCN).	In response to the request from Department of Transport to include such a reference, and its relevance to achieving an integrated cycle network.
42	Modify Part 2, Section 15.2.2 (Informing the Scheme - – Residential codings) to reference the interface with the ‘Rural Water Protection’ zone with regards to the potential coding change to the R5 lots adjacent to Tapper Road in Atwell, as follows: <i>“...in a manner that respects the current primary street frontages, and the interface with Tapper Road, and the rural water protection zone.”</i>	Whilst the R5 lots do not directly interface with the ‘Rural Water Protection’ zone, and there is not a consistent buffer of lower density residential development along Tapper Road, the area is within close proximity to the ‘Rural Water Protection’ zone and therefore should have regard for the character of that area.
43	Modify Part 2, Section 15.2.2 (Informing the Scheme - Other zones) to include reference to consideration being given to supporting the rezoning of No. 85 (Lot 1001) Prinsep Road, Jandakot to ‘Mixed Business’, including the possible concurrent rezoning with the MRS Amendment.	The subject site is being rezoned to ‘Urban’ under the MRS, and it is considered that a zoning of ‘Mixed Business’ is most appropriate given the interface with the ‘Rural Water Protection’ zone, proximity to the Jandakot Planning Investigation Area, and its location on the Jandakot Groundwater Protection Zone.
<b>MINOR MODIFICATIONS</b>		
44	Replace references throughout the document from <i>Aboriginal Heritage Act 1972</i> to the <i>Aboriginal Cultural Heritage Act 2021</i> .	The <i>Aboriginal Heritage Act 1972</i> has been repealed and replaced since the Strategy was drafted.
45	Replacing all references to ‘European Heritage’ with ‘historic heritage’; and adopting the standard definition for ‘heritage protected place’ (as defined under the Deemed Provisions) when referring to places protected under the Scheme. Include reference to the updated ‘Guidelines for State Government Heritage’.	In response to feedback from DPLH regarding heritage terminology, and to ensure consistency with the Deemed Provisions and State Planning Framework.
46	Replace all references to ‘cyclist’ with ‘bike rider’.	To reflect terminology recommended in the Department of Transport submission, given that

## City of Cockburn Draft Local Planning Strategy – Schedule of Modifications (April 2023)

No.	RECOMMENDED MODIFICATION	REASON
		many bike riders do not consider themselves 'cyclists', which is used to reference professional or sport-focused riders, and often triggers negative connotations.
47	<p>Reword text in Part 1 under the heading 'Strategic Context' (sub-heading 'Rural Water Protection zone') on page 16 as follows:</p> <p><i>'This 'Planning Investigation Area' is adjacent to Jandakot Airport which has strategic importance as an aviation base for emergency services, also making a significant economic contribution as a strategic employment area. It is one of the busiest airfields and significant pilot training bases in Australia, operating 24 hours per day, seven days per week. This is a key constraint for this area, and its operation is to be protected as far as practicable, from development that could potentially prejudice its performance.'</i></p>	To include reference to Jandakot airport as a strategic employment area.
48	<p>Modify Part 2, Section 2.6 (Water Management) to correct the following details:</p> <p>The Department of Biodiversity, Conservation and Attractions manages wetlands; Desalinated water for scheme water is managed by the Water Corporation; and Within the City of Cockburn, the Department of Water and Environmental Regulation manages the take of groundwater through the Rights in Water and Irrigation Act 1914.</p> <p>First dot point – Superficial Aquifer. Perth's (including Cockburn) integrated water supply scheme is a mixture of groundwater, surface water, desalinated water and groundwater replenishment. Correction to the sources of scheme water to include groundwater, surface water, desalinated water and groundwater replenishment.</p> <p>Second dot point – "Superficial Aquifer used by the City and private irrigation systems administered by DWER", correction to – <i>'The Department manages groundwater resources used by the City and private irrigation systems through the provisions of the Rights In Water and Irrigation Act 1914 and the Rights in Water and Irrigation Regulations 2000.'</i></p>	In response to comments from the Department of Water and Environmental Regulation (DWER) to clarify legislation and responsible agencies.
49	<p>Reword the fifth paragraph of Part 2, Section 2.6 (Water Management) to state:</p> <p><i>'New legislation may change how groundwater-dependent ecosystems are managed in the Cockburn plan area.'</i></p>	Correction of a typographical error (this is taken from DWER's 'Cockburn groundwater allocation plan' report).

## City of Cockburn Draft Local Planning Strategy – Schedule of Modifications (April 2023)

No.	RECOMMENDED MODIFICATION	REASON
50	Modify Part 2, Section 2.6.1 (Deep aquifer water), to correct the references to the deep aquifer (Leederville and Yarragadee aquifers), as sources of groundwater for Perth's integrated water supply scheme, and Section 2.6.2 (Jandakot Groundwater Protection Area) reference to Perth's public water supply to ' <i>Perth's integrated water supply scheme</i> '.	Corrections in response to the Department of Water & Environmental Regulation feedback.
51	Update Part 2, Section 2.6.4 (Superficial Aquifer) to include further detail as follows: <i>'The use of groundwater, other than for domestic purposes, is managed through the provisions of Rights in Water and Irrigation Act 1914 and the Rights in Water and Irrigation Regulations 2000.'</i>	The modifications are recommended in response to the Department of Water & Environmental Regulation feedback, and to provide further detail regarding relevant legislation.
52	Modify Part 2, Section 9 (Rural Areas) to replace the paragraph 'This is a vitally important strategic public drinking groundwater supply for Perth' under Section 9 to state: <i>'The Jandakot groundwater mound is a highly valued resource that contributes to Perth's integrated water supply scheme as well as provides ecological integrity of important wetlands, native vegetation and habitat.'</i>	To reflect the importance of the groundwater mound in terms of ecological integrity of important wetlands, native vegetation and habitat.
53	Modify Part 2, Section 9.1.1 ('Rural' zone, Beeliar and Munster) to delete the specific reference to the total megalitres of water allocation for the rural zone.	Information regarding water allocation in the rural zone was obtained from DWER at the time of drafting the Strategy, however it is acknowledged that this may change over time. Therefore it is recommended that specific reference to the total megalitres of allocation be deleted. The purpose of this information is to demonstrate that there is restricted water availability (which is likely to become further restricted), and this significantly impacts the viability of certain rural uses.
54	Update information relating to the Westport project in Part 2, Section 12.8 'Fremantle Outer Harbour – Westport'	To reflect updates to the project, as outlined in the submission from Westport, but noting that the project will continue to develop and evolve to the information contained within the Strategy should be predominately high-level.
55	Correct Part 2, Section 13.5 (Stormwater) to reference 'Better Urban Water Management' as being a WAPC document.	'Better Urban Water Management' is a WAPC not Department of Water document.
56	Correct error in Part 2, Section 14.3.1 (Wattleup Perth and Peel Industrial Investigation Area) to refer to rural development on the eastern (not western) side of Latitude 32.	Correction of an error.
57	Update Part 2, Figure 36 ('Rural' zone) to clarify the light green areas as the 'rural' zone with a notation.	To clarify the extent of the 'Rural' zone.

## City of Cockburn Draft Local Planning Strategy – Schedule of Modifications (April 2023)

No.	RECOMMENDED MODIFICATION	REASON
58	Update Part 2 text to include references to Figure 40 (Streetscape of Lorimer Road) and Figure 41 (Streetscape within the 'Rural' zone).	Currently no references are included.
59	Insert a new Appendices in Part 2 - ' <b>Appendix B: Principles for Preservation of Banjup (as adopted by Council 13 October 2022)</b> '	In recognition of Council's decision to adopt the Banjup Preservation Principles, to be used as a starting point to identify the valued character of the area.
60	Insert a new Appendices in Part 2 - ' <b>Appendix C: Activity Centre Comparison Table</b> '	Inclusion of this data supports the proposal for Cockburn Central Activity Centre to be elevated to a Strategic Centre.
<b>MAPPING UPDATES</b>		
61	Replace the dark red outline around the land south of the South Beach Recreation Reserve to orange to more accurately reflect it as a structure plan area suitable for future rationalisation.	Mapping correction.
62	Mark the alignment of Russell Road through the Latitude 32 area as a 'Proposed integrator arterial road'.	To reflect the importance of this route and match what has been done for the other arterial roads through the Hope Valley Wattleup Act area.
63	Mark the continuation of Rowley Road through to Rockingham Road as a 'Proposed primary distributor road'	Despite being located outside the City's Local Government boundary, this route serves a critical function to future planning and development that warrants recognition in the City's Local Planning Strategy.
64	Modify the Plan to relocate the west-east 'Proposed integrator arterial road' shown through the Hope Valley-Wattleup Redevelopment Area to Wattleup Road.	In recognition of the significant constraint the City's waste recovery centre poses to future construction on the current alignment, and to match the latest detailed planning being progressed by the State Government for the area.
65	Reword Appendix B of Part 1 (Activity Centres) map legend to state: <i>'Existing commercial zoned area currently developed with residential land uses'</i> .	To clarify the mapping of activity centres to make it clear where there is residential development on commercial/centre zoned land (rather than reflecting any proposal to expand centres).



## CHANGES TO THE DRAFT LOCAL PLANNING STRATEGY AS REQUIRED BY WESTERN AUSTRALIAN PLANNING COMMISSION

The Draft Local Planning Strategy was adopted by Council at the 9 October 2020 meeting and was subsequently forwarded to the Western Australian Planning Commission (WAPC) for their certification to advertise.

The table below provides an overview of the key changes required by the Department of Planning, Lands and Heritage (DPLH) and the WAPC:

TOPIC	Key changes from the draft Local Planning Strategy adopted by Council 9/10/20
<b>Urban Growth and Housing</b>	Including 'Planning Directions' that the current extent of residential densities, and the rural and rural living zone is to be maintained, to reflect more clearly what was already proposed in the Council adopted draft.
<b>Planning Areas</b> Use of 'Planning Areas' rather than 'Future Scenario Planning', and addition of new 'Planning Areas'  <b>Addition of following 'Planning Areas':</b> <ul style="list-style-type: none"> <li>• Jandakot Industrial Investigation Area</li> <li>• Glen Iris Golf Course site</li> <li>• Rural Living Precinct</li> <li>• Wattleup Industrial Investigation Area</li> <li>• Cockburn Central Activity Centre</li> <li>• Phoenix/Spearwood District Centre</li> <li>• Cockburn Coast District Centre</li> </ul>	The Local Planning Strategy Guidelines (adopted after the City's LPS was drafted) now use 'Planning Areas' for areas where there may be further investigations or detailed planning required, or where specific issues and opportunities are to be identified.  DPLH requested these areas to be shown as 'Planning Areas'. The 'Planning Directions' and Actions for these areas are generally consistent with what was already outlined in the draft Local Planning Strategy adopted by Council, and where relevant, adopted Structure Plans for the areas.  Glen Iris has been added as a 'Planning Area', but only to include the 'Planning Direction' - <i>Land use and development that achieves an appropriate interface with existing residential land uses.</i>
<b>'Planning Area' Roe 8/9 Reserve</b>	The issues and actions have been simplified.
<b>Perth and Peel @3.5million Industrial Investigation Areas</b> Inclusion of references to the Industrial Investigation Areas (Wattleup and Lake Coogee/Munster) and planning directions that are more high-level.	As part of the preparation of the draft LPS the City had undertaken some investigation into the appropriateness of these areas for industrial land use and ascertained that these uses were not appropriate for those areas. DPLH have advise that further investigation is required in this regard, and this has been noted.
<b>Lake Coogee Urban Deferred Area</b>	Wording changes to state the following 'Planning Direction': <i>Investigate whether residential development is appropriate for the area, subject to appropriate consideration of odour impacts from the Woodman Point Wastewater Treatment Plant.</i>  However, this wording change will not alter the City's proposed approach to this matter.
<b>Cockburn Central Activity Centre</b>	Remove references to a Metropolitan Region Scheme (MRS) amendment for Cockburn Central Activity Centre (to Central City Area), and alternatively state:

	<i>Investigate the opportunity for Cockburn Central Activity Centre to be reclassified to a strategic metropolitan centre based on the function of the centre.</i>
<b>Future zonings under the new local planning scheme</b>	Removal of some of the detail around future zonings to ensure the strategy remains at a high level. These zoning matters will be considered as part of the preparation of a new scheme.
<b>Removing some advocacy items</b>	While some proposed actions may have overarching benefits to planning outcomes, many are not intrinsically linked to planning processes, instrument or mechanism (ie. may relate to education or advocacy).
<b>Perth and Peel @3.5 million</b>	A number of changes have been required to ensure that the draft LPS aligns with Perth and Peel @3.5 million.
<b>Minor Changes</b>	
<ul style="list-style-type: none"> <li>• Formatting changes to align with the new Local Planning Strategy Guidelines.</li> <li>• Renaming of 'Strategies' to 'Planning Directions'.</li> <li>• Rewording of some Planning Directions and Actions to be more 'high-level' (without changing the intent itself).</li> <li>• Further detail/analysis of infill targets, and reference to Perth and Peel dwelling and infill targets.</li> <li>• Neighbourhood Plans renamed to 'Local Area Plans'.</li> <li>• Terminology changes to be consistent with State Planning Framework.</li> <li>• Consolidating some actions (ie. where there may be a number of detailed actions relating to a key overarching action) to ensure the Strategy is concise.</li> <li>• Draft Strategy Map updated, in consideration of cartographic convention, and to show Activity Centres and proposed 'Planning Areas' and reflect what is shown in Perth and <a href="#">Peel@3.5</a> million.</li> </ul>	

**Attachment 3      Draft Local Planning Strategy (as Advertised)**

Circulated under Separate Cover

**Attachment 4      Schedule of Submissions**

Circulated under Separate Cover

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Jandakot Residents and Ratepayers Association  
(Inc)  
(representing in excess of 600 members)

Submission 109/152 and 110/226  
Glen Iris Golf Course, Jandakot

24 July 2022



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## 1. Introduction

The Jandakot Residents and Ratepayers Association Inc (JRRA) would take this opportunity in its submission to remind the CoC of the statement made by Mr Daniel Arndt, then Acting Chief Executive Officer, in his letter date 2 September 2020 to Hon. Matthew Swinbourn MLC in which he acknowledged, on behalf of the City, its awareness of both the Community's concerns about the future of the land in question and how integral the Glen Iris Golf Course was to the neighbourhood. He did so by stating:

*"Many of the local residents have expressed their opposition to a redevelopment of the subject land, which would result in residential zoned land where there is currently open space and significant vegetation attributed to the former golf course. The City understands the community's concern regarding the future of this site, and how integral the golf course was to the existing neighbourhood" (underlining added)*

In view of the City's acknowledgment "how integral the golf course was to the existing neighbourhood" the JRRA and its members, as well as the wider community, request that CoC REJECT the developer's application in full and instead work with the community to find a win-win outcome.

Given the JRRA represent the interests of the actual Owners (Ratepayers and Residents) directly impacted by the proposal it is understood that the City is required, on basic legal principles, to give significant weight to the views of the owners of the affected residential lots and should not make any decision to adversely affect their amenity and interests without carefully considering their views in accordance with the principles of natural justice and other legal requirements.

In representing those Owners the JRRA trust that the City will adhere to these requisite principles in its considerations and thanks the City Officers for doing so.

## 2. Background and History

The Glen Iris Golf Course (Golf Course) is situated at the centre of the Glen Iris Golf Course Estate (Estate) in the suburb of Jandakot. The Estate consists of 773 homes including a large number of residents who purchased homes in the Estate to enjoy a unique golf course setting and lifestyle.

The Golf Course was first established in 1965 as 'The Lakes Golf Course and Country Club', a picturesque country club and resort style golf club offering a range of facilities for golfers and surrounding residents.

In the mid-1990s, the golf course and the surrounding land was developed into an integrated residential golf course estate called the 'Lakes Golf Course Estate' and later renamed to the 'Glen Iris Golf Course Estate'.

Today, the Golf Course consists of an public 18-hole international championship golf course, driving range, club house with restaurant and bar, professional golfing shop and practice putting green, and this amenity remains an integral feature of the Estate.





The original developer of the Estate owned the Golf Course continually from 1965 until its sale (by the heirs of the original developer) to the Proponent in 2020.

## 2.1. Change of Ownership

In 2017, the then long-time owners of the Golf Course met with the City of Cockburn (City) to discuss the potential sale of the land and whether the City would be interested in negotiating its purchase.

The City sought details from the owners for the purpose of conducting due diligence enquiries, but according to the City the owner was not forthcoming with this information and there were no further negotiations. There did not appear to have been any follow up by the City.

The owners subsequently proceeded to privately negotiate the sale of the land, however, not as what was then a viable golf course, but via a company called Acumen Development Solutions Pty Ltd (ACN: 160669116) (Acumen), a specialised development consultancy firm with headquarters in South Perth, offering the golf course land to developers only as SU1 zoned land with potential for rezoning and redevelopment into residential lots. Acumen is the Project Manager for Eastcourt Property Group, the eventual owners from the sale.

In March 2020, homeowners and residents of the Estate received notification of the imminent sale of the Golf Course.

On 6 April 2020, the Golf Course was sold to Eastcourt Property Group Pty Ltd (ACN: 115010823) (Eastcourt), a property development and investment company with headquarters in Applecross. Immediately after Eastcourt purchased the Golf Course, it closed

JRRA's enquiries have revealed that the Golf Course was purchased for \$27.4 million, with \$18 million paid at the time of settlement and the remaining \$9.4 million to be paid if the land is successfully rezoned.

## 3. The Communities Response

Note to the City of Cockburn when reading this section

This section relates to the combined submission, including Appendix 1 - Community Engagement Report, Scheme Amendment Map, Structure Plan

CoC Action Required: REJECT the developer's proposals as they do not address the negative impacts of this infill development.

The community response and opposition to the Proposed Estate has been substantial and unequivocal, and public interest and opposition to the Proposal continues to rise. The City has acknowledged that there is a great deal of anxiety in the community about the future of the land. Such anxiety demonstrated by in excess of 10,000 signatures on three Petitions opposing the proposal. The City is aware of those Petitions and such opposition should be taken into consideration.



JRRA represents the vast majority of affected homeowners who have concerns that the rezoning of the Golf Course and development of the Proposed Estate presents a considerable risk to amenity, lifestyle, land value, and importantly, the environment and fauna – matters that need to be taken very seriously, especially after the scathing “State of the Environment” report recently published in July ‘22 - a shocking report showing how Australia’s land and wildlife are being destroyed, in part due to climate change, and habitat loss.

Potential impacts to amenity, lifestyle, land value include:

1. Disrupting the balanced provision of, and equitable access to nature, sport and recreation opportunities that exist as a result of the inclusion of the Golf Course as a central feature of the Estate;
2. Reduction of land value by the removal of views of the golf course and the attraction of a unique lifestyle that follows from living in an exclusive integrated golf course estate;
3. Increased traffic pressures including up to 12,200 daily traffic movements on an already busy road infrastructure (principally Berrigan Drive) expected from the development of between 550 and 600 new houses as well as commercial amenities;
4. Increased pollution caused by non-exhaust emissions such as particulate matter which is now universally recognised as posing a significant negative effect on human health, particularly in urban areas;
5. Increased noise pollution as a result of increased traffic, and the elimination of vegetation that helps buffer existing noise emanating from surrounding traffic, overhead flight paths, freight trains and the public transport infrastructure being developed as part of Metronet;
6. Complete recission of the original concept that was marketed and sold as ‘an exciting upmarket residential estate established around an attractive 18-hole golf course’;
7. Disruption of the local community identity that has developed over time as a result of the Golf Course and its clubhouse being an integral feature of the Estate and a central hub for community gatherings; and
8. Generally diminishing the quality of life, vitality, community engagement and interaction, and sense of place in the Estate.

Potential environmental impacts include:

1. Irreversible loss and fragmentation of fauna habitat as a result of the clearing of flora and vegetation, much of which is home to an abundance of species of fauna that has evolved in the Estate over a number of decades;
2. Significant (negative) Interference with the federally protected habitat of the *Calyptorhynchus Latirostris* (Carnaby’s white-tailed black Cockatoo), which is recognised as endangered under the Environment Protection and Biodiversity Conservation Act 1999 (Cth) (EPBC Act);
3. Significant (negative) Interference with the federally protected habitat of the *Calyptorhynchus banksii naso* (Forest Red-tailed black Cockatoo), which is recognised as vulnerable under the EPBC Act;
4. Contamination of soils and groundwater from fuel leaks and waste products associated with the demolition of existing structures, extensive earthworks, and

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subsequent development of residential and commercial structures which would negatively impact the Jandakot Groundwater Mound;

5. Negative impacts on the southern portion of the Estate which is shown as 'wetlands' on the relevant Landgate Property Interest Report;
6. Possible disruption of high-risk acid sulphate soil areas known to cause serious environmental and health impacts;
7. Significant (negative) impact on climate change resulting from the currently proposed removal of 750 of the 1,258 mature trees; and
8. Significant (negative) impact on climate change given that via an Environmental, Science and Technology Report in October 2018, it is estimated that the GolfCourse produces enough oxygen to sustain 135,000 people annually.

The potential (negative) impacts of the Proposed Estate cannot be understated and are of a real and significant concern of JRRRA and the local community.

Consequently, on 10 September 2021, JRRRA submitted to the Hon. Dr Brad Pettitt MLC, Member for the South Metropolitan Region, a petition containing 4,553 signatures of landowners, residents and visitors of the Estate and Golf Course.

On 13 October 2021, this petition was tabled in the Legislative Council.

**This was the third Petition – all three totaling in excess of 10,000 signatures.**

#### 4. Integrated Golf Course and Housing

Note to the City of Cockburn when reading this section

This section relates to the combined submission, including the Draft Structure Plan

CoC Action Required: REJECT the developer's proposals as they do not address or acknowledge the Integrated Golf Course and Housing status

Addition to the risks to amenity, lifestyle, land value and the environment, there are both moral and legal issues that arise from the Proposed Estate.

In 1992, the then owners of the Golf Course and the adjacent land (now together comprising the Estate) made a submission to the City and the Department of Planning and Urban Development seeking the rezoning of the land to facilitate the development of what was described as an 'integrated residential golf course estate'.

As part of its submission, the owners stated that their objective was to create a desirable residential environment that capitalises on the natural abilities and characteristics of the site with an integrated 18-hole golf course as its centrepiece.

In conjunction with the owners, the City developed and released a concept plan that included a design and layout that was said to:

1. Maximise integration of the Golf Course throughout the Estate;
2. Maximise the number of homesites with direct aspect of the Golf Course;
3. Provide several homesites with secondary views of the Golf Course; and



4. Benefit the landowners and residents by providing the feeling of being within a golf course estate.

Research was referenced in the concept plan that indicated a strong demand for residents to live within a golf course estate.

The concept plan concluded with the following statement: 'As clearly demonstrated within the concept plan, the Lakes Golf Course Estate will be developed as an exciting upmarket residential estate established around an attractive 18-hole golf course'.

The concept plan was approved by the City and work commenced to develop what is now the Estate in its current configuration. The original developer held ownership of the land for a number of decades through to the date of the sale of the golf course land in April 2020.

Restrictive covenants were imposed on the land by the original developer in order to ensure the maintenance of a standard of housing and character consistent with that of an exclusive integrated golf course estate. Those Restrictive Covenants were registered with Landgate and many of those non-expiring Restrictive Covenants still exist today on a large portion of the lots within the Estate - over 240 lots. The residential land in the Estate, particularly that which directly abuts the Golf Course, has been valued and marketed with the Golf Course as an important contributing feature of the land. Many homeowners in the Estate purchased land solely on this basis.

Marketing material has described the Estate in the following ways:

1. 'The fabulous Glen Iris Golf Course at your doorstep'.
2. '...a truly prestigious development planned for a fortunate few and destined to become one of Perth's most sought-after addresses'.
3. 'Situated in a secluded parkland setting overlooking the picturesque lakes and nestled between the lush fairways of a first-class golf course'.
4. 'The Country Club will offer a host of luxury facilities for the whole family to enjoy, golf tennis, pool restaurants and children's recreations areas, all just a short stroll from your home'.
5. 'Set amongst the lush greens and fairways of the world-class Glen Iris golf course, the Estate boasts magnificent homesites, picturesque lakes, fountains and serene wooded parklands'.
6. 'Glen Iris ensures total security, a lifestyle second to none and the added benefit of being an excellent investment for the future'.
7. 'In addition to the many benefits Glen Iris already offers its residents, the planned Country Club will be centrally located within the estate'.
8. 'Complimenting the world class golf course, the club has been designed to incorporate a wide range of sporting and recreational facilities... all within a short stroll from their front door'.
9. 'Glen Iris, no other private Estate offers so much, so close to home'.
10. Land adjacent to or overlooking the fairways and greens of the Golf Course carried a higher purchase price. Homeowners were made aware that the higher price accounted for the specific lifestyle marketed to them.



- 11. The removal of the Golf Course from the Estate will have significant impacts on the value and character of the land and create a living experience entirely inconsistent with that represented to homeowners when they purchased the land.

The combined measures taken by the original developer maximised the level of profit it derived from the land, including but not limited to obtaining higher (premium) prices for the home lots sold to buyers given the character of the estate - namely its integration with the golf course.

### 5. SU1 zoning

Note to the City of Cockburn when reading this section

This section relates to the combined submission, Key Figure – Formal Structure Plan Key Figure – Indicative Subdivision Concept. Structure Plan (3.3.4, 3.3.5, 4.5, 5.1, 5.2.2, 5.6.1) Appendix 11 (p2)

CoC Action Required: REJECT the developer’s proposals as they do not recognise THAT Eastcourt and the integrated residential homeowners both fall under the SU1 “umbrella” and both “own” the SU1 definition.

The JRRA along with our members are very concerned that the issues around Zoning are not being researched in detail and are not being taken into serious consideration at all levels of City of Cockburn which can only lead to the conclusion that such dismissive efforts demonstrate a predetermined outcome. As such we provide below our questions and the responses from City Officers to verify this conclusion:

Email dated 17 April 2020
From Rachel Pleasant (Manager Strategic Planning – City of Cockburn)
“The golf course area is currently zoned ‘Special Use No. 1’ which permits a golf course estate, private recreation, hotel, convention centre and associated uses. There is also a small area, approximately 2ha, upon which the driving range is located, which is zoned ‘Residential R40’. The ‘Special Use No. 1’ zoning was adopted to facilitate the current golf course estate in the mid-1990s and at the time included a structure plan, which guided the subdivision, land use and development of the area.”

Email dated 17 July 2020 * See note below
From Carol Catherwood (Head of Planning – City of Cockburn)
“The Glen Iris plan attached was part of the scheme amendment (#56) that introduced the Special Use to the City’s previous town planning scheme (TPS2).”



Letter to Hon M Swinbourne - 2 September 2020 (then Acting CEO – City of Cockburn) and OCM Minutes dated 10 September 2020 (Page 822 of 848)
Daniel Arndt: "Attachment One – previous rezonings under District Zoning Scheme No. 2 (DZS2) – included concept plan"
"Amendment #56 – Rezoning portion of Lot 2, 3,4 and 5 Jandakot Road from Commercial – Restricted Use – Private Recreation and Hotel to Residential R15/12.5 Restricted Use Private Recreation and Hotel and Commercial - OCM Meeting July 1990 (gazette date not clear)
Amendment 56 was supported by the above Concept Plan which was adopted in 1990 as a Structure Plan to guide development"

Questions:

1. How can three very senior officers at CoC state that SU1 was introduced under Amendment #56 (DZS#2) and the Chief of Built and Natural Environment (D. Arndt) state in OCM Minutes 10/9/2020 'approved at OCM July 1990' but 'gazette date not clear', then subsequently when a copy is requested to help decipher illegible writing, later state Amendment #56 was never approved or adopted? 2. What was Mr. Arndt looking at to make the statement 'gazette date not clear'?
3. From where did Ms' Pleasant and Catherwood get their information to make these statements when (apparently) these documents cannot be found/do not exist and Amendment #56 was never adopted?

This myriad of events can only confirm not only that the CoC recommendation to the OCM was flawed in such a vital component but that misleading information was given to the Hon Minister by a Senior Executive of CoC, who in his role should require the actual facts to be clear and in full when advising MLC.

One could also suggest that this supports the belief of the existence of a predetermined outcome.

Email dated 15 October 2021 * See note below
From Claire Altieri (Strategic Planning Administration – City of Cockburn)
"Please find attached the requested amendments. Due to privacy, you may find some names have been redacted. Unfortunately, Amendment No.56 full documents were unable to be found, so I have included the Concept Map that formed part of the approval."

Questions:

1. How can there be a Concept Map that formed part of the approval, yet the Scheme Documents cannot be found, and now (per D. Arndt and D. Reynolds) were never approved or adopted?
2. From where did Ms Altieri get this information to make the statement 'that formed part of the approval'?



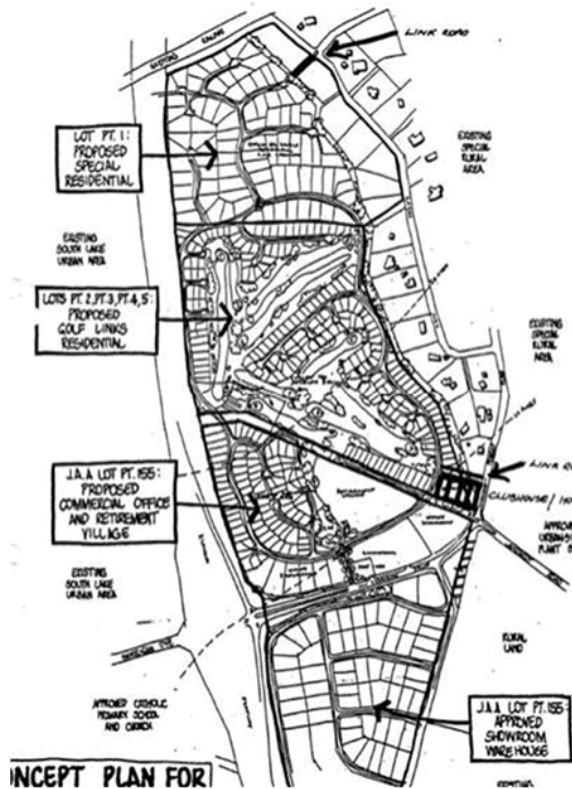
3. The City has stated that the original mistake lay with a junior staff member yet one would assume that senior officers are required to ensure that such information is verified as correct before it is stated and re-stated by three senior officers.

Note: \* Below is the Concept Plan that both Ms Catherwood and Ms Altieri provided with their respective emails: “that it was this plan that was part of the scheme amendment (#56) that introduced the Special Use to the City’s previous town planning scheme (TPS2)” and “the Concept Plan that formed part of the approval” yet there is nothing on this Concept Plan to identify that it relates to Amendment #56.

Additionally, Ms Catherwood has stated that it was it was under TPS#2 that Special Use was introduced, which contradicts Mr. Arndt’s statement that SU1 was introduced under TPS#3 - “the Special Use zoning of the land (and wording in Table 8) was adopted in 2002, long after the golf course and surrounding residential estate had been developed.”

Furthermore, that absolutely no paperwork relating to Amendment #56 can be located, from where did the junior staff member (who apparently made the original mistake) obtain his/her information in the first place?

Mr. Arndt was able to quote this Amendment in Ordinary Council Minutes (September 2020) and in his letter to Hon Swinbourne (September 2020) - even providing a description of the proposed rezoning for this Amendment, plus date of approval – OCM July 1990 - with “gazette date not clear” - and at the bottom of the page “Amendment 56 was supported by the above Concept Plan which was adopted in 1990 as a Structure Plan to guide development.” (The Concept Plan that was included in Mr. Arndt’s November 2020 Minutes is the same as that shown below.)



Yet again further actions that support the belief of the existence of a predetermined outcome.

Email dated 25 June 2022

To Daniel Arndt (Chief of Built and National Environment – City of Cockburn)

To summarise the above:

1. In 2020 Rachel Pleasant (Manager Strategic Planning) advised that it was Amendment #56 that introduced the SU1 zoning.
2. In 2020 Carol Catherwood (Head of Planning) also advised that it was Amendment #56 that introduced the SU1 zoning.
3. When asked for a copy of that specific Amendment several City Officers each confirmed that a copy could not be produced by the City.
4. In 2020 City Officers included Amendment #56 in the OCM Minutes and yet in a subsequent but direct, written reply on the issue to Hon Matthew Swinbourne CoC subsequently stated “gazette date not clear”.





5. When CoC were once again asked (post the letter to the Minister) to provide a copy of the document to see if a JRRR member could decipher 'gazette note clear', CoC stated that such a document was non-existent.
6. Ultimately, JRRR member was advised in writing by CoC that Amendment #56 was never approved or adopted.
7. By separate FOI investigation the Department of Planning, Lands and Heritage stated in their letter dated 19/10/20 that Amendment #56 could either not be located or was never received by them – however their letter further states “not all development requires WAPC approval, and approval for development may be delegated to the relevant local government.”

Questions:

1. Was development approval for Amendment #56 delegated to City of Cockburn and therefore senior officers were able to cite relevant information, yet when a copy of the document was requested, it is unable to be produced?
2. Even if an Amendment is not approved or adopted, one would assume that there would be more than just a Concept Plan in a file pocket?

Email dated 23 June 2022
From Daniel Arndt
“As described above, the Special Use zoning of the land (and wording in Table 8) was adopted in 2002, long after the golf course and surrounding residential estate had been developed.”

Questions:

1. This totally contradicts what Rachel Pleasant and Carol Catherwood independently stated – ie that SU1 was introduced in the mid 1990's/Amendment #56 - under DZS#2 as opposed to D. Arndt stating (in 2022) that SU1 was introduced in 2002 under TPS#3?
2. Why would it take approximately 7-10 years to introduce a new Special Use zone under TPS#3 - see definition\*?
3. Why would the Council introduce a new zoning “which have unique development requirements” for an already completed development?

TPS #3 - \*n) Special Use Zone

To provide for **uses which have unique development requirements that cannot be easily be accommodated by the objectives of any of the other zones included in the Scheme.**



Table 8

No.	Description of Land	Special Use	Conditions
SU 1	Berrigan Drive, Jandakot, the Glen Iris Golf Course Estate.	<u>Golf Course Estate</u> , Private Recreation, Hotel, Convention Centre and associated uses - <b>means land used and designed for a golf course, integrated with residential development</b> and associated commercial and community facilities.	Structure Plan adopted to guide Subdivision, land use and Development - Glen Iris Golf Course Estate.

Note: wording in n):

- “unique” (definition: being the only one of its kind, unlike anything else)
- “**have unique development requirements**”. This is stated in the present tense, not past tense, therefore strongly supports that SU1 was indeed introduced under DZS#2 via Amendment #56 (per R. Pleasant and C. Catherwood), which (per D. Arndt) also adopted a Structure Map, in the mid 1990’s, but now states that it was adopted “long after the golf course and surrounding residential estate had been developed”.
- “that cannot easily be accommodated by the objectives of any of the other zones included in the Scheme.” If the integrated residential component was not inextricably linked to and included in the SU1 definition, the City could have just zoned it “Restricted Use, Private Recreation” as it did under DZS#2 or “Private Clubs and Institutions - Golf Club” as it did under DZS#1. If only zoning a golf course, there would be no need to create a new unique SU1 zoning.

Note: Special Use 1 wording:

- “Golf Course Estate” (definition: used to describe a niche in the property market: residential real estate linked to a golf course)
- “integrated” (definition: with various parts or aspects linked or coordinated)

JRRA contends the above definitions are the reason that a new Special Use 1 zone was required as it did not fit the objectives of any other zone.

Email dated 8 July 2022
Reply from D. Arndt whether SU1 wording had a literal meaning:
“The City agrees that the words “means land used and designed for a golf course, integrated with residential development” have a literal meaning, however the extent to which the Special Use Zoning and Table 8 of Town Planning Scheme No.3 (TPS3) apply is clearly defined on the face of the Scheme map, and only applies to the residual golf course landholding. It is an anomaly in the scheme that occurred when the then ‘Residential’ and ‘Commercial’ zonings over the collective landholding were rationalised, and the Special Use zone first created, when TPS3 was first gazetted/adopted in February 2002*. The zoning transition that occurred at that time is best shown in the physical map extracts as depicted below:
*(JRRA note: Maps provided had incorrect dates. February 2002 is incorrect: it should be 20 December 2002)



Questions:

1. Is D. Arndt suggesting that an experienced cartographer would draw a Scheme Map which did not reflect the actual wording contained in SU1? It is inconceivable that this supposed anomaly has only just come to light 25+ years after the fact and to now suit Eastcourt's position that integrated landowners do not need to be considered in a rezoning application
2. Why have five different versions of the Scheme Map been publicly produced in different documents?

Instead of CoC making the assertion that the Scheme Map (which has been depicted in many different ways in various public documents) more accurately represents the definition of the SU1 zoning, why don't the City officers re-think their position, more appropriately apply the definition of SU1 literally (ie taking the words in their usual or most basic sense) and defend the position of the hundreds of integrated residential homeowners as, in essence, being "co-owners or tenants in common" (for want of a better analogy) of the SU1 definition, thereby giving those homeowners equal (and rightful) say whether the SU1 zoning can change without consultation to those affected homeowners.

JRRA believes that homeowners are being denied natural justice by being excluded when Eastcourt and the integrated residential homeowners both fall under the SU1 "umbrella" and both "own" the SU1 definition.

Whilst the CoC still maintains that it is only the golf course land that is included in the SU1 definition, JRRA is firmly of the view that it is both the golf course landowner AND the integrated residential homeowners who collectively form part of SU1 (taking the literal meaning of the wording in SU1) rather than (as CoC asserts) it is only a Scheme Map which is accurate.

JRRA is confident that experienced officers drafted the various zoning definitions in Town Planning Schemes and inserted the appropriate wording to accurately define the intended land use. It would be ludicrous to think that the Cartographer drew a Scheme Map first and then the drafting of the zoning occurred afterwards. Cartographers draw maps from words, not the other way round.

A cursory investigation into the zoning of other golf courses reveals that they are zoned "Private Clubs and Recreation". Why then is Glen Iris Golf Course land zoned SU1 (rather than Private Clubs and Recreation) if it does not include the integrated residential component? - perhaps because it had "unique development requirements that could not be easily be accommodated by the objectives of any of the other zones included in the Scheme." (per n) above). ("unique" definition: being the only one of its kind, unlike anything else). The Glen Iris Golf Course - on its own - is not unique: it is neither 'the only one of its kind, nor is it unlike anything else' - a golf course is a golf course. It was the creation of a Golf Course ESTATE - golf course with integrated residential component - that made it unique.



One of the functions of the JRRAs is to represent the landowners, residents and ratepayers who live in the integrated residential areas surrounding and abutting the Glen Iris Golf Course, Jandakot, in relation to the proposed Amendment No. 52.

The Association has over **six hundred** members all of which would be affected by proposed Amendment No 152.

JRRA continues making the following submissions in relation to the proposed Amendment No 152 and SU1 zoning:

The land the subject of the golf course is currently zoned Special Use 1 which specifies the following permissible land uses:

*“Golf Course Estate Private Recreation, Hotel, Convention Centre and associated uses – means land used and designed for a golf course integrated with residential development and associated commercial and community facilities.” (underlining emphasis added)*

In Eastcourt’s Scheme Amendment Application on page 13, it has emphasised (and therefore agrees) the requirement that the integrated residential component forms part of the SU1 definition, but then later asserts by way of a statement unsupported by any facts that “the golf course did not incorporate an integrated residential development component”.

The SU1 zoning does not specify that the integrated residential development must be on the subject land and the wording used, when viewed as it rightly should in light of the surrounding circumstances and the history of the Estate (consistent with various Scheme Amendments approved and adopted by the City - Amendment #56 (now in question) #64, #82, #119, and #168, in the mid to late 1990’s to approve the creation of The Lakes/Glen Iris Golf Course Estate) strongly support an interpretation that the golf course is to be and indeed subsequently has been integrated with residential development on the surrounding land, not just on the subject land.

The golf course was accordingly developed in a manner integrating it with the surrounding residential development and the development was clearly promoted and residential lots sold ( for premium pricing) as a residential development integrated with a golf course.

This was consistent with City planning documentation in relation to a previous proposed Amendment to Town Planning Scheme 2 designated Amendment #56 which contained mention of both the golf course land and various surrounding areas. The City’s planning documentation relevantly referred to a request to support an amendment to rezone the area from “Rural” to “Residential” for the following:

- 9-hole golf course (21 Ha.)
- 160 residential lots minimum lot size 700m2 (11Ha.)
- 16 commercial lots (4.3Ha.)
- Service Station Site



- Retention of the existing Club house.

The City's planning document then states:

<i>"The Nine Hole Golf Course Links would then be largely surrounded by Residential and Commercial Development.</i>
<i>The applicant notes the integration of uses has been successfully developed in other areas e.g. Meadow Springs and <u>provides a high degree of amenity for the local residents.</u>"</i>

(underlining emphasis added)

To repeat certain statements made above, in 2020 two very senior officers of the CoC in Strategic Planning (Rachel Pleasant and Carol Catherwood) stated to a member of JRRRA that it was Amendment #56 that introduced the Special Use 1 zoning but after that JRRRA member made further enquiries about Amendment #56, two other senior officers of CoC (Daniel Arndt, (then) acting CEO and now Chief of Built and Natural Environment, and David Reynolds, Co-ordinator Strategic Planning) have later alleged that Amendment #56 was never approved, nor adopted.

This is not credible and appears to only express personal views (not facts) given that public records (OCM October 2020) state that Amendment #56 was adopted (OCM July 1990 – gazette date not clear) and "Amendment #56 was supported by a concept plan which was adopted in 1990 as a Structure Plan to guide development."

What document was Mr. Arndt looking at to make these statements, later contending that the document does not exist?

It is also noted that the assertion that the Special Use 1 zoning of the land (and wording in Table 8) was only adopted in 2002 under TPS#3 (gazetted 20 December 2002), with the Chief of Built and Natural Environment stating "the Special Use zoning of the land (and wording in Table 8) was adopted in 2002, long after the golf course and surrounding residential estate had been developed." (underlining added)

Why would the description of an 18-hole golf course which had existed for decades need to be changed and labelled as "Special Use". What was "special" as the golf course land (as a golf course) had not change between the various Town Planning Schemes: it was because something unique was added to the golf course – ie an integrated residential development component.

It seems implausible that a Special Use 1 zoning (with unique development requirements) would be adopted between 7-10 years "after the fact", strongly suggesting that it is more credible that it was in fact Amendment #56 which introduced the Special Use 1 zoning, as originally stated by Rachel Pleasant and Carol Catherwood (notwithstanding the recent assertion by Daniel Arndt and David Reynolds that Amendment #56 was never adopted).

It is also relevant that in relation to Amendment #82 in relation to the District Planning Scheme No 2 "Scheme Report The Lakes Golf Course Residential Real Estate" stated on page 10, Clause 4.3.2 as follows:

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*“It is proposed that the residential estate be developed around an international standard, 18-hole public golf course. The design and layout of the golf course seeks to:*

- *Utilise some of the existing fairway of the existing Lakes Golf Course*
- *Optimise the landscape qualities of the site*
- *Minimise earthworks and retain the varied landform and vegetation*
- *Maximise integration of the golf course throughout the estate.”*

*(emphasis underlining added)*

At paragraph 4.3.3 on page 11 of that “Scheme Report The Lakes Golf Course Residential Real Estate” the following statements are made:

*“As clearly indicated in the Concept Plan the residential estate has been integrated with an 18-hole golf course. The design of the residential precincts seeks to maximise the number of residential home sites with direct aspect to the golf course and provide a number of homesites with secondary views of the golf course.*

*By locating the entry roads to the residential cells with direct views across the golf course, all residents with the residential villages will benefit from the proximity of the golf course and the ‘feeling’ of being within a golf course estate. Recent research indicates a strong demand for residents to live within a golf course estate.”*  
*(underlining emphasis added)*

**It is a matter of established fact that the golf course was integrated into a residential development.** Indeed, (at a minimum) 240 existing residences are located on old Lot 3 (approx 45ha) now Lot 7 (17.5ha) and which was within the original golf course (The Lakes).

**Restrictive Covenants:** Various devices, including restrictive covenants (which, as confirmed by Landgate, are still current and have not been extinguished on the abovementioned 240 residential lots) making express provision for the integration with the golf course, were utilised to implement the integration of the golf course with the residential development.

As mentioned above, the golf course provides a high degree of amenity for Glen Iris Golf Course Estate residents, something that will be lost, not only for the Estate residents, but also for the wider Cockburn community and beyond into other parts of this State.

The residential development was marketed and sold as a ‘Country Club Estate’ involving a Residential Estate integrated with the golf course. (See advertising brochures attached). Whilst the City has stated that they are not responsible for marketing and advertising, JRRR would assert that marketing was based on the wording contained in the various Amendments approved and adopted by Council. Furthermore, it is a known fact that advertising/marketing is still used today to attract a prospective purchaser to property.

**Golf Course Viability:** The golf course (initially known as The Lakes Golf Course and now Glen Iris Golf Course) has operated since 1965 and JRRR rejects the unsupported assertion by the



Proponent that there is any financial or other impediment to the continued operation of the golf course.

In fact, the previous golf course owners offered to sell the golf course to the City of Cockburn in 2017 but as the previous owners did not subsequently produce any historical financial records, the Council did not follow up the offer. A pro-active Council would have followed up and furthered negotiations.

To demonstrate the CoC's interest in golf courses, ratepayer funds were subsequently paid out to review the costs of building a new golf course at Coogee. Rather than pursuing an already built and functioning international standard 18 hole golf course that the CoC knew was being considered for sale it embarked on using ratepayers' funds to look at building a 9-hole (only Par 3) golf course in Coogee scheduled for 2028/29 with the CoC noting especially that there is no other public golf course (public or private) for its 120k of residents/ ratepayers within the City of Cockburn.

Interestingly the 18 hole international standard golf course at Glen Iris sold in 2020 for (an equivalent) \$18m (as set out above) whilst the independent study for the 9 hole (Par 3) golf course at Coogee came in at an estimated build cost of \$28m. The CoC is still reviewing the Coogee Golf Course proposal.

In Mr. Arndt's letter to the Hon Matthew Swinbourn dated 2 September 2020, Mr. Arndt (then acting CoC CEO) stated:

*"An application to rezone and redevelop the subject land would need to comprehensively address issues such as the impact on neighbourhood character, amongst other things. They would also be expected to provide extensive justification including but not limited to demonstrating why a golf course is no longer viable".*  
(underlining emphasis added)

**This extensive justification has been totally disregarded by the Proponent and thus, despite being well aware of the need to do so, has not been adequately addressed by Eastcourt in its SAA.**

In Mr. George Hajigabriel's (ROWE group engaged on behalf of the Proponent) email to Daniel Arndt dated 16 July 2021 (below) Mr. Arndt is in fact assisting Mr Hajigabriel to say that the golf course was not viable (and then Mr. Hajigabriel attaches a draft for Mr. Arndt to review).

*"Hi Daniel,*

*Thank you for the recent meeting with Nick, Jarrod and I in relation to Glen Iris. During the meeting you indicated that we would receive a list of any queries and additional information requested with respect to the Scheme Amendment and Structure Plan documents. Whilst we will hold off on making any substantive alterations to the documents until we have reviewed the list, you did indicate that the Scheme Amendment Document required more direct commentary in relation to the existing development potential of the land and the failing viability of the former golf course use.*

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*The attached text has been prepared for inclusion in the document to address your request. The attached is a draft and it would be greatly appreciated if you could review it and provide any comments or guidance with respect to the content.*

*Your assistance in this regard is greatly appreciated.*

*Regards,*

*George"*

**This clearly indicates direct/inappropriate/biased "coaching" of the Proponent by a Senior Executive of the CoC and indeed contradicts Mr. Arndt's direct statement to Minister Swinbourn in that, unequivocally "They would also be expected to provide extensive justification including but not limited to demonstrating why a golf course is no longer viable".**

**Questions:**

1. Mr. Arndt states in his letter to the Hon Minister that "they" - Eastcourt – would be expected to provide extensive justification.... yet Mr. Hajigabriel is seeking help from Mr. Arndt in the form of words to submit that cover the point rather than Eastcourt actually providing this justification. He and Mr. Arndt collaboratively worked together to draft wording resulting in Mr Arndt not upholding the standard that he himself had declared to the Hon Minister would be required.
2. Is this a gross derelict of duty by Mr. Arndt - to say one thing to the Minister and then deliberately offer help to support the developer's position - "failing viability"?
3. Does this suggest bias towards the developer?
4. Does this also add to the growing list of CoC responses that further the belief of the existence of a predetermined outcome?

In the unique circumstances stated above, any rezoning of the golf course land is inextricably intertwined with the zoning and uses of the surrounding 'integrated' residential land when the Town Planning Scheme is read as a whole.

In a very real and direct sense, a change to the zoning of the golf course land effects a substantive change in zoning and usage of the residential land. That land can no longer comprise residential land integrated as part of a 'Golf Course Estate' if the golf course or golf course estate ceases to be a permissible use.

This change would not only have a negative effect on the amenity of the area but would also directly adversely impact on the value of the integrated residential properties in the golf course Estate of which 240 are directly located on what was The Lakes golf course land - old Lot 3 (approximately 45ha) – now Lot 7 (17.5ha).

Two material and important consequences flow from this, namely:

The City is required, on basic legal principles, to give significant weight to the views of the owners of the affected residential lots and should not make any decision to adversely affect





their amenity and interests without carefully considering their views in accordance with the principles of natural justice and other legal requirements; and

The residential land surrounding (and integrated with) the golf course will be 'injuriously affected' for the purposes of Part 11 Division 2 of the Planning and Development Act by the proposed rezoning in that the use of that residential land as part of an 'integrated' golf course estate will, in a substantive sense, become a 'non-conforming use' of that land as a direct and inevitable result of the golf course itself ceasing to be a permissible use. This would expose the City to significant liability under a class action from the residential landowners for compensation by virtue of their land being injuriously affected by the change in zoning. It would be imprudent for the City to expose ratepayers' funds to such a substantial liability.

\*\*\*\*\*

The City officer's report to Elected Members should not only consider planning matters, but equal, if not more weight - in light of the recent "State of the Environment Report - - should be placed on the true negative impact to the environment, climate change, fauna and flora, physical health issues, mental health and wellbeing issues, the quality of life, etc to the affected community. (See Appendix B "State of the Environment Report" (comments on a flawed process).



## 6. Traffic

Note to the City of Cockburn when reading this section

This section relates to the combined submission, including Structure Plan (4.6.6, 5.2.1, 5.7.2) Appendix 9 (7.4), Appendix 5 (3.2) , Appendix 9 (3.4, 4.2, 5.1, 7.4, 8) Appendix 9 (6.4; 7.2; 7.8.2, 8)

CoC Action Required: REJECT the developer's proposals as they do not address the negatives impacts of this development.

Glen Iris Golf Course Estate North has essentially one road in and one road out, a singular road circumnavigating the estate with travel in both directions. Small feed roads are fed into this loop road with minimal homes on either side.

One road in at Dean Road is an intersection of traffic lights at Berrigan Drive and Jandakot Road. This intersection was upgraded to a highway like standard to take traffic from the various new housing estates and Jandakot Airport to the Freeway and visa versa. There is a small shopping precinct on the corner adequate for the communities needs and expectations.

The same road continues on a loop around the estate and comes out at Berrigan Drive at the Intersection of Hartwell Parade/Turnbury Green Road.

The intersection at Hartwell/Berrigan is serviced by a feeder road left from Berrigan Drive travelling east and from a right lane feeder crossing Berrigan Drive traffic going west.

Glen Iris Golf Course Estate South has one internal road, Lakes Way which is only fed from Berrigan Drive. It is a no through road at the insistence and lobbying of the residents due to huge traffic issues. Lakes Way was a feeder road from Prinsep Road through Imlah Court but was closed due to the excessive amount of traffic using Lakes Way as a feeder "ratrun" road to Berrigan Drive as it is too difficult to get out of and into Prinsep Road. The other boundary road is Prinsep Road which comes off Berrigan Drive and runs directly south to the Cockburn Train Station, ATCO Gas and various commercial buildings and transport companies within the industrial estate it services. It will in future service all those vehicles travelling to the new wave park as well.

Traffic from Prinsep Road who have difficulty turning right, take the position of travelling west (left) then driving across Berrigan into Hartwell, around the small roundabout and then turning back left into Berrigan taking them into their desired direction without sitting waiting for other traffic. Significant large and heavy vehicles run this "ratrun" as does Transperth busses to do the turnaround at the end of the 515 trip on Hartwell Parade. The roundabout vegetation is always runover by heavy vehicles and busses as is the pavement which is regularly repaired by Council due to the heavy vehicles damaging the curbing.

We believe at the insistence of the Coc officers, the Developer has indicated a new set of 4 way traffic lights be installed at a new intersection planned adjacent to the current bus stop location on Berrigan Drive when their plan to close Hartwell Parade takes affect. This intersection will connect to the development south.

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This new set of signals will take the amount to four (4) sets of traffic lights within 900 metres of each other. The signals will be required to be monitored in such a way that traffic will not build up enough to block the crossroads from the freeway offramp/onramp and not to block/backup over Berrigan/Jandakot Road intersection. To do such will back up traffic onto the Freeway in one direction and up Jandakot in the other. Traffic and heavy vehicles from the ATCO and industrial sites will back up in the new road through the development south.

This new road south will in fact invite a considerable amount of traffic including ATCO heavy vehicles and industrial vehicles to run through the new development instead of using Prinsep Road. The heavy vehicles and approximately 12,000 vehicles per day will be invited to run through the new estate and past the so called high value homes to the bottom of the estate and exit through two (2) small streets and back onto Prinsep Road.

The entry to the estate from Prinsep Road will be via a new small street lined by houses both sides adjacent to the ATCO Gas site which will allow all ATCO vehicles to cross directly into the estate, through the houses and exit at the new set of traffic lights on Berrigan.

This new cross street on Prinsep Road will be dangerous to the extreme with vehicles now clocked by speed camera at some 121kl/hr. Prinsep Road is a 60kl/hr road. Residents on this stretch of road have been begging Council to remedy the situation with some forms of speed attenuation devices however council officers have stated nothing will be done "until the new development takes place and we can do it all at one time". Council Officer have again made statements believing this development will not be stopped by way of their words and actions.

Indications are the residents in this new section of the estate will lobby hard and strongly to ensure heavy traffic does not run past their homes as they currently do on Prinsep Road. The estimate is some 70 houses will be affected by some 12,000 movements including heavy haulage. This amount of vehicles will be disruptive to home owners trying to access their properties, be a danger to pets and children, will be noisy, smelly and damage homes with the heavy haulage non stop traffic. Heavy haulage at night will bring sleepless nights as it does now to Prinsep Road residents. No traffic or speed minimalization aspects are noted on their concept and we believe this to be on purpose so as not to show the whole aspect of their flawed design.

In ending the traffic layout within the estate is poor and is only set out in a way to ensure the number of houses is maximized to the fullest extent.

## 7. Local Centre Concept Plan

Note to the City of Cockburn when reading this section

This section relates to the combined submission, including Structure Plan Local Centre Concept.

CoC Action Required: REJECT the developer's proposals as they do not address the negatives impacts of this development.



A local centre of this nature within this development is more expected by the local authority than the residents themselves. The concept plan says this Local Centre Concept is such a concept as to never get off the ground and will remain a vacant block, fenced, graffiti covered and unkept for a number of years without any action by the developer. The ground will remain as such as a “supermarket” of some 500sqm will become a below par supermarket of some sort selling items such as phone cards, greasy takeaway food, supermarket small goods at extremely high costs such as the supermarket at the Berrigan did some years ago and failed dismally. Even South Lake shopping centre with 2 Major supermarkets struggles with it’s smaller tenants and this type of local centre will be the same.

It will not support 5 – 10 shops. JRRRA investigation with its subject matter experts has shown It will support a service station.

The developers has used the words “potential” when describing the uses of the local center/community hub in its recent community newsletter, no.5 July 2022. This does not provide any assurance that any such facilities will be made available.

The developer in the end will reapply for further development approval sighting their investigation with various operators trying to make it work impossible and asking for a change to infill with more housing. Some residents have stated they are extremely happy to have this type of centre but history and the market shows it will not work and should not be include in any way in the concept plan.



### 8. Bush Fire Prone area

Note to the City of Cockburn when reading this section

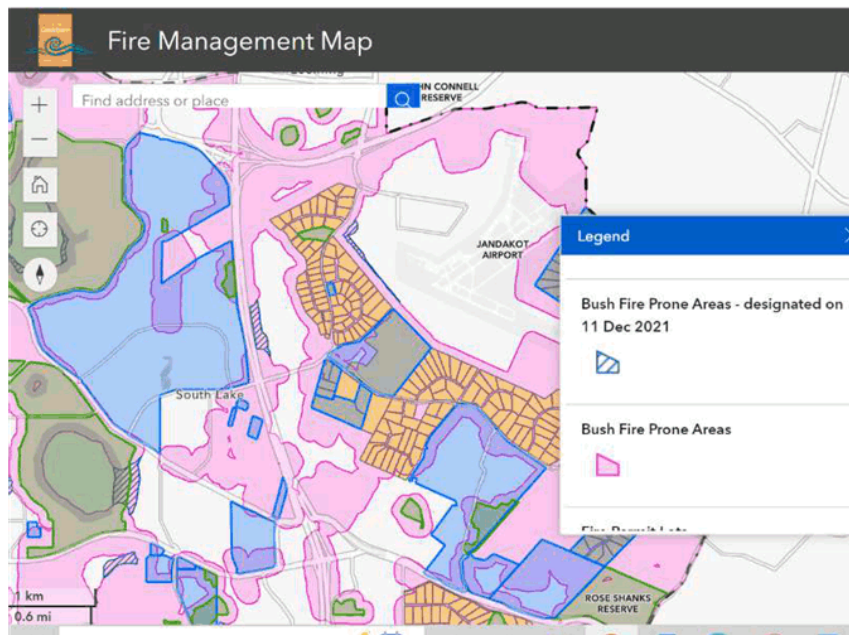
This section relates to the combined submission, including Appendix 5 Bushfire Management Plan, Appendix 5 (3.2)

CoC Action Required: REJECT the developer’s proposals as they do not include the much need 3<sup>rd</sup> entry/exit point from a safety perspective

If the Proposal is given the green light, and only two entry/ exit points exist, if there is a fire on the expanded Estate, how will fire trucks gain access on gridlocked roads?

Roads – emergency access Appendix 5 (3.2) City of Cockburn has noted concerns regarding potential access limitations north of Berrigan Drive, whereby all connections head back to a very short portion of Berrigan Drive.

The Bushfire Management Plan (Appendix 5) is of the opinion that the level of risk exposure is low. However, it is not acceptable to have an access limitation both in and out of the proposed estate, caused by massively increased population projected in our current golf course estate.



Keeping in mind the following factors re emergency vehicles possibly having delayed access in and out of the estate. Section 7.8.2 states “It is expected that with the additional trips assigned to Berrigan Drive, there will be some reduction in the performance of Berrigan Drive for through vehicles travelling to/ from Dean Road and the Kwinana Freeway”



The traffic analysis also does not consider the recently announced Wave Park that will be built on Prinsep Road and will have increased significant traffic implications for Berrigan Drive, Prinsep Road and the local area. Annual visitors to the surf park are expected to be 300,000. The traffic analysis does not consider the increased traffic from the expanded Jandakot Airport, nor traffic driving to the Jandakot Metronet station, currently being built for the Thornlie line extension. Additionally, the traffic implications do not consider the increased volume of traffic on Berrigan Drive to access Kwinana Freeway, in particular the potential build-up of traffic approaching the on-ramp going north. Will this increased traffic impede emergency vehicles getting into and out of Glen Iris during peak hours?

The City requested that a concept be prepared to explore whether sufficient space is available to accommodate an AFL (multi-use) size oval, clubroom, small playground area (can include provision of multi-use hard court, car parking, and two-bay cricket nets.) Facilitating the request from the City would require increasing the size of proposed Public Open Space area 17 [on Prinsep Road] (see Figure 9) by approximately 7,437m<sup>2</sup> and adding that area, together with the 1,843m<sup>2</sup> of Public Open Space area 17 and the 22,000m<sup>2</sup> of the existing Prinsep Park reserve, to create a new consolidated active recreation park of approximately 31,280m<sup>2</sup> in the elevated traffic risk area, also nearby the busy planned additional local shops. New additional traffic projected also from the new Industrial/ commercial estate on Jandakot Road.

Re the Prinsep Oval development an Alternate Subdivision Concept (Figure 14) has been prepared “to facilitate the ability to seek Community input” and progress for consideration by the WAPC “in the event that Council resolves to support the Prinsep Park option”. Appendix 9 – Transport Impact Assessment needs to include assessment of the additional traffic impacts that the alternate Prinsep Oval development would have on Berrigan Drive.

State Planning Policy 3.7 Planning in Bushfire Prone Areas (SPP 3.7) directs how land use should address bushfire risk management in Western Australia. It applies to all land which has been designated as bushfire prone by the Fire and Emergency Services (FES) Commissioner as highlighted on the Map of bushfire prone areas.

SPP 3.7 seeks to guide the implementation of effective risk-based land use planning and development to preserve life and reduce the impact of bushfire on property and infrastructure. It applies to all higher order strategic planning documents, strategic planning proposals, subdivision and development applications located in designated bushfire prone areas (unless exemptions apply).

Re the bushfire risk, a new signalised [i.e., traffic light] intersection is proposed along Berrigan Drive. Turnbury Park Drive access to/ from Berrigan Drive will be completely closed. A new 4-way signalised intersection will be slightly further to the east and provide connectivity between the northern and southern portions of the site, however timing of the proposed intersection is really not clear – Section 8 of Appendix 9 states the new signalised intersection on Berrigan Drive is in the “medium to longer term”, Appendix 9 Figure 7.1, which shows the total traffic generated onto the external road network by the proposed structure plan, is blurry and the numbers are difficult to read. How can a proper risk assessment be done re bushfire prone areas?

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It is imperative that in line with SPP 3.7 re 'preserve life', that Traffic Impact Assessments are done for both the Prinsep Rd Wave Park and also the Prinsep Oval development alternate subdivision concept. To not include these traffic figures authorities simply cannot undertake a proper risk assessment re elevated bushfire risk to current residents of the Glen Iris Golf Course Estate/ new residents from the proposed circa 600 houses.

City of Cockburn have already 'red flagged' a serious issue with the Eastcourt plan, i.e., that there is a potential risk re limited entry and exit routes on the northern side of Berrigan. This has been noted on the below excerpt.:

The vehicle movement network has been designed in consultation with the project traffic engineers to encourage all additional vehicle traffic generated by the proposed development to utilise the proposed internal subdivision roads, and to be directed primarily to Berrigan Drive. Through consultation with representatives from the City, it is understood there is a desire to create a road link between Dean Road and Lakes Way. Although there are two potential existing accessways that may assist in addressing this request, they are outside the Structure Plan area and may not be ideally located. Other than these accessways, the land between Dean Road and Lakes Way is held in private ownership making the provision of a new road problematic in this area.

If there was not a risk to residents, why would this have been even flagged by City of Cockburn re a road link between Dean Road and Lakes Way?

Obviously, there is an actual perceived risk to the residents of the Glen Iris Estate, as well as the proposed Glen Iris Estate (plonked in the middle of the Glen Iris Golf Course Estate.)

Notably there has already been a housefire on Glen Iris Drive, one fire closed Glen Iris Drive for six hours. What would happen if many houses were involved with a fire, say along the houses opposite the current Dean Road residents, or the current Prinsep Road residents. Could this estate become a serious death trap with an additional approximate 2k of people living in it?

House fire in the Glen Iris Golf Course Estate 2015 (when even the golf course caught fire!) and Glen Iris Drive was closed for five hours. In this incident, five crews attended (35 firefighters) and the large 'ladder' fire truck had difficulty navigating the Estate's narrow roads, especially the roundabouts, when the ladder kept getting caught in the verge trees





## 9. Aircraft Noise

Note to the City of Cockburn when reading this section

This section relates to the combined submission, including Structure Plan (3.4.6)

CoC Action Required: REJECT the developer's proposals as they **do not address the negatives impacts of this development.**

Based on the guidance within *State Planning Policy 5.3 (SPP 5.3) Land Use Planning in the Vicinity of Jandakot Airport*, the Structure Plan (Appendix 4, p17) considers the majority of the northern portion of the Glen Iris Estate to be "not adversely impacted by aircraft noise" as it is not within Jandakot Airport's Australian Noise Exposure Forecast (ANEF) contours. The southern portion of the site is within the 20-25 ANEF zone, and under SPP 5.3 residential uses are considered 'conditionally acceptable'.

SPP 5.3 notes that, according to *Australian Standard AS 2021 Acoustics - Aircraft Noise Intrusion - Building Siting and Construction (AS2021)*, "noise nuisance may still be experienced in areas below the 20 ANEF exposure level, particularly in the case of newly exposed communities". In addition, SPP 5.3 includes notes from Table 2.1 of AS2021, which advises that "the actual location of the 20 ANEF contour is difficult to define accurately, mainly because of variation in aircraft flight paths". With regards to the 20-25 ANEF zone, SPP 5.3 states that "Caution should be exercised when considering whether to facilitate the increase of sensitive land uses within this zone".

The Structure Plan (Volume 1, section 3.4.6) makes reference to SPP 5.3 which states that consideration should be given to the suitability of residential uses within the 20-25 ANEF noise exposure zone and that the "maximum dwelling density should generally be limited to R20" except where, amongst other criteria, "it can be demonstrated that the public benefits of higher density coding outweigh the negative impacts of exposing additional residents to aircraft noise."

In response, the Structure Plan states that in considering the SPP 5.3 policy measures, residential development will be limited to predominantly R20 and designating R30 density development only in strategic locations where a higher density coding is desirable. Notwithstanding JRRRA's opposition to the redevelopment of the Golf Course, JRRRA does not agree that the Structure Plan has sufficiently demonstrated a basis for any residential development above R20 density. In addition, this section of the Structure Plan only considers R20/R30 zoning and does not reference the R40 zoning which is shown in Key Figure – Formal Structure Plan and Key Figure – Indicative Subdivision Concept. JRRRA also notes that there are a number of sections of Structure Plan Volume 1 that only refer to R20/R30 zoning and omit mention of the proposed R40 zoning (e.g. sections 3.3.5, 3.4.6, 4.5, 5.1, 5.2.2.) - section 5.1 states that "Structure Plan comprises residential development with density codes ranging from R20 to R30" while section 4.5 states "The Structure Plan prescribes a base code of R20, with areas of R30 dispersed throughout in strategic locations". Given that R40 zoning is shown in Key Figure – Formal Structure Plan and Key Figure – Indicative Subdivision Concept, the omission of the proposed R40 zoning from relevant Structure Plan text is misleading.

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It is also noted that section 5.2.2 of SPP 5.3 states that a notice on title (pursuant to s70a of the Transfer of Land Act 1893), advising of the potential for noise nuisance, is to be required as a condition of any subdivision or planning approval within the frame area (defined by Roe Highway, Ranford Road, Warton Road, Armadale Road and the Kwinana Freeway). The Golf Course estate is fully within the Frame Area, and the SPP 5.3 requirement for a notice on title for all proposed development has not been appropriately acknowledged in the Structure Plan.

Section 3.4.6 of the Structure Plan Volume 1 further states that “It is also relevant to note the adjoining area, which also falls within the 20 to 25 ANEF contour zone, has been developed for residential purposes ranging in density from R20 to R40.” As noted in section 2.3 of SPP 5.3, “the ANEF contours may be reviewed every five years in association with reviews of the master plan for Jandakot Airport.” A new ANEF for Jandakot Airport has been prepared every five years in line with the Jandakot Airport Master Plan, and the Jandakot Airport ANEF contours have changed in every Master Plan iteration since the first Master Plan in 2005. The existing areas of the Glen Iris Golf Course Estate that are zoned R30 and R40 were not within the 20-25 ANEF zone (as included and applied in previous versions of SPP 5.3) and it is therefore misleading to imply that the existing ‘R20 to R40’ residential density occurred while it was within the 20-25 ANEF zone.

Section 3.4.6 of the Structure Plan Volume 1 states that the Plan has responded appropriately to the objectives and policy measures of SPP 5.3 by “Avoiding the designation of land for more sensitive land uses such as educational establishments, child-care premises and nursing homes”. This statement is erroneous and needs to be removed, as there is no justification of why education establishments, child-care premises or nursing homes would be considered ‘more sensitive’ than residential development. In fact, the AS2021 Building Site Acceptability (which is referenced and applied in the Structure Plan) considers residential, education, child-care and nursing homes to all be ‘conditionally acceptable’ in the 20-25 ANEF and ‘unacceptable’ in the greater than 25 ANEF zone.

## 10. Future Aircraft Noise

The National Airports Safeguarding Framework (NASF) is a national land use planning framework that was developed by the National Airports Safeguarding Advisory Group which comprised Commonwealth, State and Territory Government planning and transport officials, the Australian Government Department of Defence, the Civil Aviation Safety Authority, Airservices Australia and the Australian Local Government Association.

The NASF *Guideline A Measures for Managing Impacts of Aircraft Noise*<sup>1</sup> states that “AS2021 recognises that the 20 ANEF and 25 ANEF zones do not capture all high noise affected areas around an airport, and the ANEF contours are not necessarily an indicator of the full spread of noise impacts, particularly for residents newly exposed to aircraft noise.” The NASF Guideline A Attachment (Supplementary Aircraft Noise Metrics) recommends the ‘Number Above’ noise

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<sup>1</sup> <https://www.infrastructure.gov.au/infrastructure-transport-vehicles/aviation/aviation-safety/aviation-environmental-issues/national-airports-safeguarding-framework/national-airports-safeguarding-framework-principles-and-guidelines>



metric, which presents the projected number of aircraft noise events on an average day that are above a specific noise level.

The *Jandakot Airport Master Plan 2020*<sup>2</sup> includes N60 and N70 contours, as well as N60 Busy Day contours, to show the projected number of aircraft noise events for a day where Jandakot Airport will be operating at its peak daily movement level. These contours are provided below for reference.

N70 contours represent the number of aircraft noise events louder than 70 decibels (dBA). A 70 dBA outside noise will generally be experienced as a 60 dBA event inside a residence with the windows open and is the sound level that will disturb a normal conversation or other indoor activities such as watching television. The N60 contours are used to describe aircraft noise events at night. A 60 dBA event outside a residence will generally be experienced as 50 dBA indoors, which AS 2021 identifies as the level above which noise can be considered intrusive and is likely to be more disturbing during sleeping hours.

Based on the N60 and N70 contours in the *Jandakot Airport Master Plan 2020*, the Glen Iris Estate is forecast to experience 100 average daily events above 60 decibels, and 50-100 average daily events above 70 decibels. The N60 Busy Day contour shows that the Glen Iris Estate is forecast to experience 200 daily events above 60 decibels on a busy day, with some areas within the 500 daily event contours.

Section 4.2 of the *Jandakot Airport Master Plan 2020* states that the maximum theoretical operating capacity of Jandakot Airport used for the N60 and N70 noise modelling is 460,000 fixed wing and 66,000 helicopter movements. The Master Plan shows that in 2019/2020 there were 200,628 fixed wing and 25,636 helicopter movements, which means that in the future aircraft movements will be more than double what residents of the Glen Iris Golf Course Estate currently experience.

Glen Iris Golf Course Estate residents are significantly affected by aircraft noise from current operations, particularly repetitive pilot training circuits and the noisy helicopter operations. Residents have made numerous complaints to Jandakot Airport as well as the Airservices Noise Complaints and Information Service and the Commonwealth Aircraft Noise Ombudsman, and it is concerning that more residential development is being considered for an area that is already subject to invasive aircraft noise and is forecast to more than double in the future.

The World Health Organization's Burden of Disease from Environmental Noise<sup>3</sup> report states (page xv) that "The evidence from epidemiological studies on the association between exposure to road traffic and aircraft noise and hypertension and ischaemic heart disease has increased during recent years" and that "Both road traffic noise and aircraft noise increase the risk of high blood pressure."

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<sup>2</sup> <https://www.jandakotairport.com.au/corporate/master-plan.html>

<sup>3</sup> <https://www.who.int/publications/i/item/burden-of-disease-from-environmental-noise-quantification-of-healthy-life-years-lost-in-europe>



The report also states (page 55) that “Sleep disturbance is one of the most common complaints raised by noise-exposed populations, and it can have a major impact on health and quality of life. Studies have shown that noise affects sleep in terms of immediate effects (e.g. arousal responses, sleep stage changes, awakenings, body movements, total wake time, autonomic responses), after-effects (e.g. sleepiness, daytime performance, cognitive function deterioration) and long-term effects (e.g. self-reported chronic sleep disturbance)”.

It is well recognised that elderly people are more susceptible to sleep disorders and sleep disturbance, and yet the Eastcourt Structure Plan is promoting the proposed development for ‘ageing-in-place’ housing.

Based on the acknowledged limitations of the ANEF system, and the information presented in the *Jandakot Airport Master Plan 2020*, JRRRA is of the opinion that the Acoustic Assessment (Appendix 6) has not adequately considered the impact of aircraft noise for the proposed development and needs to include and detail the N60 and N70 forecasts as part of the assessment.





FIGURE 8-7 - N60 CONTOURS (BUSY DAY)



FIGURE 8-6 - N70 CONTOURS



## 10.1. Lighting Restrictions

Section 8.5 of the *Jandakot Airport Master Plan 2020* also details external lighting restrictions for the control of ground lights where they have the potential to cause confusion or distraction (from glare) to pilots in the air. Portions of the Glen Iris Estate proposed development area are within Restricted Light Zones and this needs to be addressed in the Structure Plan.

## 10.2. Solar

The sustainability commitments (section 5.3 of the Structure Plan) detail the focus on solar opportunities for the proposed development. Several studies<sup>4</sup> have shown that solar glare is a potential hazard for pilots, which could lead to accidents. However, the Structure Plan has not addressed potential risk of glare from solar panels.

Importantly there should be a study done to demonstrate the very real impact that the loss of 750+ mature trees will have in relation to air traffic noise (light aircraft/ helicopters) to current Glen Iris Golf Course Estate residents.

The psycho-acoustic effect of vegetation is estimated as the equivalent of a noise reduction of 10dB(A), and the effect is larger for higher sound exposure levels (Van Renterghem, T. Towards explaining the positive effect of vegetation on the perception of environmental noise. *Urban For. Urban Green.* (2018). doi:10.1016/j.ufug.2018.03.007)

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<sup>4</sup>

[https://www.faa.gov/data\\_research/research/med\\_humanfacs/oamtechreports/2010s/meda/201512.pdf](https://www.faa.gov/data_research/research/med_humanfacs/oamtechreports/2010s/meda/201512.pdf)



## 11. Environment

Note to the City of Cockburn when reading this section

This section relates to the combined submission, including Structure Plan (4.1.2), Appendix 4 (4.4.1, 4.3.2, 4.4.2, Appendix H (p19),

CoC Action Required: REJECT the developer's proposals as they do not address adequately the negative significant environmental impacts of this development.

On the 27 June 2022, Tanya Plibersek (Minister for the Environment) declares environment 'is back front and centre' in Australia.

Then on the 19 July 2022, Tanya Plibersek pledges new environment laws to end years of 'wilful neglect' by Coalition.

Of particular note is:

"Australia's failing environmental laws could be replaced next year, new minister Tanya Plibersek has said, while launching a scathing attack on the management of the country's most precious species and places"

"The State of the Environment report, released on Tuesday, assessed Australia's ecosystems, biodiversity and climate and their threats, as well as the effectiveness of environmental law".

Eastcourt Property Group have purchased the 54.9ha 18-hole championship Glen Iris Public Golf Course in Jandakot with its abundant wildlife, 1,258 mature trees and much needed anti-climate change open green space/ nature corridor to be replaced by concrete urban infill, increased pollution, more traffic, more stress attributing to mental health and wellbeing, plus reduced quality of life.

In addition, a proven sanctuary for many protected species and native fauna that has existed since 1965, will be lost.

As recognised by the City of Bassendean

[https://www.bassendean.wa.gov.au/documents/909/tree-retention-and-provision-\(lpp13\)](https://www.bassendean.wa.gov.au/documents/909/tree-retention-and-provision-(lpp13))

The Town recognises the increasing important of retaining trees due to their amenity, environmental and health benefits. Trees assist in mitigating the impacts of urban heat effect, reduce air pollution, improve groundwater quality, and provide important habitats for wildlife.

City of Cockburn: Mayor Logan Howlett (Perth Now 7 July 2022) stated: "While the number of ratepayers increases, the City needs to build and maintain more high-quality infrastructure and amenities for our residents and find additional resources to achieve this."

This is especially significant based upon the urbanisation on our doorstep:

- The continued development of the Calleya estate.

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- The development of the Kara estate.
- The development of the Lake Treeby estate.
- The continued expansion of the Verde industrial estate
- The continued expansion of the Jandakot Airport industrial estate.
- The development of the industrial area at Berrigan Drive/Jandakot Road.
- The development of residential units at Berrigan Drive/Lakelands school.
- Bushland turned over to a four-lane road (Jandakot Road) which ends at Warton Road – where a further residential development is underway.
- The quasi freeway of the Armadale Road and the acres of land that has been concreted over as it connects development after development all the way to the hills.
- The continued development of the land around Cockburn Central train station (including the development of the Thornlie line Metronet).
- The \$1Billion Dollar expansion of the Cockburn Gateway Shopping City precinct.

Urbanisation brings many benefits, but it must encompass balanced compensations which we now desperately need as highlighted by CoC Mayor Howlett.

The world, and the Cockburn Council, are aware and both state the perils linked to the onset of climate change which will be far more reaching for Cockburn with the planned infill of the 54.9ha. Glen Iris Golf Course.

With green open space, 1,258 mature trees, clean air, lack of pollution, low levels of traffic dust, no night light pollution, no heat build-up due to blocking prevailing winds, a certain quality of life currently exists. An integrated housing and golf course estate that independent parties state produces enough of oxygen for 135,000 people annually.

The planned Glen Iris infill of +/- 550-600 new houses will remove all of these qualities and produce more of the same which already exists in the City of Cockburn whilst decimating the protected golf course fauna and eco-system surviving since 1965 and displacing oxygen for 135,000 people annually.

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In Appendix A, the Report: “Sanctuary for the Protected Fauna (other wildlife) and Protected Flora... Quendas, Carnaby’s & Forest Red-tailed black cockatoos, Banksias” presents results of monitoring the wildlife on the golf course since June 2020. Surveys confirm the extent of the wildlife with over 70 sightings of Quenda, both individuals and families, plus flocks (over 100+) of the federally protected black cockatoos, plus 2ha of Banksia Woodland trees which provide food for the protected cockatoos.



Emerge Associates Environmental Fauna Report for Eastcourt undertook a Level 1 Desktop survey and fauna assessment which encompassed a “review of available data” which is limited and an inaccurate two “walk-throughs” to assess fauna and flora.

The major available data “Community Quenda Survey 2012” reports sighting ONE quenda and the report did not include the federally protected Carnaby or Forest Red-Tailed black cockatoos.

Two afternoon visits by the Emerge qualified experts only noted four quenda diggings because quenda are basically a nocturnal animal.

The protected black cockatoos which existed in documented flocks (100+) before Acumen Development Solutions (Project Manager) drained the lakes, were all discounted.

The Environmental Protection Authority’s final report did not even mention the word ‘quenda’ so what chance do the Priority 4 quenda have if they apparently do not exist? The EPA were advised that large numbers of quenda were inhabiting the golf course.

Independent available reports over the period (August 2020 to May 2022) include over 70 sightings for the quendas and record the demise of the protected black cockatoos due to human action.

Actions were undertaken by Acumen removing the lakes near the Carnaby roosting sites.

Emerge Associates have documented the need for water at Carnaby roosting sites plus they are aware of the duck breeding season, yet they sanctioned removal of water (August 2020) prior to the end of the duck breeding season (notwithstanding that the Project Manager was advised by WA Wildlife that the water bodies should remain until October) and many ducklings were found abandoned in residents’ gardens, by swimming pools, and with duck eggs even laid in flowerpots! (Glen Iris Drive)

Acumen stated that one of the reasons the five lakes had to be drained was to prevent people drowning, yet two “drown-proof” lakes remain, possibly due to them being artesian with associated draining problems. If valid reasons were available to Acumen for its actions in draining the lakes why was the actual draining and removal of linings undertaken in a quite clandestine manner very early on a weekend morning without notice to anyone (the CoC or local residents in particular)?

While ducks are not an endangered species, all native animals are legally protected.  
<[www.dpaw.wa.gov.au](http://www.dpaw.wa.gov.au)>

City of Cockburn correspondence (4 July 2022) also ignored the quenda: “I can see no information within the Fauna Assessment that indicate the ecologist from Emerge recorded seeing any quenda on the site visits, only diggings were recorded.”

A letter from the City of Cockburn dated 25 May 2022 advised: “On 20 April 2022, the EPA determined that the likely environmental impacts associated with the development are unlikely to have a significant impact on the environment and do not warrant formal assessment under Part IC Division 3 of the Environmental Protection Act 1986.”





**Environmental Criteria Ignored:** In its letter to Minister Swinbourn dated 2 September 2020 the Acting CEO of the City states:

“Any application to rezone the land would be required to address issues including environmental assessment. Should any endangered species or quality ecological communities be identified at the subject site; this would form a considerable amount of the assessment of a redevelopment proposal.”

Again, despite the EPA making the determination “*that the likely impacts associated with the development are unlikely to have a significant impact on the environment and do not warrant formal assessment....*” a well credentialled JRRA member with a B.Sc in Zoology, Geology and Botany, wrote to the EPA in January 2022, a copy of which assessment is included in the attached “Fauna” Report to this submission – from page 57)) clearly identifying the true fauna position.

Regrettably, the EPA did not request a full copy of this report to better inform its considerations at that time and only considered Eastcourt’s Environmental consultant, Emerge Associates’ report, who only undertook a Desktop Level 1 Assessment. The EPA made its determination on this alone.

Is it any wonder that Australia is a world leader in the extinction of species when suitably qualified persons issue peer reviewed environmental information (qualified by appropriate levels of on the ground studies) but the EPA rely solely on a Desktop Study when making such an important and significant determination?

## DEATH SENTENCE FOR GLEN IRIS FAUNA?

The human species at Glen Iris will also suffer being affected by a wide range of environmental, health and quality of life issues.

In Appendix B, the Report: “Non-exhaust Vehicle Emissions especially in Urban Areas – Effect of Particulate Matter on Human Health)” refers to the internationally recognised human problems associated with discovered vehicle and even train brake dust.

On 6 May 2021 the UK reported the death of a child: “the Coroner concluded that air pollution was a major contributor to the ultimate death of the girl. The Coroner also called on local government ministers to address holes in local legislation which allow for significant levels of air pollution to continue unchecked, far in excess of WHO guidelines.” Full report: <[brakebetter.com/dust-developments](http://brakebetter.com/dust-developments)>

Microscopic particles (PM2.5 electron microscope) penetrate human tissue and cause negative reactions including death! Electric vehicles will not solve the problem as their increased weight due to batteries will result in more brake dust than non-electric vehicles.

Brake dust will affect walkways and cycle paths close to roads and enter air conditioners during early morning temperature inversion.

City of Melville considers banning new childcares on busy roads –



Kristie Lim, Perth Now – Melville 26 August 2021 (Excerpt)...

“New Childcare centres could be banned along the busy Leach and Canning Highways, and South Street, over concerns about the pollution effect of passing traffic on young lives.

Scientific Data: **Surrounding road density of child care centers in Australia**  
<https://www.nature.com/articles/s41597-022-01172-1>

City of Melville chief executive Marten Tieleman said “the Council’s primary concern about childcare centres on busy primary roads was air quality and noise impacts.”

In Appendix C, the report: “Quality of Life” outlines how the increased density will affect all inhabitants. We now have more data on the effects of traffic, air quality, heat islands, water quality (drugs), light pollution, stress etc.

City of Cockburn Soundings 2020 say they are “leading the way on climate change. The objectives will be achieved with a comprehensive action plan to eliminate greenhouse emissions.”

Does the City of Cockburn need to build and maintain more high-quality infrastructure and amenities for our residents and find additional resources to achieve this?

Do we already have the solution and indeed the ingredients or components to reduce the forecast climate change Armageddon!

\*\*\*\*\*

City of Cockburn has plans to increase the high-rise density at the Gateway Shopping Centre and the plans to double in size.

The City of Cockburn plans to incorporate a Wave Park with associated structures of hotel and convention centre.

Cockburn has the new road links and overpass, plus a railway with station plus an adjoining airport. Why not add a 54.9ha environmental jewel with 1,258 mature trees (not a thousand saplings promised by Eastcourt; saplings that need large amounts of water twice a week).

A 54.9ha golf course (green open space) can also provide oxygen annually for 135,000 people, essential to combat climate change – not possible with the destruction of mature trees with infill of +/- 600 new houses.

The combination of our existing and planned assets will produce “high-quality infrastructure and amenities for our residents” plus “the additional resources to achieve this” are readily available to be incorporated into a “COCKBURN HUB” – good enough not only for the Cockburn residents but it could attract tourist day trips, even cruise passengers’ visits from the Fremantle docks.

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A vibrant world-class “COCKBURN HUB”, coupled with an existing workable Sanctuary for wildlife, would be more internationally attractive than a +/- 600 suburban “dormitory” to be added to the urbanisation on our doorstep.

The prolific quenda population could be saved as “the relocation of quenda to other sites is not an appropriate or recommended option.” <Govt. of WA, Dept of Biodiversity, Conservation and Attractions, Fauna Notes> Plus, with water and the currently existing roosting trees, the many federally protected Carnaby and Forest Red-tailed black cockatoos will return, together with over 60 recorded native species, rather than erect memorials.

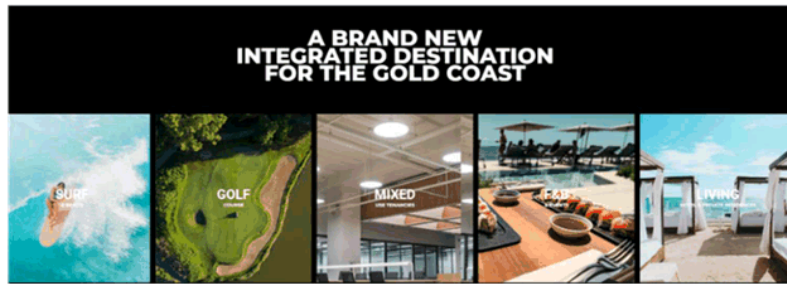
This can be achieved working in conjunction with the City of Cockburn environmental officers and the team at WA Wildlife – the first Western Australia proven animal sanctuary.

It is possible and a wave park complex with an international standard 18-hole public golf course, restaurant and amenities will cost the City of Cockburn nothing, especially as Fremantle has been named in TIME Magazine’s 50 “Greatest Places of 2022.” <Natalie Richards, Perth Now - 13 July 2022>

**EXAMPLE-**

A brand new “integrated destination for the Gold Coast” - A Development Application has been lodged by Parkwood Golf Course for a large \$300 million integrated Surf Park Village located at the Pinewood International Golf Course on the Gold Coast.

<https://golfandsurfgc.com.au>



THIS INNOVATIVE IDEA COULD RESULT IN THE  
CITY OF COCKBURN RECEIVING ITS GREATEST COMMUNITY AWARD EVER!



## 12. Extensive Justification Why a Golf Course is No Longer Viable

Note to the City of Cockburn when reading this section

This section relates to the combined submission, Formal Structure Plan

CoC Action Required: REJECT the developer's proposals as they have not provided extensive justification of why a golf course is no longer viable as specifically required and made known to the Proponent by the most Senior Executive of the CoC.

A statement made by Mr Daniel Arndt, then Acting Chief Executive Officer, in his letter date 2 September 2020 to Hon. Matthew Swinbourn MLC, stated:

*"An application to rezone and redevelop the subject land would need to comprehensively address issues such as the impact on neighbourhood character, amongst other things. They would also be expected to provide extensive justification including but not limited to demonstrating why a golf course is no longer viable"*

Eastcourt have failed in their submission to provide any justification let alone extensive justification to satisfactorily justify why the GIGC is not viable. In contrast, the JRRRA provide publicly available evidence herein that confirm in various forums that Golf in Western Australia is on the rise, in fact booming (refer to 12.1)

Contrary to Eastcourt's singular, unsubstantiated sentence on this issue within its Proposal, golf patronage at the GIGC was always strong and had continued right up until the previous landowner chose to reduce to almost net zero its investment/maintenance on the course and its facilities. The timing of this reduced investment/maintenance approach coincided (incidentally) to its sale to Eastcourt.

**Any statement suggesting a the decline of golf as a sport is not true nor is it based on fact.**

As an example, The Wembley Golf Course brought in a healthy net profit of circa \$1.329M in 2020 and, and a slight dip of \$1,278M even through Golf attendance increased during the Covid-19 pandemic. This is but one current example of what a successful Golf Course brings in when managed properly and not driven into the ground by mismanagement, lack of marketing and no other services including but not limited to drinks on course, drinks off course and meals.

The tables below are from City of Cambridge annual report 2019-2020, and demonstrate that golf is a financially viable sport.

<https://www.cambridge.wa.gov.au/Town-Council/Corporate-documents/Budget-Reporting/Annual-Reports>



25. MAJOR TRADING UNDERTAKING					
<p>The Wembley Golf Course qualifies under the minimum requirements of Section 3.59 of the Local Government Act 1995 and Regulation 9 of the Functions and General Regulations for a major trading undertaking. The Town does not operate any other major trading undertakings.</p> <p>The Wembley Golf Course operates as a stand alone activity and pays the Town an annual dividend which was \$1.708 million for 30 June 2020.</p> <p>A summary of the current year transactions is detailed below:</p>					
	2020 Actual	2020 Budget	2019 Actual		
Revenue	9,338,897	8,929,400	9,086,992		
Expenditure	7,709,630	7,576,600	7,808,472		
<b>Net Profit from Operations</b>	<b>1,629,267</b>	<b>1,352,800</b>	<b>1,278,520</b>		
Add back non-cash	1,689,966	1,650,800	1,435,920		
<b>Net Cash Surplus from Operations</b>	<b>3,319,233</b>	<b>3,003,600</b>	<b>2,714,440</b>		
Less Capital purchases	(8,903)	(10,000)	-		
Less loan principal repayment	(1,165,507)	(1,181,500)	(1,086,572)		
Transfer (to/from) reserve	(218,220)	(100,000)	(82,121)		
Cash Dividend to the Town	1,708,000	1,708,000	1,681,000		
<p>The information contained in the above Operating Statement is inclusive of all items of revenue and expenditure which has been used in the pricing structure for goods and services provided by the golf course trading undertaking. There are no other items outside the above Operating Statement used for this purpose.</p> <p>The interest expense amount of \$748,834 for the 2019-2020 financial year includes an interest charge from the reserve account of \$730,545. This has been eliminated within the Town's financial report.</p> <p>A five year forecast of expected financial performance for the golf course based on current trends in the industry and investment in the golf course hospitality and maintenance golf is as follows:</p>					
	2021	2022	2023	2024	2025
Revenue	8,991,000	9,172,400	9,114,900	9,463,600	9,612,800
Expenditure	7,708,700	8,000,700	8,136,400	8,183,800	8,214,500
<b>Net Profit from Operations</b>	<b>1,282,300</b>	<b>1,171,700</b>	<b>1,188,500</b>	<b>1,279,800</b>	<b>1,398,300</b>
Add back overheads/non-cash	2,052,600	2,216,600	2,216,600	2,216,600	2,216,600
<b>Net Cash Surplus from Operations</b>	<b>3,334,900</b>	<b>3,388,300</b>	<b>3,399,100</b>	<b>3,496,400</b>	<b>3,614,900</b>
Loan Principal Repayments	(556,900)	(1,212,300)	(1,245,000)	(1,278,500)	(1,314,500)
Transfer (to/from) reserve	(1,300,000)	(470,300)	(446,100)	(511,800)	(564,200)
Cash Dividend to the Town	1,708,000	1,708,000	1,708,000	1,708,000	1,708,000

25. MAJOR TRADING UNDERTAKING (Continued)			
<p>Wembley Golf Course Statement Of Financial Position as at 30th June 2020</p>			
	June 2020	June 2019	
<b>CURRENT ASSETS</b>			
Cash and cash equivalents	48,351	213,301	
Trade and other receivables	26,435	210,537	
Inventories	753,416	779,680	
<b>TOTAL CURRENT ASSETS</b>	<b>828,202</b>	<b>1,193,518</b>	
<b>NON-CURRENT ASSETS</b>			
Right to use assets	1,622,242		
Property, plant and equipment	21,972,094	22,156,081	
Intangible assets	12,402,564	12,787,528	
<b>TOTAL NON-CURRENT ASSETS</b>	<b>36,017,400</b>	<b>34,949,609</b>	
<b>TOTAL ASSETS</b>	<b>36,845,602</b>	<b>36,143,127</b>	
<b>CURRENT LIABILITIES</b>			
Trade and other payables	421,349	340,632	
Provisions	289,574	378,585	
Loan liabilities	499,113	-	
<b>TOTAL CURRENT LIABILITIES</b>	<b>1,210,036</b>	<b>719,217</b>	
<b>NON-CURRENT LIABILITIES</b>			
Provisions	54,187	47,999	
Loan liabilities	873,229	-	
<b>TOTAL NON-CURRENT LIABILITIES</b>	<b>927,416</b>	<b>47,999</b>	
<b>TOTAL LIABILITIES</b>	<b>2,137,452</b>	<b>767,216</b>	
<b>NET ASSETS</b>	<b>34,708,150</b>	<b>35,375,911</b>	
<b>EQUITY</b>			
Reserve/Surplus and Contributed Equity	16,371,115	20,035,148	
Revaluation Reserve	18,256,505	21,827,198	
<b>TOTAL EQUITY</b>	<b>34,627,620</b>	<b>31,862,346</b>	
<p>This statement is to be read in conjunction with the accompanying notes.</p>			

See below further substantiation



This demonstrates that Golf has not been hit hard in the last few years!

Wembley Golf Course has helped drive the Town of Cambridge budget. Credit: HeungSoon/Pixabay (user HeungSoon)

## Golfers help drive \$588k surplus forecast at Town of Cambridge

Victoria Rifici | Western Suburbs Weekly  
March 26, 2021 3:19PM

TOPICS **Western Suburbs Weekly** **City/West**

PLAYERS at Wembley Golf Course have helped drive the Town of Cambridge budget a fair way this financial year, with an estimated saving of half a million dollars in spending.

According to the Town's mid-year budget review, a hefty \$588,000 in surplus spending is due to an increase in golfers teeing off at Wembley Golf Course throughout the COVID-19 pandemic, generating an additional \$2.2 million from fee revenue, an increase in patronage and online bookings.

"This mid-year budget review of council shows its in excellent health and the management of the golf course is commendable," councillor Gary Mack said at Tuesday's council meeting.

This demonstrates what a successful golf course looks like



## Town of Cambridge proposes 1.5 per cent rate rise in 2022/23 draft budget

Pip Waller | PerthNow - Western Suburbs  
July 14, 2022 2:00AM

TOPICS [Budget](#) [Community News](#) [Local Government](#) [PerthNow - Central](#)  
[PerthNow - Western Suburbs](#)



Golf again looks like sparing Town of Cambridge ratepayers from the big rates rise many people in other local councils are facing.

The Town's draft budget for 2022-23 includes a 1.5 per cent rate rise, far less than the Town of Claremont's proposed 3.5 per cent increase and [Mosman Park's 5.9 per cent rise](#).

With a total operating revenue of \$56.4 million, the proposed 1.5 per cent increase in rates would generate \$26.5m for the town.

### This further demonstrates that Golf is a highly viable proposition. This is what the Council has reported on:

The Wembley Golf Course was a stand-out success for the Town of Cambridge in 2019/2020 despite the impact of COVID-19 when the venue was completely closed from 25 March until 11 April. This was highlighted by the capacity patronage at the venue once each social restriction stage was progressively relaxed permitting more people and facilities to return to operation.

Golf was one of the limited sports able to be played with the required social distancing. The appeal of golf, as an outlet for strong personal health benefits with the importance on both physical and mental health, was never more

With the increased patronage, the greens fairways and tees are under more stress than ever and our golf courses have been presented to the highest standard. This has been the catalyst for an increase in memberships, mini golf customers, pro shop sales and lesson bookings which augers well for the coming year and beyond.

The golf course has been busier than ever since the lifting of COVID-19 restrictions on sport and hospitality and is currently on track to display exceptional results for the coming financial year.

Melville City Council has previously adopted a Masterplan Approach for the Melville Glades Golf Club, which is also a natural bushland course and the surrounding John Connell Reserve. This previous Masterplan included the incorporation of a large proportion of residential development. Under pressure from the community and the



members of the golf club it was decided and accepted by the Melville City Council the following:

“On Tuesday June 22, 2021, Council adopted the 2021-22 financial year budget which includes funds to carry out the John Connell Reserve Masterplan.

This masterplan will not be considering residential development anywhere on the site as per a previous planning project.”

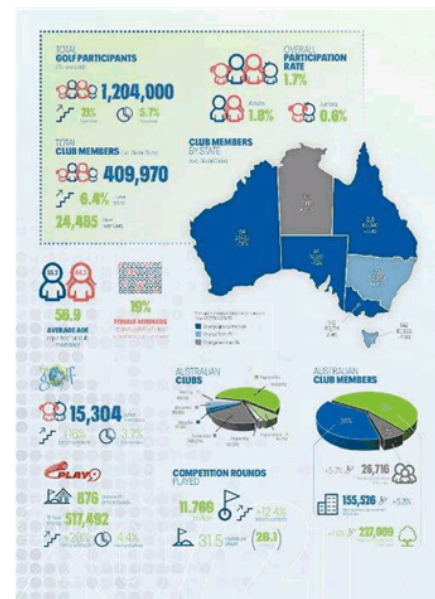
**12.1. Golf is booming in Australia:**

Yet remarkably the City of Cockburn LGA does not have an open golf course for its 120k of residents/ ratepayers. Even though the city has a growing population expected to grow to 165k by 2040. The City Officers have stated the City does not need a golf course as there are plenty of others our community can use. The CEO in a meeting with JRRR representatives just two months into his tenure stated that “I have more things to worry about than a golf course!” and then proceeded to critique to content of the concept plan my making suggestions on how and why we should work with the developer to get a better outcome “for the residents.

Where are these people expected to play golf? Most courses are so busy these days you must book a week in advance, or play at an ungodly hour.

Golf is experiencing a massive uptick as evidenced by a recent February 2022 Golf Australia report.:

The report estimates ‘total golf participants’ at 1.204 million people, according to Ausplay, **up 21 percent on the previous year.**



**13. Character Impact | Loss of: Neighbourhood Character/ Amenity/ 750+ mature trees/ 7 picturesque golf course lakes/ Social impact:**

Note to the City of Cockburn when reading this section  
 This section relates to the combined submission, including Appendix 9 (4.2), Structure Plan (3.3.6, 4.2, 4.6.5) Appendix 4 (4.3.3)





CoC Action Required: REJECT the developer’s proposals as great weight should be given to the expectation of homeowners and the reliance they placed on the original common promotional materials.

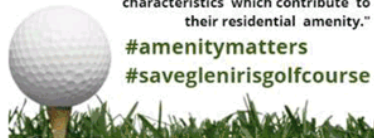
The loss of the 18-hole championship 54.9ha Glen Iris Golf Course/ green lung, with short sighted residential infill circa 600 houses, including group housing, will certainly impact the ‘vibe’ of our golf course estate.

Authorities should plan for the community not developers.

What Eastcourt is planning to do to our ‘golf course estate’ is EnvironMENTAL and human devastation!!



**“The view of residents, as well as the opinions of experts are considered to be relevant in assessing amenity as outlined in Sunbay Developments Pty Ltd and Shire of Kalamunda [2006] WASAT 74, where Barker J at [21] considered that: “... Indeed, residents of a locality are often well placed to identify the particular qualities and characteristics which contribute to their residential amenity.”**



What has been missing in action from consideration by the City of Cockburn is extensive justification including but not limited to demonstrating why the golf course was not viable and why the character of our Golf Course Estate SHOULD NOT be allowed to be impacted.

Easctourt Property Group should not just be able to breeze in with a plan that lacks imagination/ of which lacks any form of real replacement/compensation for what we the community will be losing.

They should not be allowed to have significantly (negatively) impacted the environmental aspect of the golf course land for over two years by not attending in the least to the needs of the mature eco-system that has developed on the land since 1965, TO THEN go to the EPA for assessment of the land in is purposefully devastated state This is ever so wrong! The comments by senior officers that the land is private and they can do with it what they want is



incomprehensible and utterly disgusting. To say this comment over and over again is gut wrenching and unbelievable.

If the Glen Iris Golf Course is infilled with residential housing it will create an estate within an estate. Basically, no different to other suburbs located conveniently closer to the Perth City.

Should the zoning change, some JRRRA members have said they will sell and move elsewhere. Where they will go is unknown but they built in this wonderful estate because of the golf course, because of the open space and because of the amenity and bushland associated with its existence. To stay and watch its demise is soul destroying and heartbreaking to say the least. Will the Council change the street names from the golf themed roads and streets to some other? Some of the JRRRA members have told it that they will be putting their homes on the market and moving elsewhere if the change of zoning is allowed to happen. They built or purchased here because the golf course was the main focal feature and they knowingly purchased into a 'golf course estate' of which even parks and street names were golf themed.

Others who want to buy into the Estate are waiting for the outcome of the Proposal as they have advised JRRRA that they want to live "on a golf course estate".

It was important to the golfing community that the City of Cockburn had an open public golf course for its 120k of residents/ ratepayers. It was something that is basic, let's face it a bit like having no football field, basketball court or tennis court.

JRRRA members feedback is they envisage a massive loss of 'character' with the proposed destruction of 750+ mature trees in the marvelous current nature corridor that has thrived since the 1960s. Our members tell us planting 1,000 sapling trees is simply not good enough, when many of the mature trees are decades (some hundreds) of years old, as apparent by nesting holes demonstrative in one of the flora and fauna reports being supplied as a submission attachment by JRRRA. These saplings will take at least 20 years to mature, well after the extinction of the federally protected black cockatoos. It is well documented that cockatoos are fighting for habitat to nest and artificial nesting boxes are being made and attached to trees to try and alleviate this problem.



JRRRA and the community believes that the plan put forward by Eastcourt Property Group lacks imagination, and it does not reflect the predominant character of a green open space precinct, it only offers parklets, swales and small buffer areas. Some current homes will be sandwiched



between traffic, both from the front and the back of their current homes. There is great concern re the traffic, proposed buses going by homes and the extra population anticipated to be living in the community that has so few facilities. The current character of the golf course estate is a peaceful one.

The Twin Waters Pass bridge is quite special to the community, the Eastcourt plan looks like this will be at future risk despite the Proponent stating that the bridge will remain undisturbed. THE CoC MUST ENSURE THAT THE TWIN WATERS PASS BRIDGE REMAINS. How short sighted that the plan did not make this overpass bridge a main focal point with landscaping and picturesque lakes, all mature trees retained in the area shows not much thought has been put into the indicative infill plan, probably just a desktop plan done by the looks of things, by somebody not familiar with the area.



Of large concern is that the EPA WA gave Eastcourt advice, but the proponent declined the opportunity to update the Structure Plan proposal in response to the EPA's advice. Feedback provided is that the City of Cockburn has referred both proposals to the Department of Water and Environmental Regulation, and the Department of Biodiversity Conservation and Attractions seeking their advice on this (and other) matters. It is confusing to JRRA that as a community we are being asked to put in community submissions, yet Eastcourt are not being transparent in what they will and won't be doing post the EPA's advice.

The Eastcourt indicative plan is not compatible with the current setting of the environment, there is minimal recreational compensation – certainly nothing set in concrete (potentially something jammed into an alternate Prinsep Oval multi use area). We already had footpaths to walk on, it is proposed to have a few more, the odd piece of outdoor exercise equipment seriously does not compensate for the loss of a 54.9ha recreational golf course facility that was great for the community's mental health and overall well-being!

'Loss, loss, loss of character' has been planned. The Eastcourt plan has not addressed social matters, it has not provided replacement amenity, it has not managed its cultural nor environmental impact, and the community consultation to date has not been up to par (pardon the pun) and often been shambolic and unfair to the community that SHOULD matter.

There are concerns that current residents that back on to the currently closed golf course will need to, as a result of the current proposal, block off their rear fence boundaries with Colorbond fencing for security and privacy reasons and to stop people walking past and peering into their back yards. No pathways should be constructed within the proposed buffer zones adjacent to the current see-through rear fences of houses that back on to the current golf



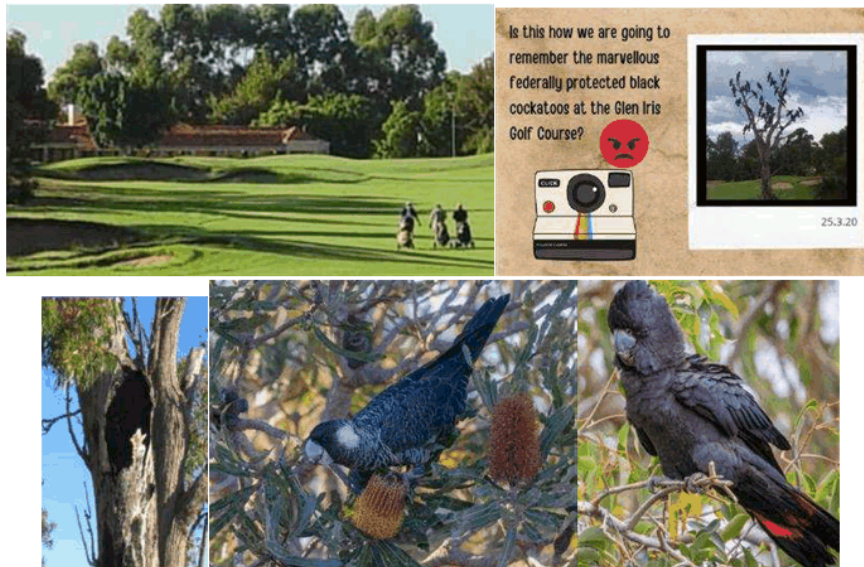
course land. Closed in, colorbond fencing will also no doubt look awful and present a graffiti risk.

The proposed buffer zones (minimum of 12 metres) must be increased to 20 metres without any construction activity within those zones. This will enable capture of many of the mature trees that would otherwise be destroyed. Increasing the minimum to 20 metres does not adversely effect the Proposal.

There is community concern of how the upkeep of the estate will be maintained to an acceptable standard, particularly as weed maintenance by City of Cockburn on medium strips and in parklands is already at times stretched and not up to acceptable standards. How many years will future purchasers have to pay for the maintenance upkeep of the proposed buffer zones and tiny parklets/ swales? Will this be a weed infested/ neglected future Glen Iris Estate?

Massive impact is expected to the MNES federally protected black cockatoos (Carnaby's and Forest Red-tailed), as the Eastcourt Property Group plan demonstrates retaining no water habitat at all for these iconic threatened species. Vehicle strike is anticipated - that is if they are not driven out completely by the lack of water habitat and loss of the 750 + mature trees. Some current banksia woodland showing as housing on the current indicative plan.

The wonderful Priority 4 quenda community that wander into back yards that abut the course are expected to diminish, that would be very sad, as how many communities have a community of quendas visit their back yards and interact with the residences' pets?





Looking into the future it is expected we will show our grandkids what used to be perched on our fence lines, in our golf course estate back yards, in part as the nearby golf course amenity has been a friendly home in the past for these endangered, vulnerable and Priority Species. Instead, we will have to take our grandkids to Cockburn train station to see the iconic birds/ banksias/ water displayed on large artwork, or perhaps at the WA Museum who already have a display of the black cockatoos matters of national environmental significance (MNES) threatened species.

As a committee and community we will shake our heads in despair that our local council, and state authorities did not have the foresight, the integrity, nor the intuition to save the 54.9ha public golf course for the community, a community that SHOULD have mattered, over a property developer who is already plentifully rich, and who took the opportune time purchasing the Glen Iris Golf Course & fragile ecosystem that was zoned Special Use 1, on the expectation that they could get it zoned 'development' due to weak WA planning laws that do not seem to protect either the community or WA golf course estates!

JRRA Committee members/ the community efforts are focussed on saving can be proud of efforts to save the Glen Iris public golf course not just for our generation, but also for future generations, and not just for the local community but for the much braider Statewide community. Committee members have met weekly since April 2020, endeavoring to see justice rightfully served.

We have all seen what happens when golf course recreational land and open green space is not retained for rapidly growing populations, Western Sydney currently is a prime example of such.

JRRA members purchased into the 'golf course estate' for the golf course amenity, the restaurant and bar amenity, the mature tree canopy, the picturesque golf course lakes, the dark sky, the spaciousness, the wildlife, the flora, the abundant birdlife, the views etc, it is

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different than most suburbs in Perth as it has an open space feel. Many residents in the estate are nature lovers.

JRRA members fear for the repercussions to the Jandakot Water Mound with yet more infill of concrete and steel over the precious mound. They particularly have concerns re the loss of golf course vegetation and the resultant additional noise impact from the ever increasing overhead air traffic from the nearby Jandakot Airport.



The character of the area will certainly change, currently there is only a smidgen of group housing, land sites range up to over 1200m2 in the current golf course estate, most blocks average around 700m2, we fear for the loss of gardens, we are concerned about group housing parking issues, rear loaded garages, some of us have viewed Eastcourt’s Providence Estate and were horrified at the standard they have set there.

Some images here are of small group sites at Eastcourt’s Providence Estate, the group housing planned at the Glen Iris Golf Course Estate is very out of character of what was purchased into. This is what can be expected from average 300m2 blocks of land, so out of character of what we currently have.:



The character of the current estate has driveways that allow for parking to be off street and on our own driveways. Our roads are not clogged up with cars/ safety issues, an issue that often comes with lots sizes that only average 300m2, as per the planned group housing. Currently residents purchased in the estate knowing that there was no social housing, residents appear



concerned that some of the group housing is slated for social housing, especially as located on the future bus route. It must be remembered this was sold as a premium estate, with premium prices, due to the golf course being in place and integrated with the surrounding houses.

The members who live on the course enjoyed calling out to the members of the Lakes Glen Iris Golf Club, on their Scottish themed day, where many wore kilts. The Glen Iris Golf Course has been in existence since the 1960's. It is rich with history, there is an inaugural member of the Glen Iris Lakes Golf Club (Barry) who is a legend, he must now get up early to play with the Glen Iris Lakes Golf Club at Kwinana Golf Club, members have rapidly depleted due to travel times and early starts, Barry rolls out of bed to ravel 35-40 minutes for tee off at 7.30am, as the course is so heavily booked. Unfortunately he must get up early to travel the long distance – all this sadly in his twilight years as he has been displaced by the Eastcourt Property Group..

Some of the residents living in the Glen Iris Golf Course Estate are golfers, feedback from many is that they miss strolling down to their golf course amenity, restaurant, and bar/ social hub. Eastcourt should never have closed these amenities. Nor should they have allowed the course to have looked so shabby.



Below is what Eastcourt Property Group's indicative Concept Plan offers.:



### 14. Implied Restrictive Covenants

Note to the City of Cockburn when reading this section



This section relates to the combined submission, including Structure Plan (4.3, 4.6.1, 5.2.5) Appendix 7 (1.13)

CoC Action Required: REJECT the developer's proposals as great weight should be given to the expectation of homeowners and the reliance they placed on the original common promotional materials.

Original marketing of a subdivision where the golf course is located plays a significant role in establishing an implied restrictive covenant.

Developers of integrated 'golf course estate' projects that include a golf course or other significant amenities integrated with residential housing should disclose in their marketing materials that these golf amenities may change or be discontinued.

If resultant homeowners were protected by legislation and proper planning laws in place in Western Australia, the previous landowner who held the land from inception of the integrated housing and golf course estate until its sale in 2020 (in this case the Wilson Family) should have been forced to advertise/sell the golf course as a 'golf course going concern' in perpetuity. The original landowner was well aware of the Restrictive Covenants that it placed on the integrated housing (a large portion of which – 240 lots) still have an ongoing (in perpetuity) Restrictive Covenant in place and registered with Landgate) yet these were not lifted prior to the sale.

Regardless of the previous long term landowner attempting to gain a higher sale price selling the golf course land/ amenities to a Property Developer (i.e., Eastcourt Property Group) for potential development, the Purchaser knew the details of the current zoning that has been in place for many decades. The Proponent is now attempting to apply for scheme zoning approval 'SU1' to 'development' simply in order to carve up the golf course land into lots for massive future profit. Having no regard to the negative effects it would be imposing on the Community both within the estate and the broader Statewide community.

The original Property Owner (Wilson Family) is clearly double dipping on profit taking as it already received premium prices for the Golf Course land, as residents paid premiums to live in the Glen Iris Golf Course Estate, or to overlook the Glen Iris Golf Course, either directly or with secondary views. They are having no regard to their very well publicized/implied undertaking to the Purchasers of the blocks about the future ongoing operation of the Glen Iris Golf Course. Those Purchasers could end up with an estate plonked within another estate.

There should be a flow on effect to purchasers, protecting those that purchased into a golf course estate, i.e., from purchaser to purchaser. IF NOT, GOLF COURSE ESTATES SHOULD NEVER HAVE BEEN ABLE TO BE DEVELOPED IN THE FIRST PLACE IN WESTERN AUSTRALIA, as it clearly rips off the good people who purchased into the golf course estate in the first place. In the Glen Iris Golf Course Estate there are golf-themed street names/ parks and golf-related easements and restrictions in the restrictive covenants that encumbered the lots - to the advantage of the golf course. i.e.: no washing lines or sheds visible from the course, 6m setbacks from the course boundary which allowed some leniency for stray golf balls etc, these and other restrictive covenants changed the way that Glen Iris Golf Course Estate residents built their 'own' homes on their 'own' blocks of land in the premium marketed estate.

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It also changed the materials used in construction of their 'own' homes, i.e., more glass at the backs of the homes adjoining the Glen Iris Golf Course etc.

Notably the 220 homes that directly 'surround' the Glen Iris Golf Course are not just neatly tucked up one corner. Many more homes within the golf course estate have secondary pleasant views of the 54.9ha golf course amenity/ nature reserve.

Original subdivision marketing materials re the Glen Iris Golf Course Estate included numerous references to the golf course, i.e., 'Magnificent quality blocks with glorious golf course views for sale' / 'Invest in an Exciting New Lifestyle etc.'

The golf course amenity (and associated amenities) importantly was also advertised as being just a short stroll from our front doors. The golf course and associated amenities were clearly used as a hook for residents to pay premium prices for the land/ houses. It is all so very wrong to now be in this position, that is causing great distress to residents of the estate, as well as golfers who have currently lost their local golf course amenity, the only one in the quickly growing City of Cockburn!


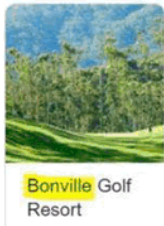
City of Cockburn were certainly aware that the blocks were originally being marketed as a 'Golf Course Estate' as evidenced by the words 'Golf Course Estate' which are included in the various zoning Amendments approved and adopted by the City.

Demonstrably the restrictive covenants that exist, as evidenced by Landgate registrations on homeowner lots in the Glen Iris Golf Course Estate, the presence of the Glen Iris Golf Course was integral to JRRR members' decision to purchase in the golf course subdivision, mid 1990's on.





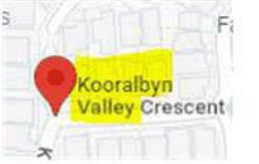







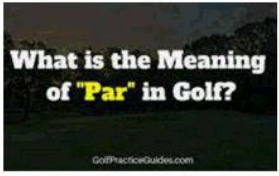
Great weight should be given to the expectation of homeowners and the reliance they placed on the original common promotional materials. Promotional materials that are still used today.

Great weight should also be given to the fact that the streets and parks were allowed to be named golf course themed names, which absolutely demonstrated that it would be a 'golf course estate' in perpetuity.









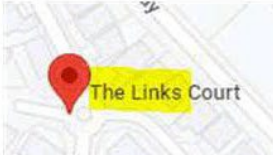

**Examples of Glen Iris Golf Course Estate, golf themed street names and parks. Strongly suggesting that residents were purchasing into a 'golf course estate' in perpetuity.:**

Glen Iris Golf Course Estate Golf Course themed 'road names':	Famous Golf Courses with the same names	
Bonville Glen		

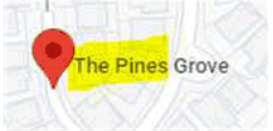

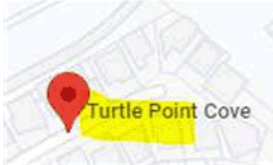

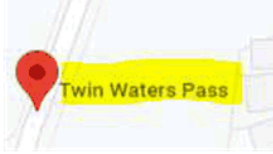

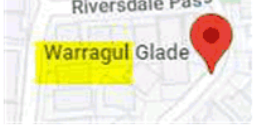

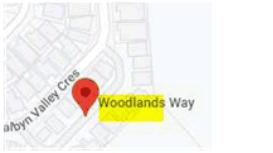
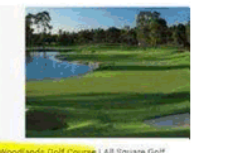
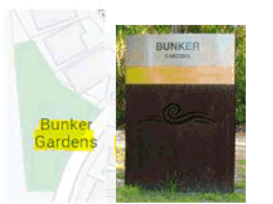



Glen Iris Golf Course Estate Golf Course themed 'road names':	Famous Golf Courses with the same names	
Coolum Mews		 <p>Mount <b>Coolum</b> Golf Club mtcoolumgolf.com</p>
Kingston Heath Mews		 <p><b>Kingston Heath</b> Golf Club   Austadiums austadiums.com</p>
Kooralbyn Valley Cr		 <p>Review: The <b>Kooralbyn Valley</b> National ... golfaustralia.com.au</p>
Kooyonga Cross		 <p>Top 100 Spotlight <b>Kooyonga</b> Golf Club ... australangolfdigest.com.au</p>
Laguna Grn		 <p><b>Laguna</b> National   Singapore Golf Course lagunanational.com</p>
Par Ct	 	 <p>What is the Meaning of <b>"Par"</b> in Golf? GolfPracticeGuides.com</p>







Glen Iris Golf Course Estate Golf Course themed 'road names':	Famous Golf Courses with the same names	
Portsea Gdns		 <p data-bbox="951 533 1141 562"><b>Portsea Golf Club</b></p>
Riversdale Pass		 <p data-bbox="951 781 1157 817">Riversdale Golf Club in Mt Waverley ... golfpass.com</p>
The Fairway		
The Lakes Blvd		 <p data-bbox="951 1319 1157 1355">Review: The Lakes Golf Club - Golf ... golfaustralia.com.au</p>
The Links Ct		 <p data-bbox="951 1554 1109 1574">What is a Links Golf Course?</p>



Glen Iris Golf Course Estate Golf Course themed 'road names':	Famous Golf Courses with the same names	
The Pines Gr		 <p>The Pines   InterContinental Sanctuary ... intercontinentalsanctuarycove.com</p>
Turtle Point Cove		 <p>Turtle Point at Kiawah Island Golf ...</p>
Twin Waters Pass		 <p>Twin Waters Golf Resort in Twin Waters ...</p>
Warragul Glade		 <p>Warragul Country Club - Golf Course ... hole19golf.com</p>
Woodlands Way		 <p>Woodlands Golf Course   All Square Golf allsquaregolf.com</p>
<b>Glen Iris Golf Course Estate Golf themed 'park names':</b>	<b>Golf Course related meaning</b>	
<i>Bunker Gardens</i>		 <p>What is a <b>Bunker</b> on a Golf Course? liveabout.com</p>



Glen Iris Golf Course Estate Golf Course themed 'road names':	Famous Golf Courses with the same names	
<p>Fairway Park</p>		 <p>The Fairway on Golf Courses: Definition ... liveabout.com</p>
<p>Yara Vista Park</p>		 <p>Yarra Yarra Golf Club - Golf Property golfproperty.com</p>

### 14.1. Advertisement Inducements

JRRA have received many calls and emails from the community saying they were attracted to purchase at Glen Iris Golf Course Estate via various advertising/ marketing inducements, no doubt advertising/marketing material scripted as a result of the following statements made in various Amendments, including Amendment #82, which the City of Cockburn adopted in 1992.

“The subject land which is only 18km from the Perth GPO is strategically placed to **offer home buyers a homesite in a very desirable golf course setting.**”

“In wanting to effectively guide and co-ordinate the development proposals in this area, Council in 1991 prepared a Concept Plan which involved reducing the golf course to 9 holes surrounded by residential development.”

“Council at its ordinary meeting held on 1 September 1992 resolved to support in principle a revised integrated Residential /Golf Course (18 hole) Concept prepared by Chappell & Lambert Planning & Design Consultants. Plan No.5 depicts the Concept Plan endorsed by Council.”

Objective:

“It is the landowners **objective to create a desirable residential environment that capitalises on the natural abilities and characteristics of the site and an integrated 18 hole golf course.....**”(page 7)

Golf Course / Club House:



**“It is proposed that the residential estate be developed around an international standard, 18-hole public golf course.** The design and layout of the golf course seeks to:

... utilise part of the existing fairway on the existing Lakes Golf Course

...maximise integration of the golf course throughout the estate;

... create an international standard 18-hole golf course;

“The golf course will be complemented by a Country Club/Tavern which will incorporate a range of sporting and recreational facilities and amenities.”

Residential:

“As clearly indicated in the **Concept Plan the residential estate has been integrated with an 18-hole golf course.** The design of the residential precincts seeks to maximise the number of residential homesites with direct aspect of the golf course and provide a number of homesites with secondary views of the golf course.”

“By locating the entry roads to the residential cells with direct views across the golf course, all residents within the residential villages will benefit from the proximity of the golf course **and the ‘feeling’ of being within a golf course estate. Recent research indicates a strong demand for residents to live within a golf course estate.**”

Conclusion:

“As clearly demonstrated within the **Concept Plan, the Lakes Golf Course Estate will be developed as an exciting upmarket residential estate established around an attractive 18-hole golf course.**”

**“Council and PDUD are therefore requested to support this development by initiating the necessary amendments to the Metropolitan Region Scheme and City of Cockburn District Town Planning Scheme No. 2.”**

The golfing estate and attached amenities were a significant drawcard for residents to pay premium prices, even though the suburb was located nearby lower economic areas, had the nearby noisy air traffic from Jandakot Airport. To this day there is still a sign warning about air traffic noise on the entry road to the northern side of Berrigan Drive of the current Glen Iris Golf Course Estate (Dean Road) warning potential purchasers of the potential air traffic noise.

This drawcard of living on a “golf course estate” continued to attract purchasers right up to the sale of the golf course in 2020.

The advertising inducements of the Glen Iris Golf Course Estate are as follows:



Brochure 1	

Brochure 2



### Brochure 3


### Brochure 4

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It is usual practice for people to rely on advertising/marketing material as an inducement to consider purchasing a property; just in the same way that we use real estate agents today to advertise and market a property we may wish to sell, or scan the real estate section of the newspaper/website for a property to purchase.

The fact that the original property developer did not disclose that the golf course land was privately owned and could be sold (and possibly rezoned in the future) could be considered misleading/misrepresentation and/or a lack of a duty to disclose such vital information. However, equally, the City of Cockburn, in approving and adopting various Amendments to allow for the creation the Glen Iris Golf Course Estate should have protected prospective purchasers by ensuring it was mandated that this disclosure was included either in any advertising/marketing material and/or (especially in) subsequent formal documentation, such as the Offer & Acceptance, and/or included in any Restrictive Covenants imposed thereon which formed part of the O&A, just in the same way that the City mandated the developer to disclose to prospective purchasers (in Restrictive Covenants) that they were aware they lived in close proximity to the Jandakot Airport with resultant aircraft noise.

Signage regarding 'noise' is still evident within the Estate today.

Excerpt from CoC Minutes re Amendment #119 dated 28 May 1996 – see (2) conditions 1 and 2 below.

- (2) require as a condition of Amendment No 119 that:-**
- 1. all residential lots within the estate are to have a memorial on title to make residents aware of the potential noise impact from the operation of the Jandakot Airport;**
  - 2. the requirements of the Federal Airports Corporation identified as a-d) in its letter to the City of Cockburn dated 31st August 1995 be applied by the developer to all residential development within the estate;**
- (3) advise the Hon Minister for Planning of the Council's decision and request that final approval be granted without the need to re-advertise the proposed modifications.**

No memorials were placed on Certificates of Title. However, Restrictive Covenants (i) stated: "these properties are within the vicinity of the Jandakot Airport and may be subject to aircraft noise/activity."



If the CoC considered that noise/activity from the Jandakot Airport was important enough to mandate, one would assume that the CoC would have also determined it was even more important to mandate disclosure that the land was privately owned and could be sold in the future (and possibly rezoned).

Not only has the original developer failed us, but equally, it could be said, so has the City of Cockburn.

This can be rectified now by REJECTING the proposed rezoning of the land from Special Use 1 to development.



## 15. Win-Win

The current building once housing the Glen Iris Golf Course Estate Clubhouse and Pro Shop should and must be left to the City of Cockburn community as a whole.

This building and amenity it provides would make a wonderful asset which could be offset by the amount of rent earned for the City and a wonderful space for meetings. It is disability approved and meets all government standards to lease this property and obtain another liquor licence, thus turning over more money for the future upkeep of the building if leased to private enterprise.

This building is double brick, air conditioned and not badly maintained up until the sale in 2020. The building was internally painted in 2018 and new carpet and tiles were installed. All the furniture is currently sitting in the building covered in dust and could be reused at next to no cost.

Internal lighting was upgraded to LED in 2018.

The car park is adequate and meets CoC standards for a restaurant and reception centre with toilets and changerooms for both male and female patrons. It was re linemarked during 2018 at the cost to the then tenant.

The restaurant kitchen was upgraded by retiling all the walls, upgrading and replacing coolroom machinery and renovating the ducted cooking hoods in 2019 when the previous owners decided to reopen the bar area and sell quick food. This upgrade was instigated by the COC Health Department as part of their application for the liquor licence to be returned. The external area had new outside shade coverings installed in 2018 with a shelf life of some 10 years.

The bar and restaurant up until late 2017 had a turnover in excess of \$2.5million dollars and paying rent and outgoings in excess of \$300,000.00 per annum not including electricity and gas.

The previous owner stopped advertising the golf course and let the course become sub standard. Bunkers were left to seed and grass overtook the sides. For special golf days members actually cut and sanded the bunkers themselves. Private golf days for businesses became rarer and rarer as corporate businesses did not want to pay top dollar for a substandard course. The restaurant including the bar was open 7 days per week from 8.00am until late. Sundays was a special family day and families from far and wide attended the club house for live music and kids special events 12 months of the year.

We would encourage the CoC to ensure the clubhouse remains in the hands of the City and investigate leasing out the building to private investment. A ground Lease would ensure the tenant would maintain the building whilst ensuring adequate money coming in from a restaurant/bar being open. With the "clubhouse" remaining the Pro Shop could be easily and cheaply turned into a function centre for the CoC Residents Associations to utilize, for play groups to attend, for Chamber of Commerce events and for private enterprise to use for Corporate conventions. This building is a wonderful event space with weddings being held at the South or North Lakes currently full of artesian water and the Receptions being held in the

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function centre. With a Funeral Parlour being planned for South Lake, this is an ideally suitable location for many wakes to be held.

This building is in a prime location and should and must remain for the Community.

- Estimated ground rent per annum: \$100,000.00 ex GST
- Lease Term: 10 year with a 10 year option
- Rent Reviews : annually to 2.5%. Market review at Option
- Turnover rent to be paid at an unnatural breakeven of \$2,000,000.00 at a % of 1.5%.
- Casual rent to community groups at a discounted rate. Casual rent to be part of turnover.

## 16. Other Points of Significance for CoC absorption

### 16.1. Planned Orchestrated Degradation

Purposeful 'significant' seemingly orchestrated degradation of the 54.9ha 18-hole championship Glen Iris Golf Course Jandakot amenity.

Has manipulation 101 of the threatened species/ Priority species habitat happened? It certainly appears so!

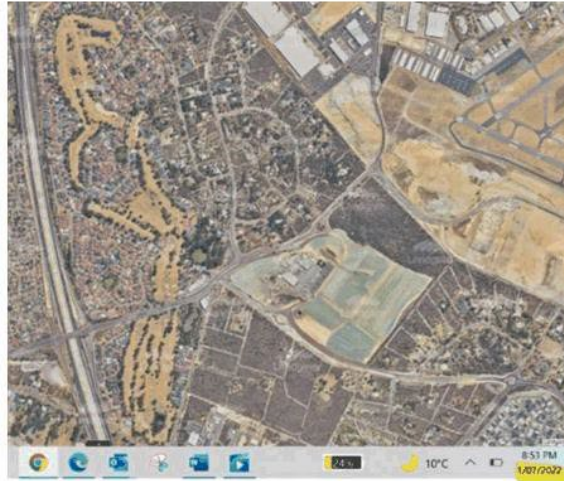
1. Settlement date of the Glen Iris Golf Course land to Eastcourt Property Group (EPG) was 6.4.20
2. Landgate records note that the agreement to purchase the site occurred in January 2020.
3. In 2022 Environmental Protection Authority WA (EPA) assessment was done on the already significantly degraded golf course land, i.e., assessment done approximately two years after EPG purchased the golf course land and after Eastcourt had ceased all watering/maintenance of the land..
4. EPA assessment decision to not go to public consultation was published April'22.
5. Note important point: When the EPA assessed the golf course land, EPG at that stage had already significantly impacted the threatened species/ priority species habitat for almost two years by removing water infrastructure, additionally purposely draining five out of seven golf course lakes that were water habitat for the federally protected black cockatoos, matter of national environmental significance (MNES). Both Carnaby's and Forest Red-tailed black cockatoos' habitat and the P4 Quenda significantly impacted. Please refer to the comprehensive flora and fauna survey report provided as an annexure with this JRRR submission.
6. The April'22 EPA assessment included that planting should be designed to reduce the risk to fauna of vehicle strike!! These threatened species/ Priority species never used to be subjected to vehicle strike on the golf course.



7. Two resting trees removed from the golf course land, one of which was located nearby roosting trees (Twin Waters Pass), and of which the Property Owner purposely drained the golf course lake in 2020, which significantly (negatively) impacted the nearby black cockatoos' roosting - as black cockatoos often roost nearby water sources, in tall trees.
8. The EPG report noted that there were approximately 20 Carnaby's Black Cockatoos recorded perching in trees during the site assessment on 11 March 2020 within the central portion of the site.
9. EPA Advice: Carnaby's Cockatoo in Environmental Impact Assessment in the Perth and Peel Region: *Flocks show site fidelity to a particular area, but will move between roost trees from day-to-day apparently in response to environmental factors (i.e., distance to food and water, influence of wind and predators).* Berry 2008; Berry and Owen 2009; Berry and Owen 2018; Finn et al. 2009; Groom 2015; Shah 2006
10. EPA Advice: Carnaby's Cockatoo in Environmental Impact Assessment in the Perth and Peel Region: *Roosts are preferred in close proximity to water (100m – 1km) and within 6km of potential feeding habitat.* Glossop et al. 2011; Le Roux 2017
11. Federally Protected matters of national environmental significance black cockatoos/ P4 quenda are important to existing residents and the wider Perth community, their habitat has to be protected - or they will become extinct.
12. The current plans anticipate **up to 8** of the 11 black cockatoo breeding habitat trees **could be** retained on site in public open space and road reserve, subject to detailed engineering and earthworks design. Once the detailed designs have been progressed to subdivision and/or development application stage, a "self-assessment in accordance with the Commonwealth Department of Agriculture, Water and the Environment (DAWE) Matters of National Environmental Significance Significant Impact Guidelines will be undertaken to determine if the confirmed impact extent raises any issues that will alter the current view that any impact to the Black Cockatoo species." (Note DAWE agency name changed to DCCEEW see below).
13. JRRALnc maintains that the proposed 'self-assessment', if it gets to the stage that Eastcourt's detailed designs are finalised, is certainly NOT SATISFACTORY and JRRALnc is of the opinion that there should be a mandatory requirement for a referral to the Department of Climate Change, Energy, the Environment and Water (DCCEEW). As the DCCEEW website states the federal department established 1.7.22, is responsible for environmental protection and conservation of biodiversity.
14. If Eastcourt are found in breach of purposeful significant degradation of the MNES habitat at the Glen Iris Golf Course Jandakot, JRRALnc would expect that federal authorities would take the matter very seriously, and that Eastcourt should face the consequences of any breaches, and there should be reinstatement of habitat, i.e., all water habitat.



THE REFERRAL TO DCCEEW CLEARLY SHOULD BE MANDATORY AND NOT LEFT UP TO EASTCOURT'S OWN DISCRETION as we all know what that 'discretion' will lead to.\*\*\*\*\*







## 16.2. Procedural Fairness

That there has been an absence of procedural fairness afforded to residents of the Glen Iris Golf Course estate. A proper and fair process of community consultation must not predetermine or be designed to procure a particular outcome. RATIONALE - The principles of procedural fairness are well established in Australian law. The principle established in *Annetts v McCann* (1990) 170 CLR 596 held that the duty of procedural fairness persists even where private or professional interests may be affected by findings of any process of inquiry.

There is also a perception based on responses from Council officers in their responses to residents.

13 October 2021 JM email to Peter Ikstrums – Streetscape Officer re the damaged fence

CoC were advised that there had been damage to a fence along the golf course land. His reply (in part) to JM was: "I believe the fence is likely to be removed **and replaced with the development**". (Emphasis added by author of this JRRR correspondence)

This demonstrates a pre-determined outcome especially as CoC Councillors did not vote to initiate the complex scheme arrangement until 9 December 2021, yet Peter Ikstrums confirmed this in October 2021 to the resident.

## 16.3. "STATE OF THE ENVIRONMENT REPORT"

(COMMENTS ON A FLAWED PROCESS)

Initially I refer to an article in *The Guardian* 19 July 2022 by Morton and Readfearn:

"State of the environment: shocking report shows how Australia's land and wildlife are being destroyed" – excerpts below:

"The health of Australia's environment is poor and has deteriorated over the past five years due to pressures of climate change, habitat loss, invasive species, pollution and mining, according to a government report that warns the natural world holds the key to human wellbeing and survival.

The state of the environment report – a review completed by scientists last year but held back by the Morrison government until after the federal election – found abrupt changes in some Australian ecosystems over the past five years, with at least 19 now showing signs of collapse or near collapse.

**"I won't be putting my head in the sand," Minister Plibersek said. "Under Labor the environment is back on the priority list."**

The report quoted the World Economic Forum in finding that environmental degradation was now considered a threat to humanity that could "bring about societal collapses with long lasting and severe consequences".

Key points from the state of the environment report

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- Since 2016, 202 animal and plant species have been listed as threatened matters of national environmental significance, following 175 being added to the list between 2011 to 2016. This has happened while the rate of discovery and description of new species has slowed considerably over the past decade. There remain many more species that are unknown than those known.
- While a government threatened species strategy had improved the trajectories of 21 priority species, many others did not show improvements. The list would increase substantially in coming years as the impact of the catastrophic 2019–20 bushfires – which killed or displaced between 1 billion and 3 billion animals – became clearer.
- Australia has lost more mammal species than any other continent, and has one of the highest rates of species decline in the developed world. More than 100 Australian species have been listed as either extinct or extinct in the wild. The major causes of extinction were introduced species and habitat destruction and clearing.
- The report found improving the state of the environment would require national leadership, integrated management across federal, state and territory systems, new forms of funding and improved monitoring and reporting.
- Plibersek told the ABC on Tuesday that the country’s environment laws were not working, authorities did not have adequate data on the scale of the problem and change was needed. “If we stick with what we’re doing now we’ll keep getting the same results,” she said.

The minister said she expected to introduce changes to environment laws, including the creation of the Environment Protection Agency, to parliament next year after consulting “very widely”, building on the work of a statutory review by the former competition watchdog chief Graeme Samuel.

**The Greens’ environment spokesperson, Sarah Hanson-Young, said the report showed “this is an emergency and in an emergency you take emergency action”.**

**She said it described a “litany of environmental wreckage fuelled by climate change and years of denial and neglect”,** and environment laws needed to be changed, including to consider the climate impacts of proposed fossil fuel developments.

**“If the minister is really alarmed by this report, then she will take immediate action to ensure no more critical habitat is cleared and polluting projects that are fuelling the climate crisis are stopped,” Hanson-Young said.**

The president of the Australian Academy of Science, Prof Chennupati Jagadish, said the report was sobering reading and the outlook for the environment was grim, with critical thresholds in many natural systems likely to be exceeded as global heating continued.

Jagadish said the report showed there had been “significant underinvestment” in the scientific knowledge and capacity needed to understand the state of the environment and called for the creation of an independent agency to manage wildlife and biodiversity data. He



said Australia should also revisit its emission reduction commitments to more rapidly respond to the climate crisis.

**Prof Euan Ritchie, from the Centre for Integrative Ecology at Deakin University, said the report was authoritative, long overdue and confirmed “Australia’s utter failure of environmental and conservation stewardship”.**

**But he said it was not too late to change the trajectory. “If we act now** and strengthen and enforce environmental laws, provide far greater investment to aid the protection and recovery of the environment and threatened species, and better engage with communities, we stand to gain substantial social, cultural, economic and environmental benefits.”

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JRRA now present a current example of how the “system” fails, not only the environment, but the very animals it is supposed to protect:

A report on the Glen Iris Golf Course Estate land/eco-system was written over a two-year period (August 2020 – May 2022) regarding the process whereby the animals and environment were ignored and 54.9ha of climate enhancing golf course land was destroyed and even ignored by the so-called “protective” departments. The report “Sanctuary for the Protected Fauna (other wildlife) and Protected Flora on the Glen Iris Golf Course Estate, Jandakot, WA” is attached.

The report refers to the Glen Iris Golf Course Estate situated 19km from central Perth, Western Australia. The course is partially situated on the Jandakot Groundwater Mound which is an unconfigured aquifer supply drinking water to Perth.

The golf course contains abundant Priority 4 Quenda and in June 2020 it contained flocks of over a hundred federally protected Carnaby’s and Forest-red tailed black cockatoos. (See attached report - pages 19, 20, 41, 42, 43, 46)

In April 2020, a developer (Eastcourt Property Group) purchased the land and are currently making application to rezone the land (from Special Use 1 to development) with +/- 550-600 new houses. If approved the proposed infill involves destroying the habitat for the animals and a fragile walled ecosystem that has existed for over 65 years, destroying 750 of the 1,250 mature trees, compromising the drinking water aquifer, and seriously affecting the quality of life for the existing homeowners (770 residences) facing and integrated into the golf course.

The developer purchased the land in April 2020 and immediately removed the golf course pumps that irrigated the golf course. Five of the seven golf course lakes were immediately drained with the linings physically removed under the cover of clandestine, early morning weekend work, to Eastcourt states “prevent people drowning”. (despite the lakes never having been fenced and having survived for many decades without any single person drowning therein) The two lakes that remain are probably left to remain with water because they may be artesian and presumably “drown-proof”.

Emerge Associates, Eastcourt’s environmental consultants, have written in other reports that protected Carnaby’s cockatoos require water adjacent to roosting trees, plus the duck -

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breeding season had commenced, ducks requiring water to escape danger and predators because the ducklings could not fly. (See page 38). Whilst ducks are not an endangered species, all native animals are legally protected <[www.dpaw.wa.gov.au](http://www.dpaw.wa.gov.au)>

Adverse reports to the City of Cockburn Council were ignored by the City on the basis that “it is private land.”

The Environmental Protection and Biodiversity Conservation Act 1999 states “if a matter of national environment significance (54.9ha) is on or near your property then you are in a position to help protect it.”

The golf course contains 2ha of “good” classified Banksia’s which provide food for roosting Carnaby’s plus the Banksia trees come under federal jurisdiction.

Having drained the golf course of water areas, the developer installed fire breaks for “resident safety and requirements” and at the same time cleared the “debris” which comprised bush for the animals inclusive of 60 species of native birds.

The golf course continued to deteriorate from their purchase date in April 2020. In December 2021 the City of Cockburn Councillors voted to initiate a complex scheme amendment and we are now currently in the 60-day public consultation period which closes on 25 July 2022.

Emerge Associates (environmental consultants) had completed a Level 1 Desktop Fauna and Flora Assessment. The Survey ignored the documented roosting trees (see page 50) for the cockatoos and two afternoon walk-throughs recorded four digging sites for Priority 4 quendas, which are nocturnal.

In January 2022, the Environmental Protection Authority in Perth was advised regards the prolific quenda (73 individual sightings by residents’ survey - see pages 19/20) plus the flocks of black cockatoos (see page 24).

“The resultant EPA report based its decision on the Scheme Amendment documentation provided by Eastcourt Property Group to the City of Cockburn and “having considered this matter, the following advice is provided. Advice under Section 48A (1) (a) Environmental Protection Act 1986. Advice given: ‘not to be assessed’ and (Not appealable).”

City of Cockburn’s letter “Notice of Advertising” dated 23 May 2022 stated: “On 20 April 2022, the Environmental Protection Authority determined that the likely impacts associated with the development are unlikely to have a significant impact on the environment and do not warrant formal assessment under Part IV Division 3 of the Environmental Protection Act 1986.” (emphasis by writer)

The EPA did not acknowledge or even mention the word “quenda” in their determination and do not encourage any correspondence.

City of Cockburn were advised of quenda numbers with the following reply on 4 July 2022:

“Emerge visited the site to identify areas considered to be natural fauna habitat. Five areas were identified as suitable quenda habitat. These were identified as scattered native woodland, planted trees and shrubs with low understory and vegetation around the artificial

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lakes. Much of this habitat is to be retained in the current structure plan. I can see no information within the fauna assessment that indicates the ecologist from Emerge recorded seeing any quenda on the site visits. Only diggings were recorded. The report does indicate that some quenda were captured on cameras. Emerge make no reference to the quantity of quendas on site. The City has not made any reference to the number of quendas on site.

In relation to the assessment of the environmental impacts by the Environmental Protection Authority, they make the determination as to what constitutes a significant impact when proposals are referred to them".

The EPA in Perth does not enter into correspondence plus any bird matters must be from a recognised Ornithologist (directions from EPA Perth).

With regard to the black cockatoos, the EPA included in its determination "Potential Significant Effects" – Clearing of Native Vegetation and black cockatoo habitat and then quoted "In addition to structure planning provisions, it is recommended that the scheme provision are modified to include reference to future development being required to prioritise black cockatoo habitat for retention."

Upon enquiry by a resident who lives in Glen Iris in a communication to the City of Cockburn as a result of Eastcourt's Indicative Subdivision Plan showing proposed housing along Hartwell Parade where the entire street contains "good" Banksias, plus mature roosting trees, the reply was:

"Upon receipt of the EPA's advice, the proponent was offered but declined the opportunity to update the Structure Plan proposal in response to the EPA's advice. The City has referred both proposals to the Department of Water and Environmental Regulation, and the Department of Biodiversity Conservation and Attractions seeking their advice on this (and other) matters." (underlining added)

There are 2ha of "good" rated Banksia trees on the golf course which come under federal jurisdiction.

Is this further evidence that the EPA's advice has again been completely ignored?

Possibly ignored by Eastcourt as it already believes that it has a predetermined outcome?

The attached report "Sanctuary for the Protected Fauna and Protected Flora...." was compiled over the period June 2020 to May 2022. It conflicts with the "official" reports as the attached report relies on actual facts whereas the official reports are lacking in facts.

The Desktop Level 1 assessment relies on published data which is limited for many areas of Perth.

The Perth Community Quenda Survey 2012 by Dr. Geoff Barrett et al reports one quenda sighting from Glen Iris (page 18 attached report). Local Council has no records of quenda, thus scarcity of nocturnal animals would be confirmed by the Emerge "walk-through" during daylight hours. Similarly, the federally protected black cockatoos were driven away due to



lack of water, removed by the developer soon after purchase. What chance do the animals have when the protectors and the system fails and even works against them?

Independent reports not submitted by recognised zoologists, professors, and ornithologists are ignored by authorities. The qualified government advisors are cautious to discuss specific cases which may conflict with their appointment conditions.

So what chance do the animals have against politics, significant political donations, experienced developers (who know the system) and a Level 1 assessment that does not favour any animals, especially the “federally protected and Priority categories”.

As of July 2022 the Glen Iris Golf Course Estate residents (and wider community) are nearing the end of the Public Advertising period with little hope for the animals as the system ensures that they will not even be acknowledged (quenda) or observed as they have been driven away (cockatoos) plus any reports must be from qualified recognised ornithologists.

The attached report outlines many notable quotations by many notably people of the times, usually politicians who seemingly override their own environmental laws, and environmental scientists monitor the resultant decline and extinction of flora and fauna.

In the case of the Glen Iris Golf Course with its profusion of wildlife, there is hope, and included herein is a copy of a letter (below) to the Premier, of WA Mr. Mark McGowan dated 14 July 2022.

#### 16.4. “URBAN INFILL vs A WORLD CLASS DEVELOPMENT?”

Eastcourt Property Group have purchased the 54.9ha Glen Iris Golf Course in Jandakot with its abundant wildlife, 1,250 mature trees and much needed anti-climate change open space to be replaced by concrete urban infill, increased pollution, more traffic, more stress attributing to mental health and wellbeing, and reduced quality of life.

In addition, a proven sanctuary for many protected species that has existed since 1965, will be lost.

The attached report outlines the abundant protected Priority 4 quenda and federally protected Carnaby’s and Forest Red-tailed black cockatoos (nearing extinction) which are currently being destroyed with the loss of open space on the golf course.

A wave park has been approved for development adjacent to the Glen Iris Golf Course, together with a hotel and potential conference centre. The City of Cockburn plans to double the size of the Gateway shopping centre and approve new high-rise apartments, all integrated with the new road system and freeway access plus a train station and nearby airport, to Rottnest and the south. Why not go further and integrate 54.9ha of golf course into the plan instead of Council developing a new 9-hole golf course at Coogee, currently costed at \$27m to be funded by ratepayers, with delivery in 2027/28 with greatly increased costs?

The Optus Stadium construction incorporated half of the Casino golf course and the Casino management investigated purchase of the (then) viable Glen Iris Golf Course. The previous



owner refused to sell. This new Cockburn Hub could be included with the new wave park development which could even attract day trips from Fremantle cruise passengers, especially as Fremantle has been named in TIME Magazine's 50 "Greatest Places of 2022." <Natalie Richards, Perth Now - 13 July 2022>

A brand new "integrated destination for the gold coast" - A Development Application has been lodged by Parkwood Golf Course for a large \$300 million integrated Surf Park Village located at the Pinewood International Golf Course on the Gold Coast.  
<<https://golfandsurfgc.com.au>>

It includes:

- An 18-hole golf course
- 'Endless Wave Pool' and beach activities
- Five storey mixed use commercial and retail building 'Surf HQ'
- Short term accommodation over eight buildings and 12 Surf & Golf villas
- Three storey mixed use medical centre
- New events centre and wedding chappel
- Large Event Lawn and Event Plaza
- Food and beverage, including micro-brewery
- Childcare centre
- Bowling alley

The Jandakot 18-hole public golf course can be incorporated into a new night-time golf with computer reality golf and attractions. The already zoned R40 area with existing proven restaurant can also be incorporated into the scheme together with increased amenities. On the basis that the land is not rezoned, I am aware of two potential buyers for the Glen Iris Golf Course and with the financed wave park, a new tourist attraction can be developed at no cost to the government.

The "doggy" area at the junction of Dean Road and Berrigan Drive (Yarra Vista Park) can also be developed into attractive facilities to blend with the new "Cockburn Hub".

In addition to the vastly improved commercial area, the mature-age encompassing recreational activities are becoming so important in the urban sprawl as Perth, Cockburn and Western Australia develop, plus there is tourism potential.

The extensive commercial and proposed recreational activities will bring far greater income to the City of Cockburn and ratepayers than all the problems associated with a +/- 600 new home development which complements climate change and needs so much water on the

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rates for all the new street saplings: thousands, as quoted in Eastcourt brochure, yet in July 2021 they say “70% reduction in groundwater use.”

Since 1965 the (then) Lakes, now Glen Iris Golf Course, has been a proven animal sanctuary for protected species.

The attached report documents the survival of some of our fast-disappearing species (the federally protected cockatoos will be extinct by 2030). Glen Iris, with its protective wall (pages 14, 15), has created a sanctuary for many years to protect our Australian protected fauna. The report outlines what we had before the lakes were emptied and the flocks (100+) of protected cockatoos flew away. They can return by refilling the lakes and retaining the mature roosting sites plus the 2ha of Banksia Woodland food, currently under threat by the developer.

Cockburn can have the first successful animal sanctuary to slow the extinction of our wildlife – if we care! We know it can work and it is more effective than a cockatoo memorial at the Cockburn train station.

“While the number of ratepayers increases, the City needs to build and maintain more high-quality infrastructure and amenities for our residents and find additional resources to achieve this.” <Mayor Howlett, Perth Now 7 July 2022>

A vibrant world-class “Cockburn Hub”, coupled with a workable sanctuary for wildlife, would be more internationally attractive than a +/- 600 suburban “dormitory” to be added to the following urbanisation on our doorstep:

- The continued development of the Calleya estate.
- The development of the Kara estate.
- The development of the Lake Treeby estate.
- The continued expansion of the Verde industrial estate
- The continued expansion of the Jandakot Airport industrial estate.
- The development of the industrial area at Berrigan Drive/Jandakot Road.
- The development of residential units at Berrigan Drive/Lakelands school.
- Bushland turned over to a four-lane road (Jandakot Road) which ends at Warton Road – where a further residential development is underway.
- The quasi freeway of the Armadale Road and the acres of land that has been concreted over as it connects development after development all the way to the hills.
- The continued development of the land around Cockburn Central train station (including the development of the Thornlie line Metronet).
- The expansion of Cockburn shopping centre.



URBAN INFILL OR A WORLD CLASS DEVELOPMENT?

Mike Smith

B.Sc. (University of Wales, Cardiff)

Resident Glen Iris Golf Course Estate, Jandakot

0403 574 815

14 July 2022

Attached report (Appendix A) : Sanctuary for the Protected Fauna (other wildlife) and Protected Flora -on the Glen Iris Golf Course Estate, Jandakot, Western Australia (August 2020 (updated to May 2022))”

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In conclusion, many words have been spoken and many promises have not been kept, plus there are questionable doubts regards the motives of many so-called government departments and environmental groups.

The politicians of course say what is necessary for the moment plus the “experts” are a voice in the wilderness and, of course, the animals cannot speak.

Result? – Shamefully - “Australia is a world leader in the extinction of species.”. Confirmed by the recent report referenced already herein via an article in The Guardian 19 July 2022 by Morton and Readfearn:

“The quenda is a protected species under the Western Australian Wildlife Conservation Act 1950”. “Environmental Protection Authority 2004. “In 2019 upgraded to Priority 4”.

“The quenda has declined significantly since the 1960’s (Kitchener et al 1978)”

“Along with predation by exotic species, habitat loss is among the ultimate threats to quenda populations because an area of native bushland is cleared, most resident animals perish.”  
<Australian State of the Environment Committee 2006, Johnson et al 2007 Caughley and Gunn 1966> – 14 and 24 years ago (sepage 12 in report).

“The number of quenda sightings is already low and has declined further in most consolidated suburbs since 1993” <City of Cockburn webpage – native animals> – yet continue to authorise the clearing of habitat, as evidenced in the list of urbanisations stated above.

**“I’m pretty annoyed to be honest with you. This is one of those things I hold dear. The preservation and conservation of endangered species is one of the things that are core to my belief system.”** <Premier of WA, Mr. Mark McGowan – Channel 9 – 6 o’clock news - 10 July 2020>

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And commentary from our current Prime Minister - "The health crisis in which we are now living has only reiterated the need for community spirit: open green spaces and affordable leisure activities" <Mr. Anthony Albanese on Marrickville Golf Course 16 September 2020>

Etc etc – AND NOW....

"I won't be putting my head in the sand. Under Labor the environment is back on the. Priority list. If we stick with what we're doing now, we'll keep getting the same results." <Minister Tanya Plibersek, ABC News, July 2022>

Politicians seemingly override their own environmental laws for maximised urban development.

Environmental scientists monitor the resultant decline and extinction of fauna and flora.

THE ANIMALS CANNOT SPEAK - DOES ANYONE REALLY CARE?

## 16.5. Joined up Sporting Developments

Our members have contacted JRRA asking why those in power have not looked at the benefits for WA with a sporting corridor comprising the Golf Course and Wave Park.

A wave park complex with an international standard 18-hole public golf course, restaurant and amenities will cost the City of Cockburn zero, and will be a major sporting hub. Along with the Cockburn Arc recreation centre and the Fremantle Dockers training facility at Cockburn, the City will be the envy of the rest of the State.

## 16.6. Planning Inconsistencies

**Why don't WA Planning authorities have legislation in place, to protect golf courses in 'golf course estates'?:**

How can 220 homes directly surround/ adjoin the Glen Iris Golf Course Jandakot WA, of which Property Owners have vested their life savings to purchase into a premium 'golf course estate', to then find that WA legislation does not protect them regarding the golf course land remaining in place in perpetuity?

How can WA golf course owners in golf course estates be allowed to simply sell to the highest bidder, rather than as a going concern? Isn't this called double dipping when they have already received premium prices for the sale of the initial blocks of land in a Golf Course Estate, such as ours in Glen Iris Jandakot?

How can residents in good faith have been forced to build their homes with six metre setbacks (changing the way they constructed their homes, i.e., more glass at the back of their homes) from the Glen Iris Golf Course boundary to then find that the golf course is no longer going to be there if the WA Planning Minister decides such? Currently proposed to be infilled with circa 550-600 houses, some of which are only on 300m<sup>2</sup> average lot sizes.

When will Council run public golf courses be told by the Government the open space now being utilized is more beneficial as housing? When will governments make local council or

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more so ensure the WAPC rezones public open space to residential areas like with the City of Melville? Furthermore, if the zoning approval gets the green light current Glen Iris Golf Course Estate residents will be overlooking satellite dishes, fencing, metal roof tops, solar panels, streetlights etc, rather than 54.9ha of lush open green space golf course land, of which is currently habitat to the Federally Protected Carnaby's and Forest Red-tailed black cockatoos, as well as Priority 4 Quenda and other wildlife/ birdlife.

220 residences that adjoin the currently closed 18-hole championship Glen Iris Golf Course boundary, were expected since the mid 90's to abide by restrictive covenants (registered on our Certificate of Titles), of which benefited the enjoyment of the Glen Iris Golf Course owners land. I.e., homes abutting the course could not having washing lines on display to the golfers, had to maintain non opaque fencing, couldn't have sheds visible from the golf course land etc

How is this at all fair City of Cockburn, WA Planning Commission, Ms Rita Saffiotti (WA Planning Minister)?

As per Government of WA legislation guidelines re 'Making good planning decisions' in considering an application for development approval the local government is to have due regard to the following matters to the extent that, in the opinion of the local government, those matters are relevant to the development the subject of the application — the amenity of the locality including the following:

- environmental impacts of the development;
- the character of the locality;
- social impacts of the development;

The above are all important points that relate to the plight of current Glen Iris Golf Course Estate residents and golfers in the City of Cockburn and wider community. The above should also be considerations for 'all authorities' to seriously consider, as all points are certainly applicable.

JRRA hopes that the CoC as required above places great emphasis on actually documenting those matters listed that are relevant to the proposed development. Sadly to date it has not done so – alarmingly a comprehensive statement/summary of Community views/expectations was absent from the CoC recommendation to its Council on 9/12/21 at the OCM.

OTHER STATE PLANNING GUIDELINES FOR DEVELOPMENT OF GOLF COURSES: Why is it that other states of Australia have planning guidelines in place that have to be considered if any golf courses are proposed to be infilled in their state? WA should embark upon setting up the framework for those development guidelines to be reviewed and appropriate ones established for this State.

<https://www.planning.vic.gov.au/policy-and-strategy/guidelines-for-golf-course-redevelopment>

i.e. excerpt: The following questions should be addressed in establishing whether golf course land should be converted to another use:

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- Does the land tenure allow for conversion?
- Is the golf course surplus to golfing?
- Are there economic alternatives to conversion?
- Should the land remain as open space?

We understand that the Glen Iris Golf Course land is privately owned, however it was a public golf course for the enjoyment of the Western Australian community for many decades. The New Owners purchased it full well knowing that it was zoned SU1 meaning: *Golf Course Estate, Private Recreation, Hotel, Convention Centre and associated uses - means land used and designed for a golf course, integrated with residential development and associated commercial and community facilities.* (Underlining emphasis).

Without approval to change from 'SU1' to development, the Property Owner will have to work within what they purchased 'SU1' zoned land. The Proponent has the ability to do so.

The WA State Government should have policies in place re golf courses and in particular strong legislation protecting golf courses in 'golf course estates'.

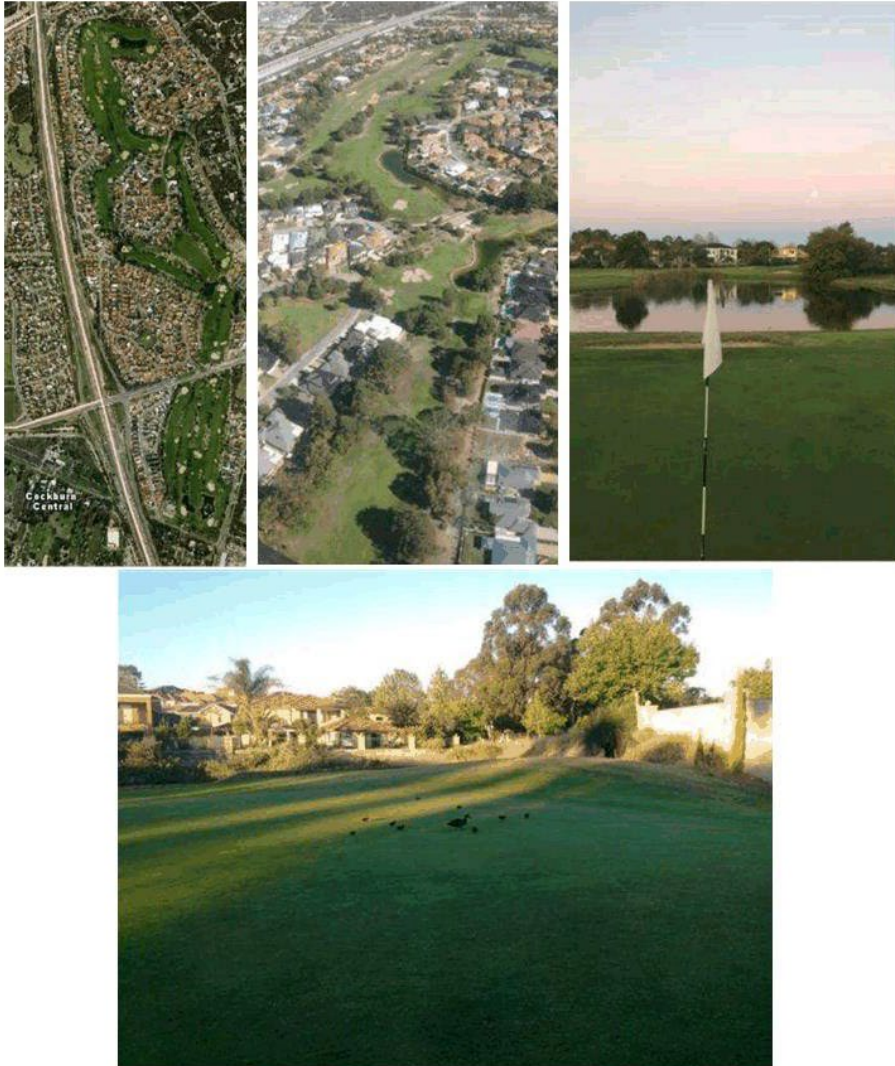
Legislation that binds that the course will be in place for perpetuity, if sold as integrated, i.e., a golf course amenity adjoining/ integrated with residential development.

JRRA represents over 600 community members, these are largely people that purchased in the Glen Iris Golf Course Estate in good faith due to the presence of the Glen Iris Golf Course, some are the golfing community that live in the current estate, bottom line the golf course amenity was integral to the residents' decisions to purchase into the 'golf course estate' subdivision. Many would not have purchased here without the key amenity feature.

Taking away the very essence of why they purchased into the golf course estate will ruin the 'character of the area', particularly with the planned loss of 750+ mature trees and seven picturesque golf course lakes (five of which have already been drained by the current Property Owner). It must be remembered that with the currently closed golf course, the Property Owner also closed the successful restaurant and bar facility of which was the community's social hub. Also closed was the putting green and driving range facility etc.

What has Eastcourt Property Group offered in return – next to nothing, certainly massive net loss.

Photos below are demonstrative of how the Glen Iris Golf Course is 'integrated with residential housing' (773 homes total) - homes of which are not just neatly tucked up in one corner.



Further problem of golf courses being infilled in Western Australia/ Golf Courses are a benefit to the general community.:

It is not just Western Australian 'golf course estates' that are under siege by Property developers.

This April'20 news article makes mention of some of the WA golf courses in the last 20 years that have been infilled by residential, commercial, industrial and even sporting stadiums etc



Golf is a sport that can be played at almost any age. It is good for overall physical and mental well-being. How many other sports can you still play in your eighties? Remembering that golf is great for social interaction, both on the course and on the 19<sup>th</sup> hole.

**The 54.9 ha golf course produces enough oxygen annually for 135,000 people. (University of Maryland, Environmental Science and Technology, October 2018, Golf benefits to the environment).**

The Glen Iris Golf Course has 1258 mature trees, of which Eastcourt are planning to cull 750+ (roughly 60%.)

- How will this help Climate Change?
- How will this help the heat island effect?
- How will this help City of Cockburn's urban forest canopy?
- Importantly how will this help to mitigate the noise from the overhead light aircraft and helicopters from the nearby located Jandakot Airport inclusive of the noisy overhead circuit training?
- How will the removal of 750+ mature trees help the wildlife and birdlife, including the threatened species that inhabit the golf course?

The Banksia woodland of which the Glen Iris Golf Course has around 2ha of 'quality' banksia, is an ecological community that provides ecosystem services and contributes to the health and wellbeing of local residents. For example, the woodlands help cool temperatures in the surrounding region; store carbon; filter and maintain aquifers, including those supplying drinking water for Perth; mitigate local flooding, soil loss, and pollution; and provide amenity. (Excerpt from Australian Govt - Banksia Woodlands of the Swan Coastal Plain: a nationally



protected ecological community). Importantly the Glen Iris Golf Course is located on the Jandakot Water Mound.

### 16.7. Jandakot Resident and Ratepayers Association (Inc) 'independent' Survey vs Eastcourt Property Group's 'pre-determined' Survey.

Towards the last quarter of 2020 Jandakot Residents and Ratepayers Association (Inc) ("JRRA") commissioned an 'independent' Survey from its members.

The Survey questions asked did not have 'pre-determined' responses - unlike Eastcourt Property Group's earlier survey which only geared questions towards redevelopment of the Glen Iris Golf Course.

For example, there were no options in Eastcourt's survey that allowed for current residents to say they did not want residential infill of the 54.9ha green-lung golf course, and instead wanted the golf course retained and revitalised.

In true clandestine fashion, on the back of each envelope containing the Survey from Eastcourt was an individual number (in very small print), done to identify where the resident lived as the person delivering the Survey also took note of the person's address. Glen Iris residents were under the (mistaken) belief that this was an anonymous Survey.

Similarly, the back of the actual Survey had the same identifying number as the envelope, which then clearly identified the homeowner's address and provided crucial demographic and other important information for Eastcourt's future planning purposes.



It was also of concern that these Surveys had a return address to the Property Developer's Project Manager, rather than to an independent survey company, meaning they could well bin any Surveys that did not include the result they were seeking.

Many questions asked by Eastcourt appeared irrelevant i.e.:



D4 Which of the following best applies to you?

- Own my home (no mortgage)
- Paying off my home (have a mortgage)
- Renting
- Other (e.g. live with parents/ boarding)
- Prefer not to answer

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- Other (e.g. live with parents/ boarding)
- Prefer not to answer

In contrast JRRA’s Independent Survey results were refreshingly ‘independently’ analysed and at “arms-length” from any JRRA influence and were drawn from a significantly higher return rate of 40.8% of people surveyed when compared to the previous Survey conducted on behalf of Eastcourt that only received 25.4%.

The results overwhelmingly confirmed that there is a HUGE opposition to the proposed residential infill development of the Glen Iris Golf Course and its fragile ecosystem. See Appendix J herein.

### 16.8. What’s Changed?

Good Golf Course management, coupled with good management of the existing Clubhouse Restaurant and Bar social amenity, will see a thriving golf course exist in the Community alongside a thriving environment of flora and fauna and a clubhouse that is a magnet for the ever-increasing number of people within the community who choose to call this part of our City of Cockburn/WA State home.

*“While the number of ratepayers increases, the City needs to build and maintain more high-quality infrastructure and amenities for our residents and find additional resources to achieve this.” <Mayor Howlett, Perth Now 7 July 2022>*



It is clear from the requisite requirements to be considered by the CoC that if some or part of City of Cockburn management do not support the original decisions around the Scheme No 2. Amendment No.64, amended 18 December 1991 and adopted 5 November 1991 that they/it must be transparently communicated to the community. JRRA is of the view that it is all of the ratepayers' democratic right as Glen Iris Golf Course Estate residents who have vested their life savings, as well as the wider City of Cockburn community that utilised/ patronised the golf course amenity to be made aware of their reasons, which to date (despite having been sought) have not been provided by the CoC.

Email 21 March 2021 addressed to the Mayor and Elected members, is as follows, and is still valid today as most of the points have not been addressed.

At the City of Cockburn Ordinary Council's meeting on the 11 March 2021, and a public question posed by a Glen Iris Golf Course Estate community member, **in relation to the Council standing by the original City of Cockburn District Zoning Scheme No 2. Amendment No.64, amended 18 December 1991 and adopted 5 November 1991.**

The response from Daniel Arndt was that

*"The City of Cockburn may not honour their original decision rezoning decisions (adopted 5 November 1991) as the circumstances around that decision have changed since circa 1991"*

Emphasis is on that "the circumstances have changed".

Eastcourt have failed entirely within its proposal to demonstrate in any way, shape or form that circumstances have changed. JRRA on the other hand as a minimum herein have demonstrated:

- the Community outrage,
- the environmental destruction that will result and
- has clearly demonstrated (by publicly available facts) that golf is viable
- the viability of well maintained and managed golf courses

JRRA's questions to the City of Cockburn are as follows:

1. Do these 'changed' circumstances and/or criteria relating to the original evaluation of the zoning cover any of the following and, if so, what is the reason for the change?
2. Any of the adopted criteria within the Structure Plan as adopted by City of Cockburn 5 November 1991?
3. The amenity – Environment, environmental impacts, the character of the locality; the social impacts of the development?
4. Natural environment and water resources?
5. Trees or other vegetation on the land?

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6. The proposed means of access to and egress from the site?
7. The volume of traffic?
8. The impact of the development on the community as a whole?
9. An integrated residential golf course estate?

If none of the above circumstances and/or criteria has changed, what has changed since 5 November 1991?

Since 1991 a number of new criteria have been introduced locally, nationally and globally that appears to evidently support the retention of the 'current' scheme approval including:

10. Light pollution impact from new developments (houses and vehicles) contributing to obesity, reduced sleep quality and impaired memory.
11. Heat island effect impact from building materials, loss of trees and fauna, contributing to heat-related deaths and heat-related illnesses such as general discomfort, respiratory difficulties, heat cramps, heat exhaustion
12. Protection of wildlife and fauna (particularly the endangered Carnaby's black cockatoos, vulnerable Red-tailed black cockatoos & Priority 4 Quenda.)
13. Increased traffic, vehicle, truck, train & (Jandakot) airport pollution/noise. Glen Iris Golf Course Estate is surrounded in all directions by ever increasing trains, planes and automobiles, due to an increased Perth population.
14. Mental Health impacts caused by the loss of amenity, increased traffic, death and destruction of wildlife.
15. Loss of long-established amenity – trees, fauna, open spaces.
16. Higher pressures on the Jandakot Groundwater Mound due to further residential infill and a drying Perth climate.
17. Further reduction in green open space in Jandakot, that affects the local and wider community.
18. Environmental pressures (recognised by COC Climate Change strategy 2020-2030)
19. Environmental pressures (recognised by WALGA in a policy statement endorsed by State council both 2009 & 2018), inclusive of this statement on the WALGA website:

**Local Government acknowledges:**

- I. *The science is clear: climate change is occurring and greenhouse gas emissions from human activities are the dominant cause.*
- II. *Climate change threatens human societies and the Earth's ecosystems.*
- III. *Urgent action is required to reduce emissions, and to adapt to the impacts from climate change that are now unavoidable.*
- IV. *A failure to adequately address this climate change emergency places an unacceptable burden on future generations.*

20. The State Govt of WA have now recognised the threat of a changing Perth Climate by implementing a new minister's position of 'Climate Action' (announced 18.3.21)



21. An ever-growing City of Cockburn. Historical 1991 population figures reported as 50,217, fast track to 2021 estimated at over 120k. It is estimated that the City's population will grow even further to over 150k of residents by 2028. All the more reason to retain an important green space ecological corridor, so that the City of Cockburn residents don't become like parts of the eastern states squabbling over golf course land/ open green space. COVID-19, which is making the world sit up and listen intently to our broken relationship with nature (of which the 54.9ha Glen Iris Golf Course embodies the absolute positive aspects of nature/the environment.)
22. Increased non-tailpipe vehicle emissions to Glen Iris Golf Course Estate affecting residents' health.
23. The JRRRA(Inc) undertook a survey (in 2020) of how many residents wanted the golf course to change to housing – a MASSIVE 98% of people said NO! Do you hear your ratepayers or are we just numbers on your ratepayer notices?
24. City of Cockburn virgin bushland is being decimated at an almighty frightening rate – apparently seemingly soon to become a concrete/steel jungle? The City has experienced rapid clearing of vegetation due to the massive recent urban development, i.e. Treeby, Calleya, and Kara estates etc. The eastern corridor being built out rapidly. More high-rise in Cockburn is planned.
25. In a recent national survey on the state of vegetation cover in metropolitan Australia the City of Cockburn had a large difference in green cover over the past seven years, witnessed as a 17% decrease since 2013 and the City's grassed area has dropped by 6.65% since 2016.
26. The City of Cockburn has developed a comprehensive Urban Forrest Plan which aims to create a healthy, diverse and thriving forest that contributes to the health and wellbeing of the Cockburn community:
  - a) The plan identifies six strategic objectives and targets, accompanied by 30 actions. Healthy and well-managed urban forests (i.e. what is already established over the years with mature trees and fragile ecosystem at the Glen Iris Golf Course Estate, some of which include nesting/roosting for the Carnaby's black cockatoos etc) have been shown to provide a wide range of social, economic, and environmental benefits to urban communities including:
    - i) Improving the health and wellbeing of residents
    - ii) Enhancing biodiversity and providing ecological corridors (which is an extremely important point re the threatened black cockatoos)
    - iii) Lowering maximum summer temperatures in urban areas
    - iv) Reducing household energy costs (up to 8%) and
    - v) Increasing amenity and property values (up to \$17,000)
    - vi) Reducing urban-heat-island-effect
18. The City of Cockburn has commissioned new artwork for the Cockburn train station – a beautiful Carnaby cockatoo has been chosen. Yet it is anticipated that these black cockatoos will be extinct



- within the Perth metro area in the foreseeable future. Residents of the Glen Iris Golf Course Estate would like to ask how can the Council highlight a bird that lives within its City boundaries yet allow for the absolute destruction of the same bird's 65 year-old (or more) habitat?
19. Greg Norman's company is the consultant for the City of Cockburn and says if the 9 hole Par 3 course in Coogee is not built (at a current estimated cost of \$28m) within the next 3 – 4 years then it will become too environmentally sensitive and it will not be built on. There is already a conveniently placed golf course currently closed at Glen Iris Golf Course Estate. Glen Iris golf Course Estate residents say knock back zoning changes, instead with foresight Council purchase the land/ amenity and reopen it as a profitable enterprise/amenity and into the world class golf course that it once was!
  20. The City of Cockburn also says in its Sports Report of the Western Suburbs - Golf is second to running/jogging as the major sport undertaken by its residents. Yet it tragically has no open public golf course for its 120k of residents? Why not we ask?
  21. If the City of Cockburn is a so-called 'friend of the environment' as it purports to be on its website and Cockburn Soundings, it should be aware that it is underperforming drastically in its protection of both the Glen Iris Golf Course Estate protected and endangered flora and fauna, as well as the residents (who purchased homes at a premium prices immediately surrounding or near the golf course) and furthermore City of Cockburn are severely letting down the general environment as well as the wider community, if the change to zoning is given the green light. Per your original approvals in the mid 1990's the homes in Glen Iris were purposely integrated around the golf course, to maximise the golf course views.
  22. A study compiled by the (University of Maryland, Environmental Science and Technology, October 2018, titled "Golf benefits to the environment" highlighted that "the average 18-hole golf course (80 acres or 32ha) produces enough oxygen annually to sustain 85,000 people. Turf grass and vegetation also have the effect of reducing the heat of an area – CRITICAL IN URBAN ENVIRONMENTS." Our Glen Iris Golf Course is approx. 55ha - extrapolated, this can annually produce enough oxygen for over 135,000 people annually. If the golf course land is rezoned how is the Council going to replace the lost oxygen?
  23. There is a lack of large restaurant/social amenities in the City of Cockburn, the nearby Gate and former CYO O'Connor are/were often full to the brim and overflowing. The Community have lost its much-treasured restaurant and bar at Glen Iris for no realistic reason.

Therefore, JRRRA implores the CoCto take these (and other relevant points set out herein) into their evaluation of any zoning application put forward by Eastcourt Property Group.

The Community (whom the CoC work in the collective interests of not just the profit interest of one landowner) simply cannot let the fragile flora and fauna eco-system that has evolved and thrived for many decades within the golf course land to be decimated and lost forever. Protected species MUST be protected within the course protective boundary walls! The



Community MUST speak up for their protection as they cannot speak up for themselves. Australia is a world leader in the extinction of endangered animals. Certainly not something to be proud of.

The City of Cockburn/WA planning authorities need to intelligently preserve this important ecological corridor, not just for this generation, but for the future generations!!!

The City of Cockburn Council must be held accountable to stand firm on their original zoning decision that attracted the residents to live on and other Community members Statewide to enjoy this lovely part of this great State. The reasons why the land is zoned SU1 and the golf course has been there for 65 years have not changed - the only thing that has changed is that we now face the greed of one profit focused developer and its environmentally non-focused Project Manager who bought land with its current zoning and have collectively failed in their duty to provide any reason to change the current zoning.

JRRA on the other hand have, on behalf of the Community (residents and Statewide) provided significant reasons to demonstrate that nothing has changed since the original Council zoning decision.

There are no compelling or justifiable reasons put forward by the Proponent for the current zoning to change. The new land owner should work within the confines of what already existed when it bought the land.

**The results overwhelmingly confirmed that there is a HUGE opposition to the proposed residential infill development of the Glen Iris Golf Course and its fragile ecosystem.**



Appendix A - Sanctuary for  
the protected fauna (other  
wildlife) and protected flora  
-  
May 2022)



# Appendix B - Quality of Life

## May 2022



# Appendix C – Brake Dust Report May 2022



# Appendix D - Mental Health Survey July'21





# Appendix E - GI Country Club Estate - Prodev Brochure



# Appendix F - GI Golf Course Estate - Satterley Brochure



# Appendix G - Putter's Green advertising





# Appendix H - Turnbury Green Advertising –



# Appendix I - Traffic lights Issue



# Appendix J - Glen Iris Golf Course Estate Survey Results Synopsis 2020

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**SUBMISSION  
LOCAL PLANNING STRATEGY  
CITY OF COCKBURN**





**SUBMISSION  
LOCAL PLANNING STRATEGY  
CITY OF COCKBURN**

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**SUBMISSION  
LOCAL PLANNING STRATEGY  
CITY OF COCKBURN**

**By urbanplan**

On behalf of Qube Property Group

Approved by: .....

Signed / Date : .....

Position: .....

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# 1. INTRODUCTION

## 1.1 PREFACE

Qube Property Group has assembled freehold properties consequent to liaison with the Lyon and Kinley Owners' Collaborative in mutual pursuit of residential development for approximately 55.7 hectares of land south of Aubin Grove, north of Rowley Road, east of Lyon Road and properties fronting Kinley Road. The lots are listed over leaf and shown at Figure 1.

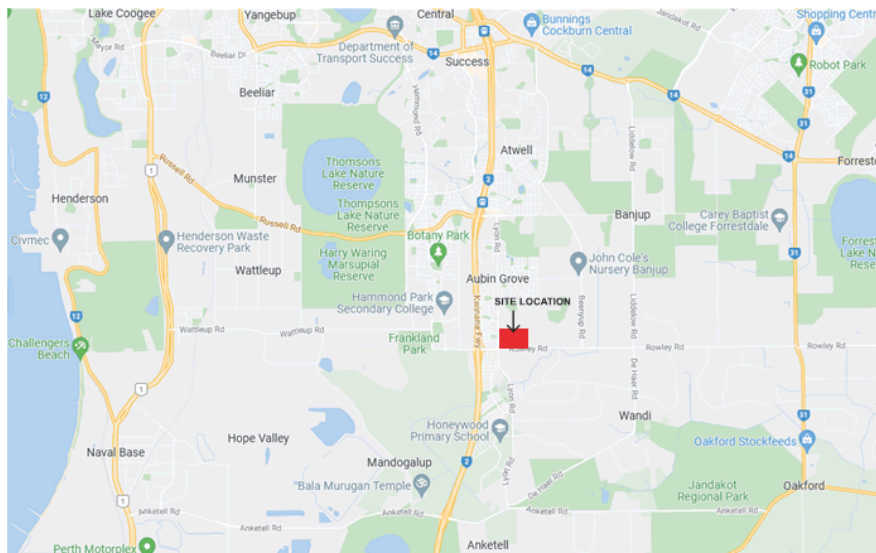
### 1.1.1 PURPOSE

The purpose of this submission is to critically examine the content of the City of Cockburn's Local Planning Strategy particularly in relation to the issues around the Rural Water Protection Zone and proposed Planning Directions and Actions in the Strategy as they relate to the subject land at Lyon and Kinley.

### 1.1.2 PREMISE

Concurrent with this critical review and submission on the Local Planning Strategy (Strategy) **urbanplan** on behalf of Qube Property Group, is in pursuit of a request to amended to the Metropolitan Region Scheme from Rural - Water Protection to Urban. This amendment request substantiates that integrated water resource and land use planning management can be achieved for the subject land while protecting the environmental attributes of that land. This request is substantiated by technical research, analysis and synthesis to support development of consolidated landholdings extending over approximately 55.7 hectares in Banjup.

Figure 1:



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Lot Lyon Road	(House No.) Lyon	Vol/Fol	Lot Kinley Road	Vol/Fol
			29	1746/889
192	(315)	2510/853	28	2210/838
191	QUBE Lyon Road Pty Ltd	2510/852	27	1903/822
100	(333) R Holm & M Georgeson (option to QUBE Lyon Road Pty Ltd)	2501/551	26	2083/280
101	(339) D & C Bushell (Option to QUBE Lyon Road Pty Ltd)	2501/552	25	1315/982
21	(343) QVM Enterprises Pty Ltd	1364/135	24	2116/626
22	(359) P & C Gill (Option to QUBE Lyon Road Pty Ltd)	1364/134	30	18/292A
23	Skycross Pty Ltd	1666/577	300	2779/76
			301	2779/77

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## 2. SUBREGIONAL STRUCTURE PLANS

*This section exemplifies the Lyon and Kinley subject land has been identified for urban development in preceding planning studies.*

### 2.1 BACKGROUND

During the 1990s, the subject land was identified for potential urban development in preceding planning studies. The following evidence has been taken extracted from material resources in the State Library archive.

#### 2.1.1 THOMSONS LAKE URBAN STRUCTURE STUDY AND SOUTH JANDAKOT DEVELOPMENT WATER RESOURCES MANAGEMENT PLAN

The Thomsons Lake Urban Structure Plan Study and South Jandakot Development Water Resources Management Plan 1989, at Annexure A, did not include the Lyon and Kinley subject land within its remit. However, Figure 1 of that study defines future urban areas coincident with the series of wellheads beneath the proposed urban development; these were later developed by the Western Australian Land Authority as Atwell and Aubin Grove. This demonstrates government agencies were mindful of the coexistence of urban development with the groundwater mound.

#### 2.1.2 URBAN EXPANSION POLICY STATEMENT

The Urban Expansion Policy Statement 1990 has the subject land included in Proposals 36 and 37. Refer Annexure B noting the land is classified as Category B. As a result of a series of constraints the Policy document states it is unlikely many Category B could be considered as potential urban land in the short term. The Policy goes onto to state in the event of unforeseen delays in developing some Category A areas, or if constraints to development removed, some of category B areas could become available for development earlier.

Given it has been proven that urban residential and groundwater resources can coexist, as constraints are resolved, the Lyon and Kinley subject land, as identified for urban uses can be developed.

#### 2.1.3 JANDAKOT LANDUSE AND WATER MANAGEMENT STRATEGY

Figure 4 of the Jandakot Land Use and Water Management Strategy (October 1992) at Annexure C, clearly shows the Lyon and Kinley subject land identified for potential urban development.

### 2.2 CONCLUSION

Clearly, as reported in Council Agenda 9 July 2015, the subject land was identified for potential urban development in preceding planning studies, more over included in the then Commission's Policy Framework as evidenced by the State Library sources.

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### 3. CITY RESOLUTION 2015

*This section addresses Council's Resolution of 9 July 2015 that demonstrates the rationale to support urban development on the subject site of Lyon and Kinley.*

#### 3.1 AGENDA ITEM CONTEXT

The following is an excerpt from the City of Cockburn Ordinary Council Meeting Agenda 9 July 2015 providing a positive recommendation to the Planning Commission to include Lyon and Kinley subject land in the Perth and Peel @ 3.5 Million framework. It demonstrates the rationale to support urban development on the subject site of Lyon and Kinley. As made by the City Council.

“There is an area shown in previous planning studies (bound by Kwinana Freeway, Rowley Road and properties on the eastern side of Kinley Rd) that has not been identified in this document (Perth and Peel @3.5). This seems to have been an oversight in leaving this area out of the urban expansion area.

The background to this area is that the land was identified as Future Urban within the Urban Expansion Policy Statement (1990) (proposals 36 and 37). This was a (then) Department of Planning and Urban Development document. Following this, DPUD released the Draft Jandakot Landuse and Water Management Strategy (1992) that identified the land as Urban (existing or proposed). However, the final (1995) version of the JLUWMS omitted the land, replacing it instead with the following text:

*The following land may have the potential for urban development in the future if the development constraints can be overcome:*

*(i) Land located on the north side of Rowley Road abutting both sides of Kinley Road and extending west to Lyon Road, Banjup*

*Possible urban development of this land should be assessed in light of the findings of the Select Committee on Development over Groundwater area in the Metropolitan Region and an assessment of environmental and drainage management issues.*

Referring back to the Urban Expansion Policy Statement (1990), proposals 36 and 37 were subject to district planning by virtue of the South Jandakot/Mandogalup District Planning Strategy (1993). The subject land was included in the district structure plan, with the following notes:

*The Structure Plan report states, “There is nothing in the physical nature of the land or environmental constraints which differentiates it from the future urban land to the north or west. However, the land has been excluded from Amendment No. 938/33 because it did not form part of the environmental approval to the Thomsons Lake Urban Structure Plan.*

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Coming back to the Select Committee reference in the JLUWMS, within the MRS documentation report on hearings which introduced the Rural Water Protection zone, the following information was stated:

*Recommendation 4.8 of the Select Committee Report indicated that where areas have been classified as Priority 2, which contain urban development and commercial zones or have been zoned for such uses, consideration should be given to changing the Priority 2 classification to Priority 3, so as to remove any inconsistency with the objectives of Priority 2. The areas zoned 'Urban' or 'Urban Deferred' in the Metropolitan Region Scheme should not be included in the RGPCZ and should be classified for Priority 3 source protection area. This will eliminate providing a precedent for further inappropriate development in Priority 2 areas. Some level of groundwater quality protection will be maintained in this area by their being retained within the Jandakot UWPCA.*

Accordingly, as this land was not zoned Urban deferred already (not part of MRS Amendment 938/33 due to not being part of environmental approvals for the Thomsons Lake Urban Structure Plan) it was retained within P2 and had its Rural zoning under the MRS shifted to the Rural Water Protection zoning.

This is important to recognise as the definition contained within the draft documents identifies urban expansion as "land that has been identified for potential urban development in preceding planning studies or represents the logical expansion of an existing urban area."

Given that this precinct of land was indeed identified in previous planning studies and represent arguably a more logical expansion of an existing urban area in comparison to the Banjup proposals, it should be included within the draft (Perth and Peel @3.5 Million) documents for urban expansion.

The area shown in previous planning studies (bounded by Kwinana Freeway, Rowley Road and properties on the eastern side of Kinley Rd) has not been identified in this document for urban expansion. This appears to have been an oversight in leaving this area out of the urban expansion area, given the definition used for urban expansion in the (Commission's strategic) document."

### 3.2 COUNCIL RESOLUTION

The Council Resolved of the July 2015 Agenda for the Aubin Grove item, pertaining to the Lyon and Kinley subject land, was to accept the submission to the Planning Commission to include that land as future urban in the review of the Perth and Peel @ 3.5 Million:

This was later reinforced by a City Officer representation to the committee presiding over the review of the Perth and Peel @ 3.5 Million.

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#### 4. PRE-LODGE MENT EARLY ENGAGEMENT 2019

*This section considers the Planning Commissions' invitation to **urbanplan** in May 2019 to present Lyon and Kinley proposal as a matter for Pre-lodgement Early Engagement consideration.*

##### 4.1 BACKGROUND

The City of Cockburn will be aware that **urbanplan** was invited by the Planning Commission, on behalf of the Owners' Collaborative, to present the Lyon and Kinley proposal as a matter for Pre-lodgement Early Engagement consideration.

At that time **urbanplan** presented the history, context and site and suitability characteristics of the land to the Commissioners. It became evident that the Department of Water and Environmental Regulation was unwilling to change its stance on the Jandakot Groundwater Protection Area and any determination on the matter was a 'planning led decision' the responsibility of the Commission.

As no application for a request to amend the MRS was before the Commission, the matter did not proceed.

##### 4.2 RESPONSE

Despite the matter not progressing through the Pre-lodgement Early Engagement process, the Departmental response was quite definitive:

The Western Australian Planning Commission is generally guided by the Perth and Peel @ 3.5 Million suite of strategic documents in consideration of MRS amendment requests.

Therefore, .... It is unlikely that the Department Planning Lands and Heritage would recommend to the WAPC that a potential MRS amendment request be initiated. However, you are encouraged to await the review of the Perth and Peel @ 3.5 Million / South Metropolitan Peel Sub-regional Framework for further strategic planning guidance.

Please note that the information in this letter is advice only, does not constitute a formal decision of the WAPC, and does not fetter any future decisions by the WAPC or the Minister for Planning. Anyone is free to lodge an amendment request with the WAPC, supported by sufficient planning justification, for a decision on whether an amendment request should be initiated.

The strategic framework review never occurred; potentially because it was not a platform of the current State Government.

Moreover, the correspondence is encouraging as it mentions 'general guide' 'issues that require further consideration' and significantly the underlined comment strongly states that:

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- does not constitute a formal decision
- does not fetter future decisions by the commission
- does not fetter future decisions by the minster
- an amendment request is able to be lodged.

At this juncture, Qube Property Group, in association with the Owners' Collaborative, decided to advance an approach in pursuit of a request to amend the MRS.

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## 5. STRATEGIC PLANNING POLICY

*This section considers the implications of State strategic planning policy as applied to the Lyon and Kinley subject land. South Metropolitan Sub-regional Planning Framework*

### 5.1.1 DEFINITION OF URBAN EXPANSION

The definition contained within the draft South Metropolitan Sub-Regional Planning Framework identifies urban expansion as:

land that has been identified for potential urban development in preceding planning studies or represents the logical expansion of an existing urban area.

It is argued the subject land meets both factors of that definition for urban expansion.

### 5.1.2 KEY PRINCIPLES

The South Metropolitan Peel Sub-regional Planning Framework for Perth and Peel @ 3.5 Million identifies a number of key planning framework principles that are fundamental to delivery urban form in the environmental context. These principles culminate to avoid, protect and mitigate the sub-region's environmental attributes that are protected under Commonwealth and State environmental legislation. The relevant key principles are:

- Avoid, protect and mitigate environmental attributes
- Develop a consolidated urban form
- Provide effective and sustainable management of water resources
- Avoid areas that are at a high risk of bushfire to manage the potential impact
- Promote shared infrastructure corridors
- Maximise the use of existing infrastructure including transport
- Increase the number of people living close to where they work
- Integrate land use and public transport

The subject land achieves a consolidated urban form for our city, can be developed to achieve effective and sustainable management of water resources, as shown elsewhere in the South West Corridor, and in doing so:

- rounds-off an existing neighbourhood
- promotes shared infrastructure corridors
- maximise the use of existing infrastructure
- increase the number of people living close to where they work
- integrate land use and public transport; while
- protecting regionally significant environmental assets.

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## 5.2 STATE PLANNING POLICY 2.9 PLANNING FOR WATER

The draft SPP 2.9 has status and will replace a suite of policies and guidance including SPP 2.3, Jandakot Groundwater Protection Area.

Draft State Planning Policy 2.9, Planning for Water, and associated Planning for Water Guidelines (DPLH, 2021a, DPLH, 2021b) provides guidance to decision-makers regarding integrated water resource and land use planning management; how land and water related provisions are implemented. Accordingly, SPP 2.9 and Planning for Water Guidelines outlines how water resource management is integrated into the planning processes.

The SPP 2.9 Guidance provides generic suite of management guidance, the implementation of which is likely to be through pragmatic strategies and actions to protect water supply source.

### 5.2.1 POLICY OUTCOMES

SPP 2.9 applies to all proposals, throughout the planning procedures, prepared and assessed under the *Planning and Development Act 2005*. The essential and achievable policy outcomes significant to the Lyon and Kinley subject land are 6.1, 6.2 and 6.5:

6.1 Planning and development maintains or enhances water quality and hydrological regimes to protect public health and support healthy ecosystems through the:

- i. protection of sensitive water resources;
- ii. protection of existing vegetation and/or restoration of cleared or degraded vegetation, preferably with endemic species;
- iii. appropriate siting and management of land uses; and
- iv. maintenance of natural flows in waterways, groundwater levels and inundation of wetlands to sustain aquatic and terrestrial habitats through the delivery of appropriate stormwater and groundwater management systems.

6.2 Waterways and wetlands have adequate foreshore areas and wetland buffers to protect, manage and conserve water quality and quantity, native vegetation, aquatic and riparian habitats, ecological linkages and associated biodiversity values.

6.4 Planning and development enhances amenity and sense of place associated with water resources, which in turn protects public health and increases resilience of the community.

Section 6.2 and 6.3 of this report respond to these outcomes and demonstrate integrated land use and water management planning can be achieved.

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### 5.2.2 CONSIDERATIONS FOR PROPOSALS AND DECISIONS

It has been proven in similar locations abutting the Jandakot Groundwater Protection Area that planning and water sensitive urban design development can maintain or even enhance hydrological regimes. Research to date for the subject land at Lyon and Kinley (refer section 6 of this report) shows:

- Protection of existing Banksia Woodland and Melaleuca vegetation and restoration of cleared or degraded vegetation can be achieved;
- Appropriate siting and management of residential development including water sensitive urban design will enhance the environmental assets;
- Maintenance of groundwater levels (by stopping rural residential groundwater extraction) and inundation of wetlands to sustain aquatic and terrestrial habitats can be achieved through the delivery of appropriate stormwater management systems; and
- Designing the development to enhance amenity and sense of place through conservation of the wetland and remnant vegetation, which in turn increases resilience of the community.

Planning for Water Guidelines, section 9.4, outlines the considerations or criteria relevant to proposals in drinking water supply areas. It is made evident in sections 6.2 and 6.3 of this report that the criteria in 9.4 of the guidance can be achieved through integrated water resource and land use planning management.

As part of the request to amend the MRS, and in accordance with planning procedures, Water Management Plan(s) will be undertaken to address the requirements of Guidelines and the Water Quality Protection Note No. 25, as reinforced in subsequent planning stages by an Local Water Management Strategy.

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## 6. LOCAL PLANNING STRATEGY CRITIQUE

*City's Local Planning Strategy identifies issues or objectives for the Rural Water Protection Zone as supported by a series of strategies for evaluating future proposals. This section responds to those criteria.*

### 6.1 COMPARISON

As an opening, over 24 years there is little change in the spatial land use composition of the 2022 Strategy when compared to the publication of the City of Cockburn's 1991 Local Planning Strategy. While dwelling yield targets are a measure of development progress in response to the objectives of Perth and Peel @ 3.5 Million, they are not a stand alone measure nor are they a minimum.

### 6.2 IDENTIFIED RURAL WATER PROTECTION ISSUES

The Strategy identifies a series of interrelated issues in the Rural Water Protection Zone. The Strategy notes:

“... (to ensure) land use and development protects the ecological integrity of important wetlands (which are hydraulically connected to groundwater), and also to maintain and increase natural vegetation cover. Tight control and limitation of land use and development is a clear imperative for the Rural Water Protection Zone.

In addition to environmental matters related to the groundwater mound, much of the area has high conservation values, including:

- Conservation Category Wetlands
- Declared rare flora
- Threatened ecological communities
- Bush Forever sites
- Extensive remnant vegetation and identified
- Ecological corridors, including regional linkages
- Bushfire prone areas.”

Consequently, the strategy lists a series of Planning Directions and Actions is listed to implement the strategies. Significant to Lyon and Kinley are items 1.1, 1.2, 2.1, 2.2 and 3.1, each is given a ranking (in brackets):

Develop mechanisms to guide tree and vegetation retention/protection, including ecological corridors while facilitating future development (Low).

Develop measures for ecological corridors, outlining revegetation, restoration and landscaping standards expected of developers, and requirements for other landscape elements to strengthen connections, such as wider verges and median strips to accommodate street trees and low understorey (Low).



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Identify measures to encourage developers to retain natural vegetation within new areas of public open space and development sites where possible, particularly areas that will be developed for passive recreation (Low).

In collaboration with relevant State agencies, ensure habitat of threatened fauna is not impacted by future development proposals (Med).

Undertake an appropriate response to the city wide high-level Strategic Bushfire Assessment by preparing an action and implementation plan (Med).

### 6.3 RESPONSE TO CRITERIA AND ACTIONS

**urbanplan** has developed a Lyon and Kinley Concept Plan derived from the topographical characteristics of the subject site that account for environmental values, remnant vegetation including TECs, protection of the Resource Enhancement Wetland including a 50 metre wetland buffer. The design provides for a topological grid pattern that accommodates water sensitive urban design drainage and linkages to facilities and open space. Approximately 12 hectares, nearly 20 percent of the subject site, is set aside to achieve conservation of these environmental assets; this is a considerable portion of the site to protect the urban bushland consisting of Banksia Woodland and the Resource Enhancement Wetland Melaleuca vegetation.

The proposal constitutes urban infill because it is the logical expansion of the existing Aubin Grove urban area. Development on this site reduces urban sprawl on the outer margins of the Perth and Peel Metropolitan Area.

The Concept Plan has been developed over a number of years inclusive of remnant vegetation and groundwater examinations. Considerations that underpin the planning led response and design rationale of the Concept Plan that respond the Strategy issues include:

#### 6.3.1 PLANNING LED RESPONSE

Protection of Resource Enhancement Wetland 6886, including a 50 metre buffer. The current state of the REW would be considered denuded consequent to past land use and clearing of the subject lot. Guidance assigned to the wetland classification of REW requires that the environmental values and attributes of the wetland are enhanced. Incumbent upon any potential development of the land requires establishment of a wetlands buffer, additional endemic species planting and application of best practice water sensitive urban design to filtrate stormwater before entering the hydrological contour. This will be achieved if the land is reclassified for urban use.

Moreover, reclassification from existing rural residential uses to Urban in the Strategy will stop any further drawdown of the hydrological contour by individual rural residential bores, thereby addressing as far as implied, the Strategy concern of the hydraulically connectivity to groundwater.

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Protection of tracts of better quality remnant Banksia Woodland. In recent years the significance of the Banksia Woodland has increased. This necessitates thorough assessment of the conservation values of that remnant vegetation as part of any potential development of the land. Vegetation condition and environmental values and attributes surveys of the subject land have been undertaken on two occasions, 2015 and 2022. In 2015 the survey classified the vegetation as 'Very Good' and 'Good'. The 2022 survey identify remnant Banksia Woodland classified as 'Good' and 'Degraded Good'. To arrest further mismanagement of indiscriminate rural residential land use and decline of remnant vegetation condition and values, the concept plan accommodates the reasonable quality classified vegetation in larger tracts; this serves to better enhance survival of the vegetation including Threatened Ecological Species. The proposed conservation of remnant vegetation in passive public open space (in addition to active) supports a green network from the south west corner of the subject land to the north east linking to the Triandra Reserve, Buckingham Reserve, Shirley Balla Swamp Reserve in the Jandakot Regional Park.

Accommodating the Guidelines for Planning in Bushfire Prone Areas SPP 3.7. Implementation of bushfire planning can be a counterpoint to conserving remnant vegetation. While the Concept Plan conserves remnant vegetation, prudent design initiatives achieve separation of potential bushfire hazard resultant of the rural residential properties to the east.

### 6.3.2 RECOMMENDATIONS

To address the Strategy issues and objectives, the Planning Directions and Actions should be amended along the following lines:

"...hydrology of the groundwater mound is complex and further studies are required to better understand this"; the Strategy should be amended to support consideration of future development on the grounds that the hydrological criteria can be addressed through Water Management Plan(s) as required by SPP 2.9 Planning for Water and its Guidance.

".... maintain and increase natural vegetation cover"; the Strategy should be amended to support consideration of future development on the grounds that retention of natural vegetation within new areas of passive public open space and development sites where possible, to be in accordance with State environmental guidance.

"....ensure habitat of threatened fauna is not impacted by future development proposals"; the Strategy should be amended to reduce the impact habitat of threatened fauna through the retention of natural vegetation, where possible, within new areas of passive public open space and development sites.

"...the City wide high-level Strategic Bushfire Assessment by preparing an action and implementation plan"; the Strategy should be amended to simply state the

requirement for bushfire prone areas be subject to the Guidelines for Planning in Bushfire Prone Areas all phases of the planning process.

#### 6.4 PLANNING DESIGN RESPONSE

Subsequent to accommodating environmental matters, the detailed design rationale benchmarking the Concept Plan provides for the following:

- In addition to tracts of remnant vegetation, maintains an increase in natural vegetation cover through design inclusion of a prominent boulevard of existing native trees.
- Creation of a high quality and large quantity of equitably distributed passive open space for Banksia Woodland conservation in addition to active open space.
- Being cognisant to the hydrological contours relative to surface water flows (note surface water essentially flows to south east whereas the wellhead is in south west corner) and the location of the wellhead J210, the road pattern has been designed to direct stormwater drainage into water sensitive designed bioretention basins mainly sited to the south and west.
- Provision of wellhead J210 separation at the corner of Lyon and Rowley Roads by conservation of 3 hectares of surrounding remnant Banksia Woodland.
- In accordance with Water Quality Protection Note No. 25, availing the opportunity for reticulated sewerage mains connection points at the junction of Geneva Close and Lyon Road (west) and at Margorelle Parkway (north).
- Creation of an east west road pattern that favours northern solar aspect.
- Facilitating internal pedestrian access to the Aubin Grove Primary School thereby avoiding use of Lyon Road.
- Promoting internal access to Lyon Road that intersects with existing road junctions facilitating access to bus routes that direct users to the Aubin Grove railway station.
- Providing a main Boulevard from Lyon Road that junctions with Kinley Road the siting of medium density residential and local convenience retail.
- Acknowledging limited access to the proposed Other Significant Road Reserve of Rowley Road, providing a vegetated buffers to that corridor. In addition to the required 10 metre road widening.
- Being mindful of the cut to fill ratio that brings opportunities to the site earthworks.
- Planning for a sewerage pump station to the lower gradient of Kinley Road.

## 7. NETT EFFECTS

*This section describes the nett effects and public benefit resultant of enabling residential development.*

### 7.1 ENVIRONMENTAL

Recognising the existing Resource Enhancement Wetland and condition of remnant Banksia Woodland and the proposed conservation required by proposed development, the nett effect would be to:

- Improve the environmental attributes of existing wetland ecology and enhance the wetland as required by the definition of 'resource enhancement wetland'.
- Protect a green network of remnant vegetation, potential forage for Black cockatoos.
- Protect wetlands and associated wetland vegetation with restoration of ambient groundwater levels.

An essential long term public benefit that can be provided by urban development of the subject land is the inclusion of a green network to protect a corridor of remnant vegetation, including the wetland and cockatoo forage supplementing the green corridor linkage to Gibb Road conservation area.

### 7.2 URBAN

The subject land is well serviced by primary, secondary and tertiary level of services and facilities. Nett long term benefits provided by development including greater population accessibility to facilities comprising the:

- Cockburn Central Regional Activity Centre and Intermodal Transport Hub, Aubin Grove Neighbourhood Centre and Rowley Road Local Centre
- Employment nodes of Cockburn Central and associated Industrial area, Latitude 32 and Kwinana
- 'Primary Regional' Freeway and proposed 'Other Regional' Rowley Road linkages providing access north-south and east-west respectively to employment centres
- Social infrastructure comprising primary secondary and tertiary education and regional and local recreation reserves plus entertainment facilities and Cockburn Central (Refer Annex D: District Sustainability Assessment).

Refer to the District Sustainability plan at Annexure D.

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### 7.3 SOCIAL INFRASTRUCTURE

The Cockburn Central Regional Activity Centre located 6 kilometres north of the subject land provides commercial, retail, employment and community facilities and includes the Jandakot Business Park.

The employment centre of Bibra Lake Industrial Park is 11 kilometres north west of the subject land. Henderson, Latitude 32 and Kwinana Industrial Area are 8 kilometres to the west.

Rowley Road provides direct vehicular access to these employment centres. This access will be improved as Rowley is classified as an Other Regional Road in the MRS network.

- The Aubin Grove Neighbourhood activity centre is situated north along Lyon Road.
- The Aubin Grove Local Centre including medical facilities fronts Lyon Road immediately west of the subject land.
- The Atwell Secondary College is 3.2 kilometres north of the subject land.
- The Aubin Grove Primary School abuts the subject land to the north. This includes Public Open Space and playing fields that immediately abutt the subject land to the northern boundary.
- The Beeliar Regional Park (Thomson Lake Nature Reserve and Harry Waring Marsupial Reserve) is three kilometres west of the subject land.

### 7.4 FINANCIAL

Placing additional residents within a well serviced district proximate to services, facilities and all transport modes is sensible planning. Such planning optimises the State's financial management of service and transport infrastructure and reduces transport costs by enabling infill development as opposed to continuing expansion of suburbs further north and south, 150 kilometres extant.

### 7.5 NETT EFFECTS

The nett effects and public benefit resulting from residential development of the Lyon and Kinley are evident; identifying the subject land as Urban in the Strategy will assist in advancing development of the land.

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## 8. METROPOLITAN REGION SCHEME

*This section examines the precedent for the coexistence of urban development and water resource protection.*

### 8.1 PRECEDENT ESTABLISHED

The precedent for coexistence of urban development over or adjacent the Jandakot Groundwater Protection Area has been established; integrated water resource and land use planning management is achievable. This is demonstrated by previous MRS amendment requests put before the Commission as shown at Annexure D: Suitability Assessment: District Level.

Amendments to the MRS adjacent or over the Jandakot Groundwater Protection Area, that have been accented to comprise:

Banjup: 1221/41

Treeby: 1346/57

Wandi: 1186/41

West Piara Waters: 1369/57

These amendments have progressed through the system including referral to the Department of Water and Environmental Regulation. The key consideration here is that the Jandakot Groundwater Protection Area can coexist with the urbanisation of the site. These amendments substantiate intensification of residential land use above the water mound.

These amendments are a representation of the decision maker exercising discretion to advance residential development, having due regard to advice from DWER inclusive of applying best practice management to protect the drinking water source. This was previously exemplified by the Western Australian Land Authority developments at Atwell and Aubin Grove immediately abutting the site.

The Wandu and Piara Waters amendments represent a coordinated planning approach by other local governments. In consideration of City of Cockburn's Council's resolution of 9 July 2015, this coordinated planning approach can also be achieved for the subject land at Lyon and Kinley.

### 8.2 MRS AMENDMENT REQUEST

There has been considerable research undertaken thus far has led Qube Property Group and the Owners' Collaborative to pursue urban classification for the Lyon and Kinley subject land. In the compilation of this research, consistency in site characteristics and context is evident in comparison and contrast to other residential development adjacent or on the Jandakot Groundwater Protection Area.

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Recognising the city relies on regional planning initiatives undertaken by the State Government, it follows suit that a coordinated procedural approach between the State, inclusive of early discussions with DWER, the City and the Qube Property Group should be initiated to assess the merits of Urban classification of the Lyon and Kinley subject land.

Accordingly, amendments to the City of Cockburn's Local Planning Strategy should be made to accommodate such an approach, as recommended in this submission.

For these reasons, concurrent with this critical review of the Strategy, **urbanplan** is in preparation of a request to amended to the Metropolitan Region Scheme from Rural - Water Protection to Urban to substantiat, through detailed research and scrutiny of the subject site, that integrated water resource and land use planning management that protects the environmental attributes of the subject land can be achieved. Consequent to summary of Submissions by the City, **urbanplan** seeks a coordinated approach from the City to implement Council 's Resolution of 9 July 2015 commencing with liaison between the city, the Department and Qube Property Groups' representatives.

## 9. CONCLUSION

As an opening, over 24 years there is little change in the spatial land use composition of the 2022 Strategy when compared to the publication of the City of Cockburn's 1991 Local Planning Strategy. While dwelling yield targets are a measure of development progress in response to the objectives of Perth and Peel @ 3.5 Million, they are not a stand alone measure nor are they a minimum.

A mainstay to the Strategy document is that the Local Planning Scheme classification of Rural Water Protection Zone for the Lyon and Kinley subject land is not challenged by the City, at best is a State led consideration.

The stance by the City suggesting that changes to spatial land use planning over the ground water mound are to be State led represents a less than coordinated approach to planning of the metropolitan area, particularly land with inner urban land fill characteristics proximate to all services and facilities. The nett effects and public benefit resulting from residential development of the Lyon and Kinley are evident; identifying the subject land as Urban in the Strategy will assist in advancing development of the land.

The precedent for coexistence of urban development over or adjacent the Jandakot Groundwater Protection Area has been established; integrated water resource and land use planning management is achievable. This is demonstrated by previous MRS amendment requests.

It follows suit that a coordinated procedural approach between the State, inclusive of early discussions with DWER, the City and the Qube Property Group should be initiated to assess the merits of Urban classification of the Lyon and Kinley subject land. Consequent to summary of Submissions by the City, **urbanplan** seeks consideration by the City Planners to be involved a coordinated approach to implement Council 's Resolution of 9 July 2015.

To support this, **urbanplan** makes recommendations for amendments to the Actions given in the Strategy. These recommendations loosen the Actions making them more flexible in line with the Planning Commission's state planning policy base that is based on merit, as scrutinised through a long and thorough panning process involving detailed research of all site characteristics.

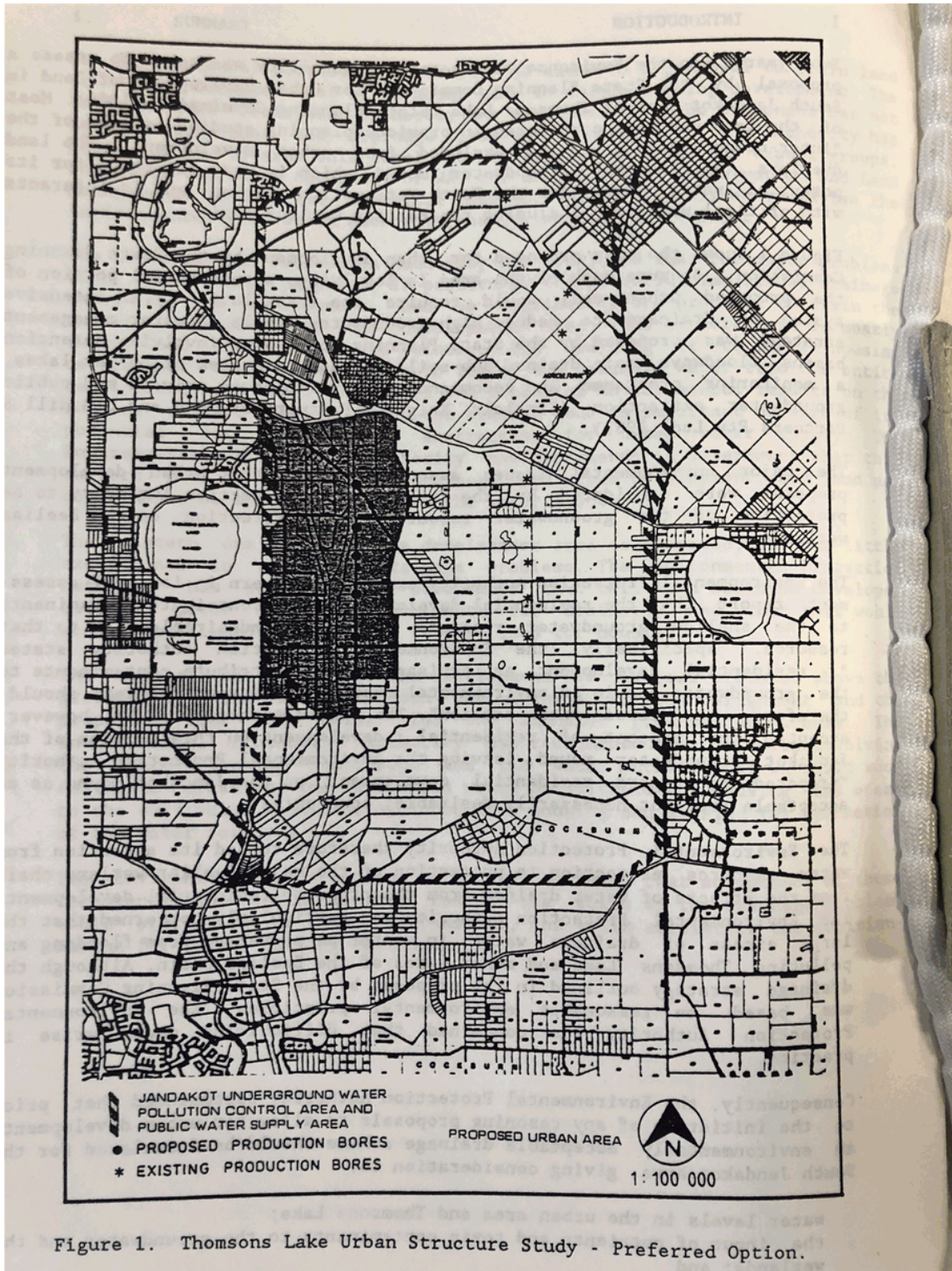
The nett effects and public benefit resulting from residential development of the Lyon and Kinley are evident; identifying the subject land as Urban in the Strategy will assist in advancing development of the land.



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## **ANNEXURE A**

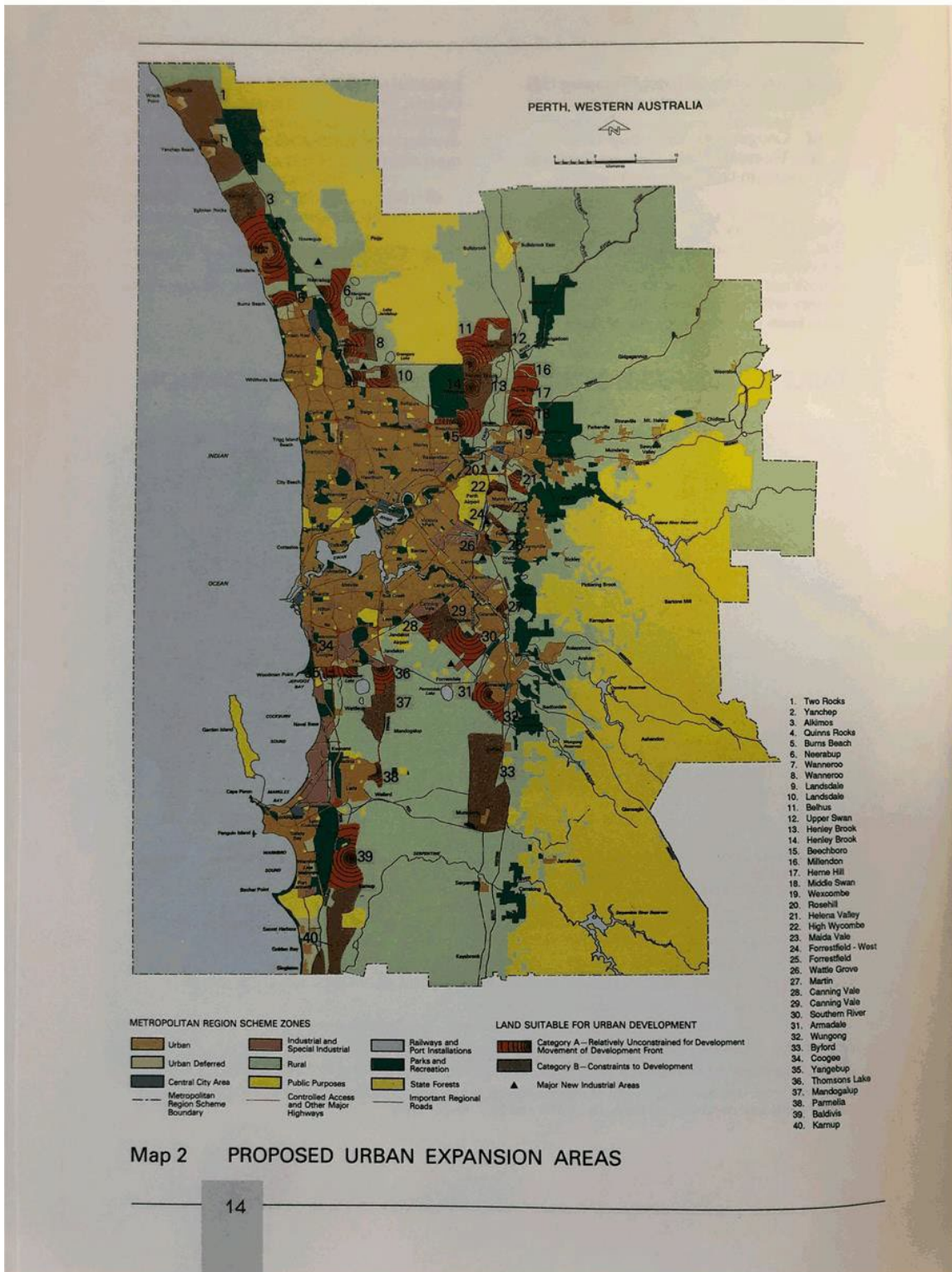
**Thomsons Lake Urban Structure Study and  
South Jandakot Development Water  
Resource Management Plan 1987**



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## **ANNEXURE B**

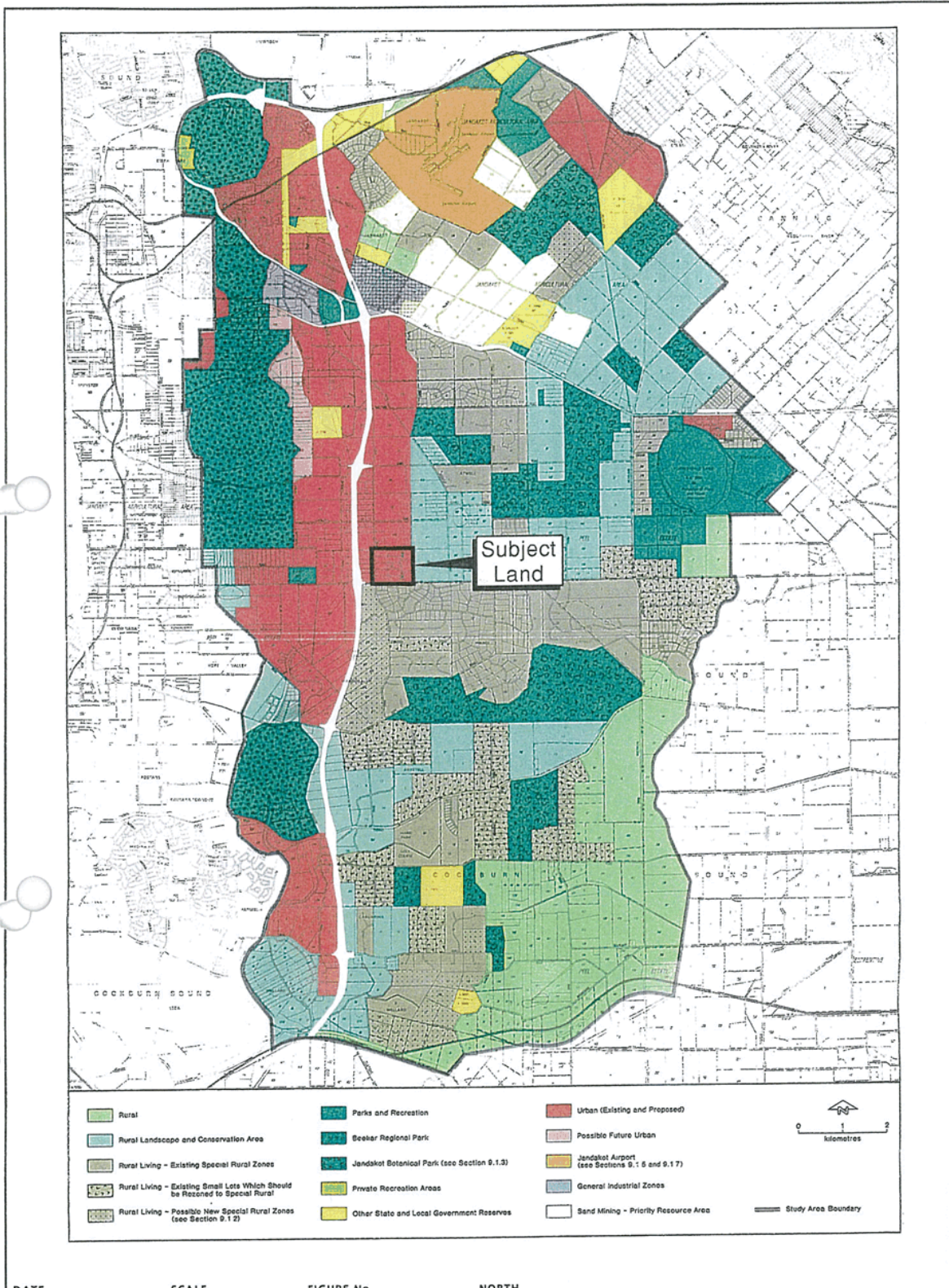
**Urban Expansion Policy Statement 1990**



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## **ANNEXURE C**

**Jandakot Land Use and Water Management  
Strategy 1992**



DATE December 1996 SCALE FIGURE No. Figure 4 NORTH

Jandakot Land Use & Water Management Strategy (October 1992)

Taylor Burrell  
Town planning and design  
Alan Tingay & Associates  
Environmental scientists  
Sinclair Knight Merz

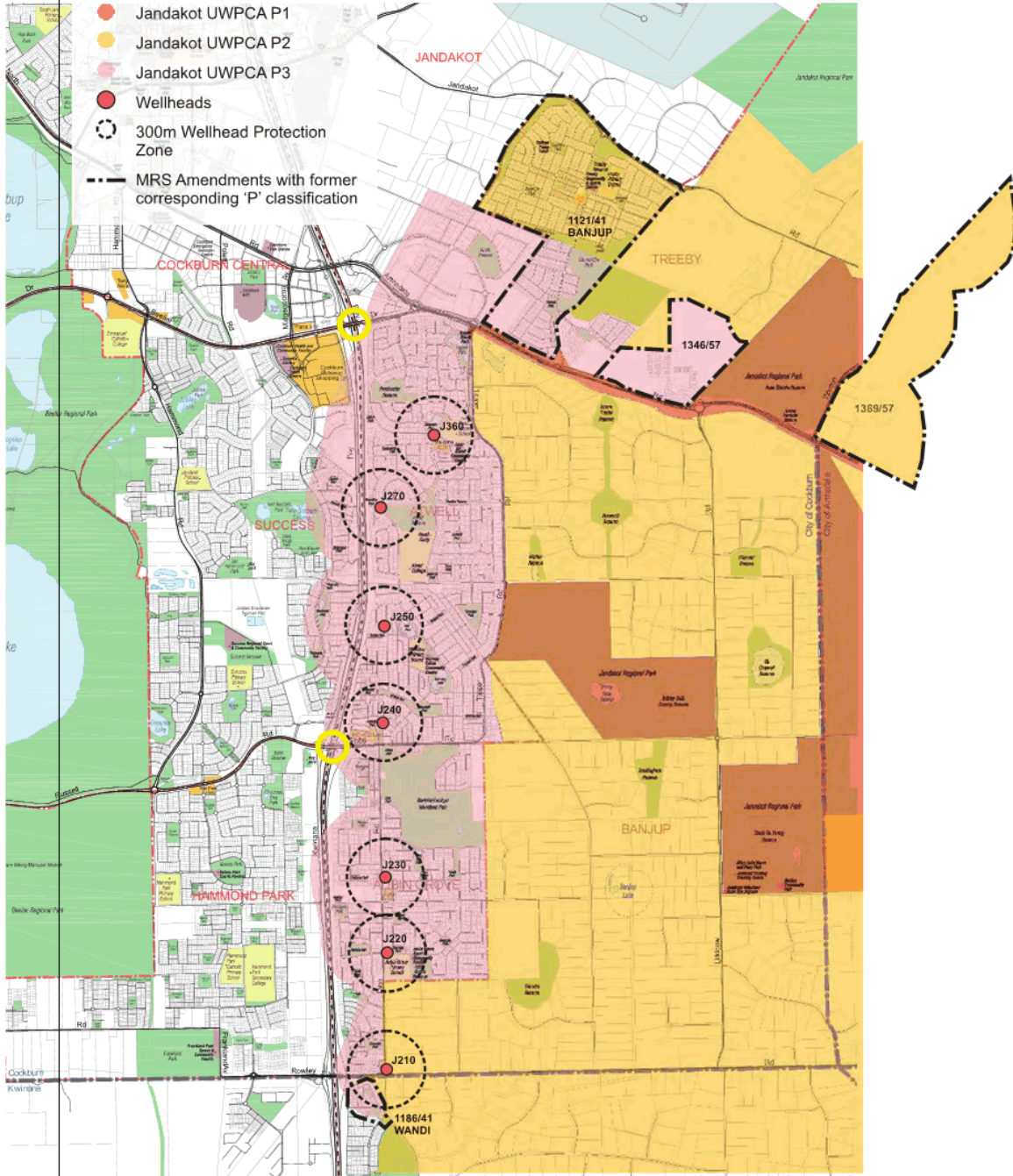
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## **ANNEXURE D**

**Sustainability Assessment: District Level**

Legend

- Subject Land
- Education
- Activity Centre
- Community Centre
- Parks
- Jandakot UWPCA P1
- Jandakot UWPCA P2
- Jandakot UWPCA P3
- Wellheads
- 300m Wellhead Protection Zone
- MRS Amendments with former corresponding 'P' classification



Suitability Assessment: District Level  
Lyon & Kinley

0 1000m  
scale 1:30,000 (A3)  
Source: City of Cockburn



## *Planning Area K: Kinley/Lyon Road Area*



The boundary of the MRS 'Rural – Water Protection' zone south of Armadale Road has a spatially regular western interface with the 'urban' zone, except for the area bounded by properties on Kinley, Lyon and Rowley Roads. This area projects further westward, interfacing with the 'urban' zone to the north (Aubin Grove) and west (Hammond Park).

These spatial characteristics, and identification within State Government strategic planning documents of the 1990s (Expansion Policy Statement & Jandakot Landuse and Water Management Strategy) as 'future' or 'potential' urban, has precipitated ongoing requests from landowners for rezoning to 'urban'.

The draft advertised *Perth and Peel @3.5million* defined urban expansion as "land that has been identified for potential urban development in preceding planning studies, or represents the logical expansion of an existing urban area". In recognition of the land's previous identification, the City's 2015 submission on *Perth and Peel @3.5million* advocated for this area to be identified as 'urban expansion'.

Whilst this recommendation was not adopted by the State Government (the land was not identified in the final adopted *Perth and Peel @3.5million* as an urban expansion or investigation area), and the City is obligated to ensure its local planning framework is consistent with the sub-regional planning framework, inclusion of the land within 'Planning Area K' is proposed to ensure an appropriate framework is in place to ensure the appropriate preparation and consideration of future MRS amendment requests for the area.

Planning Area K: Kinley/Lyon Road Area				
	<i>Planning Direction</i>	<i>Action</i>	<i>Rationale</i>	<i>Timeframe</i>
1.0	Protection of groundwater resource, including impact on wetlands.	1. Advocate for a comprehensive study demonstrating protection of the groundwater resource; in addition to the groundwater impacts on the wetland systems and other environmental values.	Any land use changes have the potential to negatively impact environmental values	Ongoing
2.0	Protection of environmental and conservation values.	1. Identification of remnant bushland, ecological corridors and areas of conservation value to be protected, and measures to protect them, prior to any land use or MRS zoning change.	Any land use changes have the potential to negatively impact environmental values.	Ongoing
3.0	Management of bushfire risk whilst protecting environmental values.	1. Ensure bushfire management is comprehensively considered as part of any MRS amendment and district structure planning process.	Area is Bushfire Prone with high environmental values.	Ongoing
4.0	Coordinated development that includes a district structure planning approach (or a structure plan for the whole 'Planning Area K') in the event of any MRS zoning change.	1. In the event of an MRS Amendment, a comprehensive structure plan for the whole 'Planning Area K' is to set out: <ul style="list-style-type: none"> <li>• Staging and/or identification of logical cells for coordinated planning.</li> <li>• Structure plan implementation requirements.</li> <li>• Infrastructure (including community infrastructure) needs and requirements.</li> <li>• Network of public open space to meet the recreational needs of the community.</li> <li>• Traffic impact assessment, including assessment of regional and local road impacts.</li> <li>• Fire management.</li> <li>• An appropriate interface, particularly to the 'Water Protection' zone.</li> <li>• Consideration of appropriate interface with Rowley Road, including restricted access, acoustic and land requirements.</li> <li>• Community benefit.</li> </ul>	In the event of MRS zoning changes and any change or intensification of development / subdivision potential, it will be critical to ensure development is not ad hoc.	As required

## *Planning Area L: Bibra Lake Recreational Area*



The Bibra Lake Recreational Area has a rich history of indigenous and historic cultural heritage, and a long history of public use and community activity. Today the area is an important conservation area, and a recreational hub of regional significance. It attracts a large and growing number of visitors and contains a variety of private commercial and public recreation features and infrastructure in a largely natural setting.

The extent of Planning Area L corresponds to the Planning Area defined in the Bibra Lake Management Plan.

### **Bibra Lake Reserve Areas**

The area east of Progress Drive and north of Gwilliam Drive comprises reserves or freehold land vested or managed by the City of Cockburn that serve a conservation and/or recreation function, including Bibra Lake Reserve and Yaakan Park. These areas are zoned 'Parks and Recreation' under the MRS and are located within Bush Forever site 244.

The foreshore and surrounds of the lake are highly accessible and, in contrast with other lakes in the southern suburbs of Perth, contain a range of well-developed recreation facilities and community-based organisations. It is also a place of connection to the natural environment, offering an opportunity to experience natural bushland within a wider suburban setting.

The Bibra Lake Reserve includes seasonal views across the water, open lawned parklands, playgrounds, BBQs, toilet facilities, a skatepark, the Bibra Lake Boardwalk and Bird Hide and walk/cycle paths.

The reserve hosts a number of community and educational facilities including the Cockburn Wetlands Education Centre, WA Wildlife Rehabilitation Centre and the 1st Bibra Lake Scout Group hall. These

collectively perform a range of valuable community services, including education and awareness raising, volunteer involvement, training, revegetation, fauna rehabilitation and applied research.

The Aboriginal Cultural and Visitors Centre is proposed to be located near the corner of Gwilliam Drive and Progress Drive. It will be a place of recognition and learning about Nyungar culture, language, music, art and dance, and a unique attraction in metropolitan Perth.

Bibra Lake is part of Beeliar Regional Park and subject to management strategies and policies contained in the Beeliar Regional Park Management Plan and the Bibra Lake Management Plan which sets out management zones, including 'conservation' and 'recreation' areas. It identifies that some scope exists to further develop and enhance recreation facilities and uses of the reserve, though such expansion needs to occur without degrading environmental, cultural and heritage values.

#### **Private Recreation Zone Precinct**

On the western side of Progress Drive, south of Gwilliam Drive is a 23.5ha area zoned 'Private Recreation' under the MRS that comprises 8 lots in private ownership, including Perth's only/major theme park, Adventure World.

Seven lots are zoned 'Special Use No. 7' which allow for a limited range of uses including education, private recreation, restaurant, health studio, club premises and fast-food outlet, and one lot zoned 'Special Use No. 8' (Adventure World) for 'Amusement Park'.

'Private recreation' is defined in the MRS as: '*Areas of significance to the regions recreation resource which are or are proposed to be managed by the private sector.*'

The vision for this precinct is to accommodate appropriate uses that respect and complement the recreational, conservation and cultural values of Bibra Lake and support and enhance the visitor experience to the precinct. This precinct is visually important as a key entry point to the Planning Area and should contribute positively to its character. Review of the 'Special Use' zones should include identifying an appropriate range of uses to deliver this vision. It is also critical that any uses introduced do not limit the current or potential future operation of the regionally significant land uses located within this and the broader precinct.

#### **Parks and Recreation (west of Progress Drive) Precinct**

South of this area is land zoned 'Parks and Recreation' which includes a mix of privately owned lots and City managed reserves. Boorn and Djidi Djidi Reserves have conservation value and provide important habitat linkages, whilst the Mary Ann Tapper Reserve has historic heritage value. There are also 3 privately owned landholdings - Lot 21 and 24 owned by Adventure World, and Lot 26 Progress Drive (Cockburn Ice Arena).

In this area, commercial and recreational uses must be consistent with the 'Parks and Recreation' reservation and the relevant management plans, and must not negatively impact the conservation, cultural or historic heritage values of the area. There is considered to be merit in considering whether these privately owned lots should be rezoned to 'Private Recreation' under the MRS to align with the northern area. A 'Special Use' zone could then be applied with an appropriate range of land uses.

Planning Area L: Bibra Lake Recreational Area				
	Planning Direction	Action	Rationale	Timeframe
1.0	<p>Protection, enhancement and promotion of the natural and cultural values of Bibra Lake wetland and associated reserves whilst:</p> <ul style="list-style-type: none"> <li>Enabling sustainable community use of the lake and associated reserves through provision of a range of conservation, recreation, cultural and environmental education opportunities.</li> <li>Accommodating appropriate uses within the 'Private Recreation' MRS zone that enhance the visitor experience to the Planning Area.</li> </ul>	<ol style="list-style-type: none"> <li>Development within the reserve areas guided by the Bibra Lake Management Plan (or equivalent).</li> <li>Ongoing implementation of the Bibra Lake Management Plan (or equivalent) and other relevant adopted plans for the area.</li> <li>Consider expanding the 'Planning Area' to include the abutting area of the former Roe Highway Reserve.</li> <li>See actions for 2.0 (Private Recreation Precinct).</li> </ol>	<p>The area has high conservation and cultural values that must be protected.</p>	Ongoing
2.0	<p>The MRS 'Private Recreation' as a 'Special Use' zone(s) to provide a range of high-quality tourism, educational, and private recreation uses, and limited range of commercial uses that:</p> <ul style="list-style-type: none"> <li>Support and enhance the visitor experience to the precinct.</li> <li>Contribute positively to the unique identity and character of the precinct as a conservation and recreation area with high landscape, amenity and cultural values.</li> </ul>	<ol style="list-style-type: none"> <li>Investigate expansion of the MRS 'Private Recreation' zone to include the privately owned lots (currently reserved 'Parks and Recreation').</li> <li>Identification of appropriate uses through review of TPS3 within a 'Special Use' zone(s), including consideration of a broader range of uses to support the Planning Area.</li> <li>Consider adding 'Tourist Development' as a permissible use within the 'Special Use', with conditions to ensure compatibility with the character and current major attractions within the Planning Area, and that minimise the removal of existing vegetation.</li> <li>Consideration of local and regional traffic impacts for proposals.</li> </ol>	<p>It is logical to consider including the privately owned and developed lots within the precinct in the MRS 'Private Recreation' zone.</p> <p>Appropriate range of uses required to support the precinct whilst protecting the identified values. 'Tourist Development' (undertaken in an appropriate manner) could provide accommodation for visitors to the Planning Area.</p>	1-5 years

3.0	<p>Development within the MRS 'Private Recreation' zone that does not detract from the natural environment and bushland setting of the Planning Area, reflecting a more natural setting than would be typically seen in commercial areas, including:</p> <ul style="list-style-type: none"> <li>• Development siting, bulk, scale and heights compatible with the bushland character of the Planning Area.</li> <li>• Development in a landscaped setting, particularly when viewed from public areas.</li> <li>• Fencing and signage etc. that does not detract from the natural setting.</li> <li>• Preservation of existing trees and vegetation to the greatest extent possible.</li> </ul>	<ol style="list-style-type: none"> <li>1. Identification of conditions in the 'Special Use' zone to ensure development contributes positively to the character of the Planning Area.</li> <li>2. Require landscape visual impact assessment for land use or development proposals that propose vegetation removal that will alter landscape character.</li> </ol>	<p>To ensure the conservation, cultural and landscape values of the precinct are protected, and to foster a unique identify and character for the precinct.</p>	1-5 years
4.0	<p>Management of bushfire risk whilst protecting environmental and cultural values.</p>	<p>Ensure bushfire management is comprehensively considered as part of any proposals for development, and that proposals are only supported where managing the bushfire risk does not have an unacceptable environmental, cultural or visual landscape impact.</p>	<p>Area is Bushfire Prone with high environmental and landscape values.</p>	Ongoing

***APPENDIX B: Principles for Preservation of Banjup (as adopted by Council October 2022)***

1. The City's continuing strategic intent for Banjup that its natural landscapes maintain a positive and memorable rural appearance.
2. Banjup is a highly appreciated, rural locality whose character and amenity will be preserved by the City.
3. Banjup will be advocated as a highly appreciated, naturally rich, rural locality to:
  - I. State and Commonwealth governments, particularly when planning its long-term future,
  - II. Property developers contemplating changes to Banjup's rural status.
4. The City will earnestly seek to resolve the zoning anomaly in the south west corner of Banjup.
5. The intended function of the roads in Banjup is for traffic within its local area and not for traffic between outside areas and they will be managed as such.
6. Reserves in Banjup will be preserved as accessible places of peace and quiet with rich flora and fauna.
7. Ecological linkages within and between reserves in Banjup will be developed and maintained for the enjoyment and amenity of residents and visitors.
8. The City will continuously encourage and inform Banjup landowners in the safe and sustainable stewardship and preservation of their lands so that future generations living in Banjup can enjoy increasingly richer flora and fauna.

# APPENDIX B

## ACTIVITY CENTRES COMPARISON TABLE (May 2022)

Location	Year	Future Ind. Service Pop (trade) Area		Main Role / Function							Diversity Performance Target	Typical retail Types				Typical Office Development				Residential Density target per gross hectare				Transport Connectivity and Accessibility				
		Catchment (current)	Catchment (future)	Total Size	Core Area	Major land uses	Frame	Commercial	Commercial (future)	Retail ratio		Retail	Retail (future)	Bulky Goods	Bulky Goods (Future)	Office	Office (future)	Jobs	Jobs (future)	Population	Population (future)	Dwellings	Dwellings (future)	Density	Rail	Bus	Bus Frequency	
		Activity Centre	date adopted	Current catchment	Future catchment	Area covered by Activity Centre Plan (ha)	main urban area - retail and high-density housing (ha)	indicative of main areas in each AC, e.g. hospitals, university, recreation, day/night, utilities (ha)	single residential < 860 / industrial land included in activity centre area (ha)	total commercial floorspace now (sqm)	total commercial floorspace target (sqm)	retail non-retail floorspace % now/target	Current floorspace (retail)	Target floorspace (retail)	Current floorspace (bulky)	Target floorspace (bulky)	Current floorspace (office)	Target floorspace (office)	Current Jobs	Target Jobs	Current Population	Target Population	Current Dwellings	Target Dwellings	Dwellings per gross hectare (current / target)	Integrated Rail - Yes/No	Integrated Bus - Yes/No	No. of bus services (estimate, 4-5pm)
Cockburn		280,000	390,000	360 ha	200 ha	200 ha Retirement Village - 5ha	160 ha	600,000 sqm	130k + ind + arc + central	10% / 7 %	60,000 sqm	90,000 (Gateway only)	42,000 sqm	N/A	100,000 sqm	25k + ind	8,500	N/A	2700	23000	1300	8500	3.2 / 4.0	Yes	Yes	40		
Mandurah (no ACP)	-	110,000	N/A	200 ha	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	8000	N/A	N/A	2500	N/A	N/A	No - 2.5km	No	30	
Fremantle (no ACP)	-	N/A	N/A	200 ha	90 ha	N/A	N/A	N/A	N/A	N/A	N/A	65,000 sqm	N/A	N/A	N/A	60,000 sqm	N/A	8,500	N/A	8000	N/A	N/A	N/A	N/A	N/A	Yes	Yes	60
Rockingham	2010	130,000	270,000	600 ha	500 ha	Service Commercial - 60ha	100 ha	N/A	N/A	N/A	N/A	N/A	100,000 sqm	N/A	N/A	100,000 sqm	N/A	20,000	N/A	36000	N/A	20000	N/A	N/A	No - 3km	No	15	
Armadale (draft - why?)	2018	110,000	300,000	85 ha	85 ha	Civic - 5ha	0 ha	N/A	N/A	N/A	N/A	55,000 sqm	80,000 sqm	N/A	N/A	N/A	N/A	3,000	18,000	450	6000	240	2000	3.2 / 4.0	Yes	Yes	15	
Joeondrup	2018	350,000	500,000	475 ha	170 ha	Hospital - 15 ha	40 ha	850,000 sqm	1,600,000 sqm	20% / 20%	165,000 sqm	270,000 sqm	130,000 sqm	100,000 sqm	90,000 sqm	200,000 sqm	17,000	45,000	4500	18000	2000	9000	8 / 47	Yes	Yes	50		
Stirling	2014	N/A	N/A	360 ha	330 ha	Hospital - 13ha	30 ha	N/A	1,000,000 sqm			150,000 sqm	450,000 sqm	inc. in retail	inc. in retail	40,000 sqm	450,000 sqm	N/A	30,000	3800	25000	1600	14000	6 / 45	Yes (future)	Yes (future)	60	
Mileend	2018	270,000	N/A	180 ha	200 ha	N/A	30 ha	160,000 sqm	1,000,000 sqm	44% / 25%	70,000 sqm	260,000 sqm	N/A	40,000 sqm	50,000 sqm	420,000 sqm	N/A	22,000	1000	14000	700	7000	6 / 43	Yes	Yes	40		
Morley	2018	N/A	N/A	220 ha	130 ha	schools - 7	90 ha	260,000 sqm	470,000 sqm	44% / 36%	115,000 sqm	170,000 sqm	20,000 sqm	40,000 sqm	60,000 sqm	120,000 sqm	5,000	6,000	N/A	12000	N/A	7000	6 / 40	No - 2km	Yes	60		
Canington	2017	280,000	370,000	200 ha	190 ha	Health and Education - 25ha	10 ha	N/A	450,000 sqm			150,000 sqm	180,000 sqm	60,000 sqm	70,000 sqm	80,000 sqm	140,000 sqm	5,000	10,000	N/A	25000	N/A	10000	x / 35	Yes (future)	Yes (future)	50	
Yanchep	2018	N/A	150,000	106 ha	106 ha	Health and Education - 25ha	0 ha	0 sqm	450,000 sqm			0 sqm	70,000 sqm	0	N/A	0	N/A	0	15,000	0	5500	0	3500	0 / 50	Yes (future)	Yes (future)	4	
Ellenbrook (secondary)	2021	N/A	N/A	160 ha	80 ha	High school - 10ha	50 ha	N/A	200,000 sqm			30,000 sqm	50,000 sqm	N/A	55,000 sqm	N/A	40,000 sqm	N/A	6000	0	4000	N/A	1700	yes (future)	yes	13		
Booragoon (secondary)																								no	yes	13		
Canning Bridge (secondary)																								yes	yes	50		
Murdoch (specialty)			44000 students	240		University, Hospital													25,000		22000			no - 1.2km	yes			
Cockburn North (Muriel)	2010	n/a		80 ha	60 ha	POS - 7ha	20 ha	360	2000	N/A	N/A	N/A	0	n/a	0	N/A	0	N/A	250	6000	200	2500	1 / 32					
Cockburn West (ARC)	2016	n/a		33 ha	27 ha	ARC + Oval - 5ha	0 ha	0	20000	0 / 10%	0	4000	0	n/a	0	N/A	N/A	N/A	0	1800	0	1000	0 / 30					
Cockburn Central	2007	n/a		16 ha	16 ha	N/A	0 ha	12000	N/A	N/A	2000	N/A	0	N/A	12000	N/A	N/A	N/A	1500	2000	620	1000	50 / 70					
Gateway	2020	120k / 170k		25 ha	25 ha	N/A	0 ha	60000	110000	7 / 80%	60000	90000	8000	N/A	0	20000	2900	N/A	0	1800	0	1000	0 /					
Cockburn East (Industry)	2018	n/a		108 ha	58 ha	POS - 10ha	55 ha	300000	N/A	N/A	0	0	30000	N/A	30000	N/A	2800	N/A	0	0	0	0	0	N/A				
Co West Industrial	n/a	n/a		85 ha	0 ha	Industry - 25 ha	25 ha	220000	N/A	N/A	0	0	4000	N/A	35000	N/A	1800	N/A	0	0	0	0	0	N/A				
South of Gateway	n/a	n/a		14 ha	14 ha	Retirement Village - 5ha	0 ha	N/A	N/A	N/A	0	0	0	0	0	0	0	0	2000	1500	500	1000						
WAPC Land Use Survey	2017							(industrial floorpace)					(industrial floorpace)		(industrial floorpace)		(industrial floorpace)		(industrial floorpace)									
Ferlane Study	2021	280k	390k																									

N/A = Not available



### 14.1.3 Development Application - DA22/0860 - 158 Tindal Avenue, Beeliar - Use Not Listed (Telecommunications Facility)

<b>Responsible Executive</b>	A/Chief of Built and Natural Environment
<b>Author(s)</b>	Acting Coordinator of Development Services
<b>Attachments</b>	<ol style="list-style-type: none"> <li>1. Development Plans <a href="#">↓</a></li> <li>2. Electromagnetic Energy Report <a href="#">↓</a></li> <li>3. Applicant Planning Report <a href="#">↓</a></li> <li>4. Schedule of Submissions <a href="#">↓</a></li> </ol>
<b>Location</b>	158 Tindal Avenue, Beeliar
<b>Owner</b>	Gary Paulik
<b>Applicant</b>	Axicom Pty Ltd
<b>Application Reference</b>	DA22/0860

#### RECOMMENDATION

That Council:

(1) APPROVES the proposal subject to the following conditions and footnotes:

##### Conditions

1. Development shall be carried out in accordance with the terms of the approved plan.
2. All stormwater must be contained and disposed of on-site. All stormwater infrastructure must be maintained for the duration of the development.
3. No building or construction activities shall be carried out before 7.00am or after 7.00pm, Monday to Saturday, and not at all on Sunday or Public Holidays.
4. The crossover is to be located and constructed to the City's Vehicle Crossover Specifications (as amended). Redundant crossovers shall be removed and the verge reinstated prior to or at the time of the installation of the approved new crossover. The crossover is to be maintained as per City's *Vehicle Crossover Specifications* in perpetuity.
5. All telecommunication facilities including the monopole constructed as a result of this development approval that cease to be used for the approved purpose must be removed from the site and the land reinstated to the satisfaction of the City of Cockburn within three months of the cessation of the use.
6. The telecommunications infrastructure shall be designed to enable co-location by other telecommunication carriers

##### Footnotes

- a) This is a Planning Approval only and does not remove the responsibility of the applicant/owner to comply with all relevant building, health and

engineering requirements of the City, or with any requirements of the City of Cockburn Town Planning Scheme No. 3 or with the requirements of any external agency.

b) Please be advised that the development must comply with the requirements of the Building Codes of Australia

(2) NOTIFIES the proponent and those who made a submission of Council's determination.

## Background

The City is in receipt of a development application (DA) which proposes the construction of a telecommunications tower at the above property.

The facility will be owned by Axicom and will host Vodafone and Optus telecommunications equipment.

The facility will provide improved Vodafone and Optus 3G and 4G mobile and data services and establish 5G services in Beeliar and the surrounding areas.

The proposal involves:

- A new 30m monopole
- The installation of a new triangle headframe on the proposed monopole
- The installation of fifteen (15) new panel antennas at the following centreline heights:
  - Three (3) new panel antennas at a centreline height of 29.5m
  - Twelve (12) new panel antennas at a centreline height of 28.5m
- The installation of twenty-one (21) new remote radio units (RRUs) mounted on the proposed headframe
- The installation of a 6-bay equipment cabinet on a concrete footing within the proposed Axicom compound
- Upgrade of the existing access track to be suitable for construction and ongoing maintenance of the proposed facility
- The installation of ancillary equipment associated with the safety and proper function of the facility, including cable trays, cabling, meter panel, submains, security fencing, antenna mounts, group meter panel etc.

## Community Consultation and Submissions

*Telecommunication Facility* is a 'use not listed' under the City's *Town Planning Scheme 3 (TPS3)*. In accordance with Clause 64 of the *Planning and Development (Local Planning Schemes) Regulations 2015*, this application is defined as a complex application for development approval.

The regulations stipulate that complex applications must be advertised by the local government for a minimum of 28 days by doing the following:

- Notifying all owners/occupiers by within 200m of the site by letter
- Erecting a sign on site
- Publishing details of the application on the local government's website.

Given the level of community interest that is often associated with telecommunication towers, the City advertised the application for a total of 40 days (18 January 2023 - 27 February 2023).

The advertising radius was also increased to 500m and the Beeliam Community Voice (Residents' Association) were contacted separately and advised of the proposal.

Additionally, City planning officers hosted a drop-in information session at the Beeliam Community Centre on Saturday 4 February 2023.

A total of 63 submissions were received during the public consultation process (refer Attachment 4 – Schedule of Submissions).

An officer response has been provided for each submission.

Due to the large number of objections, the application has been brought to Council for determination.

#### Case Law

The State Administrative Tribunal (SAT) has heard numerous cases regarding determinations as they pertain to telecommunications infrastructure.

These cases outline the general approach the SAT will likely take should the applicant choose to appeal Council's determination.

Two themes that are consistently reviewed in SAT cases pertaining to telecommunication facilities are *visual amenity impacts* and *perceptions of health risk*.

These two issues were the dominant public concerns identified during the consultation period.

The City had a previous refusal determination appealed in SAT (see *Vodafone Hutchison Australia Pty Ltd v City of Cockburn* [2018] WASAT 4).

In this matter, the SAT member noted:

*'The Tribunal has determined that application for review is to be allowed, that the determination of the respondent set aside and a decision issued that planning approval is granted subject to conditions.'*

Similar approaches have been taken by the SAT on numerous occasions as it relates to visual amenity.

Community perception regarding health risks is a topic that has also been tested in SAT (see Telstra Corporation Limited and Shire of Murray [2009] WASAT 117).

In the case referenced above, the Shire refused a development application due to the public's perception of radiation exposure and its associated risk to human health.

The SAT member noted:

*“The respondent relies upon some objectors' perceptions of potential health problems and submits that Telstra 'is unable to guarantee that the facility will not cause negative health impacts'. Perceptions, without more, are an unsuitable basis for elevating amenity concerns to the point where a proposal, which is otherwise justified and compliant, should be refused planning approval. As the former Victorian Tribunal said in **Gugliotti v City of Preston** (1988) 1 AATR 97:*

*It has long been held by various divisions of this Tribunal that the mere perception of loss of amenity by objectors is insufficient reason to deny a [planning] permit. There must be a real and genuine prospect that the apprehensions expressed in objections are more likely than not to eventuate. (Cited and followed in **Blumberg v Port Phillip CC** [2005] VCAT 1398 at [21].)*

*Further, it is disingenuous to demand a guarantee of no adverse health impact when all the currently available expert material in this field suggests that the issue simply does not arise on the basis of present knowledge and research.*

*In my opinion, the provisions of [the local planning scheme] (so far as they are, or can be made, applicable) should be invoked to determine that Telstra's approval is a use not listed but that it is a use which is nevertheless consistent with the objectives and purposes of the 'Rural' zone and it should therefore be approved upon certain conditions”.*

## Planning Framework

### Metropolitan Region Scheme (MRS) and Town Planning Scheme (TPS3)

The site is zoned Rural under both the MRS and TPS3.

The objective of the rural zone under TPS 3 is ‘to provide for a range of rural pursuits which are compatible with the capability of the land and retain the rural character and amenity of the locality.’

As mentioned above, the application is a ‘use not listed’, meaning it can be considered in all zones subject to advertising in accordance with Clause 64 of the Regulations.

### Telecommunications Act 1997 and Telecommunications (Low Impact Facilities) Determination 2018

The *Telecommunications Act 1997* and subsequent Determination (2018) exempt certain types of telecommunications developments from requiring planning approval.

As this application proposes a new tower and base station (as opposed to simple upgrades), the development is not exempt from planning approval under local planning schemes and thus requires a Development Application.

### **Report**

#### Community Need

With an increase of residential development and increasing population density, Beeliam and the surrounding suburbs have been identified as areas of high network demand.

Various providers have identified a need to improve mobile coverage and network capacity in this area.

There are no existing base stations for the relevant providers servicing this area.

The closest is at 161 Henderson Road, Munster (roughly 900m away).

The applicant has indicated that there are no viable opportunities for co-location in proximity to the targeted coverage areas.

#### Electromagnetic Energy (EME)

The City has received numerous community submissions regarding the potential health impacts associated with telecommunications towers.

It is beyond the scope of the Planning and Development Act and the City's Town Planning Scheme to make judgement on whether this infrastructure is life-threatening or hazardous to human health.

It is important to note that the issuing of a planning approval does not remove the responsibility of the infrastructure provider to comply with other acts, legislation or matters identified as a public health priority.

Accordingly, this report provides support for the infrastructure on planning grounds.

Notwithstanding, the following information on Electromagnetic Energy (EME) and radiation is provided to Council to inform a recommendation.

In this regard, the applicant has provided an *Environmental EME Report* which indicates the maximum EME exposure level to which one would be exposed.

It has been calculated that a person would be exposed to 3.41% out of the 100% public exposure limit.

This has been calculated at 168m from the base station.

The nearest residential dwelling is roughly 250m from the tower location.

As such, the EME report indicates that the dwellings would be exposed to even less than the max 3.41% calculated.

As 5G networks start to be rolled out across Australia, there has been heightened community sensitivity regarding telecommunications infrastructure.

All mobile carriers must abide by the Radiation Protection Series S-1 – Standard for Limiting Exposure to Radiofrequency Fields – 100kHz to 300GHz (2021), a safety standard prepared by the Australian Radiation Protection and Nuclear Safety Agency (ARPANSA), based on the recommendations of ICNIRP (International Commission for Non-Ionising Radiation Protection).

The Australian Communications and Media Authority (ACMA) regulate compliance with the standard.

The safety standard applies to all mobile frequencies currently used in Australia, including 3G, 4G and 5G. EME from telecommunications is not hazardous to human health.

It's non-ionising, meaning it doesn't have enough energy cause harm (ACMA, 2021).

State Planning Policy 5.2 – Telecommunications Infrastructure notes the following with respect to EME:

*'The use of mobile telephones has raised public concern about possible health issues associated with exposure to electromagnetic emissions. However, telecommunications carriers must comply with the Australian Communications and Media Authority (ACMA) Radiocommunications Licence Conditions (Apparatus Licence) Determination 2003. These licence conditions make mandatory the limits in the Australian Radiation Protection and Nuclear Safety Agency (ARPANSA) Radiofrequency (RF) Standard which sets limits for human exposure to RF electromagnetic fields from all sources, including telecommunications infrastructure.'*

ARPANSA is the primary Commonwealth agency responsible for protecting the health and safety of people and the environment from the harmful effects of radiation.

Measurement surveys undertaken by ARPANSA demonstrate that environmental radiofrequency levels near base stations for the mobile telephone network are extremely low.

The ARPANSA surveys reported that typical exposures to radiofrequency fields were well below one per cent of the Standard's public exposure limits.

It concluded that *“given the very low levels recorded and the relatively low power of these types of transmitters, it is unlikely that the radiofrequency radiation from base stations would cause any adverse health effects, based on current medical research”*.

Standards set by ARPANSA incorporate substantial safety margins to address human health and safety matters; therefore, it is not within the scope of this Policy to address health and safety matters.

Based on ARPANSA’s findings, setback distances for telecommunications infrastructure are not to be set out in local planning schemes or local planning policies to address health or safety standards for human exposure to electromagnetic emissions’

#### State Planning Policy 5.2 (SPP 5.2) – Telecommunications Infrastructure and Visual Amenity

SPP 5.2 identifies the need for telecommunications infrastructure to be developed within populated areas.

The policy largely relates to macro-level planning rather than individual developments; however, it does provide several provisions pertaining to visual amenity.

The Policy stipulates that towers should not be prominently visible from significant viewing locations such as lookouts or recreation sites.

In this case, there are no notable lookouts within the locality and the tower will be juxtaposed against Cockburn Cement, meaning that the dominant feature of the view will not be this tower.

The site is not of ecological, social or cultural significance, so there is no concern pertaining to the impacts of visual landscape values as indicated in Clause 5.1.1 of SPP 5.2.

Finally, the SPP recommends a maximum height of 30m measured from the finished ground level. This has been adhered to under the proposal.

Given that the tower will be located over 200m from the road reserve, the visual impact of the facility will be minimized.

#### **Strategic Plans/Policy Implications**

##### Local Economy

A sustainable and diverse local economy that attracts increased investment and provides local employment.

- Increased Investment, economic growth and local employment.
- A City that is 'easy to do business with'.

Community, Lifestyle & Security

A vibrant healthy, safe, inclusive and connected community.

- A safe and healthy community that is socially connected.

Listening & Leading

A community focused, sustainable, accountable and progressive organisation.

- High quality and effective community engagement and customer service experiences.

**Budget/Financial Implications**

The City is not the proponent of this application. As such, there are no budget implications for the City.

**Legal Implications**

Several objectors have indicated their intention to commence legal action against the City should this application be approved.

It is important to note that there are no third-party appeal rights for planning determinations in Western Australia.

**Risk Management Implications**

The applicant has the right to review Council's decision through the State Administrative Tribunal.

Should the applicant exercise this right, there may be financial implications, particularly where legal counsel is required

**Advice to Proponent(s)/Submitters**

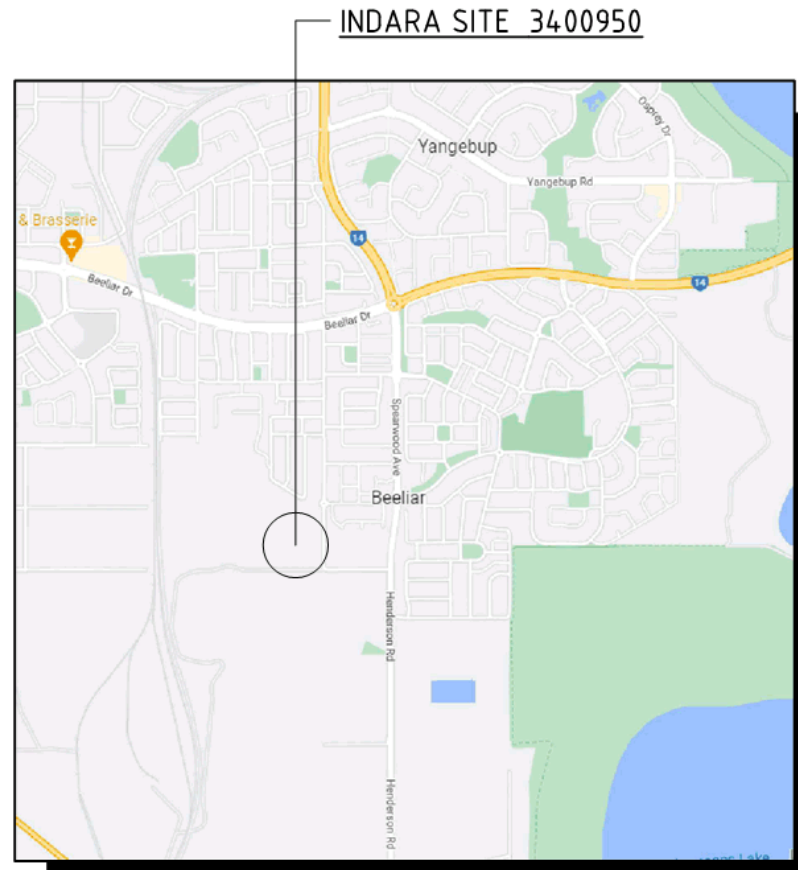
The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 13 April 2023 Ordinary Council Meeting.

**Implications of Section 3.18(3) *Local Government Act 1995***

Nil

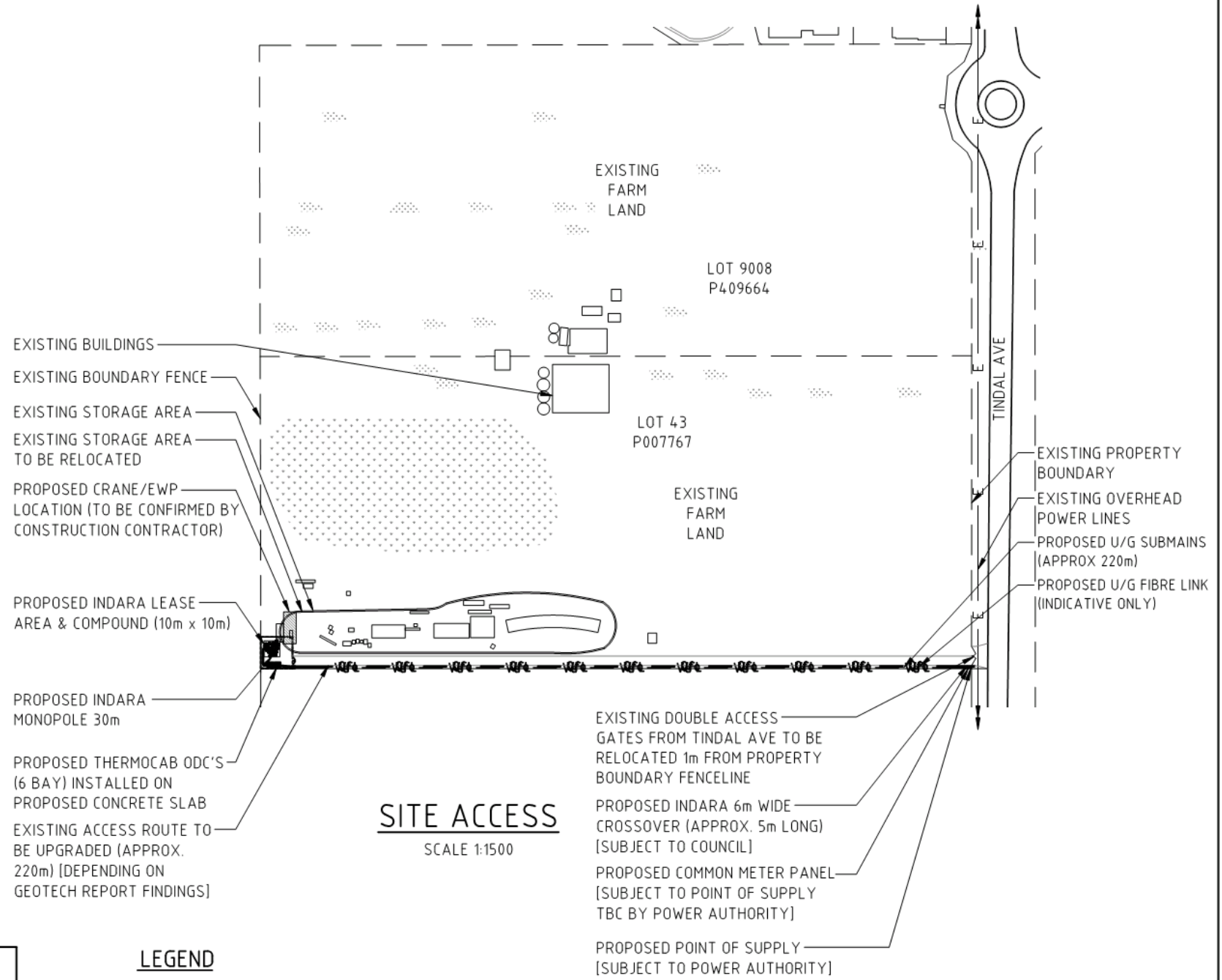


A3



COPYRIGHT © GOOGLE MAPS

**SITE LOCALITY PLAN**  
NOT TO SCALE



**SITE ACCESS**  
SCALE 1:1500

RFNSA NUMBER: XXXXXX		
CARRIER	SITE NAME	SITE ID

**LEGEND**

- / — FENCE
- — — PROPERTY BOUNDARY
- E — OVERHEAD ELECTRICAL
- Ve — Ve — UNDERGROUND ELECTRICAL
- VFL — VFL — UNDERGROUND FIBRE LINK
- Ofc — Ofc — UNDERGROUND FIBRE LINK

**NOTES:**

EXISTING UNDERGROUND CABLES INDICATIVE ONLY.

A	17.11.22	PRELIMINARY (GREENFIELD eJV - AXICOM)	INDARA	SB	DB	PH
REV	DATE	REVISION DESCRIPTION	VENDOR	DRAWN	DESIGNED	APPROD



**SITE NUMBER 640144**  
**YANGEBUP SOUTH**  
158 TINDAL AVE,  
BEELIAR,

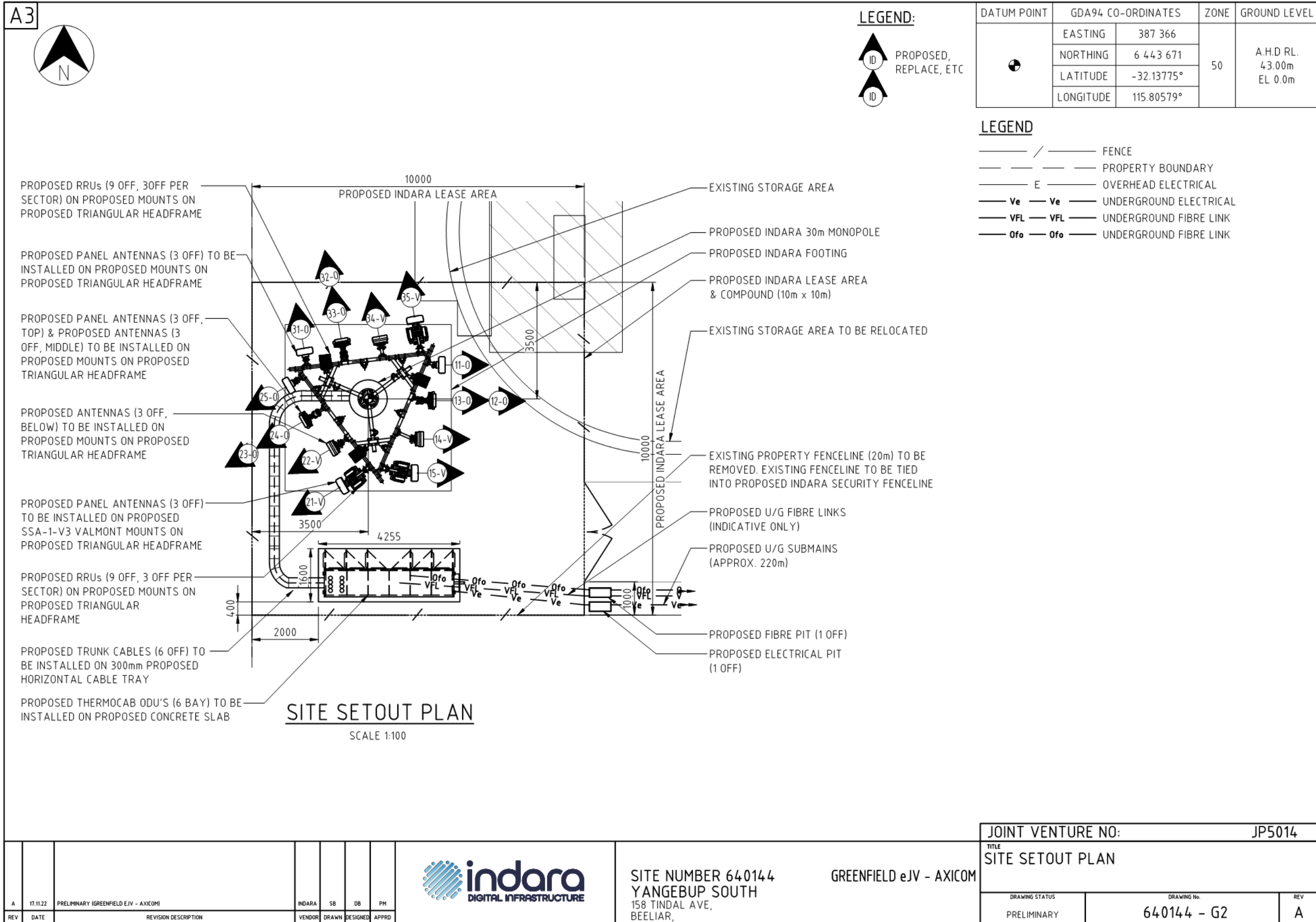
GREENFIELD eJV - AXICOM

JOINT VENTURE NO: JP5014

TITLE  
**SITE AND LOCALITY PLANS**

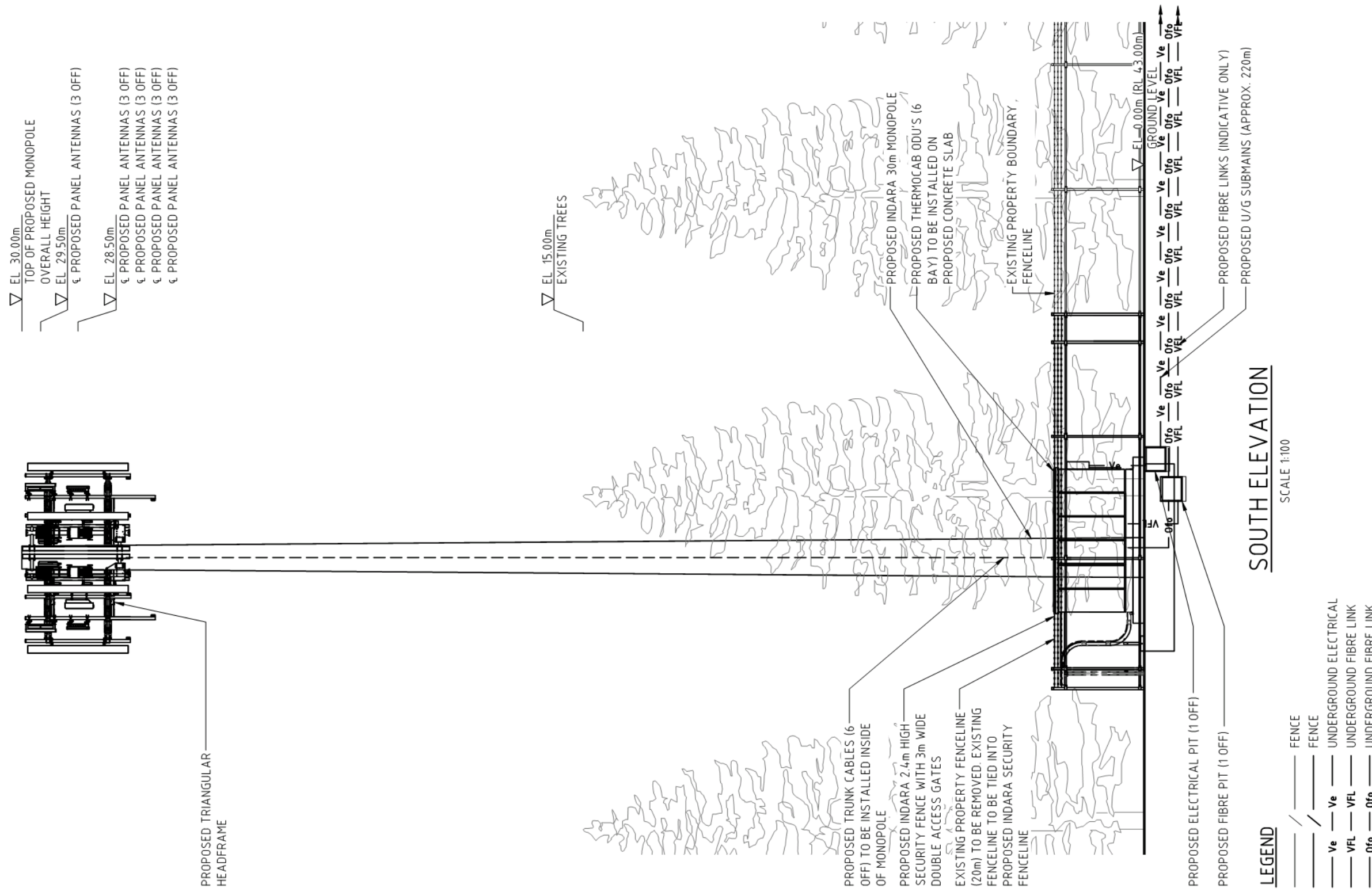
DRAWING STATUS	DRAWING No.	REV
PRELIMINARY	640144 - G1	A

Document Set ID: 11294909  
Version: 1 Version Date: 21/11/2022



Document Set ID: 11294909  
Version: 1 Version Date: 21/11/2022

A3



REV	DATE	REVISION DESCRIPTION	VENDOR	DRAWN	DESIGNED	APPRD
A	17.11.22	PRELIMINARY (GREENFIELD EJV - AXICOM)	INDARA	SB	DB	PH

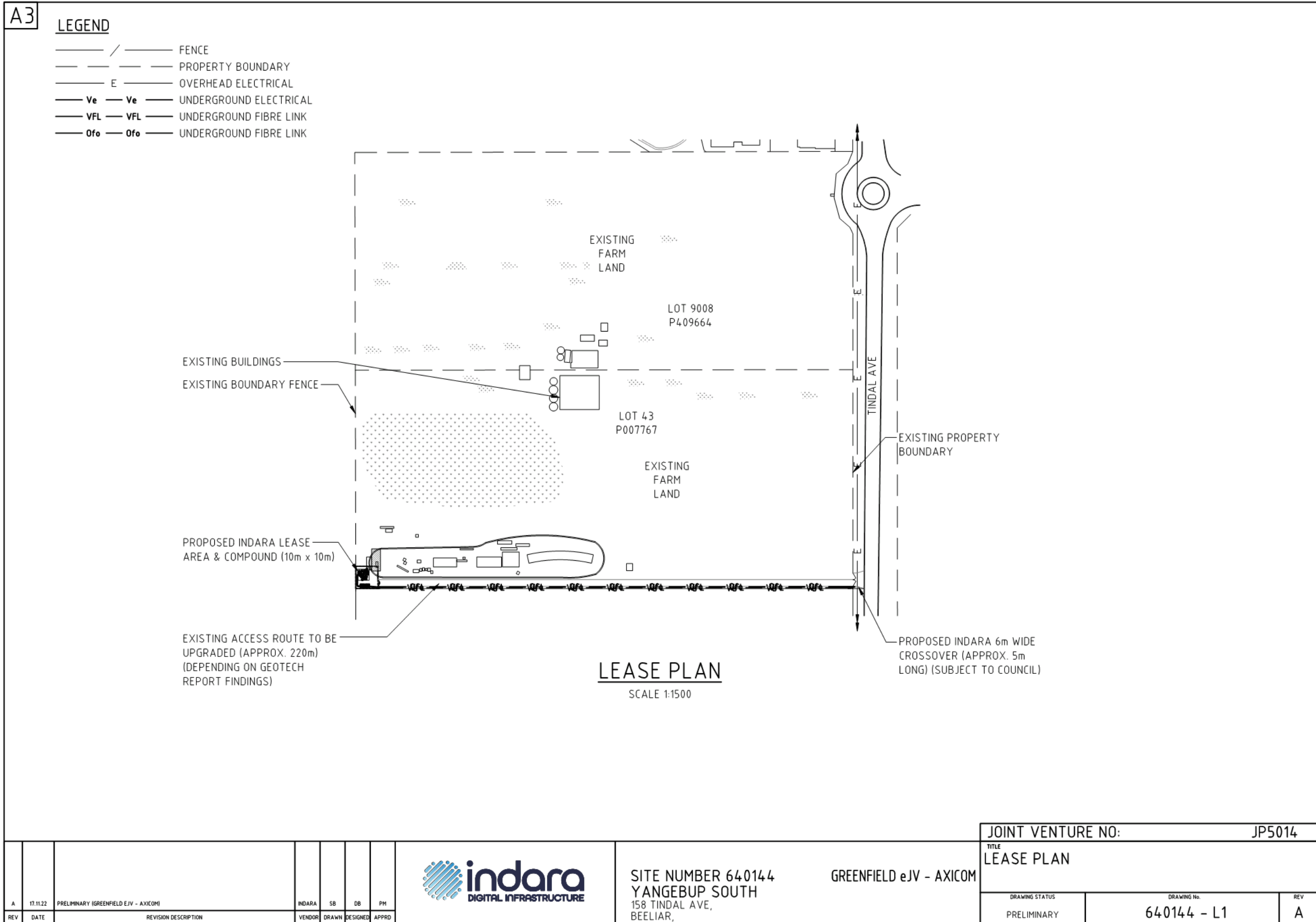


SITE NUMBER 640144  
 YANGEBUP SOUTH  
 158 TINDAL AVE,  
 BEELIAR,

GREENFIELD eJV - AXICOM

JOINT VENTURE NO: JP5014	
TITLE: SITE ELEVATION	
DRAWING STATUS: PRELIMINARY	DRAWING No: 640144 - G3
REV: A	

Document Set ID: 11294909  
 Version: 1 Version Date: 21/11/2022



Document Set ID: 11294909  
Version: 1 Version Date: 21/11/2022

# Environmental EME Report

<b>Location</b>	158 Tindal Avenue, BEELIAR WA 6164		
<b>Date</b>	02/08/2022	<b>RFNSA No.</b>	6164027

## How does this report work?

This report provides a summary of levels of radiofrequency (RF) electromagnetic energy (EME) around the wireless base station at 158 Tindal Avenue, BEELIAR WA 6164. These levels have been calculated by Nokia using methodology developed by the Australian Radiation Protection and Nuclear Safety Agency (ARPANSA).

A document describing how to interpret this report is available at ARPANSA’s website:

[A Guide to the Environmental Report.](#)

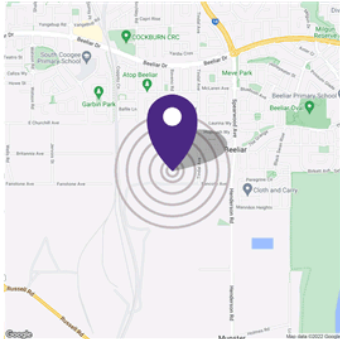
## A snapshot of calculated EME levels at this site

There are currently no existing radio systems for this site.

The maximum EME level calculated for the **proposed** changes at this site is

# 3.41%

out of 100% of the public exposure limit, 168 m from the location.



EME levels with the proposed changes	
Distance from the site	Percentage of the public exposure limit
0-50 m	1.32%
50-100 m	2.99%
100-200 m	3.41%
200-300 m	3.03%
300-400 m	1.53%
400-500 m	0.87%

For additional information please refer to the EME ARPANSA Report annexure for this site which can be found at <http://www.rfnsa.com.au/6164027>.

## Radio systems at the site

This base station currently has equipment for transmitting the services listed under the existing configuration. The proposal would modify the base station to include all the services listed under the proposed configuration.

Carrier	Existing		Proposed	
	Systems	Configuration	Systems	Configuration
<b>Vodafone</b>			5G	NR/LTE850 (proposed), NR/LTE1800 (proposed), NR/LTE2100 (proposed), NR/LTE700 (proposed), NR3500 (proposed)
<b>Optus</b>			4G, 5G	LTE700 (proposed), LTE900 (proposed), LTE1800 (proposed), NR/LTE2100 (proposed), LTE2600 (proposed), NR3500 (proposed), NR2300 (proposed)

## An in-depth look at calculated EME levels at this site

This table provides calculations of RF EME at different distances from the base station for emissions from existing equipment alone and for emissions from existing equipment and proposed equipment combined. All EME levels are relative to 1.5 m above ground and all distances from the site are in 360° circular bands.

Distance from the site	Existing configuration			Proposed configuration		
	Electric field (V/m)	Power density (mW/m <sup>2</sup> )	Percentage of the public exposure limit	Electric field (V/m)	Power density (mW/m <sup>2</sup> )	Percentage of the public exposure limit
0-50m				6.17	100.98	1.32%
50-100m				10.27	279.71	2.99%
100-200m				10.17	274.44	3.41%
200-300m				9.12	220.65	3.03%
300-400m				6.50	112.07	1.53%
400-500m				4.88	63.11	0.87%

## Calculated EME levels at other areas of interest

This table contains calculations of the maximum EME levels at selected areas of interest, identified through consultation requirements of the [Communications Alliance Ltd Deployment Code C564:2020](#) or other means. Calculations are performed over the indicated height range and include all existing and any proposed radio systems for this site.

### Maximum cumulative EME level for the proposed configuration

Location	Height range	Electric field (V/m)	Power density (mW/m <sup>2</sup> )	Percentage of the public exposure limit
South Coogee Primary School	0-3 m	1.53	6.20	0.08%
South Coogee Primary School	0-2 m	1.55	6.40	0.08%
South Coogee Primary School	0-5 m	1.51	6.07	0.08%
Visko Park	0-3 m	1.57	6.52	0.09%
Beeliar Shopping Centre	0-8 m	1.42	5.31	0.07%
Early Learning School	0-5 m	1.28	4.37	0.06%
Radonich Park	0-3 m	1.11	3.24	0.04%



# DEVELOPMENT APPLICATION FOR PLANNING CONSENT



*Proposal to install a new  
Telecommunications Facility at  
**158 Tindal Avenue  
Beeliar WA 6164***

## Statement of Environmental Effects

November 2022

Project Reference: 640144 Yangebup South

RFNSA Number: 6164027



Document prepared by:



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## EXECUTIVE SUMMARY

<b>Proposal</b>	<p>Axicom, on behalf of Vodafone and Optus, is seeking development approval for a new telecommunications facility located at 158 Tindal Avenue, Beeliar WA 6164.</p> <p>The facility will be owned by Axicom, and will host Vodafone and Optus telecommunications equipment. The facility will provide improved Vodafone and Optus 3G and 4G mobile and data services, and establish 5G services in Beeliar and the surrounding areas.</p> <p>The proposal involves:</p> <ul style="list-style-type: none"> <li>• A new 30m monopole;</li> <li>• The installation of a new triangle headframe on the proposed monopole;</li> <li>• The installation of fifteen (15) new panel antennas at the following centreline heights: <ul style="list-style-type: none"> <li>○ Three (3) new panel antennas at a centreline height of 29.5m;</li> <li>○ Twelve (12) new panel antennas at a centreline height of 28.5m;</li> </ul> </li> <li>• The installation of twenty-one (21) new remote radio units (RRUs) mounted on the proposed headframe;</li> <li>• The installation of a 6-bay equipment cabinet on a concrete footing within the proposed Axicom compound;</li> <li>• Upgrade of the existing access track to be suitable for construction and ongoing maintenance of the proposed facility; and</li> <li>• The installation of ancillary equipment associated with the safety and proper function of the facility, including cable trays, cabling, meter panel, submains, security fencing, antenna mounts, group meter panel etc.</li> </ul> <p>The monopole and associated equipment will be finished in a pale grey colour, while equipment shelters will generally be a beige or pale green colour.</p>
<b>Site Information</b>	<p><b>Lot description:</b> Lot 43 on P007767</p> <p><b>Physical address:</b> 158 Tindal Avenue, Beeliar WA 6164</p>
<b>Planning Considerations</b>	<p><b>LGA:</b> City of Cockburn</p> <p><b>Zoning:</b> Rural Zone</p> <p><b>Overlays:</b> State Planning Policy 5.2: Telecommunications Infrastructure State Planning Policy 2.4: Planning for Basic Raw Materials</p>
<b>Applicant</b>	<p>Axicom Pty Ltd Level 1, 110 Pacific Highway St Leonards NSW 2065</p> <p>Contact Person: Daniel Prior Email: daniel.prior@indara.com Our Reference: 640144 Yangebup South</p>



## 1. INTRODUCTION

Axicom together with Vodafone and Optus, are proposing to install a new telecommunications facility, located at 158 Tindal Avenue, Beeliam WA 6164.

The facility will be owned by Axicom and will host both Vodafone and Optus telecommunications equipment. The proposed facility will provide improved Vodafone and Optus 3G and 4G services and establish 5G services in Beeliam and the surrounding areas.

The proposal includes the installation of one 30m monopole, upon which Vodafone and Optus telecommunications equipment will be installed.

Please note, Axicom and Australia Tower Network have recently merged and are in the process of rebranding to form its new entity; Indara Digital Infrastructure (Indara). As our legal name is still Axicom, this Town Planning Report reflects the current name, however the proposal plans have been updated to reflect the transition to the new merged entity.

## 2. BACKGROUND

### 2.1 Demand for Network Services

Axicom is Australia's leading independent owner and provider of shared wireless telecommunications infrastructure. Axicom build and manage telecommunications facilities that are shared by Australia's mobile carriers, government agencies and other wireless service providers. Axicom also provide site acquisition, town planning and engineering services to the telecommunications industry.

Vodafone are currently undertaking a significant expansion of their national mobile network across Australia, both to improve existing 3G and 4G services and establish 5G services. Axicom are working with Vodafone to deploy new infrastructure across Australia. This proposed facility is being deployed as part of Vodafone and Optus' Extended Joint Venture program, meaning it will provide both Vodafone and Optus services.

Access to high quality telecommunications coverage is vitally important to the local community. Nationally, mobile usage continues to trend upward:

- The number of Australians who have no landline phone, but exclusively rely on their mobile phone to make and receive calls, has increased to 60% of the adult population<sup>1</sup>, up from 51% in 2018-2019<sup>2</sup>.
- 99% of Australians use a mobile phone<sup>3</sup>, up from 96% in 2018-2019<sup>4</sup>.

<sup>1</sup> <https://www.acma.gov.au/publications/2020-12/report/mobile-only-australia-living-without-fixed-line-home>

<sup>2</sup> <https://www.acma.gov.au/publications/2020-02/report/communications-report-2018-19> "At a Glance – How We Engaged", p71

<sup>3</sup> <https://www.acma.gov.au/publications/2021-04/report/communications-and-media-australia-how-we-communicate>

<sup>4</sup> <https://www.acma.gov.au/publications/2020-02/report/communications-report-2018-19> "At a Glance – Our Access to Services"



- The volume of data downloaded by mobile handset increases significantly each year, as more Australians use their phones to browse the internet and stream content<sup>5</sup>. Between 2018 and 2019, for example, the amount of data downloaded by phone handset increased by over 121%<sup>6</sup>.
- In 2020, covid-19 significantly changed the way that Australians live, work and use mobile data. There has been exponential growth in data usage as more people spent more time living and working from home; according to ACMA, average data consumption increased approximately 50% between June 2018 and June 2020<sup>7</sup>.
- 75.4% of emergency calls were made from a mobile handset in 2018-2019<sup>8</sup>.

With an influx of residential development and increasing population density, Beeliar and the surrounding suburbs have been identified as areas of high network demand. Both Vodafone and Optus have identified a need to improve mobile coverage and network capacity in this area.

As shown in **Figure 1**, there are no existing Vodafone or Optus base stations specifically servicing this area – with the closest facility servicing both networks located at 161 Henderson Road, Munster – approximately 900m southeast from the proposed facility (RFNSA: 6166001). While this facility is capable of covering the areas closest to it, it is too far away, and poorly positioned from a technical perspective, to reliably service the new residential estates in Beeliar to the northwest.

With the area already having service issues, Vodafone and Optus both consider there is significant social, economic and safety impetus to improve mobile coverage and network capacity in this location.

### 3. CANDIDATE SELECTION

#### 3.1 Upgrade and Co-Location Opportunities

Before proposing a new base station, Vodafone and Optus attempt to resolve service issues by reconfiguring or upgrading their existing base stations. If upgrades will not resolve service issues, Vodafone and Optus will consider any opportunities to co-locate on an existing mobile facility, building or other structure. If there are no co-location opportunities, Vodafone and Optus will proceed to deploy a new 'greenfield' base station.

In this instance, the closest facility is located approximately 900m southeast from the proposed facility location. The existing facility (RFNSA: 6166001) is a 35m monopole that currently hosts both Vodafone and Optus telecommunications equipment. However, due to the large spatial separation between this facility and the targeted coverage area, upgrades to this facility cannot resolve service issues in and around Beeliar.

As there are no viable opportunities for co-location in proximity to the targeted coverage area, or to upgrade an existing facility to meet service demand, Vodafone and Optus have identified that a new

<sup>5</sup> <https://www.acma.gov.au/publications/2021-05/report/communications-and-media-australia-how-we-use-internet> and

<https://www.acma.gov.au/sites/default/files/2021-05/Accessibility%20file%20-%20How%20we%20use%20the%20internet.csv>

<sup>6</sup> <https://www.acma.gov.au/publications/2020-02/report/communications-report-2018-19> "Volume of broadband and mobile data downloaded", p11

<sup>7</sup> <https://www.acma.gov.au/publications/2021-04/report/communications-and-media-australia-supply-and-use-services-2019-20>

<sup>8</sup> <https://www.acma.gov.au/publications/2020-02/report/communications-report-2018-19> "3.1 Emergency Call Service", p102



base station is required and are working with Axicom to deploy a new telecommunications facility in the area.

The proposed facility should provide coverage to significant portions of the following residential estates, identified in the following structure plans:

- Revised Meve Estate Beeliar Local Structure Plan
- Cell 10 Beeliar Local Structure Plan
- L81 (Formerly L2) Fanstone Avenue Beeliar Local Structure Plan
- Cell 6 Stock Road, Beeliar Drive Yangebup Structure Plan

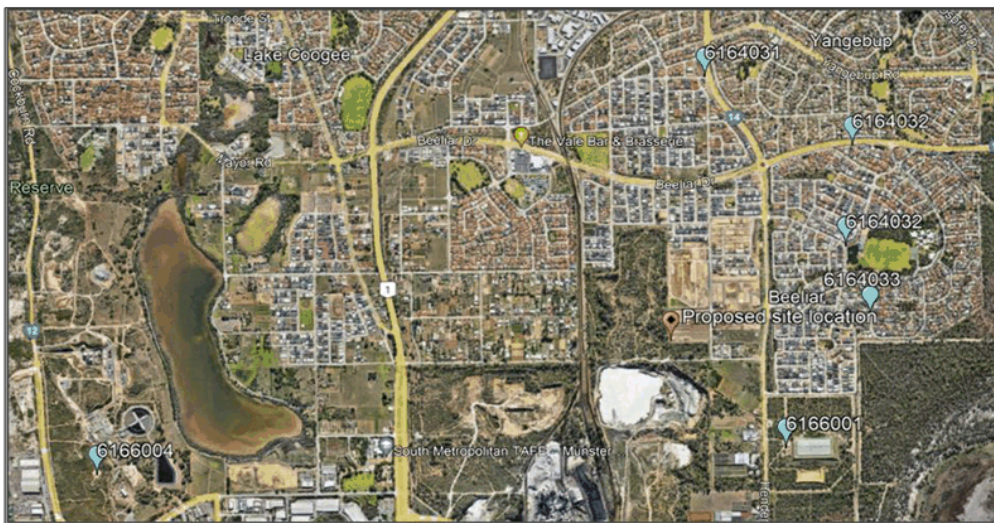


Figure 1: Proposed facility location in relation to the existing sites in the surrounding locality (Google Earth Pro).

### 3.2 Greenfield Opportunities

Axicom has assessed a number of potential candidate sites in the Beeliar area, as follows.

Site Address	Comments
Lot 305 Honey Myrtle Avenue Beeliar WA 6164 (Lot 305 on P421228)	New 35m monopole.  From a technical perspective, this candidate was able to adequately meet the coverage objectives for both Vodafone and Optus. However, due to the location in a vegetated area, the proposal was deemed to have an unacceptable environmental impact. As such, this candidate was not progressed further.
No address noted (Lot 82 on P413024)	New 35m monopole.  From a technical perspective, this candidate was able to adequately meet the coverage objectives for both Vodafone and Optus. However, due to the location in a vegetated area, the proposal was deemed to have an unacceptable environmental impact. As such, this candidate was not progressed further.



Site Address	Comments
Lot 7 Fancote Avenue Beeliar WA 6164 (Lot 7 on P027166)	New 35m monopole.  From a technical perspective, this candidate was able to adequately meet the coverage objectives for both Vodafone and Optus. However, due to the location in a vegetated area and proximity to a new playground, the proposal was deemed to have an unacceptable visual and environmental impact. As such, this candidate was not progressed further.
Atop Beeliar Reserve 81 McLaren Avenue Beeliar WA 6164 (Lot 301 on P415108)	New 35m monopole.  From a technical perspective, this candidate was able to adequately meet the coverage objectives for both Vodafone and Optus. However, the site has been redeveloped into a new community reserve to serve the recreation needs of the surrounding community, accordingly a facility in this location was considered to have an unacceptable visual impact. As such, this candidate was not progressed further.
Lot 450 Rockingham Road Munster WA 6166 (Lot 450 on P249735)	New 35m monopole.  From a technical perspective, this candidate was unable to adequately meet the coverage objectives for both Vodafone and Optus. As such, this candidate was not progressed further.
118 East Churchill Avenue Beeliar WA 6164 (Lot 15 on D074469)	New 35m monopole.  From a technical perspective, this candidate was able to adequately meet the coverage objectives for both Vodafone and Optus. However, contact was unable to be established with the landowner to enter into a lease agreement for the proposal. As such, this candidate was not progressed further.
114 East Churchill Avenue Beeliar WA 6164 (Lot 147 on P003563)	New 35m monopole.  From a technical perspective, this candidate was able to adequately meet the coverage objectives for both Vodafone and Optus. However, contact was unable to be established with the landowner to enter into a lease agreement for the proposal. As such, this candidate was not progressed further.
Beeliar Hive Shops 283 Beeliar Drive Beeliar WA 6164 (Lot 2 on P412565)	New 35m monopole.  From a technical perspective, this candidate was able to adequately meet the coverage objectives for both Vodafone and Optus. However, contact was unable to be established with the landowner to enter into a lease agreement for the proposal. As such, this candidate was not progressed further.
58 Tindal Avenue Yangebup WA 6164 (Lot 1001 on P053867)	New 35m monopole.  From a technical perspective, this candidate was able to adequately meet the coverage objectives for both Vodafone and Optus. However, due to the proximity to housing, the proposal was considered to have an unacceptable visual impact. As such, this candidate was not progressed further.



Site Address	Comments
158 Tindal Avenue Beeliar WA 6164 (Lot 43 on P007767)	New 35m monopole.  From a technical perspective, this candidate was able to adequately meet the coverage objectives for both Vodafone and Optus. Additionally, this candidate is well separated from sensitive land uses found within the locality and had a minor environmental impact as the land has previously been cleared of vegetation. Furthermore, the landowner was amenable to the proposal and willing to enter into a lease agreement. As such, this candidate was chosen as Axicom’s prime candidate and is the subject of this assessment.

**4. SITE CONTEXT**

The subject site is located at 158 Tindal Avenue, Beeliar and is formally known as Lot 43 on P007767. The proposed facility will host both Vodafone and Optus equipment and provide telecommunications services for both carriers to residents and businesses located in Beeliar and the surrounding areas.

The site is located on a large allotment retaining Rural zoning under the Cockburn Planning Scheme. The site is a largely undeveloped rural lot, which is currently a working farm. The land to the north and east is predominantly comprised of new residential estates, with a significant level of new development currently in progress. To the south and west are the Cockburn Cement Munster quarry and a vegetation corridor respectively, which affords a significant visual buffer from residences to the south and west. Refer to **Figure 2** to **Figure 4**.



**Figure 2: Proposed facility location and surrounding locality (Nearmap).**



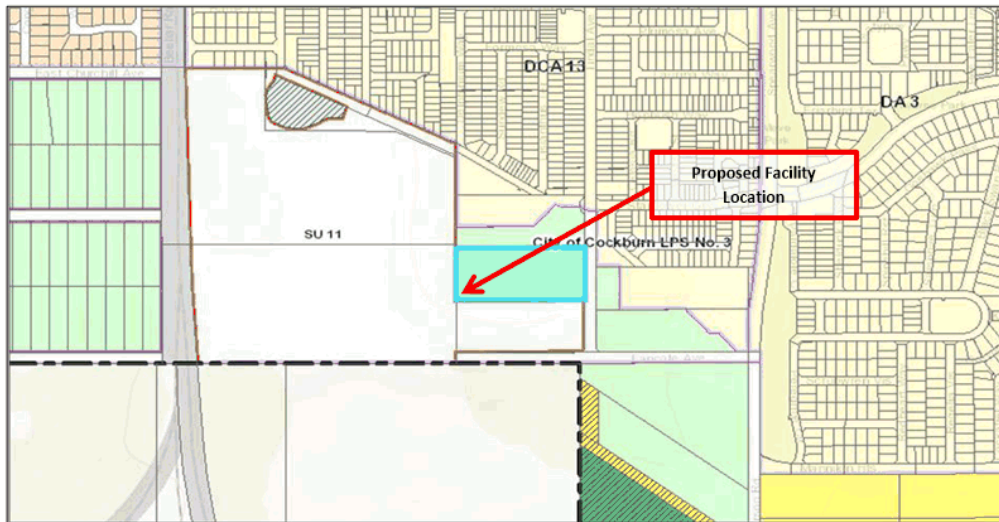


Figure 3: Zoning Map (PlanWA).

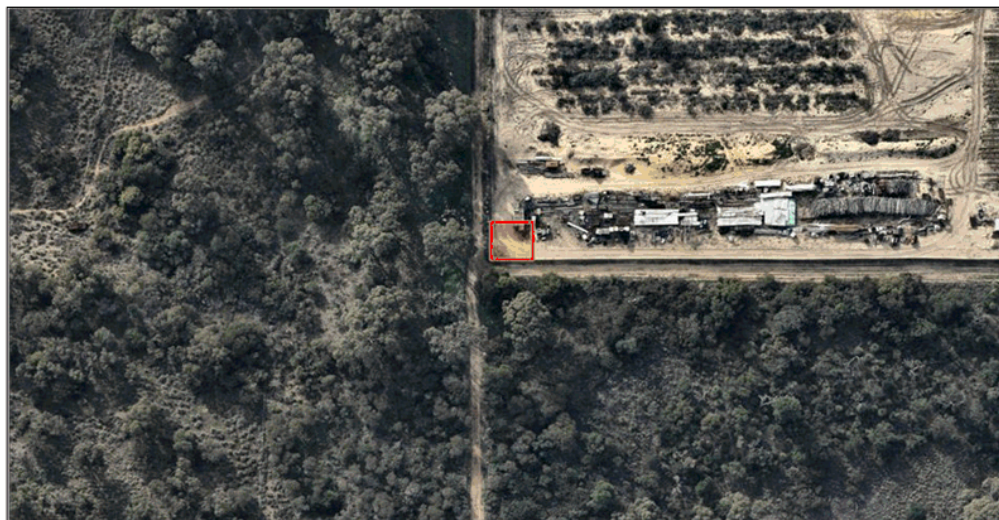


Figure 4: Proposed facility location within the subject property, as outlined in red (Nearmap).



## 5. PROPOSED WORKS

### 5.1 Equipment to be Installed

The proposed works consist of:

- A new 30m monopole;
- The installation of a new triangle headframe on the proposed monopole;
- The installation of fifteen (15) new panel antennas at the following centreline heights:
  - Three (3) new panel antennas at a centreline height of 29.5m;
  - Twelve (12) new panel antennas at a centreline height of 28.5m;
- The installation of twenty-one (21) new remote radio units (RRUs) mounted on the proposed headframe;
- The installation of a 6-bay equipment cabinet on a concrete footing within the proposed Axicom compound;
- Upgrade of the existing access track to be suitable for construction and ongoing maintenance of the proposed facility; and
- The installation of ancillary equipment associated with the safety and proper function of the facility, including cable trays, cabling, meter panel, submains, security fencing, antenna mounts, group meter panel etc.

The monopole and associated equipment will be finished in a pale grey colour, while equipment shelters will generally be a beige or pale green colour.

Additionally, Axicom, as a neutral host, design and construct telecommunications facilities with the potential to accommodate additional carriers and other communications providers, such as government entities and wireless service providers, at a future time if required, minimising the proliferation of telecommunications facilities being deployed in Beelihar.

Refer **Appendix 1** for proposal plans.

### 5.2 Site Access and Parking

The property will be accessed via the existing access track off Tindal Avenue.

The existing access gate will be realigned, one metre from the property boundary fence line, in line with the existing access track running along the southern property boundary. A new 6m wide crossover will be installed to facilitate access.

There is adequate space within the subject lot for construction staging and parking. Once the facility is operational, it will require access only 2-4 times annually for routine maintenance, during which a four wheel drive vehicle is normally used. The facility will otherwise operate on an unmanned basis, and will not generate significant vehicle traffic.



### 5.3 Acoustics

Minor noise impacts could be expected at the construction phase and will only be of a brief duration, and any conditions concerning hours of operation will be complied with.

Once operational, the facility will not be a significant generator of noise. The only part of the facility that generates noise are the open loop fresh air-cooling fans mounted on the doors of the proposed outdoor units.

These do not operate continuously and only turn on when required. Noise emitted is comparable to a domestic air conditioner, in compliance with background levels prescribed by Australian Standard AS1055. The project is not expected to cause a noise nuisance.

### 5.4 Utilities, Stormwater and Drainage

Axicom propose to connect the facility to mains power from the existing power pole on Tindal Avenue, approximately 200m east of the proposed facility location.

The proposed facility will operate on an unmanned basis and does not require connection to reticulated water or sewerage, nor will it emit any dust, heat, smoke, gaseous plumes, particulates, or generate waste.

Axicom have not prepared a stormwater or drainage plan for this project. Telecommunications facilities do not generally require specific stormwater or drainage works, because they have only a small footprint with minimal hardstand surfacing, and thus generate minimal runoff. The tower's foundation will be below the ground surface and there will be no hardstand surfacing (paved surface/concrete etc). As a result, the facility will have negligible impact on stormwater runoff and drainage.

## 6. LEGISLATIVE CONTEXT

### 6.1 Commonwealth Legislation

#### 6.1.1 Telecommunications Act 1997 and Telecommunications (Low-Impact Facilities) Determination 2018

The *Telecommunications Act 1997* regulates telecommunications carrier activities, and gives certain powers to carriers to undertake maintenance and installation works. The *Telecommunications (Low-Impact Facilities) Determination 2018* (LIFD) provides that certain proposals are 'Low Impact' and do not require development approval, providing they fall within the parameters of the Determination.

As the proposal involves the establishment of a new tower, which is not permissible as a 'Low Impact' activity, Council development consent is required.



### 6.1.2 C564:2020 Mobile Phone Base Station Deployment Code

The Communications Alliance Limited *C564:2020 Mobile Phone Base Station Deployment Code* (the Deployment Code) is an industry code of practice registered by the Australian Communications and Media Authority. It applies to all licenced telecommunications carriers. The Deployment Code sets guidelines for site selection, community consultation, design, installation and operation of telecommunications facilities.

It is also a requirement of the Code that an ARPANSA EME compliance report be prepared for all new mobile base stations. A copy of the ARPANSA EME Report is enclosed in **Appendix 2**.

Sections 4.1 and 4.2 are relevant to this proposal. These sections require completion of precautionary approach checklists for site selection, infrastructure design and site operation. The proposal has been sited and designed with consideration for these principles. It is a requirement of the WA State Planning Policy (SPP) 5.2 – Telecommunications that compliance with the Deployment Code is addressed. Accordingly, copies of the checklists are available in **Appendix 3**.

### 6.1.3 Environmental Protection and Biodiversity Conservation Act 1999

This proposal is not likely to have, an impact on any matter of 'national environmental significance'.

## 6.2 State Legislation

### 6.2.1 Planning and Development Act 2005

The *Planning and Development Act 2005* establishes the planning and development system framework in Western Australia.

The project is considered to be 'Development' as defined by Section 4 of the Act:

- (a) Any demolition, erection, construction, alteration of or addition to any building or structure on the land;
- (b) The carrying out on the land of any excavation or other works;
- (c) In the case of a place to which a protection order made under the Heritage Act 2018 Part 4 Division 1 applies, any act or thing that –
  - (i) Is likely to change the character of that place or the external appearance of any building; or
  - (ii) Would constitute an irreversible alteration of the fabric of any building.

### 6.2.2 Metropolitan Region Scheme

As noted in Clause 5 of the Metropolitan Region Scheme (MRS), the Western Australian Planning Commission (WAPC) is the responsible authority for the carrying out of the scheme, except where it has been delegated to another authority in accordance with Section 16 of the *Planning and Development Act 2005*.



In this instance, authority was delegated to the City of Cockburn upon the gazetting of the City of Cockburn Town Planning Scheme No. 3 (Cockburn TPS) on 14 February 1992. Refer to Section 6.3 for further information.

### 6.2.3 State Planning Policy 3.7: Planning in Bushfire Prone Areas

The site is identified as being within a Bushfire Prone area, as such SPP3.7 applies.

The site has not been formally assessed for bushfire risk. The proposal is for an unmanned utility structure in an area that is experiencing significant residential expansion, with four nearby fire stations (Cockburn Volunteer Emergency Services, Cockburn Central CFRS Fire Station, Hope Valley Fire Station and Mandogalup Fire Station), which are anticipated to be within a seven to twelve minute drive of the proposed site location. As such, the facility itself is not considered to result in additional risk of bushfire in the area (such as through emission of excessive heat or open flame).

The entirety of the premises is subject to bushfire risk; this site is considered to be the most favourable location on premises for a new telecommunications facility despite its bushfire classification.

Pursuant to 6.7.2 of the SPP, the proposed facility is also considered “unavoidable development” – the location of the facility has been chosen because it best balances technical, visual and environmental outcomes, as well as being in a position preferred by the landowner to minimise disruptions to the operation of their farm. The facility will also significantly strengthen mobile coverage in the event of a natural disaster.

Axicom can, at Council’s request, seek a bushfire assessment to assist with assessment of the project.

### 6.2.4 State Planning Policy 5.2: Telecommunications Infrastructure

The proposal is considered to be compliant with the objectives of SPP5.2 – Telecommunications Infrastructure, an assessment of the proposal is provided in **Appendix 4**.

### 6.2.5 State Planning Policy 2.4: Planning for Basic Raw Materials

The proposal is not an extractive industry enterprise. As such, it has not been assessed against the objectives of SPP2.4 - Planning for Basic Raw Materials.

## 6.3 Local Planning Legislation – City of Cockburn Town Planning Scheme No. 3

### 6.3.1 Rural Zone

The site is subject to the requirements of the Cockburn TPS and retains a Rural zoning.

As telecommunications facilities are not listed within Table 1 – Zoning Table, it is considered to be a ‘Uses not listed’ and therefore subject to the provisions of Clause 3.4.2. Subject to the proposal being consistent with the objectives of the particular zone, Council, exercising its discretionary powers, may approve the proposal under Cockburn TPS.

An assessment of the proposal against the objectives of the zone is provided in **Appendix 5**.



## 7. ENVIRONMENTAL IMPACTS

### 7.1 Emissions and Health

Whilst Axicom does not operate its own telecommunications network, they provide the means for Australia's mobile carriers, government agencies and wireless service providers to do so. Axicom acknowledges that people are genuinely concerned about the safety of electromagnetic energy (EME) from mobile phone base stations.

As 5G networks start to be rolled out across Australia, there has been heightened community sensitivity regarding telecommunications infrastructure. Vodafone and Optus rely on the advice of the Australian government, other international governments, and peak health bodies such as the World Health Organization (WHO). These organizations take the view, based on the latest scientific consensus, that mobile base stations are safe.

#### **World Health Organization**

*Despite extensive research, to date there is no evidence to conclude that exposure to low level electromagnetic fields is harmful to human health.*

<https://www.who.int/peh-emf/about/WhatisEMF/en/index1.html>

*Studies to date provide no indication that environmental exposure to RF fields, such as from base stations, increases the risk of cancer or any other disease.*

<https://www.who.int/features/qa/30/en/>

#### **The Australian Government**

*Mobile phone networks and other wireless telecommunications emit low-powered radio waves also known as radiofrequency (RF) electromagnetic energy (EME). This is different to ionising radiation associated with nuclear energy or use in medicine. The radio waves to which the general public is exposed from telecommunications are not hazardous to human health.*

<https://www.health.gov.au/news/safety-of-5g-technology>

*There is no substantiated evidence that EME causes any harm to humans when it is below the limits specified in Australian safety standards.*

<https://www.infrastructure.gov.au/sites/default/files/safety-of-mobile-phone-networks-factsheet.pdf>

*Health authorities around the world, including ARPANSA and the World Health Organization, have examined the scientific evidence regarding possible health effects from base stations. Current research indicates that there are no established health effects from the low exposure to the RF EME from mobile phone base station antennas.*

<https://www.arpansa.gov.au/understanding-radiation/radiation-sources/more-radiation-sources/mobile-phone-base-stations>

*Current research indicates that there is no established evidence for health effects from radio waves used in mobile telecommunications.*



[https://www.arpansa.gov.au/sites/default/files/arpansa\\_submission\\_to\\_inquiry\\_into\\_5g\\_in\\_australia\\_1.pdf](https://www.arpansa.gov.au/sites/default/files/arpansa_submission_to_inquiry_into_5g_in_australia_1.pdf)

All mobile carriers must abide by the *Radiation Protection Series S-1 – Standard for Limiting Exposure to Radiofrequency Fields – 100kHz to 300GHz (2021)*, a safety standard prepared by the Australian Radiation Protection and Nuclear Safety Agency (ARPANSA), based on the recommendations of ICNIRP (International Commission for Non-Ionising Radiation Protection). The Australian Communications and Media Authority (ACMA) regulate compliance with the standard. The safety standard applies to all mobile frequencies currently used in Australia, including 3G, 4G and 5G.

The Standard operates by placing a limit on the strength of the signal (or RF EME) that mobile carriers can transmit to and from any network base station. The environmental standard restricts the signal strength to a level low enough to protect all people at all times. It has a significant safety margin, or precautionary approach, built into it.

An ARPANSA format compliance report has been prepared and is attached. The compliance report demonstrates the maximum signal strength of a proposed facility, assuming that it is handling the maximum number of users 24-hours a day.

This proposal will comply with the ACMA mandated exposure standard. Maximum EME levels from this facility will equate to **3.41%** of the standard.

Note mobile base stations are designed to operate at minimum, not maximum, power levels at all times. The facility will only operate at a level necessary to accommodate the number of customers using the facility at any one time. Accordingly, while EME levels from the facility cannot exceed **3.41%** of the standard, they will generally be much lower than this level. The ARPANSA EME report is attached for Council's reference.

Refer to **Appendix 2** for a copy of the ARPANSA EME Report.

## 7.2 Environment and Biodiversity

The subject lot has been extensively cleared of native vegetation to establish the farm. As no vegetation removal is required to establish the proposed facility and access track realignment, this proposal is not expected to have an adverse impact on the local environment.

Landscaping is not currently proposed, as the proposed facility needs to be clear of vegetation for safety and operational purposes.

## 7.3 Heritage

The subject lot is not identified as being heritage listed, however, should any artefacts be identified during build, works will cease, and the appropriate additional investigations undertaken.



## 7.4 Natural Hazards

### 7.4.1 Flooding

The subject lot is not identified as being subject to flooding.

### 7.4.2 Bushfire

The subject lot is identified as being bushfire prone, refer to Section 6.2.3 for further information.

## 7.5 Visual Impact

Telecommunications facilities, by their nature, must be tall enough to protrude above the surrounding environment to function. Axicom acknowledge the facility will be visible from a number of perspectives within the area – however, given the separation from surrounding residences and community sensitive land uses, any amenity impacts are considered appropriate in this context.

At this location, a 30m monopole is the minimum height required to meet the targeted coverage objectives for both Vodafone and Optus. Axicom, as a neutral host, design and construct telecommunications facilities with the ability to accommodate additional carriers in the future. While the proposed height of this facility is required in order to achieve Vodafone and Optus' service objectives, the facility will be designed to accommodate additional carriers and other communications providers, such as government entities and wireless service providers, at a future time if required – thus minimising the likelihood of additional base stations being deployed in this area.

With regards to the design of the facility, certain measures have been taken to ensure visual impact is mitigated as much as practicable, these include:

- Use of a monopole is proposed. Monopoles are considered to be a sympathetic inclusion to the environment when compared to other structure types, such as lattice towers, because of their slimmer profile.
- To minimize visual bulk, Vodafone and Optus antennas will be mounted on a single headframe, reducing visual impact compared with separate headframes at different heights.
- The facility has been sited in a rural area to maximise the separation from residences and community sensitive land uses. Note the proposed facility is located approximately 250m from the nearest residence and was the best location for a facility in the area, given the significant residential growth that is occurring at this time.
- The monopole and associated equipment will be finished in a pale grey, which is considered to be the most sympathetic finish with regards to blending into the natural landscape in all weathers. It will also match other existing vertical elements in the area, such as light poles and power poles. The 6-bay equipment cabinet will generally be a beige or pale green colour. However, Axicom will considered an alternate colour scheme if requested by Council.





As shown in **Figure 2** to **Figure 4**, the Cockburn Cement Munster quarry and nearby vegetation afford a significant visual buffer from residences to the south and west, though it is noted that the top of the monopole and headframe may protrude above the tree line and be visible from certain vantage points in the locality. However, due to the geographic separation the facility is unlikely to be a visual focal point.

The facility has been located in the southwest corner of the lot, to afford the maximum separation possible, from the residential properties to the north and east. Several indicative photomontages have been prepared to provide a visual representation of the amenity impacts of a monopole in this location, refer to **Appendix 6**.

Overall, Axicom do not consider the proposed facility to have an adverse visual impact.

## 8. CONCLUSION

Axicom are seeking consent to install a new mobile base station at 158 Tindal Avenue, Beeliar.

The new facility is proposed in order to resolve mobile network issues in Beeliar and the surrounding areas, improving mobile services for residents, businesses, community uses and emergency services in the area. Both Optus and Vodafone will be installing equipment on this facility.

The site is considered favourable from a planning perspective, noting its substantial separation from sensitive land uses and minimal visual and environmental impact.

Axicom consider that the proposed site represents the best possible outcome to provide necessary services to the Beeliar community. The proposal also broadly complies with the applicable planning requirements.

Given the significant public benefit afforded by the proposal, it is requested that consent be granted to undertake the project.



#### APPENDIX 4: STATE PLANNING POLICY 5.2: TELECOMMUNICATIONS INFRASTRUCTURE

4 Policy Objectives	
Objective	Comment
a) <i>Facilitate the provision of telecommunications infrastructure in an efficient and environmentally responsible manner to meet community needs.</i>	Complies. The facility is being deployed in an area that is currently experiencing significant residential growth and development. The location of the facility has been chosen because it best balances technical, visual and environmental outcomes, as well as being in a position agreeable with the landowner to minimise disruptions to the operation of their farm.
b) <i>Manage the environmental, cultural heritage, visual and social impacts of telecommunications infrastructure.</i>	<p>Project achieves this objective.</p> <ul style="list-style-type: none"> <li>• Environmental impact has been minimised as far as practical, noting the location needed to be balanced against technical, visual and environmental outcomes, as well as being in a position preferred by the landowner.</li> <li>• Cultural and heritage impacts are considered minimal. The facility is not in an area of heritage significance.</li> <li>• The facility will have social benefits for the local area through improved mobile and data services, without compromising local amenity.</li> <li>• Regarding visual impacts, telecommunications facilities, by their nature, must be tall enough to protrude above the surrounding environment to function. Axicom acknowledge the facility will be visible from a number of perspectives within the area – however, given the separation from surrounding residences and community sensitive land uses, any amenity impacts are considered appropriate in this context.</li> <li>• At this location, a 30m monopole is the minimum required to meet the targeted coverage objectives for both Vodafone and Optus. Axicom, as a neutral host, design and construct telecommunications facilities with the ability to accommodate additional carriers in the future. While the proposed height of this facility is required in order to achieve Vodafone and Optus' service objectives, the monopole will be designed with sufficient capacity to accommodate additional carriers and other communications providers, such as government entities and wireless service providers, at a future time if required – thus minimising the likelihood of additional base stations being deployed in this area.</li> </ul> <p>With regards to the design of the facility, certain measures have been taken to ensure visual impact is mitigated as much as practicable, these include:</p> <ul style="list-style-type: none"> <li>○ Use of a monopole is proposed. Monopoles are considered to be a sympathetic inclusion to the environment when compared to other structure types, such as lattice towers, because of their slimmer profile.</li> </ul>



	<ul style="list-style-type: none"> <li>○ To minimize visual bulk, Vodafone and Optus antennas will be mounted on a single headframe, reducing visual impact compared with separate headframes at different heights.</li> <li>○ The facility has been sited in a rural area to maximise the separation from residences and community sensitive land uses. Note the proposed facility is located approximately 250m from the nearest residence.</li> <li>• The monopole and associated equipment will be finished in a pale grey, which is considered to be the most sympathetic finish with regards to blending into the natural landscape in all weathers. It will also match other existing vertical elements in the area, such as light poles and power poles. The outdoor cabinets will generally be a pale green colour. However, Axicom will consider an alternate colour scheme if requested by Council.</li> </ul>
<p><i>c) Ensure that telecommunications infrastructure is included in relevant planning processes as essential infrastructure for business, personal and emergency reasons.</i></p>	<p>Objectives are more applicable to Council decision-making rather than to the Axicom proposal.</p>
<p><i>d) Promote a consistent approach in the preparation, assessment and determination of planning decisions for telecommunications infrastructure.</i></p>	
<p><b>5 Policy Measures</b></p>	
<p><b>5.1 Visual Impacts</b></p>	
<p><b>For telecommunications infrastructure to be effective, structures are generally located prominently, at high points in the landscape or on top of buildings, where they are more likely to be visible to the public. The planning authority may exercise discretion in addressing the visual impacts of telecommunications infrastructure. Visual impacts of an infrastructure development proposal should be assessed by applying the following set of policy measures to guide the location, siting and design of the structure</b></p>	
<p><b>5.1.1 The benefit of improved telecommunications services should be balanced with the visual impact on the surrounding area.</b></p>	
<p><b>Measure</b></p>	<p><b>Comment</b></p>
<p><i>i) Assessment of the visual impact of development proposals for telecommunications infrastructure should be made on a case by case basis.</i></p>	<p>From a technical perspective, telecommunications facilities must be tall enough to protrude above the surrounding environment in order to function. Axicom acknowledge the facility will be visible from a number of perspectives within the area, however we consider that visual impact is appropriate within the context of the surrounding area.</p>
<p><i>ii) Telecommunications infrastructure should be sited and designed to minimise visual impact and whenever possible:</i></p>	<p>Complies. The location of the facility has been specifically chosen because of its separation from the surrounding residential development. Note, due to the significant residential growth, this candidate was considered the most appropriate for the area, as it was the only one that afforded a level of separation from the nearest houses.</p>
<p><b>5.1.1 The benefit of improved telecommunications services should be balanced with the visual impact on the surrounding area.</b></p>	
<p><b>Measure</b></p>	<p><b>Comment</b></p>
<p><i>a) Be located where it will not be prominently visible from significant viewing locations such as scenic routes, lookouts and recreation sites.</i></p>	<p>Complies. The proposed facility is not in the vicinity of any significant viewing locations such as scenic routes or lookouts. While the facility will be visible from Honeymyrtle Park, the facility has been located as far from the park as practical.</p>



<p>b) Be located to avoid detracting from a significant view of a heritage item or place, a landmark, a streetscape, vista or a panorama, whether viewed from public or private land.</p>	<p>Complies. The proposed facility has been sited in an area well away from local landmarks, heritage items or significant vistas. Additionally, the facility has been located as far from residential properties as practical.</p>
<p>c) Not be located on sites where environmental, cultural heritage, social and visual landscape values may be compromised.</p>	<p>Complies. The location of the facility has been chosen because it best balances technical, visual and environmental outcomes, as well as being in a position agreeable to the landowner to minimise disruptions to the operation of their farm.</p>
<p>d) Display design features, including scale, materials, external colours and finishes that are sympathetic to the surrounding landscape.</p>	<p>Complies. The facility will be finished in a pale grey, which is considered to be the most sympathetic finish with regards to blending into the natural landscape in all weathers. It will also match other existing vertical elements in the area, such as light poles and power poles. However, Axicom will consider an alternate colour scheme if requested by Council.</p>
<p>iii) In addition to the existing exemptions under the Telecommunication Act, local governments should consider exempting telecommunications infrastructure from the requirement for development approval where:</p> <p>a) The infrastructure has a maximum height of 30 metres from finished ground level.</p> <p>b) The proposal complies with the policy measures outlined in this policy.</p> <p>c) The proponent has undertaken notification of the proposal in a similar manner to 'low impact facilities' as defined and set out in the Mobile Phone Base Station Deployment Industry Code (C564:2011).</p>	<p>Not applicable, noting that the facility will require development consent.</p>
<p>iv) Telecommunications infrastructure should be located where it will facilitate continuous network coverage and/or improved telecommunications services to the community.</p>	<p>Complies. The facility has been located in a position to provide continuous network coverage and/or improved telecommunications services to the surrounding community.</p>
<p><b>5.1.1 The benefit of improved telecommunications services should be balanced with the visual impact on the surrounding area.</b></p>	
<p><b>Measure</b></p>	<p><b>Comment</b></p>
<p>v) Telecommunications infrastructure should be co-located and whenever possible:</p>	<p>Complies. While there were no existing structures on which to co-locate, this proposal specifically involves co-location, as two mobile carriers have already committed to co-locate on this facility.</p>
<p>a) Cables and lines should be located within an existing underground conduit or duct.</p>	<p>Complies as far as practicable. Where available and feasible, existing underground conduits will be utilised.</p>
<p>b) Overhead lines and towers should be co-located with existing infrastructure and/or within existing infrastructure corridors and/or mounted on existing or proposed buildings.</p>	<p>Complies as far as practicable. The proposal does not include installation of overhead cabling. The telecommunications tower is not co-located with infrastructure, but has been sited in a position where its visual impact is minimised as far as practical.</p>



6.3.1 Information to be submitted when lodging a development application	
Item	Comment
a) A report demonstrating compliance with the Mobile Phone Base Station Deployment Industry Code (C564:2011), excluding Sections 6 and 7 (which only apply to developments that do not require development approval).	<p>The current code is the C564:2020 Mobile Phone Base Station Deployment Code. Sections 4.1 and 4.2 of the Code, relating to site selection and design, are applicable to the proposal. An assessment of these items is included in <b>Appendix 3</b>.</p> <p>Should Council allow this project to proceed without DA consent, a consultation plan will be prepared and submitted to Council in accordance with Section 6 of the Code, which applies to new sites that do not require development approval.</p>
b) A statement and/or a map indicating the extent to which the proposed facility addresses the network capacity for future demand and/or current gaps in service.	Specific carrier information has not been provided, because this facility will service two carriers, however, <b>Figure 1</b> demonstrates that there is a gap in existing telecommunications infrastructure within this area.
c) A statement about the extent to which the proposed facility complies with any relevant local planning scheme or planning policy adopted under a scheme and (if applicable) justification for any variation from the relevant scheme or policy provisions.	Section 6 of this report addresses relevant legislative requirements.
d) Plans and coloured graphic illustrations, including photo simulations, showing the type of facility and its relationship with adjacent development, including the proposal's elevations showing the extent, height and appearance, proposed materials and colour, any screening or fencing, and any external lighting.	Proposal plans are attached to this application. A visual impact assessment has been undertaken, refer to Section 7 of this report.
e) Details of any significant environmental constraints, including those associated with the species, condition and significance of any vegetation to be removed.	Refer Section 7 of this report.
f) Map and a statement about where the proposed facility is to be located. If the facility is proposed within an infrastructure easement or corridor, consultation with other users is to be demonstrated.	Refer Sections 3 and 4 of this report.
g) A statement explaining how the proposed facility addresses the policy measures for the location, siting and design of telecommunications infrastructure set out in Section 5.1.1 of this Policy.	Refer Sections 6 and 7 of this report.



**APPENDIX 5: RURAL ZONE**

Note, the proposal has not been assessed against Clause 4.10 of Part 4 – General Development Requirements, as none of the criteria were applicable to a telecommunications facility.

<b>3.2 Objectives of the Zones; Rural Zones is subject to the following provisions:</b>	
<p><i>k) To provide for a range of rural pursuits which are compatible with the capability of the land and retain the rural character and amenity of the locality.</i></p>	<p>Complies. The facility will provide improved Vodafone and Optus 3G and 4G mobile and data services, and establish 5G services in Beeliar and the surrounding areas, delivering essential services to support the range of rural, and residential pursuits utilised by the community, noting the area is undergoing significant residential development at this time.</p> <p>Given the small scale of the development, the proposal is not considered to have an adverse impact on the rural character and amenity of the locality. The location of the facility has been chosen because it best balances technical, visual and environmental outcomes, as well as being in a position agreeable with the landowner to minimise disruptions to the operation of their farm.</p>

No.	Type of Submission	Submission	Officer Comments
1	Objection	<p>I submit opposition to the DA22/0860 - 158 Tindal Avenue, Beeliar development plan.</p> <p>This is a development that will further adversely impact property values in an area already bearing the strain of Cockburn Cement and their perpetual pollution (which they were found guilty of...again...but never properly rectify because the fine they pay is a fraction of the revenue they make); the high voltage overhead power lines also putting negative pressure on the land value; and the new industrial park proposal by Cockburn Cement for their northern border, which will cause Beeliar to become a rat-run to and from the site, not only affecting the housing market, but also presenting a safety hazard in an area with young families.</p> <p>It's as if City of Cockburn are determined to install as many industrial, unsightly, or unsafe infrastructure projects in this area as they can. If Cockburn Cement are so intent on using their northern boundary, they can sell it to this telecommunications company for the installation of the antenna. This removes the antenna from a housing area, Cockburn Cement are using their land, and the industrial park doesn't cause traffic flow issues through a built up area (which will absolutely happen, whether you think it will or not).</p> <p>What does the council hold against the residents of this area that these projects are constantly foisted upon us?</p>	<ul style="list-style-type: none"> <li>• Property values are not valid planning concerns</li> <li>• Proponent is not responsible for Cockburn Cement</li> <li>• Proposal is not related to the proposed Industrial Precinct</li> <li>• Applicant did consider Cockburn Cement's property (450 Rockingham Road, Munster) as a potential candidate site however this site was unable to adequately meet the coverage objectives for both Vodaphone and Optus</li> </ul>
2	Objection	<p>I am writing to express my strong concern over the proposed telecommunications infrastructure that has been submitted to Cockburn City Council, to be erected very close to our home. Not only does this concern pertain to my household, but to the many family members, friends and loved ones who live within close proximity to the proposed site on Tindal avenue. The concerns pertain not just to the unsightly aesthetics of having a telecommunications tower erected in a highly populated residential area, outside the front door of residents on Tindal avenue in direct line of sight from homes (mine and hundreds of others), but the obvious health risks that simply cannot be ignored.</p> <ul style="list-style-type: none"> <li>• The proposed site is not a safe distance from residents</li> <li>• It has been proven that people who live within 400m of a telecommunications tower, tend to develop cancers</li> <li>• Studies show that there are both short-term and long-term risks in living 300-400m of a tower</li> <li>• Radiation beams coming from the tower means that residents will be constantly exposed to radio frequency radiation (antenna signals, tower to tower communications etc)</li> <li>• Radio frequency waves have adverse health effects and the erection of this tower will bring with it additional imposed risks on residents that were not there at the time that they made an "informed" decision on where to build or purchase their home</li> </ul>	<ul style="list-style-type: none"> <li>• The monopole will be located over 200m away from the nearest dwelling</li> <li>• The maximum EME level calculated for the proposal is 3.41% of the maximum prescribed public exposure limit</li> <li>• Shielding or covering telecommunication towers or locating them away from populated areas diminishes their effectiveness and will result in the need for more</li> </ul>

		<ul style="list-style-type: none"> <li>• Consider how many residents impacted by the erection of this tower, would have proceeded with their choice to live where they live if the tower had been there at the time? The World Health Organisation (WHO) has declared that radio frequency radiation (RFR) to be cancer causing since 2011</li> <li>• RFR has been associated with the same level of carcinogens as lead, sulphur fumes and vehicle exhausts</li> <li>• Radiation also poses risks to pregnancy (EMF – electromagnetic frequency radiation) whereby mothers to be and children younger than 10 years of age are critically susceptible to ill effects of EMF radiation</li> <li>• Where antennas point as well as the number of antennas is critical. RFR is greater if the antennas are facing you and the greater the number of antennas, the higher the degree of radiation incident to you</li> <li>• Having a tower in direct line of sight to residents on Tindal avenue as well as neighbouring residents including myself, will result in a higher degree of radiation. There are no natural barriers, ie. Stone, rock, dense foliage etc. It begs the question of what sort of shielding procedures will be in place</li> <li>• Before any consideration can be made by Cockburn City Council, the questions posed to the applicant should be around the health and well-being of residents and what measures will be taken to mitigate radiation In conclusion, residing near a telecommunications tower is simply NOT HEALTHY and it is NOT SAFE. I personally do not accept my health or that of my family or loved ones, being put at risk or left in the hands of a Corporate giant. I believe it would be completely irresponsible and an act of criminal negligence on the part of Cockburn City Council, to allow such a structure to be erected by ignoring a known and obvious risk and disregarding the lives and safety of its residents. I thank you for the opportunity to give my feedback and thank you for your consideration in advance.</li> </ul>	<p>in order to maintain adequate coverage levels.</p> <ul style="list-style-type: none"> <li>• Electromagnetic Energy (EME) from telecommunications is subject to strict regulations in Australia. The Australian Communications and Media Authority (ACMA) enforces these regulations. EME from telecommunications is not hazardous to human health. It's non-ionising. This means it doesn't have enough energy cause harm</li> </ul>
3	Non-Objection	No comment	
4	Non-Objection	No Comment	
5	Objection	It is going to look ugly for the area and can have a impact (sic) on my property value, it is a no no (sic) thank you	<ul style="list-style-type: none"> <li>• The subject site features a small hill that rises roughly 4m from the road, behind which this monopole is proposed to be placed, at a distance of over 200m to the road reserve. This will diminish the visual impact.</li> <li>• Property values are not valid planning concerns</li> </ul>



6	Objection	Devalue my property and its a (sic) eyesore	<ul style="list-style-type: none"> <li>As per submission 5</li> </ul>
7	Non-objection	Please make pole look like a tree. Thanks !	<ul style="list-style-type: none"> <li>Making this look like a tree may make the development look more out of place</li> </ul>
8	Objection	I have seen your proposal for the telecommunication facility for Optus and Vodafone. I am strongly going to decline on the proposal as it will drop the value off my property. No one will want to live near a 30m tall ugly Optus tower just to get better phone reception that we don't struggle with currently. There is absolutely no value to any residents in the Atop Estate to have this tower. Thank you	<ul style="list-style-type: none"> <li>As per submission 5</li> </ul>
9	Non-objection	No comment	
10	Non-Objection	No comment	
11	Objection	This proposed tower will pose a risk to our health. They are not safe and emit dangerous levels of radiofrequency. In the long term, 5G is a huge danger to our health. I object to this proposal and suggest more research is done on the dangers from non-conventional places that tell the truth rather than government sites that do not. Thank you.	<ul style="list-style-type: none"> <li>The maximum EME level calculated for the proposal is 3.41% of the maximum prescribed public exposure limit</li> <li>With an increase of residential development, Beeliar and the surrounding suburbs have been identified as areas of high network demand.</li> <li>EME from telecommunications is subject to strict regulations in Australia. The Australian Communications and Media Authority (ACMA) enforces these regulations. EME from telecommunications is not</li> </ul>

			<p>hazardous to human health. It's non-ionising. This means it doesn't have enough energy cause harm</p>
12	Objection	<p>I live around the corner from this address 158 Tindal Ave and I would not like this to be outside my address. I think it is definitely an eyesore as it is now. Have these residents been well advised as to what is happening? Please find a bushland for this.</p>	<ul style="list-style-type: none"> <li>• Placing these structures in the bush would diminish their effectiveness</li> <li>• The subject site features a small hill that rises roughly 4m from the road, behind which this monopole is proposed to be placed, at a distance of over 200m to the road reserve. This will diminish the visual impact.</li> </ul>
13	Objection	<p>My family and I strongly object to the proposal for a couple of reasons. Firstly, electromagnetic radiation causing health effects to us all and secondly, we believe this would be an eyesore in the area and we believe would [it] would decrease the value to our property</p>	<ul style="list-style-type: none"> <li>• EME from telecommunications is subject to strict regulations in Australia. The Australian Communications and Media Authority (ACMA) enforces these regulations. EME from telecommunications is not hazardous to human health. It's non-ionising. This means it doesn't have enough energy cause harm</li> <li>• Property values are not valid planning concerns</li> </ul>
14	Objection	<p>The proposal of a 30-meter monopole tower 260 meters from my property has raised several serious concerns. Firstly, the increase in electromagnetic radiation could potentially pose health risks to those who live near it and other health problems such as headaches, fatigue, and sleep disturbances. Additionally, the tower could harm</p>	<ul style="list-style-type: none"> <li>• As per submission 13</li> <li>• The site is not an ecologically significant site</li> </ul>

		local wildlife, including insects and bird populations negatively impacting the delicate balance of the ecosystem. Furthermore, the unsightly tower would likely have a negative impact on the aesthetic appeal of the area, leading to a definite decrease in the value of my property and surrounding homes. As a homeowner within close range of this monopole tower, I STRONGLY OBJECT to the proposal	
15	Non-objection	No comment	
16	Objection	It would be an eyesore from our property. Cockburn cement has greatly impacted our quality of life in Beeliar and another ugly structure would be another insult.	<ul style="list-style-type: none"> <li>The subject site features a small hill that rises roughly 4m from the road, behind which this monopole is proposed to be placed, at a distance of over 200m to the road reserve. This will diminish the visual impact.</li> <li>Proponent is not responsible for Cockburn Cement</li> </ul>
17	Objection	Lot of cockatoos and galahs, ravens hang around there, and in the park opposite. 5G waves will harm them. Also, it will be an eye-sore and will affect property rates in the surrounding [area]. <ul style="list-style-type: none"> <li>Cause visual pollution</li> <li>5G waves could be harmful for senior citizens</li> </ul>	<ul style="list-style-type: none"> <li>As per submissions 13 and 14</li> </ul>
18	Objection	As a resident in close proximity to the proposed Telecommunications Facility, I'm emailing to inform you that I object to the approval of the facility to be built. My reasoning is that it will create an eye sore which will devalue my property. Should the facility go ahead, I'll consider legal action for compensation in relation to reasonable equity loss, due to the facility.	<ul style="list-style-type: none"> <li>Property Values are not valid planning concerns</li> <li>Third Party Appeal Rights are non-existent in Western Australia</li> </ul>
19	Non-Objection	Water Corporation has no objection to the proposal.	<ul style="list-style-type: none"> <li>Noted</li> </ul>
20	Objection	Thank you for the opportunity to comment on the proposed installation of a 30m high telecommunications antenna at 158 Tindal Avenue Beeliar. I would like to state my objection to the proposed telecommunications antenna at 158 Tindal Avenue Beeliar. In my opinion the area is already congested with unsightly infrastructure such as the existing transmission power lines and on the horizon to the south of the proposed antenna location the large and extremely ugly refractory column located at the Cockburn cement facility. This particular location in the suburb is still largely in development and no doubt in time shall expand to many more dwellings in the	<ul style="list-style-type: none"> <li>The site is currently zoned rural and falls within the Air Quality Buffer. The City's Draft Local Planning Strategy does not identify this site as</li> </ul>

		area around the proposed antenna, The before mentioned infrastructure and existing cement process factory have already made enough of a negative aesthetic impact on the area, making it unappealing to potential buyers or investors with dwellings in that particular area of the suburb attracting lower valuations than those with similar amenities in the greater suburb. in my opinion the suburb and its neighbouring suburbs do not need any more unsightly high-rise structures such as this, we already have Cockburn cement and the recently added Americold Logistics building both can be seen from kilometres away additional infrastructure such as the one proposed at 158 Tindal Avenue is not going to benefit the residents of this area.	<p>suitable for residential development.</p> <ul style="list-style-type: none"> <li>• Property values are not valid planning concerns</li> <li>• The proposal will be located over 200m away from the powerlines along Tindal Avenue</li> </ul>
21	Non-Objection	No comment	
22	Objection	Due to the large power lines located on Tindal Ave, we have enough electricity and radiation in this area . Please consider doing something about these along with Cockburn Cement for our health and safety.	<ul style="list-style-type: none"> <li>• The will be located over 200m away from the powerlines on Tindal Avenue</li> </ul>
23	Objection	Extremely concerns about the increasing or high exposure to radio frequency pollution.	<ul style="list-style-type: none"> <li>• As per submission 13</li> </ul>
24	Objection	I do have concerns as we have no proof that there will not be long term health issues caused by the EME levels. I have been around long enough to have seen the consequences of Thalidomide and I have been affected by Mesothelioma caused by asbestos. Because no one knew or they chose not to accept that there was a problem. Secondly, the value of all the properties around the area will be affected. I do feel that the shire should be protecting its rate payers in this instance. Thank you for giving me the opportunity to have my say.	<ul style="list-style-type: none"> <li>• EME from telecommunications is subject to strict regulations in Australia. The Australian Communications and Media Authority (ACMA) enforces these regulations. EME from telecommunications is not hazardous to human health. It's non-ionising. This means it doesn't have enough energy cause harm</li> <li>• The maximum EME level calculated for the proposal is 3.41% of the maximum prescribed public exposure limit</li> </ul>

25	Objection	I am concerned about the high radiation levels indicated for this tower, apposed [sic] to the one located near Coles in Beeliar. Also, there are new homes around this area and it will impact the real estate prices. There is so much land available a little further south which would not have the same effect as to [sic] a residential are. Who would be responsible for medical bills for health issues caused by the fall out?	<ul style="list-style-type: none"> <li>As per submissions 20 and 24</li> </ul>
26	Non-objection	No comment	
27	Non-Objection	No comment	
28	Objection	<p>I confirm that we wish to raise a united street objection to the proposed tower location, all neighbours in residence on Honeymrtle avenue are in objection to the proposal and will be submitting their objection notice independently.</p> <p>To start with the environmental that impact report is enough for the council to not even consider this application as viable as it is too close to a residential area and although the Australian government current regulations are vaguely [sic] within a acceptable range, the data and the fact the emissions recorded at 3,8% impact versus 0.08 at nearby towers is not acceptable. Also it was rated at 168 meters from tower where the Australian Gov regulations require 400 - 1 km radius to be reported. Someone in your organisation is clearly not reading information and regulations that readily available online. It is frightening you would publish a report not fact checked!</p> <p>From my learnt experience with a broadcast media background career the experience is way different and it does affect daily life and health. The Enviro report is for 168 meter radius which effects all the properties on the street adding and facing the Tindal Ave property which I understand is registered as rural however it does now back onto a new residential area. It's simply not feasible and frankly I will use every resource available to stop this from happening with in our living environment, it's not healthy there at 12 transponders on that tower and it needs to go somewhere in order for them to achieve this. Hello is the lucrative opportunity for the property owners who are by all accounts are very elderly and no longer operating the property as a flower garden and will certainly benefit from the income along with the council from the lease. Simply the council can't ignore the obvious facts in this proposed placement. And whilst you have stated you are only involved in a planning approval capacity, this matter is the responsibility of the Cockburn City Council and in my opinion, whoever in your organisation allowed this to progress to application on the data published can either not read or has no understanding of the consequences. Your organisations role is to 'represent the rate payers' as listed in your charter not a 3rd party provider.</p>	<ul style="list-style-type: none"> <li>The 0.08% refers to the cumulative exposure contributed by existing infrastructure. This does not suggest that the nearby towers are less intensive – it follows that because they are further away, their contributing exposure level is weaker. If one moved closer, the 0.08% figure would increase</li> <li>The 168m figure represents the highest exposure level (being 3.84% of the maximum 100% prescribed)</li> <li>The City is not aware of any government regulation that mandates a 400 – 1km reporting distance, especially given the maximum exposure has been calculated to occur at 168m.</li> </ul>

	<p>Apart from significant Health Hazards which of course can be debated hiding behind data reports... It is not going to happen so a alternative site needs to be found. Also the general property value for homes within a 1km range are reported to drop in value by 7/8 %.</p> <p>I am passionate about this matter as having worked for over 10 years in TV broadcast with the 7 Network, we experienced first hand the effects these towers which we leased in various locations which caused horrific outcomes per location which we spent a lot of time and money in addressing .. For example only 3 units were installed in the then north Sydney corporate office to replace the older tech. The bookkeeper sitting closest to the unit ended up loosing [sic] her hair after 3 months of the install. We had issues with staff experiencing fatigue, insomnia and a general agitation due to the constant static buzzing the units cause.</p> <p>EXTENRAL MATERIAL PROVIDED AS PART OF SUBMISSION: <a href="https://www.mobilesafety.com.au/mobile-phone-towers/">https://www.mobilesafety.com.au/mobile-phone-towers/</a></p> <p>EXTERNAL MATERIAL PROVIDED AS PART OF SUBMISSION: <a href="https://environics.co.in/mobile-tower-radiation-and-its-impact-on-human-health/">https://environics.co.in/mobile-tower-radiation-and-its-impact-on-human-health/</a></p>	<ul style="list-style-type: none"> <li>• The current use of the land is not pertinent to the application and the owner’s intention for financial gain has no bearing on the development application.</li> <li>• The City has a statutory responsibility to assess all development application that are submitted and must follow due process.</li> <li>• The applicant had considered a number of candidate sites prior to the submission of the development application.</li> <li>• Property values are not valid planning concerns</li> <li>• Third Party Appeal rights are non-existent in Western Australia.</li> </ul>
29	<p>I just want respectful consultation from the council with the property owners in the local vicinity of the proposed tower.</p> <p>Whatever environmental report is issued I know first hand the effects of living within 500 meters of a tower and the one proposed is 250 meters away from the first property owner ... his name is <b>[Name Removed]</b>. He didn’t receive any communication from the council and he has a letterbox? Nor did we at <b>[address removed]</b>... perhaps as we have not yet installed letterbox.</p> <p>My cousin designed the regional GSM mobile network for Telstra in the 90’s. He said the current regulations in Australia are socially based for connectivity but in actuality the access to faster / stronger signal bandwidth is paramount over KNOWN Reports and data regarding exposure. Put simply a zero exposure is recommended</p>	<ul style="list-style-type: none"> <li>• The City delivers letters to all owner/occupiers within 500m of the site, held a community drop in session at the Beeliar Community Centre and posted details of the application to the Comment on Cockburn site. A sign was also erected on site. The resident in question</li> </ul>

		<p>and that’s why the towers were originally designed to be placed in locations that would not allow 24/7 hr exposure such as freeways in populated areas and also in regional locations. He shared a incident where a Telstra maintenance technician attended a 4 unit tower with his junior offsider who’s job was to temporarily power down the tower for maintenance... he failed to do so and the technician was exposed for 15 minutes to tower radiation. After falling ill he was given a terminal diagnosis with only months to live as all his vital organs were essentially destroyed by radioactive exposure!</p> <p>I can forward any reports available of which there are many but I am sure you can also access them.</p> <p>We can already see a tower from our properties close to Russell Road... we don’t need another which is in fact a mega tower with the number of transponders proposed , plus the height of it .. it will dominate the entire vicinity and is 100% too close for comfort in any rationale to consider.</p> <p>So what do we do? Do I have to lobby the council members individually? Who and when will decide on the application? It is unfeasible and frightening that a application could go through to being published on your website... if Cockburn Cement works rejected the application with the acres of land they hold there is a reason for that... we owners already have to deal with the concrete dust and burning smell from the cement works and as this was there before we purchased the properties we were conscious of it... but the daily impact is different to what we expected to say the least! The rural classified landholding of the not in operation market garden across the street that would be the property used is no reasonable excuse to consider this location as the council has approved the sale of lots to rate payers and that should be taken into consideration along with the potential health impact. Without a intention to threaten the council, we will go to court to defend if it is to proceed. It simply can’t happen!</p>	<p>was sent a letter to his address.</p> <ul style="list-style-type: none"> <li>• The Australian Government and relevant regulator do not recognise a 0% exposure cap</li> <li>• The tower will back against Cockburn Cement, meaning that its visual impact will not be as prominent as that of existing structures.</li> <li>• The operations of Cockburn Cement are not the responsibility of the proponent.</li> <li>• The zoning (rather than use) of the land is Rural. As a non-listed use under TPS3, this application can be considered (subject to advertising)</li> <li>• Third Party Appeal rights are non-existent in Western Australia</li> </ul>
30	Non-Objection	No Comment	
31	Objection	I feel it will devalue my area. There is already lots of large unsightly electrical towers/poles on Tindal Ave and surrounding. I feel by adding more it will deter families from wanting to move to this area, therefore the current property market will decrease. I would hope that the City of Cockburn does more in this area to make it a nice, clean and attractive area to live. Not more industrial facilities like what is proposed. More heavy vehicles using	<ul style="list-style-type: none"> <li>• Property values are not valid planning considerations.</li> <li>• The onus for beautification projects is not on the proponent.</li> </ul>

		the roads, more noise, more dust. I feel the area is becoming too industrial. Need more greenery, more parks, more shade. More needs to be done to beautify the verges, not adding metal structures.	<ul style="list-style-type: none"> <li>• The use as a telecommunications tower is not considered industrial.</li> <li>• The use of the land for telecommunications will not likely increase the movement of trucks throughout the network post-construction.</li> <li>• The number of existing lines is not indicative of community need. Improved infrastructure is required as areas become more densely populated.</li> </ul>
32	Objection	It's a beautiful bit of land there, why not have a park or more houses and not Metal frame will make the street look awful	<ul style="list-style-type: none"> <li>• The lot is privately owned and the City can not direct the landowner to development it a certain way.</li> </ul>
33	Objection	Tindal Avenue already has substantial overhead powerlines. We are also dealing with the ongoing dust and health impacts of Cockburn Cement. So now the City of Cockburn is suggesting to devalue our properties further? This is more than enough for one section population, City of Cockburn. Another area can host the phone tower in an elevate area where it will be far more beneficial.	<ul style="list-style-type: none"> <li>• The proposed site is over 200m away from the overhead powerlines along Tindal Avenue.</li> <li>• Proponent is not responsible for Cockburn Cement</li> <li>• Property values are not valid planning considerations</li> </ul>
34	Support	We have next to no signal in our house and garbage internet speeds. This would really help us and our business	<ul style="list-style-type: none"> <li>• Noted</li> </ul>
35	Objection	The danger of the closeness to public and property. The value on our houses is going down every day because of crime, graffiti, power poles and Cockburn cement. We don't need another challenge and danger to our health.	<ul style="list-style-type: none"> <li>• As per submission 33</li> </ul>



36	Objection	I want a tower in the area. Unsightly	<ul style="list-style-type: none"> <li>• As per submission 5</li> </ul>
37	Objection	<p>The Australian government and telecommunications industry take the view that radiation from telecommunications networks is not a health risk. The weight of their opinion and the evidence of a large number of studies is that there is no evidence of health risk posed. Often however this research is conducted and / or funded by the telecommunications industry itself. Radio-frequency radiation from various sources has been found to have an association with leukemia, heart problems, cancer, brain tumours, reproductive problems &amp; effects on the nervous system, learning and performance, sleep, immunity, hormones and genes. These effects have been found at levels sometimes many hundreds of times lower than levels approved by international guidelines and standards.</p>	<ul style="list-style-type: none"> <li>• As per submission 28</li> </ul>
38	Objection	<p>I'm concerned about the safety of having a 5g tower close to residents. The report says tower is 250m away after some research safe distance is 400m min. Also is the tower going to be located in the middle of the property at 158 Tindal ave or closer to street? I wish the residents of Beeliar were notified by letter of this proposed site and deadline I happened to come across it on Facebook this morning the day of the cut off.</p>	<ul style="list-style-type: none"> <li>• As per submissions 28 and 29</li> </ul>
39	Objection	<p>We oppose this because the radiation from the tower is a huge health concern for the young community. The young women like myself wanting to start a family being exposed to this radiation and 5g is not healthy at all. Huge lines conducting this energy and frequency has been proven to implicate pregnancies and healthy living. Let alone young kids. Not only the health concern but the infrastructure being built would also devalue our homes. We have enough of smell and pollution from Cockburn Cement. The dust and health hazards from that business is quite literally destroying our health. We do not need a facility from a telecoms business like that at the end of our street to contribute to the pollution in the community. Radio frequency and energy released causes migraines and insomnia which can not only be extremely debilitating, but dangerous for those who try to push through the struggle and are on the roads or even their mental health. 5g towers-yes need to be closer together so the frequency can be conducted more efficiently but they also draw more energy and what is the plan for energy conduction and the environment, are you using a solar 11icro system or just drawing from the new huge lines that will be installed?? The excuse of "The facility will also significantly strengthen mobile coverage in the event of a natural disaster. " is just plain stupid. If power goes out, there is a fire or a storm rips apart the tower, the facility has no use anyway except being a danger to the houses surrounding with prone to fire outbreak, destroying homes next to the facility and also the farm land for the owner. And if Axicom want to use the information from the WHO I highly suggest they do some better independent research as relying on the WHO's lying asses is nothing the community should believe. Also how great is it that a company based in NSW is doing a proposal for the community in Beeliar when they're not even from here? There is plenty of land in the Cockburn Cement facility so a proposal on their property would be better so we don't have to have this eyesore or unhealthy facility so close to our homes and growing families.</p>	<ul style="list-style-type: none"> <li>• As per submission 28.</li> <li>• Proponent is not responsible for Cockburn Cement.</li> <li>• A monopole will not produce dust hazards.</li> <li>• How the tower will draw electricity is not a valid planning concern.</li> <li>• There are no houses within 200m of the monopole so there would be no expectation that it poses a threat if damaged. The owners of the farmland have provided their consent for the application to go ahead.</li> <li>• Where the proponent is bases has no bearing on the merits of the application.</li> </ul>

40	Support	I'm with Optus as a phone provider and it would be beneficial for me to have a better network	<ul style="list-style-type: none"> <li>• Noted</li> </ul>
41	Support	Vodafone signal is terrible in this area, so the new mast can only be a good thing.	<ul style="list-style-type: none"> <li>• Noted</li> </ul>
42	Support	To help improve Vodafone reception	<ul style="list-style-type: none"> <li>• Noted</li> </ul>
43	Objection	<p>Whilst current literature is inconclusive in regard to remote radio towers and effects on health to humans. It has mentioned higher exposure levels to mobile phone users closer to the towers. My concerns are that this proposal does not state where other telecommunication remote radio towers are located (eg Telstra), for a better decision making process on the accumulative effect. Including the already large towers situated along Tindal Ave. If the shire, as stated in your advice/comment log, you are unable to comment on workings/technological data of this proposal, who is measuring the accumulative build up of RF exposure, and who regularly audits these companies operating outputs, (see below) for a difference in exposure compliance distance. Obviously 1.5m compliance distance is acceptable, 25m is not. Also is the shire calculating the build up of environmental toxicity in the area, with the already present cement works in this area, with already large data supporting toxic elements in this suburb. (Cockburn Pollution Stoppers · CONVICTED SERIAL POLLUTER DOUBLES DOWN Cockburn Cement Limited (CCL) was convicted on 1 December on 6 charges that in 2019 it emitted unreasonable odours (pollution) which had adverse effects on our community. It still has charges pending for allegedly polluting our community in 2020. What has been its response? In the last 24 hours CCL has been emitting its usual stinking odour almost non-stop. This odour has been reported as reaching as far away as Mount Henry Bridge yesterday. How do you stop a corporation arrogantly ignoring government authorities and the people affected by its misbehaviour? – TAKE AWAY THEIR MONEY. Substantial financial penalties are essential to stop this convicted serial polluter from treating fines for pollution as just a minor cost of doing business. Fines in region of \$10 million per offence and continuing daily penalties of \$1 million for continuing offences will focus CCL's attention on what it needs to do. The Government must substantially increase penalties under the Environmental Protection Act to protect our community now.) Surely these companies can use one tower system and rent/lease the services for all business's for the betterment of their clients (eg telechoice leases 80% of 12icroce's towers for services), and only the shires and the state government can lead in these protocols based on the less harmful/unsightly infrastructure for man and nature. <a href="https://www.ncbi.nlm.nih.gov/pmc/articles/PMC7337122/">https://www.ncbi.nlm.nih.gov/pmc/articles/PMC7337122/</a> Compliance distance is the distance from an antenna beyond which RF exposures cannot exceed applicable exposure limits. Wireless technology manufacturer Ericsson is one of the few firms presently selling 5G base station equipment. At a recent conference, an Ericsson RF safety expert presented calculations for two example cases (in terms of a site's total radiated energy) for deployment of 5G base stations (Törnevik 2017). One was a small cell radio with eight beams transmitting collectively less than 1 W at 28 GHz, which had a calculated public exposure compliance</p>	<ul style="list-style-type: none"> <li>• As per submission 28</li> <li>• The environmental EME report provides the cumulative impact of nearby towers as part of the calculations.</li> <li>• The Australian Communications and Media Authority (ACMA) enforces these regulations pertaining to EME and exposure.</li> <li>• The proponent is not responsible for Cockburn Cement</li> <li>• <i>Environmental Toxicity</i> (assumed air quality levels) is not pertinent to this application.</li> <li>• Telecommunications towers do not emit odours.</li> <li>• The City does not set penalties prescribed under the Environmental Protection Act</li> <li>• The calculated EME exposure is 3.48% of the maximum 100% prescribed under the regulations.</li> </ul>

		distance of 1.5 m in the boresight direction. The second was a 13icrocell site with a 200-W, 3.5-GHz 5G transmitter co-located with a group of 2G, 3G, and 4G antennas. In this case, the public exposure compliance distance when combined with the output of the 2G, 3G, and 4G emissions was 25 m. This evaluation was conducted assuming that all the antennas were transmitting at maximum power simultaneously, which is an extremely conservative assumption. Further refinements in methodology are needed to assess exposure from 5G transmitters, both environmental exposures from base stations and local exposures from transmitters used close to the body. Accurate exposure assessment of the narrow and often moving beams of transmitted signals is needed both to assess compliance with exposure limits and for health and safety research. Substantial efforts are underway on these issues by working groups of the International Committee on Electromagnetic Safety (ICES) and the International Electrotechnical Commission (IEC 2017).	
44	Objection	Already have numerous tall towers in Tindal Avenue. As emissions are very high in this area we don't need to be increasing them. We have enough pollution with the cement works, which enough of an ongoing problem	<ul style="list-style-type: none"> <li>As per submission 28</li> </ul>
45	Support	Better reception	<ul style="list-style-type: none"> <li>Noted</li> </ul>
46	Objection	There is already a major power line along Tindal avenue, additional communication infrastructure is not good looking. Also query if it would cause any health issue due to electromagnetic/ radio	<ul style="list-style-type: none"> <li>As per submission 28</li> </ul>
47	Objection	My family and I are concerned that houses are much too close to the proposed site. With Cockburn Cement already so close, we don't support the addition of another facility that is a detriment to the health of residents in the area.	<ul style="list-style-type: none"> <li>Proponent is not responsible for Cockburn Cement</li> </ul>
48	Objection	After doing some research and living less than 400m from this tower, the risks are too high. Having cancer in My family I don't want anything that will raise my cancer risk so close to where I live.	<ul style="list-style-type: none"> <li>As per submission 28</li> </ul>
49	Objection	No long term health research has been done.	<ul style="list-style-type: none"> <li>As per submission 28</li> </ul>
50	Objection	We already have high voltage transmission lines running through the estate. The tower would be another health risk.	<ul style="list-style-type: none"> <li>This application does not pertain to high voltage powerlines. The proposal is located over 200m away from the power lines along Tindal Ave.</li> <li>The number of existing lines is not indicative of community need. Improved infrastructure is required as areas become more densely populated.</li> </ul>

51	Objection	Health reasons and eyesore. There are plenty of communication lines already through the suburb so I propose they move them elsewhere to maintain property value.	<ul style="list-style-type: none"> <li>Property value is not a valid planning concern</li> <li>The number of existing lines is not indicative of community need. Improved infrastructure is required as areas become more densely populated.</li> </ul>
52	Objection	In my research, that proposed Telecommunication Facility- Cellular Antenna- is too close in our residence in my estimate we are in 400 meters radius from the site. Reasons; 1. Studies show serious “ Biological and Health Effects” within 400 meter of cellular transmitters. 2. Property devaluation	<ul style="list-style-type: none"> <li>As per submission 28</li> </ul>
53	Objection	This is a residential area where we are already being impacted by existing infrastructure and commercial land uses. We understood that when we bought here, but don't add to this by placing additional commercial infrastructure that could go elsewhere. Not to mention the health risks associated with mobile towers and equipment, we know that there is some impact. Locate this somewhere where there are less houses / development - there is no need to use this land. There is other land available within the area. I am strongly against this proposal and will submit additional formal complaints to the City of Cockburn should this development proceed.	<ul style="list-style-type: none"> <li>As per submission 28</li> <li>The number of existing lines is not indicative of community need. Improved infrastructure is required as areas become more densely populated.</li> <li>Applicant completed extensive candidate search. This site was chosen as the best suited location.</li> </ul>
54	Objection	A German study reported that people living within 400m of cell towers had over 4 times the normal rate for new cancers (City of Naila 2004). In an Israeli study, the relative risk of Cancer was about 4 times greater within 350m of the cell tower (Wolf et al. 1997). Based on findings like these, a minimum safety distance of ¼ mile might be considered prudent. And again, individuals with EMF hypersensitivity or other serious health issues may want to consider a much greater safety distance, perhaps a half mile or even more.	<ul style="list-style-type: none"> <li>As per submission 28</li> </ul>
55	Non-Objection	Because the area is already filled with other electrical towers and power poles, another one wont hurt.	<ul style="list-style-type: none"> <li>Noted</li> </ul>
56	Objection	Too large and too close to residential homes.	<ul style="list-style-type: none"> <li>There are no maximum heights prescribed for this kind of development and no</li> </ul>

			recommended buffer distances.
57	Objection	There are some study's that link Electromagnetic radiation from cell phone towers to elevated cancer incidence. Will look ugly and will decrease house value in surrounding areas.	<ul style="list-style-type: none"> <li>As per submission 28</li> </ul>
58	Objection	This is a ridiculous idea, why would you put a big telecommunications tower in the middle of residential homes, its an eye sore. Find somewhere else for it	<ul style="list-style-type: none"> <li>Applicant completed extensive candidate search. This site was chosen as the best suited location.</li> </ul>
59	Objection	i do not want more poles and signals in the area, we already overloaded with electrical supply etc.	<ul style="list-style-type: none"> <li>The number of existing lines is not indicative of community need. Improved infrastructure is required as areas become more densely populated.</li> </ul>
60	Support	for better network coverage in the area	<ul style="list-style-type: none"> <li>Noted</li> </ul>
61	Non-Objection	Unsure	
62	Objection	I purchased this property in 5 Hopbush Way in 2017. In that time the property value has not increased in this time. When I do go to sell this property, I will either break even or at a loss. Hence the reason why I am not in-favour of having a large communication hub less that 1km from the property, as this will devalue the property even further. Why can't it be placed in a more rural area on Russell Road, why does it have to be so close to built-up housing. I understand these hubs are important for good communication, but they don't need to be as close to housing as this one is. I have already had firsthand experience in selling a property with high tension wire structures in close proximity to housing and this de-valued the home by almost \$20,000+. Given that I am already at a loss with my current property I really can't afford to lose this money again. Please re-consider the placement of this hub.	<ul style="list-style-type: none"> <li>Property values are not valid planning considerations.</li> <li>The site is zoned Rural under TPS3</li> <li>There are no separation distances prescribed for this kind of infrastructure.</li> </ul>
64	Objection	I am writing to you today as I have concerns regarding the proposal. I will be sending my objection to [City Officer] formally however I wanted to communicate with you as councillors. As I understand after speaking to [City Officer], the application for the 5G tower will be determined by him alone. As qualified and skilled as he may be, I feel that an application as controversial as this should go to you as councillors. Please know that your community do not want this and there are serious health and safety concerns over something like this that are not fully understood by current medical science. When things are this controversial it should be the elected council that make the decisions and not the bureaucrats - i mean that in the nicest of terms. It is not fair for	<ul style="list-style-type: none"> <li>Application will be determined by Council</li> </ul>

		<p>[City Officer] to be making the controversial decisions here. Having this at a councillors meeting will allow to community to speak up and be part of the conversation.</p> <p>I am asking you to listen to your community. Please instruct [City Officer] to bring this application to council and please vote NO! After speaking with many residents in Beeliar and our residents' association, I can tell you that this is not what the community want and need.</p>	
63	Non-Objection	I am good with whatever	<ul style="list-style-type: none"><li>• Noted</li></ul>

## 14.2 Finance

### 14.2.1 Payments Made from Municipal Fund and Local Procurement Summary - February 2023

**Author** A/Head of Finance

**Attachments** 1. Credit Card Expenses Report - January 2023 [↓](#)  
2. Payments Listing - February 2023 [↓](#)

#### RECOMMENDATION

That Council:

- (1) RECEIVES the list of payments from the Municipal Fund during the month of February 2023, as attached to the Agenda.

#### Background

Council has delegated its power to make payments from the Municipal or Trust Fund to the Chief Executive Officer and other sub-delegates under Delegated Authority 'Local Government Act 1995 - Payment from Municipal and Trust Funds'.

Regulation 13(1) of the *Local Government (Financial Management) Regulations 1996* requires a list of accounts paid under this delegation to be prepared and presented to Council each month.

It should be noted that the City no longer holds any funds within the Trust Fund, following legislative amendments requiring public open space (POS) cash in lieu contributions to now be held in Municipal reserves.

#### Submission

N/A

#### Report

Payments made under delegation during the month of February totalled \$14.975 million, and a listing of these is attached to the agenda for review by Council.

These comprise:

- EFT payments (suppliers and sundry creditors) - \$11.526 million (769 payments)
- Payroll payments - \$3.337 million (2 fortnights)
- Corporate credit cards – total of \$102,811 (61 cards used)
- Bank transactional fees (BPay and merchant fees) - \$9,953.

The City has several payment runs each month to ensure its trade suppliers are paid on a timely basis, particularly local and small businesses.

Also attached is the monthly credit card payments report, showing January transactions (paid in February) by cardholder position.

There were four transactions made on the Acting CEO’s credit card for \$1,904.

Local Procurement

Monthly statistics on local and regional procurement spend are summarised below, showing the spend amounts and percentages against total spend:

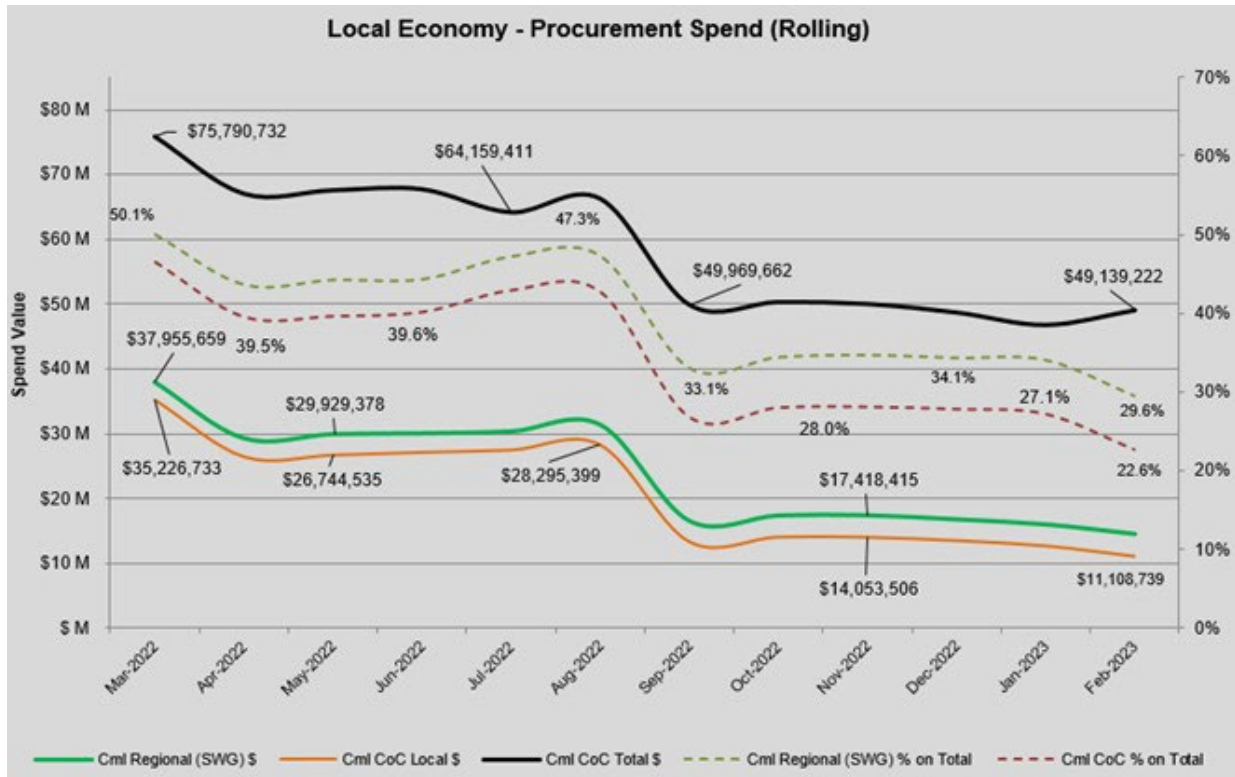
Procurement Report - Local Buy Summary & Trends				February 2023		
Monthly Statistics	Total SWG Spend	\$961,539	CoC Local \$	7.4%	Local/Regional \$	11.8%
	CoC Local Spend	\$608,820	CoC Local %	35.0%	Local/Regional %	39.9%

In February, local spending within Cockburn made up 7.4 percent of the City’s monthly spend, comprising 35.0 percent of all procurement transactions made for the month.

Within the Perth South West region, this increased to 11.8 percent of monthly spend from 39.9 percent of transactions.



The following one year rolling chart to February 2023 tracks the City’s procurement spend with businesses located within Cockburn and the Perth South West region (the chart has been altered from previous reports to remove the cumulative effect and provide improved analysis):



The 12-month rolling local Cockburn spend was \$11.11 million, representing 22.6 percent of the City’s total spend, with \$14.54 million or 29.6 percent of total spend within the Perth South West regional area.

These results track the City’s performance in achieving Council’s “local and regional economy” principle contained within its Procurement Policy (i.e. a buy local procurement preference).

**Strategic Plans/Policy Implications**

Local Economy

A sustainable and diverse local economy that attracts increased investment and provides local employment

- Thriving local commercial centres, local businesses, and tourism industry.

Listening and Leading

A community focused, sustainable, accountable, and progressive organisation

- Best practice Governance, partnerships, and value for money
- High quality and effective community engagement and customer service experiences.

**Budget/Financial Implications**

All payments made have been provided for within the City's Annual Budget, as adopted and amended by Council.

**Legal Implications**

This item ensures compliance with s6.10(d) of the *Local Government Act 1995* and Regulations 12 and 13 of the *Local Government (Financial Management) Regulations 1996*.

**Community Consultation**

N/A

**Risk Management Implications**

Council is receiving the list of payments already made by the City under delegation in meeting its contractual obligations.

This is a statutory requirement and allows Council to review and clarify any payment that has been made.

**Advice to Proponents/Submitters**

N/A

**Implications of Section 3.18(3) *Local Government Act 1995***

Nil

City of Cockburn  
Credit Card Transactions Report

Transactions Post Date Between 30-Dec-2022 and 30-Jan-2023

Reference	Date	Status	Service Provider	Card Liability	Description
<b>Acting CEO</b>				<b>1,903.64</b>	
000368	11/01/2023	Completed	UDIAWA	377.40	Conferences and Seminars
000368	16/01/2023	Completed	CPP CONVENTION CENTRE	13.12	Parking Expenses
000368	16/01/2023	Completed	CPP CONVENTION CENTRE	13.12	Parking Expenses
000368	25/01/2023	Completed	OMED FIDAYI PTY LTD	1,500.00	Motor vehicle expenses
<b>Acting CFO</b>				<b>1,493.16</b>	
CC79296	16/01/2023	Outstanding	QANTAS	497.72	Travel and Accommodation
CC79302	16/01/2023	Outstanding	QANTAS	497.72	Travel and Accommodation
CC79306	16/01/2023	Outstanding	QANTAS	497.72	Travel and Accommodation
CC79703	18/01/2023	Outstanding	SINGAPORE AIRLINES	638.70	Travel and Accommodation
CC79704	18/01/2023	Outstanding	SINGAPORE AIRLINES	638.70	Travel and Accommodation
CC79706	18/01/2023	Outstanding	SINGAPORE AIRLINES	638.70	Travel and Accommodation
CC79658	19/01/2023	Outstanding	SINGAPORE AIRLINES	-638.70	Travel and Accommodation
CC79660	19/01/2023	Outstanding	SINGAPORE AIRLINES	-638.70	Travel and Accommodation
CC79662	19/01/2023	Outstanding	SINGAPORE AIRLINES	-638.70	Travel and Accommodation
<b>Adult Services Coordinator</b>				<b>560.13</b>	
000350	12/01/2023	Completed	BIG W 0455	77.50	Supplies and Materials purchases
000350	18/01/2023	Completed	MISS MAUD	161.75	Consumables
000350	20/01/2023	Completed	WOOLWORTHS 4367	20.00	Consumables
000350	20/01/2023	Completed	WOOLWORTHS 4367	7.20	Consumables
000350	23/01/2023	Completed	COLES 0490	5.68	Consumables
000350	25/01/2023	Completed	PERTH FESTIVAL	288.00	Training & Professional Development
<b>Art and Culture Coordinator</b>				<b>382.00</b>	
000367	12/01/2023	Completed	ARTSOURCE	275.00	Subscriptions and Memberships
000367	24/01/2023	Completed	RED DOT STORES	27.00	Supplies and Materials purchases
000367	25/01/2023	Completed	SPOTLIGHT 104	80.00	Supplies and Materials purchases

**Branch Manager - Spearwood Library**

**1,601.03**

000349	5/01/2023	Completed	MYO*GREEN WORLD INDOOR	198.00	Professional Services
000349	5/01/2023	Completed	WANEWSDTI	613.21	Subscriptions and Memberships
000349	6/01/2023	Completed	Neverfail	46.20	Supplies and Materials purchases
000349	9/01/2023	Completed	RED DOT STORES	15.98	Supplies and Materials purchases
000349	10/01/2023	Completed	BIG W 0455	5.00	Supplies and Materials purchases
000349	10/01/2023	Completed	Priceless Discounts P	21.50	Supplies and Materials purchases
000349	10/01/2023	Completed	Priceless Discounts P	10.00	Supplies and Materials purchases
000349	10/01/2023	Completed	Priceless Discounts P	5.00	Supplies and Materials purchases
000349	17/01/2023	Completed	NEVERFAIL SPRINGWTR	46.20	Supplies and Materials purchases
000349	24/01/2023	Completed	AMAZON AU RETAIL	629.99	Equipment purchases
000349	27/01/2023	Completed	PHOENIX NEWSAGENCY	9.95	Supplies and Materials purchases

**Branch Manager Coolbellup Library**

**623.41**

000343	4/01/2023	Completed	WOOLWORTHS 4703	3.95	Supplies and Materials purchases
000343	6/01/2023	Completed	Booktopia Pty Ltd	67.50	Supplies and Materials purchases
000343	9/01/2023	Completed	BUNNINGS 453000	44.40	Supplies and Materials purchases
000343	9/01/2023	Completed	OFFICEWORKS 0616	7.65	Supplies and Materials purchases
000343	19/01/2023	Completed	Booktopia Pty Ltd	141.43	Supplies and Materials purchases
000343	20/01/2023	Completed	SP JB HI-FI ONLINE	264.78	Supplies and Materials purchases
000343	20/01/2023	Completed	WOOLWORTHS 4703	26.00	Supplies and Materials purchases
000343	24/01/2023	Completed	WOOLWORTHS 4703	9.70	Supplies and Materials purchases
000343	25/01/2023	Completed	TARGET AUSTRALIA PTY L	148.00	Supplies and Materials purchases
000343	27/01/2023	Completed	TARGET AUSTRALIA PTY L	-90.00	Supplies and Materials purchases

**Branch Support Librarian**

**911.66**

000347	3/01/2023	Completed	OFFICEWORKS	27.94	Supplies and Materials purchases
000347	5/01/2023	Completed	BP EX THOMSNS L 5992	4.10	Consumables
000347	9/01/2023	Completed	AMAZON AU	80.99	Supplies and Materials purchases
000347	9/01/2023	Completed	AMAZON AU	20.94	Supplies and Materials purchases
000347	9/01/2023	Completed	BIGW ONLINE	34.90	Supplies and Materials purchases
000347	9/01/2023	Completed	Booktopia Pty Ltd	234.04	Supplies and Materials purchases
000347	9/01/2023	Completed	GILBERT AND SONS FRE	29.98	Consumables
000347	9/01/2023	Completed	SP JB HI-FI ONLINE	163.89	Supplies and Materials purchases
000347	11/01/2023	Completed	KMART	27.50	Supplies and Materials purchases
000347	11/01/2023	Completed	YELLOW RAVEN CAFE	3.20	Consumables
000347	12/01/2023	Completed	BIG W 0444	70.00	Supplies and Materials purchases

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000347	16/01/2023	Completed	NEWS LIMITED	56.00	Supplies and Materials purchases
000347	17/01/2023	Completed	FAIRFAX SUBSCRIPTIONS	99.00	Supplies and Materials purchases
000347	20/01/2023	Completed	AMAZON AU	33.85	Supplies and Materials purchases
000347	23/01/2023	Completed	AMAZON MARKETPLACE AU	25.33	Supplies and Materials purchases

**Child Care Services Manager** **780.00**

CC79594	9/01/2023	Outstanding	MYO*Harmony Kids	780.00	Subscriptions and Memberships
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**City Facilities Manager** **1,971.20**

CC79620	6/01/2023	Outstanding	CLASSIC HIRE	387.20	Hire of equipment and facilities
CC79622	6/01/2023	Outstanding	YANGEBUP IGA	18.15	Consumables
CC79376	12/01/2023	Outstanding	SP FIRST AID DISTRIB	770.00	Supplies and Materials purchases
CC79160	17/01/2023	Outstanding	MYO*ACCESS TECHNOLOGIE	245.85	Subscriptions and Memberships
CC79174	17/01/2023	Outstanding	OFFICELINE* OFFICELINE	550.00	

**Civil Infrastructure Manager** **478.28**

000360	11/01/2023	Completed	OIL/ENERGY PTY LTD	478.28	Motor vehicle expenses
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**Lockburn ARC Manager** **2,476.66**

000357	19/01/2023	Completed	PLE Computers	316.00	Equipment purchases
000357	23/01/2023	Completed	SP JB HI-FI ONLINE	899.00	Equipment purchases
000357	23/01/2023	Completed	SQ *PERTH BOUNCY CAST	630.83	Events and Functions
000357	27/01/2023	Completed	SQ *PERTH BOUNCY CAST	630.83	Events and Functions

**Lockburn Care Manager** **2,588.55**

CC79717	3/01/2023	Outstanding	OFFICEWORKS 0616	221.21	Supplies and Materials purchases
CC79676	4/01/2023	Outstanding	SURGICAL HOUSE	381.63	Supplies and Materials purchases
CC79450	12/01/2023	Outstanding	XERO AU INV-25888555	59.00	
CC79182	17/01/2023	Outstanding	EZI*SANDWAI	1,545.43	Subscriptions and Memberships
CC79310	27/01/2023	Outstanding	Woolworths Online	295.65	Consumables
CC79176	30/01/2023	Outstanding	Woolworths Online	85.63	Consumables

**Lockburn Parenting Services Coordinator** **-639.56**

CC79731	25/01/2023	Outstanding	INTNL TRANS FEE REFUND	-15.60	Bank and other fees
CC79730	27/01/2023	Outstanding	MISCELLANEOUS CREDIT	-623.96	

**Collection Development Librarian** **1,658.44**

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000342	10/01/2023	Completed	Booktopia Pty Ltd	130.70	Supplies and Materials purchases
000342	20/01/2023	Completed	Booktopia Pty Ltd	214.73	Supplies and Materials purchases
000342	20/01/2023	Completed	SP JB HI-FI ONLINE	350.72	Supplies and Materials purchases
000342	23/01/2023	Completed	BOLINDA PUBLISHING	452.30	Supplies and Materials purchases
000342	24/01/2023	Completed	BIG W 0455	2.99	Supplies and Materials purchases
000342	24/01/2023	Completed	Booktopia Pty Ltd	201.13	Supplies and Materials purchases
000342	24/01/2023	Completed	Priceless Discounts P	21.00	Supplies and Materials purchases
000342	25/01/2023	Completed	BOFFINS BOOKSHOP PTY L	284.87	Supplies and Materials purchases

**Communications and Marketing Manager**

**4,227.15**

CC79693	3/01/2023	Outstanding	DROPBOX*MP77QB4ZSKGN	18.69	Subscriptions and Memberships
CC79723	3/01/2023	Outstanding	FACEBK *4LJE5LKMT2	291.12	Subscriptions and Memberships
CC79592	9/01/2023	Outstanding	FACEBK *UHB5PLB9H2	1,375.00	Subscriptions and Memberships
CC79529	10/01/2023	Outstanding	NEWS LIMITED	40.00	Subscriptions and Memberships
CC79418	12/01/2023	Outstanding	COOGECOMMON230111NB	150.00	Subscriptions and Memberships
CC79314	16/01/2023	Outstanding	ASANA.COM	404.58	Subscriptions and Memberships
CC79318	16/01/2023	Outstanding	FAIRFAX SUBSCRIPTIONS	59.00	Subscriptions and Memberships
CC79593	20/01/2023	Outstanding	OFFICEWORKS	438.95	Supplies and Materials purchases
CC79582	23/01/2023	Outstanding	FACEBK *K7VQ9N38P2	5.22	Subscriptions and Memberships
CC79583	23/01/2023	Outstanding	FACEBK *CMFN3L78P2	40.00	Subscriptions and Memberships
CC79585	23/01/2023	Outstanding	CREATIVE MARKET	75.19	Subscriptions and Memberships
CC79589	23/01/2023	Outstanding	INTNL TRANSACTION FEE	1.88	Bank and other fees
CC79490	25/01/2023	Outstanding	Mailchimp	859.09	Subscriptions and Memberships
CC79236	27/01/2023	Outstanding	iStock.com	176.00	Subscriptions and Memberships
CC79240	27/01/2023	Outstanding	INTNL TRANSACTION FEE	0.04	Bank and other fees
CC79360	27/01/2023	Outstanding	BITLY.COM	49.78	Subscriptions and Memberships
CC79366	27/01/2023	Outstanding	LNK.BIO	1.42	Subscriptions and Memberships
CC79368	27/01/2023	Outstanding	INTNL TRANSACTION FEE	1.24	Bank and other fees
CC79208	30/01/2023	Outstanding	FACEBK *DZPTXLB8P2	40.00	Subscriptions and Memberships
CC79216	30/01/2023	Outstanding	SP YETIAU	199.95	Subscriptions and Memberships

**Community Development Coordinator**

**763.98**

000375	5/01/2023	Completed	SLIMLINE WAREHOUSE	587.98	Supplies and Materials purchases
000375	13/01/2023	Completed	CURTIN UNIVERSITY	176.00	Advertising

**Coordinator Work Health and Safety**

**24.00**

CC79698	3/01/2023	Outstanding	ANNUAL FEE	24.00	Bank and other fees
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**Customer Experience and Marketing Lead**

**738.89**

CC79380	12/01/2023	Outstanding	CRICUT	13.99	Supplies and Materials purchases
CC79259	16/01/2023	Outstanding	FACEBK 2LFU6MFDE2	259.06	Subscriptions and Memberships
CC79601	20/01/2023	Outstanding	SOCIAL CLUB GROUP PTY	53.90	Subscriptions and Memberships
CC79410	25/01/2023	Outstanding	EPSON AUST PTY LTD	411.94	Supplies and Materials purchases

**Customer Experience Coordinator**

**298.95**

CC79362	13/01/2023	Outstanding	OFFICEWORKS	298.95	Supplies and Materials purchases
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**Customer Experience Coordinator - ARC**

**2,523.87**

000359	3/01/2023	Completed	FACEBK *K9R8KJ7D52	45.96	Advertising
000359	6/01/2023	Completed	INTNL TRANSACTION FEE	13.27	Bank and other fees
000359	6/01/2023	Completed	KEEPME PTE LTD	530.82	Subscriptions and Memberships
000359	6/01/2023	Completed	KMART 1229	95.00	Supplies and Materials purchases
000359	9/01/2023	Completed	INTNL TRANSACTION FEE	4.27	Bank and other fees
000359	9/01/2023	Completed	OUTGROW	170.76	Subscriptions and Memberships
000359	10/01/2023	Completed	FACEBK *MUXQFK76Z2	7.32	Advertising
000359	10/01/2023	Completed	SurveyMonkeyCore	384.00	Subscriptions and Memberships
000359	13/01/2023	Completed	FACEBK *3H5TRJXC52	200.00	Advertising
000359	13/01/2023	Completed	SP ENVIRODRY TOWEL	221.10	Consumables
000359	16/01/2023	Completed	REECE 6029	18.32	Supplies and Materials purchases
000359	18/01/2023	Completed	APPLE.COM/AU	149.00	Subscriptions and Memberships
000359	18/01/2023	Completed	OFFICEWORKS 0620	54.88	Supplies and Materials purchases
000359	18/01/2023	Completed	SQ *MANDURAH VOLLEYBA	70.00	Training & Professional Development
000359	18/01/2023	Completed	WOOLWORTHS 4394	204.95	Events and Functions
000359	19/01/2023	Completed	BIG W 0444	59.90	Equipment purchases
000359	19/01/2023	Completed	KMART 1362	103.00	Equipment purchases
000359	19/01/2023	Completed	WOOLWORTHS 4394	58.70	Consumables
000359	20/01/2023	Completed	FACEBK *RQ9UXJXC52	200.00	Subscriptions and Memberships
000359	23/01/2023	Completed	NETREGISTRY	31.45	Subscriptions and Memberships
000359	27/01/2023	Completed	COCKBURN ARC	-293.00	Subscriptions and Memberships
000359	30/01/2023	Completed	FACEBK *2FJ42L7R82	7.57	Advertising
000359	30/01/2023	Completed	INTNL TRANSACTION FEE	4.55	Bank and other fees
000359	30/01/2023	Completed	QUICKTAPSURVEY	182.05	Subscriptions and Memberships

**Events and Culture Manager**

**1,995.30**

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000363	10/01/2023	Completed	PAYPAL *CHAMBERSENT	118.73	Consumables
000363	13/01/2023	Completed	MYO*Coogee Beach Surf	15.50	Events and Functions
000363	20/01/2023	Completed	WWW.NCHSOFTWARE.COM	63.44	Application, Licence, Registration Fees
000363	25/01/2023	Completed	CHAMPION MUSIC	1,678.88	Professional Services
000363	30/01/2023	Completed	BUNNINGS 303000	69.00	Equipment purchases
000363	30/01/2023	Completed	BUNNINGS 303000	29.85	Equipment purchases
000363	30/01/2023	Completed	BUNNINGS 303000	9.95	Supplies and Materials purchases
000363	30/01/2023	Completed	BUNNINGS 303000	9.95	Supplies and Materials purchases

**Events Officer****1,315.01**

CC79522	10/01/2023	Outstanding	COCKBURN POLICE STATIO	87.40	
CC79524	10/01/2023	Outstanding	COCKBURN POLICE STATIO	87.40	
CC79685	18/01/2023	Outstanding	Limepay*DiscPartySupp	505.37	
CC79572	23/01/2023	Outstanding	BIG W 0455	82.00	Supplies and Materials purchases
CC79574	23/01/2023	Outstanding	WOOLWORTHS 4367	21.84	Consumables
CC79460	25/01/2023	Outstanding	SPOTLIGHT PTY LTD	320.00	Supplies and Materials purchases
CC79312	27/01/2023	Outstanding	LATTE LAB	175.00	Consumables
CC79190	30/01/2023	Outstanding	7-ELEVEN 3010	36.00	Consumables

**Executive Governance and Strategy****3,620.00**

000352	25/01/2023	Completed	GOVERNANCE INSTITUTE	3,620.00	Training & Professional Development
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**Executive Officer****7,117.06**

000377	6/01/2023	Completed	BUSINESS NEWS PTY LT	2,500.00	Conferences and Seminars
000377	6/01/2023	Completed	SPACETOCO VENUE HIRE	176.00	Hire of equipment and facilities
000377	19/01/2023	Completed	Flight Centre	2,640.54	Travel and Accommodation
000377	19/01/2023	Completed	Flight Centre	1,406.12	Travel and Accommodation
000377	25/01/2023	Completed	Rumbles Cafe	260.00	Meeting/Workshop Catering
CC79252	27/01/2023	Outstanding	NEWS LIMITED	20.00	Subscriptions and Memberships
CC79304	27/01/2023	Outstanding	NEWS LIMITED	20.00	Subscriptions and Memberships
CC79212	30/01/2023	Outstanding	FAIRFAX SUBSCRIPTIONS	26.00	Subscriptions and Memberships
CC79224	30/01/2023	Outstanding	WANEWSDTI	12.40	Subscriptions and Memberships
000371	30/01/2023	Completed	WANEWSDTI	28.00	Subscriptions and Memberships
000377	30/01/2023	Completed	WANEWSDTI	28.00	Subscriptions and Memberships

**Executive officer to the Mayor and Council****24.00**

000351	3/01/2023	Completed	ANNUAL FEE	24.00	Bank and other fees
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10-Mar-2023



**Family & Community Development Manager**

**1,198.12**

000370	16/01/2023	Completed	Flight Centre	1,068.32	Conferences and Seminars
000370	20/01/2023	Completed	Humanitix	30.00	Conferences and Seminars
000370	24/01/2023	Completed	SP YARNMARKETPLACE	99.80	Supplies and Materials purchases

**Lead of Community Development**

**-21.95**

CC79232	16/01/2023	Outstanding	MISCELLANEOUS CREDIT	-21.95	
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**Lead of Community Safety & Ranger Svcs**

**1,268.45**

CC79602	6/01/2023	Outstanding	BUNNINGS 303000	151.22	Supplies and Materials purchases
CC79412	12/01/2023	Outstanding	LinkedIn 7830951926	110.00	Subscriptions and Memberships
CC79416	12/01/2023	Outstanding	Starlink Internet	139.00	Subscriptions and Memberships
CC79668	19/01/2023	Outstanding	CITY OF SWAN	8.00	Parking Expenses
CC79670	19/01/2023	Outstanding	VISUAL WORK WEAR	28.60	Supplies and Materials purchases
CC79672	19/01/2023	Outstanding	STAR CAR WASH GROUP	75.00	
CC79673	19/01/2023	Outstanding	SP KITBAGAU	296.80	
CC79591	20/01/2023	Outstanding	INTNL TRANSACTION FEE	0.68	Bank and other fees
CC79628	20/01/2023	Outstanding	VENNGAGE.COM	27.25	
CC79537	24/01/2023	Outstanding	CITY OF SUBIACO	2.00	Parking Expenses
CC79316	27/01/2023	Outstanding	FARRINGTON DRY CLEANER	100.00	
CC79320	27/01/2023	Outstanding	FARRINGTON DRY CLEANER	60.00	
CC79326	27/01/2023	Outstanding	FARRINGTON DRY CLEANER	45.00	
CC79330	27/01/2023	Outstanding	FARRINGTON DRY CLEANER	60.00	
CC79332	27/01/2023	Outstanding	SQ *LOUIS BAXTERS	11.50	
CC79336	27/01/2023	Outstanding	COLES 0494	100.00	Consumables
CC79340	27/01/2023	Outstanding	COLES 0494	53.40	Consumables

**Lead of Develop Assessment & Compliance**

**1,352.94**

000373	17/01/2023	Completed	TOTALLY WORK WEAR FR	313.19	Equipment purchases
000373	18/01/2023	Completed	SPACETOCO VENUE HIRE	63.75	Hire of equipment and facilities
000373	23/01/2023	Completed	PLANNING INSTITUTE OF	920.00	Conferences and Seminars
000372	30/01/2023	Completed	GURU BROTHERS CAFE AND	56.00	Meeting/Workshop Catering

**Lead of Information & Technology**

**10.00**

CC79733	24/01/2023	Outstanding	WILSON PARKING AUSTRAL	2.50	Parking Expenses
CC79734	24/01/2023	Outstanding	WILSON PARKING AUSTRAL	2.50	Parking Expenses

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CC79735	24/01/2023	Outstanding	WILSON PARKING AUSTRAL	2.50	Parking Expenses
CC79732	25/01/2023	Outstanding	WILSON PARKING AUSTRAL	2.50	Parking Expenses

**lead of Information and Technology****32.85**

CC79695	3/01/2023	Outstanding	INTNL TRANSACTION FEE	0.74	Bank and other fees
CC79719	3/01/2023	Outstanding	CLOUDFLARE	29.61	Subscriptions and Memberships
CC79587	9/01/2023	Outstanding	WILSON PARKING AUSTRAL	2.50	Parking Expenses

**lead of Library and Cultural Services****4,002.99**

CC79267	16/01/2023	Outstanding	JB HI FI COCKBURN	149.00	Supplies and Materials purchases
CC79644	19/01/2023	Outstanding	FACEBK *NBVYXLB9H2	588.81	Advertising
CC79530	24/01/2023	Outstanding	BIG W 0455	1,600.00	Supplies and Materials purchases
CC79532	24/01/2023	Outstanding	BIG W 0455	400.00	Supplies and Materials purchases
000346	24/01/2023	Completed	iSubscribe Pty Ltd	599.40	Subscriptions and Memberships
000346	24/01/2023	Completed	POPULAR MECHANICS MAGA	130.57	Subscriptions and Memberships
000346	24/01/2023	Completed	INTNL TRANSACTION FEE	3.26	Bank and other fees
000346	25/01/2023	Completed	iSubscribe Pty Ltd	178.00	Subscriptions and Memberships
000346	25/01/2023	Completed	iSubscribe Pty Ltd	89.00	Subscriptions and Memberships
000346	25/01/2023	Completed	iSubscribe Pty Ltd	69.95	Subscriptions and Memberships
000346	25/01/2023	Completed	iSubscribe Pty Ltd	65.00	Subscriptions and Memberships
000346	25/01/2023	Completed	SP UNIVERSAL SHOP AU	130.00	Subscriptions and Memberships

**lead of Projects****1,574.59**

CC79454	11/01/2023	Outstanding	SEC*CITY OF COCKBURN	171.65	Subscriptions and Memberships
CC79694	18/01/2023	Outstanding	SP COLLINS DEBDEN	34.54	Subscriptions and Memberships
CC79641	19/01/2023	Outstanding	ORACLE AUSTRALIA	818.40	Subscriptions and Memberships
CC79631	20/01/2023	Outstanding	PAYPAL *KELYN TRAIN	550.00	

**lead of Sustainability and Environment****988.50**

CC79382	12/01/2023	Outstanding	Humanitix	718.50	Subscriptions and Memberships
CC79456	25/01/2023	Outstanding	WWW.PIANC.ORG.AU	270.00	Subscriptions and Memberships

**leath Promotion Officer****493.62**

000369	13/01/2023	Completed	EB *Injury Prevention	100.00	Conferences and Seminars
000369	18/01/2023	Completed	ALDI STORES - BEELIAR	47.99	Supplies and Materials purchases
000369	18/01/2023	Completed	ALDI STORES - SOUTH LA	53.99	Supplies and Materials purchases
000369	19/01/2023	Completed	RED DOT STORES	13.00	Supplies and Materials purchases

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000369	25/01/2023	Completed	ALDI STORES - SPEARWOOD	215.64	Supplies and Materials purchases
000369	25/01/2023	Completed	BIG W 0455	42.00	Supplies and Materials purchases
000369	27/01/2023	Completed	COLES EXPRESS 6906	21.00	Supplies and Materials purchases

**Infrastructure & Operations Coordinator**

**2,975.42**

000358	3/01/2023	Completed	Canva* 03653-3377609	17.99	Subscriptions and Memberships
000358	3/01/2023	Completed	GECKOBOARD	294.65	Subscriptions and Memberships
000358	3/01/2023	Completed	INTNL TRANSACTION FEE	7.37	Bank and other fees
000358	3/01/2023	Completed	INTNL TRANSACTION FEE	0.71	Bank and other fees
000358	3/01/2023	Completed	ZUBTITLE.COM	28.32	Subscriptions and Memberships
000358	5/01/2023	Completed	NAAVI PTY LTD	25.00	Subscriptions and Memberships
000358	9/01/2023	Completed	BIG W 0444	31.75	Consumables
000358	9/01/2023	Completed	Mailchimp	171.98	Subscriptions and Memberships
000358	9/01/2023	Completed	PAYPAL *STEPHENWALK	851.91	Events and Functions
000358	9/01/2023	Completed	PAYPAL *STEPHENWALK	851.91	Events and Functions
000358	9/01/2023	Completed	WOOLWORTHS 4394	11.85	Consumables
000358	11/01/2023	Completed	SQ *PERTH BOUNCY CAST	630.83	Events and Functions
000358	16/01/2023	Completed	COLES 0490	34.20	Consumables
000358	19/01/2023	Completed	SUCCESS HEALTH P/L	16.95	Supplies and Materials purchases

**Handfill Supervisor HWRP**

**507.10**

000335	25/01/2023	Completed	THE RIGGING SHED	485.10	Supplies and Materials purchases
000335	27/01/2023	Completed	BP NAVAL BASE 7770	22.00	Consumables

**Library Technician**

**1,945.01**

000341	30/12/2022	Completed	AMAZON MARKETPLACE AU	59.87	Supplies and Materials purchases
000341	30/12/2022	Completed	BIGW ONLINE	123.00	Supplies and Materials purchases
000341	30/12/2022	Completed	Booktopia Pty Ltd	209.19	Supplies and Materials purchases
000341	3/01/2023	Completed	Booktopia Pty Ltd	118.81	Supplies and Materials purchases
000341	6/01/2023	Completed	Booktopia Pty Ltd	119.82	Supplies and Materials purchases
000341	9/01/2023	Completed	SP JB HI-FI ONLINE	276.73	Supplies and Materials purchases
000341	12/01/2023	Completed	BIGW ONLINE	35.00	Supplies and Materials purchases
000341	13/01/2023	Completed	Booktopia Pty Ltd	435.91	Supplies and Materials purchases
000341	16/01/2023	Completed	BIGW ONLINE	70.00	Supplies and Materials purchases
000341	16/01/2023	Completed	Booktopia Pty Ltd	100.37	Supplies and Materials purchases
000341	16/01/2023	Completed	SP JB HI-FI ONLINE	273.76	Supplies and Materials purchases
000341	17/01/2023	Completed	AMAZON MARKETPLACE AU	25.00	Supplies and Materials purchases

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000341	23/01/2023	Completed	AB* ABEBOOKS.CO JOCGNC	26.90	Supplies and Materials purchases
000341	23/01/2023	Completed	INTNL TRANSACTION FEE	0.67	Supplies and Materials purchases
000341	25/01/2023	Completed	AMAZON MARKETPLACE AU	69.98	Supplies and Materials purchases

**Library Technology Coordinator**

**1,641.35**

CC79606	6/01/2023	Outstanding	MAILCHIMP *MISC	360.76	Subscriptions and Memberships
CC79510	10/01/2023	Outstanding	Windcave	226.06	
CC79470	11/01/2023	Outstanding	APPLE.COM/BILL	14.99	Subscriptions and Memberships
CC79263	16/01/2023	Outstanding	OFFICEWORKS	53.65	Supplies and Materials purchases
CC79170	17/01/2023	Outstanding	DREAMITHOS* DREAMIT HO	234.70	
CC79553	23/01/2023	Outstanding	AMAZON AU	629.99	Supplies and Materials purchases
CC79521	24/01/2023	Outstanding	OFFICEWORKS	58.54	Supplies and Materials purchases
CC79525	24/01/2023	Outstanding	Windcave	47.30	Supplies and Materials purchases
CC79420	25/01/2023	Outstanding	CATCH	15.36	Supplies and Materials purchases

**Manager Advocacy and Engagement**

**165.61**

000376	10/01/2023	Completed	WA Police Check/POST	58.70	Training & Professional Development
000376	18/01/2023	Completed	CPP COUNCIL HOUSE	11.11	Motor vehicle expenses
000376	20/01/2023	Completed	DOME TRINITY	33.10	Meeting/Workshop Catering
000376	27/01/2023	Completed	SQ *GORDON ST GARAGE	34.70	Meeting/Workshop Catering
000376	27/01/2023	Completed	WANEWSDTI	28.00	Subscriptions and Memberships

**Manager Business & Economic Development**

**1,093.77**

000334	6/01/2023	Completed	CITY OF PERTH PARKING-	4.04	Parking Expenses
000334	19/01/2023	Completed	CROWN PERTH PARKING	25.21	Parking Expenses
000334	23/01/2023	Completed	MOMENTS CAFE	25.50	Meeting/Workshop Catering
000334	24/01/2023	Completed	City of Joondalup	4.80	Parking Expenses
000334	24/01/2023	Completed	City of Joondalup	2.20	Parking Expenses
000334	25/01/2023	Completed	INTNL TRANSACTION FEE	20.19	Bank and other fees
000334	25/01/2023	Completed	PLAZA PREMIUM GROUP	807.57	Travel and Accommodation
000334	27/01/2023	Completed	CITY OF VINCENT	3.46	Parking Expenses
000334	27/01/2023	Completed	CRANKED COFFEE	5.80	Consumables
000334	27/01/2023	Completed	Maltia Caffe	195.00	Meeting/Workshop Catering

**Manager Environment Health**

**285.22**

000374	19/01/2023	Completed	ENVIROLAB SERVICES W	285.22	Professional Services
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**Manager Recreation Services****165.00**

000337	18/01/2023	Completed	Parks and Leisure Aust	165.00	Conferences and Seminars
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**Organisational Development Coordinator****3,660.90**

CC79669	4/01/2023	Outstanding	ATI MIRAGE TRAINING	387.00	Training & Professional Development
CC79640	5/01/2023	Outstanding	INTNL TRANSACTION FEE	0.82	Bank and other fees
CC79655	5/01/2023	Outstanding	ST JOHN AMBULANCE AUST	160.00	Training & Professional Development
CC79657	5/01/2023	Outstanding	POLINODE - NETWORKS	32.90	Training & Professional Development
CC79666	5/01/2023	Outstanding	ST JOHN AMBULANCE AUST	160.00	Training & Professional Development
CC79627	6/01/2023	Outstanding	ST JOHN AMBULANCE AUST	55.00	Training & Professional Development
CC79575	9/01/2023	Outstanding	WESTERN DIAGNOSTIC P	485.38	Training & Professional Development
CC79486	11/01/2023	Outstanding	ST JOHN AMBULANCE AUST	160.00	Training & Professional Development
CC79386	12/01/2023	Outstanding	ATI MIRAGE TRAINING	569.25	Training & Professional Development
CC79394	12/01/2023	Outstanding	ATI MIRAGE TRAINING	569.25	Training & Professional Development
CC79354	13/01/2023	Outstanding	Ezypay*Fire and Safety	200.00	Training & Professional Development
CC79286	16/01/2023	Outstanding	ST JOHN AMBULANCE AUST	640.00	Training & Professional Development
CC79288	16/01/2023	Outstanding	TONY AVELING & ASSOCIA	-145.00	Training & Professional Development
CC79188	17/01/2023	Outstanding	HAPPINESS CO	25.00	Training & Professional Development
CC79464	25/01/2023	Outstanding	ST JOHN AMBULANCE AUST	160.00	Training & Professional Development
CC79472	25/01/2023	Outstanding	ST JOHN AMBULANCE AUST	55.00	Training & Professional Development
CC79192	30/01/2023	Outstanding	SAFETYCULTURE	146.30	Training & Professional Development

**Parenting Service Coordinator****190.00**

CC79647	5/01/2023	Outstanding	SEC*CITY OF COCKBURN	190.00	Hire of equipment and facilities
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**Parks Operations Coordinator****1,911.35**

CC79611	6/01/2023	Outstanding	DIRECTCOMMSUPPLIES	935.00	Subscriptions and Memberships
CC79617	6/01/2023	Outstanding	DIRECTCOMMSUPPLIES	555.50	Subscriptions and Memberships
CC79543	9/01/2023	Outstanding	IDOM MELVILLE PTY LT	160.00	
CC79598	9/01/2023	Outstanding	eBay O*02-09551-68069	41.26	Supplies and Materials purchases
CC79682	19/01/2023	Outstanding	OFFICEWORKS	187.69	Supplies and Materials purchases
CC79595	20/01/2023	Outstanding	BIGW ONLINE	31.90	Supplies and Materials purchases

**People Experience Business Partner****4,000.00**

CC79639	19/01/2023	Outstanding	AU* SEEK MUL1674016022	4,000.00	Advertising
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**Recycling Supervisor****854.76**

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CC79581	9/01/2023	Outstanding	BUNNINGS 303000	168.39	Supplies and Materials purchases
CC79584	9/01/2023	Outstanding	COLES 0494	220.70	Consumables
CC79500	11/01/2023	Outstanding	COLES 0494	78.00	Consumables
CC79402	12/01/2023	Outstanding	WAVECOM INSTRUMENT	185.90	Supplies and Materials purchases
CC79656	19/01/2023	Outstanding	BUNNINGS 303000	201.77	Equipment purchases

**Senior Centre Coordinator****977.50**

000356	6/01/2023	Completed	SEC*CITY OF COCKBURN	66.50	Hire of equipment and facilities
000356	6/01/2023	Completed	SEC*CITY OF COCKBURN	57.00	Hire of equipment and facilities
000356	19/01/2023	Completed	SP JB HI-FI ONLINE	854.00	Equipment purchases

**Senior Centre Programs Booking Officer****4,682.45**

000380	9/01/2023	Completed	TICKETEK PTY LTD NSW	1,359.00	Events and Functions
000380	9/01/2023	Completed	TICKETEK PTY LTD NSW	679.50	Events and Functions
000380	9/01/2023	Completed	TICKETEK PTY LTD NSW	9.95	Events and Functions
000380	11/01/2023	Completed	DEPARTMENT OF TRANSPOR	94.00	Application, Licence, Registration Fees
000380	17/01/2023	Completed	BENTLEYPINEST230412NB	410.00	Events and Functions
000380	20/01/2023	Completed	GROSVENOR HOTEL	280.00	Events and Functions
000380	20/01/2023	Completed	THE PERTH MINT	221.00	Events and Functions
000380	25/01/2023	Completed	CITY OF PERTH	34.00	Events and Functions
000380	25/01/2023	Completed	NEW NORCIA SERVICES	1,595.00	Events and Functions

**Senior Library Manager****1,973.75**

000353	30/12/2022	Completed	BIGW ONLINE TALL	80.90	Supplies and Materials purchases
000353	30/12/2022	Completed	Games World Cockburn T	143.97	Supplies and Materials purchases
000353	5/01/2023	Completed	DOORDASH*PIZZA CRAFT T	122.29	Meeting/Workshop Catering
000353	13/01/2023	Completed	MYO*GREEN WORLD INDOOR	495.00	Hire of equipment and facilities
000353	13/01/2023	Completed	WANEWSDTI TALL	288.00	Supplies and Materials purchases
000353	16/01/2023	Completed	AMAZON AU TALL	629.99	Equipment purchases
000353	23/01/2023	Completed	COLES 0490 TALL	9.60	Supplies and Materials purchases
000353	30/01/2023	Completed	BIGW ONLINE TALL	166.00	Supplies and Materials purchases
000353	30/01/2023	Completed	TARGET 5036 TALL	38.00	Supplies and Materials purchases

**Senior Youth Justice and Outreach Worker****442.17**

000354	10/01/2023	Completed	DOT - LICENSING	162.90	Application, Licence, Registration Fees
000354	10/01/2023	Completed	DOT - LICENSING	20.40	Application, Licence, Registration Fees
000354	11/01/2023	Completed	OFFICEWORKS 0620	4.99	Supplies and Materials purchases

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000354	12/01/2023	Completed	Subway Jankadot	14.85	Consumables
000354	24/01/2023	Completed	DOT - LICENSING	46.80	Application, Licence, Registration Fees
000354	25/01/2023	Completed	CIRCUM WASH	18.00	Professional Services
000354	25/01/2023	Completed	COLES 0490	64.50	Consumables
000354	27/01/2023	Completed	MCDONALDS THOMSNS LKE	16.30	Consumables
000354	27/01/2023	Completed	MISS MAUD	34.95	Meeting/Workshop Catering
000354	27/01/2023	Completed	TONY ALE FRUIT & VEGET	24.98	Meeting/Workshop Catering
000354	27/01/2023	Completed	YELLOW RAVEN CAFE	33.50	Meeting/Workshop Catering

**Social Club Coordinator** **625.80**

CC79671	4/01/2023	Outstanding	Woolworths Online	193.00	Consumables
CC79555	23/01/2023	Outstanding	Woolworths Online	397.20	Consumables
CC79511	24/01/2023	Outstanding	WANEWSDTI	35.60	Subscriptions and Memberships

**Strategic Procurement Manager** **14,699.24**

000364	11/01/2023	Completed	Flight Centre	2,527.58	Travel and Accommodation
000364	16/01/2023	Completed	Flight Centre	3,730.56	Travel and Accommodation
000364	18/01/2023	Completed	CURTIN UNIVERSITY	4,276.23	Training & Professional Development
000364	24/01/2023	Completed	DEPT OF HOME AFFAIRS	650.37	Supplies and Materials purchases
000364	25/01/2023	Completed	AUSTRALIAN INST BUS	3,514.50	Training & Professional Development

**Support Services Lead** **1,509.24**

000336	11/01/2023	Completed	SPACETOCO VENUE HIRE	90.00	Hire of equipment and facilities
000336	12/01/2023	Completed	OFFICEWORKS 0620 JAND	61.51	Supplies and Materials purchases
000336	16/01/2023	Completed	Flight Centre	1,068.32	Travel and Accommodation
000336	16/01/2023	Completed	WILSON PARKING PER139	3.04	Parking Expenses
000336	17/01/2023	Completed	CPP HIS MAJESTYS	22.21	Parking Expenses
000336	17/01/2023	Completed	SEC* CITY OF COCKBURN	133.00	Hire of equipment and facilities
000336	18/01/2023	Completed	CPP HIS MAJESTYS	22.21	Parking Expenses
000336	20/01/2023	Completed	EB *Business Building	25.00	Application, Licence, Registration Fees
000336	23/01/2023	Completed	CHOICE SUBSCRIPTION	83.95	Subscriptions and Memberships

**Waste Collection Supervisor** **1,288.65**

CC79700	3/01/2023	Outstanding	ANNUAL FEE	24.00	Bank and other fees
000339	11/01/2023	Completed	BOSS INDUSTRIAL	954.70	Consumables
000338	13/01/2023	Completed	TOTAL TOOLS OCONNOR	9.95	Motor vehicle expenses
000338	27/01/2023	Completed	SQ *BIN BOMB PTY LTD	300.00	Consumables

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**Waste Services Manager**

**2,292.82**

CC79681	4/01/2023	Outstanding	OIL/ENERGY PTY LTD	214.50	
CC79607	6/01/2023	Outstanding	WASTE MANAGEMENT	1,817.32	Subscriptions and Memberships
CC79426	12/01/2023	Outstanding	POST HAMILTON HILL LHA	87.00	
CC79430	12/01/2023	Outstanding	POST HAMILTON HILL LHA	87.00	
CC79350	13/01/2023	Outstanding	POST HAMILTON HILL LHA	87.00	

**Young Peoples Services Coordinator**

**1,511.02**

000344	4/01/2023	Completed	BIGW ONLINE	119.50	Supplies and Materials purchases
000344	4/01/2023	Completed	Booktopia Pty Ltd	250.00	Supplies and Materials purchases
000344	4/01/2023	Completed	Booktopia Pty Ltd	107.58	Supplies and Materials purchases
000344	4/01/2023	Completed	MULTILIT PTY LIMITED	452.00	Supplies and Materials purchases
000344	4/01/2023	Completed	PAYPAL *TOYS R US	434.95	Supplies and Materials purchases
000344	6/01/2023	Completed	BIGW ONLINE	111.00	Supplies and Materials purchases
000344	18/01/2023	Completed	COLES 0490	2.00	Meeting/Workshop Catering
000344	18/01/2023	Completed	TONY ALE FRUIT & VEGET	16.99	Meeting/Workshop Catering
000344	19/01/2023	Completed	MUFFIN BREAK GATEWAY	17.00	Meeting/Workshop Catering

**Young Peoples Services Librarian**

**581.62**

000365	9/01/2023	Completed	BIGW ONLINE	52.50	Supplies and Materials purchases
000365	16/01/2023	Completed	BIG W 0455	53.00	Supplies and Materials purchases
000365	16/01/2023	Completed	Booktopia Pty Ltd	285.45	Supplies and Materials purchases
000365	16/01/2023	Completed	DYMOCKS BOORAGOON	178.88	Supplies and Materials purchases
000365	17/01/2023	Completed	WOOLWORTHS 4318	11.79	Supplies and Materials purchases

**Youth Centre Coordinator**

**618.70**

000355	3/01/2023	Completed	PAYPAL *SPOTIFY	15.99	Disputed Transaction
000355	18/01/2023	Completed	SPOTLIGHT PTY LTD	245.00	Supplies and Materials purchases
000355	19/01/2023	Completed	YELLOW RAVEN CAFE	9.00	Consumables
000355	23/01/2023	Completed	COLES 0490	43.60	Consumables
000355	23/01/2023	Completed	THE HOYTS CORPORATIO	300.00	Events and Functions
000355	25/01/2023	Completed	MISCELLANEOUS CREDIT	-15.99	Disputed Transaction
000355	30/01/2023	Completed	COLES 0490	21.10	Consumables

**Youth Development Officer**

**1,875.34**

CC79683	4/01/2023	Outstanding	THE HOYTS CORPORATIO	156.00	Events and Functions
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CC79659	5/01/2023	Outstanding	BOUNCE HOLDINGS AUSTRALIA	390.00	Events and Functions
CC79661	5/01/2023	Outstanding	BOUNCE HOLDINGS AUSTRALIA	390.00	Events and Functions
CC79663	5/01/2023	Outstanding	COLES 0490	78.25	Consumables
CC79636	6/01/2023	Outstanding	COLES ONLINE	116.96	Consumables
CC79586	9/01/2023	Outstanding	THE HOYTS CORPORATION	300.00	Events and Functions
CC79388	12/01/2023	Outstanding	COLES 0490	127.95	Consumables
CC79214	17/01/2023	Outstanding	COLES 0490	64.87	Consumables
CC79218	17/01/2023	Outstanding	COLES 0490	84.28	Consumables
CC79222	17/01/2023	Outstanding	COLES 0490	27.70	Consumables
CC79711	18/01/2023	Outstanding	COLES 0490	103.17	Consumables
CC79667	19/01/2023	Outstanding	COLES 0490	36.16	Consumables

**Total Cards - 61** **102,810.71**

Report Run On: 10-Mar-2023 16:39:03

February 2023 PAYMENT LISTING

MUNICIPAL FUND

PAYMENT No.	ACCOUNT No.	PAYEE	PAYMENT DESCRIPTION	DATE	VALUE \$
EF156329	28208	Art Plus Public Art	Artistic	3/02/2023	26,555.00
EF156330	10152	Aust Services Union	Payroll Deductions	6/02/2023	854.60
EF156331	10154	Australian Taxation Office	Payroll Deductions	6/02/2023	528,962.00
EF156332	10305	Child Support Agency	Payroll Deductions	6/02/2023	1,239.98
EF156333	11001	Local Government Racing & Cemeteries Employees Union Lgrceu	Payroll Deductions	6/02/2023	44.00
EF156334	19726	Health Insurance Fund Of Wa	Payroll Deductions	6/02/2023	1,066.80
EF156335	27874	Smartsalary	Salary Packaging/Leasing Administration	6/02/2023	12,055.68
EF156336	28458	Easi Group	Novated Leasing	6/02/2023	3,875.15
EF156337	10589	Fines Enforcement Registry	Fines Enforcement Fees	7/02/2023	3,888.00
EF156338	11308	Boss Industrial Formally Sba Supplies	Hardware Supplies	7/02/2023	2,651.92
EF156339	11619	Titan Ford	Purchase Of Vehicles & Servicing	7/02/2023	85,739.34
EF156340	13834	Sulo Mgb Australia Pty Ltd	Mobile Garbage Bins	7/02/2023	33,042.97
EF156341	21678	Iannello Designs	Graphic Design	7/02/2023	1,188.00
EF156342	26400	Premier Artists Pty Ltd	Entertainment Services	7/02/2023	41,250.00
EF156343	26470	Scp Conservation	Fencing Services	7/02/2023	11,836.00
EF156344	26987	Cti Risk Management	Security - Cash Collection	7/02/2023	629.55
EF156345	27334	Westcare Print	Printing Services	7/02/2023	269.50
EF156346	99997	Family Day Care	Fdc Payment W/E 5/2/2023	9/02/2023	49,733.31
EF156347	11741	Western Australian Treasury Corporation	Loan Repayments	13/02/2023	35,191.78
EF156348	12565	Southern Metro Regional Council - Loans	Loan Repayment	14/02/2023	342,982.38
EF156349	19533	Woolworths Ltd	Groceries	14/02/2023	3,106.29
EF156350	20535	Home-Grown Theatre	Drama Classes	14/02/2023	2,200.00
EF156351	22806	Chevron Australia Downstream Fuels Pty Ltd	Fuel Supplies	14/02/2023	132,423.71
EF156352	26512	Xcellerate It Pty Ltd	It Equipment - Ocr Project	14/02/2023	28,251.86
EF156353	26987	Cti Risk Management	Security - Cash Collection	14/02/2023	2,425.30
EF156354	27031	Downer Edi Works Pty Ltd	Asphalt Services	14/02/2023	234,037.12
EF156355	27154	Suez Recycling & Recovery Pty Ltd	Waste Services	14/02/2023	148,197.36
EF156356	27386	Gc Sales (Wa)	Garden Supplies	14/02/2023	1,960.20
EF156357	27523	Robert Lawrence Toohey	High Pressure Cleaning	14/02/2023	3,711.00
EF156358	27640	Range Ford	Motor Vehicles	14/02/2023	26,284.72
EF156359	27855	Total Landscape Redevelopment Service Pty Ltd	Tree Watering	14/02/2023	57,570.70
EF156360	27896	Rmc Rail Services Pty Ltd	Rail Traffic Management	14/02/2023	572.00
EF156361	28393	Sage Copywriting	Marketing/Copywriting	14/02/2023	2,598.75
EF156362	28408	Rc Vegetation Services Pty Ltd	Mowing Services	14/02/2023	6,025.93
EF156363	88888	Sundry Creditor Eft	Bond Refund	14/02/2023	500.00
EF156364	88888	Mr Jordan K Bunce	Bond Refund	14/02/2023	100.00
EF156365	99997	E Z Sears	Compost Bin Rebate	14/02/2023	50.00
EF156366	99997	Kurtis Woods	Crossover Rebate	14/02/2023	300.00
EF156367	99997	Colin Sia	Crossover Rebate 38 Coachwood, Treeby	14/02/2023	300.00
EF156368	99997	Vivian Roy Booysen	Crossover Rebate 11 Santorini, Coogee	14/02/2023	300.00
EF156369	99997	David N Moussa	Crossover Rebate, 22 Lisianthus, Beelihar	14/02/2023	300.00
EF156370	99997	Aden Da Luz	Grants, Donations & Refunds	14/02/2023	73.00
EF156371	99997	Gold Estates Holdings Pty Ltd	Grants, Donations & Refunds	14/02/2023	15,033.93
EF156372	99997	Justine Feng	Nappy And Sanitary Rebate - J Feng	14/02/2023	67.12
EF156373	99997	Nida Omo	Nappy And Sanitary Rebate - N Omo	14/02/2023	10.00
EF156374	99997	Wahida Ul-Haq	Nappy And Sanitary Rebate - W Ul-Haq	14/02/2023	50.00
EF156375	99997	Lucy Simons	Nappy And Sanitary Rebate - L Simons	14/02/2023	50.00

EF156376	99997	Mary Jordan	Food For Day Centre 24/01/23 - M Jordan	14/02/2023	225.39
EF156377	99997	Jack Pickering	Refund - Accidental Charge Mixed Netball	14/02/2023	73.00
EF156378	99997	Clement Thye	Refund Of Overcharge	14/02/2023	73.00
EF156379	99997	Sally Piotrowski	Refund_Client Deceased	14/02/2023	61.80
EF156380	99997	Home Instead Perth	Grants, Donations & Refunds	14/02/2023	9,482.20
EF156381	99997	Robyn Pow	Bird Bath Rebate	14/02/2023	15.00
EF156382	99997	Ann Eileen Mcminn	Bird Bath Rebate	14/02/2023	45.00
EF156383	99997	Laura Landau	Bird Bath Rebate	14/02/2023	49.50
EF156384	99997	Aubin Grove Community Association	Aubin Grove Community	14/02/2023	1,370.00
EF156385	99997	Ngaru Pou Inc	Performace Aus Day	14/02/2023	900.00
EF156386	99997	Samantha Standish	Staff Amenties - Marina	14/02/2023	43.35
EF156387	99997	Chandni Dhawan	Grants, Donations & Refunds	14/02/2023	18.00
EF156388	99997	Violas	Senior Security Subsidy Scheme	14/02/2023	200.00
EF156389	99997	Nevilla & Raymond Younger	Senior Security Subsidy Scheme	14/02/2023	300.00
EF156390	99997	Paul Sant & Hilda Janzen	Senior Security Subsidy Scheme	14/02/2023	200.00
EF156391	99997	Kate Donjerkovich	Senior Security Subsidy Scheme	14/02/2023	300.00
EF156392	99997	S & C Blackburn	Senior Security Subsidy Scheme	14/02/2023	200.00
EF156393	99997	Lynnette Hall	Senior Security Subsidy Scheme	14/02/2023	200.00
EF156394	99997	Margaret Watkins	Senior Security Subsidy Scheme	14/02/2023	100.00
EF156395	99997	Ivo Ruza Radovanovic	Senior Security Subsidy Scheme	14/02/2023	300.00
EF156396	99997	Tia & Mark Phillips	Docket H602914/1 Etfpos Refund	14/02/2023	945.40
EF156397	99997	Salma Hall	Grants, Donations & Refunds	14/02/2023	180.00
EF156398	99997	Jin Cheng Wu & Ting - Fang Yang	Grants, Donations & Refunds	14/02/2023	249.00
EF156399	99997	Inka Developments	Grants, Donations & Refunds	14/02/2023	171.65
EF156400	99997	Kevin Muir	2 X Urns Retavision	14/02/2023	243.20
EF156401	99997	Jandakot Bushfire Brigade	Jvbfb Reimbursement 9.2.23	14/02/2023	3,501.20
EF156402	99997	Marie Jordan	Cash Exp Claim 02.02.23	14/02/2023	156.25
EF156403	99997	Cockburn Community Men's Shed	Small Events Sponsorship	14/02/2023	3,000.00
EF156404	99997	Gemma Gibbons	Bird Bath Rebate	14/02/2023	14.50
EF156405	99997	L A Burrows	Bird Bath Rebate	14/02/2023	50.00
EF156406	99997	Melanie Irving	Bird Bath Rebate	14/02/2023	24.75
EF156407	99997	Catherine E. Loayza Tribeno	Bird Bath Rebate	14/02/2023	14.49
EF156408	99997	Coc Grants, Donations & Refunds	Grants, Donations & Refunds	14/02/2023	26.40
EF156409	99997	Miss Melanie Kate Marai	Bird Bath Rebate	14/02/2023	35.99
EF156410	99997	Daniel And Tamasen Miskiewicz	Bird Bath Rebate	14/02/2023	50.00
EF156411	99997	Carla Appelgrein	Bird Bath Rebate	14/02/2023	17.39
EF156412	99997	Lisa Holwerda	Bird Bath Rebate	14/02/2023	35.99
EF156413	99997	Sarah Holman	Bird Bath Rebate	14/02/2023	50.00
EF156414	99997	Mr Paul Douglas Eckersley	Bird Bath Rebate	14/02/2023	50.00
EF156415	99997	Coc Grants, Donations & Refunds	Grants, Donations & Refunds	14/02/2023	49.99
EF156416	99997	A Peck And A Warne	A.Peck Cash Exp 7.2.23	14/02/2023	170.00
EF156417	99997	Ngaru Pou Inc	Inv 1_Aust Day Coogee Event	14/02/2023	900.00
EF156418	99997	Stanley Muranda	Crossover Rebate	14/02/2023	300.00
EF156419	99997	Lakeland Senior High School P&C Assoc In	Community Grant-Sept 2022 Mentally Healt	14/02/2023	2,000.00
EF156420	11758	Water Corp Utility Account Only - Please Refer To 11760 When Raising Po	Water Usage / Sundry Charges	17/02/2023	24,948.99
EF156421	99996	K J Hockey	Rates and Property related refunds	16/02/2023	30.00
EF156422	99996	Madalina Didilescu	Rates and Property related refunds	16/02/2023	150.00
EF156423	99996	Department Of Fire & Emergency Services	Rates and Property related refunds	16/02/2023	2,839.54
EF156424	99996	Kenny Developments	Rates and Property related refunds	16/02/2023	320.00
EF156425	99996	Shelford Constructions Pty Ltd	Rates and Property related refunds	16/02/2023	295.00
EF156426	99996	James Best	Rates and Property related refunds	16/02/2023	877.21
EF156427	99996	Remo And Debra Piromalli	Rates and Property related refunds	16/02/2023	379.07
EF156428	99996	Holly He	Rates and Property related refunds	16/02/2023	527.74
EF156429	99996	Glen Ingraham	Rates and Property related refunds	16/02/2023	1,110.60

EF156430	99996	Clint Brenchley	Rates and Property related refunds	16/02/2023	1,212.00
EF156431	99996	Clint Williams	Rates and Property related refunds	16/02/2023	840.36
EF156432	99996	Mitchell Kent	Rates and Property related refunds	16/02/2023	215.50
EF156433	99996	Mr Thiam Lim	Rates and Property related refunds	16/02/2023	69.04
EF156434	99996	Muthuhara Saranga Dharmaratne	Rates and Property related refunds	16/02/2023	1,676.08
EF156435	99996	Bibra Lake Project Pty Ltd	Rates and Property related refunds	16/02/2023	519.10
EF156436	10035	Adventure World	Entertainment Services	17/02/2023	1,455.00
EF156437	10058	Alsco Pty Ltd	Hygiene Services/Supplies	17/02/2023	325.28
EF156438	10091	Aslab Pty Ltd	Asphalting Services/Supplies	17/02/2023	5,726.16
EF156439	10184	Benara Nurseries	Plants	17/02/2023	610.50
EF156440	10207	Boc Gases	Gas Supplies	17/02/2023	298.03
EF156441	10226	Bridgestone Australia Ltd	Tyre Services	17/02/2023	32,787.29
EF156442	10231	Brooks Hire	Hire Services - Equipment	17/02/2023	20,070.48
EF156443	10244	Building & Const Industry Training Fund	Levy Payment	17/02/2023	16,659.74
EF156444	10246	Bunnings Building Supplies Pty Ltd	Hardware Supplies	17/02/2023	2,664.27
EF156445	10333	Cjd Equipment Pty Ltd	Hardware Supplies	17/02/2023	10,920.63
EF156446	10359	Cockburn Painting Service	Painting Supplies/Services	17/02/2023	9,878.00
EF156447	10368	Cockburn Wetlands Education Centre	Community Grant	17/02/2023	588.00
EF156448	10484	Department Of Mines, Industry Regulation And Safety	Building Services Levy	17/02/2023	37,317.37
EF156449	10535	Workpower Incorporated	Employment Services - Planting	17/02/2023	26,035.72
EF156450	10590	Department Of Fire And Emergency Services	Esl Levy & Related Costs	17/02/2023	4,435.40
EF156451	10683	Gronbek Security	Locksmith Services	17/02/2023	439.03
EF156452	10783	Jandakot Metal Industries Pty Ltd	Metal Supplies	17/02/2023	564.30
EF156453	10784	Jandakot Primary School	School Graduation Awards 2011	17/02/2023	300.00
EF156454	10879	Les Mills Aerobics	Instruction/Training Services	17/02/2023	3,196.94
EF156455	10888	Lj Caterers	Catering Services	17/02/2023	4,862.88
EF156456	10913	Bucher Municipal Pty Ltd	Purchase Of New Plant / Repair Services	17/02/2023	7,890.44
EF156457	10923	Major Motors Pty Ltd	Repairs/Maintenance Services	17/02/2023	151.25
EF156458	10938	Mrp Pest Management	Pest & Weed Management	17/02/2023	2,179.26
EF156459	10944	Mcleods	Legal Services	17/02/2023	10,808.25
EF156460	10991	Beacon Equipment	Mowing Equipment	17/02/2023	2,960.55
EF156461	11028	Neverfail Springwater Ltd	Bottled Water Supplies	17/02/2023	499.15
EF156462	11036	Northlake Electrical Pty Ltd	Electrical Services	17/02/2023	182,295.53
EF156463	11112	Perth Airport Municipalities Group	Membership Renewal	17/02/2023	500.00
EF156464	11177	Pitney Bowes Australia Pty Ltd	Gis Software	17/02/2023	1,171.50
EF156465	11182	Premium Brake & Clutch Services Pty Ltd	Brake Services	17/02/2023	1,346.40
EF156466	11244	Research Solutions Pty Ltd	Research Services	17/02/2023	5,089.89
EF156467	11247	Richgro Wa	Gardening Supplies	17/02/2023	201.96
EF156468	11307	Satellite Security Services Pty Ltd	Security Services	17/02/2023	2,621.30
EF156469	11308	Boss Industrial Formally Sba Supplies	Hardware Supplies	17/02/2023	308.00
EF156470	11334	Shenton Pumps	Pool Equipment/Services	17/02/2023	32,616.53
EF156471	11364	Silver Chain Group Limited	Home Care/Respite Services	17/02/2023	88.00
EF156472	11375	Slater Gartrell Sports	Sport Supplies	17/02/2023	1,749.00
EF156473	11425	Resource Recovery Group	Waste Disposal Gate Fees	17/02/2023	2,640.00
EF156474	11465	Speedo Australia Pty Ltd	Swim Wear Supplies	17/02/2023	7,041.65
EF156475	11469	Sports Turf Technology Pty Ltd	Turf Consultancy Services	17/02/2023	21,650.75
EF156476	11483	St John Ambulance Aust Wa Operations	First Aid Courses	17/02/2023	1,776.50
EF156477	11556	Technifire 2000	Fire Fighting Equipment	17/02/2023	1,348.27
EF156478	11557	Technology One Ltd	It Consultancy Services	17/02/2023	2,365.00
EF156479	11625	Nutrien Water	Reticulation Supplies	17/02/2023	23,302.32
EF156480	11651	Tree Watering Services	Tree Watering Services	17/02/2023	6,241.50
EF156481	11722	Wa Hino Sales & Service	Purchase Of New Trucks / Maintenance	17/02/2023	826.41
EF156482	11789	Walga	Advertising/Training Services	17/02/2023	858.00
EF156483	11793	Western Irrigation Pty Ltd	Irrigation Services/Supplies	17/02/2023	31,239.90

EF156484	11806	Westrac Pty Ltd	Repairs/Mtnce - Earthmoving Equipment	17/02/2023	12,519.82
EF156485	11854	Zipform Pty Ltd	Printing Services	17/02/2023	5,055.68
EF156486	11873	Wattleup Tractors	Hardware Supplies	17/02/2023	968.00
EF156487	11985	Ivo Grubelich	Bus Hire	17/02/2023	12,574.58
EF156488	12014	Tutt Bryant Equipment Bt Equipment Pty Ltd T/As	Excavating/Earthmoving Equipment	17/02/2023	17,177.93
EF156489	12153	Hays Personnel Services Pty Ltd	Employment Services	17/02/2023	13,498.73
EF156490	12295	Stewart & Heaton Clothing Co. Pty Ltd	Clothing Supplies	17/02/2023	10,653.32
EF156491	12656	Coogee Beach Surf Lifesaving Club Inc	Poore Grove Slsc Development Costs	17/02/2023	2,025.00
EF156492	12693	Suffling, Thomas James T/A Rivergods	Outdoor Activities - Rafting	17/02/2023	1,650.00
EF156493	13563	Green Skills Inc	Employment Services	17/02/2023	36,101.11
EF156494	13825	Jackson Mcdonald	Legal Services	17/02/2023	5,895.45
EF156495	13873	Cockburn Ses	Traffic Management Services	17/02/2023	3,300.00
EF156496	14530	Donald Veal Consultants Pty Ltd	Consultancy Services	17/02/2023	22,319.00
EF156497	15393	Stratagreen	Hardware Supplies	17/02/2023	1,018.71
EF156498	15588	Natural Area Consulting Management Services	Weed Spraying	17/02/2023	43,040.54
EF156499	16064	Cms Engineering	Airconditioning Services	17/02/2023	27,294.94
EF156500	16107	Wren Oil	Waste Disposal Services	17/02/2023	33.00
EF156501	16396	Mayday Rental	Road Construction Machine Hire	17/02/2023	14,036.00
EF156502	16533	Total Packaging	Packaging	17/02/2023	463.38
EF156503	16653	Complete Portables Pty Ltd	Supply & Hire Of Modular Buildings	17/02/2023	1,022.58
EF156504	16846	Action Glass & Aluminium	Glazing Services	17/02/2023	299.75
EF156505	16985	Wa Premix	Concrete Supplies	17/02/2023	2,341.68
EF156506	17471	Pirtek (Fremantle) Pty Ltd	Hoses & Fittings	17/02/2023	129.40
EF156507	17553	Altus Traffic Pty Ltd	Traffic Control Services	17/02/2023	21,235.97
EF156508	17600	Lightforce Asset Pty Ltd (Erections)	Guard Rails	17/02/2023	2,160.00
EF156509	17608	Nu-Trac Rural Contracting	Beach Cleaning/Firebreak Construction	17/02/2023	19,857.96
EF156510	17927	Sharyn Egan	Artistic Services	17/02/2023	715.00
EF156511	18073	Paramount Security Services	Security Services	17/02/2023	23,267.75
EF156512	18122	Signman	Signage	17/02/2023	3,146.00
EF156513	18203	Natsync Environmental	Pest Control	17/02/2023	3,392.50
EF156514	18216	Regen4 Environmental Services	Consultancy - Environmental	17/02/2023	532.40
EF156515	18272	Austraclear Limited	Investment Services	17/02/2023	306.85
EF156516	18533	Friends Of The Community Inc.	Donation	17/02/2023	6,125.00
EF156517	18962	Sealanes (1985) P/L	Catering Supplies	17/02/2023	2,721.07
EF156518	19533	Woolworths Ltd	Groceries	17/02/2023	15.60
EF156519	19617	Digital Scanning & Microfilm Equipment - Ds & Me	Microfilm Equipment	17/02/2023	660.00
EF156520	20000	Aust West Auto Electrical Pty Ltd	Auto Electrical Services	17/02/2023	9,575.61
EF156521	20146	Data#3 Limited	Contract It Personnel & Software	17/02/2023	93,557.93
EF156522	20885	Tactile Indicators Perth	Tactiles	17/02/2023	4,400.00
EF156523	21005	Brain Teasers Oz Pty Ltd	Trivia Packs	17/02/2023	924.00
EF156524	21291	The Worm Shed	Environmental Education	17/02/2023	140.00
EF156525	21294	Cat Haven	Animal Services	17/02/2023	4,147.28
EF156526	21469	John Hughes Volkswagon	Purchase Of New Vehicle	17/02/2023	11,811.30
EF156527	21577	Lavan	Legal Services	17/02/2023	35,279.53
EF156528	21627	Manheim Pty Ltd	Impounded Vehicles	17/02/2023	2,127.40
EF156529	21665	Mmj Real Estate (Wa) Pty Ltd	Property Management Services	17/02/2023	317.14
EF156530	21697	Ict Express Pty Ltd	Consultancy Services - It	17/02/2023	2,035.00
EF156531	21744	Jb Hi Fi - Commercial	Electronic Equipment	17/02/2023	711.97
EF156532	21747	Unicare Health	Wheelchair Hire	17/02/2023	726.00
EF156533	21782	Westcoast Timber Flooring	Flooring Supplies	17/02/2023	13,420.00
EF156534	21798	The Civil Group	Consultancy - Engineering	17/02/2023	605.00
EF156535	21946	Ryan's Quality Meats	Meat Supplies	17/02/2023	1,161.30
EF156536	22106	Intellife Group	Services - Daip	17/02/2023	14,040.98
EF156537	22388	Carrington's Traffic Services	Traffic Management Services	17/02/2023	561.00

EF156538	22404	Cleverpatch Pty Ltd	Arts/Craft Supplies	17/02/2023	3,492.80
EF156539	22541	Surfing Western Australia Inc.	Training Services - Surfing	17/02/2023	1,200.00
EF156540	22569	Sonic Health Plus Pty Ltd	Medical Services	17/02/2023	209.00
EF156541	22589	Jb Hi Fi - Cockburn	Electrical Equipment	17/02/2023	1,340.00
EF156542	22623	Landmark Products Ltd	Landscape Infrastructure	17/02/2023	7,291.01
EF156543	22658	South East Regional Centre For Urban Landcare Inc (Sercul)	Urban Landcare Services	17/02/2023	10,720.95
EF156544	22806	Chevron Australia Downstream Fuels Pty Ltd	Fuel Supplies	17/02/2023	73,525.93
EF156545	22859	Top Of The Ladder	Gutter Cleaning Services	17/02/2023	2,299.00
EF156546	22913	Opal Australian Paper	Envelopes	17/02/2023	130.41
EF156547	23215	Melville Mazda	Purchase Of New Vehicles	17/02/2023	85.25
EF156548	23258	Carlisle Event Hire Pty Ltd	Hire-Party/Function Equipment	17/02/2023	5,561.60
EF156549	23288	Ariane Roemmele	Amusement - Children's Activities	17/02/2023	1,250.00
EF156550	23298	Therapy Focus	Entertainment Services	17/02/2023	582.98
EF156551	23457	Totally Workwear Fremantle	Clothing - Uniforms	17/02/2023	3,822.99
EF156552	23506	Hudson Global Resources (Aust) Pty Ltd	Human Resources Consult	17/02/2023	24,946.80
EF156553	23570	A Proud Landmark Pty Ltd	Landscape Construction Services	17/02/2023	26,206.40
EF156554	23767	Public Transport Authority Western Australia	Consultancy Services - Planning	17/02/2023	9,664.64
EF156555	23971	Find Wise Location Services	Locating Services - Underground	17/02/2023	558.80
EF156556	24275	Truck Centre Wa Pty Ltd	Purchase Of New Truck	17/02/2023	6,368.64
EF156557	24298	Tanks For Hire	Equipment Hire	17/02/2023	1,710.50
EF156558	24643	Bibliotheca Rfid Library Systems Australia Pty Ltd	Purchase Of Library Tags	17/02/2023	1,173.06
EF156559	24655	Automasters Spearwood	Vehicle Servicing	17/02/2023	3,369.10
EF156560	24736	Zenien	Cctv Camera Licences	17/02/2023	26,111.60
EF156561	24748	Pearmans Electrical & Mechanical Services P/L	Electrical Services	17/02/2023	4,987.17
EF156562	24816	Consolidated Training Services	Training Services	17/02/2023	2,016.00
EF156563	24945	Bridge 42 Pty Ltd	Project Management Services	17/02/2023	8,548.10
EF156564	24978	Ambius	Plants Supplies	17/02/2023	58.96
EF156565	25063	Superior Pak Pty Ltd	Vehicle Maintenance	17/02/2023	416.63
EF156566	25110	Toyota Material Handling Pty Ltd	Spare Parts	17/02/2023	379.50
EF156567	25121	Imagesource Digital Solutions	Billboards	17/02/2023	3,712.50
EF156568	25127	Milmar Distributors	Printing Services - Id Cards	17/02/2023	186.00
EF156569	25128	Horizon West Landscape & Irrigation Pty Ltd	Landscaping Services	17/02/2023	36,441.43
EF156570	25264	Acurix Networks Pty Ltd	Wifi Access Service	17/02/2023	6,360.20
EF156571	25418	Cs Legal	Legal Services	17/02/2023	9,708.20
EF156572	25586	Envirovap Pty Ltd	Hire Of Leachate Units	17/02/2023	29,012.50
EF156573	25645	Yelakitj Moort Nyungar Association Inc	Welcome To The Country Performances	17/02/2023	800.00
EF156574	25813	Lg Connect Pty Ltd	Erp Systems Development	17/02/2023	2,987.78
EF156575	26067	Sprayking Wa Pty Ltd	Chemical Weed Control Services	17/02/2023	16,010.50
EF156576	26114	Grace Records Management	Records Management Services	17/02/2023	1,402.81
EF156577	26195	Play Check	Consulting Services	17/02/2023	825.00
EF156578	26211	Amcom Pty Ltd	Internet/Data Services	17/02/2023	9,078.30
EF156579	26257	Paperbark Technologies Pty Ltd	Arboricultural Consultancy Services	17/02/2023	18,564.15
EF156580	26303	Gecko Contracting Turf & Landscape Maintenance	Turf & Landscape Maintenance	17/02/2023	216,128.69
EF156581	26314	Cpe Group	Temporary Employment Services	17/02/2023	14,436.45
EF156582	26403	Ches Power Group Pty Ltd	Engineering Solutions / Back Up Generato	17/02/2023	3,500.95
EF156583	26470	Scp Conservation	Fencing Services	17/02/2023	55,472.51
EF156584	26516	Ultimate Limestone	Construction Services	17/02/2023	3,850.00
EF156585	26547	Pj Wright & Associates Pty Ltd	Consultancy Services	17/02/2023	3,520.00
EF156586	26558	Healthcare Australia Pty Ltd	Temporary Employment Services	17/02/2023	2,896.92
EF156587	26614	Marketforce Pty Ltd	Advertising	17/02/2023	2,809.12
EF156588	26618	Global Spill Control Pty Ltd	Road Safety Products	17/02/2023	92.27
EF156589	26639	Safeguard Industries	Security Doors, Screens And Roller Shutt	17/02/2023	891.00
EF156590	26651	Faunatrack	Fauna Survey	17/02/2023	20,592.00
EF156591	26677	Australia And New Zealand Recycling Platform Limited	Not- For-Profit Member Services Body	17/02/2023	1,842.47

EF156592	26705	Creative Adm	Marketing Services	17/02/2023	9,135.50
EF156593	26709	Talis Consultants Pty Ltd	Waste Consultancy	17/02/2023	4,403.19
EF156594	26735	Shane Mcmaster Surveys	Survey Services	17/02/2023	11,330.00
EF156595	26739	Kerb Doctor	Kerb Maintenance	17/02/2023	59,723.09
EF156596	26745	Embroidme Myaree	Embroidery	17/02/2023	531.30
EF156597	26761	The Sand Card Company	Entertainment Services	17/02/2023	995.00
EF156598	26773	Laser Corps Combat Adventruers	Entry Fees	17/02/2023	2,138.00
EF156599	26778	Robert Walters	Recruitment Services	17/02/2023	6,816.82
EF156600	26782	Soft Landing	Recycling Services	17/02/2023	81,885.65
EF156601	26783	Leslie Hinton	Entertainment	17/02/2023	1,210.00
EF156602	26791	Monsterball Amusement & Hire	Amusement Hire	17/02/2023	4,989.99
EF156603	26867	Einsteins Australia	Childrens Workshops	17/02/2023	726.00
EF156604	26888	Media Engine	Graphic Design, Marketing, Video Product	17/02/2023	9,126.00
EF156605	26898	Spandex Asia Pacific Pty Ltd	Signage Supplier	17/02/2023	5,748.23
EF156606	26917	Cirrus Networks Pty Ltd	It Network & Telephony Services	17/02/2023	23,407.89
EF156607	26929	Elan Energy Matrix Pty Ltd	Recycling Services	17/02/2023	1,446.50
EF156608	26944	Mrkvi	Master Ceremonies	17/02/2023	1,210.00
EF156609	26957	Jbs & G Australia Pty Ltd	Consultancy - Enviromental	17/02/2023	2,750.00
EF156610	26964	South Metropolitan Tafe	Education	17/02/2023	2,117.12
EF156611	26985	Access Icon Pty Ltd	Drainage Products	17/02/2023	5,646.01
EF156612	26987	Cti Risk Management	Security - Cash Collection	17/02/2023	2,163.80
EF156613	26988	Bladon Wa Pty Ltd	Promotional Products	17/02/2023	21,986.25
EF156614	27002	Cockburn Party Hire	Hire Services	17/02/2023	3,478.00
EF156615	27010	Quantum Building Services Pty Ltd	Building Maintenance	17/02/2023	19,428.50
EF156616	27015	Intelli Trac	Gps Tracking	17/02/2023	2,685.10
EF156617	27023	Solargain Pv Pty Ltd	Solar Energy Provider	17/02/2023	1,224.00
EF156618	27031	Downer Edi Works Pty Ltd	Asphalt Services	17/02/2023	151,904.71
EF156619	27032	Wtp Australia Pty Ltd	Quantity Surveyors	17/02/2023	1,584.00
EF156620	27034	Adelby Pty Ltd	Firebreak Construction	17/02/2023	253.00
EF156621	27044	Graffiti Systems Australia	Graffiti Removal & Anti-Graffiti Coating	17/02/2023	17,017.00
EF156622	27046	Tfh Hire Services Pty Ltd	Hire Fencing	17/02/2023	1,990.72
EF156623	27052	Event Marquees	Marquee Hire	17/02/2023	3,389.40
EF156624	27054	Vocus Pty Ltd	Telecommunications	17/02/2023	15,909.75
EF156625	27059	Frontline Fire & Rescue Equipment	Manufacture-Fire Vehicles/Equipment	17/02/2023	8,300.70
EF156626	27065	Westbooks	Books	17/02/2023	6,521.14
EF156627	27075	Colleagues Nagels	Printing Services	17/02/2023	3,193.05
EF156628	27082	Kulbardi Pty Ltd	Stationery Supplies	17/02/2023	2,810.39
EF156629	27098	Q2 (Q-Squared)	Digital Data Service	17/02/2023	1,100.00
EF156630	27130	Motio Play Pty Ltd	Digital Marketing & Software Service Pro	17/02/2023	2,815.61
EF156631	27131	West Coast Commercial Industries	Lockers	17/02/2023	273.90
EF156632	27177	Initial Hygiene	Hygiene	17/02/2023	3,524.05
EF156633	27189	Healthstrong Pty Ltd	Home Care	17/02/2023	198.00
EF156634	27198	Green Promotions Pty Ltd	Promotional Supplies	17/02/2023	13,134.00
EF156635	27237	Lobel Events	Event Lighting	17/02/2023	22,268.68
EF156636	27241	Landscape Elements	Landscaping Services	17/02/2023	59,552.14
EF156637	27246	Veale Auto Parts	Spare Parts Mechanical	17/02/2023	67.30
EF156638	27269	Payrix Australia	Payment Processing	17/02/2023	11,742.81
EF156639	27346	Office Line	Furniture Office	17/02/2023	17,694.60
EF156640	27348	Message Media	Telecommunications	17/02/2023	490.24
EF156641	27352	Bikewise	Transport Promotions	17/02/2023	440.00
EF156642	27355	Playmaster	Playground Equipment	17/02/2023	643.50
EF156643	27385	Programmed Electrical Technologies	Electrical Services	17/02/2023	287.50
EF156644	27396	Ankeet Mehta Spearwood Newspaper Round Delivery	Newspaper Delivery	17/02/2023	57.85
EF156645	27401	Emprise Mobility	Mobility Equipment	17/02/2023	2,486.40

EF156646	27403	Freedom Fairies Pty Ltd	Amusement	17/02/2023	2,667.50
EF156647	27427	Home Chef	Cooking/Food Services	17/02/2023	433.75
EF156648	27437	Pb Reticulation & Maintenance Services Pty Ltd	Irrigation Services	17/02/2023	176.00
EF156649	27448	Selectro Services Pty Ltd	Electrical	17/02/2023	1,889.58
EF156650	27450	Aaa Production Services	Hire Pa/Satge Systems	17/02/2023	86,248.06
EF156651	27455	Site Protective Services	Cctv Parts	17/02/2023	31,823.07
EF156652	27499	Hodge Collard Preston Architects	Architects	17/02/2023	2,750.00
EF156653	27507	Serco Facilities Management Pty Ltd	Cleaning Services	17/02/2023	6,280.75
EF156654	27518	Kyocera Document Solutions Australia Pty Ltd	Photocopying Machines	17/02/2023	2,326.27
EF156655	27534	Ralph Beattie Bosworth	Quantity Survey	17/02/2023	2,172.50
EF156656	27539	Jasmin Carpentry & Maintenance	Carpentry	17/02/2023	4,884.00
EF156657	27548	Standing Fork	Catering	17/02/2023	8,184.00
EF156658	27566	Thuroona Services	Asbestos Removal	17/02/2023	1,584.00
EF156659	27575	Shred X Secure Destruction	Document Destruction	17/02/2023	44.26
EF156660	27579	Soco Studios	Photography Services	17/02/2023	1,694.00
EF156661	27592	Hey Jay Fix It!! Home Maintenance Service	Home Maintenance	17/02/2023	240.00
EF156662	27617	Atturra Business Applications	Consultancy - It	17/02/2023	11,550.00
EF156663	27622	Truegrade Medical Supplies	Medical Supplies	17/02/2023	5,556.17
EF156664	27631	Aquatic Services Wa Pty Ltd	Pool Equipment & Maintenance	17/02/2023	996.82
EF156665	27635	Mammoth Security	Security	17/02/2023	50.09
EF156666	27650	Datacom Systems (Au) Pty Ltd	It Sales, Consulting & Service	17/02/2023	203.74
EF156667	27657	Positive Balance Massage	Massage Therapy	17/02/2023	400.00
EF156668	27660	Future Power Wa Pty Ltd	Electrical	17/02/2023	3,437.50
EF156669	27664	Disability Awareness Training	Training Disabilities	17/02/2023	1,400.00
EF156670	27676	Blue Force Pty Ltd	Security Services	17/02/2023	66,891.01
EF156671	27695	Qtm Pty Ltd	Traffic Management	17/02/2023	70,861.85
EF156672	27699	Microway	Software	17/02/2023	4,326.76
EF156673	27701	Perth Better Homes	Shade Sails	17/02/2023	59,433.72
EF156674	27754	Trugrade Pty Ltd	Graffiti Products	17/02/2023	1,723.92
EF156675	27797	City Lift Services Pty Ltd	Lift Maintenance	17/02/2023	643.50
EF156676	27817	Squashworld Hilton	Hiring Services	17/02/2023	600.00
EF156677	27818	Modus Compliance Pty Ltd	Consultant Engineering	17/02/2023	990.00
EF156678	27829	Smec Australia Pty Ltd	Consultancy - Engineering	17/02/2023	28,977.91
EF156679	27831	Butler And Brown	Event Management	17/02/2023	33,000.00
EF156680	27850	Dowsing Group Pty Ltd	Concreting Services	17/02/2023	196,724.97
EF156681	27852	First 5 Minutes Pty Ltd	Training & Education	17/02/2023	814.00
EF156682	27855	Total Landscape Redevelopment Service Pty Ltd	Tree Watering	17/02/2023	60,214.00
EF156683	27865	Pritchard Francis Consulting Pty Ltd	Engineering Services	17/02/2023	2,599.30
EF156684	27868	The Basketball Man	Basketball Equipment	17/02/2023	1,210.00
EF156685	27879	Precision Badges Wa	Badges	17/02/2023	435.82
EF156686	27885	Stevens Mcgann Willcock And Copping Pty Ltd	Consultancy - Mechanical Engineering	17/02/2023	462.00
EF156687	27886	Bbc Entertainment	Entertainment Agency	17/02/2023	2,090.00
EF156688	27887	The Wilding Project	Sports/Exercise Classes	17/02/2023	1,440.00
EF156689	27894	Homecare Physiotherapy	Healthcare	17/02/2023	14,677.27
EF156690	27899	Nature Calls Portable Toilets	Hire - Portable Loos	17/02/2023	1,327.50
EF156691	27909	Fe Technologies	Rfid Equipment And Tags	17/02/2023	1,069.20
EF156692	27917	Go Doors Advanced Automation	Door Maintenance & Repair	17/02/2023	4,186.60
EF156693	27944	Nedlands Yacht Club Inc	Yacht Club	17/02/2023	1,870.00
EF156694	27946	Kambarang Services Pty Ltd	Training Indigenous Cultute	17/02/2023	2,750.00
EF156695	27965	Stantec Australia Pty Ltd	Engineering Services	17/02/2023	21,621.60
EF156696	28003	Taylor Made Design	Graphic Design	17/02/2023	517.00
EF156697	28031	Brandon's Shredding Boxes	Recycling	17/02/2023	60.00
EF156698	28049	Copy Magic	Printing Services	17/02/2023	1,166.00
EF156699	28061	Go2cup	Paper Cups	17/02/2023	1,336.50



EF156700	28070	Marlbroh Bingo Enterprises	Printing Services	17/02/2023	324.00
EF156701	28078	Crayon Australia Pty Ltd	Licensing	17/02/2023	9.70
EF156702	28080	Yacht Grot 1985 Pty Ltd	Marine	17/02/2023	1,858.12
EF156703	28082	For Blue Pty Ltd	Consultancy - Economic	17/02/2023	3,025.00
EF156704	28136	Shore Water Marine Pty Ltd	Marine Repair & Maintenance Services	17/02/2023	18,232.50
EF156705	28144	Baby Bunting	Sanitary Supplies	17/02/2023	664.50
EF156706	28164	Aaro Group Pty Ltd	Drainage Services	17/02/2023	9,751.50
EF156707	28168	Sifting Sands	Sand Cleaning	17/02/2023	4,655.75
EF156708	28181	Seaview Rentals	Aquarium Servicing	17/02/2023	98.00
EF156709	28184	Spearwood Veterinary Hospital	Veterinary Hospital	17/02/2023	450.00
EF156710	28189	Mercury Messengers Pty Ltd	Courier Service	17/02/2023	1,930.75
EF156711	28190	People On Bicycles Pty Ltd	Bicycle Education	17/02/2023	760.00
EF156712	28191	Enviro Sweep	Sweeping Services	17/02/2023	4,719.00
EF156713	28196	Brightmark Group Pty Ltd	Cleaning Services	17/02/2023	31,829.07
EF156714	28197	Lite N Easy Pty Ltd	Food Supplies	17/02/2023	230.95
EF156715	28201	Select Fresh	Food Supplies	17/02/2023	1,195.92
EF156716	28206	Perth Region Tourism Organisation Inc	Tourism	17/02/2023	2,145.00
EF156717	28211	Nordic Fitness Equipment	Fitness Equipment	17/02/2023	2,970.00
EF156718	28214	Beyond Skateboarding	Skateboarding Clinics	17/02/2023	800.00
EF156719	28215	Complete Office Supplies Pty Ltd	Stationery	17/02/2023	5,026.01
EF156720	28218	Laminar Capital Pty Ltd	Financial Services	17/02/2023	1,496.00
EF156721	28231	Typeset Pty Ltd	Editorial And Business Communications Se	17/02/2023	2,640.00
EF156722	28233	Western Maze Wa Pty Ltd	Waste Collection Services	17/02/2023	47,976.50
EF156723	28241	Swift Flow Pty Ltd	Plumbing	17/02/2023	44,276.24
EF156724	28247	Fremantle City Centre Podiatry	Consultancy - Heritage	17/02/2023	85.80
EF156725	28251	Kids Just Wanna Have Fun Amusement Hire	Hire Services	17/02/2023	1,500.00
EF156726	28254	Cleantex Pty Ltd	Laundry Service	17/02/2023	432.98
EF156727	28258	Garden Care West	Gardening Services	17/02/2023	3,374.25
EF156728	28264	Garden Organics	Organics Processing	17/02/2023	43,859.70
EF156729	28265	Tree Care Wa	Vegetation Maintenance Services	17/02/2023	19,722.12
EF156730	28267	C-Wise	Composts Manufacturing	17/02/2023	19,536.00
EF156731	28270	Volunteer Home Support	Aged Care	17/02/2023	312.79
EF156732	28275	Farrington Dry Cleaners	Dry Cleaning	17/02/2023	60.00
EF156733	28277	Gesha Coffee Co	Coffee Supplies	17/02/2023	1,826.20
EF156734	28283	Mills Recruitment	Recruitment Services	17/02/2023	11,815.65
EF156735	28284	Urban Jungle Indoor Rock Climbing	Rock Climbing	17/02/2023	528.00
EF156736	28289	Grafton General Products	Mobility Equipment	17/02/2023	6,425.10
EF156737	28302	Ohura Group Pty Ltd	Industrial Relations Consulting	17/02/2023	4,872.00
EF156738	28303	Miracle Recreation Equipment	Playground Equipment	17/02/2023	9,281.14
EF156739	28304	Moray & Agnew	Legal Services	17/02/2023	207.68
EF156740	28318	Ati-Mirage	Training	17/02/2023	1,155.00
EF156741	28321	Perth Tactiles Pty Ltd	Tactiles	17/02/2023	2,016.00
EF156742	28329	The Rigging Shed	Lifting Equipment And Testing	17/02/2023	425.70
EF156743	28349	Cable Locates & Consulting	Underground Utility Location And Survey	17/02/2023	8,600.90
EF156744	28361	Indoor Gardens Pty Ltd	Hiring Indoor Plants	17/02/2023	765.60
EF156745	28364	Behind Pty Ltd	Entertainment - Band	17/02/2023	1,925.00
EF156746	28371	Flexi Staff	Employment Services	17/02/2023	50,715.34
EF156747	28373	Bos Civil	Civil Construction & Landscaping	17/02/2023	50,529.88
EF156748	28388	Professional Search Group Pty Ltd	Recruitment Services	17/02/2023	352.57
EF156749	28390	Logo Appointments	Employment Services	17/02/2023	2,576.89
EF156750	28392	Mcs Civil Contracting	Engineering/Earthworks	17/02/2023	11,924.00
EF156751	28396	Industrial Decontamination Services Pty Ltd	Decontamination Services	17/02/2023	203.02
EF156752	28409	Sanpoint Pty Ltd (Ld Total)	Landscape Services	17/02/2023	30,133.02
EF156753	28414	Jaye Lee Snowden	Aboriginal Engagement Consulting	17/02/2023	1,700.00

EF156754	28419	Adam Puffler	Scooter Events	17/02/2023	730.00
EF156755	28423	Jordies Garden Bags	Waste Services	17/02/2023	3,341.25
EF156756	28426	Power Paving Pty Ltd	Paving Services	17/02/2023	6,160.00
EF156757	28428	Wa Bolts Pty Ltd	Fixings & Fasteners	17/02/2023	7.28
EF156758	28437	Building & Industrial Cleaning Services	Clenaing Services	17/02/2023	57,300.39
EF156759	28443	Kilmore Group Pty Ltd	Construction	17/02/2023	51,648.73
EF156760	28446	Simon Mullumby	Entertainment Band	17/02/2023	900.00
EF157087	28443	Kilmore Group Pty Ltd	Construction	28/02/2023	22,135.17
EF156762	28454	Aussie Natural Spring Water	Water Supplies	17/02/2023	86.10
EF156763	28460	Mermaid Melody	Entertainment	17/02/2023	890.00
EF156764	28461	Carealrt	Entertainment	17/02/2023	6.49
EF156765	28463	Antree Dnh Pty Ltd	Gardening	17/02/2023	686.40
EF156766	28468	Mcnaught Media Pty Ltd	Marketing	17/02/2023	495.00
EF156767	28469	Committee For Perth Limited	Committee For Perth	17/02/2023	7,260.00
EF156761	28453	Kit Prendergast	Native bee surveys	17/02/2023	1,100.00
EF156768	28472	Mma Subsea Services Pty Ltd	Environmental Approvals	17/02/2023	33,687.50
EF156769	28484	Eds Environmental Visus Group Pty Ltd	Environmental Approvals	17/02/2023	528.00
EF156771	88888	Dan Pupovac	Bond Refund	17/02/2023	150.00
EF156772	99997	Centrepont Church	Community Grant Xmas Carnival & Easter	17/02/2023	6,600.00
EF156773	99997	Spearwood Progress Association	Small Events Sponsorship	17/02/2023	3,000.00
EF156774	99997	A Peck And A Warne	A.Peck Staff Expenses 15.02.23	17/02/2023	64.65
EF156775	99997	Bibra Lake Residents Association	Small Events Sponsorship	17/02/2023	2,300.00
EF156776	99997	Abigail Wang	Unused Ryde Credits	17/02/2023	30.00
EF156777	27492	Superchoice Services Pty Limited	Payroll Deductions	16/02/2023	971,319.91
EF156778	10047	Alinta Energy	Natural Gas & Electricity Supply	17/02/2023	24,657.40
EF156779	11794	Synergy	Electricity Usage/Supplies	17/02/2023	166,422.73
EF156780	12025	Telstra Corporation	Communications Services	17/02/2023	1,730.66
EF156781	10152	Aust Services Union	Payroll Deductions	17/02/2023	854.60
EF156782	10154	Australian Taxation Office	Payroll Deductions	17/02/2023	524,327.00
EF156783	10305	Child Support Agency	Payroll Deductions	17/02/2023	1,239.98
EF156784	11001	Local Government Racing & Cemeteries Employees Union Lgrceu	Payroll Deductions	17/02/2023	44.00
EF156785	19726	Health Insurance Fund Of Wa	Payroll Deductions	17/02/2023	1,066.80
EF156786	27874	Smartsalary	Salary Packaging/Leasing Administration	17/02/2023	12,008.43
EF156787	28458	Easi Group	Novated Leasing	17/02/2023	3,875.15
EF156788	10118	Australia Post	Postage Charges	21/02/2023	47,920.89
EF156789	22806	Chevron Australia Downstream Fuels Pty Ltd	Fuel Supplies	21/02/2023	85,895.14
EF156790	26987	Cti Risk Management	Security - Cash Collection	21/02/2023	386.30
EF156791	27965	Stantec Australia Pty Ltd	Engineering Services	21/02/2023	18,515.75
EF156792	28166	Australian Waterslides And Leisure	Welding Services	21/02/2023	4,752.00
EF156793	99997	Family Day Care	Fdc Payment W/E 19/02/2023	23/02/2023	51,787.75
EF156794	11867	Kevin John Allen	Elected Member Sitting Fees & Allowances	28/02/2023	2,705.83
EF156795	12740	Logan Howlett	Elected Member Sitting Fees & Allowances	28/02/2023	11,725.09
EF156796	19059	Carol Reeve-Fowkes	Elected Member Sitting Fees & Allowances	28/02/2023	2,705.83
EF156797	25353	Philip Eva	Elected Member Sitting Fees & Allowances	28/02/2023	2,705.83
EF156798	27326	Michael Separovich	Elected Member Sitting Fees & Allowances	28/02/2023	2,705.83
EF156799	27327	Chontelle Stone	Monthly Elected Member Allowance	28/02/2023	2,705.83
EF156800	27475	Lara Kirkwood	Elected Member Sitting Fees & Allowances	28/02/2023	2,998.33
EF156801	27871	Tom Widenbar	Elected Member Sitting Fees & Allowances	28/02/2023	4,622.43
EF156802	27872	Phoebe Corke	Elected Member Sitting Fees & Allowances	28/02/2023	2,705.83
EF156803	28238	Tarun Dewan	Elected Member Sitting Fees & Allowances	28/02/2023	2,705.83
EF156804	99996	Brenda Fletcher	Rates and Property related refunds	27/02/2023	30.00
EF156805	99996	Zelda Haskins	Rates and Property related refunds	27/02/2023	77.50
EF156806	99996	Heath Kirton	Rates and Property related refunds	27/02/2023	50.00
EF156807	99996	Jarrah Stevenson-Marsh	Rates and Property related refunds	27/02/2023	128.40

EF156808	99996	Valerio Fiorini	Rates and Property related refunds	27/02/2023	256.35
EF156809	99996	Grand Patios	Rates and Property related refunds	27/02/2023	56.65
EF156810	99996	Stretford End Pty Ltd	Rates and Property related refunds	27/02/2023	789.60
EF156811	99996	Sovereign Building Company	Rates and Property related refunds	27/02/2023	56.65
EF156812	99996	Kody Patterson	Rates and Property related refunds	27/02/2023	56.65
EF156813	99996	Complete Approvals	Rates and Property related refunds	27/02/2023	171.65
EF156814	99996	Ultimate Additions	Rates and Property related refunds	27/02/2023	147.00
EF156815	99996	Shane Freeman	Rates and Property related refunds	27/02/2023	147.00
EF156816	99996	Michael Ilich	Rates and Property related refunds	27/02/2023	3,924.49
EF156817	99996	Orana Property Wa Trust Acc	Rates and Property related refunds	27/02/2023	408.50
EF156818	99996	Zk Kubicki	Rates and Property related refunds	27/02/2023	530.06
EF156819	99996	Surendra Maharjan	Rates and Property related refunds	27/02/2023	2,020.72
EF156820	99996	Rebecca Claire Thirsk	Rates and Property related refunds	27/02/2023	260.00
EF156821	99996	Daniel A Vandy	Rates and Property related refunds	27/02/2023	541.93
EF156822	88888	Mr Simon Voges	Bond Refund	28/02/2023	500.00
EF156823	88888	Gold Estates Holdings Pty Ltd	Bond Refund	28/02/2023	26,185.71
EF156824	88888	Gold Estates Holdings Pty Ltd	Bond Refund	28/02/2023	41,286.16
EF156825	88888	Gold Estates Holdings Pty Ltd	Bond Refund	28/02/2023	60,529.46
EF156826	88888	Mr Murray Jones	Bond Refund	28/02/2023	500.00
EF156827	88888	Sundry Creditor Eft	Bond Refund	28/02/2023	20,369.00
EF156828	88888	Harry Pulling	Bond Refund	28/02/2023	100.00
EF156829	99997	Jeni Morris	Bird Bath Rebate	28/02/2023	26.40
EF156830	99997	Ad And Lm Kennan	Bird Bath Rebate	28/02/2023	49.99
EF156831	99997	Angela Hine	Bird Bath Rebate	28/02/2023	26.39
EF156832	99997	John & Elizabeth Wotherspoon	Bird Bath Rebate	28/02/2023	50.00
EF156833	99997	Sarah Travia	Bird Bath Rebate	28/02/2023	39.99
EF156834	99997	Jana Kucerova	Bird Bath Rebate	28/02/2023	50.00
EF156835	99997	Servau Offcl Departmental Recpts&Pay	Trans Charges For Centrepay	28/02/2023	244.53
EF156836	99997	David Proctor	Hose Stand	28/02/2023	100.00
EF156837	99997	Shayne Savic	Inv Cockbum4/2/23	28/02/2023	1,750.00
EF156838	99997	Louise Troy	Bird Bath Rebate	28/02/2023	30.00
EF156839	99997	Jean Catherine Parry	Senior Security Subsidy Scheme	28/02/2023	200.00
EF156840	99997	Michael Harry Fordham & Vivienne Helene	Senior Security Subsidy Scheme	28/02/2023	100.00
EF156841	99997	N Monaco & J F Monaco	Senior Security Subsidy Scheme	28/02/2023	100.00
EF156842	99997	Bw & Za Fassom	Senior Security Subsidy Scheme	28/02/2023	100.00
EF156843	99997	Pr&C Cochrane	Senior Security Subsidy Scheme	28/02/2023	100.00
EF156844	99997	Margaret Watkins	Senior Security Subsidy Scheme	28/02/2023	100.00
EF156845	99997	S.M Walker	Senior Security Subsidy Scheme	28/02/2023	300.00
EF156846	99997	Suzanne Yvonne Willing	Senior Security Subsidy Scheme	28/02/2023	100.00
EF156847	99997	Lynne A Willett	Senior Security Subsidy Scheme	28/02/2023	300.00
EF156848	99997	Trudy D'souza	Senior Security Subsidy Scheme	28/02/2023	300.00
EF156849	99997	Joel Cooper	Pen C090 - Refund	28/02/2023	840.00
EF156850	99997	Scott Hunt	Qualification Renewal	28/02/2023	44.00
EF156851	99997	Unity Training Services	Inv 00003901	28/02/2023	995.00
EF156852	99997	Linda Joyce Coelho & Jose Luis Coelho	Senior Security Subsidy Scheme	28/02/2023	200.00
EF156853	99997	Beverley Hawke	Senior Security Subsidy Scheme	28/02/2023	200.00
EF156854	99997	Brian & Treasur D'cruz	Senior Security Subsidy Scheme	28/02/2023	300.00
EF156855	99997	Maureen Marriott	Senior Security Subsidy Scheme	28/02/2023	200.00
EF156856	99997	June Menner	Senior Security Subsidy Scheme	28/02/2023	200.00
EF156857	99997	Manuel Da Silva Aboim	Senior Security Subsidy Scheme	28/02/2023	200.00
EF156858	99997	Miss Georgia Chave	Individual Sponsorship	28/02/2023	1,000.00
EF156859	99997	Reidy, Julie	Training Reimbursement	28/02/2023	44.00
EF156860	99997	Maria Alessandrino	Grants, Donations & Refunds	28/02/2023	150.00
EF156861	99997	Abigail Wang	Unused Ryde Credits	28/02/2023	30.00

EF156862	99997	Ian R Napler	Arc Refund Request - Ian Napler	28/02/2023	49.00
EF156863	99997	Joan Advantage Maciver	Arc Refund Request - Joan Maciver	28/02/2023	43.70
EF156864	99997	Mark Green	Staff Reimbursement	28/02/2023	756.40
EF156865	99997	Mr Graeme Maxwell Mercer	Prescription Safety Glasses - Graeme Mer	28/02/2023	399.95
EF156866	99997	Paul & Clare Cooper	Unspent Fund Balance - Hcp Package.	28/02/2023	3,622.47
EF156867	99997	Allianz Australia Insurance Limited	Grants, Donations & Refunds	28/02/2023	61.50
EF156868	99997	Julia Christensen	Grants, Donations & Refunds	28/02/2023	12.28
EF156869	99997	Servau Offcl. Departmental Recpts&Paymen	Inv 180141106 Centrepay/02	28/02/2023	228.69
EF156870	99997	Finishing Wa	Grants, Donations & Refunds	28/02/2023	3,030.50
EF156871	99997	Krystle Bucclarelli	Refund	28/02/2023	72.00
EF156872	99997	Bianca Tipper	Refund For Cancellation	28/02/2023	216.00
EF156873	10047	Alinta Energy	Natural Gas & Electricity Supply	28/02/2023	730.85
EF156874	11794	Synergy	Electricity Usage/Supplies	28/02/2023	66,907.03
EF156875	12025	Telstra Corporation	Communications Services	28/02/2023	2,697.96
EF156876	10058	Alsco Pty Ltd	Hygiene Services/Supplies	28/02/2023	260.22
EF156877	10207	Boc Gases	Gas Supplies	28/02/2023	723.11
EF156878	10221	Bp Australia Pty Ltd	Diesel/Petrol Supplies	28/02/2023	26,738.63
EF156879	10226	Bridgestone Australia Ltd	Tyre Services	28/02/2023	21,312.26
EF156880	10239	Budget Rent A Car - Perth	Motor Vehicle Hire	28/02/2023	2,635.91
EF156881	10246	Bunnings Building Supplies Pty Ltd	Hardware Supplies	28/02/2023	2,070.67
EF156882	10287	Centreline Markings	Linemarking Services	28/02/2023	2,090.00
EF156883	10325	City Of Fremantle	Contributions & Cost Sharing	28/02/2023	250.00
EF156884	10333	Cjd Equipment Pty Ltd	Hardware Supplies	28/02/2023	3,561.12
EF156885	10359	Cockburn Painting Service	Painting Supplies/Services	28/02/2023	1,664.30
EF156886	10384	Proglility Pty Ltd	Communication Services	28/02/2023	34,265.00
EF156887	10528	Easifleet	Vehicle Lease	28/02/2023	1,139.01
EF156888	10535	Workpower Incorporated	Employment Services - Planting	28/02/2023	16,674.90
EF156889	10589	Fines Enforcement Registry	Fines Enforcement Fees	28/02/2023	5,026.30
EF156890	10590	Department Of Fire And Emergency Services	Esl Levy & Related Costs	28/02/2023	137.22
EF156891	10679	Grasstrees Australia	Plants & Planting Services	28/02/2023	6,881.60
EF156892	10683	Gronbek Security	Locksmith Services	28/02/2023	847.01
EF156893	10768	Institute Of Public Works Engeering Aust - Wa Inc	Membership Fees	28/02/2023	1,089.00
EF156894	10787	Jandakot Accident Repair Centre	Panel Beating Services	28/02/2023	4,813.98
EF156895	10888	Lj Caterers	Catering Services	28/02/2023	2,950.42
EF156896	10892	Local Government Professionals Australia Wa	Subscription	28/02/2023	360.00
EF156897	10913	Bucher Municipal Pty Ltd	Purchase Of New Plant / Repair Services	28/02/2023	5,129.65
EF156898	10923	Major Motors Pty Ltd	Repairs/Maintenance Services	28/02/2023	3,198.45
EF156899	10938	Mrp Pest Management	Pest & Weed Management	28/02/2023	3,322.90
EF156900	10944	Mcleods	Legal Services	28/02/2023	11,386.55
EF156901	10982	Modern Teaching Aids Pty Ltd	Teaching Aids	28/02/2023	115.34
EF156902	10991	Beacon Equipment	Mowing Equipment	28/02/2023	5,927.45
EF156903	11036	Northlake Electrical Pty Ltd	Electrical Services	28/02/2023	4,814.32
EF156904	11077	P & G Body Builders Pty Ltd	Plant Body Building Services	28/02/2023	1,155.00
EF156905	11182	Premium Brake & Clutch Services Pty Ltd	Brake Services	28/02/2023	13,025.32
EF156906	11307	Satellite Security Services Pty Ltd	Security Services	28/02/2023	17,134.17
EF156907	11308	Boss Industrial Formally Sba Supplies	Hardware Supplies	28/02/2023	1,053.60
EF156908	11334	Shenton Pumps	Pool Equipment/Services	28/02/2023	159.50
EF156909	11361	Sigma Chemicals Pty Ltd	Chemical Supplies	28/02/2023	9,506.64
EF156910	11375	Slater Gartrell Sports	Sport Supplies	28/02/2023	5,269.00
EF156911	11399	South Coogee Volunteer Bushfire Brigade	Expense Reimbursements	28/02/2023	1,289.71
EF156912	11425	Resource Recovery Group	Waste Disposal Gate Fees	28/02/2023	6,265.76
EF156913	11449	Spearwood Florist Ultimate Co Pty Ltd	Floral Arrangements	28/02/2023	125.00
EF156914	11556	Technifire 2000	Fire Fighting Equipment	28/02/2023	261.74
EF156915	11619	Titan Ford	Purchase Of Vehicles & Servicing	28/02/2023	16,816.05

EF156916	11625	Nutrien Water	Reticulation Supplies	28/02/2023	968.00
EF156917	11651	Tree Watering Services	Tree Watering Services	28/02/2023	3,397.50
EF156918	11701	Vibra Industrial Filtration Australasia	Filter Supplies	28/02/2023	415.25
EF156919	11722	Wa Hino Sales & Service	Purchase Of New Trucks / Maintenance	28/02/2023	491.45
EF156920	11789	Walga	Advertising/Training Services	28/02/2023	242.00
EF156921	11795	Western Power	Street Lighting Installation & Service	28/02/2023	296.00
EF156922	11806	Westrac Pty Ltd	Repairs/Mtnce - Earthmoving Equipment	28/02/2023	1,044.04
EF156923	11828	Worldwide Online Printing - O'connor	Printing Services	28/02/2023	1,369.65
EF156924	11841	Yangebup Family Centre Inc	Venue Hire / Grants & Donations	28/02/2023	313.00
EF156925	11847	Yangebup Primary School	Grants & Donations	28/02/2023	319.00
EF156926	11854	Zipform Pty Ltd	Printing Services	28/02/2023	980.01
EF156927	11873	Wattleup Tractors	Hardware Supplies	28/02/2023	726.00
EF156928	12153	Hays Personnel Services Pty Ltd	Employment Services	28/02/2023	10,841.23
EF156929	12295	Stewart & Heaton Clothing Co. Pty Ltd	Clothing Supplies	28/02/2023	8,426.57
EF156930	13102	Michael Page International (Australia) Pty Ltd	Employment Services	28/02/2023	14,988.24
EF156931	13563	Green Skills Inc	Employment Services	28/02/2023	6,244.35
EF156932	13764	Ddls Australia Pty Ltd	Computer Software	28/02/2023	1,584.00
EF156933	13825	Jackson Mcdonald	Legal Services	28/02/2023	21,705.20
EF156934	14297	Artref Pty Ltd	Printing Cartridges	28/02/2023	1,754.43
EF156935	14350	Baileys Fertiliser	Fertiliser Supplies	28/02/2023	1,940.40
EF156936	15393	Stratagreen	Hardware Supplies	28/02/2023	495.00
EF156937	15550	Apac Aid Inc	Plants & Landscaping Services	28/02/2023	473.00
EF156938	15588	Natural Area Consulting Management Services	Weed Spraying	28/02/2023	28,405.79
EF156939	16064	Cms Engineering	Airconditioning Services	28/02/2023	20,369.68
EF156940	16107	Wren Oil	Waste Disposal Services	28/02/2023	16.50
EF156941	16396	Mayday Rental	Road Construction Machine Hire	28/02/2023	6,380.00
EF156942	16846	Action Glass & Aluminium	Glazing Services	28/02/2023	2,640.00
EF156943	17471	Pirtek (Fremantle) Pty Ltd	Hoses & Fittings	28/02/2023	4,174.39
EF156944	17555	Maia Financial	Equipment Lease Payments	28/02/2023	37,121.35
EF156945	17600	Lightforce Asset Pty Ltd (Erections)	Guard Rails	28/02/2023	2,530.00
EF156946	18203	Natsync Environmental	Pest Control	28/02/2023	1,980.00
EF156947	18962	Sealanes (1985) P/L	Catering Supplies	28/02/2023	1,659.98
EF156948	19533	Woolworths Ltd	Groceries	28/02/2023	2,147.99
EF156949	19541	Turf Care Wa Pty Ltd	Turf Services	28/02/2023	8,098.42
EF156950	19558	Complete Fire Design	Fire Consultancy Services	28/02/2023	25,305.51
EF156951	20000	Aust West Auto Electrical Pty Ltd	Auto Electrical Services	28/02/2023	17,268.10
EF156952	20549	A1 Carpet, Tile & Grout Cleaning	Cleaning Services - Tiles/Carpet	28/02/2023	2,090.00
EF156953	21577	Lavan	Legal Services	28/02/2023	125,879.03
EF156954	21665	Mmj Real Estate (Wa) Pty Ltd	Property Management Services	28/02/2023	25,350.65
EF156955	21744	Jb Hi Fi - Commercial	Electronic Equipment	28/02/2023	711.97
EF156956	21747	Unicare Health	Wheelchair Hire	28/02/2023	895.50
EF156957	21877	Wellness On Wheels	Workplace And Event Remedial Massage	28/02/2023	450.00
EF156958	22106	Intellife Group	Services - Daip	28/02/2023	21,389.88
EF156959	22388	Carrington's Traffic Services	Traffic Management Services	28/02/2023	4,233.63
EF156960	22553	Brownes Food Operations	Catering Supplies	28/02/2023	444.68
EF156961	22569	Sonic Health Plus Pty Ltd	Medical Services	28/02/2023	3,712.50
EF156962	22613	Vicki Royans	Artistic Services	28/02/2023	750.00
EF156963	22639	Shatish Chauhan	Training Services - Yoga	28/02/2023	2,145.00
EF156964	22913	Opal Australian Paper	Envelopes	28/02/2023	862.13
EF156965	23351	Cockburn Gp Super Clinic Limited T/A Cockburn Integrated Health	Leasing Fees	28/02/2023	1,113.29
EF156966	23457	Totally Workwear Fremantle	Clothing - Uniforms	28/02/2023	2,792.70
EF156967	23506	Hudson Global Resources (Aust) Pty Ltd	Human Resources Consult	28/02/2023	6,384.05
EF156968	23579	Daimler Trucks Perth	Purchase Of New Truck	28/02/2023	205.69
EF156969	23600	Ironbark Sustainability	Consultancy Services - Environmental	28/02/2023	10,604.00

EF156970	24275	Truck Centre Wa Pty Ltd	Purchase Of New Truck	28/02/2023	7,900.89
EF156971	24506	Amaranti's Personal Training	Personal Training Services	28/02/2023	750.00
EF156972	24643	Bibliotheca Rfid Library Systems Australia Pty Ltd	Purchase Of Library Tags	28/02/2023	4,840.63
EF156973	24655	Automasters Spearwood	Vehicle Servicing	28/02/2023	8,839.10
EF156974	24974	Scott Print	Printing Services	28/02/2023	2,271.50
EF156975	25121	Imagesource Digital Solutions	Billboards	28/02/2023	5,289.90
EF156976	25128	Horizon West Landscape & Irrigation Pty Ltd	Landscaping Services	28/02/2023	198.00
EF156977	25418	Cs Legal	Legal Services	28/02/2023	20,406.42
EF156978	25645	Yelakitj Moort Nyungar Association Inc	Welcome To The Country Performances	28/02/2023	400.00
EF156979	25731	Wheelie Clean	Cleaning Services	28/02/2023	915.20
EF156980	25736	Blue Tang (Wa) Pty Ltd T/As Emerge Associates (The Trustee For The Reef Unit Trust) Emerge Associates	Consultancy Services	28/02/2023	1,004.30
EF156981	26114	Grace Records Management	Records Management Services	28/02/2023	1,295.23
EF156982	26195	Play Check	Consulting Services	28/02/2023	330.00
EF156983	26257	Paperbark Technologies Pty Ltd	Arboricultural Consultancy Services	28/02/2023	415.00
EF156984	26303	Gecko Contracting Turf & Landscape Maintenance	Turf & Landscape Maintenance	28/02/2023	67,039.86
EF156985	26314	Cpe Group	Temporary Employment Services	28/02/2023	1,191.96
EF156986	26399	Paperscout The Trustee For Peters Morrison Family Trust	Graphic Design Services	28/02/2023	1,463.00
EF156987	26403	Ches Power Group Pty Ltd	Engineering Solutions / Back Up Generato	28/02/2023	2,262.85
EF156988	26470	Scp Conservation	Fencing Services	28/02/2023	10,054.00
EF156989	26558	Healthcare Australia Pty Ltd	Temporary Employment Services	28/02/2023	522.40
EF156990	26576	Wizard Training Solutions	Training Services	28/02/2023	3,905.00
EF156991	26610	Tracc Civil Pty Ltd	Civil Construction	28/02/2023	874,928.20
EF156992	26614	Marketforce Pty Ltd	Advertising	28/02/2023	1,496.08
EF156993	26625	Andover Detailers	Car Detailing Services	28/02/2023	720.71
EF156994	26677	Australia And New Zealand Recycling Platform Limited	Not- For-Profit Member Services Body	28/02/2023	3,545.08
EF156995	26735	Shane Mcmaster Surveys	Survey Services	28/02/2023	3,245.00
EF156996	26739	Kerb Doctor	Kerb Maintenance	28/02/2023	290.18
EF156997	26773	Laser Corps Combat Adventruess	Entry Fees	28/02/2023	990.00
EF156998	26778	Robert Walters	Recruitment Services	28/02/2023	3,992.71
EF156999	26901	Alyka Pty Ltd	Digital Consultancy And Web Development	28/02/2023	1,611.50
EF157000	26923	Woodlands	Rubbish Collection Equipment	28/02/2023	21,798.65
EF157001	26929	Elan Energy Matrix Pty Ltd	Recycling Services	28/02/2023	1,705.00
EF157002	26940	Floorwest Pty Ltd	Floor Coverings	28/02/2023	330.00
EF157003	26985	Access Icon Pty Ltd	Drainage Products	28/02/2023	1,540.64
EF157004	26987	Cti Risk Management	Security - Cash Collection	28/02/2023	1,210.75
EF157005	27010	Quantum Building Services Pty Ltd	Building Maintenance	28/02/2023	26,055.42
EF157006	27011	Baileys Marine Fuel Australia	Fuel	28/02/2023	2,703.77
EF157007	27023	Solargain Pv Pty Ltd	Solar Energy Provider	28/02/2023	396.00
EF157008	27031	Downer Edi Works Pty Ltd	Asphalt Services	28/02/2023	392,714.08
EF157009	27034	Adelby Pty Ltd	Firebreak Construction	28/02/2023	1,859.00
EF157010	27044	Graffiti Systems Australia	Graffiti Removal & Anti-Graffiti Coating	28/02/2023	22,748.44
EF157011	27046	Tfh Hire Services Pty Ltd	Hire Fencing	28/02/2023	242.13
EF157012	27053	Readspeaker	Software	28/02/2023	8,184.44
EF157013	27065	Westbooks	Books	28/02/2023	85.56
EF157014	27082	Kulbardi Pty Ltd	Stationery Supplies	28/02/2023	2,893.57
EF157015	27085	Savills Project Management Pty Ltd	Project Management	28/02/2023	25,180.59
EF157016	27098	Q2 (Q-Squared)	Digital Data Service	28/02/2023	3,960.00
EF157017	27154	Suez Recycling & Recovery Pty Ltd	Waste Services	28/02/2023	156,854.78
EF157018	27168	Nightlife Music Pty Ltd	Music Management	28/02/2023	931.06
EF157019	27243	Arjohntleigh Pty Ltd	Supply, Repairs Health Equipemnt	28/02/2023	599.50
EF157020	27246	Veale Auto Parts	Spare Parts Mechanical	28/02/2023	341.60
EF157021	27291	Auslan Stage Left	Consultancy - Interpreting	28/02/2023	990.00
EF157022	27334	Westcare Print	Printing Services	28/02/2023	665.50
EF157023	27355	Playmaster	Playground Equipment	28/02/2023	78,815.00

EF157024	27374	Southern Cross Cleaning	Commercial Cleaning	28/02/2023	13,684.73
EF157025	27401	Emprise Mobility	Mobility Equipment	28/02/2023	9,529.00
EF157026	27432	Lg Solutions Pty Ltd	Financial Services	28/02/2023	8,195.00
EF157027	27455	Site Protective Services	Cctv Parts	28/02/2023	9,605.55
EF157028	27472	About Bunting	Bunting Flags	28/02/2023	798.60
EF157029	27499	Hodge Collard Preston Architects	Architects	28/02/2023	31,089.85
EF157030	27507	Serco Facilities Management Pty Ltd	Cleaning Services	28/02/2023	108,012.19
EF157031	27529	Wa Library Supplies	Library Supplies & Furniture	28/02/2023	2,244.00
EF157032	27539	Jasmin Carpentry & Maintenance	Carpentry	28/02/2023	3,753.64
EF157033	27560	Artem Design Studio Pty Ltd	Architectural Services	28/02/2023	5,808.00
EF157034	27622	Truegrade Medical Supplies	Medical Supplies	28/02/2023	235.95
EF157035	27631	Aquatic Services Wa Pty Ltd	Pool Equipment & Maintenance	28/02/2023	12,872.75
EF157036	27650	Datacom Systems (Au) Pty Ltd	IT Sales, Consulting & Service	28/02/2023	56,065.68
EF157037	27676	Blue Force Pty Ltd	Security Services	28/02/2023	20,085.74
EF157038	27695	Qtm Pty Ltd	Traffic Management	28/02/2023	25,833.87
EF157039	27701	Perth Better Homes	Shade Sails	28/02/2023	605.00
EF157040	27717	Moore Stephens (Wa) Pty Ltd	Accounting Services	28/02/2023	16,500.00
EF157041	27809	Ra-One Pty Ltd	Software	28/02/2023	18,892.50
EF157042	27831	Butler And Brown	Event Management	28/02/2023	45,320.00
EF157043	27850	Dowsing Group Pty Ltd	Concreting Services	28/02/2023	207,820.69
EF157044	27855	Total Landscape Redevelopment Service Pty Ltd	Tree Watering	28/02/2023	308.00
EF157045	27865	Pritchard Francis Consulting Pty Ltd	Engineering Services	28/02/2023	10,859.20
EF157046	27886	Bbc Entertainment	Entertainment Agency	28/02/2023	1,320.00
EF157047	27890	Tabec Pty Ltd	Engineering Services	28/02/2023	4,576.00
EF157048	27894	Homecare Physiotherapy	Healthcare	28/02/2023	7,621.62
EF157049	27908	Raubex Construction	Engineering Civil	28/02/2023	123,475.01
EF157050	27917	Go Doors Advanced Automation	Door Maintenance & Repair	28/02/2023	1,705.69
EF157051	27943	Chargefox Pty Ltd	Software Development	28/02/2023	3,503.50
EF157052	28003	Taylor Made Design	Graphic Design	28/02/2023	374.00
EF157053	28043	Veris Australia Pty Ltd	Survey Services	28/02/2023	1,199.00
EF157054	28080	Yacht Grot 1985 Pty Ltd	Marine	28/02/2023	506.00
EF157055	28106	Centre For Accessibility Australia	Digital Accessibility	28/02/2023	6,050.00
EF157056	28136	Shore Water Marine Pty Ltd	Marine Repair & Maintenance Services	28/02/2023	11,039.60
EF157057	28154	Remplan	Economic & Demographic Modelling & Analy	28/02/2023	5,456.00
EF157058	28191	Enviro Sweep	Sweeping Services	28/02/2023	2,748.91
EF157059	28197	Lite N Easy Pty Ltd	Food Supplies	28/02/2023	1,051.25
EF157060	28201	Select Fresh	Food Supplies	28/02/2023	380.16
EF157061	28211	Nordic Fitness Equipment	Fitness Equipment	28/02/2023	2,970.00
EF157062	28214	Beyond Skateboarding	Skateboarding Clinics	28/02/2023	4,015.00
EF157063	28233	Western Maze Wa Pty Ltd	Waste Collection Services	28/02/2023	14,762.00
EF157064	28241	Swift Flow Pty Ltd	Plumbing	28/02/2023	12,406.87
EF157065	28253	Barry Emerald	Artist	28/02/2023	400.00
EF157066	28258	Garden Care West	Gardening Services	28/02/2023	165.00
EF157067	28261	Hazed Services Pty Ltd	Safety - Roof	28/02/2023	880.00
EF157068	28265	Tree Care Wa	Vegetation Maintenance Services	28/02/2023	82,372.01
EF157069	28267	C-Wise	Composts Manufacturing	28/02/2023	36,907.86
EF157070	28277	Gesha Coffee Co	Coffee Supplies	28/02/2023	716.00
EF157071	28283	Mills Recruitment	Recruitment Services	28/02/2023	5,476.46
EF157072	28297	Techbrain	IT Consultancy	28/02/2023	1,324.40
EF157073	28303	Miracle Recreation Equipment	Playground Equipment	28/02/2023	8,937.50
EF157074	28319	Wavelength Consulting Pty Ltd	Engineering - Project Management	28/02/2023	11,000.00
EF157075	28343	Hemsley Paterson	Valuation Services	28/02/2023	2,200.00
EF157076	28354	Sauna And Steam Wa	Carpentry Services	28/02/2023	2,531.42
EF157077	28359	P&M Automotive Equipment	Hoist Servicing	28/02/2023	357.50

EF157078	28371	Flexi Staff	Employment Services	28/02/2023	18,278.65
EF157079	28390	Logo Appointments	Employment Services	28/02/2023	838.99
EF157080	28392	Mcs Civil Contracting	Engineering/Earthworks	28/02/2023	2,618.00
EF157081	28419	Adam Puffler	Scooter Events	28/02/2023	1,803.00
EF157082	28423	Jordies Garden Bags	Waste Services	28/02/2023	1,336.50
EF157083	28428	Wa Bolts Pty Ltd	Fixings & Fasteners	28/02/2023	212.00
EF157084	28431	Skate Sculpture	Skate Park Design	28/02/2023	5,280.00
EF157085	28436	Blue Leaf Water Cremation	Cremation Services	28/02/2023	572.00
EF157086	28437	Building & Industrial Cleaning Services	Clenaing Services	28/02/2023	83,025.41
EF156770	28487	Perth Is Ok Kelleway Whelan Holdings Pty Ltd	Digital Media	17/02/2023	5,808.00
EF157088	28454	Aussie Natural Spring Water	Water Supplies	28/02/2023	68.88
EF157089	28473	Ecoquip Australia Pty Ltd	Solar Mobile Lighting Tower Hire	28/02/2023	1,529.00
EF157090	28477	Cascade Pty Ltd (David Price Consulting)	Consulting	28/02/2023	12,100.00
EF157091	28478	Aither Pty Ltd	Consulting	28/02/2023	11,304.70
EF157092	28493	Heat Exchangers Wa Pty Ltd	Parts and service	28/02/2023	1,736.02
EF157093	28500	Fieldey Art Fields, Haylee Ann (Fieldey Art)	Art murals and workshops	28/02/2023	5,500.00
EF157094	28501	Quality Comics The Trustee For Mccrackan Family Trust	bookshop	28/02/2023	1,208.00
EF157095	10747	linet Limited	Internet Services	28/02/2023	1,009.88
EF157096	11758	Water Corp Utility Account Only - Please Refer To 11760 When Raising Po	Water Usage / Sundry Charges	28/02/2023	4,618.21
EF157097	11760	Water Corporation	Sewer Easement	28/02/2023	11,136.72
		<b>TOTAL OF 769 EFT PAYMENTS</b>			<b>11,526,643.97</b>
		<b>LESS: CANCELLED EFT PAYMENTS</b>			
EF155104	99996	Rates And Property Related Eft Refunds (Not Bonds)	Rates And Property Related Refunds	10/02/2023	-30.00
EF155900	88888	Sundry Creditor Eft	Refund	8/02/2023	-500.00
EF156363	88888	Sundry Creditor Eft	Refund	23/02/2023	-500.00
EF156408	99997	Coc Grants, Donations & Refunds	Grants, Donations & Refunds	23/02/2023	-26.40
EF156415	99997	Coc Grants, Donations & Refunds	Grants, Donations & Refunds	23/02/2023	-49.99
		<b>TOTAL CANCELLED EFT PAYMENT</b>			<b>-1,106.39</b>
		<b>TOTAL EFT PAYMENTS ( EXCL. CANCELLED PAYMENTS)</b>			<b>11,525,537.58</b>
		<b>ADD: BANK FEES</b>			
		BPAY BATCH FEE			15.75
		MERCHANT FEES COC			2,045.17
		MERCHANT FEES MARINA			187.95
		MERCHANT FEES ARC			2,217.07
		MERCHANT FEES VARIOUS OUT CENTRES			1,122.33
		NATIONAL BPAY CHARGE			4,151.04
		RTGS/ACLR FEE			-
		NAB TRANSACT FEE			213.30
		MERCHANDISE / OTHER FEES			-
					<b>9,952.61</b>
		<b>ADD: CREDIT CARD PAYMENTS</b>			<b>102,810.71</b>
		<b>ADD: PAYROLL PAYMENTS</b>			



COC-02/02/23 Pmt 000224579303 City of Cockburn	2/02/2023	5,542.01
COC-03/02/23 Pmt 000224807820 City of Cockburn	7/02/2023	18,643.51
COC-03/02/23 Pmt 000225566657 City of Cockburn	16/02/2023	2,618.14
COC-06/02/23 Pmt 000225442050 City of Cockburn	15/02/2023	555.59
COC-12/02/23 Pmt 000225475353 City of Cockburn	15/02/2023	1,637,799.65
COC-16/02/23 Pmt 000225572768 City of Cockburn	16/02/2023	944.28
COC-19/02/23 Pmt 000225969526 City of Cockburn	22/02/2023	2,561.08
COC-21/02/23 Pmt 000225969656 City of Cockburn	22/02/2023	4,387.75
COC-22/02/23 Pmt 000225967241 City of Cockburn	22/02/2023	691.00
COC-27/01/23 Pmt 000224390572 City of Cockburn	1/02/2023	7,302.20
COC-29/01/23 Pmt 000224467506 City of Cockburn	1/02/2023	1,652,340.12
COC-31/01/23 Pmt 000224807343 City of Cockburn	7/02/2023	1,907.55
COC-31/01/23 Pmt 000225568110 City of Cockburn	16/02/2023	1,098.19
274713412091602002 SuperChoice P/L CITY OF COCKBURN	16/02/2023	402.77
274713412091602003 SuperChoice P/L CITY OF COCKBURN	16/02/2023	30.89
274713412091602004 SuperChoice P/L CITY OF COCKBURN	16/02/2023	143.39
274713412091602005 SuperChoice P/L CITY OF COCKBURN	16/02/2023	71.05
		<b>3,337,039.17</b>
<b>TOTAL PAYMENTS MADE FOR THE MONTH</b>		<b>14,975,340.07</b>

## 14.2.2 Monthly Financial Reports - February 2023

**Author(s)** A/Head of Finance

**Attachments** 1. Monthly Financial Report February 2023 [↓](#)

### RECOMMENDATION

That Council:

- (1) ADOPTS the Monthly Financial Report containing the Statement of Financial Activity and other financial information for the month of February 2023, as attached to the Agenda.

### Background

*Local Government (Financial Management) Regulations 1996* prescribe that a Local Government is to prepare each month a Statement of Financial Activity.

Regulation 34(2) requires the Statement of Financial Activity to be accompanied by documents containing:

1. Details of the composition of the closing net current assets (less restricted and committed assets)
2. Explanation for each material variance identified between year to date (YTD) budgets and actuals
3. Any other supporting information considered relevant by the Local Government.

Regulation 34(4)(a) prescribes that the Statement of Financial Activity and accompanying documents be presented to Council within two months after the end of the month to which the statement relates.

The regulations require the information reported in the statement to be shown either by nature or type, statutory program, or business unit.

The City has chosen to report the information according to nature or type and its organisational business structure.

*Local Government (Financial Management) Regulations 1996* - Regulation 34 (5) states "Each financial year, a Local Government is to adopt a percentage or value, calculated in accordance with the Australian Accounting Standards, to be used in statements of financial activity for reporting material variances."

This regulation requires Council to annually set a materiality threshold for the purpose of disclosing budget variances within monthly financial reporting.

The materiality threshold has been set by Council at \$300,000 for the 2022-23 financial year (FY23).

Detailed analysis of budget variances is an ongoing exercise, with necessary budget amendments either submitted for Council approval through the Expenditure Review Committee or included in the City's mid-year budget review required by legislation.

### **Submission**

N/A

### **Report**

The attached Monthly Financial Report for February 2023 has been prepared in accordance with the Local Government Act and the associated Financial Management Regulations.

This was reviewed by management, with the following commentary addressing key financial results and the City's budgetary performance to the end of the month.

#### Opening Surplus

The amended budget opening surplus is now \$11.93 million which includes the \$3.86 million mid-year budget review adjustment adopted by Council in February, \$2.50 million original budget surplus and \$5.56 million municipal funding for the City's carry forward projects.

#### Closing Surplus

The City's YTD closing surplus to the end of February was \$77.37 million, versus a YTD budget of \$70.74 million.

This represents a favourable variance of \$6.64 million, inclusive of variances across the FY23 operating and capital budget reported in the following sections.

#### Operating Revenue

Operating revenue of \$158.98 million was \$1.35 million behind YTD budget for February.

The following table summarises the operating revenue budget performance by nature:

Revenue from operating activities	Amended		YTD Actual \$	YTD Variance \$
	Full Year Budget \$	YTD Budget \$		
Rates	118,200,126	118,002,269	117,660,728	(341,541)
Specified Area Rates	624,000	623,000	603,860	(19,140)
Operating Grants, Subsidies, Contributions	11,863,320	7,227,429	7,038,905	(188,524)
Fees and Charges	40,972,049	29,058,859	28,684,127	(374,732)
Interest Earnings	6,425,280	4,658,933	4,731,180	72,247
Profit/(Loss) Asset Sale	1,444,042	759,925	263,164	(496,761)
<b>Total</b>	<b>179,528,817</b>	<b>160,330,415</b>	<b>158,981,964</b>	<b>(1,348,451)</b>

Material variances identified in the City's operating revenue were identified as follows:

- Rates (\$0.34 million under YTD budget) mainly due to the part year rates received are behind the YTD by \$0.35 million.

### Operating Expenditure

Operating expenditure to the end of February of \$107.56 million was under YTD budget by \$10.52 million.

The following table summarises the operating expenditure budget variance performance by nature:

Expenditure from operating activities	Amended		YTD Actual \$	YTD Variance \$
	Full Year Budget \$	YTD Budget \$		
Employee costs	69,593,027	46,161,374	43,087,779	(3,073,595)
Materials & Contracts	44,747,678	27,734,254	21,897,911	(5,836,343)
Utility charges	6,209,122	4,164,452	3,888,346	(276,106)
Depreciation/Amortisation	42,461,606	27,205,278	26,700,329	(504,949)
Interest/Finance Costs	500,921	214,549	210,599	(3,950)
Insurance expenses	2,275,300	2,275,300	2,345,078	69,778
Other expenditure	15,013,811	10,321,115	9,429,398	(891,717)
<b>Total</b>	<b>180,801,465</b>	<b>118,076,322</b>	<b>107,559,440</b>	<b>(10,516,882)</b>

Significant variances identified in the City's operating expenditure were identified as follows:

- Employee Costs (\$3.07 million under YTD budget):
  - There are salary budget variances across all business units totalling \$2.63 million, due to the Enterprise Agreement increase factored into the budget not being paid and above average staff vacancies due to tight labour market is still occurring this period.
  - Leave entitlement reductions contributed a net \$0.80 million to the variance, reflecting the positive impact of leave management strategies.
  - Indirect employee costs also had an underspend of \$0.45 million (training, recruitment, conferences etc.)
- Materials and Contracts (\$5.84 million under YTD budget):
  - Environment, Parks & Streetscapes service unit showing a \$1.52 million underspend against YTD budget mostly due to timing issues seasonal factors.
  - Community Development projects were \$0.43 million behind YTD budget.
  - Sustainability & Environment collectively had \$0.78 million underspend; \$0.35 million of it is due to several projects have not started yet.
  - Recreation Services service unit had \$0.41 million underspend.
  - Financial Accounting service unit had \$0.54 million underspend mostly due to insurance claims and payments.
- Other Expenditure (\$0.89 million under YTD budget):
  - Community Grants had \$0.66 million underspend – timing issue.

### Capital Expenditure

Council adopted a capital works program of \$50.9 million in the FY23 annual budget, that is now \$69.11 million following carry forwards and the mid-year budget review adopted by Council.

The City had spent \$24.44 million on its capital program to the end of February, representing an underspend of \$9.03 million against YTD budget (timing variances).

The following table shows the budget performance by asset class:

Capital acquisitions	Amended		YTD Actual \$	YTD Actual Variance \$
	Budget \$	YTD Budget \$		
Land	1,680,000	1,680,000	1,680,000	0
Buildings	8,815,920	5,477,342	3,352,403	(2,124,939)
Furniture & Equipment	77,000	77,000	5,000	(72,000)
Plant and equipment	10,304,992	3,067,241	1,729,825	(1,337,416)
Capital acquisitions	Amended		YTD Actual \$	YTD Actual Variance \$
	Budget \$	YTD Budget \$		
Information technology	4,022,732	501,628	258,818	(242,810)
Infrastructure - roads	25,175,682	12,660,308	11,888,460	(771,848)
Infrastructure - drainage	3,070,903	2,550,704	821,280	(1,729,424)
Infrastructure - footpath	1,989,439	1,260,669	542,385	(718,284)
Infrastructure - parks hard	3,950,935	2,878,293	2,336,539	(541,754)
Infrastructure - landscaping	1,742,132	1,037,020	538,620	(498,400)
Infrastructure - landfill site	4,922,172	367,428	394,458	27,030
Infrastructure - marina	2,254,348	1,319,223	793,434	(525,789)
Infrastructure - coastal	1,099,933	594,318	102,855	(491,463)
<b>Total</b>	<b>69,106,188</b>	<b>33,471,173</b>	<b>24,444,077</b>	<b>(9,027,096)</b>

Significant variances identified in the City's capital expenditure were identified as follows:

- Buildings construction had a net budget variance of \$2.12 million under YTD budget due to timing issue.
- Plant and equipment had a net budget variance of \$1.34 million under YTD budget mostly due to supply chain issue.
- Roads infrastructure had a net budget variance of \$0.77 million under YTD budget due to timing issue.
- Drainage infrastructure had a net budget variance of \$1.73 million under YTD budget as several projects have not eventuated.

#### Non-Operating Grants, Subsidies and Contributions

The City's budget for capital grants and contributions is a net \$20.77 million. This includes \$24.74 million in funding to be received, less an outgoing contribution of \$3.97 million to the State Government for underground power in South Lake.

Non-operating revenue of \$8.98 million was recognised to the end of February, \$0.09 million over YTD budget.

### Financial Reserves

A detailed schedule of the City's financial reserves is included in the financial report, showing a balance of \$180.68 million held at the end of February (\$181.11 million last month).

Council funded reserves made up \$151.54 million of the balance, \$12.08 million for restricted and legislated purposes, and another \$17.06 million for developer contribution plans.

Transfers in and out of financial reserves are made in accordance with budgetary requirements.

### Cash and Financial Assets

The City's closing cash and financial assets investment holding at month's end totalled \$263.33 million (up from \$255.69 million last month).

This balance included financial assets (term deposits and investments) of \$252.36 million, and cash and cash equivalent holdings (cash at bank and at call deposits) of \$10.96 million.

\$184.50 million of these funds were internally and externally restricted, representing the City's financial reserves and liability for bonds and deposits held.

The remaining \$78.82 million represented unrestricted municipal funds for the City's operating activities and liabilities.

### Investment Performance, Ratings and Maturity

The City's term deposit portfolio running yield has continued increasing to an annualised 2.89 percent as of 28 February (up from 2.87 percent last month and 2.78 percent the month before).

This underperformed the City's KPI target rate of 3.85 percent, comprising RBA cash rate of 3.35 percent (end of February) plus a 0.50 percent performance margin.

The swiftness of back-to-back increases made to the official cash rate by the Reserve Bank of Australia (RBA), means many of the City's existing term deposits are yielding well under the City's current KPI target. The City is still investigating costs and benefits of breaking some longer dated term deposits placed with various institutions. Where this shows a net benefit, the City will execute the strategy.

The latest three cash rate increases have been for 0.25 percent (rather than 0.50 percent), indicating the RBA may be nearing the top of this tightening cycle.

Performance against the KPI target continues to improve each month with the City's new deposits being placed at relatively higher rates.

New investments placed during the month were at rates ranging between 4.65 and 4.95 percent over various durations.

Current term deposit investments are fully compliant with Council's Investment Policy requirements, as indicated below:

Investment Policy Compliance		
Legislative Requirements	✓	Fully compliant
Portfolio Credit Rating Limit	✓	Fully compliant
Institutional Exposure Limits	✓	Fully compliant
Term to Maturity Limits	✓	Fully compliant

The portfolio also includes several reverse mortgage securities purchased under previous policy and statutory provisions.

These have a face value of \$2.435 million and market value of \$1.591 million, although the City currently carries them at a book value of \$0.86 million (net of a \$1.575 million impairment provision made several years ago).

The City continues receiving interest and capital payments, with \$0.565 million returned to date of the original \$3.0 million invested.

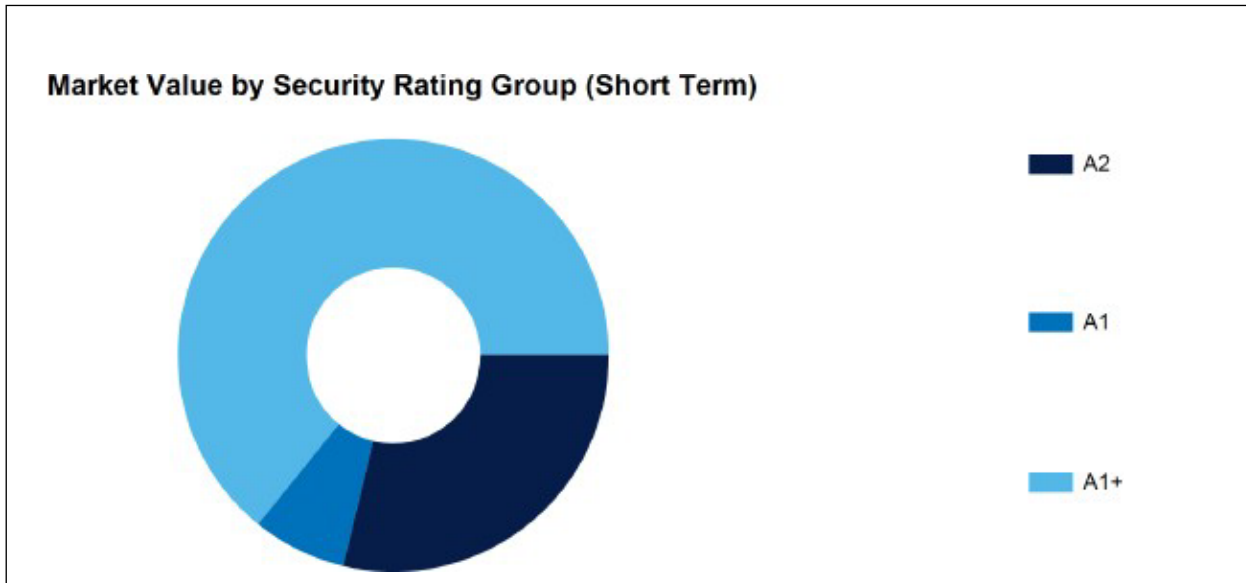
The City's investments were held with the following financial institutions as at 28 February (inclusive of accrued interest):

Issuer	Market Value	% Total Value
AMP Bank Ltd	13,664,149.08	5.57%
Australian Unity Bank	2,048,920.54	0.83%
Bank of Queensland Ltd	20,004,806.84	8.15%
Commonwealth Bank of Australia Ltd	85,616,260.54	34.88%
Credit Union Australia Ltd t/as Great Southern Bank	5,012,607.39	2.04%
Defence Bank Ltd	5,060,458.90	2.06%
Emerald Reverse Mortgage Trust	1,586,920.35	0.65%
Macquarie Bank	0.00	0.00%
Members Equity Bank Ltd	4,081,309.60	1.66%
MyState Bank Ltd	20,717,672.42	8.44%
National Australia Bank Ltd	25,691,450.69	10.47%
Suncorp-Metway Ltd	21,841,066.29	8.90%
Westpac Banking Corporation Ltd	40,124,712.66	16.35%
Portfolio Total	245,450,335.28	100.00%

The City's short-term deposits (less than 12 months) made up 76.0 percent (\$186.60 million) of the City's portfolio, compared to 71.0 percent (\$175.86 million) last month.

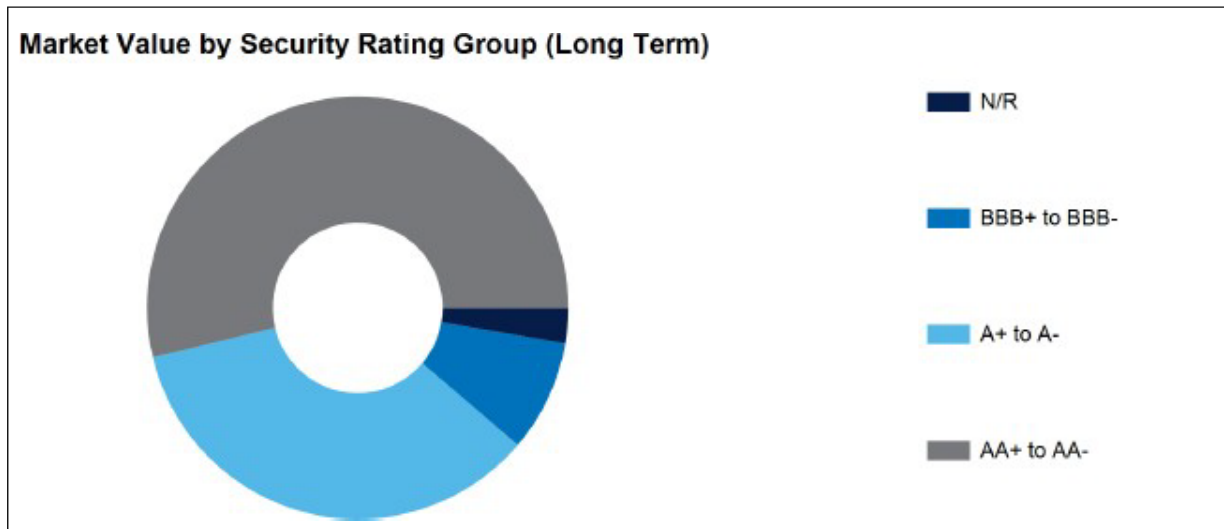


These were classified under the following credit ratings:



Deposits invested between 1 and 3 years made up 23.8 percent (\$58.5 million) of the City’s portfolio, compared to 29.0 percent (\$71.38 million) last month.

These were classified under following credit ratings:



Investment in Fossil Fuel Free Banks

At month end, the City held \$60.40 million (24.8 percent) of its investment portfolio with banks considered non-funders of fossil fuel related industries (up from \$58.95 million or 23.9 percent last month).

The amount invested with fossil fuel free banks fluctuates depending on the competitiveness of deposit rates being offered and the capacity of fossil fuel free banks to accept funds.

The City will always endeavour to preference a fossil fuel free investment, given a similar deposit rate.

### Rates Debt Recovery

The collectible rates and charges for 2022-23 (comprising arrears, annual levies and part year rating) totals \$142.59 million.

To the end of February, the City had collected \$120.62 million (84.59 percent), leaving a balance outstanding of \$21.97 million (15.41 percent).

The City has also received \$1.30 million in prepayments for future year's rates.

In terms of overdue and delinquent rates accounts under formal or legal debt recovery processes, the City had 244 properties owing a total of \$1.04 million in combined rates and legal fees (down from 313 properties last month owing \$1.20 million).

Given the size of the City's ratepayer base (around 53,000 properties), this reflects the City's effective processes in controlling and managing overdue rates accounts.

Formal debt recovery activities are commenced when ratepayers have overdue rates and have not committed to instalment or other payment arrangements or sought relief under the City's Financial Hardship Policy.

### Trade and Sundry Debtors

The City had \$2.99 million in outstanding trade and sundry debtors to the end of February (\$2.86 million last month).

Those debts overdue by more than 90 days made up \$613k or 20.5 percent of total debts outstanding (\$342k or 12.0 percent last month).

The 90-day debtors included debts owing by State Government related entities totalling \$252k (no risk) and lease monies owed by naval base tenants totalling \$319k (on payment plans).

## **Strategic Plans/Policy Implications**

### Listening and Leading

A community focused, sustainable, accountable and progressive organisation

- Best practice Governance, partnerships and value for money.

## **Budget/Financial Implications**

Budget amendments are referred to Council's Expenditure Review Committee for recommendation.

Council's adopted budget surplus for FY23 of \$360,899 has reduced over the year to \$282,954 due to various Council decisions (including the mid-year budget review).

These budget surplus changes are listed at Note 8 in the financial report.

**Legal Implications**

N/A

**Community Consultation**

N/A

**Risk Management Implications**

It is important that Council reviews the performance of its adopted budget each month for revenue, expenditure, and the closing financial position.

This enables it to be informed on and identify any potential financial risks.

**Advice to Proponents/Submitters**

N/A

**Implications of Section 3.18(3) *Local Government Act 1995***

**Nil**

**CITY OF COCKBURN**  
**MONTHLY FINANCIAL REPORT**  
**(Containing the Statement of Financial Activity)**  
**For the Period Ended 28 February 2023**

LOCAL GOVERNMENT ACT 1995  
LOCAL GOVERNMENT (FINANCIAL MANAGEMENT) REGULATIONS 1996

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MONTHLY FINANCIAL REPORT  
FOR THE PERIOD ENDED 28 FEBRUARY 2023

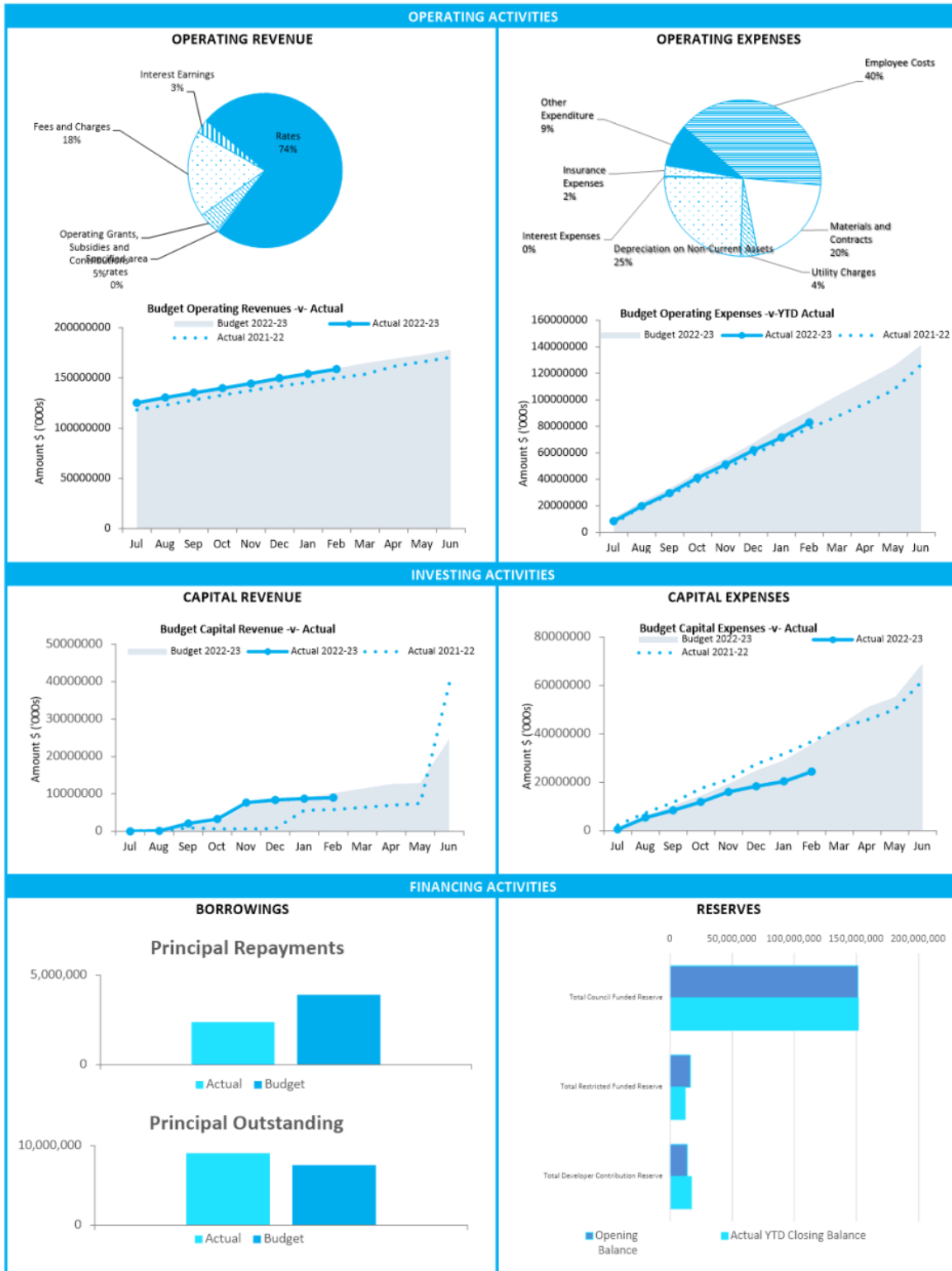
SUMMARY INFORMATION

Funding surplus / (deficit) Components				
<b>Funding surplus / (deficit)</b>				
	Amended Budget	YTD Budget (a)	YTD Actual (b)	Var. \$ (b)-(a)
Opening	\$11.93 M	\$11.93 M	\$11.93 M	(\$0.00 M)
Closing	\$0.28 M	\$68.51 M	\$77.37 M	\$8.86 M
Refer to Statement of Financial Activity				
<b>Cash and financial assets</b>				
	\$263.33 M	% of total		
Unrestricted Cash	\$79.40 M	30.2%		
Restricted Cash	\$183.93 M	69.8%		
Refer to Note 2 - Cash and Financial Assets				
Key Operating Activities				
<b>Amount attributable to operating activities</b>				
Amended Budget	YTD Budget (a)	YTD Actual (b)	Var. \$ (b)-(a)	
\$36.43 M	\$70.48 M	\$81.50 M	\$11.02 M	
Refer to Statement of Financial Activity				
				<b>Employee Cost</b>
				YTD Actual (\$43.09 M) % Variance
				YTD Budget (\$46.16 M) (6.7%)
				Refer to Statement of Financial Activity
<b>Rates Revenue</b>		<b>Fees and Charges</b>		<b>Materials &amp; Contracts</b>
YTD Actual	\$118.26 M % Variance	YTD Actual	\$28.68 M % Variance	YTD Actual (\$21.90 M) % Variance
YTD Budget	\$118.63 M 0.0%	YTD Budget	\$29.06 M (1.3%)	YTD Budget (\$27.73 M) (21.0%)
Refer to Statement of Financial Activity		Refer to Statement of Financial Activity		Refer to Statement of Financial Activity
Key Investing Activities				
<b>Amount attributable to investing activities</b>				
Amended Budget	YTD Budget (a)	YTD Actual (b)	Var. \$ (b)-(a)	
(\$46.16 M)	(\$25.66 M)	(\$14.06 M)	\$11.61 M	
Refer to Statement of Financial Activity				
<b>Proceeds on sale</b>		<b>Asset Acquisition</b>		<b>Capital Grants</b>
YTD Actual	\$1.41 M %	YTD Actual	\$24.44 M % Spent	YTD Actual (\$8.98 M) % Received
Amended Budget	\$2.18 M 64.7%	Amended Budget	\$69.11 M 35.4%	Amended Budget (\$20.77 M) 43.2%
Refer to Note 3 - Disposal of Assets		Refer to Note 4 - Capital Acquisition		Refer to Note 4 - Capital Acquisition
Key Financing Activities				
<b>Amount attributable to financing activities</b>				
Amended Budget	YTD Budget (a)	YTD Actual (b)	Var. \$ (b)-(a)	
(\$1.92 M)	\$11.77 M	(\$2.00 M)	(\$13.77 M)	
Refer to Statement of Financial Activity				
<b>Borrowings</b>		<b>Reserves</b>		
Principal repayments	\$2.38 M	Reserves balance	\$180.68 M	
Interest expense	\$0.21 M	Interest earned	\$0.31 M	
Principal due	\$9.05 M	Refer to Note 6 - Cash Reserves		
Refer to Note 5 - Borrowings				

This information is to be read in conjunction with the accompanying Financial Statements and notes.

MONTHLY FINANCIAL REPORT  
FOR THE PERIOD ENDED 28 FEBRUARY 2023

SUMMARY INFORMATION - GRAPHS



This information is to be read in conjunction with the accompanying Financial Statements and Notes.

STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2023

BY NATURE OR TYPE

	Ref Note	Amended Budget	YTD Budget (a)	YTD Actual (b)	Var. \$ (b)-(a)	Var. % (b)-(a)/(a)	Var.
		\$	\$	\$	\$	%	
<b>Opening funding surplus / (deficit)</b>	1(c)	11,927,765	11,927,765	<b>11,927,765</b>	0	0.00%	
<b>Revenue from operating activities</b>							
Rates		118,200,126	118,002,269	<b>117,660,728</b>	(341,541)	(0.29%)	▼
Specified area rates		624,000	623,000	<b>603,860</b>	(19,140)	(3.07%)	
Operating grants, subsidies and contributions		11,863,320	7,227,429	<b>7,038,905</b>	(188,524)	(2.61%)	
Fees and charges		40,972,049	29,058,859	<b>28,684,127</b>	(374,732)	(1.29%)	▼
Interest earnings		6,425,280	4,658,933	<b>4,731,180</b>	72,247	1.55%	
Profit/(loss) on disposal of assets		1,444,042	759,925	<b>263,164</b>	(496,761)	(65.37%)	▼
		<b>179,528,817</b>	<b>160,330,415</b>	<b>158,981,964</b>	(1,348,451)		
<b>Expenditure from operating activities</b>							
Employee costs		(69,593,027)	(46,161,374)	<b>(43,087,779)</b>	3,073,595	6.66%	▲
Materials and contracts		(44,747,678)	(27,734,254)	<b>(21,897,911)</b>	5,836,343	21.04%	▲
Utility charges		(6,209,122)	(4,164,452)	<b>(3,888,346)</b>	276,106	6.63%	
Depreciation on non-current assets		(42,461,606)	(27,205,278)	<b>(26,700,329)</b>	504,949	1.86%	▲
Interest expenses		(500,921)	(214,549)	<b>(210,599)</b>	3,950	1.84%	
Insurance expenses		(2,275,300)	(2,275,300)	<b>(2,345,078)</b>	(69,778)	(3.07%)	
Other expenditure		(15,013,811)	(10,321,115)	<b>(9,429,398)</b>	891,717	8.64%	▲
		<b>(180,801,465)</b>	<b>(118,076,322)</b>	<b>(107,559,440)</b>	10,516,882		
Non-cash amounts excluded from operating activities	1(a)	37,706,729	28,226,274	<b>30,081,716</b>	1,855,442	6.57%	▲
<b>Amount attributable to operating activities</b>		<b>36,434,081</b>	<b>70,480,367</b>	<b>81,504,240</b>	11,023,873		
<b>Investing activities</b>							
Proceeds from non-operating grants, subsidies and contributions		20,765,807	8,884,999	<b>8,976,611</b>	91,612	1.03%	
Proceeds from disposal of assets	3	2,179,894	1,145,986	<b>1,410,911</b>	264,925	23.12%	
Payments for property, plant and equipment and infrastructure	4	(69,106,188)	(33,471,173)	<b>(24,444,077)</b>	9,027,096	26.97%	▲
<b>Amount attributable to investing activities</b>		<b>(46,160,487)</b>	<b>(23,440,189)</b>	<b>(14,056,555)</b>	9,383,634		
<b>Financing Activities</b>							
Transfer from reserves	6	49,018,225	19,784,016	<b>13,612,212</b>	(6,171,804)	(31.20%)	▼
Repayment of debentures	5	(3,900,000)	(1,950,000)	<b>(2,381,916)</b>	(431,916)	(22.15%)	▼
Transfer to reserves	6	(47,036,630)	(6,063,428)	<b>(13,230,934)</b>	(7,167,506)	(118.21%)	▼
<b>Amount attributable to financing activities</b>		<b>(1,918,405)</b>	<b>11,770,588</b>	<b>(2,000,638)</b>	(13,771,226)		
<b>Closing funding surplus / (deficit)</b>	1(c)	<b>282,954</b>	<b>70,738,532</b>	<b>77,374,812</b>	6,636,280		

KEY INFORMATION

▲ ▼ Indicates a variance between Year to Date (YTD) Actual and YTD Actual data as per the adopted materiality threshold.

Refer to Note 9 for an explanation of the reasons for the variance.

This statement is to be read in conjunction with the accompanying Financial Statements and Notes.

**KEY TERMS AND DESCRIPTIONS  
FOR THE PERIOD ENDED 28 FEBRUARY 2023**

**NATURE OR TYPE DESCRIPTIONS**

**REVENUE**

**RATES**

All rates levied under the *Local Government Act 1995*. Includes general, differential, specified area rates, minimum rates, interim rates, back rates, ex-gratia rates, less discounts and concessions offered. Exclude administration fees, interest on instalments, interest on arrears, service charges and sewerage rates.

**OPERATING GRANTS, SUBSIDIES AND CONTRIBUTIONS**

Refers to all amounts received as grants, subsidies and contributions that are not non-operating grants.

**NON-OPERATING GRANTS, SUBSIDIES AND CONTRIBUTIONS**

Amounts received specifically for the acquisition, construction of new or the upgrading of identifiable non financial assets paid to a local government, irrespective of whether these amounts are received as capital grants, subsidies, contributions or donations.

**REVENUE FROM CONTRACTS WITH CUSTOMERS**

Revenue from contracts with customers is recognised when the local government satisfies its performance obligations under the contract.

**FEES AND CHARGES**

Revenues (other than service charges) from the use of facilities and charges made for local government services, sewerage rates, rentals, hire charges, fee for service, photocopying charges, licences, sale of goods or information, fines, penalties and administration fees. Local governments may wish to disclose more detail such as rubbish collection fees, rental of property, fines and penalties, other fees and charges.

**SERVICE CHARGES**

Service charges imposed under *Division 6 of Part 6 of the Local Government Act 1995. Regulation 54 of the Local Government (Financial Management) Regulations 1996* identifies these as television and radio broadcasting, underground electricity and neighbourhood surveillance services. Exclude rubbish removal charges. Interest and other items of a similar nature received from bank and investment accounts, interest on rate instalments, interest on rate arrears and interest on debtors.

**INTEREST EARNINGS**

Interest and other items of a similar nature received from bank and investment accounts, interest on rate instalments, interest on rate arrears and interest on debtors.

**OTHER REVENUE / INCOME**

Other revenue, which can not be classified under the above headings, includes dividends, discounts, rebates etc.

**PROFIT ON ASSET DISPOSAL**

Excess of assets received over the net book value for assets on their disposal.

**EXPENSES**

**EMPLOYEE COSTS**

All costs associate with the employment of person such as salaries, wages, allowances, benefits such as vehicle and housing, superannuation, employment expenses, removal expenses, relocation expenses, worker's compensation insurance, training costs, conferences, safety expenses, medical examinations, fringe benefit tax, etc.

**MATERIALS AND CONTRACTS**

All expenditures on materials, supplies and contracts not classified under other headings. These include supply of goods and materials, legal expenses, consultancy, maintenance agreements, communication expenses, advertising expenses, membership, periodicals, publications, hire expenses, rental, leases, postage and freight etc. Local governments may wish to disclose more detail such as contract services, consultancy, information technology, rental or lease expenditures.

**UTILITIES (GAS, ELECTRICITY, WATER, ETC.)**

Expenditures made to the respective agencies for the provision of power, gas or water. Exclude expenditures incurred for the reinstatement of roadwork on behalf of these agencies.

**INSURANCE**

All insurance other than worker's compensation and health benefit insurance included as a cost of employment.

**LOSS ON ASSET DISPOSAL**

Shortfall between the value of assets received over the net book value for assets on their disposal.

**DEPRECIATION ON NON-CURRENT ASSETS**

Depreciation expense raised on all classes of assets.

**INTEREST EXPENSES**

Interest and other costs of finance paid, including costs of finance for loan debentures, overdraft accommodation and refinancing expenses.

**OTHER EXPENDITURE**

Statutory fees, taxes, allowance for impairment of assets, member's fees or State taxes. Donations and subsidies made to community groups.



STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2023

STATUTORY REPORTING BY BUSINESS UNIT

	Ref Note	Amended Budget	YTD Budget (a)	YTD Actual (b)	Var. \$ (b)-(a)	Var. % (b)-(a)/(a)	Var.
		\$	\$	\$	\$	%	
<b>Opening funding surplus / (deficit)</b>	1(c)	11,927,765	11,927,765	<b>11,927,765</b>	(0)	(0.00%)	
<b>Revenue from operating activities</b>							
Office of the CEO		0	0	<b>759</b>	759	0.00%	
Corporate Strategy		0	0	<b>2,158</b>	2,158	0.00%	
Governance, Risk & Compliance		1,653	1,102	<b>864</b>	(238)	(21.60%)	
Finance		127,872,961	124,705,716	<b>124,541,364</b>	(164,352)	(0.13%)	
Information & Technology		0	0	<b>916</b>	916	0.00%	
Procurement		1,927	1,927	<b>1,927</b>	0	0.00%	
Library & Cultural Services		279,324	210,328	<b>114,347</b>	(95,981)	(45.63%)	
Recreation Infrastructure & Services		14,001,313	9,602,930	<b>9,617,527</b>	14,597	0.15%	
Community Development & Services		8,024,815	5,088,087	<b>4,789,203</b>	(298,884)	(5.87%)	
Community Safety & Ranger Services		1,206,960	826,649	<b>973,623</b>	146,974	17.78%	
Development Assessment & Compliance		3,577,282	2,591,966	<b>2,200,480</b>	(391,486)	(15.10%)	▼
Planning		148,702	(13,420)	<b>25,276</b>	38,696	(288.35%)	
Sustainability & Environment		847,076	550,235	<b>345,551</b>	(204,684)	(37.20%)	
Operations & Maintenance		20,335,073	14,423,385	<b>13,859,130</b>	(564,255)	(3.91%)	▼
Projects		0	0	<b>84,865</b>	84,865	0.00%	
Property & Assets		2,934,730	2,143,509	<b>2,129,418</b>	(14,091)	(0.66%)	
Business & Economic Development		0	0	<b>1,760</b>	1,760	0.00%	
People Experience		297,000	198,000	<b>292,798</b>	94,798	47.88%	
		<b>179,528,816</b>	<b>160,330,414</b>	<b>158,981,966</b>	(1,348,448)		
<b>Expenditure from operating activities</b>							
Executive Support		(3,423,979)	(2,206,460)	<b>(2,036,347)</b>	170,113	7.71%	
Corporate Strategy		(389,536)	(235,092)	<b>(155,373)</b>	79,719	33.91%	
Governance, Risk & Compliance		(1,010,050)	(707,226)	<b>(676,149)</b>	31,077	4.39%	
Finance		(7,538,565)	(5,405,591)	<b>(4,942,800)</b>	462,791	8.56%	▲
Information & Technology		(8,418,881)	(5,840,571)	<b>(5,661,866)</b>	178,705	3.06%	
Procurement		(936,248)	(566,033)	<b>(554,327)</b>	11,706	2.07%	
Library & Cultural Services		(7,973,014)	(5,440,369)	<b>(5,041,401)</b>	398,968	7.33%	▲
Recreation Infrastructure & Services		(15,733,866)	(10,112,236)	<b>(9,393,488)</b>	718,748	7.11%	▲
Community Development & Services		(13,137,407)	(8,919,451)	<b>(7,000,755)</b>	1,918,696	21.51%	▲
Community Safety & Ranger Services		(6,666,132)	(3,985,400)	<b>(3,260,373)</b>	725,027	18.19%	▲
Development Assessment & Compliance		(6,735,280)	(4,123,802)	<b>(3,465,206)</b>	658,596	15.97%	▲
Planning		(3,134,073)	(1,661,489)	<b>(1,528,195)</b>	133,294	8.02%	
Sustainability & Environment		(4,244,605)	(2,837,335)	<b>(2,093,736)</b>	743,599	26.21%	▲
Operations & Maintenance		(81,456,920)	(52,149,710)	<b>(50,602,867)</b>	1,546,843	2.97%	▲
Projects		(1,093,167)	(1,826,137)	<b>(1,570,091)</b>	256,046	14.02%	
Property & Assets		(11,253,097)	(7,412,759)	<b>(6,514,715)</b>	898,044	12.11%	▲
Stakeholder Management		(1,067,949)	(720,242)	<b>(665,574)</b>	54,668	7.59%	
Communications & Marketing		(1,780,180)	(1,120,800)	<b>(1,076,421)</b>	44,379	3.96%	
Customer Experience		(1,248,337)	(802,141)	<b>(612,173)</b>	189,968	23.68%	
Business & Economic Development		(607,806)	(424,620)	<b>(354,400)</b>	70,220	16.54%	
People Experience		(3,268,578)	(2,137,544)	<b>(2,111,244)</b>	26,300	1.23%	
Transformation, Culture & Innovation		(612,800)	(378,349)	<b>(217,896)</b>	160,453	42.41%	
Internal Recharging		929,007	937,037	<b>1,975,958</b>	1,038,921	(110.87%)	
		<b>(180,801,463)</b>	<b>(118,076,320)</b>	<b>(107,559,439)</b>	10,516,881		
Non-cash amounts excluded from operating activities	1(a)	37,706,729	28,226,274	<b>30,081,716</b>	1,855,442	6.57%	▲
<b>Amount attributable to operating activities</b>		<b>36,434,082</b>	<b>70,480,368</b>	<b>81,504,243</b>	11,023,875		
<b>Investing Activities</b>							
Proceeds from non-operating grants, subsidies and contributions		20,765,807	8,884,999	<b>8,976,611</b>	91,612	1.03%	
Proceeds from disposal of assets	3	2,179,894	1,145,986	<b>1,410,911</b>	264,925	23.12%	
Payments for property, plant and equipment and infrastructure	4	(69,106,188)	(33,471,173)	<b>(24,444,077)</b>	11,250,837	26.97%	▲
<b>Amount attributable to investing activities</b>		<b>(46,160,487)</b>	<b>(23,440,189)</b>	<b>(14,056,555)</b>	11,607,375		
<b>Financing Activities</b>							
Transfer from reserves	6	49,018,225	19,784,016	<b>13,612,212</b>	(6,171,804)	(31.20%)	▼
Repayment of debentures	5	(3,900,000)	(1,950,000)	<b>(2,381,916)</b>	(431,916)	(22.15%)	▼
Transfer to reserves	6	(47,036,630)	(6,063,428)	<b>(13,230,934)</b>	(7,167,506)	(118.21%)	▼
<b>Amount attributable to financing activities</b>		<b>(1,918,405)</b>	<b>11,770,588</b>	<b>(2,000,638)</b>	(13,771,226)		
<b>Closing funding surplus / (deficit)</b>	1(c)	<b>282,954</b>	<b>70,738,533</b>	<b>77,374,811</b>			

KEY INFORMATION

▲ ▼ Indicates a variance between Year to Date (YTD) Actual and YTD Actual data as per the adopted materiality threshold. Refer to threshold. Refer to Note 9 for an explanation of the reasons for the variance.

The material variance adopted by Council for the 2022-23 year is \$300,000 or 0.00% whichever is the greater.

This statement is to be read in conjunction with the accompanying Financial Statements and notes.

**MONTHLY FINANCIAL REPORT  
FOR THE PERIOD ENDED 28 FEBRUARY 2023**

**BASIS OF PREPARATION**

**BASIS OF PREPARATION**

**REPORT PURPOSE**

This report is prepared to meet the requirements of *Local Government (Financial Management) Regulations 1996 , Regulation 34* . Note: The statements and accompanying notes are prepared based on all transactions recorded at the time of preparation and may vary due to transactions being processed for the reporting period after the date of preparation.

**BASIS OF ACCOUNTING**

This statement comprises a special purpose financial report which has been prepared in accordance with Australian Accounting Standards (as they apply to local governments and not-for-profit entities) and Interpretations of the Australian Accounting Standards Board, and the *Local Government Act 1995* and accompanying regulations.

The *Local Government (Financial Management) Regulations 1996* take precedence over Australian Accounting Standards. Regulation 16 prohibits a local government from recognising as assets Crown land that is a public thoroughfare, such as land under roads, and land not owned by but under the control or management of the local government, unless it is a golf course, showground, racecourse or recreational facility of State or regional significance. Consequently, some assets, including land under roads acquired on or after 1 July 2008, have not been recognised in this financial report. This is not in accordance with the requirements of *AASB 1051 Land Under Roads paragraph 15* and *AASB 116 Property, Plant and Equipment paragraph 7*.

Accounting policies which have been adopted in the preparation of this financial report have been consistently applied unless stated otherwise. Except for cash flow and rate setting information, the report has been prepared on the accrual basis and is based on historical costs, modified, where applicable, by the measurement at fair value of selected non-current assets, financial assets and liabilities.

**PREPARATION TIMING AND REVIEW**

Date prepared: All known transactions up to 28 February 2023

**SIGNIFICANT ACCOUNTING POLICES**

**CRITICAL ACCOUNTING ESTIMATES**

The preparation of a financial report in conformity with Australian Accounting Standards requires management to make judgements, estimates and assumptions that effect the application of policies and reported amounts of assets and liabilities, income and expenses. The estimates and associated assumptions are based on historical experience and various other factors that are believed to be reasonable under the circumstances; the results of which form the basis of making the judgements about carrying values of assets and liabilities that are not readily apparent from other sources. Actual results may differ from these estimates.

**THE LOCAL GOVERNMENT REPORTING ENTITY**

All funds through which the City controls resources to carry on its functions have been included in the financial statements forming part of this financial report.

In the process of reporting on the local government as a single unit, all transactions and balances between those funds (for example, loans and transfers between funds) have been eliminated.

All monies held in the Trust Fund are excluded from the financial statements. A separate statement of those monies

**GOODS AND SERVICES TAX**

Revenues, expenses and assets are recognised net of the amount of GST, except where the amount of GST incurred is not recoverable from the Australian Taxation Office (ATO). Receivables and payables are stated inclusive of GST receivable or payable. The net amount of GST recoverable from, or payable to, the ATO is included with receivables or payables in the statement of financial position. Cash flows are presented on a gross basis. The GST components of cash flows arising from investing or financing activities which are recoverable from, or payable to, the ATO are presented as operating cash flows.

**ROUNDING OFF FIGURES**

All figures shown in this statement are rounded to the nearest dollar.

NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2023

NOTE 1  
STATEMENT OF FINANCIAL ACTIVITY INFORMATION

(a) Non-cash items excluded from operating activities

The following non-cash revenue and expenditure has been excluded from operating activities within the Statement of Financial Activity in accordance with Financial Management Regulation 32.

	Notes	Amended Budget	YTD Budget (a)	YTD Actual (b)
<b>Non-cash items excluded from operating activities</b>				
		\$	\$	\$
<b>Adjustments to operating activities</b>				
Less: (Profit)/loss on asset disposals	3	(1,444,042)	(759,925)	(263,164)
Less: Movement in liabilities associated with restricted cash		(3,410,835)	1,780,921	3,358,592
Less: Movement in leased liabilities		0	0	(7,975)
Less: Movement in other liabilities		100,000	0	71,178
Movement in employee benefit provisions (non-current)		0	0	161,348
Add: Public Open Space payment (non-current)		0	0	61,408
Add: Depreciation on assets		42,461,606	27,205,278	26,700,329
<b>Total non-cash items excluded from operating activities</b>		<b>37,706,729</b>	<b>28,226,274</b>	<b>30,081,716</b>

(b) Adjustments to net current assets in the Statement of Financial Activity

The following current assets and liabilities have been excluded from the net current assets used in the Statement of Financial Activity in accordance with *Financial Management Regulation* 32 to agree to the surplus/(deficit) after imposition of general rates.

		Last Year Closing	This Time Last Year	Year to Date
		30 June 2022	28 February 2022	28 February 2023
<b>Adjustments to net current assets</b>				
Less: Reserves - restricted cash	6	(181,064,488)	(167,538,372)	(180,683,210)
Less: Bonds & deposits		(4,303,373)	(5,377,172)	(3,818,003)
Add: Borrowings	5	3,934,065	3,261,287	1,552,149
Add: Lease liabilities		114,651	79,579	106,676
Add: Financial assets at amortised cost - non-current	2	101,900,141	910,452	43,860,774
<b>Total adjustments to net current assets</b>		<b>(79,419,004)</b>	<b>(168,664,226)</b>	<b>(138,981,614)</b>
Cash and cash equivalents	2	20,777,520	10,764,601	10,964,771
Financial assets at amortised cost	2	92,800,000	227,500,000	198,500,000
Rates receivables		2,204,257	34,796,268	21,074,518
Receivables		5,731,365	7,221,455	8,479,804
Other current assets		4,773,962	2,717,843	4,186,631
<b>Less: Current liabilities</b>				
Payables		(16,710,475)	(14,012,028)	(13,943,382)
Borrowings	5	(3,934,065)	(3,261,287)	(1,552,149)
Contract liabilities	7	(4,994,711)	(11,819,741)	(2,741,599)
Lease liabilities		(114,651)	(79,579)	(106,676)
Provisions	7	(9,186,433)	(9,642,377)	(8,505,486)
<b>Less: Total adjustments to net current assets</b>	1(b)	<b>(79,419,004)</b>	<b>(168,664,226)</b>	<b>(138,981,614)</b>
<b>Closing funding surplus / (deficit)</b>		<b>11,927,765</b>	<b>75,520,924</b>	<b>77,374,811</b>

CURRENT AND NON-CURRENT CLASSIFICATION

In the determination of whether an asset or liability is current or non-current, consideration is given to the time when each asset or liability is expected to be settled. Unless otherwise stated assets or liabilities are classified as current if expected to be settled within the next 12 months, being the Council's operational cycle.

NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2023

OPERATING ACTIVITIES  
NOTE 2  
CASH AND FINANCIAL ASSETS

Description	Classification	Unrestricted	Restricted	Total Cash	Institution
		\$	\$	\$	
<b>Cash on hand</b>					
Cash at bank	Cash and cash equivalents	940,336	0	940,336	NATIONAL AUSTRALIA BANK
Cash on hand	Cash and cash equivalents	24,435	0	24,435	
Term deposits - current	Cash and cash equivalents	10,000,000	0	10,000,000	NATIONAL AUSTRALIA BANK
Term deposits - current	Financial assets at amortised cost	0	11,700,000	11,700,000	BANK OF QUEENSLAND
Term deposits - current	Financial assets at amortised cost	0	78,300,000	78,300,000	COMMONWEALTH BANK
Term deposits - current	Financial assets at amortised cost	2,000,000	0	2,000,000	ASIA UNITED BANK
Term deposits - current	Financial assets at amortised cost	4,000,000	0	4,000,000	MEMBER'S EQUITY
Term deposits - current	Financial assets at amortised cost	0	26,000,000	26,000,000	WESTPAC
Term deposits - current	Financial assets at amortised cost	9,859,561	14,640,439	24,500,000	NATIONAL AUSTRALIA BANK
Term deposits - current	Financial assets at amortised cost	13,000,000	0	13,000,000	SUNCORP
Term deposits - current	Financial assets at amortised cost	5,000,000	0	5,000,000	CREDIT UNION AUSTRALIA
Term deposits - current	Financial assets at amortised cost	5,000,000	0	5,000,000	DEFENCE BANK
Term deposits - current	Financial assets at amortised cost	8,500,000	0	8,500,000	AMP
Term deposits - current	Financial assets at amortised cost	20,500,000	0	20,500,000	MYSTATE BANK
Other investment - non current	Financial assets at amortised cost	0	860,774	860,774	BARCLAYS BANK
Other investment - non current	Financial assets at amortised cost	0	8,000,000	8,000,000	BANK OF QUEENSLAND
Other investment - non current	Financial assets at amortised cost	0	6,500,000	6,500,000	COMMONWEALTH BANK
Other investment - non current	Financial assets at amortised cost	0	14,000,000	14,000,000	WESTPAC
Other investment - non current	Financial assets at amortised cost	0	11,000,000	11,000,000	NATIONAL AUSTRALIA BANK
Other investment - non current	Financial assets at amortised cost	0	8,500,000	8,500,000	SUNCORP
Other investment - non current	Financial assets at amortised cost	0	5,000,000	5,000,000	AMP
<b>Total</b>		<b>78,824,332</b>	<b>184,501,213</b>	<b>263,325,545</b>	
<b>Comprising</b>					
		<b>Unrestricted</b>	<b>Restricted</b>	<b>Total Cash</b>	
		\$	\$	\$	
Cash and cash equivalents		10,964,771	0	10,964,771	
Financial assets at amortised cost		67,859,561	184,501,213	252,360,774	
		<b>78,824,332</b>	<b>184,501,213</b>	<b>263,325,545</b>	

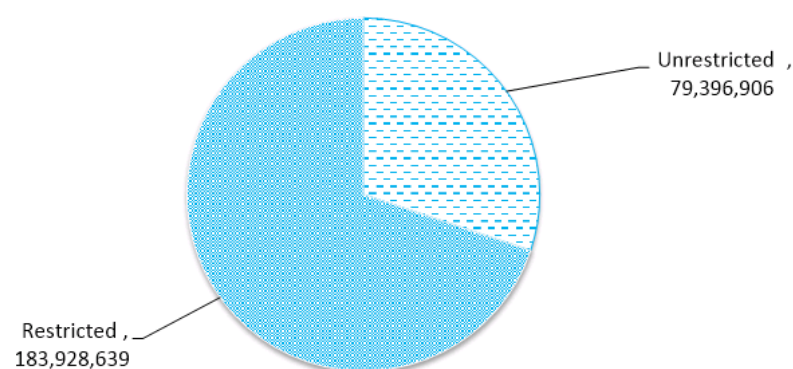
**KEY INFORMATION**

Cash and cash equivalents include cash on hand, cash at bank, deposits available on demand with banks and other short term highly liquid investments highly liquid investments with original maturities of three months or less that are readily convertible to known amounts of cash and which are subject to an insignificant risk of changes in value and bank overdrafts. Bank overdrafts are reported as short term borrowings in current liabilities in the statement of net current assets.

The local government classifies financial assets at amortised cost if both of the following criteria are met:

- the asset is held within a business model whose objective is to collect the contractual cashflows, and
- the contractual terms give rise to cash flows that are solely payments of principal and interest.

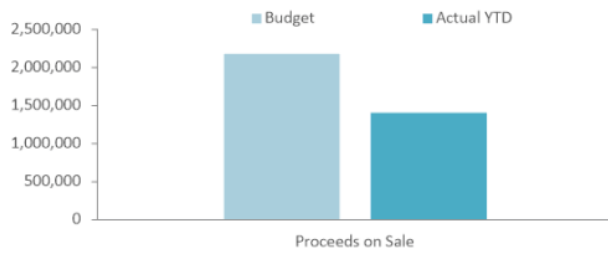
Financial assets at amortised cost held with registered financial institutions are listed in this note other financial assets at amortised cost are provided in Note 4 - Other assets.



NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2023

OPERATING ACTIVITIES  
NOTE 3  
DISPOSAL OF ASSETS

Asset Ref.	Asset description	Budget				YTD Actual			
		Net Book Value	Proceeds	Profit	(Loss)	Net Book Value	Proceeds	Profit	(Loss)
		\$	\$	\$	\$	\$	\$	\$	\$
	Plant and equipment	735,852	2,179,894	1,444,042	0	1,147,747	1,410,911	263,164	0
		<b>735,852</b>	<b>2,179,894</b>	<b>1,444,042</b>	<b>0</b>	<b>1,147,747</b>	<b>1,410,911</b>	<b>263,164</b>	<b>0</b>



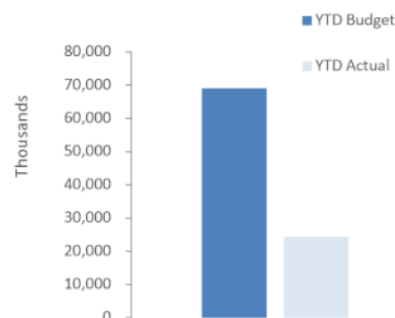
NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2023

INVESTING ACTIVITIES  
NOTE 4  
CAPITAL ACQUISITIONS

Capital acquisitions	Amended		YTD Actual	YTD Actual Variance
	Budget	YTD Budget		
	\$	\$	\$	\$
Land	1,680,000	1,680,000	1,680,000	0
Buildings	8,815,920	5,477,342	3,352,403	(2,124,939)
Furniture and equipment	77,000	77,000	5,000	(72,000)
Plant and equipment	10,304,992	3,067,241	1,729,825	(1,337,416)
Information technology	4,022,732	501,628	258,818	(242,810)
Infrastructure - roads	25,175,682	12,660,308	11,888,460	(771,848)
Infrastructure - drainage	3,070,903	2,550,704	821,280	(1,729,424)
Infrastructure - footpath	1,989,439	1,260,669	542,385	(718,284)
Infrastructure - parks hard	3,950,935	2,878,293	2,336,539	(541,754)
Infrastructure - parks landscaping	1,742,132	1,037,020	538,620	(498,400)
Infrastructure - landfill site	4,922,172	367,428	394,458	27,030
Infrastructure - marina	2,254,348	1,319,223	793,434	(525,789)
Infrastructure - coastal	1,099,933	594,318	102,855	(491,463)
<b>Payments for Capital Acquisitions</b>	<b>69,106,188</b>	<b>33,471,173</b>	<b>24,444,077</b>	<b>(9,027,096)</b>
<b>Total Capital Acquisitions</b>	<b>69,106,188</b>	<b>33,471,173</b>	<b>24,444,077</b>	<b>(9,027,096)</b>
<b>Capital Acquisitions Funded By:</b>				
	\$	\$	\$	\$
Capital grants and contributions	(20,765,807)	(8,884,999)	(8,976,611)	(91,612)
Other (disposals & C/Fwd)	(2,179,894)	(1,145,986)	(1,410,911)	(264,925)
Cash backed reserves				
Plant & Vehicle Replacement	(6,615,861)	(1,890,519)	(868,286)	1,022,233
Information Technology	(26,794)	(17,864)	(26,794)	(8,930)
Major Building Refurbishment	(171,568)	(171,568)	(76,568)	95,000
Waste & Recycling	(5,554,676)	(1,834,382)	(358,231)	1,476,151
Land Development and Investment Fund	(5,959,291)	(4,172,078)	(3,783,241)	388,838
Roads & Drainage Infrastructure	(880,162)	(291,494)	(156,488)	135,007
Community Infrastructure	(1,661,307)	(927,770)	(203,532)	724,239
Port Coogee Special Maintenance - SAR	(393,666)	(162,500)	(113,666)	48,834
Community Surveillance	(200,250)	(93,320)	0	93,320
Restricted Grants & Contributions	(4,554)	(4,554)	(4,555)	(1)
CIHCF Building Maintenance	(100,000)	(100,000)	(121,031)	(21,031)
Cockburn ARC Building Maintenance	(150,000)	(100,000)	0	100,000
Carry Forward Projects	(7,719,724)	(4,495,039)	(2,649,467)	1,845,572
Port Coogee Marina Assets Replacement	(487,816)	(58,664)	(60,666)	(2,002)
Port Coogee Waterways - WEMP	(345,000)	(157,500)	0	157,500
Public Open Space - Various	(125,693)	(83,792)	(125,693)	(41,901)
Contribution - operations	(15,764,125)	(8,879,144)	(5,508,339)	3,370,805
<b>Capital funding total</b>	<b>(69,106,188)</b>	<b>(33,471,173)</b>	<b>(24,444,077)</b>	<b>9,027,096</b>

**SIGNIFICANT ACCOUNTING POLICIES**

All assets are initially recognised at cost. Cost is determined as the fair value of the assets given as consideration plus costs incidental to the acquisition. For assets acquired at no cost or for nominal consideration, cost is determined as fair value at the date of acquisition. The cost of non-current assets constructed by the local government includes the cost of all materials used in the construction, direct labour on the project and an appropriate proportion of variable and fixed overhead. Certain asset classes may be revalued on a regular basis such that the carrying values are not materially different from fair value. Assets carried at fair value are to be revalued with sufficient regularity to ensure the carrying amount does not differ materially from that determined using fair value at reporting date.



NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2023

## FINANCING ACTIVITIES

## NOTE 5

## BORROWINGS

## Repayments - borrowings

Information on borrowings Particulars	Loan No.	1 July 2022	New Loans		Principal Repayments		Principal Outstanding		Interest Repayments	
			Actual	Budget	Actual	Budget	Actual	Budget	Actual	Budget
		\$	\$	\$	\$	\$	\$	\$	\$	
<b>Community amenities</b>										
SMRC		1,434,065	0	0	1,131,916	1,400,000	302,149	34,065	27,408	50,020
<b>Recreation and culture</b>										
To assist fund the Cockburn Central West development	8	10,000,000	0	0	1,250,000	2,500,000	8,750,000	7,500,000	183,192	350,000
<b>C/Fwd Balance</b>		11,434,065	0	0	2,381,916	3,900,000	9,052,149	7,534,065	210,599	400,020
<b>Total</b>		11,434,065	0	0	2,381,916	3,900,000	9,052,149	7,534,065	210,599	400,020
Current borrowings		3,226,983					1,552,149			
Non-current borrowings		8,207,082					7,500,000			
		11,434,065					9,052,149			

All debenture repayments were financed by general purpose revenue.

## KEY INFORMATION

All loans and borrowings are initially recognised at the fair value of the consideration received less directly attributable transaction costs. After initial recognition, interest-bearing loans and borrowings are subsequently measured at amortised cost using the effective interest method. Fees paid on the establishment of loan facilities that are yield related are included as part of the carrying amount of the loans and borrowings.

NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2023

OPERATING ACTIVITIES  
NOTE 6  
CASH RESERVES

Reserve name	Opening Balance	Budget Interest Earned	Actual Interest Earned	Budget Transfers In (+)	Actual Transfers In (+)	Budget Transfers Out (-)	Actual Transfers Out (-)	Budget Closing Balance	Actual YTD Closing Balance
	\$	\$	\$	\$	\$	\$	\$	\$	\$
<b>Council Funded</b>									
Staff Payments & Entitlements	762,036	0	0	1,000,000	0	(41,475)	0	1,720,561	762,036
Plant & Vehicle Replacement	12,190,360	0	0	1,320,000	572,574	(6,615,861)	(868,286)	6,894,499	11,894,648
Information Technology	1,674,702	0	0	1,500,000	0	(26,794)	(26,794)	3,147,908	1,647,908
Major Building Refurbishment	18,113,555	0	0	2,350,000	0	(171,568)	(76,568)	20,291,987	18,036,987
Waste & Recycling	16,226,330	0	0	6,133,456	0	(5,634,676)	(400,996)	16,725,110	15,825,334
Land Development and Investment Fund	8,173,172	0	0	963,272	0	(9,135,291)	(3,783,241)	1,153	4,389,932
Roads & Drainage Infrastructure	11,747,017	0	0	5,500,000	0	(974,818)	(156,488)	16,272,199	11,590,529
Naval Base Shacks	1,261,186	0	0	30,000	30,000	0	0	1,291,186	1,291,186
Community Infrastructure	31,702,223	0	0	10,000,000	0	(2,513,549)	(203,532)	39,188,674	31,498,692
Insurance	2,391,204	0	0	0	0	(558,840)	(21,597)	1,832,364	2,369,607
Greenhouse Action Fund	908,938	0	0	200,000	0	0	0	1,108,938	908,938
HWRP Post Closure Management & Contan	3,385,769	0	0	1,500,000	0	(60,000)	(287)	4,825,769	3,385,482
Municipal Elections	151,420	0	0	150,000	0	0	0	301,420	151,420
Community Surveillance	987,870	0	0	200,000	0	(450,250)	(90,542)	737,620	897,328
Waste Collection	8,312,856	0	0	1,607,149	2,000,000	(852,500)	0	9,067,505	10,312,856
Environmental Offset	248,759	0	0	0	0	0	0	248,759	248,759
Bibra Lake Management Plan	15,267	0	0	0	0	0	0	15,267	15,267
CIHCF Building Maintenance	11,706,747	0	0	800,000	565,221	(100,000)	(121,031)	12,406,747	12,150,937
Cockburn ARC Building Maintenance	6,675,048	0	0	1,500,000	0	(150,000)	0	8,025,048	6,675,048
Carry Forward Projects	12,322,168	0	0	7,414,128	5,563,428	(8,512,208)	(2,836,522)	11,224,088	15,049,074
Port Coogee Marina Assets Replacement	2,084,887	0	0	300,000	300,000	(487,816)	(60,666)	1,897,071	2,324,221
Coogee Beach Foreshore Management	0	0	976	0	116,165	0	0	0	117,141
<b>Total Council Funded Reserve</b>	<b>151,041,516</b>	<b>0</b>	<b>976</b>	<b>42,468,005</b>	<b>9,147,388</b>	<b>(36,285,646)</b>	<b>(8,646,549)</b>	<b>157,223,875</b>	<b>151,543,330</b>



NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2023

OPERATING ACTIVITIES  
NOTE 6  
CASH RESERVES

Reserve name	Opening Balance	Budget Interest Earned	Actual Interest Earned	Budget Transfers In (+)	Actual Transfers In (+)	Budget Transfers Out (-)	Actual Transfers Out (-)	Budget Closing Balance	Actual YTD Closing Balance
	\$	\$	\$	\$	\$	\$	\$	\$	\$
<b>Restricted Funded</b>									
Aged and Disabled Asset Replacement	465,834	0	6,232	0	0	0	0	465,834	472,066
Welfare Projects Employee Entitlements	1,053,457	0	6,685	0	0	0	0	1,053,457	1,060,142
Port Coogee Special Maintenance - SAR	2,040,979	0	26,535	400,000	396,347	(663,123)	(330,346)	1,777,856	2,133,515
Port Coogee Waterways - SAR	194,765	0	3,680	105,000	95,052	0	0	299,765	293,497
Family Day Care Accumulation Fund	11,600	0	155	0	0	0	0	11,600	11,755
Naval Base Shack Removal	826,130	0	11,201	35,000	35,000	0	0	861,130	872,331
Restricted Grants & Contributions	5,388,608	0	0	3,625	0	(4,068,649)	(4,430,168)	1,323,584	958,440
Public Open Space - Various	5,182,134	0	65,808	0	0	(125,693)	(125,693)	5,056,441	5,122,249
Port Coogee Waterways - WEMP	1,075,185	0	14,845	0	0	(482,231)	(55,676)	592,954	1,034,353
Cockburn Coast SAR	85,406	0	1,241	55,000	48,426	(12,048)	(11,008)	128,358	124,065
<b>Total Restricted Funded Reserve</b>	<b>16,324,097</b>	<b>0</b>	<b>136,382</b>	<b>598,625</b>	<b>574,825</b>	<b>(5,351,744)</b>	<b>(4,952,891)</b>	<b>11,570,978</b>	<b>12,082,413</b>
<b>Developer Contribution Plans</b>									
Community Infrastructure (DCA 13)	783,582	0	17,496	3,000,000	1,967,125	(2,979,783)	(12,771)	803,799	2,755,432
Developer Contribution Plans - Various	12,915,294	0	156,429	970,000	1,230,313	(4,401,052)	0	9,484,242	14,302,035
<b>Total Developer Contribution Reserve</b>	<b>13,698,876</b>	<b>0</b>	<b>173,925</b>	<b>3,970,000</b>	<b>3,197,438</b>	<b>(7,380,835)</b>	<b>(12,771)</b>	<b>10,288,041</b>	<b>17,057,468</b>
<b>Total Cash Reserve</b>	<b>181,064,488</b>	<b>0</b>	<b>311,283</b>	<b>47,036,630</b>	<b>12,919,651</b>	<b>(49,018,225)</b>	<b>(13,612,212)</b>	<b>179,082,893</b>	<b>180,683,210</b>

NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2023

OPERATING ACTIVITIES  
NOTE 7  
OTHER CURRENT LIABILITIES

Other current liabilities	Note	Opening Balance 1 July 2022	Liability Increase	Liability Reduction	Closing Balance 28 February 2023
		\$	\$	\$	\$
<b>Contract liabilities</b>					
Unspent grants, contributions and reimbursements - non-operating		4,994,711	5,689,686	(7,942,798)	2,741,599
<b>Total unspent grants, contributions and reimbursements</b>		4,994,711	5,689,686	(7,942,798)	2,741,599
<b>Provisions</b>					
Annual leave		4,812,649	40,355,109	(41,036,056)	4,131,702
Long service leave		4,373,784	0	0	4,373,784
<b>Total Provisions</b>		9,186,433	40,355,109	(41,036,056)	8,505,486
<b>Total other current assets</b>		<b>14,181,144</b>	<b>46,044,795</b>	<b>(48,978,854)</b>	<b>11,247,085</b>
Amounts shown above include GST (where applicable)					

KEY INFORMATION

**Provisions**

Provisions are recognised when the City has a present legal or constructive obligation, as a result of past events, for which it is probable that an outflow of economic benefits will result and that outflow can be reliably measured.

Provisions are measured using the best estimate of the amounts required to settle the obligation at the end of the reporting period.

**Employee benefits**

**Short-term employee benefits**

Provision is made for the City's obligations for short-term employee benefits. Short-term employee benefits are benefits (other than termination benefits) that are expected to be settled wholly before 12 months after the end of the annual reporting period in which the employees render the related service, including wages, salaries and sick leave. Short-term employee benefits are measured at the (undiscounted) amounts expected to be paid when the obligation is settled.

The City's obligations for short-term employee benefits such as wages, salaries and sick leave are recognised as a part of current trade and other payables in the calculation of net current assets.

**Other long-term employee benefits**

The City's obligations for employees' annual leave and long service leave entitlements are recognised as provisions in the statement of financial position.

Long-term employee benefits are measured at the present value of the expected future payments to be made to employees. Expected future payments incorporate anticipated future wage and salary levels, durations of service and employee departures and are discounted at rates determined by reference to market yields at the end of the reporting period on government bonds that have maturity dates that approximate the terms of the obligations. Any remeasurements for changes in assumptions of obligations for other long-term employee benefits are recognised in profit or loss in the periods in which the changes occur. The City's obligations for long-term employee benefits are presented as non-current provisions in its statement of financial position, except where the City does not have an unconditional right to defer settlement for at least 12 months after the end of the reporting period, in which case the obligations are presented as current provisions.

**Contract liabilities**

An entity's obligation to transfer goods or services to a customer for which the entity has received consideration (or the amount is due) from the customer. Grants to acquire or construct recognisable non-financial assets to identified specifications be constructed to be controlled by the City are recognised as a liability until such time as the City satisfies its obligations under the agreement.

**NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2023**

**NOTE 8  
BUDGET AMENDMENTS**

Amendments to original budget since budget adoption. Surplus/(Deficit)

Project/ Activity	Description	Council Resolution	Classification	Non Cash Adjustment	Increase in Available Cash	Decrease in Available Cash	Amended Budget Running Balance
				\$	\$	\$	\$
	<b>Budget adoption</b>						360,899
OP4908	Expenditure Review Committee May 2022 - Womens Shed investigation	OCM 09/06/22 #18.1	Operating Expenses			(20,000)	340,899
OP7965	Recruitment expenses. Confidential item. Marina boardwalk refurbishment funded from Reserves and budget surplus	OCM 14/07/22 #0142-0152	Operating Expenses			(100,000)	240,899
CW6268	Transfer from Marina Reserves to fund marina boardwalk refurbishment	OCM 11/08/22 #18.1	Capital Expenses			(98,000)	142,899
CW6268	Various Amendments as per 8 Sept 22 OCM	OCM 11/08/22 #18.1	Transfer from Reserve		65,332		208,231
Various	Amendments as per 8 Sept 22 OCM	OCM 08/09/22				(120,000)	88,231
Various	Amendments as per 9 June 22 OCM to the Events Calendar	OCM 09/06/22 #18.1	Operating Expenses			(19,000)	69,231
GL 531	External grant from Department of Communities	OCM 13/10/22 #18.1	Operating Revenue		39,922		109,153
CW7577	Expenditure reduction - budget unspent	OCM 13/10/22 #18.1	Capital Expenses		10,000		119,153
CW7664	Missed from carry forward - increased in expenditure	OCM 13/10/22 #18.1	Capital Expenses			(26,000)	93,153
CW7664	Reserve funding for purchasing animal capsule	OCM 13/10/22 #18.1	Transfer from Reserve		26,000		119,153
OP9881	Increased muni funding required to fund traffic officer on casual contract	OCM 13/10/22 #18.1	Operating Expenses			(50,000)	69,153
OP9851	Cost of demolishing at Lot 147 Mopsa Way, Coolbellup	OCM 13/10/22 #18.1	Operating Expenses			(80,000)	(10,847)
OP9851	Waste Reserve to fund cost of demolition	OCM 13/10/22 #18.1	Transfer from Reserve		80,000		69,153
OP4905	Muni fund required for traffic study in Banjup	OCM 13/10/22 #18.1	Operating Expenses			(80,000)	(10,847)
OP4900	Indonesian delegation visit	OCM 08/12/22 #0283	Operating Expenses			(20,298)	(31,145)
Various	Amendments as per Expenditure Review Committee November 2022	OCM 08/12/22 #0284	Capital Expenses		96,000		64,855
Various	Amendments as per Expenditure Review Committee November 2022	OCM 08/12/22 #0284	Transfer to Reserve			(3,275,000)	(3,210,145)
Various	Amendments as per Expenditure Review Committee November 2022	OCM 08/12/22 #0284	Operating Revenue		6,240,740		3,030,595
Various	Amendments as per Expenditure Review Committee November 2022	OCM 08/12/22 #0284	Operating Expenses			(4,221,994)	(1,191,399)
Various	Amendments as per Expenditure Review Committee November 2022	OCM 08/12/22 #0284	Transfer from Reserve		1,192,815		1,416
Various	Mid-year budget review	OCM 09/02/23 #0010			296,538		297,954
OP6297	Public consultation at Omeo Dive Trail	OCM 09/06/22 #0118	Operating Expenses			(15,000)	282,954
				<b>0</b>	<b>8,047,347</b>	<b>(8,125,292)</b>	

**NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2023**

**NOTE 9  
EXPLANATION OF MATERIAL VARIANCES**

The material variance thresholds are adopted annually by Council as an indicator of whether the actual expenditure or revenue varies from the year to date Actual materially.

The material variance adopted by Council for the 2022-23 year is \$300,000 or 0.00% whichever is the greater.

Reporting Program	Var. \$	Var. %	Timing/ Permanent	Explanation of Variance
<b>Revenue from operating activities</b>				
Development Assessment & Compliance	(391,486)	(15.10%)	▼ Timing	Revenue brought forward
Operations & Maintenance	(564,255)	(3.91%)	▼ Timing	Revenue brought forward
<b>Expenditure from operating activities</b>				
Library & Cultural Services	398,968	7.33%	▲ Timing	Expenditure delayed
Recreation Infrastructure & Services	718,748	7.11%	▲ Timing	Expenditure delayed
Community Development & Services	1,918,696	21.51%	▲ Timing	Expenditure delayed
Finance	462,791	8.56%	▲ Timing	Insurance claim reimbursement delayed
Community Safety & Ranger Services	725,027	18.19%	▲ Timing	Expenditure delayed
Sustainability & Environment	743,599	26.21%	▲ Timing	Expenditure delayed
Operations & Maintenance	1,546,843	2.97%	▲ Timing	Expenditure brought forward
Development Assessment & Compliance	658,596	15.97%	▲ Timing	Expenditure delayed
Property & Assets	898,044	12.11%	▲ Timing	Expenditure delayed
Payments for property, plant and equipment and infrastructure	9,027,096	26.97%	▲ Timing	Expenditure delayed
Transfer to reserves	(7,167,506)	(118.21%)	▼ Timing	Revenue brought forward (DCP13)
<b>Investing activities</b>				
Repayment of debentures	(431,916)	(22.15%)	▼ Timing	Expenditure brought forward
Transfer from reserves	(6,171,804)	(31.20%)	▼ Timing	Expenditure brought forward

## 14.3 Community Services

### 14.3.1 Parking Restriction Proposal - Coogee Beach Surf Life Saving Club

**Responsible Executive** Chief of Community Services

**Author** Parking Compliance Officer

**Attachments** 1. Proposed Parking Restrictions (Emergency Access) at Poore Grove, Coogee [↓](#)

#### RECOMMENDATION

That Council:

- (1) APPROVES the installation of 'No Stopping Road or Verge Zone' signage to the subject site (listed within the report) within the Coogee Beach Surf Life Saving Club carpark.

#### Background

The Coogee Beach Surf Life Saving Club is a well-known destination for beach users.

Due to the limited public transport options, many users drive to the site, resulting in the carparks regularly at or over capacity during summer.

As a result of the parking demand, many users illegally park, often in dangerous locations.

In December 2022, the City of Cockburn received a request from the Coogee Beach Holiday Park, requesting the establishment of parking controls to keep their rear emergency access gate clear.

During peak periods, the emergency gates are blocked by vehicles, compromising the use of the emergency vehicle access point.

#### Submission

N/A

#### Report

The Coogee Beach Holiday Park has requested that parking restrictions be installed to keep their rear emergency access gate clear.

As this is an evacuation and emergency access point, parking restrictions are required to keep it clear.

Over the past three (3) months, 19 infringements have been issued for vehicles parked illegally on Poore Grove within the Surf Life Saving Club carpark.

Within close proximity of the subject site three (3) infringements were issued for abandoned vehicles in the carpark.



Image 1 – Subject Site located within the yellow box

The City proposes that the following parking restrictions be installed:

1. 'No Stopping Zone' signs be established within the subject location; and
2. Ground marking installed for the proposed 'No Stopping Zone'.

The proposed restrictions are intended to keep the emergency access clear and allow the Community Safety and Ranger Services Team to take appropriate action if vehicles park in or block the emergency accessway.

### **Strategic Plans/Policy Implications**

#### Community, Lifestyle & Security

A vibrant healthy, safe, inclusive and connected community.

- A safe and healthy community that is socially connected.

#### Listening & Leading

A community focused, sustainable, accountable and progressive organisation.

- High quality and effective community engagement and customer service experiences.

**Budget/Financial Implications**

The proposed works can be completed within the existing operating budget for FY23.

**Legal Implications**

Per clause 8 of the Parking and Parking Facilities Local Law 2007, council are required (by resolution) to approve any new parking enforcement signage.

**Community Consultation**

No wider community consultation has occurred due to the logical requirement to keep the emergency accessway clear of parked vehicles.

The restrictions have been requested by the Coogee Beach Holiday Park.

**Risk Management Implications**

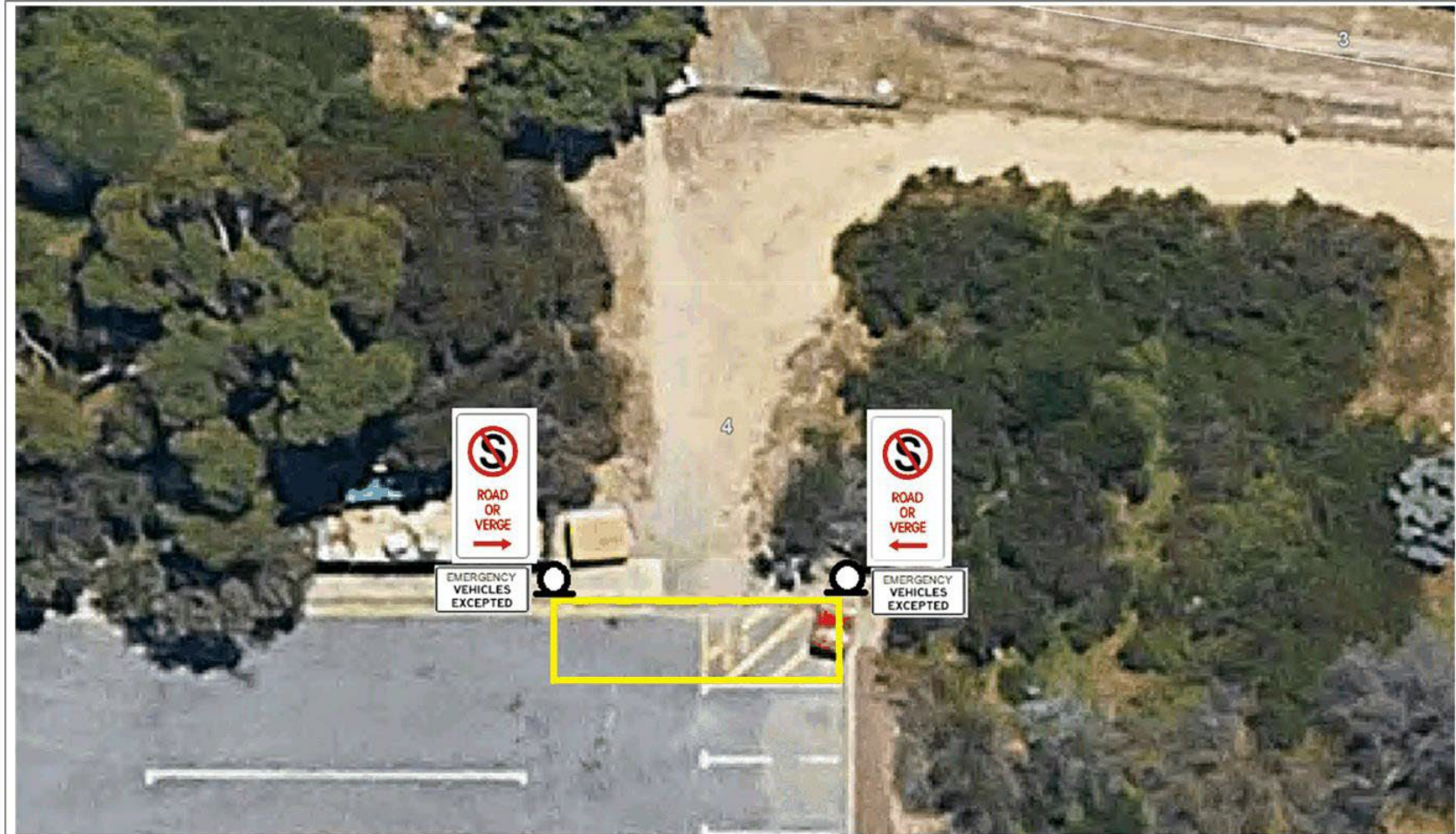
This item has 'minor' reputational risk associated with the recommendation.

**Advice to Proponent(s)/Submitters**

N/A

**Implications of Section 3.18(3) *Local Government Act 1995***

Nil



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	DISCLAIMER - The City of Cockburn provides the information contained herein and bears no responsibility or liability whatsoever for any errors, faults, defects or omissions of information contained in this document.		



**14.3.2 Parking Report - Brindabella Avenue, Aubin Grove**

**Responsible Executive** Chief of Community Services

**Author** Parking Compliance Officer and Head of Community Safety and Ranger Services

**Attachments** 1. Brindabella Ave Petition [↓](#)

**RECOMMENDATION**

That Council:

- (1) NOTES this report.
- (2) NOTES the City will provide a response to the principal petitioner post the Ordinary Council Meeting.

**Background**

The City received a petition on 27 February 2023 from residents along Brindabella Avenue and surrounding streets in Aubin Grove.

The intention of the petition is to have the median strip in Brindabella Avenue changed to an on-street parking facility (refer Attachment 1).

The petition was signed by 14 petitioners.

Due to ongoing requests by the primary petitioner, the City has produced this report for Council to note the outcome of the Administration's review.

A response will be provided to the principal petitioner post the April OCM.

**Submission**

N/A

**Report**

Since late 2022 the principal petitioner has been contacting the City regarding on-street parking availability.



*Image 1 – Subject Site*

We understand the petition was started due to vehicles being infringed for illegally parking within the median strip. Parking in this manner is an offence under the City's Parking Local law.

Prohibited parking on a median strip is not unique to Cockburn and is also an offence under the State's Road Traffic Code.

As such, if on-street parking were to be approved, the median strip would need to be physically altered and widened. Otherwise, vehicles could be considered parking against the flow of traffic, which is also an offence under the City's Local Parking Law and Road Traffic Code.

Road widening would likely be required to ensure that any future parking facility complies with relevant Australian standards.

Asides from the high capital cost, changing the overall street layout is impractical, as the proposed on-street parking would inhibit residents from utilising their vehicle crossover. This could result in residents needing to use nearby roundabouts to enter their property.

Modifying the road network to facilitate kerbside parking would also not be supported when assessed against the City Local Area Traffic Management Policy, as it stands, the road is already traffic calmed and functioning as intended.

As a result of this context, the City's officers intend to write to the petitioner and inform him that the proposal is not supported.

**Strategic Plans/Policy Implications**Community, Lifestyle & Security

A vibrant healthy, safe, inclusive and connected community.

- A safe and healthy community that is socially connected.

City Growth & Moving Around

A growing City that is easy to move around and provides great places to live.

- An integrated, accessible and improved transport network.

**Budget/Financial Implications**

N/A

**Legal Implications**

N/A

**Community Consultation**

N/A

**Risk Management Implications**

This report has 'minor' reputational risk associated with the recommendation.

As this is a report to inform Council pre-emptively of a potential parking matter, there is no risk for implementation.

**Advice to Proponent(s)/Submitters**

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 13 April 2023 Ordinary Council Meeting.

**Implications of Section 3.18(3) *Local Government Act 1995***

Nil



PETITION FORM

To: Chief Executive Officer  
City of Cockburn  
9 Coleville Crescent  
SPEARWOOD WA 6163

PO Box 1215  
BIBRA LAKE WA DC 6965

We, the undersigned electors of the City of Cockburn request (clearly define the purpose)

Be Permitted where applicable in the centre of our street not in garden beds to be allowed to park vehicles for visitors, TRADES, etc attending our houses.

for the following reasons (provide summary of reasons supporting requests)

THIS IS NOT A DANGER TO ANYONE  
We have all been using this for parking for the past 14 years without any issues  
Our Street is a Quiet Street and is not a major Thoroughfare  
There is no other parking available other than up on Gaebler Rd which is a busy Rd or down the end of the Rd at the Park  
On the West side of Brindabella Ave there is even less available space on our driveways due to the footpath and can not possibly fit a car and trailer on property lengthways restricting more parking.

We have never had a problem for over 14 years but now it is and is apparently illegal why??

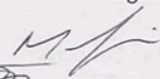

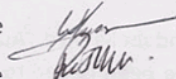
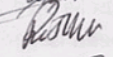

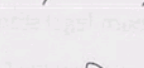
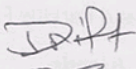
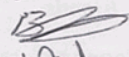

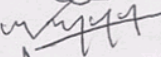


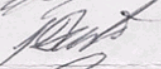

As the initiator of this petition, my name is Wayne Simpson

For further information I can be contacted via email simo69boating@hotmail.com

or by telephone 0400 500 285

City of Cockburn Petition Form

Document Set ID: 7281645  
Version: 1, Version Date: 27/02/2018

Resident	Address	Signature
MARCUS FIRTH	231 Gaebler Road Aubin Grove	
GIND VALLELONGA	233 Gaebler Road Aubin Grove	
WAYNE SIMPSON	3 Brindabella Ave Aubin Grove	
ROBERT WEBSTER	4 Brindabella Ave Aubin Grove	
Tim RICHARDSON	5 Brindabella Ave Aubin Grove	
Jared Godby	6 Brindabella Ave Aubin Grove	
John Van der Drift	7 Brindabella Ave Aubin Grove	
Blake Scott	8 Brindabella Ave Aubin Grove	
melissa Janelle Fuller	9 Brindabella Ave Aubin Grove	
Lydia Yeong	10 Brindabella Ave Aubin Grove	
Arjun Mohan	12 Brindabella Ave Aubin Grove	
Greg Hanwarso	14 Brindabella Ave Aubin Grove	
Justin Cato	16 Brindabella Ave Aubin Grove	
MARK MADRIGAL	18 Brindabella Ave Aubin Grove	

6.

Brindabella Parking 2022

### 14.3.3 Port Coogee Marina - Crane Location Study

**Responsible Executive** Chief of Community Services

**Author(s)** Marina Manager

**Attachments** 1. Crane Location Study - Port Coogee Marina - WGAWA Pty Ltd  
[↓](#)

#### **RECOMMENDATION**

That Council:

- (1) NOTES the Crane Location Study;
- (2) ENDORSES recommendation, Option 5 Rotate Crane and Option 7 Paint Crane; and
- (3) INCLUDES \$15,000 for consideration in the FY24 capital budget.

**TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL**

#### **Background**

A report tabled at the 14 July 2022 Ordinary Council Meeting for Port Coogee Marina Expansion, Item 21.1, and more particularly in reference to the installation of a crane, resulted in the following Alternative Motion:

That Council:

- (1) DEFERS works on the installation until such time to allow further discussion and a detailed report on options of the crane's current location, necessity, other smaller crane options, and relocation three metres to the west of the current proposed location, to minimise impacts on residents
- (2) INVESTIGATES options for turning the crane item into a public art piece.

#### **Submission**

City Officers engaged Engineering and Project Management company, Wallbridge Gilbert Aztec (WGA) to prepare the attached report. Cost \$15,000.

#### **Report**

Following the 14 July 2022 Ordinary Council Meeting, City Officers engaged WGA to prepare a detailed report on the Alternative Motion.

After an onsite investigation, an Options Assessment was carried out detailing four (4) alternative locations, plus a modification to the existing crane location.

Other smaller crane options and the crane as a public art piece were also considered.

All options were listed with advantages and disadvantages, taking into consideration sufficient waterway for a 15m vessel, direct line of site of residential homes, and impact to the boat pen capacity of the marina.

A rough order of magnitude costs was also provided.

The four (4) alternative locations in the report outlining the loading areas for a crane, and the crane itself have been specified for up to 15m vessels, due to some waterway restrictions.

Currently the marina caters for a range of vessel sizes from jet skis up to 30m vessels.

If larger vessels were to be included for crane use, a taller crane with longer arm would be required to ensure enough clearance for increased height and width of vessels to safely manoeuvre items.

A table using a traffic light system was used to compare all options against the same criteria, which identified their recommendation, Option 6 - move the crane 3m to the west, rotate 90 degrees, modify crane boom/arm and paint grey, costing over \$200,000.

Option 5 to modify the crane boom/arm in its current location and Option 7 have significantly less cost at \$10,000 and \$5,000 respectively.

These options are both effective and require minimal works. Although the visual impact to a resident home remains, it can be reduced by modifying the crane arm and locking position, creating a more streamlined view of the crane.

For the significant time and financial savings, coupled with optimum crane functionality in a more centred position, is recommended.

It is important to note that since the 14 July 2022 OCM, the Marina Expansion project reached practical completion and opened to new customers on 1 October 2022, bringing 69 new marina berths across three concrete floating jetties.

The \$5.8M project including the 90m long jarrah public boardwalk, loading area and additional public parking bays was funded from the City's Land Development & Infrastructure Reserve and increased marina revenue will be used to repay this Reserve.

General Municipal funds are not used to support the marina business or infrastructure.

Since the opening of the new jetties, the views of the Southern Peninsula now contain 15m to 25m vessels, some with masts and superstructure taller than the proposed crane.

As Frasers Property develop the southern peninsula with multi-storey residential building, in accordance with the approved Local Structure Plan, the visual impact of the crane will blend into the background of this development.

**Option 7 - Original location paint crane - \$5,000**



**Option 6 - Recommended Crane Location Study option - move the crane 3m to the west, rotate 90 degrees, modify crane boom/arm and paint grey, \$200,000 estimate, and 6 month timeframe**





**Option 5 and 7 - Original location, modify existing crane boom/arm and paint grey - \$15,000**

The Port Coogee Revised Local Structure Plan (LSP) 2016 introduction summarises the initial approvals from relevant stakeholders, from 1992 to 2003 in relation to a residential marina. The LSP was adopted by Council in March 2004.

*The LSP guides the development of the Port Coogee marina, marina residential, other water based residential, dry land residential, retail, and public amenity uses.*

28% of the marina customers live in North Coogee. Highlighting the successful nature of the business, attracting visitors and customers to the area, aligning with strategy.

**Strategic Plans/Policy Implications**Community, Lifestyle & Security

A vibrant healthy, safe, inclusive and connected community.

- Accessible and inclusive community, recreation and cultural services and facilities that enrich our community.

Listening & Leading

A community focused, sustainable, accountable and progressive organisation.

- High quality and effective community engagement and customer service experiences.

**Budget/Financial Implications**

The recommended option is estimated at \$15,000 and will be listed in the draft FY24 budget

Funds from the Land Development & Infrastructure Reserve would be used to deliver any modifications to the crane or location.

**Legal Implications**

N/A

**Community Consultation**

N/A

**Risk Management Implications**

Should Council not endorse the recommendation there is a significant safety risk to pen holders, customers and contractors continuing to launch items over the boardwalk, resulting in personal injury, damage to equipment and property.

In addition, the crane and loading platform are in a public space, requiring the City to mitigate risks associated public access and use.

Furthermore, Council risks the marina being at reduced occupancy due the lack of crane facilities to support pen holder requirements to load items onto their boats.

As the marina village develops it leaves no suitable option for the City to launch or retrieve any ad hoc marine items or equipment which have been required in the past from the Southern Peninsula and Maraboo Bridge.

There is a potential increase in vandalism or damage to the crane should Council proceed with the making it a piece of public art as a result of the public wanting to engage with the crane.

**Advice to Proponent(s)/Submitters**

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 13 April 2023 Ordinary Council Meeting.

**Implications of Section 3.18(3) *Local Government Act 1995***

Nil



City of Cockburn

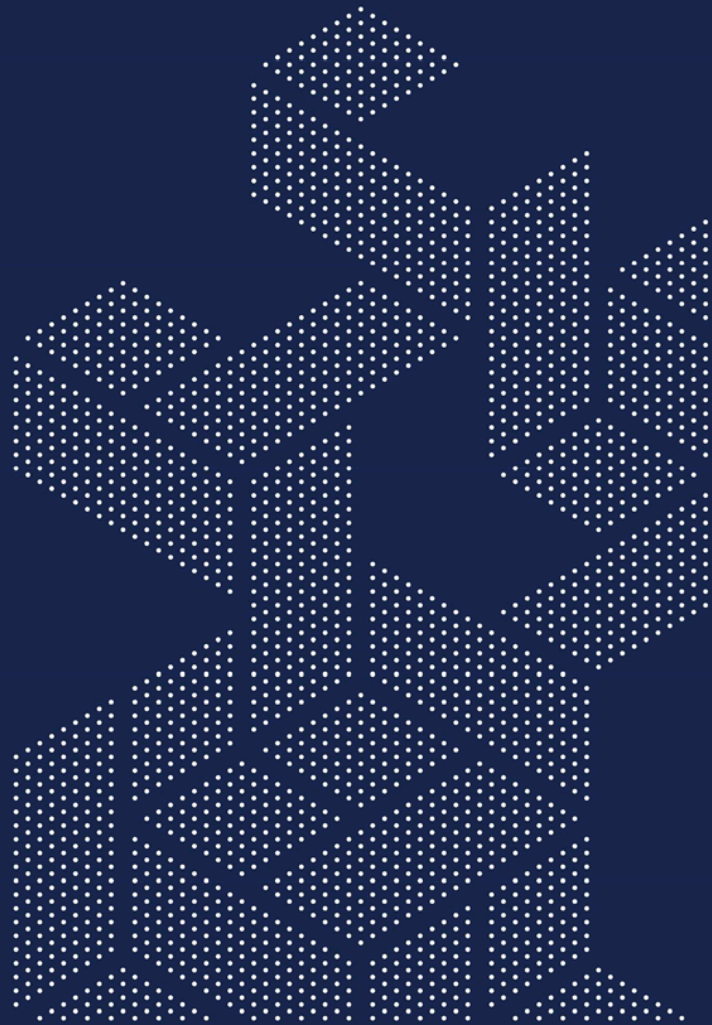
# Port Coogee Crane Location Study

## OPTIONS ASSESMENT

WGA200944

WGA200944-RP-MA-0003\_1

10 February 2023



**Revision History**

REV	DATE	ISSUE	ORIGINATOR	CHECKER	APPROVER
A	01/02/2023	Draft Issue for Client Review	MF	BL	SD
0	10/02/2023	Issue for Use	MF	BL	BL
1	10/02/2023	Issue for Use – Revised Minor Comments	MF	BL	BL

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**Appendix A** Current Jib Crane

**Appendix B** Load Platform Structural Drawings

# 1 INTRODUCTION

## 1.1 Background

### 1.1.1 General

At the City of Cockburn (City) Council Meeting held on 14 July 2022, Council resolved to "Defer works on the installation (of a crane) until such time to allow further discussion and a detailed report on options of the crane's current location, necessity, other smaller crane options, and relocation three meters to the west of the current proposed location, to minimise impacts on residents." The City has engaged WGA to investigate and produce this report. The crane is currently meant to be installed at the loading platform on the southern end of Maraboo Island, as part of the marina expansion project. The loading dock is shown in Figure 1.



Figure 1: Site Location



**1.1.2 Intended Use and Necessity of a Crane at the Marina**

WGA understands that the crane will be primarily used to transfer items from vessel to vehicle and vice versa. These items include tenders, heavy equipment, etc. The crane will be used by recreational users who have undergone an induction, running through operation and safety aspects of the crane. Users will be unsupervised and will be using the crane at their own risk.

It has been reported by the marina manager that a number of potential clients (e.g. pen holders) have been lost, as a result of no such facility being available at the marina. Furthermore, not installing the crane will result in customers continuing to launch items over the boardwalk, ultimately resulting in personal injury, damage to equipment and property.

It is also understood that the crane will be used by City personnel to conduct maintenance works within the marina and surrounding areas. Minor operations (e.g. launching docking pontoons) are sometimes undertaken on a cordoned off section of Ngarkal Beach, which is not ideal and poses a risk to beach goers. A crane would significantly reduce the risk that marina maintenance operations pose to recreational users.

**1.1.3 Previous Lifting Operations at Port Coogee**

Previous lifting operations were undertaken at the southern edge of the marina, shown in Figure 2. A mobile crane was hired to lift equipment, such as vessel docks, out of the water and placed onto the temporary car park for maintenance works or disposal.



**Figure 2: Location in Which Previous Lifting Operations Were Undertaken at Port Coogee**

This is no longer a feasible solution as development of the southern peninsula is now underway and the car park will be removed. Modification of the development plan is possible, which is discussed later in this report.

At the time of writing this report, it is unclear to WGA whether the temporary car park and adjacent seawall has been designed for lifting operations.

### 1.1.4 Crane Selection Process

WGA understands that there was an internal selection process undertaken to determine a suitable crane for the marina. Following discussions with marina personnel, the crane selection criteria was chosen by estimating load size and dimension, the freeboard of the vessel, safety requirements, guard railing, etc.

The selection criteria was also supported by an exercise conducted by City personnel, whereby information was gathered from other yacht clubs and marinas in the area and comparing their lifting capabilities. The outcome of this exercise is shown in Figure 3.

Yacht Club	Jib Crane capacity (T)	Arm Reach (M)	Height (M)	Crane Company	Launching Ramp	Works Area/ Travel Lift (T)	Installed (Years)	Comments
Port Coogee Marina (proposed)	1.5T	3m	5.5	Vector Lifting	no	no	-	
Royal Perth	2 lbs 1T and 3T	0m	0m	0m	no	25T	0m	Jib cranes for member use
Royal Perth Annex	2 lbs 1T and 3T	3-4m	4-5m	Vector Lifting	no	no	30+ years	Jib cranes for member use
Fresh Water Bay	4 x 15T - 3T	2.5m	3.2 - 5.3m	Specialised Lifting Services	yes and beach	15T and 30T		Cranes 30+ Lift 50+
Fishing Boat Harbour	no	-	-	-	no	no		Service jolly where vessels can arrange an external crane
Swan Yacht Club	no	-	-	-	1	50 T	50+ years	
Aquarama	no	-	-	Wiggins Bull	Flo - use forklift	Marine Forklift - Dry Stack 6T	10+ years	Requires forklift ticket - not for customer use
Fremantle Sailing Club	2.5T	4	6.5m	Vector Lifting	6 lane	75T and 50T	11 years	Jib crane for member use

**Figure 3: Details of Crane Particulars from Other Yacht and Marinas in the Area**

Figure 3 shows that the size and capacity of the jib crane selected for the marina, is in line with the specifications of cranes at other facilities in Perth.

WGA also understands from discussions with City personnel that an electric crane was initially deemed preferable over hydraulically powered cranes, as electric cranes were deemed safer to users and the general public. Electric cranes can be isolated completely from power at the switch of a key in the control panel, meaning any damage to exposed cabling would not pose an immediate hazard while the crane is not being used. Electric cranes require less maintenance than a hydraulic system as there is no need to change oil, oil filters or hoses. There is also a risk that oil can leak out of the crane into the water course leading to environmental contamination.

## 1.2 Scope of Work

As per the 'Council Decision' from June 2022 OCM Item 21.1, the scope of work is to investigate options and prepare a detailed report for a marina crane in relation to:

- Other suitable locations
- Necessity of a crane for the marina and waterways
- Smaller and or other crane options and styles
- Relocation of the current proposed crane 3m to the west
- The crane as a public art piece

## 1.3 Purpose

The purpose of this report is to assess a number of potential options regarding location and type of crane and to provide a recommendation that considers stakeholder requirements.

## 1.4 Exclusions

The following items are excluded from WGA's scope:

- Engineering design of the options presented within this document
- Detailed cost estimates (costs provided within this report are indicative only)

## 2 CURRENT SITUATION

### 2.1 Location

The exact location in which the crane is to be installed is shown in Figure 4, along with the location of the crane in relation to the impacted lot.

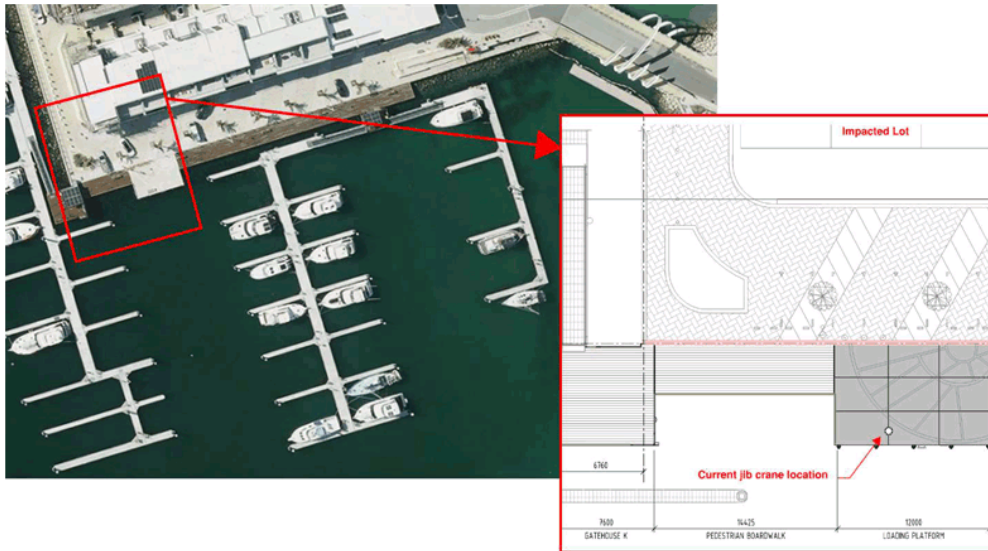
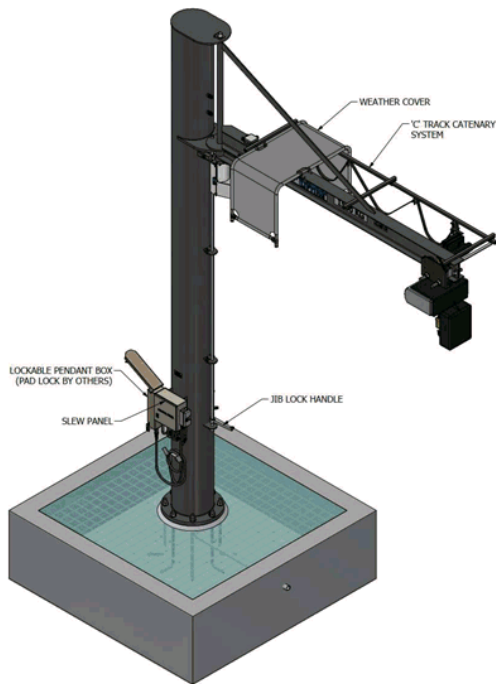


Figure 4: Current Crane Location

### 2.2 Crane Specifications

As part of the Port Coogee Marina Expansion Stage 3, the crane shown in Figure 5 was procured for installation at the loading platform. The crane is designed and built by Vector Lifting. The crane model is a Practicus PS 1530 and is classified as a free standing, slewing jib crane. The maximum slew radius is 270 degrees. A 3D render of the crane is shown in Figure 5.



**Figure 5: 3D Render of Current Jib Crane to be Installed on the Loading Platform**

The jib crane has the following specifications:

- Capacity: 1500kg
- Swing radius: 3.0m
- Overall height: 5.8m
- Column diameter: 0.4m

A 3D render of what the crane will look like once installed at its intended location is shown in Figure 6. The full design drawing can be found in Appendix A.

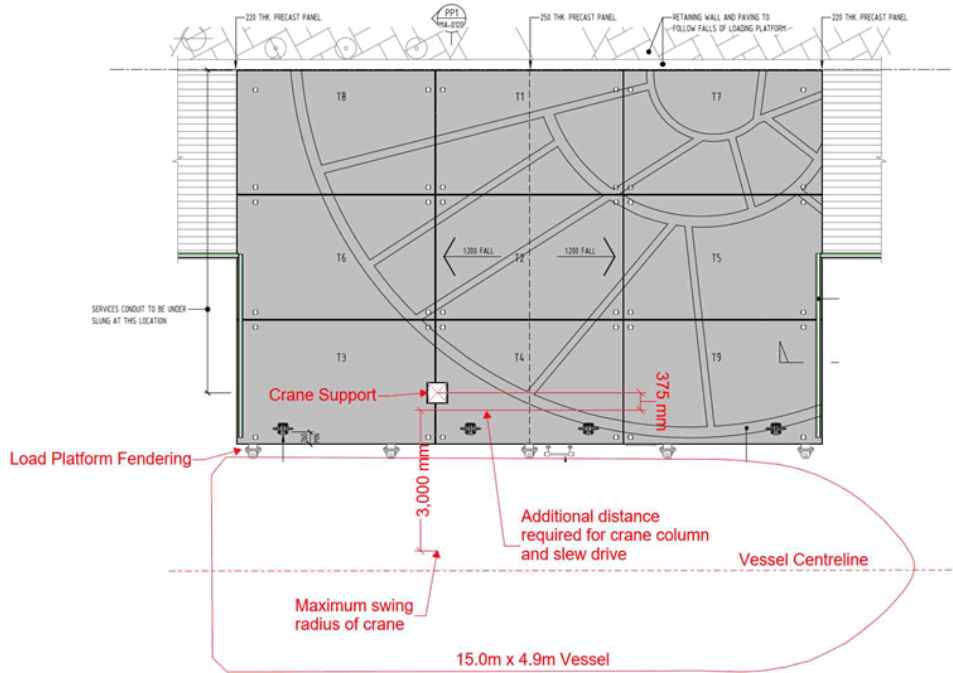


**Figure 6: 3D Render Showing What the Current Crane Will Look Like When Installed at its Design Location**

### 2.3 Reach

Whilst WGA was not involved in the crane selection process, we have estimated the maximum reach by determining the likely maximum beam of a vessel whilst also accounting for the distance from the crane to the edge of the deck and any fendering.

The Australian Standard for Marina Design (AS 3962) provides some guidance relating to the beam of a vessel. For a 15m vessel, the typical beam would be 4.9m. Figure 7 illustrates that despite the long reach, the jib of the crane is just shy of the vessel centreline, indicating that the reach of the crane from the loading platform to the design vessel is sufficient and should not be any less.

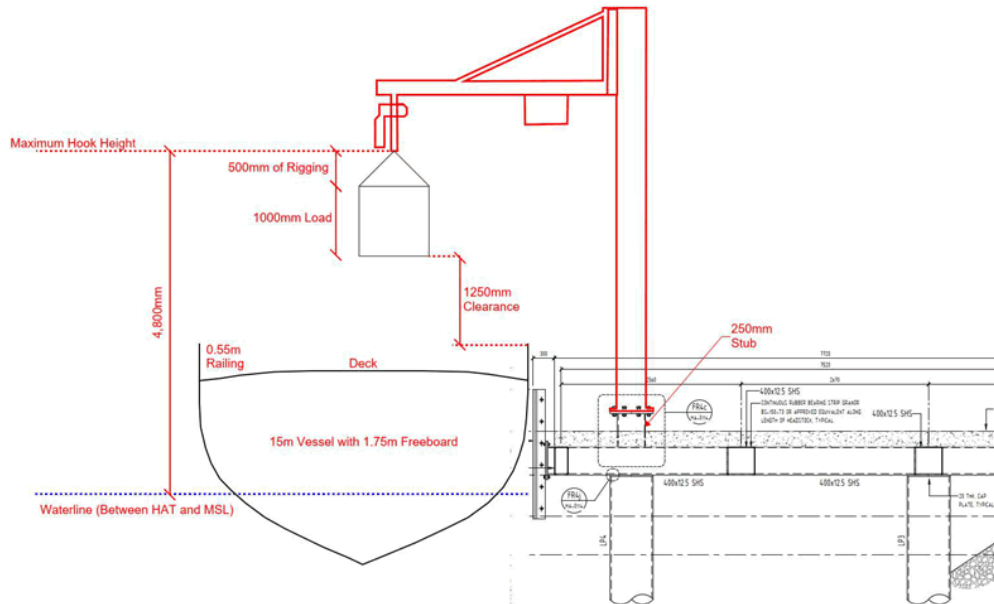


**Figure 7: Crane Reach with 15m Vessel Against Loading Platform**

It should be noted that a 15m vessel is shown as this is noted to be the maximum design vessel for berthing, according to structural drawing WGA200944-DR-MA-0100. Relevant structural drawings can be found in Appendix B.

## 2.4 Height

An estimation of the approximate clearance between the load and the guard railing on a boat is shown in Figure 8.



**Figure 8: Crane Height with Load**

It should be noted that all dimensions are approximate and there are likely several other considerations to be factored in as well.

### 2.5 Crane Slew

The crane will slew (rotate) in a 270-degree arc, as shown in Figure 9.

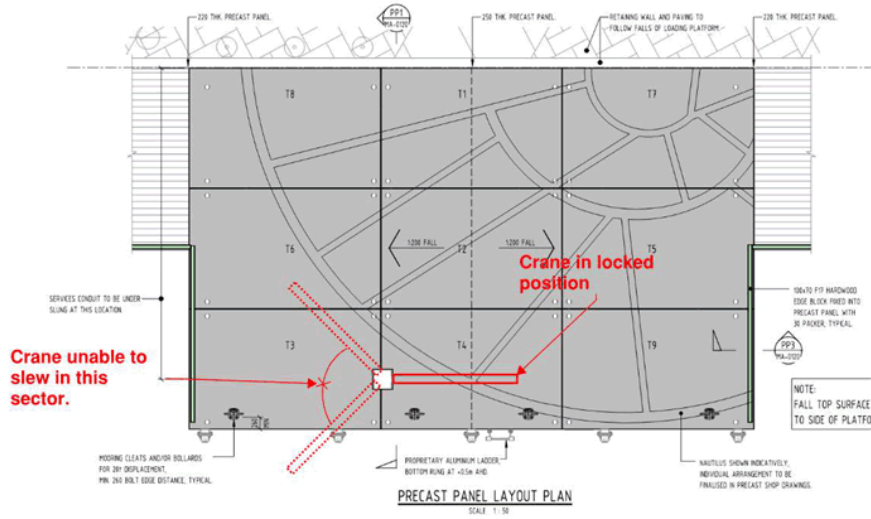


Figure 9: 270 Degree Crane Slew When Installed at the Crane's Intended Location

## 3 OPTIONS ASSESSMENT

### 3.1 General

The options assessment considers:

- Other suitable locations
- Modifications to the existing crane
- Other crane types
- Crane as an art piece
- Do nothing

This is further detailed below.

### 3.2 Site Investigation

An on site investigation was conducted by Mr Michael Fisher, BE (Hons) Ocean, MIEAust, Maritime Engineer at WGA, with Ms Bing Lim, MProjMgt, BE(Hons), BCom, Senior Project Manager at WGA, on 20 January 2023, Samantha Standish, Port Coogee Marina Manager was also present. The site investigation included a high-level inspection of the current location of the jib crane and a walkover of the entire marina facility to determine alternative locations for a new crane to be located which are deemed suitable.

### 3.3 Assessment Criteria

The general engineering criteria used to determine whether a location would be suitable, is listed as follows:

- Sufficient space within the waterway for a 15m vessel to berth alongside the load platform.
- Sufficient space for a vehicle to access the load platform (limited vehicle access is deemed acceptable).
- Must not be in the direct line of sight of residential tenancies.
- Impact to existing marina boat pen capacity should be limited as far as practicable.

### 3.4 Other Suitable Locations

#### 3.4.1 General

Following the site investigation, WGA determined that there are four other appropriate locations within the marina, refer Figure 10, if it were decided that the crane is required to be relocated:

- Location 1: Approximately 3m west of current crane location
- Location 2: Near the western end of Maraboo Loop Bridge
- Location 3: At the western end of Pantheon Avenue on the pedestrian walkway
- Location 4: At the southern end of the marina, along the future development





Figure 10: Suitable Locations for a New Crane at Port Coogee

**3.4.2 Location 1**

Location 1 is located approximately 3m west of the current proposed location of the crane, shown in Figure 11.



**Figure 11: Location 1**

Advantages and disadvantages for Location 1 are described within Table 1.

**Table 1: Advantages and Disadvantages of Location 1**

ADVANTAGES	DISADVANTAGES
The loading platform was considered as part of the planning phase of the marina expansion and as such, adequate room for vehicles and vessels have been provisioned for.	The crane will still be visible to residents and the view from the western-most house will now become impacted as a result of moving the crane west.
The loading platform has been specifically designed to accommodate larger than normal loads within the marina.	Requires modification to existing platform and fabrication of two new concrete panels with unique designs.
Infrastructure for the crane is already in place. Minimal works (compared to other options) will be required to move the crane within the footprint of the loading platform.	
No new piles will be required to be installed as steel piles have been specifically designed for a crane/load platform.	

3.4.3 Location 2

Location 2 is situated on the western side of Maraboo Loop Bridge on the pedestrian walkway. This site was deemed suitable as it is out of the direct line of site of any residents and there is sufficient space within the waterway to accommodate a load platform, with some modifications to boat pens directly adjacent to the proposed location. Location 2 is shown in Figure 12.



Figure 12: Location 2

Significant modification of the marina is required in order to accommodate a new loading platform within this area including modification of pedestrian access as well as potential modifications to the seawall. Figure 13 illustrates some of the changes required.

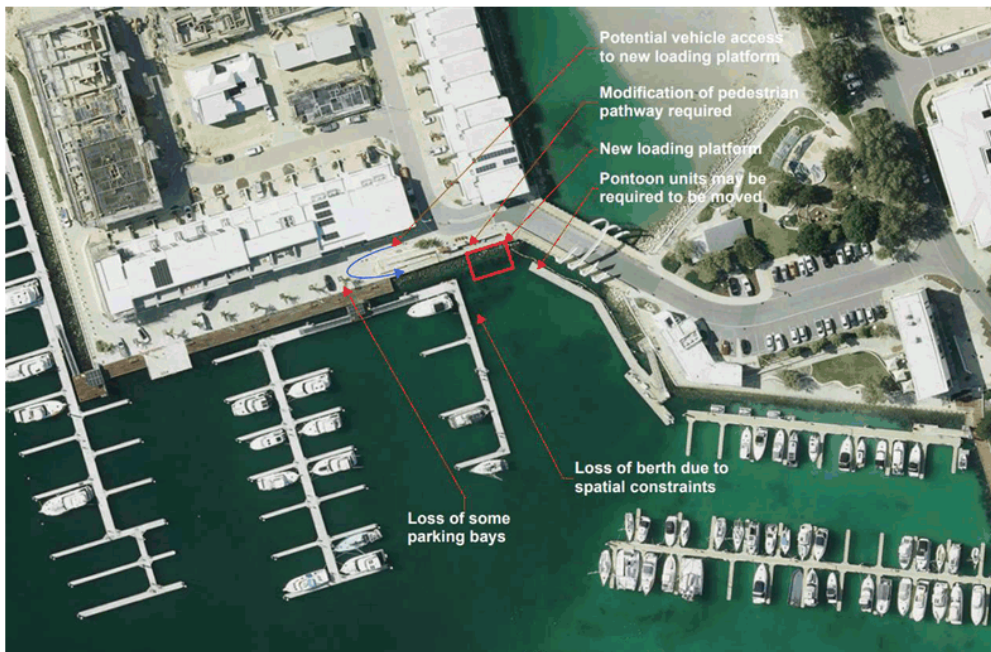


Figure 13: Location 2

Advantages and disadvantages for Location 2 are discussed in Table 2.

**Table 2: Advantages and Disadvantages of Location 2**

ADVANTAGES	DISADVANTAGES
A crane situated in Location 2 will not impact any residents' views.	Significant modification to the marina infrastructure will be required. The CAPEX investment required for this work will be significant.
Facility can be designed for any crane type.	Loss of parking bays for residents/marina users.
	Loss of revenue due to removal of berths.
	Reduced public amenity as a result of shortening the Public Jetty near Maraboo Loop Bridge.
	Timeframes for planning, design and construction will likely be in excess of a year. In this time, there will be no crane within the marina, potentially resulting in revenue loss.
	Pedestrian access and road access to the area may be limited during construction.
	Lost capital as the existing loading platform will no longer be used for its intended purpose.
	Less accessibility for users, and no vehicle access currently.
	The crane will require a longer reach due to the rock revetment and walkway in place.
	Potential loss of disabled access ramp during construction.
	Significant modification required to ensure vehicle access is achievable for loading and unloading of equipment.

3.4.4 Location 3

Location 3 is positioned on the western end of Pantheon Avenue, along the pedestrian walkway. Location 3 was deemed suitable as there is a large open space within the waterway to accommodate a vessel parked alongside the proposed loading platform. Access to the loading platform will be provided by the existing pedestrian access in the area. As this area is busy, access will need to be managed by marina staff. Bollards should be in place at the entrance to ensure that the pathway is only used when the crane/platform is needed. Access will be similar to the current vehicle access at Ngarkal Beach. The proposed arrangement is shown in Figure 14 and Figure 15.

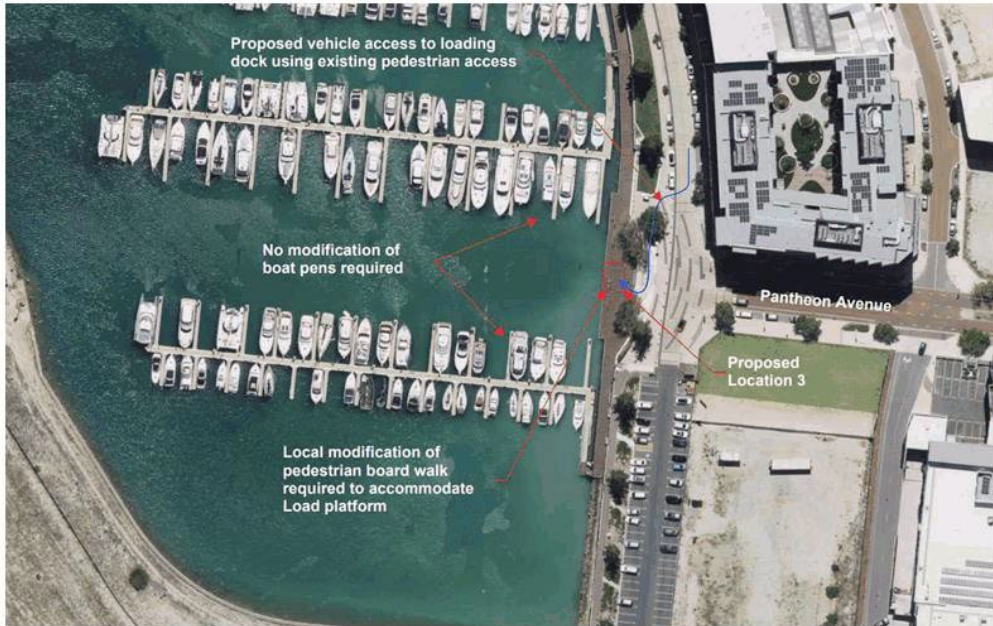


Figure 14: Location 3



Figure 15: Proposed Location 3

Advantages and disadvantages for Location 3 are discussed in Table 3.

**Table 3: Advantages and Disadvantages of Location 3**

ADVANTAGES	DISADVANTAGES
No modification of boat pen arrangement is required.	Pantheon Avenue is the main road into the marina, the crane may impact the visual amenity of the marina.
A crane situated in Location 3 will not impact any residents' views.	The steel piled supports for the boardwalk will likely have to be upgraded in order to accommodate the additional loads.
Larger vessels (greater than 15m) can be designed to berth against the new load platform.	Significant modification of the pedestrian access and stairways would be required in order for a vehicle to access the loading platform.
	All hours access of load platform will not be available due to the large number of pedestrians in area.
	Lost capital as the existing loading platform will no longer be used for its intended purpose.
	Traffic management required at the intersection of Chieftain Esplanade and Pantheon Avenue, which is the main entrance into the marina village.
	Visual amenity for local business in the area may be impacted.

**3.4.5 Location 4**

Location 4 is situated on the southern side of the marina, where development of the land has only just begun.



**Figure 16: Location 4**

The current development plans show that the area directly adjacent to the waterway has been designated as a waterfront park, as described in the 2016 Port Coogee Local Structure Plan Report. An extract of the report is shown in Figure 17.



**Figure 17: Extract from the 2016 Port Coogee Local Structure Plan Report (page 63)**

Figure 17 shows that there is a significant amount of space available both on land and in the waterway.

Advantages and disadvantages for Location 4 are discussed in Table 4.

**Table 4: Advantages and Disadvantages of Location 4**

ADVANTAGES	DISADVANTAGES
Due to differences in height between housing and the waterfront park, there will be no visual impact to residents.	No crane amenity will be available until land development is complete.
There is space within the waterway for a new loading platform, and any new pens can be accommodated around the design.	Planning approval has likely already been provided and any modifications may result in delays to the project.
Larger vessels (greater than 15m) can be designed to berth against the new loading platform.	New infrastructure, including service routes will be expensive to construct.
	Lost capital as the existing loading platform will no longer be used for its intended purpose.
	Vehicles access is not planned for the waterpark area.
	A load platform will need to be designed as part of the future Stage 4 expansion.

### 3.5 Modification to Existing Crane

A number of minor modifications can be made to the existing crane which may lessen the visual impact the crane has on residents.

#### 3.5.1 Change Locking Position of Crane

The locking position of the crane is the position in which the crane stays when it is not in use. Currently, the locking position is parallel with the front edge of the loading platform, as shown previously in Figure 6. Changing the locking position of the arm will provide a streamline view of the crane which would have a lesser impact on the resident’s view of the marina. A 3D render of this is shown in Figure 19.



**Figure 18: Crane Installed at its Intended Location with Locking Position Changed**



Advantages and disadvantages of changing the locking position is discussed in Table 5.

**Table 5: Advantages and Disadvantages of Changing the Locking Position**

ADVANTAGES	DISADVANTAGES
Likely the most cost-effective solution as it only requires installing the crane at a different orientation. Minor modifications may be required to adjust control panels and electrical services to suit the new orientation.	Crane is still located in the middle of the affected resident's lot.
Crane has a more streamlined profile.	

**3.5.2 Move Crane to Location 1, Change Locking Position & Crane Boom**

Moving the crane to Location 1 will create a less obstructed view of the marina. The crane would no longer be located directly in the middle of the affected resident's property. Changing the locking position, as described in Section 3.5.1 will also be required in order to minimise the obstruction. A small access platform for crane operation is required on the western side of the crane as well. A 3D render of this option is shown in Figure 19.



**Figure 19: 3D Render Showing Crane in Location 1**

The new locking position and slew radius will look as shown in Figure 20, if the crane is rotated anticlockwise from its original locking position. This arrangement shows that there will be limited reach over the water.

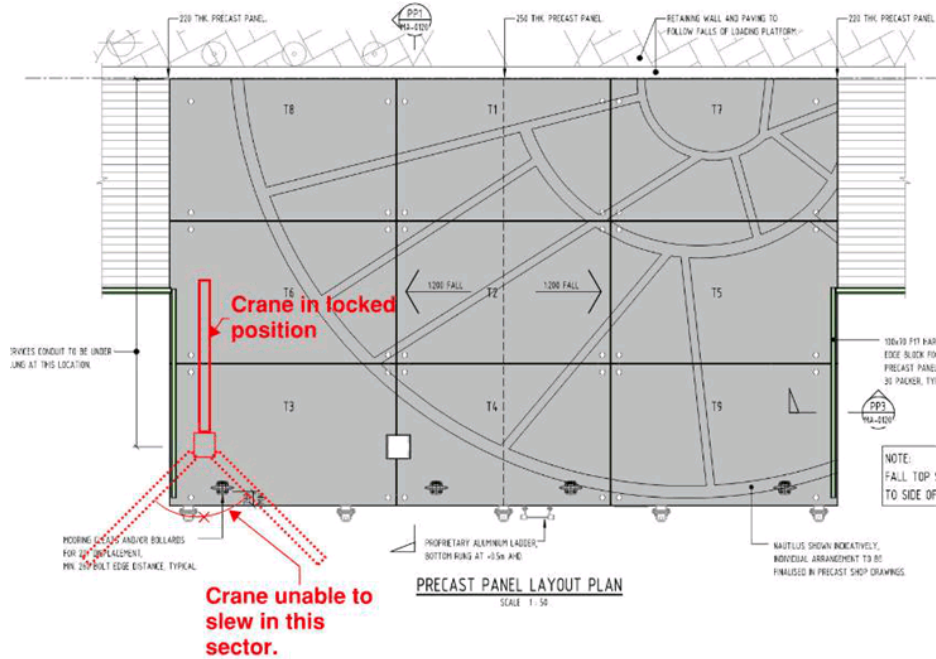


Figure 20: Crane 270 Degree Crane Slew at Location 1

A new crane boom that can slew 360 degrees can be installed to achieve the required reach.

Advantages and disadvantages for this option are discussed in Table 6.

Table 6: Advantages and Disadvantages of Changing the Locking Position of the Crane at Location 1

ADVANTAGES	DISADVANTAGES
Crane is not directly in the line of site of the affected resident.	The view of residents on the western-most lot will now become affected by the crane.
The profile of the crane is more streamlined.	Requires modification to existing platform and fabrication of two new concrete panels with unique designs. New access platform can be fabricated into the new concrete panel.
Minor modifications to the electrical services are required.	Users will need to consider their berthing orientation when lifting items onto their vessels.
The loading platform was considered as part of the planning phase of the marina expansion and as such, adequate room for vehicles and vessels have been provisioned for.	The crane will still be visible to residents and the view from the western-most house will now become impacted as a result of moving the crane west.
The loading platform has been specifically designed to accommodate larger than normal loads within the marina.	Modification of crane boom will be required.
Infrastructure for the crane is already in place. Minimal works (compared to other options) will be required to move the crane within the footprint of the loading platform.	

ADVANTAGES	DISADVANTAGES
No new piles will be required to be installed as steel piles have been specifically designed for a crane/load platform.	

**3.5.3 Paint Crane Another Colour**

As shown in Figure 21, the crane is currently painted black, which would stand out within the marina. By changing the colour of the crane to something that allows it to blend in more, such as grey, it may become more conspicuous and affect the view less.

As seen in Figure 19, changing the colour of the crane from black to grey will allow the crane to blend into the background more and impact the visual amenity less.





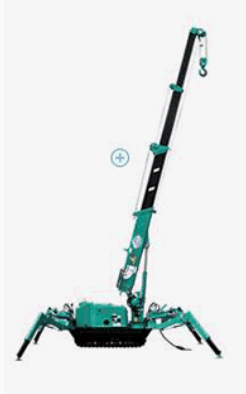
Figure 21: Current Colour of Jib Crane

### 3.6 Other Crane Types

#### 3.6.1 Mobile/Portable Cranes

There are a number of mobile portable cranes currently on the market, all with varying specifications. A few of these have been summarised in Table 7.

Table 7: Available Mobile/Portable Cranes

NAME	CAPACITY	RADIUS/HEIGHT	IMAGE	REFERENCE
Star Worker 1000 Trailer Crane	1000kg	20m/24.5m		<a href="http://ggrgroup.com">Starworker 1000 Trailer Crane (ggrgroup.com)</a>
M1 Micro Crane	905kg	7m/7m		<a href="#">microcrane-m1-spec-sheet.pdf (smartrigcranes.com)</a>
MC104C Spider Crane	995kg	5.1m/5.5m		<a href="http://maeda-minicranes.com">MC104C   SPIDER CRANE   MAEDA MINI CRANES (maeda-minicranes.com)</a>

Note that these cranes are only examples, further work should be done in order to determine the most suitable crane for the marina, following stakeholder selection of the preferred option.

Advantages and disadvantages of mobile/portable boom type cranes are discussed in Table 8.



**Table 8: Advantages and Disadvantages of a Mobile/Portable Crane**

ADVANTAGES	DISADVANTAGES
The crane is out of sight for residents and users of the marina.	Capital investment is required.
A new location for the loading platform is not required.	As the crane is usually stored away, marina users may be unaware that this facility exists.
Less maintenance as a result of environmental exposure as the crane can be stored away.	Engineering assessment of the loading platform may be required to ensure that deck has sufficient capacity for the crane outrigger loads.
	Controls maybe complicated and an online induction may not be suitable to train users. Marina personnel maybe required to operate the crane.
	Requires use of crane charts which can be complicated to understand. Boat users may not always know the weight of the item they are lifting and/or the required reach.
	A suitable storage location near the loading platform would be required.

**3.6.2 Davit/Telescopic Boom Cranes**

There are a number of Davit/Telescopic boom cranes currently on the market, all with varying specifications. A few of these have been summarised in Table 9.

**Table 9: Available Davit/Telescopic Boom Cranes**

NAME	CAPACITY	RADIUS/ HEIGHT	IMAGE	REFERENCE
Vector Lifting Davit Cranes	Up to 1000kg	Up to 3m reach		<a href="#">Davit Cranes – Vector Lifting Vector Lifting</a>
SM2000R Yacht Crane	909kg	2.7m/6m		<a href="#">Marine Cranes Australia – 682kg/909kg SM1500R &amp; SM2000R</a>

Advantages and disadvantages of davit/telescopic boom type cranes are discussed in Table 10.

**Table 10: Advantages and Disadvantages of a Mobile/Portable Crane**

ADVANTAGES	DISADVANTAGES
The locking position of these cranes is generally quite low, and therefore the crane will reduce the visual impact to the affected resident.	The locking position of these cranes is generally quite low, meaning that the boom of the crane will be able to be climbed on and the lifting hook will be exposed, which is a safety concern.
	Lift capacities can vary at different lift radii. Requires use of crane charts which can be complicated to understand. Boat users may not always know the weight of the item they are lifting and/or the required reach.
	Davit cranes are generally hydraulically powered, which is less preferred type of crane, as discussed in Section 1.1.4.
	Further engineering will be needed to ensure the structure can support a portable crane, hence further modification to the structure may be necessary.

### 3.7 The Crane as a Public Art Piece

#### 3.7.1 General

An example of a crane in front of properties, which has been turned into an art piece is shown in Figure 22 and Figure 23. The crane is privately owned and maintained, and is located along the canals in Mandurah, WA.

It should be noted that at the time of writing this report, the exact nature of the cranes reach, and lift capacity are unknown to WGA.



Figure 22: Example of Crane as Art Piece – Angle 1



Figure 23: Example of Crane as Art Piece – Angle 2

Figure 22 and Figure 23 show that it is possible to have a custom-built crane that will fit into its surroundings. The crane will need to be custom designed and fabricated, which can be a costly exercise. A number of companies offer custom crane solutions; however these are generally in relation to geometrical and site constraints rather than a custom shape.

Advantages and disadvantages are discussed in Table 11.

**Table 11: Advantages and Disadvantages of a Custom Crane**

ADVANTAGES	DISADVANTAGES
The crane can be designed to fit on the current mounts on the load platform.	The custom solution will likely be expensive.
The crane can be designed to be more appealing to the eye.	Lost capital, the existing crane will need to be sold.
The crane can be designed to meet the exact specifications that the City requires.	Electrical services may need to be adjusted to suit the custom crane.
Potential to be funded through the City's Public Artwork Policy.	Modifications likely required to the loading platform in order to accommodate the rotating base.

### 3.8 Do Nothing

This option considers the advantages and disadvantages of not installing a crane within the marina, this is discussed in Table 12.

**Table 12: Advantages and Disadvantages of Not Installing a Crane at the Marina**

ADVANTAGES	DISADVANTAGES
Residents are no longer affected by the visual impact of the crane.	No lifting facilities at the marina, boat users who require this service will need to go to another facility or launching ramp.
No installation costs involved.	Loss of potential berth users at the marina as a result of no lifting facilities.
	Launching of vessels and lifting of equipment for marina maintenance and operations will be conducted as per current practise, which takes a considerable amount of time.
	Lost capital as the existing loading platform will no longer be used for its intended purpose and the crane will need to be sold.
	Loading platform now has a redundant pedestal which may become a safety hazard as the pedestal is currently under a temporary bench.
	Boat owners continue to launch and retrieve larger items in an unsafe manner.



### 3.9 Summary of Options and Rough Order of Magnitude Costs

As part of WGA's scope, we have provided a Rough Order of Magnitude (ROM) cost in order to evaluate like for like. It should be noted that a ROM has an approximate accuracy of -25% and +75% of the estimated value.

**Table 13: Summary of Options**

NO.	OPTION	SUMMARY OF WORKS	ORDER OF MAGNITUDE COST (CAPEX INVESTMENT ONLY)
1	Move Location 1	<ul style="list-style-type: none"> <li>Install pedestal at new location</li> <li>Minor adjustments to electrical services and control panels</li> <li>Install walking platform around crane</li> </ul>	\$ 160,000
2	Move Location 2	<ul style="list-style-type: none"> <li>Design and build new loading platform</li> <li>Modify existing pedestrian accessway to accommodate vehicles</li> <li>Remove some berths and a portion of the pedestrian pontoon in order to accommodate space for new loading platform</li> </ul>	\$ 770,000
3	Move Location 3	<ul style="list-style-type: none"> <li>Design and build new loading platform</li> <li>Modify pedestrian pathway to accommodate vehicles</li> </ul>	\$ 670,000
4	Move Location 4	<ul style="list-style-type: none"> <li>Design and build new loading platform</li> </ul>	\$ 580,000
5	Rotate Crane	<ul style="list-style-type: none"> <li>Rotate crane 90 degrees</li> <li>Minor adjustments to electrical services and control panels</li> </ul>	\$ 10,000
6	Move Crane to Location 1, Rotate & Modify Crane Boom	<ul style="list-style-type: none"> <li>Install pedestal at new location</li> <li>Rotate crane 90 degrees</li> <li>Minor adjustments to electrical services and control panels</li> </ul>	\$ 195,000
7	Paint crane	<ul style="list-style-type: none"> <li>Engage contractor to apply marine grade paint</li> </ul>	\$ 5,000
8	Mobile/Portable Crane	<ul style="list-style-type: none"> <li>Purchase and install new crane</li> </ul>	\$ 45,000
9	Davit Telescopic Boom Crane	<ul style="list-style-type: none"> <li>Purchase and install new crane</li> </ul>	\$ 55,000
10	Public Art Piece	<ul style="list-style-type: none"> <li>Seek crane supplier willing to undertake custom design and fabrication of crane</li> <li>Modify mounting and electrical services as specified by supplier</li> </ul>	\$ 250,000
11	Do Nothing	<ul style="list-style-type: none"> <li>No works required to be undertaken</li> </ul>	\$ 0

# 4 RECOMMENDATION

## 4.1 Assessment Criteria

WGA have used the following key criteria in order to assess which option will be best suited to the City:

- Sufficient space within the waterway for a 15m vessel to berth alongside the load platform
- Sufficient space for a vehicle to access the load platform (limited vehicle access is deemed acceptable)
- Minimises the impact to visual amenity for residents
- There is no impact to boat pen capacity within the marina
- Cost
- Modifications required to marina and existing infrastructure
- Crane user functionality

## 4.2 Option Comparison

In order to assess each of the options, WGA have adopted a traffic light system, as shown in Table 14.

Each option has been allocated a colour, based on WGA's assessment of how well the option meets each criteria. The colour descriptions are as follows:

- Green – The option meets the criteria described previously, with some minor deviations
- Orange – There are some impacts on the assessed criteria
- Red – The option does not meet the criteria

**Table 14: Comparison of Options**

OPTION	VESSEL SPACE	VEHICLE SPACE	VISUAL IMPACT	IMPACT TO BOAT PENS	COST	MODIFICATIONS	FUNCTIONALITY
Move Location 1							
Move Location 2							
Move Location 3							
Move Location 4							
Rotate Crane							
Move Crane to Location 1 and Rotate							
Paint Crane							
Mobile/Portable Crane							
Davit Telescopic Boom Crane							
Public Art Piece							
Do Nothing							

Options which do not meet the criteria, and have been given a 'red' designation, were excluded from further consideration. Options with a 'yellow' or 'green' were considered further. Of each of the shortlisted options, WGA have determined that the most suitable options would be to:

- Move the crane to Location 1 (3m west of the current intended location)
- Rotate the crane such that the profile is reduced
- Paint the crane grey

By adopting the options as described above, costs will be kept low, and the visual impact of the crane will be significantly reduced, resulting in a reduced impact on the visual amenity of the marina for the affected residents.

The most cost efficient options (to rotate the crane and/or paint the crane grey) involve installing the crane at its intended location. This reduces visual impact to the impacted resident. To further minimise visual impact, the option to move the crane to Location 1 is the recommended option.

It is envisioned that the following work would be required for the recommended option:

- Engineering design and verification.
- Preparation of pile and removal of grout topping.
- Fabrication and painting of steelwork to provide a base for the crane and a load path to the pile, noting this external member is not the same size as that which currently supports the crane.
- Cut off existing pedestal stub and repaint.
- Potential fabrication of two new concrete panels. Whilst it may have been possible to re-use T4 and cast one new panel the slabs are architectural with unique designs embedded into the surface.
- Walking/access platform around the new crane location.
- Re-paint crane.

Costs are estimated in excess of approximately \$200,000 with an associated six-month delay. This does not include potential loss of revenue from marina customers and/or project contract penalties.

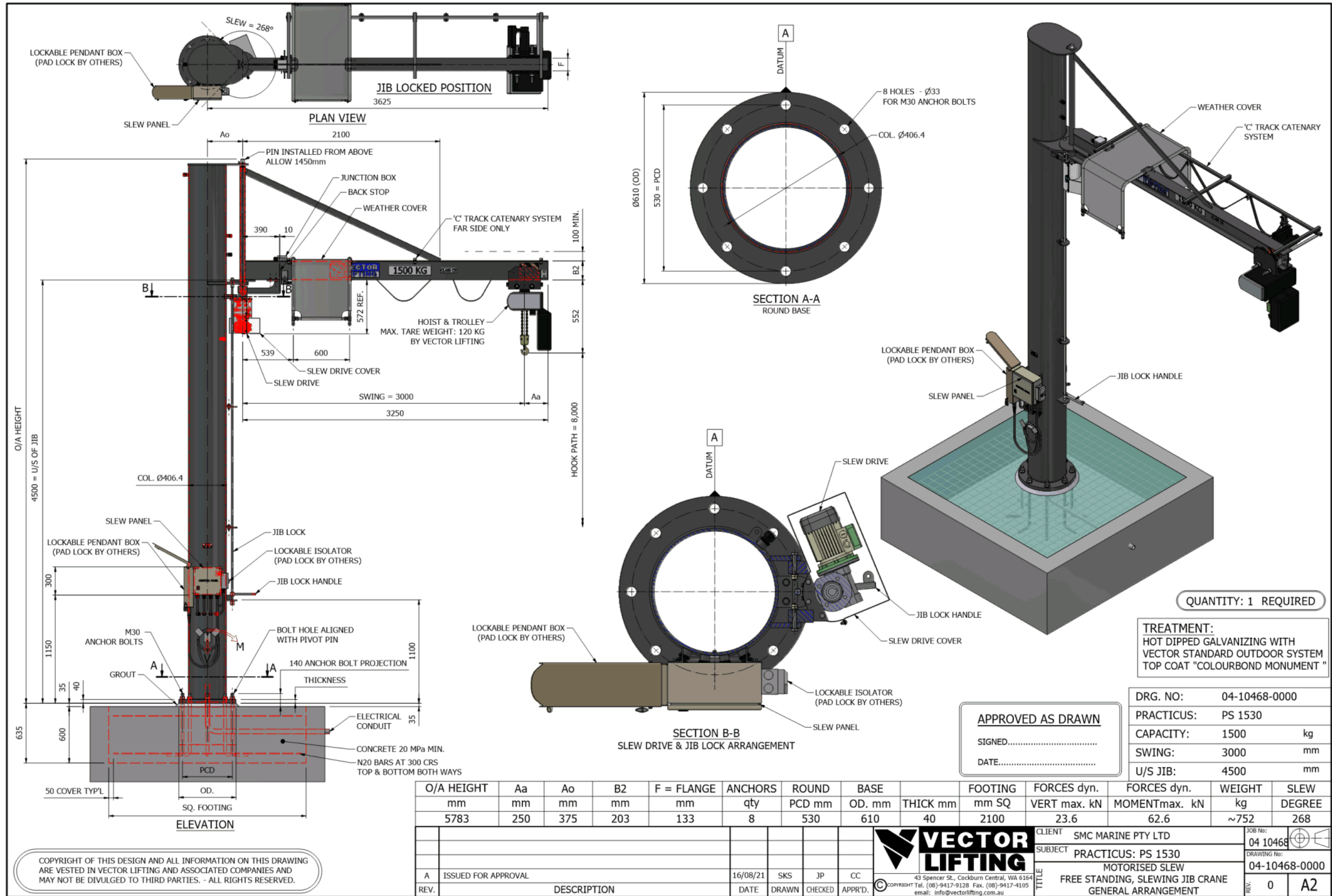
The recommended option is shown in Figure 24.



**Figure 24: 3D Render of Recommended Option with Crane Painted Gray**

**APPENDIX A**  
**CURRENT JIB CRANE**





QUANTITY: 1 REQUIRED

TREATMENT:  
HOT DIPPED GALVANIZING WITH  
VECTOR STANDARD OUTDOOR SYSTEM  
TOP COAT "COLOURBOND MONUMENT"

DRG. NO:	04-10468-0000
PRACTICUS:	PS 1530
CAPACITY:	1500 kg
SWING:	3000 mm
U/S JIB:	4500 mm

**APPROVED AS DRAWN**  
SIGNED.....  
DATE.....

O/A HEIGHT	Aa	Ao	B2	F = FLANGE	ANCHORS	ROUND	BASE	FOOTING	FORCES dyn.	FORCES dyn.	WEIGHT	SLEW
mm	mm	mm	mm	mm	qty	PCD mm	OD. mm	THICK mm	VERT max. kN	MOMENTmax. kN	kg	DEGREE
5783	250	375	203	133	8	530	610	40	23.6	62.6	~752	268

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email: info@vectorlifting.com.au

CLIENT	SMC MARINE PTY LTD	JOB No:	04 10468
SUBJECT	PRACTICUS: PS 1530	DRAWING No:	04-10468-0000
TITLE	MOTORISED SLEW FREE STANDING, SLEWING JIB CRANE GENERAL ARRANGEMENT	REV.	0 A2

REV.	DESCRIPTION	DATE	DRAWN	CHECKED	APPR'D.
A	ISSUED FOR APPROVAL	16/08/21	SKS	JP	CC

**APPENDIX B**  
LOAD PLATFORM STRUCTURAL  
DRAWINGS



GENERAL NOTES

- G1 THESE DRAWINGS MUST BE READ IN CONJUNCTION WITH ALL CONSULTANT DRAWINGS AND WITH SUCH OTHER WRITTEN INSTRUCTIONS ISSUED DURING THE COURSE OF THE CONTRACT.
- G2 THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE STRUCTURE IN A STABLE CONDITION AND ENSURING NO PART SHALL BE OVER STRESSED DURING CONSTRUCTION ACTIVITIES.
- G3 THESE DRAWINGS SHALL NOT BE SCALED. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS NOTED OTHERWISE (U.N.O.).
- G4 THE STRUCTURAL DESIGN CRITERIA IS AS PER TABLE G1.
- G5 COMPONENTS HAVE GENERALLY BEEN DESIGNED IN ACCORDANCE WITH AS4997:2005 GUIDELINES FOR THE DESIGN OF MARITIME STRUCTURES AND AS3962:2020 MARINA DESIGN. WHERE ADDITIONAL GUIDANCE IS REQUIRED BEYOND THE SCOPE OF THE AUSTRALIAN STANDARD, BRITISH STANDARD BS6349 MARITIME STRUCTURES AND PLANS GUIDELINE, GUIDELINES FOR THE DESIGN OF FENDER SYSTEMS (2007) HAVE BEEN USED.
- G6 SUBSTITUTIONS SHALL BE APPROVED BY THE SUPERINTENDENT IN WRITING PRIOR TO PROCEEDING WITH THE WORK.
- G7 REFER DISCREPANCIES TO THE SUPERINTENDENT FOR RESOLUTION BEFORE PROCEEDING WITH THE WORK.
- G8 ANY DISCREPANCIES DISCOVERED DURING CONSTRUCTION ACTIVITIES BETWEEN THE EXISTING STRUCTURE AND THE DRAWINGS SHALL BE REPORTED TO THE SUPERINTENDENT FOR FURTHER ADVICE.
- G9 VERIFY ALL DIMENSIONS PRIOR TO CONSTRUCTION & FABRICATION.
- G10 ALL PROPRIETARY PRODUCTS SHALL BE INSTALLED STRICTLY IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
- G11 LEVEL DATUM = +mHD U.N.O.
- G12 REVEITEMENT LEVELS & CONTOURS SHOWN ON THE DRAWINGS ARE INDICATIVE ONLY - REFER SURVEY DRAWINGS 18152-F02-2.0 & 18152-F03-2.0 FOR CURRENT AS-CONSTRUCTED LEVELS & PROFILES.

DESIGN CRITERIA - (TABLE G1)

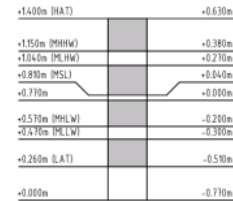
<b>GENERAL:</b>	
- IMPORTANCE LEVEL:	1
- ANNUAL PROBABILITY OF EXCEEDANCE:	1:100 (NO VESSELS AT BERTH)
<b>DESIGN LIFE:</b>	
- FIXED STRUCTURES (BOARDWALK, LOADING PLATFORM, GATEHOUSE PLATFORM)	= 50 YEARS
- GATEHOUSE STRUCTURE	= 50 YEARS
- TIMBER DECKING	= 25 YEARS
- STEELWORK PAINT/CORROSION PROTECTION SYSTEM	= 25 YEARS
- FILE WRAP PROTECTION SYSTEM	= 25 YEARS
- REFER TO CORROSION PROTECTION PLAN (TABLE G2) FOR PROTECTION STRATEGY AND ASSOCIATED CORROSION ALLOWANCES	
- RECOMMENDED INSPECTION STRATEGY IS AN ADVANCED VISUAL CONDITION ASSESSMENT (TO PORTS AUSTRALIA WHARF STRUCTURES CONDITION ASSESSMENT MANUAL WSCAM) EVERY 5 YEARS WITH INTERMEDIATE BASIC VISUAL ASSESSMENT AT 2.5 YEARS	
<b>LIVE LOADS:</b>	
<b>BOARDWALK &amp; GATEHOUSE PLATFORM</b>	
- UNIFORMLY DISTRIBUTED LOAD	= 5 kPa
- CONCENTRATED POINT LOAD (OVER 100mm x 100mm FOOTPRINT)	= 4 kN
<b>LOADING PLATFORM</b>	
- UNIFORMLY DISTRIBUTED LOAD	= 10 kPa
- CONCENTRATED POINT LOAD (OVER 300mm x 150mm FOOTPRINT AT 1.8m SPACING)	= 45 kN
<b>WAVE &amp; CURRENT LOADS:</b>	
- MAX. WAVE HEIGHT (Hmax)	= 0.62m
- PERIOD	= 17s
- DESIGN STORM SURGE LEVEL	= +1.49 m AHD
- CURRENT	= 0.0 m/s
<b>MOORING:</b>	
- BOLLARD ADOPTED AS PER AS4097 - TABLE C1 FOR VESSEL DISPLACEMENT UP TO 20t.	
<b>BERTHING:</b>	
- BERTHING ENERGY ASSUMED BY COMBINATION OF FENDERING, STRUCTURAL DEFLECTIONS AND VESSEL DEFLECTIONS.	
<b>15m VESSEL (LOADING PLATFORM)</b>	
- MAX. VESSEL DISPLACEMENT	= 20t
- NORMAL BERTHING VELOCITY	= 0.3 m/s
- ABNORMAL BERTHING FACTOR	= 2
- ALLOWABLE HULL DEFLECTIONS	= 75mm
- NORMAL BERTHING ENERGY	= 0.81 kN.m
- ABNORMAL BERTHING ENERGY	= 1.61 kN.m

CORROSION PROTECTION PLAN - (TABLE G2)

STRUCTURE COMPONENT	STRUCTURAL ELEMENT	CORROSION PROTECTION METHOD
BOARDWALK	PLES	ALL NEW STEEL PILES SUPPORTING THE STRUCTURE TO BE COATED IN ACCORDANCE WITH THE PILE SPECIFICATION AND HAVE DENS0 SEASHIELD 100 SERIES (OR APPROVED EQUIVALENT) INSTALLED FROM THE PILE CAP TO THE SEA BED WITH NO SACRIFICIAL ANODES REQUIRED.
	STRUCTURAL STEELWORK	SURFACE PROTECTION IN ACCORDANCE WITH TABLE S2 PAINT TO BE MAINTAINED IN ACCORDANCE WITH INSPECTION AND MAINTENANCE PLAN 2mm CORROSION ALLOWANCE TO ALL EXPOSED FACES.
	FRP ELEMENTS	NO CORROSION PROTECTION PLAN REQUIRED.
LOADING PLATFORM	PLES	ALL NEW STEEL PILES SUPPORTING THE STRUCTURE TO BE COATED IN ACCORDANCE WITH THE PILE SPECIFICATION AND HAVE DENS0 SEASHIELD 100 SERIES (OR APPROVED EQUIVALENT) INSTALLED FROM THE PILE CAP TO THE SEA BED FOR REVEITEMENT PILES, AND THE PILE CAP TO 750mm MIN. BELOW LAT FOR SEAWARD PILES SACRIFICIAL ANODES REQUIRED ON THE SEAWARD PILES ONLY.
	STRUCTURAL STEELWORK	SURFACE PROTECTION IN ACCORDANCE WITH TABLE S2 PAINT TO BE MAINTAINED IN ACCORDANCE WITH INSPECTION AND MAINTENANCE PLAN 2mm CORROSION ALLOWANCE TO ALL EXPOSED FACES.
GATEHOUSE PLATFORM	PLES	ALL NEW STEEL PILES SUPPORTING THE STRUCTURE TO BE COATED IN ACCORDANCE WITH THE PILE SPECIFICATION AND HAVE DENS0 SEASHIELD 100 SERIES (OR APPROVED EQUIVALENT) INSTALLED FROM THE PILE CAP TO THE SEA BED FOR REVEITEMENT PILES, AND THE PILE CAP TO 750mm MIN. BELOW LAT FOR SEAWARD PILES SACRIFICIAL ANODES REQUIRED ON THE SEAWARD PILES ONLY.
	STRUCTURAL STEELWORK	SURFACE PROTECTION IN ACCORDANCE WITH TABLE S2 PAINT TO BE MAINTAINED IN ACCORDANCE WITH INSPECTION AND MAINTENANCE PLAN 2mm CORROSION ALLOWANCE TO ALL EXPOSED FACES.
	FRP ELEMENTS	NO CORROSION PROTECTION PLAN REQUIRED.
GATEHOUSE STRUCTURE	ALUMINIUM ELEMENTS	NO CORROSION PROTECTION PLAN REQUIRED. ISOLATION TO BE ENSURED BETWEEN ALL DISSIMILAR METALS.
	STRUCTURAL STEELWORK	SURFACE PROTECTION IN ACCORDANCE WITH TABLE S2 PAINT TO BE MAINTAINED IN ACCORDANCE WITH INSPECTION AND MAINTENANCE PLAN 2mm CORROSION ALLOWANCE TO ALL EXPOSED FACES. ISOLATION TO BE ENSURED BETWEEN DISSIMILAR METALS.

EXISTING STRUCTURE/DEMOLITION NOTES

- D1 THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE STRUCTURE IN A STABLE CONDITION AND ENSURING NO PART SHALL BE OVER STRESSED DURING CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL PROVIDE INDEPENDENT STRUCTURAL CERTIFICATION FOR ALL BUILDERS WORKS NOT DETAILED ON THE DRAWINGS SUCH AS PROPPING, SHORING, TEMPORARY RETAINING ETC.
- D2 ANY DISCREPANCIES DISCOVERED DURING CONSTRUCTION ACTIVITIES BETWEEN THE EXISTING STRUCTURE AND THE CONTRACT DOCUMENTS SHALL BE REPORTED TO THE SUPERINTENDENT FOR FURTHER ADVICE.
- D3 WHEN NEW STRUCTURE IS TO BE FIXED TO EXISTING STRUCTURE, THE SUPER INTENDENT SHALL BE CALLED TO INSPECT THE EXPOSED EXISTING STRUCTURE TO WORKING DAY SHALL BE PROVIDED PRIOR TO ALL INSPECTIONS.
- D24 ALL CONSTRUCTION POUR JOINTS TO BE SCABBLED AND CLEANED UNLESS NOTED OTHERWISE.
- D25 ALL CONCRETE SHALL BE MECHANICALLY VIBRATED.
- D26 CHASES AND PENETRATIONS, OTHER THAN THOSE SHOWN ON THE STRUCTURAL DRAWINGS, SHALL NOT BE MADE IN CONCRETE MEMBERS UNLESS APPROVED BY WGA.
- D27 PROVIDE 75mm MIN. OF LEAN MIX CONCRETE TO BOTTOM OF ALL TRENCHES AND / OR PADS POURED THE SAME DAY WHEN EXCAVATED.



CD: AHD  
FREMANTLE LOW  
WATER MARK 194.9  
REFER FREMANTLE FISHING BOAT HARBOUR SUBMERGENCE CURVE 2016.

DATUM & TIDAL LEVELS

SCALE 1:25

CONCRETE NOTES

- C1 ALL WORKMANSHIP AND MATERIALS SHALL BE IN ACCORDANCE WITH AS3900, THE SPECIFICATION AND THE DRAWINGS.
- C2 FORMWORK SHALL BE TO AUSTRALIAN STANDARD AS3610.
- C3 THE BUILDER SHALL CO-ORDINATE WITH ALL TRADES TO ENSURE THAT PROVISION IS MADE FOR ALL NECESSARY REBATES OR OPENINGS IN CONCRETE, AND CASTING IN OF CONDUITS, WHETHER OR NOT SPECIFICALLY DETAILED ON THE DRAWINGS.
- C4 THE BUILDER SHALL CO-ORDINATE WITH ALL TRADES TO ENSURE THAT PROVISION IS MADE FOR CASTING IN OF ANCHORS FOR FENDERS AND CHAIN BRACKETS AS DETAILED ON SUPPLIER DRAWINGS OR SPECIFICATIONS.
- C5 CLEAR CONCRETE COVER TO REINFORCEMENT FOR CONCRETE ELEMENTS SHALL BE AS PER TABLE C1.
- C6 REQUIRED SURFACE FINISH AND CLASS OF FORMWORK FOR CONCRETE SHALL CONFORM WITH TABLE C2 UNLESS OTHERWISE INSTRUCTED BY WGA.
- C7 SCHEDULE OF CONCRETE PROPERTIES TO BE USED FOR THE PARTICULAR SECTION OF WORK SHALL BE AS FOLLOWS UNLESS OTHERWISE INSTRUCTED. MIX DESIGNS SHALL BE SUBMITTED BY THE SUB-CONTRACTOR TO THE ENGINEER FOR INSPECTION 28 DAYS PRIOR TO POUR. (REFER TABLE C3).
- C8 ALL REINFORCEMENT IN SLABS AND BEAMS SHALL BE SUPPORTED ON INERT CHAIRS TO GIVE THE REQUIRED COVER. SPACING OF THE CHAIRS TO SLAB REINFORCEMENT SHALL NOT EXCEED 80mm IN EACH DIRECTION.
- C9 WGA SHALL BE GIVEN A MINIMUM OF 72 HOURS PRIOR NOTICE FOR ALL REQUIRED INSPECTIONS.
- C10 ALL CONCRETE IN CONTACT WITH GROUND SHALL BE POURED ON A 0.2mm DAMP PROOF MEMBRANE U.N.O.
- C11 ALL CONCRETE SHALL BE CURED FOR A MINIMUM OF 7 DAYS AFTER POURING BY COVERING WITH A PVC MEMBRANE WEIGHED DOWN TO PREVENT AIR FLOW BENEATH IT. IF CURING COMPOUNDS ARE TO BE USED THE CONTRACTOR IS TO SUBMIT DETAILS FOR WGA'S APPROVAL PRIOR TO USE.
- C12 MINIMUM LAPS FOR REINFORCEMENT SHALL BE AS PER TABLE C4.
- C13 ALL BAR CRANKS TO BE NO GREATER THAN 1# IN U.N.O.
- C14 TEMPLATES SHALL BE USED FOR STARTER BARS TO COLUMNS AND H.D. BOLTS. ALL TEMPLATES TO BE SECURED TO FORMWORK.
- C15 ALL REINFORCEMENT SHALL BE MANUFACTURED BY COMPANIES ACCREDITED TO AS6001 WHICH HOLD A VALID CERTIFICATE OF APPROVAL, ISSUED BY THE AUSTRALIAN CERTIFICATION AUTHORITY FOR REINFORCING AND STRUCTURAL STEELS (ACRS). EVIDENCE OF COMPLIANCE MUST BE SUBMITTED PRIOR TO COMMENCEMENT ON SITE. REINFORCEMENT GRADE AS PER TABLE C5.
- C16 PLACING IN SEVERE WEATHER FOR LARGE FLAT AREAS OF CONCRETE, EXCEEDING 15m<sup>2</sup> IN AREA, WHEN:
  - THE AIR TEMPERATURE > 20° AND THE FORECAST WIND SPEED > 40km/hr OR
  - THE AIR TEMPERATURE > 25° AND THE FORECAST WIND SPEED > 25km/hr OR
  - THE AIR TEMPERATURE > 30° AND THE FORECAST WIND SPEED > 15km/hr
 PROVIDE A WORK METHOD STATEMENT FOR APPROVAL WHICH INCLUDES THE PREDICTED EVAPORATION RATE (IN ACCORDANCE WITH THE CEMENT, CONCRETE AND AGGREGATE AUSTRALIA DATASHEET FOR HOT WEATHER CONCRETING) AND METHODOLOGY FOR LIMITING THE EVAPORATION RATE. STATEMENT TO BE PROVIDED TO WGA A MINIMUM OF 14 DAYS BEFORE FIRST CONCRETE POUR. POURING WILL NOT BE PERMITTED WHERE THE PREDICTED EVAPORATION RATE IS GREATER THAN 1.0kg/m<sup>2</sup>/hr.
- C17 SIZES OF CONCRETE ELEMENTS DO NOT INCLUDE THICKNESS OF ANY APPLIED FINISHES.
- C18 FORMS SHALL BE CHAMFERED FOR RE-ENTRANT ANGLES AND FILLETED FOR CORNERS WHERE THESE WILL BE EXPOSED TO VIEW IN THE COMPLETED PROJECT.
- C19 CONSTRUCTION JOINTS, WHERE NOT SHOWN ON THE DRAWINGS, SHALL BE LOCATED TO THE APPROVAL OF WGA.
- C20 BARS SHALL BE DISTRIBUTED OVER THE EXTENT INDICATED U.N.O.
- C21 REINFORCEMENT SPLICES SHALL NOT BE MADE IN POSITIONS OTHER THAN THOSE SHOWN ON THE STRUCTURAL DRAWINGS WITHOUT THE PRIOR APPROVAL OF WGA.
- C22 THE MINIMUM CLEAR SPACING BETWEEN CONDUITS, CABLES, PIPES AND BARS SHALL BE AS REQUIRED BY AS3900 BUT NOT LESS THAN THREE BAR DIAMETERS U.N.O.
- C23 TYPICAL REINFORCEMENT NOTATION: 230#2@200 BF
  - 23 - DENOTES NUMBER OF BARS REQUIRED
  - N - DENOTES GRADE OF REINFORCEMENT
  - 24 - DENOTES BAR DIAMETER IN MILLIMETRES
  - 200 - DENOTES BAR SPACING IN MILLIMETRES
  - BF - ABBREVIATION (REFER TABLE C5 FOR TYPICAL ABBREVIATIONS)
- C24 ALL CONSTRUCTION POUR JOINTS TO BE SCABBLED AND CLEANED UNLESS NOTED OTHERWISE.
- C25 ALL CONCRETE SHALL BE MECHANICALLY VIBRATED.
- C26 CHASES AND PENETRATIONS, OTHER THAN THOSE SHOWN ON THE STRUCTURAL DRAWINGS, SHALL NOT BE MADE IN CONCRETE MEMBERS UNLESS APPROVED BY WGA.
- C27 PROVIDE 75mm MIN. OF LEAN MIX CONCRETE TO BOTTOM OF ALL TRENCHES AND / OR PADS POURED THE SAME DAY WHEN EXCAVATED.

COVER (mm) - (TABLE C1)

LOCATION	TOP	BOTTOM	SIDES
PRECAST DECK PLANK	40	40	40
IN-SITU CONCRETE ABUTMENT WALL	75	75	75

SURFACE FINISH - (TABLE C2)

LOCATION	SURFACE	TYPE OF FINISH	CLASS OF FORMWORK
DECK PLANKS	TOP FACE	TWO TONE SURFACE FINISH ACHIEVED WITH WASHED AGGREGATE USING RETARDANT AND CUTOUTS OR SIMILAR APPROVED TO ACHIEVE LAYOUT AS PER DRAWINGS. 10mm SACRIFICIAL TO BE ADDED TO PRECAST TO ACCOUNT FOR CEMENTITIOUS LOSSES TO ACHIEVE DESIGN COVER AND DEPTH.	1
DECK PLANKS	BOTTOM SIDE FACE	OFF-FORM	2

CONCRETE PROPERTIES - (TABLE C3)

LOCATION	GRADE (MPa)	MAX. AGGREGATE (mm)	SLUMP
PRECAST CONCRETE	S50*	20	80±15
* HANSON IMAGECRETE 'OYSTER CREME' COLOUR & AGG MIX			
* S5 CLASS CONCRETE SHALL COMPLY WITH THE FOLLOWING REQUIREMENTS:			
- MINIMUM CEMENTITIOUS CONTENT OF 400kg/m <sup>3</sup>			
- THE 96 DAY DRYING SHRINKAGE SHALL NOT EXCEED 600 MICROSTRAIN, AS DETERMINED IN ACCORDANCE WITH AS1012-13			
- THE WATER/CEMENT RATIO SHALL NOT EXCEED 0.40			

BAR LAPS - (TABLE C4)

BAR SIZE	LAP
115	800mm
200	1000mm
24	1300mm

REINFORCEMENT - (TABLE C5)

BARS	GRFP GRADE @ MIN. TENSILE
LOGATURES AND TIES	GRFP GRADE @ MIN. TENSILE 630 MPa

REINFORCEMENT NOTATION - (TABLE C6)

ABBREVIATIONS	DENOTES
B, BF	BARS IN BOTTOM FACE
FF	BARS IN FAR FACE
EW	EACH WAY
EF	BARS IN EACH FACE
BF	BARS IN SIDE FACE
NF	BARS IN NEAR FACE
NSCP	NOT SHOWN ON PLAN
T, TF	BARS IN TOP FACE
BB	BOTTOM MOST BOTTOM BARS
TT	TOP MOST TOP BARS
B1	BOTTOM BARS PLACED FIRST
B2	BOTTOM BARS PLACED SECOND
T1	TOP BARS PLACED FIRST
T2	TOP BARS PLACED SECOND
CENT	BARS PLACED CENTRALLY
ARR	ALTERNATE BARS REVERSED
ALT	BARS ALTERNATING
SIAG	BARS STAGGERED
HORIZ	HORIZONTAL BARS
VERT	VERTICAL BARS
MAX	MAXIMUM
MIN	MINIMUM
CTS	AT CENTRES (SPACING)
TYP	TYPICAL

PILING NOTES

- P1 ALL PILES SHALL BE MIN. GRADE 350MPa.
- P2 THE CONTRACTOR SHALL MAINTAIN AND PROVIDE COMPLETE DRIVING RECORDS OF ALL PILE DRIVES IN ACCORDANCE WITH AS2159.
- P3 PILES TO BE LOCATED WITHIN LIMITS SPECIFIED IN TABLE P1 (IN ANY DIRECTION FROM THEIR INTENDED DESIGN POSITION AT CUT OFF LEVEL).
- P4 THE CONTRACTOR SHALL UNDERTAKE AN AS CONSTRUCTED SURVEY, WHICH IDENTIFIES TRUE TOP OF PILE PLAN POSITION. THESE RESULTS SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL.
- P5 MAXIMUM PILE OUT OF VERTICALITY AFTER DRIVING TO BE WITHIN LIMITS SPECIFIED IN TABLE P1.
- P6 PILE CUT OFF LEVEL TOLERANCE IS ± 5 mm.
- P7 ALL PILES TO BE DRIVEN TO THE STIPULATED MINIMUM PENETRATION AND TO DESIGN GEOTECHNICAL CAPACITIES OF NOT LESS THAN THE STIPULATED DESIGN ACTION.
- P8 THE GEOTECHNICAL STRENGTH REDUCTION FACTOR (Q<sub>d</sub>) USED IN DETERMINING THE DESIGN GEOTECHNICAL CAPACITY IN ACCORDANCE WITH AS2159:2009 Ed 5 Q<sub>d</sub> x ULTIMATE GEOTECHNICAL STRENGTH (R<sub>ult</sub>) Q<sub>d</sub> = 0.56
- P9 OUTSIDE FACE OF ALL PILES TO BE CORROSION PROTECTED FROM TOP DOWN TO 2m (MIN) BELOW SEA BED.
- P10 ALL PILE SPLICES TO BE FSWB. REFER STEELWORK NOTES FOR WELDING REQUIREMENTS.
- P11 PDA TESTING TO BE UNDERTAKEN TO VERIFY STRUCTURAL COMPRESSIVE CAPACITY OF 3 PILES OVER JETTY EXTENT. PILES SELECTED AND METHODOLOGY FOR TESTING TO BE REVIEWED BY ENGINEER PRIOR TO INSTALLATION.
- P12 LATERAL LOAD TESTING OF 4 PILES TO BE UNDERTAKEN TO VERIFY LATERAL CAPACITY OF PILES > PILES SELECTED AND METHODOLOGY FOR TESTING TO BE REVIEWED BY SUPERINTENDENT PRIOR TO INSTALLATION. REFER PILING SPECIFICATION WGA200944-SP-MA-0003 FOR FURTHER TESTING DETAILS.
- P13 ALL PILE CAPS TO HAVE MINIMUM 10° INCLINATION FOR DRAINAGE.

PILING TOLERANCES - (TABLE P1)

TYPE	PLAN POSITIONAL TOLERANCE mm (AT CUTOFF LEVEL)	OUT OF VERTICALITY
BOARDWALK PILES	75	1.75
LOADING PLATFORM PILES	75	1.75
GATEHOUSE STRUCTURE PILES	75	1.75
MOORING PILES	150	1.75

CORROSION PROTECTION

- CP1 ALL NEW PILES MUST BE COATED IN ACCORDANCE WITH THE PILE SPECIFICATION AND SLEEVED WITH DENS0 SEASHIELD 100 SERIES (OR SIMILAR APPROVED EQUIVALENT). CONTRACTOR TO ENSURE WATER-TIGHT FINISH ACHIEVED, REFER TO PILING SPECIFICATION FOR FURTHER INFORMATION.
- CP2 SACRIFICIAL ANODES ARE REQUIRED ON SEAWARD FACING PILES ON THE GATEHOUSE STRUCTURE & LOADING PLATFORM ONLY.
- CP3 MAXIMUM PILE OUT OF VERTICALITY AFTER DRIVING TO BE WITHIN LIMITS SPECIFIED IN TABLE P1.
- CP4 ALL BOLTED FIXINGS (OTHER THAN STAINLESS STEEL BOLTS/NON-FERROUS FIXINGS) TO HAVE CORROSION COATING (DENS0-RAMIC OR SIMILAR APPROVED EQUIVALENT) APPLIED TO MANUFACTURER SPECIFICATION ON COMPLETION.
- CP5 RUBBER BEARING STRIPS TO BE PLACED BETWEEN ALL PRE-CAST CONCRETE PANELS AND STEELWORK.
- CP6 BITUMINOUS TAPE OR SIMILAR APPROVED EQUIVALENT TO BE PLACED BETWEEN ALL CONTACTING/CONCEALED STEEL SURFACES.

PRECAST CONCRETE NOTES

- PC1 ALL WORKMANSHIP AND MATERIALS SHALL BE IN ACCORDANCE WITH AUSTRALIAN STANDARD AS3900, AS3610, AS3660 PARTS 1 AND 2, THE SPECIFICATION AND DETAILS PROVIDED ON THE DRAWINGS, UNLESS OTHERWISE INSTRUCTED BY THE SUPERINTENDENT.
- PC2 FORMWORK SHALL BE CLASS 2 TO AS3610 U.N.O. FINISH TO TROWELLED FACE SHALL BE EQUIVALENT TO CLASS 3 FINISH TO AS3610 U.N.O. REFER ALSO TO CIA 248 PRECAST CONCRETE HANDBOOK. USE BROOM FINISH TO TRAFFICABLE SURFACES.
- PC3 PANELS HAVE BEEN DESIGNED FOR THE IN-PLACE CONDITION AND SO THE CONTRACTOR MUST MAKE THEIR OWN ASSESSMENT AS TO ANY EXTRA REINFORCEMENT, LIFTING EYES, ETC. THAT MAY BE REQUIRED TO SUIT THE PROPOSED TRANSPORTATION AND HANDLING METHODS. ANY SUCH EXTRA MATERIAL OR HANDLING REQUIREMENTS MUST BE ALLOWED. ANY SUCH FITTINGS MUST BE CONCEALED UPON COMPLETION AND SUBJECT TO THE APPROVAL OF THE SUPERINTENDENT.
- PC4 CONTRACTOR SHALL PROVIDE ALL NECESSARY INSERTS, TEMPORARY SUPPORTS AND LIFTING GEAR NECESSARY FOR SAFE AND PROPER TRANSPORTATION, ERECTION, LIFTING AND PROPPING.
- PC5 SHOP DETAILS OF ALL PRECAST PANELS ARE TO BE DRAWN AND SUBMITTED TO THE SUPERINTENDENT AND / OR ENGINEER TO ENABLE INSPECTION AND ALTERATIONS (IF NECESSARY) PRIOR TO COMMENCEMENT OF MANUFACTURE. REFER ALSO TO SPECIFICATIONS, ALLOW FOR 7 WORKING DAYS FOR SUPERINTENDENT REVIEW OF SHOP DRAWINGS.
- PC6 ACCEPTANCE OF THE COMPLETED PRECAST PANELS WILL BE UPON DELIVERY TO THE SITE, AND PANELS CRACKED OR DAMAGED IN ANY MANNER SHALL BE REJECTED PRIOR TO ERECTION. LIKEWISE ANY PANELS DAMAGED DURING OR FOLLOWING ERECTION, SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR OR, WHERE APPROVED, SHALL BE REPAIRED IN ACCORDANCE WITH THE SPECIFICATION.
- PC7 PANELS REQUIRING MANDATORY INSPECTIONS BY THE SUPERINTENDENT SHALL BE NOTED ON THESE DRAWINGS OR RETURNED SHOP DRAWINGS. PANELS NOTED SHALL NOT BE POURED UNTIL INSPECTED. MINIMUM NOTICE OF 1 WORKING DAY SHALL BE PROVIDED PRIOR TO ALL INSPECTIONS.
- PC8 THE CONTRACTOR SHALL REFER TO ALL OTHER TRADES TO ENSURE THAT OPENINGS ARE LEFT WHERE NECESSARY AND THAT FIXINGS, FITTINGS AND ASSOCIATED COMPONENTS REQUIRED TO BE INSTALLED ARE PROVIDED AND BUILT IN AS THE WORK PROCEEDS.
- PC9 REFER TO TABLE PC1 FOR PRECAST CONCRETE PROPERTIES. FULL MIX DESIGN TO BE SUBMITTED TO THE SUPERINTENDENT FOR INSPECTION PRIOR TO CONSTRUCTION OF PANELS.
- PC10 REFER TO TABLE PC2 FOR MINIMUM CLEAR COVER TO REINFORCEMENT IN WALL PANELS.
- PC11 METAL FIXINGS, FERRULES, BOLTS, NUTS, WASHERS, LIFTERS, CAST IN ITEMS ETC ARE TO BE 316 STAINLESS STEEL UNLESS OTHERWISE SPECIFIED. USE ONLY GRADE 4.8 OR 8.8 BOLTS AS SPECIFIED. DO NOT WELD, INCLUDING TACK WELD, 8.8 BOLTS OR NUTS UNDER ANY CIRCUMSTANCES.
- PC12 ALL CAST-IN FERRULES AND ALL BRACING INSERTS (FERRULES) ARE TO BE M20 x 95 UNLESS NOTED OTHERWISE AND TO HAVE N x 2 x 400 LONG CROSS BAR THROUGH THE FERRULE SECURELY & ANCHORED BEHIND THE REINFORCING UNLESS OTHERWISE SHOWN. ALL FERRULES ARE TO BE SUPPLIED BY AN APPROVED SUPPLIER AND ALL FERRULES SHALL HAVE TEST DATA CONFIRMING THEIR LOAD CAPACITIES IN ULTIMATE LIMIT AND WORKING LOAD LIMIT (WLL) TO AS3850.
- PC13 ALL PROPOSED GROUT TYPES SHALL BE SUBMITTED TO SUPERINTENDENT FOR REVIEW. GROUT IS TO BE NON-SHRINK WITH A MINIMUM COMPRESSIVE STRENGTH OF 40MPa @ 28 DAYS.
- PC14 MINIMUM CONCRETE CHARACTERISTIC COMPRESSIVE STRENGTH, CONFIRMED BY TESTING, AT TIME OF LIFTING SHALL BE 25MPa.
- PC15 PROVIDE 20mm CHAMFERS TO ALL PANEL EDGES U.N.O.

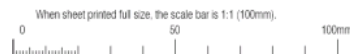
PRECAST CONCRETE PROPERTIES - (TABLE PC1)

REFER TABLE C3

COVER (mm) - (TABLE PC2)

DECKS/PANELS (FRP)	40
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CONSTRUCTION ISSUE	REV	DATE	DESCRIPTION	DRAFT	ENG.	CHKD.
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City of Cockburn  
welcomes to work

WGA  
WALLBRIDGE GILBERT  
AZTEC

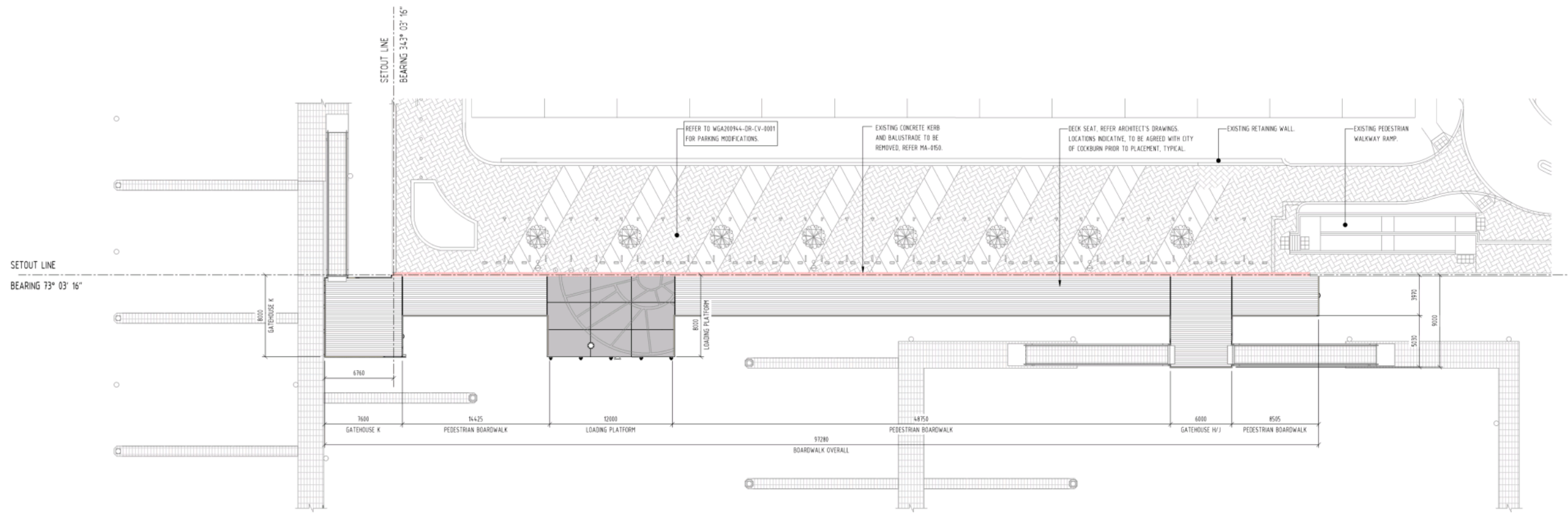
Level 1, 66 Kings Park Road  
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PORT COOGEE MARINA EXPANSION  
MARABOO LOOP,  
NORTH COOGEE WA 6163  
BOARDWALK NOTES - SHEET 1

A1 DOCUMENT NUMBER  
Job Number Sheet No Rev.

Design Drawn  
JB DC WGA200944-DR-MA-0100 0





BOARDWALK GENERAL ARRANGEMENT PLAN  
SCALE 1:200

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CONSTRUCTION ISSUE

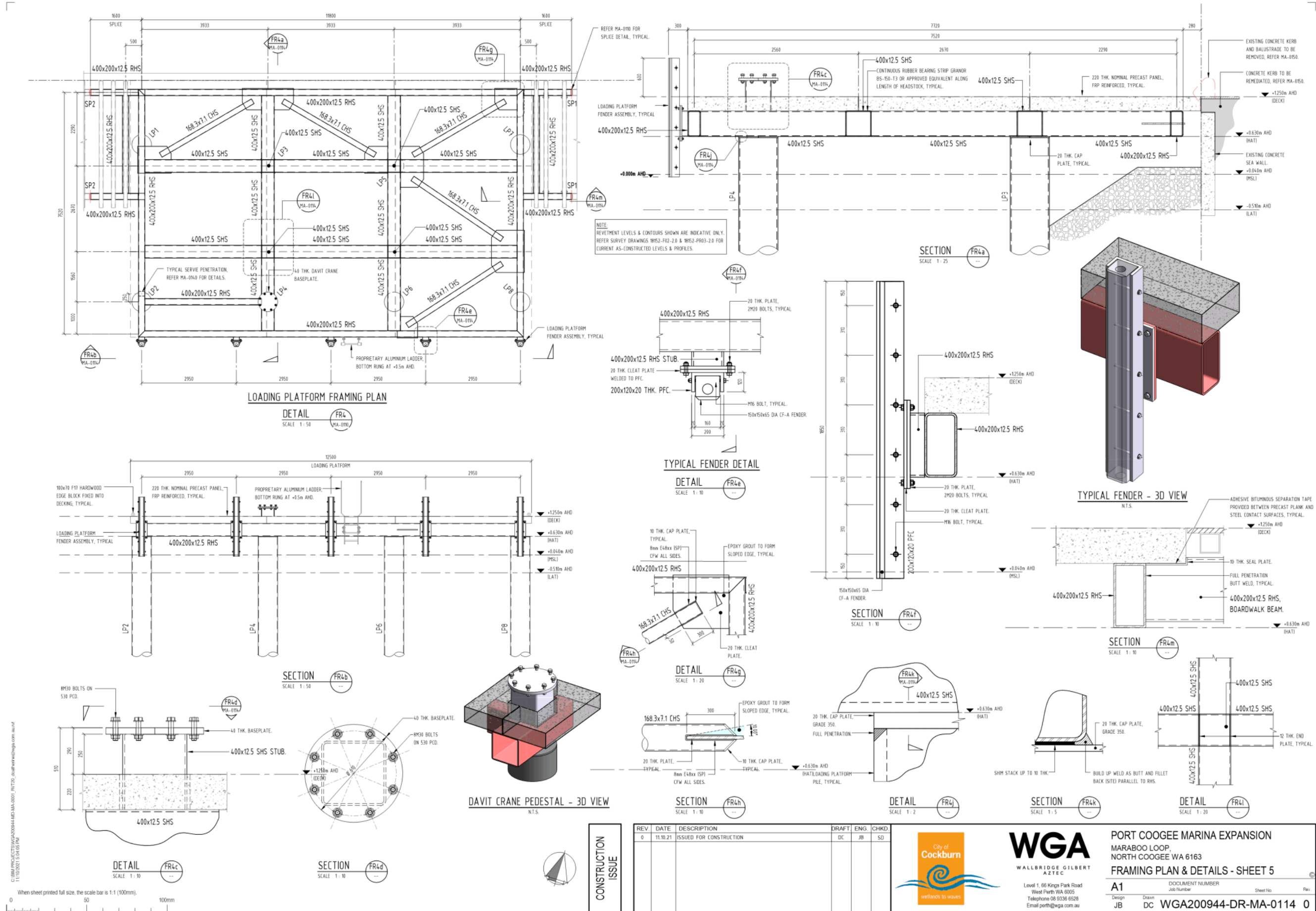
REV.	DATE	DESCRIPTION	DRAFT	ENG.	CHKD.
0	11.10.21	ISSUED FOR CONSTRUCTION	DC	JB	SD



**WGA**  
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PORT COOGEE MARINA EXPANSION  
MARABOO LOOP,  
NORTH COOGEE WA 6163  
**BOARDWALK GENERAL ARRANGEMENT PLAN**

A1 DOCUMENT NUMBER  
Job Number Sheet No. Rev.  
Design Drawn  
JB DC WGA200944-DR-MA-0105 0



REV	DATE	DESCRIPTION	DRAFT	ENG.	CHKD.
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City of  
Cockburn  
welcomes to waves

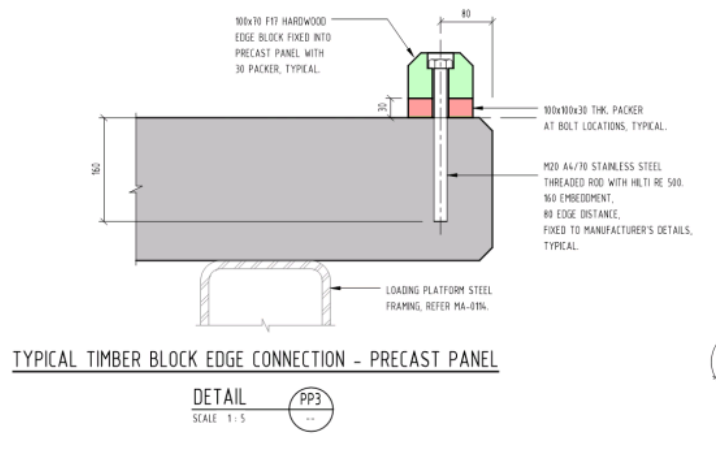
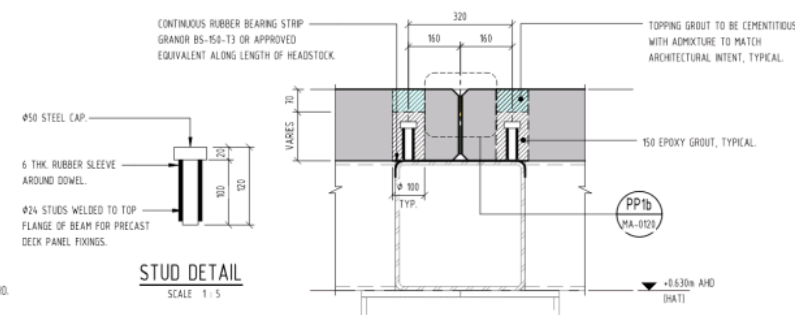
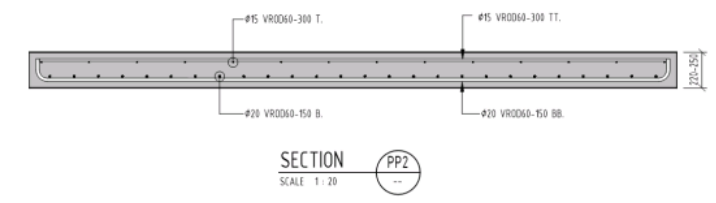
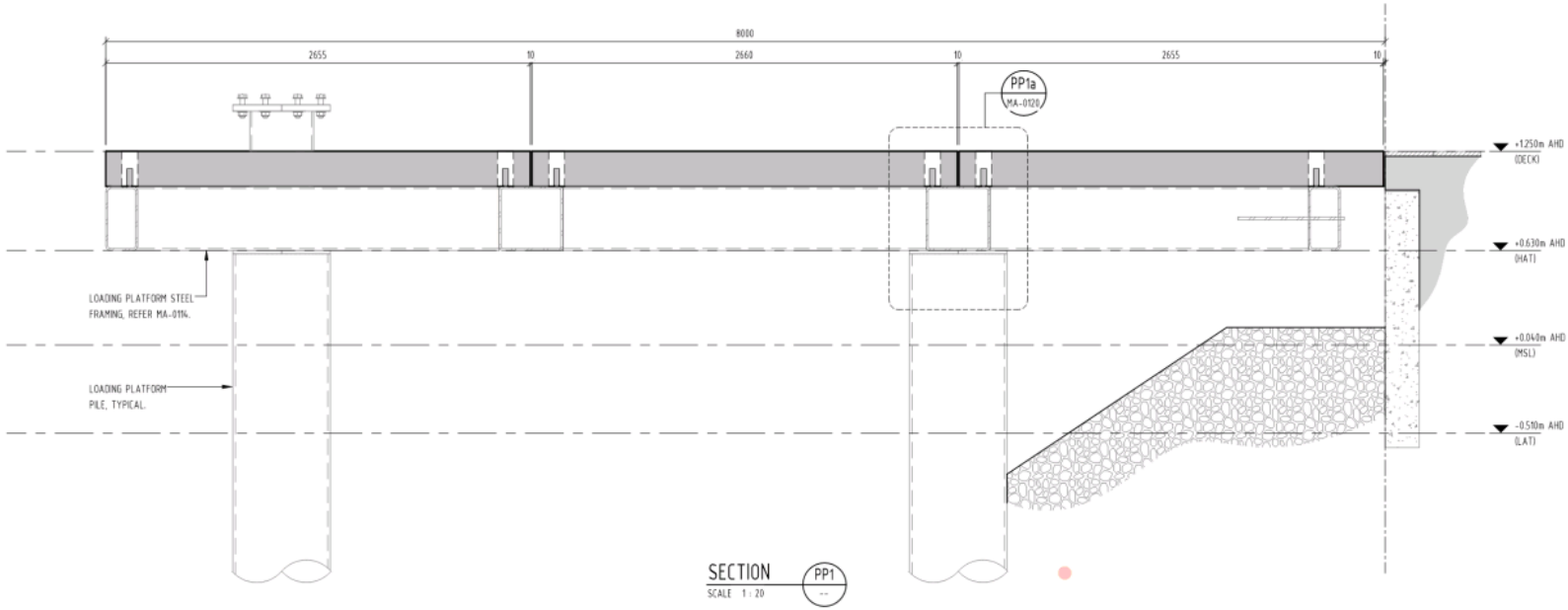
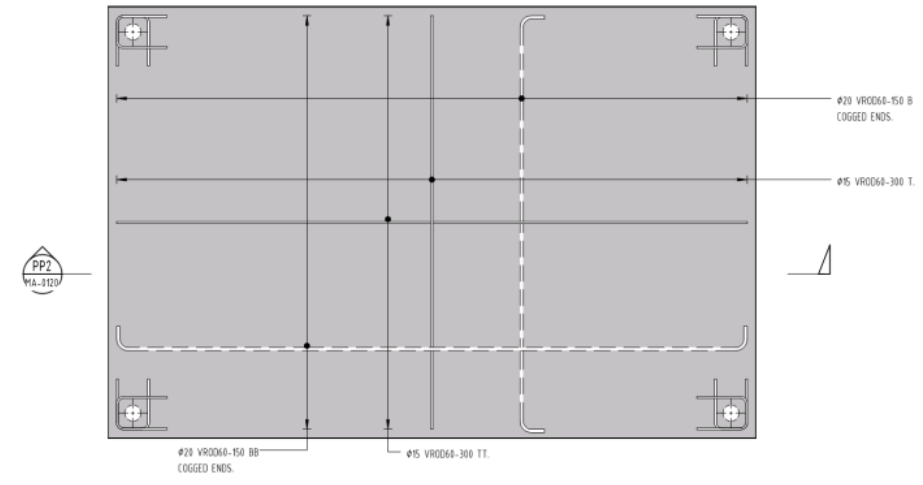
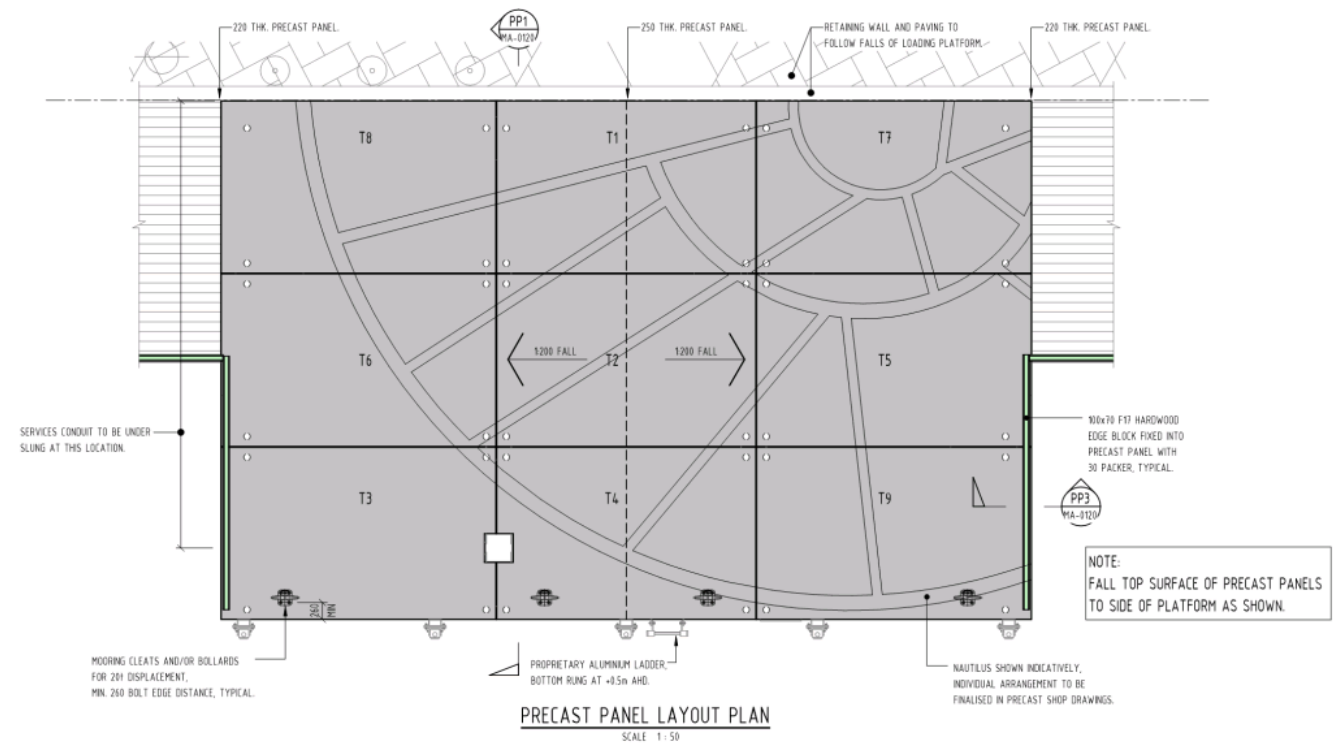
**WGA**  
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**PORT COOGEE MARINA EXPANSION**  
MARABOO LOOP,  
NORTH COOGEE WA 6163  
**FRAMING PLAN & DETAILS - SHEET 5**

**A1** DOCUMENT NUMBER  
Job Number  
Sheet No. Rev.

Design Drawn  
JB DC **WGA200944-DR-MA-0114 0**



NOTE: RELEVANT LEVELS & CONTOURS SHOWN ARE INDICATIVE ONLY. REFER SURVEY DRAWINGS W152-F02-2.0 & W152-P003-2.0 FOR CURRENT AS-CONSTRUCTED LEVELS & PROFILES.

NOTE: CONTRACTOR TO PROVISION PATHWAY FOR SERVICES TO RUN FROM UNDER DECK AND UP CRANE POST IN SHOP DRAWINGS.

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City of  
Cockburn  
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**WGA**  
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**PORT COOGEE MARINA EXPANSION**  
MARABOO LOOP,  
NORTH COOGEE WA 6163  
**PRECAST LAYOUT, SECTIONS AND DETAILS**

**A1** DOCUMENT NUMBER  
Job Number Sheet No Rev.  
Design Draw WGA200944-DR-MA-0120 0  
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CONSTRUCTION ISSUE



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## 14.4 Governance and Strategy

### 14.4.1 Reimbursement of Legal Fees - Cr Dewan

<b>Responsible Executive</b>	Executive Governance and Strategy
<b>Author</b>	Manager Legal and Compliance
<b>Attachments</b>	N/A

#### RECOMMENDATION

That Council:

- (1) REIMBURSES Cr Tarun Dewan for legal fees of \$1,650, in accordance with the Access to Legal Services for Elected Members and Employees Policy.

#### Background

The Executive Governance and Strategy has received a request for reimbursement of legal fees incurred in relation to advice received in accordance with the Access to Legal Services for Elected Members and Employees Policy (the Policy).

The request for reimbursement was received after the advice was received, therefore this matter is presented to Council for decision, as required by clause (3)3. in the Policy.

#### Submission

N/A

#### Report

Cr Dewan has incurred legal fees for services provided in relation to his role as an Elected Member and has requested reimbursement of the fees paid.

All required documents have been provided by Cr Dewan to the Executive Governance and Strategy as required by the Policy.

Cr Dewan had indicated he would be seeking legal advice in relation to a matter permitted under the Policy, and where such approval is sought prior to engagement of the legal service provider, in accordance with the Policy, the Executive Governance and Strategy can approve the request.

Elected Members are required to follow the requirements of the Policy where the request is made in advance of the engagement of the services.

In this instance the formal request for reimbursement has occurred after the services were provided, therefore the Policy requires the matter be presented to Council for approval.

The request is compliant with the Policy and the recommendation is for Council to endorse the reimbursement of the fees paid by Cr Dewan in accordance with the Policy.

### **Strategic Plans/Policy Implications**

#### Listening & Leading

A community focused, sustainable, accountable and progressive organisation.

- Best practice Governance, partnerships and value for money.

### **Budget/Financial Implications**

If approved by Council, the expenditure would be met through the City's existing Municipal budget provisions.

### **Legal Implications**

In accordance with the requirements of s5.123 of the *Local Government Act 1995*, details related to this matter cannot be disclosed.

### **Community Consultation**

N/A

### **Risk Management Implications**

There is a low-level risk associated with this item.

Council have adopted the Access to Legal Services for Elected Members and Employees to guide decision making related to this item.

### **Advice to Proponent(s)/Submitters**

N/A

### **Implications of Section 3.18(3) *Local Government Act 1995***

Nil

**14.4.2 Chief Executive Officer Recruitment**

<b>Responsible Executive</b>	Executive Governance and Strategy
<b>Author(s)</b>	Manager Legal and Compliance
<b>Attachments</b>	N/A

**RECOMMENDATION**

That Council:

- (1) CERTIFIES that Daniel John Simms was employed in the position of Chief Executive Officer of the City of Cockburn in accordance with the Chief Executive Officer Standards for Recruitment, Performance and Termination March 2021; and
- (2) NOTIFIES the Departmental CEO of the above certification.

**TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL**

**Background**

Employment of a Chief Executive Officer (CEO) is a fundamental role of Council.

The CEO is responsible for implementing Council's strategic vision and leading the City's administration.

The recruitment of a CEO must be conducted in accordance with the Chief Executive Officer Standards for Recruitment, Performance and Termination (CEO Standards).

This process seeks to ensure the principles of merit, equity and transparency are applied.

The Council has extended an offer of employment to Daniel John Simms, which has been accepted.

**Submission**

N/A

**Report**

The minimum standard for recruitment and selection will be met if:

1. The Council has identified and agreed to the qualifications and selection criteria necessary to effectively undertake the role and duties of the CEO within that particular local government context.

The Council, through the recruitment process, identified the qualifications and selection criteria to undertake the role and duties of the CEO at the City of Cockburn.

2. The Council has approved, by absolute majority, the Job Description Form (JDF) which clearly outlines the qualifications, selection criteria and responsibilities of the position. The JDF is made available to all applicants.

The Council endorsed the JDF by absolute majority on 14 July 2022. Council endorsed changes to the JDF by absolute majority on 1 December 2022.

3. The local government has established a selection panel to conduct the recruitment and selection process. The panel must include at least one independent person who is not a current elected member, human resources consultant, or staff member of the local government.

The Council established a Selection Panel which comprised of all Elected Members and an Independent Member. Ms Helen Hardcastle was appointed by the Council as the Independent Member of the Selection Panel.

4. The local government attracts applicants through a transparent, open, and competitive process (this is not necessary for vacancies of less than one year). The local government must advertise a vacancy for the position of CEO in the manner prescribed.

The Council appointed Beilby Downing Teal as the Executive recruitment advisor to facilitate the recruitment process in a transparent, open, and competitive manner. State-wide public notice was effected on 18 December 2022.

5. The local government has assessed the knowledge, experience, qualifications, and skills of all applicants against the selection criteria.

The Selection Panel assessed the knowledge, experience, qualifications, and skills of applicants against the selection criteria, with the support of the Executive Recruitment advisor, at various Selection Panel meetings.

6. The local government has verified the recommended applicant's work history, qualifications, referees, and claims made in their job application.

The Executive Recruitment advisor verified the applicant's work history, qualifications, referees, and claims made in their job description, and the Selection Panel were satisfied as to the applicant's suitability for the role based on the above.

7. The appointment is merit-based, with the successful applicant assessed as clearly demonstrating how their knowledge, skills and experience meet the selection criteria.

The Council consider the appointment of the CEO to have been conducted based on merit, with the successful applicant clearly demonstrating how their knowledge, skills and experience meet the selection criteria by their application and in the interview with the Selection Panel.

8. The appointment is made impartially and free from nepotism, bias, or unlawful discrimination.



The Council is satisfied the employment of the CEO meets the principles prescribed in s5.40 of the *Local Government Act 1995* including but not limited to, the appointment was made impartially, free from nepotism, bias, or unlawful discrimination.

9. The Council has endorsed by absolute majority the final appointment.

On 28 March 2023 Council endorsed the final appointment by absolute majority.

10. The Council has approved the employment contract by absolute majority.

On 28 March 2023 Council endorsed the employment contract by absolute majority.

In accordance with the requirements of regulations 18FB of the *Local Government (Administration) Regulations 1996* the Council certifies that the CEO's employment was in accordance with the City's CEO Standards, as demonstrated herein.

### **Strategic Plans/Policy Implications**

#### Listening & Leading

A community focused, sustainable, accountable, and progressive organisation.

- Best practice Governance, partnerships and value for money.
- Employer of choice focusing on equity, innovation and technology.

### **Budget/Financial Implications**

There are no budget implications from the recommendation in this report.

### **Legal Implications**

#### *Local Government (Administration) Regulations 1996*

Regulation 18FB. Certification of compliance with adopted standards for CEO recruitment (Act s. 5.39B(7)).

### **Community Consultation**

N/A

### **Risk Management Implications**

The endorsement of the certification by Council is a statutory requirement.

Council must conduct the CEO recruitment in accordance with the CEO Standards and provide notification to the Departmental CEO of the resolution within 14 days of the resolution being made.

### **Advice to Proponent(s)/Submitters**

N/A

**Implications of Section 3.18(3) *Local Government Act, 1995***

Recruitment of a Chief Executive Officer is to be in accordance with s5.39A, B and C.

## 14.5 Corporate Affairs

### 14.5.1 Cockburn Blue Innovation Hub Lease between City of Cockburn and Hero Pty Ltd – 1 Channel Close, Henderson

<b>Responsible Executive</b>	Executive Corporate Affairs
<b>Author(s)</b>	Manager Business and Economic Development and Executive Governance and Strategy
<b>Attachments</b>	1. Summary of Industry Funding Model and Commitments <b>(Confidential)</b>

#### RECOMMENDATION

That Council:

- (1) DELEGATES authority to the Acting Chief Executive Officer for the following:
  1. Finalise and enter a lease with Hero Pty Ltd, for 1 Channel Close, Henderson, based on the Terms and Conditions outlined in the report; subject to the City finalising founding partner commitments of \$1.35M over three years;
  2. Enter into a commercial lease up to the approved leasing budget amount for a suitable alternate property, should Hero Pty Ltd not finalise a lease agreement with the City;
  3. Finalise and enter into founding partner and sub-licence agreements;
- (2) NOTES the industry-funded investment model proposed for the Cockburn Blue Innovation Hub.

#### Background

At the 8 September 2022 Ordinary Council Meeting, Council endorsed the following:

That Council:

- (1) ENDORSES the continued exploration of options for the development of a Blue Economy-Defence Innovation Hub; and
- (2) GIVES in principle agreement for a financial contribution of up to \$50,000 to the Innovation Hub, the amount to be determined at the next Expenditure review committee meeting.

At the 8 December 2022 Ordinary Council Meeting, Council noted the following:

That Council:

- (1) NOTES progress being made on the development of a Blue Economy-Defence Innovation Hub and no requirement for a further financial contribution at this stage of the project

The Cockburn Blue Innovation Hub will be an advanced research and innovation facility to develop and test new maritime practices and technology and develop local business supply chain capability and capacity.

The Hub aims to support the local economy by:

- Building capacity and capability of local Cockburn businesses within the blue economy
- Increasing the local defence supply chain within Cockburn to enable building sovereign manufacturing capabilities
- Developing advanced technology research opportunities for sustainable marine practices
- Attracting local, national and international investment to Cockburn
- Addressing skills gap shortages for local industry.

The Hub will facilitate up to 40 Cockburn businesses to build capacity and capability by working directly with major maritime organisations, defence prime contractors, universities, and registered training organisations.

### **Submission**

N/A

### **Report**

Since the December Ordinary Council Meeting, there has been significant progress made on the development of the Cockburn Blue Innovation Hub 'The Hub' (previously referred to as the Blue Economy-Defence Innovation Hub) and a significant amount of interest and commitment from the Cockburn business sector.

To progress any further, The Hub requires a suitable physical location.

Research undertaken by the City has identified 1 Channel Close, Henderson as a "fit for purpose" facility to locate The Hub by both City officers and investment partners, noting that there is extremely limited availability of facilities or warehousing in Henderson that would be suitable for a facility such as this.

The City is proposing to lease 1 Channel Close, Henderson for an initial three-year period, with options of an additional two plus five years (3+2+5) for the 10,698 sqm property including Land and Buildings.

Total annual rent of \$700,000 plus outgoings of \$134,950 per annum have been negotiated by the City's Property and Assets team and are considered commercially acceptable.

The City proposes to enter into the lease for the whole facility (total land area of 10,698 sqm including buildings totalling 5,075sqm) commencing July 2023, at which point 38 per cent (4,018sqm) will already be occupied under a sub-lease arrangement until December 2023 (or potentially longer).

From January 2024, it is estimated that for the remainder of the initial three-year tenure, industry funding of The Hub will be achieved by a combination of founding partners and sub-leases.

The Hub will be industry-funded as follows:

- Up to five Founding Partners will contribute a lump sum and in-kind investment to support the overall start-up and operation of The Hub
- Blue Economy SME business sub-licensees will pay a commercial rate for leasing space within The Hub, with the ultimate goal to lease out all lettable space within facility.

Based on negotiations the City has undertaken with both potential founding partners and sub-licensees, non-binding commitments are in discussion or have been reached which would provide the following funding and occupancy from the start of the lease arrangement:

- \$450,000 per annum Founding Partner funding to be sought prior to entering into a lease arrangement
- \$522k per annum sub-licensee income, equating to a 64 per cent building occupancy rate
- Forecasts indicate an occupancy rate from 1 July at 71 per cent increasing to 100 percent over the first twelve months.

It is estimated that this income will more than off-set the rent and other expenses incurred throughout the lease tenure.

### **Strategic Plans/Policy Implications**

#### Local Economy

A sustainable and diverse local economy that attracts increased investment and provides local employment.

- Increased Investment, economic growth and local employment.

#### Environmental Responsibility

A leader in environmental management that enhances and sustainably manages our local natural areas and resources.

- Sustainable resource management including waste, water and energy.

**Budget/Financial Implications**

Financial modelling forecasts demonstrate that over the initial three-year period, The Hub is financially viable, resulting in no additional funding requirements outside nominated budgets for FY24 (refer Confidential Attachment 1).

**Legal Implications**

N/A

**Community Consultation**

Significant stakeholder engagement has been undertaken with State Government and the Cockburn business sector and both support and interest in The Hub is positive and significant.

**Risk Management Implications**

As with any commercial endeavour, there are some associated risks, however, the following factors mitigate the potential risks associated with entering into this lease agreement:

- Lease agreement won't be signed until the City finalises Founding Partner commitments of \$1.35M over the initial three year term to ensure The Hub is financially viable and sustainable
- Sub-licensee already in place for 38 per cent of the facility from day one with the potential for extension
- Seven non-binding sub-licence commitments have been received for The Hub before any expressions of interest have been opened
- Significant interest from industry and business received to date and continuing to be received

- AUKUS announcement and impending Defence Strategic Review release will increase interest in Henderson and The Hub
- Warehouse and office space in Henderson is extremely limited and finding sub-licensees will not be difficult given demand far outweighs supply.

Although non-binding commitments have been established with sub-licensees, these can only be finalised once a lease has been entered into by the City, so there is a component of risk associated with interested parties not following through on the commitments.

There is also a risk around 'doing nothing' and not progressing with the development of The Hub.

If not secured through this lease arrangement, the facility may become unavailable, and it may be difficult to find an appropriate alternative.

Without a physical facility, the significant industry-funding that has been negotiated for the benefit of the Cockburn business sector and local economy may be lost.

#### **Advice to Proponent(s)/Submitters**

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 13 April 2023 Ordinary Council Meeting.

#### **Implications of Section 3.18(3) *Local Government Act 1995***

Nil

## 15. Reports-Standing Committee

### 15.1 Expenditure Review Committee Meeting – 23/03/2023

#### 15.1.1 Budget Amendments for the FY 23 Municipal Budget

**Responsible Executive** A/Chief Financial Officer

**Author** A/Head of Finance

**Attachments** N/A

#### Officer Recommendation/Committee Recommendation

That Council:

(1) AMENDS the FY23 Municipal Budget as detailed and summarised below:

Nature	Budget Surplus Impact \$
Operating Expenditure - Increase	\$52,217
Capital Expenditure - Increase	\$653,500
Proceeds from sale – Increase	-\$32,000
Transfers from Reserves - Increase	-\$633,717
Transfers to Reserves - Increase	\$20,000
<b>Net Budget Surplus – Decrease</b>	<b>-\$60,000</b>

**TO BE CARRIED BY ABSOLUTE MAJORITY OF COUNCIL**

#### Background

The Expenditure Review Committee (ERC) will review amendments to the adopted Municipal Budget before Council amends the budget, as required under the Terms of Reference of the ERC.

#### Submission

N/A

#### Report

A number of budget expenditure amendments have been submitted for consideration of the ERC.



The following expenditure amendments are submitted for consideration and recommendation to the next Ordinary Council Meeting, in this case 13 April 2023:

Description (Service Unit)	Expend. \$	Income \$	Reserves \$	Budget Surplus Impact
New project – Community engagement for clubroom upgrades at Beeliar Reserve and Tempest Park. (funded from CW6284/6285)	+\$5,000		-\$5,000	Nil
Reduce existing project clubrooms upgrade capital work (CW6284/6285)	-\$5,000		+\$5,000	Nil
Final expenditure for lease agreement between the City and Port Catherine Developments (funded from Land Dev. Reserve)	+\$47,217		-\$47,217	Nil
New project – urgent rectification works at 17 Imlah Court. (funded from CW6416).	+\$26,000			-\$26,000
Reduce existing project Men's Shed Improvements (CW6416)	-\$26,000			+\$26,000
New project – to relocate toy library and fit out (funded from CW6401)	+\$45,000			-\$45,000
Existing project Jeans Willis Centre pathways redesign (CW6401)	-\$45,000			+\$45,000
New project – Operations depot dangerous goods and fire safety audit works. (funded from CW6478)	+\$23,000			-\$23,000
Reduce paving replacement and kerbing upgrade (CW6478)	-\$23,000			+\$23,000
New project – Playground soft-fall renewal program	+\$60,000			-\$60,000
New project – Henderson Redevelopment Stage 2. Funded from Waste & Recycling Reserve	+\$552,500		-\$552,500	Nil
Vehicle replacement b/fwd for Mayor's vehicle (PL542). (funded from Plant Reserve & trade-in)	\$46,000	-\$32,000	-\$14,000	Nil
Coolbellup Financial Counselling surplus (GL350) from restricted grants reserve			-\$20,000	
Coolbellup Financial Counselling (GL350) to Long Service Leave reserve			+\$20,000	Nil

Description (Service Unit)	Expend. \$	Income \$	Reserves \$	Budget Surplus Impact
<b>Total</b>	<b>\$705,717</b>	<b>-\$32,000</b>	<b>-\$613,717</b>	<b>-\$60,000</b>

### Strategic Plans/Policy Implications

#### Listening & Leading

A community focused, sustainable, accountable and progressive organisation.

- Best practice Governance, partnerships and value for money.

### Budget/Financial Implications

The impact on the City's budget surplus from the budget amendments recommended in this report will be a net decrease of \$60,000.

The current net budget surplus is showing \$297,954, as reported in the January 2023 Financial Report (adopted at March OCM).

An Absolute Majority of Council will be required to amend the FY23 Municipal budget.

### Legal Implications

N/A

### Community Consultation

N/A

### Risk Management Implications

The budget amendments required are for the good governance of the City.

There is a low-level risk impact to the City's operational and budget performance should Council not approve the budget amendments contained in this report.

### Advice to Proponent(s)/Submitters

N/A

### Implications of Section 3.18(3) *Local Government Act 1995*

Nil

**15.1.2 Opex Funding for Street Tree Audit Outcomes**

<b>Responsible Executive</b>	Chief of Operations
<b>Author</b>	Head of Operations and Maintenance
<b>Attachments</b>	N/A

**Officer Recommendation/Committee Recommendation**

That Council:

- (1) AMENDS the FY23 Municipal Budget by increasing the contract operating expenditure for street trees management by \$213,000 (funded from the City's budget surplus).

**TO BE CARRIED BY ABSOLUTE MAJORITY OF COUNCIL**

**Background**

The Parks, Environment and Streetscapes Service Unit engage an Arboriculturist to complete an audit on the City's Street trees every five years.

The audit ensures the team can strategically manage these living assets in accordance with the Urban Forest Plan and sound arboriculture practices.

The report details work in order of priority to mitigate risks associate with the environment the trees are located.

**Submission**

N/A

**Report**

A street tree audit was recently completed in September 2022 which identified 63,544 street trees within the City.

This is an increase of approximately 25,000 trees from the last audit in 2018.

The significant increase is related to the collection of trees within the streetscapes, median islands, annual street tree plantings and new subdivisions.

The audit collects information to ensure a proactive management approach to these significant assets and investment of the City's funds.

Key attribute data is collected through a visual inspection along with referencing through previous data if available.

Data collected includes:

- Location
- Species
- Canopy size
- Height
- Photographic appearance
- Valuation
- Health
- Condition.

The data is used to calculate the following information pertaining to the City's urban forest:

- Tree population
- Species diversity
- Tree health target
- Future street tree planting opportunities
- Condition Ratings

As detailed in the table below the City, currently has a street tree population with a 65% rating of good.

With the Urban Forest Plan setting a target of 70% of trees in the City's streets, parks and public open space to be rated as in 'Good' condition there is still a substantial amount of investment required in our trees.

Street Trees Audited	Condition Rating Dead	Condition Rating Poor	Condition Rating Fair	Condition Rating Good	Condition Rating % of Good
63,586	1,358	2,337	18,797	41,094	65%

In addition to the condition rating of the City's urban canopy, the audit has identified the following tree maintenance requirements based on a risk profile with the following outcomes:

- 1,208 trees at high-risk of failure
- 4,967 trees at medium-risk of failure.

To mitigate the risks associated with these trees additional funding to this year's allocation is required.

The table below outlines the financial impact and the level of funding required to complete the works based on the level of risk.

<b>High Risk Trees</b>				
Work Type	Various Prune Types	Powerline Prune	Tree Removal	Total
Quantity of trees	797	274	137	1,208
Contract rate per tree		\$52	\$643	
Cost	\$109,495	\$14,467	\$88,146	<b>\$212,108</b>
<b>Medium Risk Trees</b>				
Work Type	Various Prune Types	Powerline Prune	Tree Removal	Total
Quantity of trees	3,533	751	683	4,967
Contract rate per tree		\$52	\$643	
Cost	\$399,288	\$39,653	\$439,442	<b>\$878,383</b>
			<b>Total</b>	<b>\$1,090,491</b>

It is proposed to increase the FY23 Street Tree Budget by \$213,000 to complete the works associated with the high-risk trees.

The City’s tree pruning contractor has been advised of these works and confirmed availability of resources to complete these works subject to Council endorsement.

The value of the medium-risk works will be included in the draft FY24 Street Tree budget and completed through the first six months.

**Strategic Plans/Policy Implications**

Environmental Responsibility

A leader in environmental management that enhances and sustainably manages our local natural areas and resources.

- Protection and enhancement of our natural areas, bushland, parks and open spaces.

**Budget/Financial Implications**

The FY23 Street Trees budget includes an allocation of \$776,230 for contract tree management services.

This will increase by \$213,000 to \$989,230 and the City’s budgeted surplus (currently \$297,954) will be reduced by \$213,000

Another \$900,000 will be included in the draft FY24 Municipal budget to complete the recommended works.

An Absolute Majority of Council will be required to amend the FY23 Municipal budget.

**Legal Implications**

N/A

**Community Consultation**

N/A

**Risk Management Implications**

The street tree audit process is integral to the management of the City's urban forest canopy and a key function in prioritising tree maintenance recommendations to mitigate our financial risks.

The audit seeks to mitigate the City's liability as a result of street tree limbs which are in poor condition and have the potential to cause damage to property and persons.

**Advice to Proponent(s)/Submitters**

N/A

**Implications of Section 3.18(3) *Local Government Act 1995***

Nil

### 15.1.3 Establishment of Business Fibre Zone - Cockburn Central Business District

<b>Responsible Executive</b>	Executive Corporate Affairs
<b>Author</b>	Economic Development Officer
<b>Attachments</b>	1. NBN Proposal for co-funding additional Business Fibre Zone for Cockburn Central <a href="#">↓</a>

#### Officer Recommendation/Committee Recommendation

That Council:

- (1) NOTES the options proposed by NBNCO for a Business Fibre Zone in Cockburn Central;
- (2) APPROVES the establishment of the recommended Business Fibre Zone (Option 2) for the Cockburn Central area, including publication of the zone on the NBN website; and
- (3) AMENDS the FY23 Municipal Budget by including the cost for Option 2 of \$77,000 (funded from the City's budget surplus).

**TO BE CARRIED BY ABSOLUTE MAJORITY OF COUNCIL**

#### Background

At the 8 September 2022 Ordinary Council Meeting, Council recommended:

The City engage with NBNCO to investigate the creation of a business fibre zone covering the precinct of Cockburn Central, covering the suburbs of Cockburn Central, Success, Treeby, Jandakot, and Atwell, for the purpose of ensuring that businesses in the area have access to the highest level of business grade fibre.

NBN has designated 321 BFZ locations across Australia, with 30 in WA. We currently have three business fibre areas in Cockburn, covering Henderson and the AMC, Bibra Lake, and the Canning Vale area covering Jandakot Airport Commercial Park, this leaves Cockburn Central as one of the few areas in Cockburn, and in fact the greater Perth metro area, where there is a significant business presence not covered by business grade fibre.

By engaging with the NBNCO the City could start the process of rectifying this, giving a boost to the local businesses in the area, and accelerating the development of Cockburn Central.

A Business Fibre Zone (BFZ) is a designated area where NBN will remove the legacy copper wiring between the node and the premises and replace with optic fibre for any business which requests it.

This service is referred to as Enterprise Ethernet which is end-to-end optic fibre.

Business premises within these areas which request this service have no upfront build cost for the installation.

### Submission

Following discussions with NBN in mid to late 2022 the City made a submission to NBN Commercial Team requesting Cockburn Central Business District be designated as a BFZ.

NBN has responded to the City's request offering to designate Cockburn Central as a BFZ with the City contributing to the cost. (See Table below):

Option 1 is the area identified by the City of Cockburn in its submission to NBN. Option 2 is the larger area identified by NBN as a second option providing increased cover.

	Approximate Area (km <sup>2</sup> )	Estimated current number of businesses*	Cost to City of Cockburn
Option 1	4.9	1,673	\$36,000
Option 2	8.4	3,139	\$77,000

\*Business count generated from ABR data (April 2021) in REMPLAN

### Report

A BFZ is sought for the Cockburn Central Business District to service the significant concentration of commercial activity in this precinct and to facilitate and support the planned investment in a range of infrastructure and industry.

Cockburn Central Town Centre has evolved from the small district level activity centre in the early 2000's to a vibrant mixed use activity centre today which aspires to be a Strategic Centre of Perth Metropolitan Region.

Cockburn Central is a multipurpose centre that provides a diversity of uses.

These include the full range of economic and community services to an estimated population catchment of 280,000 in health, community, retail, commercial and social services.

It is integrated with the principal arterial road network of southern Perth, all modes of public transport and freight logistics.

The Perron Group has committed to a substantial expansion of Cockburn Gateway Shopping City valued at \$1.06B.

Stage1A of the development includes a 21,578sqm shopping centre expansion and surrounding retail and commercial tenancies.



In entertainment and night-time economy there is an expansion to Cockburn ARC and the establishment of the Surf Park.

The principles for development at Cockburn Central Town Centre are a coordinated and integrated approach to planning transport, retail, service, business, industrial, residential and recreational land uses that will magnify the success of this Regional Centre providing support to the complimentary business services which the surrounding hinterland demands and providing opportunities for business agglomeration.

The City needs to attract investment in Cockburn Central and digital connectivity is an essential element of that attraction.

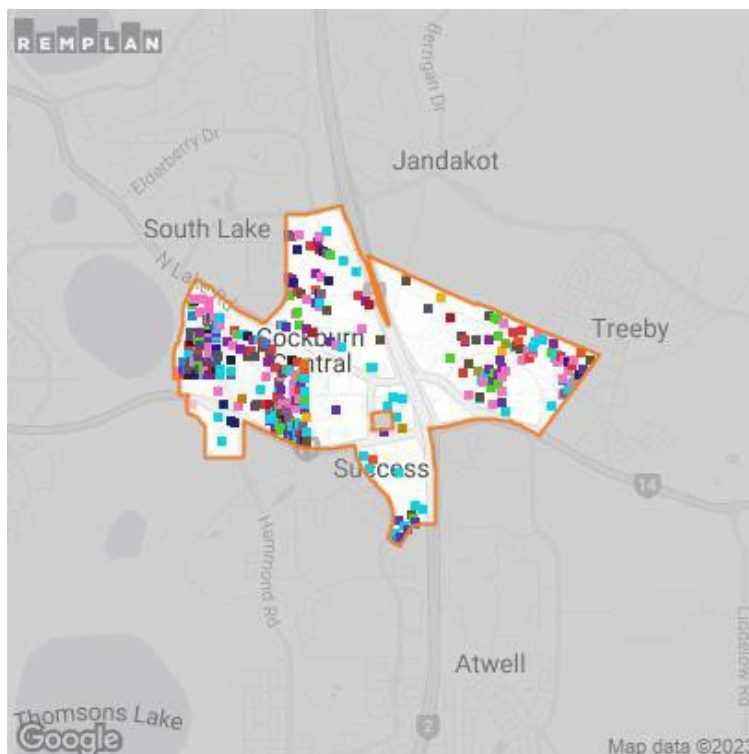
A BFZ is an initiative for the future which provides some insurance that Cockburn Central continues to be an inviting place to live, work, visit and invest now and into the future.

### **Option 1**

This Option was the City's desired area in the submission presented to NBN.

As shown in the map below, this area focussed exclusively on Cockburn Central Business District and excluded the surrounding sub-precincts and suburbs.

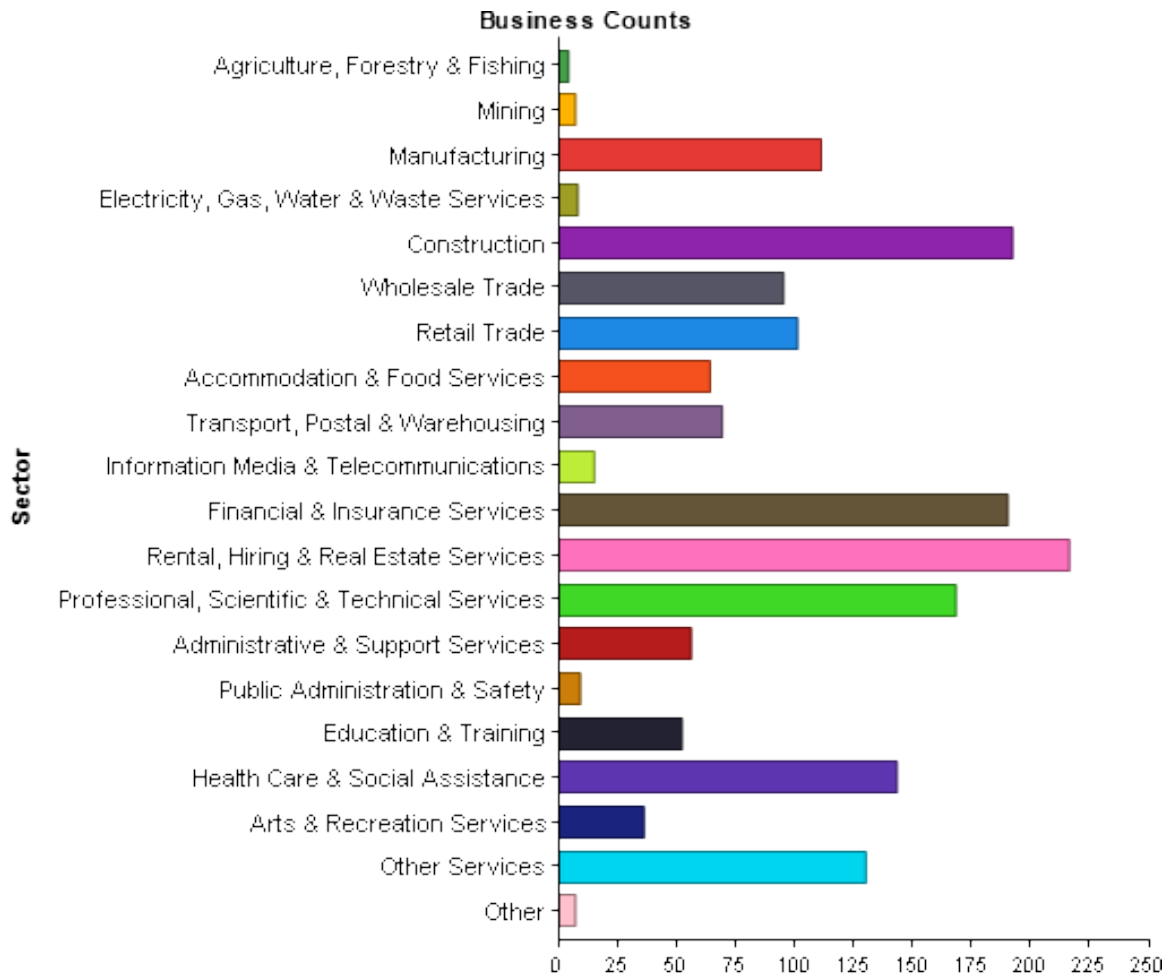
This area equals 4.9km<sup>2</sup> approx.



Option One - 1,673 businesses (estimated) in 20 separate industries sectors are captured in this area

The dots represent the estimated 1,673 individual businesses operating in this area and are coloured according to their industry sector.

The graph below presents the industry sectors and the number of businesses in each sector.

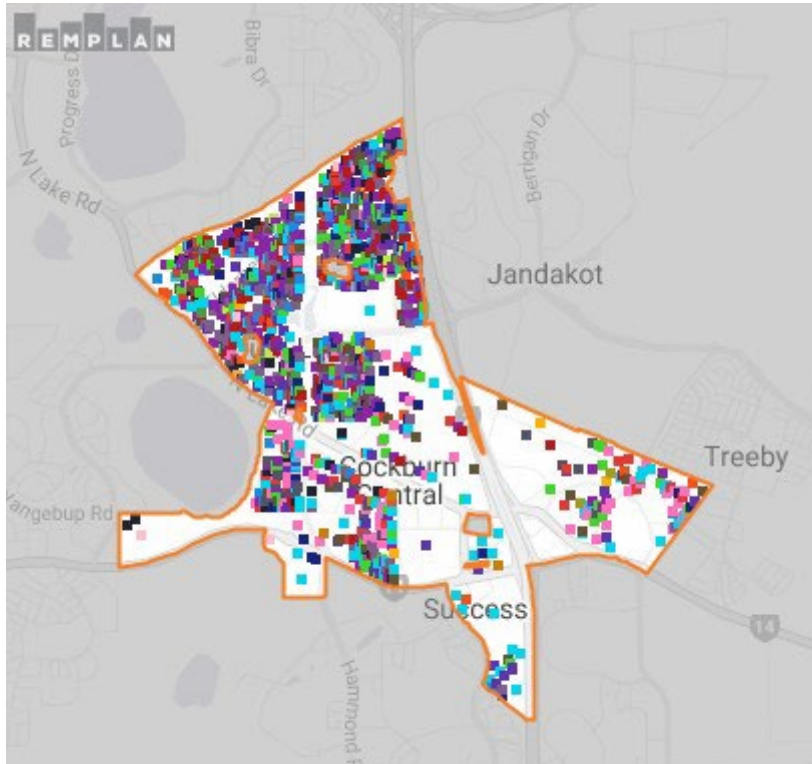


Option One - 20 Industry sectors and the number of businesses in each

### Option Two

This area covers 8.4km<sup>2</sup> and embraces an estimated 3,139 individual businesses.

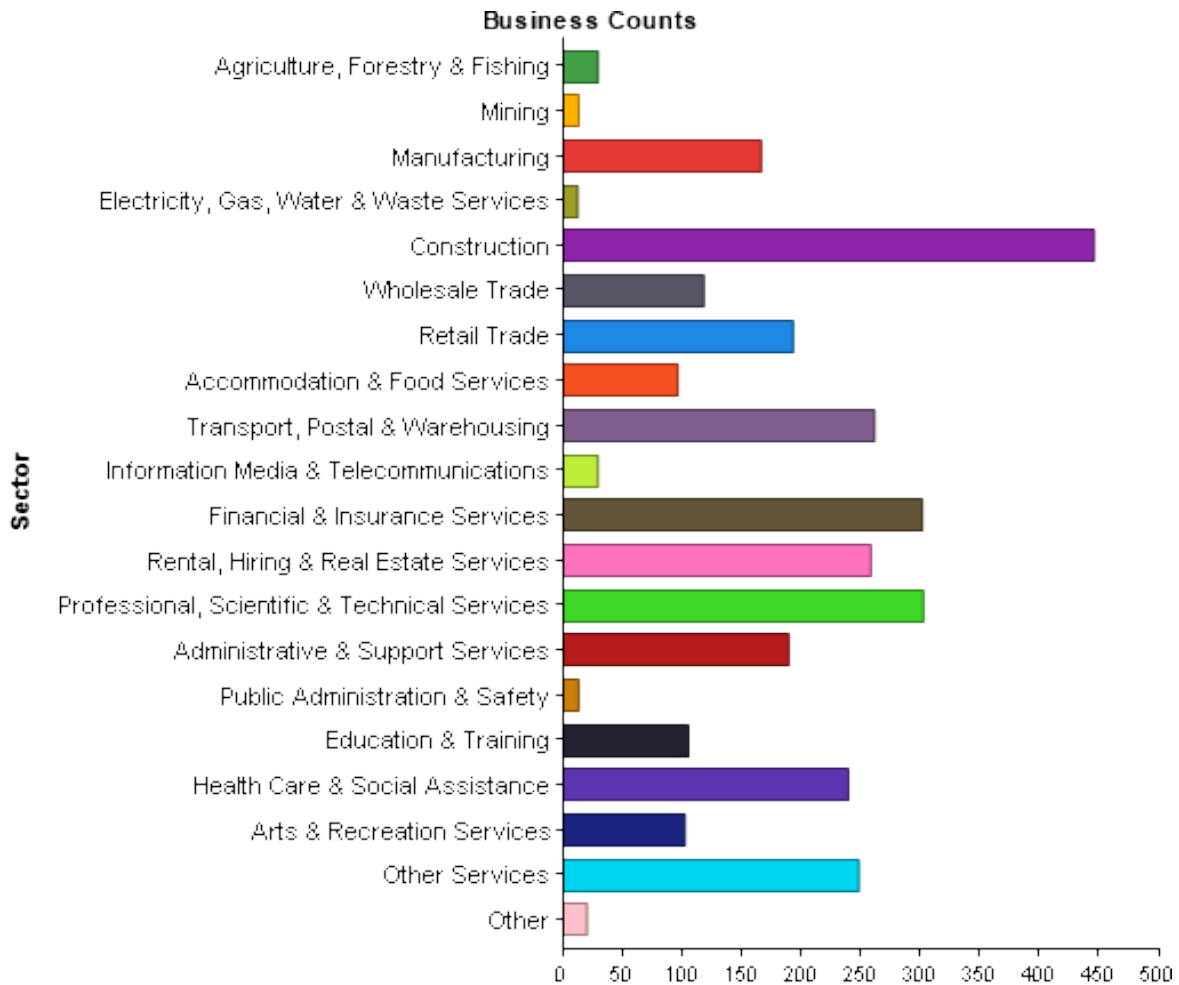
The map and graph below illustrate the spread of these businesses and the number in each industry sector.



**Option Two - 3,139 businesses (estimated) in 20 separate industries sectors are captured in this area.**

It should be noted this area was defined by NBN. It joins the Bibra Lake BFZ.

While obviously offering the subsidised Enterprise Ethernet to a larger number of businesses, it avoids a potential issue where the additional businesses sandwiched between BFZ's would narrowly miss out on accessing this very valuable business asset if option one was implemented.



**Option Two - 20 Industry sectors and the number of businesses in each**

## Strategic Plans/Policy Implications

### Local Economy

A sustainable and diverse local economy that attracts increased investment and provides local employment.

- Increased Investment, economic growth and local employment.
- Thriving local commercial centres, local businesses and tourism industry.
- A City that is 'easy to do business with'.

### Community, Lifestyle & Security

A vibrant healthy, safe, inclusive and connected community.

- Accessible and inclusive community, recreation and cultural services and facilities that enrich our community.

### City Growth & Moving Around

A growing City that is easy to move around and provides great places to live.

- Cockburn Central as the capital of Perth's South Metro Region.

**Budget/Financial Implications**

Option Two is recommended.

The City will incur a one-off charge of \$77,000 (ex GST)

This charge was not budgeted as the proposal was not identified until this year. It is requested the funding be sourced from the City's Municipal budget surplus.

**Legal Implications**

There are no legal implications identified.

**Community Consultation**

No community consultation is required.

**Risk Management Implications**

There are no implicit risks associated with this proposal, save for that of a lost opportunity for Cockburn Central generally and the local economy as a whole.

The competitive nature for investment and development means the City has a very important role to implement the service for the largest area possible.

The standard of digital connectivity is as critical to the viability of every business as is the supply of water and electricity.

Recommendation to adopt Option Two as defined by NBN.

**Advice to Proponent(s)/Submitters**

N/A

**Implications of Section 3.18(3) *Local Government Act, 1995***

Nil



nbn-Confidential: Commercial

Friday, 3 February 2023

Mr. Michael Faulkner  
Manager Business and Economic Development  
City Of Cockburn, WA. 6164

### **nbn™ Business Fibre Zone Partnership – Quote for Cockburn Central**

Dear Michael,

Thank you for requesting an estimate to partner with **nbn** to create a new **nbn™ Business Fibre Zone**. We are pleased to provide the indicative information below, based on our understanding of your requirements.

This program enables state and local governments to partner with **nbn** to further expand **nbn™ Enterprise Ethernet** into regional and remote Australia at no upfront build cost to providers. Business premises within these areas will also have access to **nbn** premium-grade business products at CBD zoned pricing. Business **nbn™ Enterprise Ethernet** is a product designed for providers serving end users who need high levels of speed, performance and reliability when moving their vital data.

To prepare business fibre zones areas, **nbn** has used publicly available ABS population areas. The areas align to Mesh Blocks – historically contained 30-60 persons

We have reviewed your request and prepared a quote of the anticipated costs. The quote comprises the Councils contribution and the number of businesses that will benefit from the partnership.

The information set out in this letter is indicative only, non-binding and may change over time, including as we work together to formalise your requirements. If your requirements change or **more than 60 business days** has passed since the date of this letter, **nbn** will reissue a revised letter which may contain revised costs.

#### **Proposed Boundary Overview**

The boundary coverage of the area you have identified is shown in Appendix A. Based on the information you have provided and the business demographics within this area, an indicative quote to create a Business Fibre Zone is outlined below.

Level 13, 100 Mount Street, North Sydney NSW 2060  
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Phone (02) 9926 1900  
Web nbn.com.au

Fax (02) 9926 1901

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## Co-investment quote

Project ID: [Cockburn Central]	
Creating new BFZ or Expanding existing BFZ?	New
Number of Mesh Blocks for co-investment	To be determined. Initial review of area presented; requires clarification of boundaries.
Total number of Mesh Blocks in proposed BFZ	Option 1 – 49 meshblocks Option 2 – 122 meshblocks
Estimated number of businesses in BFZ after co-investment	Option 1 – 917 business Option 2 – 1222 meshblocks
Estimated government contribution	Option 1 – \$36,000 + GST Option 2 – \$77,000 + GST

### Next steps:

1. Please review this Business Fibre Zone Quote and ensure details match your requirement, including that the desired scope area aligns to the boundary in Appendix A.
2. If you wish to modify any details of this Quote, please contact Corinne Hawke in the first instance at [corinnehawke@nbnco.com.au](mailto:corinnehawke@nbnco.com.au)
3. If you wish to proceed with this quote to contract, please advise your **nbn** representative, Corinne Hawke.
4. Once you accept the final quote **nbn** will issue a contract.
5. After the contract has been entered into by both parties. **nbn** will issue an invoice for the contribution amount required.
6. Following payment, **nbn** will adjust its systems to ensure that agreed premises within the defined boundaries are guaranteed a \$0 build cost and provided with CBD zoned pricing when contracting for an Enterprise Ethernet service with their chosen RSP.
7. **nbn** will undertake any required network build once an RSP places an order for Enterprise Ethernet through **nbn**'s portal.

If any further information is required or you wish to modify any details of this quote, please contact your **nbn** representative.

Yours sincerely,

### Naxin McTeigue

General Manager, Commercial and Contract Management  
Customer, Products and Marketing  
**NBN Co Limited**

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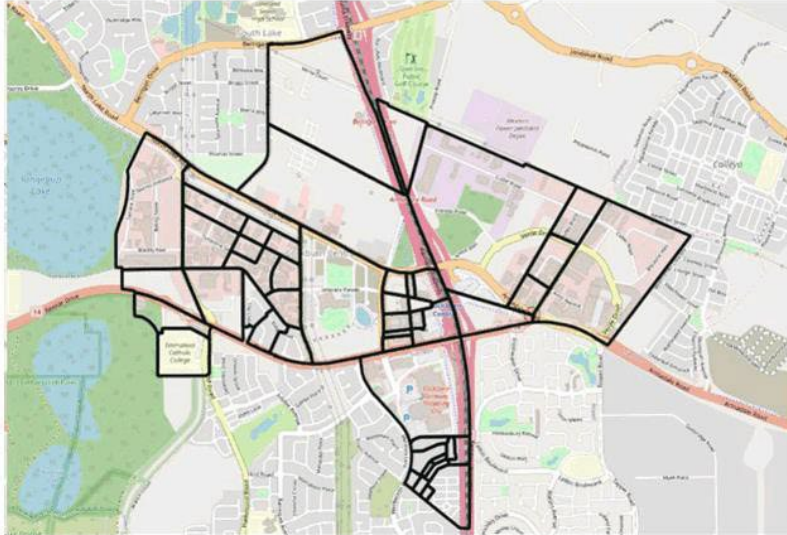
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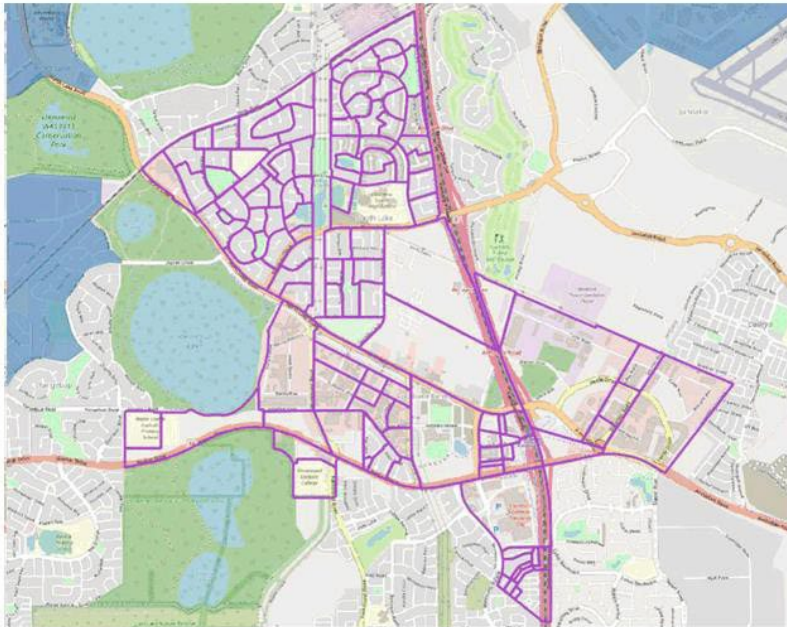
Appendix A – Business Fibre Zone Area Coverage

Street level view

Option 1 – Council nominated area – Cockburn Central Shopping / Industrial Precinct



Option 2 – Expanded north to include South Lakes



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Please note:

- Carriage services (**nbn**<sup>™</sup> Ethernet and **nbn**<sup>™</sup> Enterprise Ethernet) will continue to be ordered separately by residents within the proposed boundary, via their chosen Retail Service Provider.
- **nbn** has assumed the locations within the desired zone are either Premises (as per the meaning in **nbn**'s WBA) or are within the category of other sites that **nbn** has determined are capable of receiving **nbn** services ("Eligible Premises"). If any locations in this letter are not in fact Eligible Premises, then **nbn** may exclude these locations from any revised **nbn** solution.
- As part of the Business Fibre Zone initiative, **nbn** will retain 100% ownership of the infrastructure asset and as such, will be responsible to maintain the asset and be the sole recipient of any revenues derived from the asset.

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## 16. Committee Minutes

### 16.1 Expenditure Review Committee Meeting – 23/03/2023

#### Recommendation

That Council RECEIVES the Minutes of the 23 March 2023 Expenditure Review Committee Meeting.

## 17. Motions of Which Previous Notice Has Been Given

Nil

## 18. Notices Of Motion Given At The Meeting For Consideration At Next Meeting

## 19. New Business of an Urgent Nature Introduced by Members or Officers

## 20. Matters to be Noted for Investigation, Without Debate

### Cr Eva submitted the following Matter for Investigation on 22 March 2023:

Investigate the construction of on-street carparking embayments and footpaths on the southern side of Parkway Drive, between Bibra Drive and the existing school car park.

#### Reason

Removing the existing soil and replacing it with hard stand and bitumen would enable easier parking at the school.

It can also allow for overflow parking at the Skate Park the City of Cockburn's events at the Skate Park has demonstrated that there is inadequate parking. This could alleviate the issue by creating another option.

## 21. Confidential Business

Nil

## 22. Resolution of Compliance

### **RECOMMENDATION**

That Council is satisfied that resolutions carried at this Meeting and applicable to items concerning Council provided services and facilities, are:-

- (1) integrated and co-ordinated, so far as practicable, with any provided by the Commonwealth, the State or any public body;
- (2) not duplicated, to an extent Council considers inappropriate, services or facilities as provided by the Commonwealth, the State or any other body or person, whether public or private; and
- (3) managed efficiently and effectively.

## 23. Closure of Meeting