

Ordinary Council Meeting - 8 November 2024

Item 15.2.2

Policy Review – General Review of Remaining Local Planning Policies

Attachments

1. Local Planning Policy 1.2 - Residential Design Guidelines
2. Local Planning Policy 1.6 - Lodging Houses
3. Local Planning Policy 1.12 - Noise Attenuation
4. Local Planning Policy 1.14 - Waste Management
5. Local Planning Policy 1.15 - Tourist Accommodation
6. Local Planning Policy 1.17 - Non-Residential Uses in Residential Zones
7. Local Planning Policy 3.7 - Signs and Advertising
8. Local Planning Policy 4.2 - Cockburn Central-North (Muriel Court) Structure Plan Design Guidelines
9. Local Planning Policy 4.3 - Newmarket Precinct Design Guidelines
10. Local Planning Policy 4.4 - Heritage Conservation Design Guidelines
11. Local Planning Policy 4.5 - Naval Base Holiday Park Heritage Area
12. Local Planning Policy 4.6 - Cockburn Coast Design Guidelines for Robb-Jetty and Emplacement Precincts
13. Local Planning Policy 4.7 - Phoenix Activity Centre Design Guidelines
14. Local Planning Policy 5.5 - Local Development Plans
15. Local Planning Policy 5.6 - Vehicle Access
16. Local Planning Policy 5.8 - Sea Containers
17. Local Planning Policy 5.18 - Subdivision and Development - Street Trees

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

Policy Type

Local Planning Policy

Policy Purpose

The policy aims to ensure that the 10 design principles of State Planning Policy 7.0 'Design of the Built Environment' are implemented with regard to medium density development. Specifically the policy aims to ensure that development contributes to:

- Local context and identified neighbourhood character;
- Diversity and quality of the City's housing stock, providing sustainable, safe, functional, comfortable homes;
- Tree canopy cover and minimisation of the heat Island effect;
- A safe and comfortable pedestrian and cycling environment.

Town Planning Scheme No.3 (TPS 3) (Clause 4.4.4) provides for the application of this policy.

Policy Statement

- (1) Attachment 1 contains the Residential Design Guidelines.
- (2) This policy applies to all grouped dwellings.
- (3) This policy applies to multiple dwellings (areas coded less than R40).
- (4) This policy applies to single houses on lots with a frontage less than 10.5m wide; single houses on lots less than 260m².
- (5) The following sections of the policy apply to all dwellings (where applicable):
 - i) split coded lots (Clause 9.1 & 9.2)
 - ii) retained dwellings (Clause 10.3)
 - iii) garage widths (Clause 10.4)
 - iv) fencing (Clause 10.5)
- (6) This policy does not apply to land which is subject to a Local Development Plan (LDP) adopted under TPS 3.
- (7) Intended neighbourhood character is defined for the purposes of applying this policy into three categories as follows, defined in Appendix 1 of the Residential Design Guidelines (Attachment 1):
 - *'Garden Neighbourhood Character'*
 - *'New Garden Neighbourhood Character'*
 - *'Urban Character'*

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

- (8) This policy does not exempt compliance with all other requirements of TPS 3, the R-Codes or other relevant City of Cockburn Policies and/or the Building Code of Australia/relevant Australian Standard(s).
- (9) Design Statement

Development applications relating to three or more grouped dwellings; or any number of multiple dwellings (in areas coded less than R40) , shall be accompanied by a Design Statement, demonstrating the application addresses the following:

1. The Design Principles of the R-Codes where 'deemed to comply' provisions have not been met;
2. Providing an explanation of how the proposal addresses the identified future neighbourhood character, Design Principles of SPP 7 and this Policy; and
3. TPS 3.

The statement shall be between 1 and no more than 5 pages (depending on the size and complexity of the proposal) and be accompanied with a plan illustrating the proposed development and the local contextual considerations including relationship to adjacent properties and interface with the street frontage (Site Context Plan). The aim of the design statement is to explain how design quality requirements of the abovementioned policies have been achieved. Appendix 2 provides example questions an applicant may consider.

Strategic Link:	Town Planning Scheme No.3
Category	Planning – Town Planning & Development
Lead Business Unit:	Statutory Planning
Public Consultation: (Yes or No)	Yes
Adoption Date: (Governance Purpose Only)	11 June 2020
Next Review Due: (Governance Purpose Only)	June 2021
ECM Doc Set ID: (Governance Purpose Only)	4517027

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2

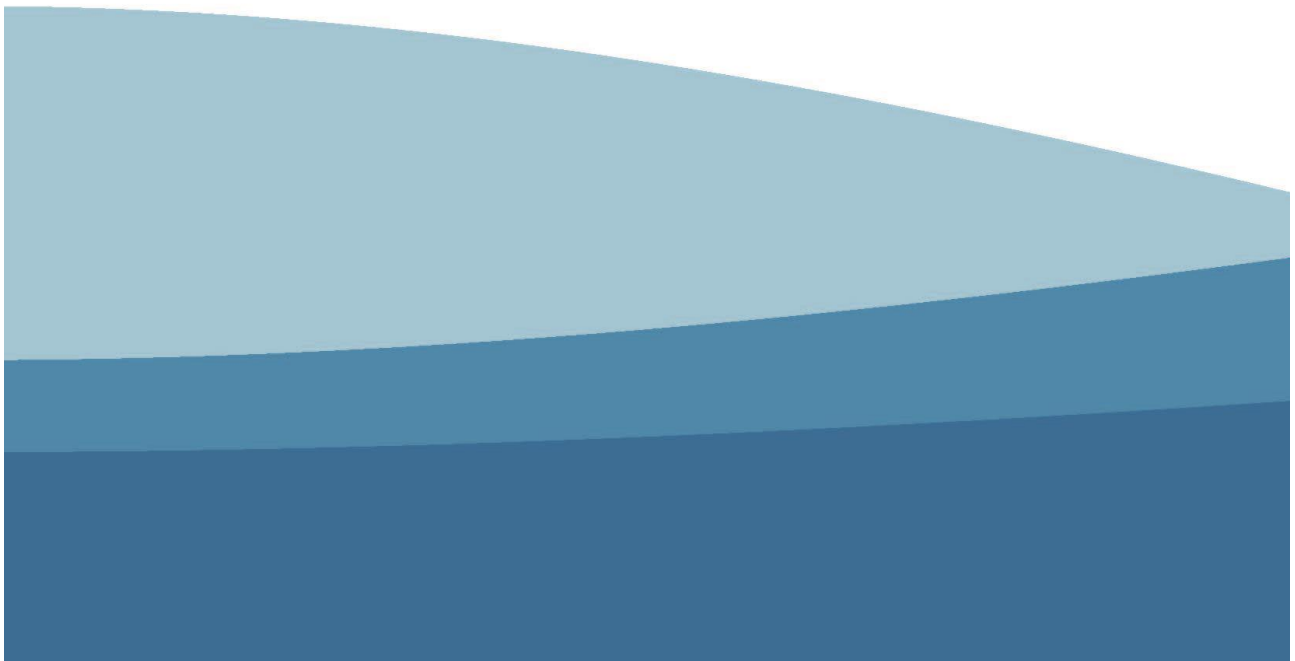


Attachment 1 - Residential Design Guidelines



Attachment 1

City of Cockburn
Local Planning Policy 1.2
Residential Design Guidelines



Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

Design Principle 1: Context and Character

Good design responds to and enhances the distinctive characteristics of a local area, contributing to a sense of place.

OBJECTIVES

To ensure that development:

- O 1.1** Responds positively and contributes to the intended future neighbourhood character of the area, as set out in Appendix 1.
- O 1.2** Demonstrates consideration of the local context, and achieves a sympathetic relationship and interface with adjacent sites, streetscapes and the surrounding neighbourhood.
- O 1.3** In areas undergoing change, that the design responds to the identified future character of the locality, while mitigating impacts on existing residents and development.
- O 1.4** Development includes deep soil areas with sufficient area and volume to sustain healthy plant and tree growth to contribute positively to the identified neighbourhood character.

DESIGN GUIDANCE

DG 1.1 GARDEN AREA

A Garden Area is to be provided for each grouped/multiple dwelling to support and sustain the development of tree canopy, and to ensure that dwellings respect and contribute positively to the identified neighbourhood character. Garden Areas shall:

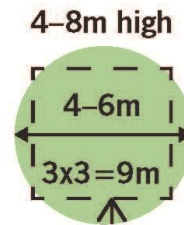
- a) Be a minimum area of 9m² located wholly on site;
- b) Be landscaped, uncovered, unpaved, free draining soil;
- c) Be a minimum length and width dimension of 3m;
- d) Be a minimum of 1m from any building, roof, fence or structure;
- e) Not be used for vehicle parking or access;
- f) Contain no buildings, patios, pergolas, swimming pools or external fixtures;
- g) Be distributed appropriately throughout the development;
- h) Be co-located with existing trees where possible (and the trees are an appropriate species), and in locations best suited to the development of a viable tree canopy;
- i) Be designed to account for irrigation and drainage pathways to reduce staining and ongoing maintenance of the planting infrastructure and the building fabric;

- j) Be accessible for routine pruning as may be required;
- k) Demonstrate measures to manage leaf litter impacts to minimise likely maintenance, and ensure ease of maintenance.

DG 1.2 DRIVEWAYS AND ACCESS

Development to reduce the visual impact of vehicle entries and circulation areas within the site on the intended future neighbourhood character, including consideration of the following:

- a) Changes in materials, colour, levels or landscaping to delineate pedestrian and vehicle circulation areas and define pedestrian paths in shared areas;
- b) locating vehicle entries to minimise ramp lengths and excavation;



- c) where required, incorporating aesthetically pleasing traffic calming devices that are integrated into the design such as changes in paving material or textures;
- d) reducing sections to single lane (3m width) to allow for the incorporation of Garden Areas and landscaping;
- e) minimising the visual impact of unavoidable long driveways through changing alignments and screen planting;
- f) Siting of crossovers to ensure safe and efficient traffic flows and promote intended future neighbourhood character;
- g) No additional crossovers to lots abutting Regional or major roads unless the existing crossover cannot be used for the proposed development. (Major Road defined in Australian Standard 1348: 'Roads and traffic engineering' and the Austroads Glossary of Terms as "A road to which is assigned a permanent priority for traffic movement over that of other roads";
- h) New developments with shared/common property access ways to utilise the shared/common property access way without additional crossover(s) wherever possible;
- i) New or modified crossovers located a minimum of 1m from existing street trees;
- j) Services such as water metres and power domes to be annotated on the site plans demonstrating they do not impact access to rear dwellings.

DG 1.3 CAR PARKING

- a) New carports and garages added to existing retained dwellings designed to complement the style and

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

- finish of the existing dwelling, considering materials, colour and roof pitch.
- b) Visitor and resident car parking designed to minimise the negative visual and environmental impacts on amenity and the identified neighbourhood streetscape character.

DG 1.4 CORNER LOTS

Development of corner lots shall contribute to enhancement of intended future neighbourhood character and improve passive surveillance, as follows:

- a) One dwelling facing each street (where possible);
- b) Corner dwellings to be designed to address both primary and secondary streets;
- c) Existing blank/solid fencing to the secondary street to be removed and replaced with open style fencing for no less than 50% of the boundary length;
- d) Battleaxe subdivision designs on corner lots will generally not be supported (see diagram below);
- e) Subdivision designs resulting in two long narrow lots with narrow frontages to the primary street generally not supported (see Figure 1).

DG 1.5 OUTDOOR LIVING AREAS

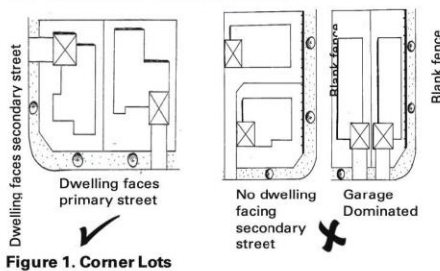


Figure 1. Corner Lots

Outdoor Living Areas (OLAs) may be supported in the front setback of a dwelling or between the dwelling and primary street where:

- a) The OLA achieves the design principles of Clause 5.3.1 of the R-Codes;
- b) The OLA is developed including provision of a level area either paved and drained or turfed and fenced with complementary fencing (mix of solid and open-style);
- c) Any roofing material covering the OLA matches or is complementary to the colour and material of the dwelling; and
- d) A separate drying area provided behind the dwelling alignment.

Design Principle 2: Landscape Quality

Good design recognises that together landscape and buildings operate as an integrated and sustainable system, within a broader ecological context.

OBJECTIVES

- O 2.1** To ensure the creation of attractive and functional outdoor spaces designed with people in mind.
- O 2.2** To ensure landscape design enhances streetscape and pedestrian amenity; improves the visual appeal and comfort of open space areas; and provides an attractive outlook for habitable rooms.
- O 2.3** To ensure soft landscaping creates external environments that interact in a considered manner with built form, resulting in well integrated, engaging places that contribute to local identity and streetscape character.

DESIGN GUIDANCE

DG 2.1 LANDSCAPING

- a) Development Applications for 3 or more Grouped dwellings to include a Landscape Plan containing a Site Schedule and a Site Plan. (See Appendix 3 for Landscape Plan requirements.)
- b) A semi-mature small-medium sized tree (as a minimum) shall be provided in each Garden Area with a minimum 90 litre pot size of an appropriate species considering:
 - i. Size at maturity
 - ii. Siting
 - iii. Root impacts
 - iv. Maintenance requirements
- c) Driveways separated along their length from adjacent structures and walls by a 0.5m wide landscaped area to be densely planted to reduce the visual prominence of adjacent walls and structures, to reduce the apparent width of the driveway, and to assist in dissipating heat build-up in walls and paving materials. This requirement does not apply to driveways under a roofed structure, and at doorways to buildings.
- d) For driveways where 5 or more dwellings are proposed containing straight sections longer than 35m (measured from front lot boundary), the width of the driveway landscaping area to extend a further 1m x 1.5m at no more than 10m intervals along one side of the driveway (Example provided in Figure 3). Selected plant species within these areas shall be taller than the rest of the driveway landscaping.

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

This is to:

- Create a sense of visual relief for visitors to these developments;
- Reduce the impact of extensive hardscape materials from the street and contribute positively to intended neighbourhood character.

Consideration will be given to alternative layouts that meet the desired design principle of reducing the impact of hardscapes in and around driveways.

DG 2.2 VERGES

- Removal of street trees located within verge areas is strictly not permitted without specific approval of the City. Where removal is approved two replacement trees are to be located and installed (Details of suitable species selection can be found at Appendix 4).
- Verges to be landscaped in accordance with the City's Residential Verge Development Guide.

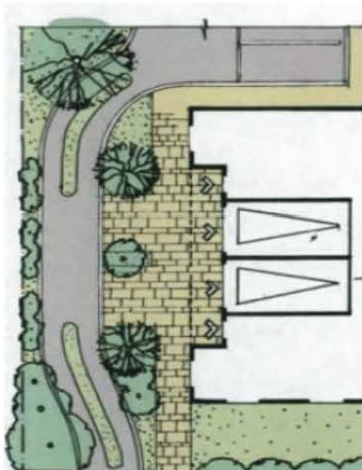


Figure 3: Curved pavement and landscape planting to reduce visual impact of driveway.

Design Principle 3: Built form and scale

Good design ensures that the massing and height of development is appropriate to its setting and successfully negotiates between existing built form and the intended future character of the local area.

OBJECTIVES

- O 3.1** To ensure that good design delivers buildings and places of a scale that respond to landform characteristics and existing built fabric in a considered manner, mitigating the potential for negative amenity impacts on both private land and the public realm.
- O 3.2** The scale, massing and height of new development should respond positively to that of the adjoining buildings, the topography, the general pattern of heights, and the views, vistas and landmarks of the place, reinforcing a coherent local identity.
- O 3.3** The orientation, proportion, composition, and articulation of built form elements should deliver an outcome that is suited to the purpose, defines the public domain, contributes to the character of adjacent streetscapes and parks, and provides good amenity for people at ground level.

DESIGN GUIDANCE

DG 3.1 BULK AND SCALE

- Developments to demonstrate how the siting, bulk and scale of new development does not dominate the streetscape; and any adjacent POS, and how they enhance and complement the intended future character.
- Bulk and scale of new buildings to consider the impact on neighbouring properties, the streetscape, and any adjacent POS, and to achieve a compatible interface.
- Overall shape, volume and arrangement of the parts of any new development should not dominate the identified neighbourhood character, with design elements reducing building bulk.

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

Design Principle 4: Functionality and build quality

Good design meets the needs of users efficiently and effectively, balancing functional requirements to perform well and deliver optimum benefit over the full life-cycle.

OBJECTIVES

To ensure that good design achieves:

- O 4.1** Good build quality through the use of durable materials, finishes, elements and systems that are - resilient to wear and tear expected from its intended use; easy to maintain; easy to upgrade; weather well over time; and does not have excessive maintenance requirements.
- O 4.2** Considers the full life-cycle of the development and mitigation of potential climate change impacts.
- O 4.3** Accommodates services in an integrated manner, without detriment to the appearance, functionality and serviceability of the final outcome.

DESIGN GUIDANCE

DG 4.1 WASTE STORAGE

Waste storage facilities to minimise negative impacts on the streetscape, building entries and the amenity of residents.

DG 4.2 EXTERNAL MATERIALS

Exterior material finishes to be durable and low-maintenance to ensure:

- The dwelling performs well over time with reasonable levels of maintenance for the property owner;
- A high-quality external appearance, contributing positively to the intended neighbourhood character over the full life-cycle of the dwelling.

Design Principle 5: Sustainability

Good design optimises the sustainability of the built environment, delivering positive environmental, social and economic outcomes.

OBJECTIVES

- O 5.1** Sustainable built environments that use passive environmental design measures at various scales, responding to local climate and site conditions by providing optimal orientation, shading, thermal performance and natural ventilation.
- O 5.2** To reduce reliance on technology for heating and cooling minimises energy use, resource consumption and operating costs over the life-cycle of the project.
- O 5.3** Building layouts that respond to the streetscape, topography and site attributes while optimising solar and daylight access within the development.
- O 5.4** Dwellings are designed to optimise natural ventilation of habitable rooms.

DESIGN GUIDANCE

DG 5.1 SOLAR AND DAYLIGHT ACCESS

- a) The development is sited and designed to optimise the number of dwellings receiving winter sunlight to private open space and via windows to habitable rooms.
- b) Windows are designed and positioned to optimise daylight access for habitable rooms.
- c) The development incorporates shading and glare control to minimise heat gain and glare from mid-spring to autumn

DG 5.2 NATURAL VENTILATION

- a) Individual dwellings to be designed to optimise natural ventilation of habitable rooms.
- b) To achieve better natural ventilation consider design solutions such as:
 - i. High and low level ventilation openings oriented between 45 degrees to 90 degrees of the prevailing cooling wind direction
 - ii. windows in at least two rooms and connecting doors located at the rear of the room rather than adjacent to the windows
 - iii. using stack effect ventilation/solar chimneys or wind scoops to naturally ventilate internal building areas or rooms such as bathrooms and laundries
 - iv. designing courtyards or building indentations that are open on one side and have a width-to depth ratio of at least 3:1.

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

Design Principle 6: Amenity

Good design provides successful places that optimise internal and external amenity for occupants, visitors and neighbours, providing environments that are comfortable, productive and healthy.

OBJECTIVES

To ensure that good design:

- O 6.1** Provides internal rooms and spaces that are adequately sized, comfortable and easy to use and furnish, with good levels of daylight, natural ventilation and outlook.
- O 6.2** Overall floor area of the dwelling and the dimensions of individual rooms that are large enough to accommodate differing arrangements of furniture depending on individual preferences and requirements.
- O 6.3** Provides good levels of internal amenity and also includes the provision of appropriate levels of acoustic protection and visual privacy, adequate storage space, and ease of access for all.

DESIGN GUIDANCE

DG 6.1 SIZE AND LAYOUT OF DWELLINGS

- a) The internal size and layout of dwellings is functional with the ability to flexibly accommodate furniture settings and personal goods, appropriate to the expected household size.
- b) The arrangement of doors, windows, circulation paths and electrical fittings should maximise flexibility and functionality of all dwellings.
- c) Ceiling heights and room dimensions provide for well proportioned spaces that facilitate good natural ventilation and daylight access, and maximise useability. In this regard the Table below will be used for guidance for minimum room sizes.

Habitable room type	Minimum internal floor area	Minimum internal dimension
Master bedroom	10m ²	¹ 3m ²
Other bedrooms	9m ²	¹ 3m ²
Living room	N/A	4m

¹Excluding robes

Design Principle 7: Legibility

Good design results in buildings and places that are legible, with clear connections and easily identifiable elements to help people find their way around.

OBJECTIVES

To achieve good design that:

- O 7.1** Makes places easy to navigate and ensure they are well-connected to existing movement networks.
- O 7.2** Considers sightlines, with built form responding to important vantage points.
- O 7.3** Ensures that movement is always easy for everyone who uses it, whether they are on foot or by bicycle, public transport or private vehicle.
- O 7.4** Seeks to prioritise pedestrian movement priority over vehicular movement.
- O 7.5** Ensures that access and circulation within developments contributes to a fine-grain network of direct and connected routes within and beyond the site and avoid creating large non-permeable blocks.

DESIGN GUIDANCE

DG 7.1 PEDESTRIAN LINKS

- a) Pedestrian links should be direct, with clear sightlines and passive surveillance from dwellings within the development, while maintaining appropriate privacy for residents.
- b) Consideration should be given to the provision of a gate and clearly defined path leading to front doors.

DG 7.2 CONNECTIVITY

On larger sites consider opportunities to provide pedestrian links through the site to provide connectivity to adjacent key pedestrian routes and key destinations including open space, main streets, activity centres and public transport.

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

Design Principle 8: Safety

Good design optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use.

OBJECTIVES

To promote safety and security by:

- O 8.1** Maximising opportunities for passive surveillance of public and communal areas.
- O 8.2** Providing clearly defined, well-lit, secure access points that are easily maintained and appropriate to the purpose of the development.
- O 8.3** Achieving a positive, clearly defined relationship between public and private spaces and addresses the need to provide optimal safety and security both within a development and to the adjacent public realm.
- O 8.4** Design of vehicular transport routes that integrate safety requirements in a manner that mitigates negative impacts on pedestrian amenity.

DESIGN GUIDANCE

DG 8.1 PASSIVE SURVEILLANCE

- a) Dwelling design to balance the need for safe and efficient vehicle access and egress, with the needs of pedestrians, cyclists, and other road users, in addition to the impacts on the identified streetscape character.
- b) Clearly defined front door entry to dwellings which incorporates features such as a portico/awning. Variations to these features are encouraged where front doors address an internal driveway to create greater visual interest.
- c) Garages and carports located in front of an existing dwelling to allow at least one major opening (window) facing the primary street and retain a clear entry to enable adequate surveillance of the street.

DG 8.2 SHARED/Common AREA LIGHTING

- a) For 6 or more dwellings, bollard lighting to light shared/common access ways into developments (including the existing retained dwellings).
- b) Light levels to comply with the relevant Australian standards.
- c) Lighting to be automatically switched on by falling ambient light levels.
- d) Details of the proposed bollard lights to be listed in the Landscape Plan schedule.

Design Principle 9: Community

Good design responds to local community needs as well as the wider social context, providing environments that support a diverse range of people and facilitate social interaction.

OBJECTIVES

To promote good design that:

- O 9.1** Achieves a mix of dwelling types, providing housing choice for different demographics, living needs and household budgets, and accommodating all ages and abilities.
- O 9.2** Has some capacity to adapt to changing demographics, an ageing population, new uses and people with disability so that it makes a positive contribution to the City's housing stock and the needs of the community.

To promote the following through split-codings:

- O 9.3** Improved surveillance of public open space ("POS") and activation of the streets opposite and adjacent to POS.
- O 9.4** Opportunities for dwelling diversity within the immediate vicinity of POS while achieving an improved balance between open space and dwelling floorspace through two-storey construction for higher density developments.
- O 9.5** The assembly of land parcels into larger development sites that can be developed in a more coordinated manner.
- O 9.6** Promotion of two storey construction for higher density developments so as to achieve an improved balance between open space and dwelling floorspace.

PROVISIONS

DG 9.1 SPLIT CODED R30/40 LOTS

Split coded R30/R40 residential lots may be developed up to the stated maximum R40 density where development is consistent with this policy and the following criteria:

- a) At least one of the dwellings is two storey or incorporates a habitable mezzanine/loft (excluding bedrooms) to create variety in design and height and provide opportunity for surveillance of the POS;
- b) New dwellings located on the front portion of a lot to have major windows fronting the street, and must not be orientated to solely face internal driveways (as shown in Figure 4);
- c) Wherever possible rear dwellings should be designed so that significant sections of the front elevations can be seen from the street (i.e. major openings to internal living areas)(as shown in Figure 5);

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

- d) Development on lots larger than 1500m² shall also demonstrate a suitable level of variety in design and height and promote surveillance of the POS.

DG 9.2 SPLIT CODED R30/40/60 LOTS

Split coded R30/R40/R60 residential lots may be developed at R40 or R60, where development is consistent with the requirements of this policy and the following criteria:

- a) In the absence of built development, land within any of the split coded areas depicted on the Scheme Map will only be granted subdivision approval up to the identified base code – that being Residential R30.
- b) Built development refers to buildings constructed to plate height in accordance with an approved Development Application.

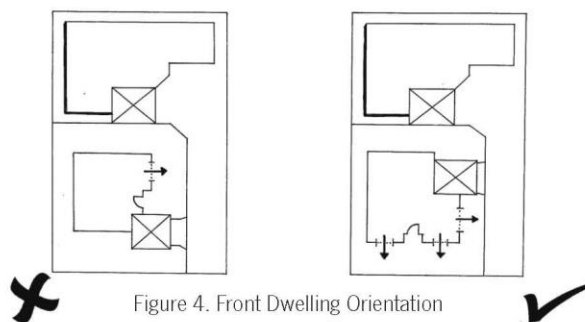


Figure 4. Front Dwelling Orientation

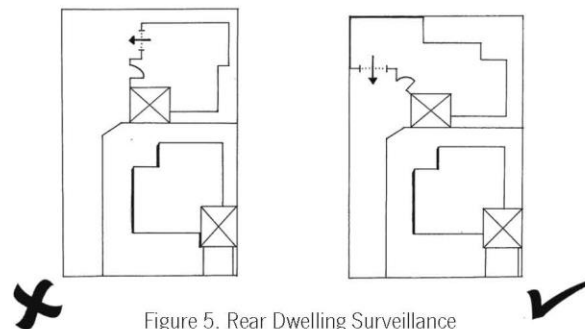


Figure 5. Rear Dwelling Surveillance

R40 Development Criteria	R60 Development Criteria
<ol style="list-style-type: none"> At least one of the dwellings is two storey or incorporates a habitable mezzanine/loft (excluding bedrooms) to create variety in design and height and provide opportunity for surveillance of the POS. New dwellings located on the front portion of a lot to have major windows fronting the street, and must not be orientated to solely face internal driveways (as shown in Figure 4). Wherever possible rear dwellings should be designed so that significant sections of the front elevations can be seen from the street (i.e. major openings to internal living areas) (as shown in Figure 5). Development on lots larger than 1500m² shall also demonstrate a suitable level of variety in design and height and promote surveillance of the POS. 	<ol style="list-style-type: none"> Development assembles more than one existing lot or the development site is over 2000m² in area. The majority of buildings (50%) are two storeys. Dwellings fronting a public street must address the primary street by way of design, fenestration, entry and must contain major opening(s) to a living area and/or master bedroom. Development shall demonstrate a suitable level of variety in design, height and rooflines and promote surveillance of the street and private access way. Development adjacent to POS must comply with the criteria set out in DG 9.1.

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

Design Principle 10: Aesthetics

Good design is the product of a skilled, judicious design process that results in attractive and inviting buildings and places that engage the senses.

OBJECTIVES

- | | |
|---------------|--|
| O 10.1 | To ensure that good design achieves a visually appealing and coherent outcome that addresses all aesthetic considerations from the articulation of building form through to the selection and detailing of materials and building elements, enabling integrated responses to the character of the place. |
| O 10.2 | To ensure that development responds to the local context, and addresses the intended future character. |
| O 10.3 | Street facing development and landscape design retains and enhances the amenity and safety of the adjoining public domain, including the provision of shade. |

DESIGN GUIDANCE

DG 10.1 FAÇADE DESIGN

Frontages to the street(s) to include:

- a) Well-articulated building facades, through architectural features such as balconies, verandahs, porticos, awnings, plinths and other such elements including cladding.
- b) Building facades visible from the public realm to contain a minimum of three different finished materials such as face brick, painted render and/or painted weatherboard and to incorporate a minimum of two of the following architectural features to provide a consistent architectural character:
 - i. Roof features such as gable ends (open or finished), flat roofs (where concealed by parapet walls), skillion roofs or dormer windows.
 - ii. Wall features such as decorative parapet walls, feature walls (including cladding), treated plinths and exposed brickwork.
 - iii. Protruding feature elements around major openings;
 - iv. A balcony or Juliette balcony.
 - v. Window awnings or window lintels.
 - vi. Porticos.
 - vii. Decorative treatment / moulding to parapet walls, lintels, window sills or horizontally expressed plinths to change in floor levels.

DG 10.2 FACADES TO DRIVEWAYS

Facades to internal driveways to include articulation and setback variations to create visual interest, provide landscaping opportunities, and avoid creation of a monotonous interface and hostile setting for grouped dwellings.

DG 10.3 RETAINED DWELLINGS

- a) Dwellings retained as part of a subdivision or development of grouped or multiple dwellings shall be of a standard capable of being upgraded so its appearance is not out of character with the new development. Upgrading may involve the following requirements:
 - i. Bagging or rendering external walls, replacing or professionally recoating non-masonry walls or professionally cleaning existing brickwork;
 - ii. Replacing or professionally recoating faded or discoloured roof tiles or metal sheeting;
 - iii. Replacing/repairing and painting gutters and downpipes;
 - iv. Replacing/upgrading driveways which are un-drained and extensively cracked or in a state of disrepair;
 - v. Modifying, upgrading or replacing damaged or dilapidated windows and frames;
 - vi. Demolishing unauthorised or poorly maintained additions, flat roof carports/extensions, sleep outs and constructing quality replacement structures (if required) which match or complement the existing dwelling and new development where visible from the street or public domain;
 - vii. Improvements to existing landscaping;
 - viii. Replacement of substandard or asbestos fencing where visible from the street.
- b) Works to be undertaken on the existing dwelling to be completed prior to subdivision clearance or occupation of the new dwelling (whichever comes first), imposed as a development or subdivision condition where appropriate.

DG 10.4 GARAGES AND MINIMUM FRONTAGES

A single-storey dwelling with a double width garage or carport will generally not be supported on a lot with a frontage of less than 10.5m (at the boundary). Double width shall refer to an opening of 4.8m or greater capable of accommodating two vehicles side by side.

DG 10.4 FENCING

Secondary street fencing to be permeable above 1.2m for no less than 50% of the boundary length so it does not detract from the identified neighbourhood character and that active frontages are achieved to secondary streets.

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

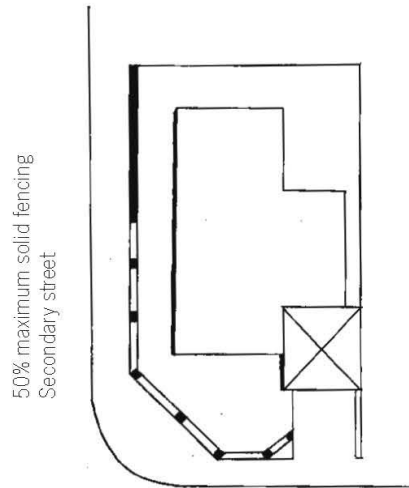


Figure 3. Fencing on corner lots

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

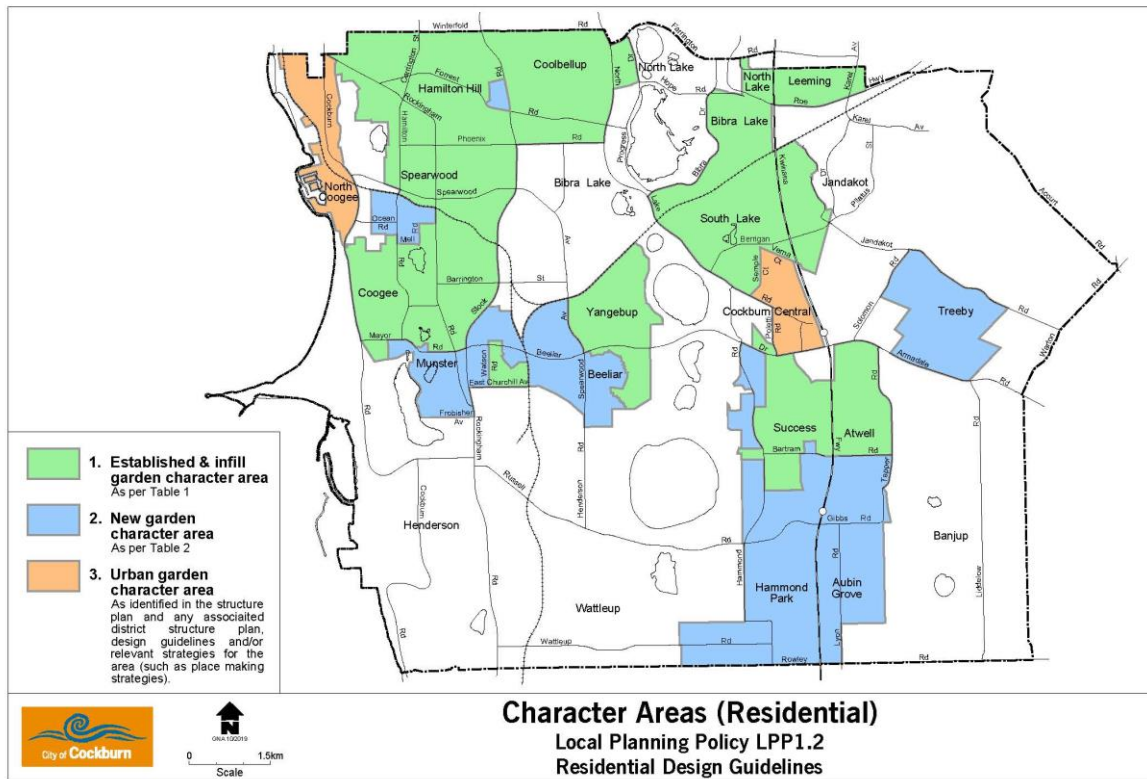
Appendix 1 - Intended Neighbourhood Character

No.	Character Area	Intended future character
1	Infill Garden Character Area	As per Table 1
2	New Garden Character Area	As per Table 2
3	Urban Garden Character Area	As identified in the Structure Plan(s) and any associated District Structure Plan, Design Guidelines and/or relevant adopted strategies (such as Place Making Strategies).

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines



Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

TABLE 1: INFILL GARDEN CHARACTER AREAS – INTENDED FUTURE CHARACTER

	Current Valued Character Elements	Intended Future Desired Infill Character
1. Front Setbacks	<p>Open front setbacks characterised by predominately soft green elements, often including lawn, mature vegetation and trees, with hardscaped elements being integrated and subservient.</p> <p>These are valued for: Contributing to the creation of a green, leafy streetscape character, with landscaping softening the appearance of built form elements.</p> <p>a) Creating open space for the amenity of dwelling occupants. b) Improving dwelling occupant and pedestrian comfort by creating shade and reducing glare. c) Reducing the heat-island effect.</p>	<p>1.1 Streetscapes that have a garden character, with landscaping and trees within the site, front setback, and verge areas that soften the appearance of the built form.</p> <p>1.2 Hardscaping and car parking areas being subservient elements of the streetscape.</p> <p>1.3 Open front setback areas that include landscaping, and the opportunity for trees to enhance the streetscape character.</p> <p>1.4 Landscaping in front setbacks and verges that contribute to the amenity of pedestrians and cyclists.</p> <p>1.5 Views of front gardens from the street maintained wherever possible.</p>
2. Open Space	<p>Dwellings set amongst landscaping and open space, often including mature vegetation such as shrubs and trees.</p> <p>These are valued for:</p> <p>a) Contributing to the creation of a green, leafy streetscape character, with landscaping softening the appearance of built form elements. b) Creating open space for the amenity of dwelling occupants. c) Improving dwelling occupant and pedestrian comfort by creating shade and reducing glare. d) Reducing the heat-island effect.</p>	<p>2.1 Dwellings in a garden setting, with open spaces and landscaping creating amenity and functional outdoor spaces for occupants.</p> <p>2.2 A reduced reliance on technology for heating and cooling and minimisation of energy use by minimising hardscaping and maximising opportunities for trees and plants.</p>
3. Bulk & Scale	<p>Dwellings with a form, scale, bulk, style and roof line that generally does not detract from the open, landscaped streetscape, with a style that is distinctly 'suburban residential' in character; aligning with the predominately residential function of the neighbourhood.</p>	<p>3.1 Grouped dwellings with a bulk, scale and form that can sit comfortably alongside single dwellings, and does not dominate the suburban garden streetscape character.</p>
4. Dwelling Design & Liveability	<p>Dwelling facades characterised by:</p> <p>a) Different elements and details of design interest, including openings and articulation (rather than being one flat surface), and a roofline and eaves that provide visual interest; b) Durable external material finishes that generally weather well and maintain a good appearance over time; c) Variety of openings to the street(s) that serve to provide visual interest, break up the facade, and provide passive surveillance; and d) Garages, carports that do not visually dominate the façade.</p> <p>Dwellings that provide high levels of amenity; meet the needs of occupants and flexibly accommodate furniture and personal goods.</p>	<p>4.1 Dwellings that are visually appealing, with facades and rooflines that include different elements of design interest.</p> <p>4.2 Dwellings on corner lots that address and provide visual interest to both streets.</p> <p>4.3 Dwellings that have openings to the street(s) to provide visual interest and passive surveillance.</p> <p>4.4 Durable external material finishes that weather well and maintain a good appearance over time.</p> <p>4.5 Well-designed dwellings that provide high levels of amenity for occupants, flexibly accommodate furniture and personal goods, and contribute to the creation of diverse and high quality housing to meet the needs of the community.</p>
5. Crossovers & Driveways	<p>Predominately one (single or double crossover) for each existing established residential property which:</p> <p>a) Contribute to the creation of a green, leafy streetscape character, with landscaping softening the appearance of built form elements. b) Minimise disruption to the pedestrian and cyclist environment, thereby improving safety and comfort. c) Maximises opportunities for street trees and landscaping in verge areas which reduces the heat-island effect.</p>	<p>5.1 Streets that are pedestrian and cyclist friendly, with:</p> <ul style="list-style-type: none"> Minimal crossover interruptions to maximise pedestrian and cyclist safety, comfort and convenience. Trees and landscaping in front setbacks and verges creating shade and reducing glare. <p>5.2 The width and number of crossovers minimised to:</p> <ul style="list-style-type: none"> Minimise hard standing and maximise tree and landscaping opportunities in the verge and front setback Maximise street tree retention Minimise conflict between vehicles and pedestrian/cyclist movement and maximise pedestrian and cyclist safety and comfort.

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

TABLE 2: NEW GARDEN NEIGHBOURHOOD CHARACTER AREAS – INTENDED FUTURE CHARACTER

	Key Elements of Future Intended Character	Intended Future Character Objectives
1. Front Setbacks	Streetscapes that have a landscaped garden character, with landscaping and trees within the site, front setback, and verge areas that soften the appearance of the built form.	<ul style="list-style-type: none"> 1.1 To create open front setback areas that include landscaping, and the opportunity for trees to contribute to the creation of a landscaped streetscape character. 1.2 To create functional on-site open spaces that contribute to the amenity of dwelling occupants and visitors. 1.3 To minimise hardscaping elements in the front setback and verge and maximise opportunities for landscaping. 1.4 To facilitate landscaping in front setbacks and verges that contributes to the amenity of pedestrians and cyclists. 1.5 To maintain views of front gardens from the street wherever possible.
2. Open Space	Dwellings in a garden setting, with open spaces and landscaping creating amenity and functional spaces for occupants.	<ul style="list-style-type: none"> 2.1 To reduce reliance on technology for heating and cooling and minimise energy use by minimising hardscaping and maximising opportunities for trees and plants. 2.2 Locate open space to optimise connection with living areas and support landscaping and tree planting.
3. Dwelling Design and Liveability	<ul style="list-style-type: none"> a) Dwellings with facades that include different elements of design interest, including articulation. b) Dwellings that have openings to the street(s), providing visual interest and passive surveillance. c) Well-planned, liveable dwellings that provide high levels of amenity for occupants, and contribute to the creation of diverse and high quality housing for the community. 	<ul style="list-style-type: none"> 3.1 Dwellings with an internal layout that is functional for occupants, providing the ability to flexibly accommodate furniture and personal goods. 3.2 Dwellings that have healthy and comfortable living environments for occupants, 3.3 Durable external material finishes that weather well and maintain a good appearance over time.
4. Crossovers & Driveways	<p>Streets that are pedestrian and cyclist friendly, with:</p> <ul style="list-style-type: none"> a) Trees and landscaping in front setbacks and verges creating shade and reducing glare. b) Minimal crossover interruptions to maximise pedestrian and cyclist safety, comfort and convenience. 	<ul style="list-style-type: none"> 4.1 To minimise the number and width of crossovers to: <ul style="list-style-type: none"> a) Maximise tree and landscaping opportunities in the verge and front setback b) Minimise hard standing. c) Maximise street tree retention d) Minimise conflict between vehicles and pedestrian/ cyclist movement and maximise pedestrian and cyclist safety and comfort.
5. Bulk & Scale	<ul style="list-style-type: none"> c) Grouped dwellings with a bulk, scale and form that does not dominate the current or future desired landscaped streetscape character. d) Grouped dwellings that contribute to the creation of a suburban residential neighbourhood character whilst contributing to housing diversity. 	<ul style="list-style-type: none"> 5.1 To ensure that the siting, bulk and scale of new development does not dominate the streetscape or public realm, or create a monotonous built form interface to internal driveways and common property. 5.2 To ensure that the bulk and scale of new buildings considers the impact on neighbouring properties and the streetscape, and achieves a compatible interface. 5.3 New development that enhances and complements the identified streetscape character.

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

Appendix 2 – Example questions for consideration of a Design Quality Statement



1. Does the proposal look appropriate in its context? Does the development respond to the intended future character and desirable elements of the area? Provide details.
2. Has the proposal considered the scale of surrounding development? In areas undergoing transition, does the bulk and scale of development consider any future aspirations of the locality? Provide details.
3. Is the built form appropriate for the site? For example how does the development interface with surrounding public domain areas including public open spaces, the street and does it provide a good level of internal amenity?
4. Have facades been designed to include a variety of elements of visual interest (minimum of three) as required by this policy? Outline and identify these features.
5. Where a development includes driveways do facades address the driveways and include variation in setbacks/features, as required by this policy? Provide details.
6. Does the proposal consider sustainable design solutions? Provide details.
7. Does the proposed landscaping integrate with the built form and result in a good aesthetic quality for both occupants and the adjoining public domain? Provide details.
8. Has amenity been considered through appropriate room dimensions, access to sunlight, natural ventilation, private outdoor spaces, privacy etc? Provide details.
9. Does the proposal provide a good level of security both internally and in public areas? Provide details.
10. Has the development considered the localities social context and housing affordability? For example through the provision of a range of housing types? Provide details.
11. Does the proposals aesthetics contribute to the existing or desired future character of the area? Provide details.
12. What site constraints were considered and how did these inform the final design? Provide details

Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines

Appendix 3 – Landscape Plan Requirements

The Site Schedule shall include the following:

1. Applicant name, Lot and Deposited Plan Number and address;
2. Area in square meters of:
 - a. Lot;
 - b. Verge excluding 2m wide back of kerb footpath reserve, crossover and footpaths;
 - c. Impervious paving, includes areas beneath roofed structures (buildings and sheds), paving required for car parking bays, and vehicle access driveways (excludes paving proposed but not required for these specific purposes);
 - d. Required 'Garden Area'
 - e. Area of impervious paving required other than that listed in c) above;
 - f. Area of unpaved, free draining soil.

The Landscape Plan is to be a dimensioned plan, drawn to scale, indicating the arrangement of the following:

1. The ground floor plan of dwellings and other roofed buildings proposed, including finished floor level, doors, dashed roof gutter line and window positions;
2. External paving and steps – extent of hard materials proposed;
3. Elevations of the finished ground level above Australian Height Datum (AHD) to describe:
 - a. Sudden changes in level
 - b. Highest and lowest points
 - c. Corners of areas of continuous paved or unpaved finish,
 - d. Method of draining all paved areas
4. Unroofed structures including but not limited to:
 - a. Retaining walls,
 - b. Swimming pools,
 - c. Fences and gates,
 - d. Light fittings,
5. Proposed planting, including:
 - a. Botanical and common species name,
 - b. Size of nursery stock to be used,
 - c. Anticipated mature height of species,
 - d. Numbers of plants of each species,
 - e. Methods of cultivating planting such as
 - i. type and depth of mulch
 - ii. details of tree staking, and
 - iii. method of irrigating the planting (source of water, is a reticulation system and controller proposed, source of water – nominated caretaker for site/common areas or separate supply points from each dwelling and so on).



Title	Residential Design Guidelines
Policy Number (Governance Purpose)	LPP 1.2



Attachment 1 - Residential Design Guidelines



6. North point, bar scale and scale of drawing at a stated paper size (such as 1:100 Scale when printed on A3 sheet).

Example landscape plan:



Title	Lodging Houses
Policy Number (Governance Purpose)	LPP 1.6



Attachment 2 Lodging Houses

Policy Type

Local Planning Policy

Policy Purpose

The City's objectives for residential development include the provision of a variety of housing to meet the needs of different household types. This includes a commitment to the development of lodging houses that generally comply with the requirements of the City's Town Planning Scheme No. 3 (TPS 3) and this Policy. This Policy seeks to ensure the establishment of a lodging house is suitable to the nature and character of the locality within which it will be situated, and is conveniently positioned relative to local shops, community infrastructure and public transport services. The purpose is as follows:

To inform the design, assessment and determination process for lodging house proposals within the City of Cockburn.

- (1) To provide for the development of lodging houses in a suitable and sustainable manner.
- (2) To ensure lodging houses are developed evenly across the City, in locations where lodgers enjoy convenient access to local shops, community infrastructure and public transport services.
- (3) To ensure lodging houses are developed in a manner that adequately provides for the needs of lodgers, including their comfort, safety, and well-being.
- (4) To ensure lodging houses through appropriate design, development and management take into account the context of a location in terms of scale of development, and the relationship of such to adjoining development for the purpose of protecting the amenity of both lodgers and the occupants of adjoining properties (residential in particular).

Policy Statement

- (1) Application
 1. This Policy applies to all lodging house proposals on land zoned Residential under TPS 3 and any proposal for a lodging house on non-residential zoned land abutting residentially zoned land.
 2. The Policy can be used as a guide for the development of lodging houses on any other land.

Title	Lodging Houses
Policy Number (Governance Purpose)	LPP 1.6



Attachment 2 Lodging Houses

3. A “Lodging House” means any building or structure, permanent or otherwise, and any part thereof, in which the provision is made for lodging or boarding more than 6 persons, exclusive of the family or the keeper thereof, for hire or reward, but the term does not include:
 - (a) Premises licensed under a publican’s general licence, limited hotel licence, or wayside-house licence, granted under the Liquor Control Act 1988;
 - (b) Residential accommodation for students in a non-government school within the meaning of the School Education Act 1999; or
 - (c) Any building comprising residential flats.

(2) Development Guidelines

In contemplating the establishment of a lodging house within the City of Cockburn, the following development guidelines need to be considered in the preparation and presentation of an application to the City and/or Council for determination.

1. Size

- (a) The development of a lodging house should be for no more than 30 lodgers.

2. Location

- (a) Lodging houses in a Residential Zone are to be separated by a minimum distance of 200m, measured in a straight line.
- (b) A lodging house is to be located within 800m, measured in a straight line, from any part of the route to any part of the lot of local shops, community infrastructure and public transport services ie. a bus stop/station or train station (a 5-10 minute walking distance).

3. Site Planning

- (a) All lodging houses should comply with the requirements of the Residential Design Codes of Western Australia (R-Codes) and/or relevant City of Cockburn Policies in the following areas: streetscape requirements (setbacks), boundary setback requirements, open space requirements (open space provision), access requirements, site works, building height and privacy requirements;
- (b) All lodging houses are to comply with the parking requirements of TPS 3 which requires one (1) bay for every four (4) beds. Additional parking is to be provided at a rate of one (1) visitor bay for every six (6) beds, and one (1) exclusive bay for every on-site supervisor or staff member. Bicycle parking provision is to be provided at a rate of one (1) rack or similar for every five (5) lodgers.
- (c) Outdoor communal areas are to be centrally located on-site i.e. separated from common boundaries with adjoining residential properties,

Title	Lodging Houses
Policy Number (Governance Purpose)	LPP 1.6



Attachment 2 Lodging Houses

or situated within the front setback, suitably screened with transparent fencing for the benefit of lodger privacy.

- (d) Parking areas are to be suitably located relative to adjoining properties, with a minimum 2.5m landscaped separation.
- (e) Waste management storage and collection areas are to be conveniently located on-site, and appropriate in terms of location relative to adjoining properties.
- (f) Areas of site not used for a particular purpose i.e. as outdoor communal space, car parking and/or for waste management purposes, are to be developed and used for landscaping only.
- (g) Site planning is to provide for the sustainable development of any new lodging house building, providing for reduced energy consumption and cost while improving lodger comfort. As a minimum, orientation should maximise cross ventilation opportunities during summer and solar gain (warmth and natural lighting) during Winter.
- (h) All outdoor lighting (of parking areas, access ways and communal space/s) is to be in accordance with the requirements of Australian Standard AS4282-1997: 'Control of Obtrusive Outdoor Lighting' to protect against light spill.
- (i) Any signage erected for the purpose of identifying the use of a site for a lodging house is to be no greater than 1.0m x 1.5m in dimension, erected at the front of the site in a location that does not affect safe pedestrian movement or sight lines for safe vehicular access/egress into/out of the property (the details in respect of which are to be provided at the time of application for the development).

(3) Building Design

- (a) The adaptive re-use of an existing building for lodging house purposes is supported where the building is generally consistent with the character and scale of development of the local built environment, and can be adapted in a manner that is generally compliant with all other requirements of this Policy.
- (b) For new build development, the design shall have regard to the design requirements of State Planning Policy 3.7 (Residential Design Codes) Volume 2 – Apartments, where the City deems necessary.
- (c) The scale and external appearance of a new lodging house building is to be typically residential, consistent in terms of scale and appearance with the builtform prevalent in the street and on adjoining properties (within the parameters of the requirements listed in point 2.3(a) above).

Title	Lodging Houses
Policy Number (Governance Purpose)	LPP 1.6



Attachment 2 Lodging Houses

- (d) All lodging houses are to have a clearly defined, street fronting entrance point, preferably centrally located in the front elevation of the building. All entry points are to be suitably weather protected for the benefit of lodgers and visitors.
 - (e) Ablutions, laundries and recreation rooms should be located within the main building on-site. Recreation rooms in particular, should be located central to the building floor plan for the benefit of all lodgers whilst providing separation to adjoining properties.
 - (f) All externally placed building hardware, including air conditioning condenser units and small satellite dishes i.e. exempt from requiring development approval in accordance with the City's Domestic Satellite Dishes Policy (LPP 1.9), is to be suitably positioned and/or screened so as to not be publicly visible, or visible from adjoining properties. The placement of air conditioning condenser units should also have regard for how they might impact acoustically on adjoining properties.
- (4) Management
- a) Each lodging house development application submitted to the City of Cockburn is to be accompanied by a Management Plan that addresses and/or includes the following information:
 - i. Lodging house hours;
 - ii. Lodging house rules, including a lodging 'Code of Behaviour';
 - iii. Visitor rules;
 - iv. Security and access;
 - v. Waste management and the receiving of deliveries;
 - vi. Management and supervision information, including contact details.
 - b) Waste management information is to detail the following:
 - i. Waste likely to be generated (type and amount);
 - ii. Size, type and location of storage area to be provided;
 - iii. Measures to promote a high level of recycling;
 - iv. Method and management of collection.

The City's Health and Building service units should be consulted in respect of the above requirements.

Title	Lodging Houses
Policy Number (Governance Purpose)	LPP 1.6



Attachment 2 Lodging Houses

Strategic Link:	Town Planning Scheme No. 3
Category	Planning - Town Planning & Development
Lead Business Unit:	Statutory Planning
Public Consultation: (Yes or No)	Yes
Adoption Date: (Governance Purpose Only)	10 November 2022
Next Review Due: (Governance Purpose Only)	November 2024
ECM Doc Set ID: (Governance Purpose Only)	4709501



Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12

Policy Type

Local Planning Policy

Policy Purpose

Increasingly, residential development within the City of Cockburn is occurring within town centres and intense mixed use community areas, with a mix of residential and non-residential land uses. Maintaining the sustainability of the City and encouraging vibrant communities with residential land uses close to transport corridors, places of employment, shops and cafes, requires buildings to be designed and constructed with appropriate noise attenuation measures.

The City of Cockburn requires noise attenuation of residential and other noise-sensitive land uses to protect the long term health and amenity of residents from road, rail and aircraft noise, as well as noise from non-residential land uses and other sources. Appropriate noise attenuation must be demonstrated and implemented through the development process to achieve compliance with appropriate noise legislation, standards and policies.

The purpose of the Noise Attenuation Policy and associated Guidelines is to:

- (1) Detail the noise attenuation and noise management reporting process when undertaking development within the City of Cockburn.
- (2) Provide details of the City's requirements for the different types of acoustic reports.
- (3) Provide guidance to applicants as to when an acoustic consultant should be engaged and provide information on the type of assistance an acoustic consultant might provide.

Policy Statement

- (1) Acoustic Reports, Noise Management Plans and/or Noise Exposure Forecast Worksheets will be required as per the below *Acoustic Reporting Requirements* Table:

Development Stage	Acoustic Reporting Requirements
Structure Planning	At Structure Planning stage, the proponent must demonstrate that the proposed development can be made to comply with the relevant requirements. Suitable modelling of existing and/or future noise levels may be required, as well as detail of the anticipated noise mitigation measures. An Acoustic Report must accompany a Structure



Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12

Development Stage	Acoustic Reporting Requirements
	<p>Plan, variations to a Structure Plan that materially alter the Plans intent, or a Local Development Plan. The Acoustic Report shall be prepared in accordance with the City of Cockburn Noise Attenuation Guidelines where:</p> <ol style="list-style-type: none"> 1. Noise sensitive development is proposed in the vicinity of an existing or future major road, rail infrastructure or a freight handling facility as required by SPP 5.4; 2. The land is located within either the Core Area or Frame Area for Jandakot Airport as identified by SPP 5.3; or 3. Noise sensitive development and commercial, industrial or light industry land uses or infrastructure facilities are proposed in close proximity.
Subdivision Application	At Subdivision stage the City will request the preparation of either a Noise Exposure Forecast Worksheet OR a suitable Noise Management Plan as a condition of consent, as well as appropriate conditions to implement noise requirements through the development process as described in the City of Cockburn Noise Attenuation Guidelines, as detailed in Clause 1.
Local Development Plan	Where appropriate, the Local Development Plan must implement the requirements of the Noise Exposure Forecast Worksheet or Acoustic Report. Wording of the noise management requirements of a Local Development Plan must be in plain language that is suitably clear for prospective purchasers to understand.
Subdivision (Clearance)	<p>All noise mitigation requirements of the Noise Exposure Forecast Worksheet or Acoustic Report must be implemented for subdivision conditions to be cleared.</p> <p>Where Quiet House Design packages are recommended, all design requirements (including the provision of fresh air where relevant), must be implemented by a Local Development Plan. Any noise mitigation walls or barriers are to have been constructed prior to clearance being sought. Notifications on title must be suitably demonstrated prior to clearance being sought.</p>
Development Application – including Joint	An Acoustic Report must accompany a Development Application, in accordance with the City of Cockburn Noise Attenuation Guidelines where:



Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12

Development Stage	Acoustic Reporting Requirements
Development Assessment Panel Applications	<ol style="list-style-type: none"> 1. Non-residential development that includes plant and equipment which is likely to be a source of noise (such as air conditioning systems, compressors, public address systems, amplified music, musical instruments) within 100m of noise sensitive land uses; or 2. A change of use is proposed that may involve noise emitting activity such as entertainment or amplified music; or 3. A land use which cause large numbers of people to congregate (such as hotels, sporting clubs, taverns and other licensed/non-licensed premises) is proposed, including accommodation premises (such as lodging houses and backpacker establishments); or 4. Noise sensitive developments are proposed in the vicinity of an existing or future major road or rail infrastructure as required by SPP 5.4; or 5. A freight handling facility is proposed in the vicinity of noise sensitive land uses; or 6. Noise sensitive developments within the Core Area or Frame Area for Jandakot Airport as identified by SPP 5.3; or 7. Noise sensitive land uses, mixed use or multistorey residential developments are proposed within intense mixed use community areas such as Cockburn Coast, Port Coogee, Muriel Court, Cockburn Central Town Centre and Cockburn Central West; or 8. An application is deemed likely to contribute to, or be exposed to, heightened ambient noise, at the discretion of the City.
Prior to the submission of the Building Permit Application (as condition of DA)	<p>Final development design must achieve compliance with the identified noise criteria and suitably implement any noise mitigation requirements.</p> <p>In accordance with the conditions of Development Approval for a development:</p> <p>An Acoustic Report is to be submitted to the City for approval PRIOR to submission of the Building Permit Application. This Acoustic Report is to detail final design specifications and construction methods with which the proposed development will ensure compliance with the relevant noise criteria.</p>



Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12

Development Stage	Acoustic Reporting Requirements
Building Permit Application	<p>Building Permit Applications must clearly demonstrate compliance with all relevant Planning requirements.</p> <ul style="list-style-type: none"> Where a Building Permit Application is required to comply with a Local Development Plan, the plans must clearly demonstrate that all Noise Management requirements will be incorporated into the structure of the dwelling (for instance specific window suite construction and glazing). Where air conditioning or mechanical ventilation is required, installation details suitable to demonstrate compliance with the noise mitigation requirements must be included on the Building Permit plans. Where a Building Permit Application is required to comply with conditions of Development Approval, all relevant documents specified by those condition(s) must be submitted and all relevant conditions complied with. This may include a requirement for written confirmation from a recognised acoustic consultant to that the plans have been checked and confirmed as compliant with the requirements of the relevant Acoustic Reports.
Building Occupancy or Strata Approval Certificates	<p>Upon completion of construction and in accordance with conditions of Development Approval, a Form BA7 Notice of Completion or Form BA15 Building Approval Certificate Strata must be accompanied by written confirmation from the Builder to demonstrate that all requirements of the relevant Acoustic Report(s) were incorporated into the completed development.</p> <p>Where deemed appropriate, the City may require the builder to provide with the FormBA7 Notice of Completion written confirmation from an acoustic consultant that the building complies with the requirements of the acoustic report or that a final acoustic assessment of the finished development has been undertaken</p>
<p><i>Important note:</i> All buildings and structures are to comply with the relevant fire rating, safety, BAL and National Construction Code requirements. Noise attenuation methods shall not compromise the safety or structural integrity of the development.</p>	

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



NOISE ATTENUATION GUIDELINES

March 2022



Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12

Table of Contents

1.0	<i>Introduction</i>	1
2.0	<i>Acoustic Reports</i>	1
2.1	Who can help me with preparing an Acoustic Report?	1
2.2	When must I submit an Acoustic Report?	1
2.3	What must the Acoustic Report contain?	2
3.0	<i>What criteria should the Acoustic Report address?</i>	5
3.1	Noise Intrusion (Noise Sensitive Development).....	5
3.2	Vibration Intrusion (Noise Sensitive Development)	6
3.3	Noise Emissions (All Developments)	6
3.4	What will happen if the Acoustic Report submitted does not follow these Guidelines?	7
4.0	<i>Noise Source Identification</i>	7
4.1	Noise from Road, Rail and Freight Infrastructure	7
4.2	Vibration from Rail and Freight Infrastructure.....	8
4.3	Mechanical Plant and Equipment	9
4.4	Co-existing Land Uses	10
4.5	Noise Sensitive Developments in Proximity to Jandakot Airport.....	10
4.6	Breakout and Street Noise.....	11
5.0	<i>Noise Treatment Solutions</i>	14
5.1	Development Planning and Design	14
5.2	Construction Methods and Materials	14
6.0	<i>Related Legislation and Documents</i>	16
8.0	<i>Appendix A – Noise Source Identification Map</i>	19
9.0	<i>Appendix B – Noise Exposure Forecast Worksheet</i>	20

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



1.0 Introduction

Mixed land use areas, with residential and commercial land uses in close proximity, are actively encouraged by the City of Cockburn in town centre and intense mixed use community areas. A mix of residential and commercial activities provides residents and visitors to an area with access to places of employment, shops and cafes, and can contribute to the vibrancy of a community.

Locating residential land uses close to main roads, marina's, other transport infrastructure and non-residential areas, assists in maintaining the sustainability of the City and encourages active transport within the locality. However this mix of non-residential and residential land uses, and proximity to transport infrastructure, can result in an increase in ambient noise levels to residential areas.

It is therefore necessary to consider potential noise and amenity impacts throughout the planning process to ensure that appropriate noise attenuation measures are incorporated into the design and construction of developments within the City.

2.0 Acoustic Reports

The purpose of an Acoustic Report is to assess the noise environment either affecting or affected by, a development. It demonstrates how the proposed development will be designed, constructed or otherwise managed, to minimise noise emissions from a development and/or noise intrusion upon a development.

2.1 Who can help me with preparing an Acoustic Report?

The acoustic report must be prepared by a recognised Acoustic Consultant with suitable qualifications, appropriate expertise and local experience in the assessment of transport and environmental noise. They or their organisation must be:

- eligible to hold membership of the Australian Acoustical Society (AAS) in the grade of Member or Fellow (designated by the post-nominal letters M.A.A.S. or F.A.A.S. respectively); and/or
- represent a company holding current corporate membership to the Australian Association of Acoustical Consultants (AAAC); and or
- an acoustics engineer, defined as a person eligible for professional membership to the Institute of Engineers Australia (MIEAust).

An Acoustic Report may not be accepted in the absence of proof of membership, at the City's discretion.

2.2 When must I submit an Acoustic Report?

An acoustic report must be submitted at the earliest practicable planning stage for a development, in accordance with the City of Cockburn's Noise Attenuation Policy. Notwithstanding this, an Acoustic Report may be requested after initial assessment of a proposal by the City's Officers.

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



Applicants may contact the City's Environmental Health Service for assistance in determining whether an Acoustic Report is required

2.3 What must the Acoustic Report contain?

The Acoustic Report must address all matters necessary to demonstrate that the outcomes for reporting have been achieved as detailed by these Guidelines, to the City's satisfaction.

The Acoustic Report should identify the name and qualification/experience of the person preparing the report and include a description of the project with suitably detailed plans.

Details of noise measurements and predictions, as well as the criteria of the assessment and the assessed sources of noise should be clearly identified in the Acoustic Report. Proposed treatment solutions should be clearly identified. Clear calculations and information demonstrating how the assigned noise levels required under the EPNRs have been determined must also be provided.

A summary of the City's Acoustic Reporting Requirements is described in Table 1 overleaf.

Table 1: Acoustic Reporting Requirements during the Development Process

Development Stage	Acoustic Reporting Requirements
Structure Planning	At Structure Planning stage, the proponent must demonstrate that the proposed development can be made to comply with the relevant requirements. <i>Suitable modelling of existing and/or future noise levels may be required, as well as detail of the anticipated noise mitigation measures.</i>
Subdivision Application	At Subdivision stage, the proponent must demonstrate mitigation strategies to achieve compliance with the relevant criteria. Suitable modelling of future noise levels is required at this stage. <ul style="list-style-type: none"> For noise sensitive development within the trigger distance of road or rail infrastructure under SPP 5.4, either a Noise Exposure Forecast Worksheet (see Appendix B) or a preliminary Acoustic Report is required. For noise sensitive development within the Frame Area for Jandakot Airport, a preliminary Acoustic Report as described in Section 3 of these Guidelines is required. Where noise sensitive development is proposed in proximity to existing commercial or industrial

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



Development Stage	Acoustic Reporting Requirements
	<p>activities, an Acoustic Report as identified in Section 3 of these Guidelines is required.</p> <ul style="list-style-type: none"> For commercial or industrial development, an Acoustic Report is required to demonstrate that the requirements of the EPNRs can be achieved and demonstrate that there will be no adverse amenity impact upon nearby noise sensitive premises in accordance with Section 4 is required. <p>Where a previous Acoustic Report has been provided during the Structure Planning stage, the Subdivision Application must be supported by a further Acoustic Report demonstrating design solutions to protect the noise sensitive premises from excessive noise and identifying finished lot levels.</p> <p>Conditions appropriate for the implementation of the Noise Exposure Forecast Worksheet or Acoustic Report will be recommended by the City to the WAPC. <i>Where it is not possible for modelling to reference the final lot levels, the City may recommend requirements be confirmed by a final Acoustic Report prior to Subdivision Clearance.</i></p>
Local Development Plan	<p>Where appropriate, the Local Development Plan must implement the requirements of the Noise Exposure Forecast Worksheet or Acoustic Report.</p> <p>Wording of the noise management requirements of a Local Development Plan must be in plain language that is suitably clear for prospective purchasers to understand.</p>
Subdivision (Clearance)	<p>All noise mitigation requirements of the Noise Exposure Forecast Worksheet or Acoustic Report must be implemented for subdivision conditions to be cleared.</p> <p>Where Quiet House Design packages are recommended, all design requirements (including the provision of fresh air where relevant), must be implemented by a Local Development Plan. Any noise mitigation walls or barriers are to have been constructed prior to clearance being sought. Notifications on title must be suitably demonstrated prior to clearance being sought.</p>
Development Application – including Joint Development Assessment Panel	<p>Development Applications must demonstrate that compliance with the identified criteria can be achieved. The proponent must detail how mitigation strategies will be implemented to achieve compliance with the relevant noise criteria. Suitable modelling of existing and/or future noise levels is required at this stage.</p>

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



Development Stage	Acoustic Reporting Requirements
Applications	<ul style="list-style-type: none"> • For noise sensitive development within the trigger distance of road or rail infrastructure under SPP 5.4, either a Noise Exposure Forecast Worksheet (see Appendix B) or Acoustic Report is required. • For noise sensitive development within the Frame Area for Jandakot Airport, an Acoustic Report as described in Section 3 of these Guidelines is required. • Where noise sensitive development is proposed in proximity to existing commercial or industrial activities, an Acoustic Report demonstrating how compliance with the noise criteria described in Section 3 of these Guidelines is required. • For commercial or industrial development, an Acoustic Report is required to demonstrate that the requirements of the EPNRs can be achieved and that there will be no adverse amenity impact upon any nearby noise sensitive premises. <p>Conditions appropriate for the implementation of the requirements of the Noise Exposure Forecast Worksheet or Acoustic Report will be applied to the Development Approval issued by the City or recommended to the JDAP, as described below.</p> <p>Where relevant, a condition may be applied requiring Notification on the Certificate of Title in accordance with the relevant SPP and/or the City's LPP 1.12 and this Guideline.</p>
Prior to the submission of the Building Permit Application (as condition of DA)	<p>Final development design must achieve compliance with the identified noise criteria and suitably implement any noise mitigation requirements.</p> <p>In accordance with the conditions of Development Approval for a development:</p> <p>An Acoustic Report is to be submitted to the City for approval PRIOR to submission of the Building Permit Application. This Acoustic Report is to detail final design specifications and construction methods with which the proposed development will ensure compliance with the relevant noise criteria.</p>
Building Permit Application	<p>Building Permit Applications must clearly demonstrate compliance with all relevant Planning requirements.</p> <ul style="list-style-type: none"> • Where a Building Permit Application is required to

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



Development Stage	Acoustic Reporting Requirements
	<p>comply with a Local Development Plan, the plans must clearly demonstrate that all Noise Management requirements will be incorporated into the structure of the dwelling (for instance specific window suite construction and glazing). Where air conditioning or mechanical ventilation is required, installation details suitable to demonstrate compliance with the noise mitigation requirements must be included on the Building Permit plans.</p> <ul style="list-style-type: none"> Where a Building Permit Application is required to comply with conditions of Development Approval, all relevant documents specified by those condition(s) must be submitted and all relevant conditions complied with. This may include a requirement for written confirmation from a recognised acoustic consultant to that the plans have been checked and confirmed as compliant with the requirements of the relevant Acoustic Reports.
Building Occupancy or Strata Approval Certificates	<p>Upon completion of construction and in accordance with conditions of Development Approval, a Form BA7 Notice of Completion or Form BA15 Building Approval Certificate Strata must be accompanied by written confirmation from the Builder to demonstrate that all requirements of the relevant Acoustic Report(s) were incorporated into the completed development.</p> <p>Where deemed appropriate, the City may require the builder to provide with the FormBA7 Notice of Completion written confirmation from an acoustic consultant that the building complies with the requirements of the acoustic report or that a final acoustic assessment of the finished development has been undertaken.</p>
<p><i>Important note:</i> All buildings and structures are to comply with the relevant fire rating, safety, BAL and National Construction Code requirements. Noise attenuation methods shall not compromise the safety or structural integrity of the development.</p>	

3.0 What criteria should the Acoustic Report address?

The following minimum standards shall be applied as the relevant reference criteria upon which the Acoustic Report is to be prepared:

3.1 Noise Intrusion (Noise Sensitive Development)

Noise sensitive developments are to be designed to achieve the following sound levels:

[5]

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



- (a) Leq 35 dB(A) in sleeping areas (bedrooms); and
- (b) Leq 40 dB(A) in living/work areas and other habitable rooms.

For all other developments, noise intrusion is to be controlled to achieve the indoor design sound levels for buildings as set out in Australian Standard AS/NZS2107: “Acoustics - Recommended Design Sound Levels and Reverberation Times for Building Interiors”.

For noise sensitive developments within the trigger distance of future or existing road and rail infrastructure, the report must address the requirements of SPP 5.4.

Important note about low frequency noise:

The Leq level should not be unduly biased toward the lower frequencies of the octave band spectrum. If lower frequencies are dominant in sound levels taken during the sampling phase of reporting (below 200Hz or a 15-20dB difference between LA and LC levels), the Acoustic Consultant shall discuss the findings with the City in developing appropriate solutions to ensure that low frequency noise is appropriately attenuated, prior to the submission of the final Acoustic Report.

3.2 Vibration Intrusion (Noise Sensitive Development)

For noise sensitive developments in close proximity to rail infrastructure or other sources of vibration, the Acoustic Report must address ground-borne vibration levels to ensure that occupants of the development are not exposed to an unacceptable level of vibration.

Noise sensitive developments are to be designed to achieve the following vibration levels, as described in Table 2 of Appendix A of AS 2670.2-1990 “*Evaluation of human exposure to whole-body vibration; Part 2: Continuous and shock-induced vibration in buildings (1 to 80 Hz)*”:

- (a) Infill development of existing residential zoned areas: 1.4 - 2.0 times the base curve (or the equivalent); or
- (b) North Coogee (including South Beach, Port Coogee and Cockburn Coast) as well as other new rezoning or subdivision of noise sensitive development: 1.4 times the base curve (or the equivalent);

3.3 Noise Emissions (All Developments)

Noise emissions from all developments including freight handling facilities and new infrastructure, is to comply with the “Assigned Levels” as detailed in the EPNRs.

New road or rail infrastructure, must also address the relevant requirements of SPP 5.4.

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



3.4 What will happen if the Acoustic Report submitted does not follow these Guidelines?

It is likely that delays will result in the Application Process (Development Application or Building Permit Application) until an amended report or supplementary information to the City's satisfaction has been submitted, or the City may refuse your Development or Building Permit Application.

Important note:

Acoustic Reports may be submitted by the City to the Department of Water and Environmental Regulation Noise Branch for comment or to another Acoustic Consultant for peer review, at the City's discretion.

4.0 Noise Source Identification

Acoustic reports must identify all noise sources relevant to a development, including those which may require detailed assessment at a later stage. Appendix A of this Guideline contains a map to assist proponents in identifying noise sources that may affect different areas of the City. These noise sources may include major roads and rail-lines, areas affected by current and future aircraft noise as well as intense mixed use community areas where higher ambient noise levels may be present, now or in the future.

Environmental noise sources that are relevant within the City of Cockburn include the following:

4.1 Noise from Road, Rail and Freight Infrastructure

Major roads and railways contribute to the ambient environmental noise of an area, and can cause health and amenity impacts to noise sensitive receivers. The City can provide road traffic forecast information for major roads within the City as well as providing advice on future roads that may be proposed in the vicinity of a proposed development.

Under SPP 5.4 Road and Rail Noise (September 2019), a proponent may choose to submit either a Noise Exposure Forecast Worksheet or an Acoustic Report with their subdivision or development application. Appendix B of this Guideline includes a Noise Exposure Forecast Worksheet template based on the requirements of SPP 5.4, which may be completed by a person other than an acoustic consultant using information from the City's Intramaps website as well as using the Department of Planning Lands and Heritage Plan WA website.

In relation to Acoustic Reports for freight rail, the report must describe a representative sample of trains. Where ground borne vibration or low frequency noise are identified as a concern (such as in association with the freight rail lines) the Acoustic Report must clearly identify low frequency noise, ground borne and air borne vibration levels (see Section 4.6).

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



Final details of proposed noise and vibration attenuation measures must be provided to the satisfaction of the City either:

- (a) PRIOR to the submission of the Building Permit Application for Development Applications; or
- (b) PRIOR to the determination of the Structure Plan, Subdivision Application or Subdivision Clearance.

To protect the residential amenity of noise sensitive development in the vicinity of the Spearwood Crossing Loop, noise sensitive development in this area must be designed to attenuate received noise levels from the idling train, to achieve indoor compliance with the EPNRs.

Notifications on title will be required as a condition of development or subdivision approval (including strata subdivision) to inform future residents of the existence of future ambient noise levels that may affect amenity of residential premises. A Notification under Section Section 165 of the Planning and Development Act 2005 is to be placed on the Certificate(s) of Title of the proposed lot(s) / subject lot(s) [DELETE AS APPLICABLE]. Notice of this Notification is to be included on the diagram or plan of survey (Deposited Plan).

The Notification is to state words to the effect:

'This lot or dwelling is in the vicinity of an operating freight rail line servicing the Ports and industrial areas of Fremantle and Kwinana and operates 24 hours a day, 7 days a week. Residential amenity may be affected by noise and vibration and other impacts from freight rail traffic using the rail line'

Important note:

In relation to noise from freight rail lines, State Planning Policy 5.4 requires compliance with a LAeq(night) target of 50dB(A) for the 8 hours period between 10pm and 6am. It is however strongly recommended that a more conservative assessment method be considered, such as the LAeq(night) noise emission of a single representative train pass (LAeq(120sec) or similar) or consideration of the LAmx levels as proposed by the Freight and Logistics Council's Bulletin No 7 (dated October 2015) to ensure that residential amenity is adequately protected, particularly with regard to sleeping areas.

4.2 Vibration from Rail and Freight Infrastructure

For noise sensitive developments in close proximity (within 80m) of rail infrastructure or other sources of vibration, an Acoustic Report should also assess ground-borne vibration levels to ensure that occupants of the development are not exposed to an adverse amenity impact.

An Acoustic Report assessing vibration should make reference to the criteria described in Section 3.2 based on monitoring of trains over a three to four day period necessary to ensure a 'representative' number of train

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



movements have been observed. The number of trains observed must be sufficient to obtain an acceptable level of repeatability, particularly during periods of low traffic and may require the monitoring equipment to be left for a longer period.

The Acoustic Report is to address the vibration levels of each train during the monitoring period and address compliance with either:

- (a) Australian Standard 2670.2-1990 "Evaluation of human exposure to whole-body vibration; Part 2: Continuous and shock induced vibration in buildings (1 to 80 Hz)";
- (b) ISO 2631-2:2003 "Evaluation of human exposure to whole-body vibration Part 2: Vibration in buildings (1 Hz to 80 Hz)"; or
- (c) British Standard BS6472-2008: "Evaluation of Human Exposure to Vibration in Buildings (1 Hz to 80 Hz)".

4.3 Mechanical Plant and Equipment

Applicants for activities which may be a source of noise, are to detail anticipated noise levels from increased vehicular traffic as a result of the development (truck movements, loading activities, car doors slamming etc) as part of the development application. Where noise from an activity exceeds, or significantly contributes to, a noise level that exceeds the assigned levels, an Acoustic Report must be provided demonstrating how noise levels will be suitably attenuated and any potential amenity impact minimised.

With relation to Mechanical Plant and Equipment noise, the Acoustic Report is to comment on:

- (a) the likely impact of mechanical plant and equipment noise sources (such as air conditioners, traffic gates, exhaust systems);
- (b) practical building design solutions that could be implemented to minimise the impact of existing mechanical plant and equipment noise sources; and
- (c) the proposed location of all mechanical plant and equipment at the development site; and provide details of how noise emissions from these sources will be contained, so as not to create unwanted sound at nearby properties.

Important note:

Noise from mechanical equipment used in emergency situations which are an integral part of the development (for instance fire pumps, smoke exhaust fans and the like) must comply with the assigned levels for routine testing and maintenance purposes.

Where it is determined that compliance may not be achieved, the Acoustic Consultant shall discuss the findings with the City to develop appropriate solutions and must be able to demonstrate that all practicable and

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



reasonable measures will be implemented to minimise the noise impact onto nearby receivers, prior to the submission of the final Acoustic Report.

4.4 Co-existing Land Uses

Whilst it is important to ensure that a proposed development is designed to reasonably negate external/existing noise sources from impacting on noise sensitive areas within the proposed development, it is equally important that a development is designed to ensure that internal noise sources do not impact on surrounding existing or future land uses.

In some instances, pre-existing non-conforming uses may remain in close proximity to proposed noise sensitive development. Where a noise sensitive development is proposed in the vicinity of a pre-existing commercial or industrial land use, the Applicant must provide an Acoustic Report demonstrating noise attenuation suitable address any adverse amenity impact in accordance with these Guidelines.

4.5 Noise Sensitive Developments in Proximity to Jandakot Airport

The Jandakot Airport is the principal general aviation airport in Western Australia. Aircraft training movements can have a substantial effect on the amenity of surrounding noise sensitive land uses, due to the frequency of noise events and the inability to screen aircraft noise emissions from reaching affected parties on the ground.

Development in the 20ANEF and 25 ANEF Contours for Jandakot Airport (as identified by the Jandakot Airport Master Plan)

Applications for noise sensitive development or subdivision will require:

- Support by an Acoustic Report demonstrating compliance within SPP 5.3 and AS2021:2015 “Acoustics – Aircraft Noise Intrusion - Building Siting and Construction”.

Applications for other development will require:

- Support by an Acoustic Report demonstrating compliance with the Indoor Design Sound Levels identified in Appendix 2 of SPP 5.3

Appendix A of this Guideline shows the Frame Area within the City of Cockburn, identified by SPP 5.3. Applications for noise sensitive development or subdivision within the Frame Area for Jandakot Airport are required to provide 6.38mm laminated glass to all habitable rooms (including kitchens), in accordance with the requirements of the Western Australian Planning Commission publication “Aircraft Noise Insulation for Residential Development in the Vicinity of Perth Airport” 2004.

Appropriate conditions of approval will be applied or recommended to ensure that noise management requirements within the 20 ANEF and 25 ANEF

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



contours and Frame areas. For subdivision this may necessitate a Local Development Plan to implement façade treatment to future noise sensitive development.

A condition requiring the Applicant to place a Notification under Section 165 of the Planning and Development Act on the Certificate of Title will be required for all noise sensitive development sites subject of a subdivision or development application within the 20 ANEF and 25 ANEF contours and Frame areas. The wording of the Notification must be consistent with the wording provided in SPP 5.3.

Important note:

In relation to noise from general aviation aircraft movements from Jandakot Airport, much of the Frame area falls within the area identified by the “Noise Above Contours” identified by the Jandakot Airport Master Plan corresponding with:

- (a) 20 or more daily events greater than 70 dB(A);
- (b) 50 or more daily events of greater than 65 dB(A);
- (c) 100 events or more daily events of greater than 60 dB(A); or
- (d) 6 or more events of greater than 60 dB(A) between the hours of 11pm and 6am

4.6 Breakout and Street Noise

Developments in the vicinity of lifestyle uses such as marinas, cafes, restaurants, hotels and entertainment venues such as small bars, are integral to the vitality and enjoyment of town centre and intense mixed use community areas. Outdoor eating areas associated with hotels, cafés and restaurants can also be a source of breakout noise.

Urban locations containing a mix of lifestyle uses contribute to the vitality of an area and are characterised by increased levels of pedestrian activity.

Noise associated with these uses may include:

- Movement to and from different establishments,
- Persons getting into and out of parked cars, and
- Occasional anti-social behaviour.

The above can contribute significantly to general ambient environmental noise (particularly when this occurs late at night), impacting on the amenity of residential premises.

The Applicant is to ensure that the Acoustic Report:

- (a) Identifies breakout noise sources that impact on sound levels received at the subject property, and in particular those noise sources within a 100m radius;

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



- (b) Provides comment on the likely amenity impact of all localised street noise sources (factors such as proximity to local business districts, late night venues, setbacks from footpaths and proximity to schools which may be adversely affected by noise emissions shall be considered); and
- (c) Specifies sound levels recorded at the property during representative time periods (corresponding with related breakout noise sources or peak periods of street noise; for example):
 - i. Nightclubs to be monitored during hours of operation after 10.00pm at night on a Friday or Saturday;
 - ii. Cafes and Outdoor Eating Areas to be monitored during peak trade periods;
 - iii. Street and Background Noise to be monitored during peak periods; or
 - iv. Locations near sports grounds and stadiums are to be monitored during periods of normal use; particularly when used at capacity (where practicable);
- (d) Addresses the character of noise sources in terms of frequency analysis, and in particular identifies whether low frequency noise is present (minimum of octave bands to be provided); and
- (e) Thereafter, provides comment on practical building design solutions to minimise the impact of such noise sources.

For noise sensitive land uses, mixed use or multistorey residential developments within intense mixed use community areas such as Cockburn Coast, Port Coogee, Muriel Court, Cockburn Central Town Centre and Cockburn Central West (as identified in Appendix A), where potential sources of break out noise are not yet present, the applicant is to liaise with the City for guidance on determining potential noise sources, potential street and break out noise levels for the modelling of potential noise impacts.

Notifications on title will be required as a condition of development or subdivision approval (including strata subdivision) to inform future residents of the existence of future ambient noise levels that may affect amenity of residential premises. A Notification under Section 70A of the Transfer of Land Act with wording to the effect:

“This land is located within the <insert description here> town centre. The vision for this town centre is that of a vibrant mixed use area. Residential amenity, therefore, may be affected by the mixed use nature of the location, including noise and other impacts. Further information is available on request from the City of Cockburn.”

Important note:

Standardised theoretical sound level limits should not be used for calculation purposes, unless such use is absolutely necessary due to noise sources (to be modelled) occurring so infrequently that performing measurements on-site is impractical, or where potential noise sources are not yet present

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



Table 2: Acceptable Vibration Treatment Options

<u>Development Area</u>	<u>Vibration level</u>	<u>Potential Treatment Responses</u>
North Coogee and future rezoning or subdivision of noise sensitive development in proximity to the freight rail line	Curve 1.4 and below	No vibration treatment or notification required. Notification on Title as described in Section 4.2. Acoustic treatment to mitigate vibration to be incorporated into the development based upon development specific acoustic advice. Proposed mitigation strategies (for instance mitigation trenches, vibration isolation of the building structure or slab) based on specialised acoustic advice, are to be identified as early as possible during development process.
Infill development of existing residential zoned areas	Curve 1.4 and below	No vibration treatment or notification required.
	Between Curve 1.4 and Curve 2	Notification on Title as described in Section 4.2 No vibration treatment required.
	Above Curve 2	Notification on Title as described in Section 4.2. Acoustic treatment to mitigate vibration to be incorporated into the development based upon development specific acoustic advice. Proposed mitigation strategies (for instance mitigation trenches, vibration isolation of the building structure or slab) based on specialised acoustic advice, are to be identified as early as possible during development process.

Final details of proposed vibration attenuation measures must be provided to the satisfaction of the City either:

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



- (a) PRIOR to the submission of the Building Permit Application for Development Applications; or
- (b) PRIOR to the determination of the Structure Plan or Subdivision Clearance.

Where ground borne vibration or low frequency noise are identified as a concern Acoustic Report must clearly identify low frequency noise, ground borne vibration and air borne vibration levels.

5.0 Noise Treatment Solutions

The intent of noise treatments is to provide appropriate opportunities for rest and respite within noise sensitive premises (where people rest and sleep) and to allow a multitude of diverse land uses to co-exist.

5.1 Development Planning and Design

Once all relevant noise sources have been identified, it will be possible for the design team to consider the use of development design techniques to minimise noise intrusion and emission. Seeking the advice of an acoustic consultant at the earliest planning stage provides greater opportunity to consider noise attenuation options, which can minimise or reduce the construction costs.

When considering the land use planning strategies, the following hierarchy of controls is suggested:

- (a) Increasing the distance between noise sources and noises sensitive premises;
- (b) Using zoning mechanisms to limit the potential for land use conflict;
- (c) The use of buildings, barriers, walls and bunds as shields or similar acoustic design principals;
- (d) The use of quiet house design.

Where final lot levels have not been determined at the time of a Subdivision Application, a draft Acoustic Report based on estimated or existing lot levels must be provided to demonstrate that compliance with the relevant requirements can be achieved.

To effectively implement an Acoustic Report, a Local Development Plan may be necessary to control construction materials and methods for individual lots. Arrangements should be made to ensure that prospective purchasers are made aware of noise attenuation requirements.

The Applicant is to ensure that the Acoustic Consultant's recommendations have been incorporated into the development design and documentation.

5.2 Construction Methods and Materials

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



Prior to submission of the Building Permit Application, an Acoustic Report detailing all relevant sources of noise, and the final design specifications and structural methods by which noise will be attenuated must be submitted to the City.

Important Note:

All buildings and structures are to comply with the relevant fire rating and noise attenuation requirements as set out in the National Construction Code (as amended) and the Building Code of Australia (as amended). Acoustic attenuation methods shall not compromise the safety or structural integrity of the development.

Noise Insulation (Protection against noise intrusion)

- (a) Windows: consider window size and location; selection of fixed panels or operable windows; use of heavyweight or thicker glass, double glazing and laminated glazing; window frame type and window seal construction; and specific acoustic performance requirements.
- (b) Walls: consider construction/product detail for walls requiring higher acoustic performance (brick, stud, concrete tilt panel, rammed earth); acoustic attenuation for exhaust vents, conduits and piping through walls; and requirements for external doors, including the door construction (solid timber, laminated glazing or double glazing), frame construction and application of door seals.

Important Note:

Where stud wall construction is proposed, evidence should be provided to demonstrate that low frequency sound, regenerated noise, airborne and ground borne vibration will be suitably attenuated. Detail of the recommended construction façade is to demonstrate suitable attenuation where these characteristics are identified, by favouring solutions with high $Rw+Ctr$ acoustic rating in lieu of construction with high Rw only

- (c) Roof and Ceiling: consider the selection of roof; specific acoustic requirements for sealing of roof (such as use of sark or anticon insulation); upgraded acoustic performance for ceilings; closing, sealing or elimination of eaves; and insulation of ceiling voids.
- (d) Ventilation: consider the potential for noise intrusion from ventilation services (such as service ducts or commercial exhaust systems) to enter into noise sensitive areas through walls and ceilings; attenuation of service ducts and all air paths, such as through ducts to the exterior, suitably attenuate low frequency noise and vibration.

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



Noise Emission Mitigation Measures:

The “assigned levels” detailed in the EPRs must be complied with in relation to noise generated/emitted from the premises. Applicants are encouraged to consider design and construction measures that are tailored specific to the noise source for that development.

6.0 Related Legislation and Documents

Related legislation:

The Environmental Protection Act 1986 and the Environmental Protection (Noise) Regulations 1997 (EPNRs) –provide powers for officers of the Department of Environment Regulation, WA Police Service and authorised Environmental Health Officers from Local Governments to deal with noise.

The Liquor Act 1988 – under Section 117 of this legislation the Director Liquor Licensing Division has the power to deal with noise issues and anti-social behaviour.

The Planning and Development Act 2005 (P&D Act) - this legislation provides the determining authority with the power to impose conditions on Development Approvals that protect the amenity of an area.

The Building Act 2011 - this legislation provides the authority to set building standards and ensure that they are met through the issue of building and occupancy permits to protect the health and safety of a buildings inhabitants.

In addition to the above listed legislation, reference is made in these Guidelines to the following State Planning documents prepared and adopted under the P&D Act:

State Planning Policy 4.1 “State Industrial Buffer Policy”, Gazetted May 1997 (SPP 4.1);

Draft State Planning Policy 4.1 “State Industrial Buffer”, November 2017 (Draft SPP 4.1);

State Planning Policy 5.3 “Land Use Planning in the Vicinity of Jandakot Airport”, 2006 (SPP 5.3);

State Planning Policy 5.4 “Road and Rail Noise”, dated September 2019 (SPP 5.4);

Road and Rail Noise Guidelines, dated September 2019 (the Guidelines);

Western Australian Planning Commission publication “Aircraft Noise Insulation for Residential Development in the Vicinity of Perth Airport” 2004.

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



In relation to Federal Aviation requirements, reference is also made to the following documents prepared in accordance with the requirements of the Federal Department of Infrastructure and Regional Development:

National Airports Safeguarding Framework;

Jandakot Airport Master Plan and references to the “Noise Above Contours” identified by the Master Plan.

7.0 Definitions

“**Acoustic Consultant**” is the term used to describe a specialist consultant with expertise in the science of acoustics, noise, and/or vibration. This highly technical field is further divided into broad areas of expertise. See also Section 2.1 - Who can help me with preparing an Acoustic Report?

“**Acoustic Report**” a specialist report prepared by an Acoustic Consultant addressing noise and vibration emissions from or received by a development or site.

An Acoustic Report may take the form of:

- A “Noise Impact Assessment”- a report that assesses the noise impacts of a development with regard to noise emissions or received.
- A “Noise Management Plan”- a Report that demonstrates how the noise levels will be attenuated or otherwise managed for a development to comply with the relevant criteria.

“**Ambient Environmental Noise**” is sound emitted from the full complement of activities and land uses within an area that is not easily discernible and includes a combination of sound from sources such as traffic, wind, rustling trees, wildlife, pets, mechanical equipment and people, including break out noise.

“**Assigned Level**” has that same meaning given to it under the EPNRs.

“**Breakout Noise**” is a clearly distinguishable sound emitted from local land use activities such as outdoor eating areas, public gathering areas and pedestrian routes, pubs, nightclubs and the like.

“**Development**” has the same meaning given to it under the P&D Act being: “the development or use of any land, including —

- any demolition, erection, construction, alteration of or addition to any building or structure on the land;
- the carrying out on the land of any excavation or other works...”

“**Development Application**” has the same meaning given to it under the P&D Act being: “an application under a planning scheme, or under an interim development order, for approval of development.

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



“**Frame Area**” for Jandakot Airport has the same meaning as under SPP 5.3, being the area between the 20ANEF contour and Roe Highway, Ranford Road, Warton Road, Armadale Road and Kwinana Freeway, which may have a reduced level of residential amenity due to aircraft noise. The Frame Area is shown in Appendix A.

“**Noise**” is defined as unwanted sound or vibration.

“**Noise Exposure Forecast**” is a brief initial assessment conducted under SPP 5.4 to demonstrate whether outdoor noise levels from a transport source, , will comply with the Target Noise Levels when received at a development site.

“**Noise-Sensitive Land Use**” and “Noise-Sensitive Development” have the same meaning as under SPP 5.4 and the EPNRs, being premises occupied solely or mainly for residential or accommodation purposes, rural premises and premises including (but not limited to):

- (a) a caravan park or camping ground, a tavern, hotel or other premises that provide accommodation for the public;
- (b) a hospital, aged care facility, home or institution for the care of persons;
- (c) an education facility;
- (d) child care facility; and
- (e) places of worship.

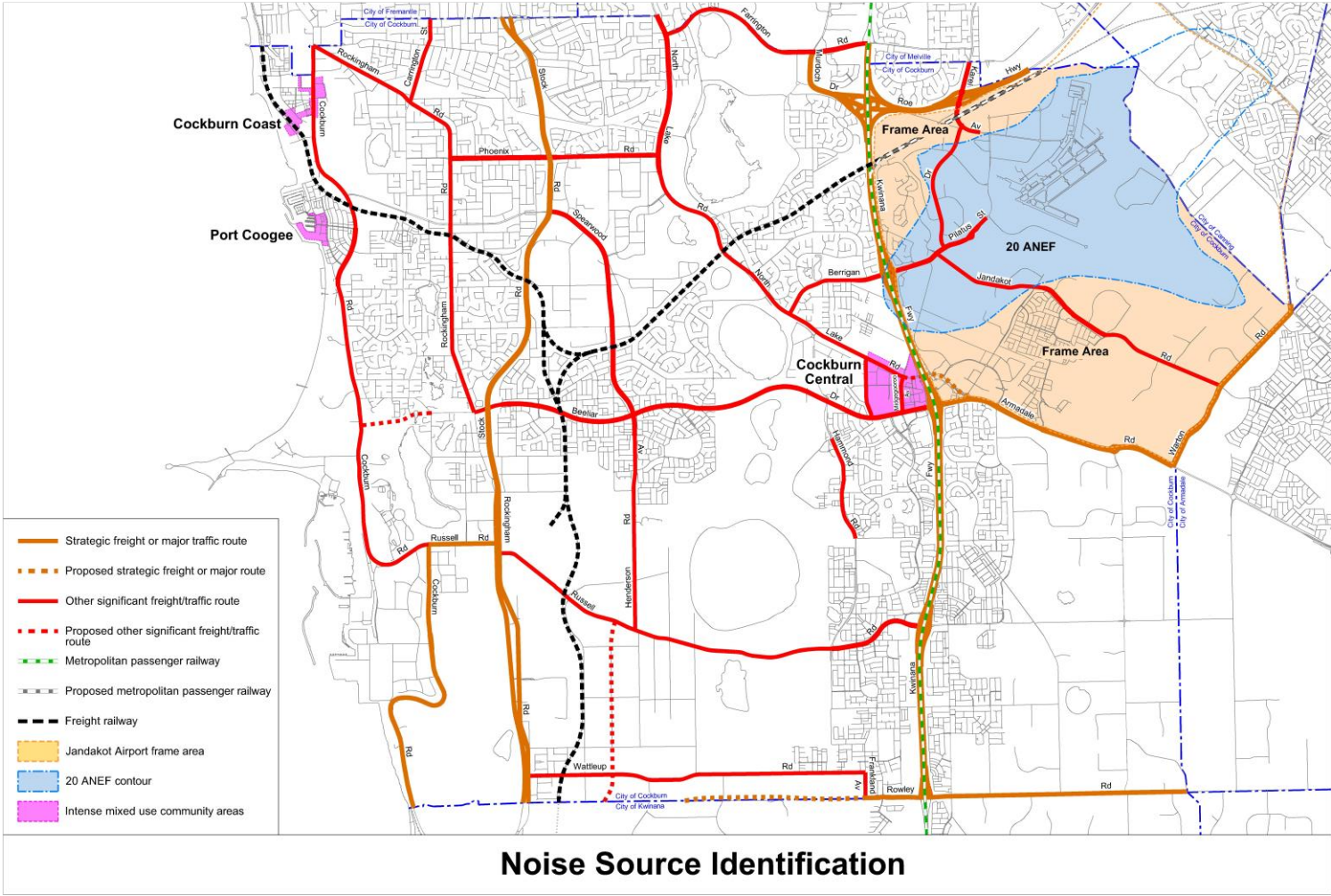
“**Noise Target**” is the level of noise set by Table 2 in SPP 5.4 as an acceptable level of noise for development in proximity to road and rail infrastructure. If a development takes place in an area where outdoor noise levels meet the Noise Target, no attenuation measures are required.

“**Residential development**” has the same meaning as in the Residential Design Codes, being development of permanent accommodation for people, and may include all dwellings, the residential component of mixed-use development, and residential buildings proposing permanent accommodation.

Title	Attachment 3 Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



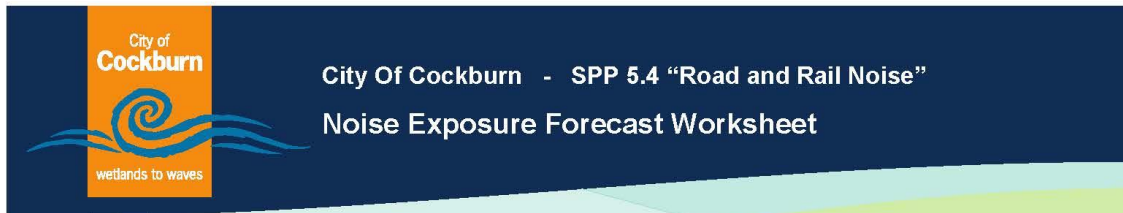
8.0 Appendix A – Noise Source Identification Map



Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



9.0 Appendix B – Noise Exposure Forecast Worksheet



SPP 5.4 "Road and Rail Noise" (September 2019) is intended to protect noise sensitive land uses from the adverse health and amenity impacts of road and rail noise. An assessment against SPP 5.4 is required where any part of the lot is within the specified trigger distance. This assessment may be in the form of a Noise Exposure Forecast, as described in the Road and Rail Noise Guidelines (the Guidelines).

The City has prepared a **Noise Exposure Forecast Worksheet** to assist developers of noise sensitive development within the trigger distances to provide the information required by the City to assess the application and to assist applicants to determine whether a development specific acoustic report is required.

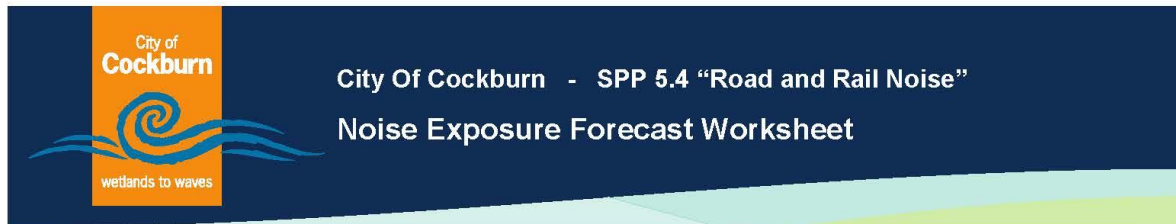
Applicant's Details:		
Applicant's Name:	Phone:	
Contact Person:	Mobile:	
Email:	ABN:	
Mail Address:	Street address	
	Suburb	Post code
Development Details:		
Property Address:	Street address	
	Suburb	Post code
Description of development:		

To complete the Worksheet overleaf, you will need to consider all road and rail corridors within 200-300 metre of the development site. You can determine whether the road or rail corridor needs to be considered by reviewing the property location on the:

1. Department of Planning, Lands and Heritage's Plan WA website at www.planning.wa.gov.au – use the SPP theme and select the 5.4 layer to view the strategic road and rail corridor trigger distances; and
2. the City of Cockburn's Intramaps Online Mapping System at www.cockburn.wa.gov.au - use the Planning-Zoning Module to locate other significant transport routes in the vicinity of the proposed development.

This information is used to complete the Noise Exposure Forecast Summary overleaf.

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



Part A: Noise Exposure Level			
Complete a screening assessment of noise levels for the proposed development below using Table 2 of the Guidelines:			
List below the relevant road transport corridor(s)	Distance between development and transport corridor	Number of lanes on road (both directions)	Noise level
	metres	Lanes	dB
	metres	Lanes	dB
<u>Adjustment up where multiple roads:</u>			
<ul style="list-style-type: none"> Where dB levels for each road is equal to or within 1dB of one another, add 3dB to the highest noise level Where there is a 2 or 3dB difference between noise levels, add 2 dB to the highest noise level 			dB
<u>Adjustment down for shielded single storey development:</u>			
<ul style="list-style-type: none"> Where screening development of a similar height / scale covers more than 50% of the noise exposed frontage, reduce the noise level by 4dB 			dB
Final Noise Level			dB

Part B: Noise Exposure Category		
tick <input checked="" type="checkbox"/> appropriate box		
<input type="checkbox"/> Package A	<input type="checkbox"/> Package B	<input type="checkbox"/> Package C
<input type="checkbox"/> Package A + (freight rail only)	<input type="checkbox"/> Package B + (freight rail only)	<input type="checkbox"/> Package C + (freight rail only)
NOTE:		
<ul style="list-style-type: none"> Where the Exposure Category is D or E, noise-sensitive land-use or development is not recommended under SPP 5.4. Larger or more complex developments, or noise sensitive subdivisions directly abutting a strategic road and rail corridors, may require a development specific acoustic report to be prepared. This assessment does not address noise mitigation for emissions associated with the development. These will need to be subject of a development specific acoustic report. 		

Declaration:

I, the person completing this Noise Exposure Forecast Worksheet understand that:

- Quiet House Design treatments, noise barriers and other controls identified in the Road and Rail Noise Guidelines for this Noise Exposure Category will be applied as Conditions of Approval for this development; and
- Implementation of the relevant Quiet House Design treatments for a subdivision application will necessitate a Local Development Plan to implement the requirements of SPP 5.4.

Signature of applicant: _____ Date: _____

In the case of a company, the signing officer must state position in the company.

Contact details:

City of Cockburn | 9 Coleville Crescent Spearwood | PO BOX 1215, Bibra Lake DC WA 6965 | T: 08 9411 3444

Title	Noise Attenuation
Policy Number (Governance Purpose)	LPP 1.12



Strategic Link:	Town Planning Scheme No. 3
Category	Planning - Town Planning & Development
Lead Business Unit:	Statutory Planning
Public Consultation: (Yes or No)	Yes
Adoption Date: (Governance Purpose Only)	10 November 2022
Next Review Due: (Governance Purpose Only)	November 2024
ECM Doc Set ID: (Governance Purpose Only)	4518974

Title	Waste Management
Policy Number (Governance Purpose)	LPP 1.14



Attachment 4 Waste Management

Policy Type

Local Planning Policy

Policy Purpose

A considerable volume of waste is generated by large-scale grouped and multiple dwellings, Non-Residential developments and Mixed-Use developments. To ensure developments are functional, and for the purpose of protecting the amenity of a locality, on-site management and collection of waste requires due consideration at the subdivision and development application stages. Attention also needs to be paid to the issue of waste minimisation during the construction stage of a development (reducing landfill volumes), and the potential for resource recovery across the life of a project and building occupancy in perpetuity.

This policy details requirements pertaining to waste management and minimisation - to be considered in the, design, construction and management of larger developments. The policy is informed in part by the principles and objectives of the City's Sustainability Policy, Waste Management and Education Strategy 2013-2023. The State's Waste Avoidance and Resource Recovery Strategy 2030 and the State Government's Waste Avoidance and Resource Recovery Act.

The purpose of this policy is to:

- (1) Provide standards and/or requirements aimed at minimising waste whilst achieving higher levels of resource recovery across the life of a development. This includes: pre-development stages in the process including demolition and remediation (if applicable), the construction stage and development occupancy;
- (2) Achieve development that is more functionally designed and effectively managed in terms of waste management and resource recovery for the benefit of a range of stakeholders, including: the developer, property owners and/or development occupants, property owners in the vicinity, waste collecting personnel/contractors, and waste management facilities/businesses;
- (3) Provide for the safer, more hygienic handling (storage), collection and sorting of waste and recyclables by residents and employees, and those working in the waste management industry;
- (4) Provide for waste management and minimisation in a manner that protects the environment, including: the prevention of both 'on' and 'off' site pollution, with a greater emphasis on Ecologically Sustainable Development (ESD), notably higher levels of resource recovery and increased recycling (contributing to a reduction in use of natural resources);
- (5) Provide for waste management and minimisation in a manner that protects the environment, including: the prevention of both 'on' and 'off' site pollution, with a greater emphasis on Ecologically Sustainable Development (ESD), notably higher

Title	Waste Management
Policy Number (Governance Purpose)	LPP 1.14



Attachment 4 Waste Management

levels of resource recovery and increased recycling (contributing to a reduction in use of natural resources);

Policy Statement

(1) At the Subdivision Application stage:

1. The City will seek waste management information, where appropriate and supported by the Department of Planning, Lands and Heritage. Measures may include:
 - a) Use of communal bin storage, identified at the point of collection;
 - b) Conditions of subdivision that support waste management; and
 - c) A Waste Management Plan (WMP), where considered necessary.

(2) At the Development application stage:

A WMP is to be submitted and approved by the City for the following development proposals:

1. Residential Development
 - (a) Grouped dwelling proposals where simple bin presentation for road-side collection is not possible or desirable as determined by City;
 - (b) Four or more grouped or multiple dwellings;
 - (c) Aged or dependant persons developments comprising more than 10 beds/dwellings or where simple bin presentation for road-side collection is not possible or desirable as determined by the City; and
 - (d) Lodging houses.

It is noted that (particularly in the case of grouped dwellings), development applications may be submitted for individual lots rather than for the development as a whole. In this case, the first development application to be submitted will be required to provide the WMP, and this WMP shall provide scope for modification to include the additional lots when they reach the development application stage. This requirement is only applicable where a WMP was not previously prepared at earlier planning stages.

2. Mixed Use Development (comprising both Residential and Non-Residential).
3. Non-Residential Development
 - (a) All types of new non-residential development where waste is generated, including commercial (office, showroom, warehouse), industrial (all types), retail (shops) and food and beverage type establishments (cafes, restaurants etc).
 - (b) Change of Use proposals where significant waste is expected to be generated, as determined by the City.

Title	Waste Management
Policy Number (Governance Purpose)	LPP 1.14



Attachment 4 Waste Management

(3) Information required for WMPs:

1. In the case of demolition and/or remediation:
 - (a) Site plan showing on-site materials storage areas, collection points and vehicular access/egress arrangements;
 - (b) Details of materials and quantities thereof to be re-used (on-site), recycled or to be sent to landfill;
 - (c) Measures to be implemented for the purpose of minimising the delivery of waste generally to landfill.

2. In the case of all new development:
 - (a) Waste minimisation measures during construction, including: the separation on-site of like materials for re-use or recycling, nominated on-site collection points, and the requirements/expectations of the builder and sub-contractors regarding waste.
 - (b) Waste minimisation measures during occupation, including: the on-site separation of materials for recycling, on-site collection points, and the expectations of owners and/or occupants/tenants.
 - (c) A Site Plan identifying:
 - i. Location and size of the bin store area;
 - ii. In the case of on-site collection – collection points, pedestrian/vehicular access to these points, and swept paths; and
 - iii. In the case of road-side collection – location of the bin pad area.
 - (d) An estimation of waste volume likely to be generated when the development is occupied, including the potential for an increase in volume (due to an increase in the density of a residential development or a change of use in non-residential development). In this regard, the City provides a weekly recycling service. Where four (4) or more dwellings are proposed the City requires a reduction in the bin numbers to 1 set (1 yellow-top recycling and 1 general waste red top) of bins/3 residential units managed by a caretaker or equivalent.
 - (e) Details of the intended method of collection and associated equipment i.e. by the City of Cockburn and/or private contractor ensuring servicing bins can be completed by the waste truck without reversing, time and management of collection i.e. by individuals, strata managers and/or caretaker/s, cleaning and maintenance of on-site waste management areas etc;
 - (f) Details of advice to be provided to property owners and/or development occupants (both residential and non-residential) regarding the WMP;
 - (g) Details of any contract/s with cleaners and/or waste contractors, including the requirements of the contract/s;

Title	Waste Management
Policy Number (Governance Purpose)	LPP 1.14



Attachment 4 Waste Management

- (h) Plans to incorporate new waste management technologies and practices in the WMP;
 - (i) Details of how a WMP will continue to be applied in perpetuity across the life of a development, via for example, reference to such in the Strata Management Statement (including details regarding the process for changing a WMP and the City's involvement in such).
3. In the case of Residential Development only:
- (a) On-site storage capable of containing a minimum of one (1) weeks' waste and recycled material within a communal bin store;
 - (b) The provision of a communal bin store that has a minimum size of one (1) square metre per dwelling, with a two (2) metre minimum length and width dimension. This requirement stands regardless of (2(d) above) where the reduction in bin numbers is 1 set of bins/3 residential units or the waste volume estimates;
 - (c) On-site storage being conveniently located to the point of collection, out of public view or suitably screened, and having reasonable separation from residents' outdoor living areas;
 - (d) Suitably located and signed (if required according to the City of Cockburn's specification) road side bin collection pad/s (taking into account pedestrian and vehicular movement across the front of, or into and out of a property, walls, fencing and landscaping at the front of a property, and kerbside parking);
 - (e) Removal of bins from the collection point (internal or road side) on the day of collection.
4. In the case of Mixed Use and Non-Residential Development only:
- (a) On-site storage capable of containing a minimum of one (1) weeks' waste and recycled material;
 - (b) Area of waste storage:
 - i. Office/Showroom/Warehouse: 1m²/200m² Gross Floor Area (GFA);
 - ii. Industrial: 1m²/100m² GFA. Note: waste generated "use" specific and storage capacity to relate to industry type;
 - iii. Retail (shop): 2m²/200m² GFA;
 - iv. Restaurant/Café: 1m²/20 seats, or 1m²/100 meals served;
 - v. Tourist Accommodation (hotel, serviced apartments etc): 1m²/12 rooms.
 - (c) The potential for up to two (2) collections per week taking place at food premises (restaurants/cafes) or premises involved in food processing/production (or similar activities).
 - (d) Premises involved in the processing, retail and/or wholesale of animal products where the waste generated contains 20% by

Title	Waste Management
Policy Number (Governance Purpose)	LPP 1.14



Attachment 4 Waste Management

weight or volume of fish, poultry or meat, and/or generates 50 litres of seafood, poultry or meat waste per day, must refrigerate this waste prior to collection.

(4) General Bin Store and Waste Collection Requirements (applicable to all development):

1. Mandatory (Required)

(a) For bin stores in basements:

- i. Bin stores shall be provided in the basement, part basement or undercroft level of a development when waste is collected from one, or all of these levels
 - ii. Where waste and recyclables are to be collected in the basement level or similar, collection is to take place in the vicinity of the bin store. The bin presentation area or collection point is to be flat, with the travel path between the bin store and collection point/vehicle clear of steps or kerbs. The distance between the bin store and the presentation should be ideally no greater than 10m;
 - iii. Minimum clearance required in the basement, part basement or undercroft levels of a development is 2.4m. This includes clearance to all structural beams, pipe work, services or similar. The City's Waste Services shall be notified prior to any modification to the basement clearances;
 - iv. Access ramps and driveway gradients serving basements, part basements or undercroft areas are to cater for long wheel base vehicles 7.2m in length with a maximum gradient of 1:4.5; and
 - v. The bin store shall be fitted with double doors.
- (b) Minimum driveway width for on-site collection shall be four (4) metres. On-site manoeuvring shall be provided to ensure waste vehicle ingress/egress in forward gear.
- (c) Where a turn-table is to be installed to facilitate forward ingress/egress, the turn-table shall have a minimum 20 tonne capacity.
- (d) Where approval is given for the collection of waste and recyclables from the road (at the pre-application stage, or via the development application process), consideration needs to be given to a 12.5m long truck where access and/or manoeuvrability are difficult or limited.
- (e) Waste storage, management and collection for the Residential component and the Non-Residential component within in Mixed Use developments are to be separated and self-contained/secured (with separate access arrangements). Where waste storage is secured, a compatible key system is necessary to enable access by collecting personnel/contractors. This includes the City of Cockburn where the City is the collector. The City's Waste Services Unit is to be consulted regarding the system prior to installation. All costs associated with the

Title	Waste Management
Policy Number (Governance Purpose)	LPP 1.14



Attachment 4 Waste Management

system are the responsibility of the developer, property owner/s and/or the strata managers.

- (f) A caretaker or strata management representative is to manage waste and recycling to ensure bins are filled consecutively, with only full bins to be presented on collection day.
- (g) All putrescible waste awaiting collection is to be stored in a Mobile Garbage Bin/s (MGB). Alternative storage containers with permanent tight fitting lids and smooth washable internal surfaces may be approved by the City.
- (h) Internal bin stores shall be separately ventilated with a system complying with Australian Standard 1668 (AS1668). The ventilation outlet is not to be in the vicinity of windows or intake vents associated with other ventilation systems.
- (i) For all bin stores, except where less than five (5) dwellings are proposed:
 - i. Bin stores shall have 75mm concrete floors grading to a 100mm industrial floor waste (including a charged 'water-trap' connected to sewer or an approved septic system), with a hose cock to enable bins and/or the enclosure to be washed out. An alternate floor surface may be approved by the City's Waste Manager. The bin store(s) must be a minimum of 2m wide to allow a horizontal bin wash and ensuring the effluent grades to sewer;
 - ii. Bin store internal walls shall be cement rendered (solid and impervious) to enable easy cleaning. Ceilings in bin stores shall be finished with a smooth faced, non-absorbent material capable of being easily cleaned;
 - iii. Bin store walls and ceilings shall be finished or painted in a light colour;
 - iv. Bin stores shall be constructed in a manner that prevents the entry of vermin;
 - v. Bin stores shall be provided with artificial lighting, sensor or switch controlled both internal/external to the room or area. All lighting in open areas is to comply with AS4282-1997 (Control of Obtrusive Outdoor Lighting). This clause does not apply to developments of five dwellings or less unless the bin store is enclosed;
 - vi. External bin stores shall be surrounded by 1.8m high walls/fencing with a self-closing gate; and Bin stores require the following signs and/or information to be displayed:
 - 1. A sign stating "NO STANDING" at the entrance to the room/area.
 - 2. A clearly visible "DANGER" sign in the vicinity of the entrance to the room/area.
 - 3. Standard signage (details available in the City's Resource Recovery Calendar for Domestic Waste

Title	Waste Management
Policy Number (Governance Purpose)	LPP 1.14



Attachment 4 Waste Management

distributed with Rates Notices) explaining waste management and what materials are suitable for recycling (to be posted/erected in all bin stores.

- (j) Any external bin store greater than 20m² is required to be covered to prevent storm water entering the sewer.

2. Preferred (Recommended)

- (a) Double doors should be fitted to bin stores to improve accessibility and bin movement.
- (b) Where applicable in the case of non-residential use or development, waste contract provisions should require the collection and recycling of low/high grade office paper and other office equipment, including: batteries, smoke detectors, fluorescent tubes, computers and televisions from the waste stream.
- (c) The provision of a minimum of 0.5m² per dwelling 'bulky' storage space in residential development.
- (d) A bulky storage or fenced in area in non-residential development. Minimum 8m in volume (m³), suitably signed and made available for the storage of electronic goods.
- (e) As waste vehicles idle for extended periods outside bin stores basements or ground floor carparks, designers should consider the classification and placement of thermal detectors so as not to trigger a false alarm.

(5) Advice

1. Where road side collection is not considered appropriate by the City's Waste Manager due to concerns relating to pedestrian and/or vehicular conflict, and potential impacts on the amenity of a locality, alternate collection methods are to be determined and incorporated in the WMP in consultation with the Waste Manager.
2. Waste chutes are not permitted in Multiple Dwelling or Mixed Use developments.
3. As laneways are designed for the servicing of waste, the City may require residents to place their bins on one side of the laneway to increase the operating space and safe movement of the waste truck

Title	Waste Management
Policy Number (Governance Purpose)	LPP 1.14



Attachment 4 Waste Management

Strategic Link:	Town Planning Scheme No. 3
Category	Planning - Town Planning & Development
Lead Business Unit:	Statutory Planning
Public Consultation: (Yes or No)	Yes
Adoption Date: (Governance Purpose Only)	10 November 2022
Next Review Due: (Governance Purpose Only)	November 2024
ECM Doc Set ID: (Governance Purpose Only)	4517800

Title	Tourist Accommodation
Policy Number (Governance Purpose)	LPP 1.15



Attachment 5 Tourist Accommodation

Policy Type

Local Planning Policy

Policy Purpose

The City of Cockburn is well placed to accommodate tourists with good access to Perth CBD and Fremantle, major transport networks, health and educational facilities, and access to world-class beaches. Tourist and other short-term accommodation proposals are therefore expected to increase within the City and Town Planning Scheme No. 3 (TPS 3) provides limited development standards for such proposals.

The purpose of this policy is to provide development standards for the following land use types as defined in TPS 3:

- Tourist Accommodation (including cabins, chalets, short-stay self-contained accommodation and similar forms of tourist accommodation);
- Bed and Breakfast;
- Hotel;
- Motel;
- Holiday Home (Standard); and
- Caravan Parks.

The policy does not provide development standards for long-stay Park Home, Residential Parks, Lifestyle Villages or Hotels that do not contain accommodation.

Policy Statement

(1) Definitions

Short-term accommodation – accommodation for a maximum of 6 persons, inclusive of a keeper if they reside at the dwelling, for a period of up to 6 months within any 12 month period.

Keeper/Manager – a person who permanently resides at a premises providing short-term accommodation and is responsible for the upkeep and management of the accommodation.

Guest – a person who is accommodated within the short-term accommodation on a temporary basis for a fee, but is not subject to a Residential Tenancy Agreement.

Visitor – A person who is visiting a guest accommodated within short-term accommodation but who is not a guest themselves.

Title	Tourist Accommodation
Policy Number (Governance Purpose)	LPP 1.15



Attachment 5 Tourist Accommodation

(2) Management Plans

All proposals for tourist accommodation and other uses outlined in the purpose section of this policy shall include the lodgement of a management plan which shall address:

1. Control of Noise and other disturbances;
2. Complaints management procedures;
3. Security of guests, visitors and neighbours;
4. Control of anti-social behaviour and potential conflict between tourists/guests/visitors and permanent residents of the area;
5. Car parking for guests and visitors;
6. Number of bedrooms and beds; and
7. Contact details of Owner and Keeper/Manager.

(3) Tourist Accommodation

1. A keeper/manager is not required to reside at the premises whilst it is being used for tourist accommodation purposes;
2. The use of one or more existing multiple or grouped dwellings within a complex for the purposes of tourist accommodation shall generally not be supported.
3. Proposals including a mix of uses including permanent residential and tourist accommodation within the same development should provide sufficient separation of the uses to ensure the amenity of permanent residents is adequately protected.
4. Proposals for tourist accommodation in the form of apartments on residential zoned land shall be designed so that the building height generally accords with the maximum, building height permitted for multiple dwelling development.
5. Proposals for tourist accommodation in the form of apartments shall include an entrance/foyer with a reception desk attended by staff during check-in and check-out times.
6. Car parking for tourist accommodation shall be located on-site and in accordance with the TPS 3 provision.

(4) Hotels and Motels

1. Proposals for hotels providing accommodation and motels shall include an entrance/foyer with a reception desk attended by staff during check-in and check-out times.
2. Car parking for hotels and motels shall be in accordance with the TPS 3 provision.
3. Proposals for motels on residential zoned land shall be designed so that the building height generally accords with the maximum, building height permitted for multiple dwelling development.
4. Proposals including a mix of uses including permanent residential and hotel/motel uses within the same development should provide sufficient separation of the uses to ensure the amenity of permanent residents is adequately protected.

(5) Bed and Breakfast

1. The keeper/manager of the Bed and Breakfast must reside permanently on-site whilst the Bed and Breakfast is operating.

Title	Tourist Accommodation
Policy Number (Governance Purpose)	LPP 1.15



Attachment 5 Tourist Accommodation

2. Access to a dining area and laundry facilities shall be provided for Bed and Breakfast guests.
 3. Parking shall be provided at the rate of 1 car parking bay per guest bedrooms in addition to car bays required for the keeper/manager which shall be in accordance with the Residential Design Codes in residential areas or equivalent in rural zoned areas.
- (6) Holiday Home (Standard)
1. Holiday Homes (Standard) shall be generally located in areas designated as suitable for tourism-type uses. In other areas, it shall be demonstrated that Holiday Homes will not cause conflict with surrounding ordinary dwellings.
 2. A keeper/manager is not required to reside at the premises whilst it is being used for holiday home purposes.
 3. Car Parking for Holiday Homes shall be provided in accordance with the Single House requirements of the Residential Design Codes
- (7) Residential Dwellings
1. Proposals for the use of a portion of a residential dwelling where the owner/occupier is present and where the provision of breakfast is not provided are not considered to be defined as Tourist Accommodation and planning approval is not required.
 2. Proposals for the use of a single house where the owner/occupier is not present whilst it is being used for tourist accommodation or short-stay purposes shall be defined as a Holiday Home (Standard) and requires planning approval.
 3. Proposals for the use of a grouped or multiple dwelling where the owner/occupier is not present whilst it is being used for tourist accommodation or short-stay purposes shall be defined as Tourist Accommodation and requires planning approval. As per the Tourist Accommodation section above, these types of proposals shall generally not be supported.
- (8) Caravan Parks
- Development standards for caravan parks are contained in the Western Australian Planning Commission Planning Bulletin 49/2014 'Caravan Parks' and shall be used by the City in the assessment of these proposals.
- (9) Other Legislative Requirements
- Proposals for some forms of tourist accommodation may be defined as Class 3 buildings under the Building Code of Australia and may require a portion of units to be designed for persons with disabilities, may be required to comply with Australian Standard 1670.1:2015 Fire detection, warning, control and intercom systems - System design, installation and commissioning – Fire. Some proposals may also be required to comply with the City of Cockburn's Health Local Laws relation to Lodging Houses.

Title	Tourist Accommodation
Policy Number (Governance Purpose)	LPP 1.15



Attachment 5 Tourist Accommodation

Strategic Link:	Town Planning Scheme No. 3
Category	Planning - Town Planning & Development
Lead Business Unit:	Statutory Planning
Public Consultation: (Yes or No)	Yes
Adoption Date: (Governance Purpose Only)	10 November 2022
Next Review Due: (Governance Purpose Only)	November 2024
ECM Doc Set ID: (Governance Purpose Only)	5092850

Title	Non Residential Uses in Residential Zones
Policy Number (Governance Purpose)	LPP 1.17



Attachment 6 Non-Residential Uses in Residential Zones

Policy Type

Local Planning Policy

Policy Purpose

There are many uses that are capable of approval under Town Planning Scheme No.3 (TPS 3) in residential zones through the use of discretion which do not consist of a dwelling. These uses include Bed and Breakfast, Childcare Premises, Civic Use, Medical Centres, Consulting Rooms, Educational Establishments, Institutional Buildings, Place of Worship, Bank, Office, Motel, Public Amusement, Reception Centre, Restaurant, Health Studio, Hospital, Convenience Store, Lunch Bar, Home Store, Commercial Vehicle Parking, Industry – Cottage and Hobby Farm.

The above uses, whilst capable of approval are not appropriate in all locations throughout all residential zones and should be strategically located and developed to meet the needs of the community, protect residential amenity and be compatible with the local area.

The purpose of this policy is to provide guidance in assessment of planning applications for development in a residential zone that does not include a dwelling where discretion is required to be exercised in the decision making process. For the purposes of application of this policy, residential zones include the 'Development' zone where a Local Structure Plan identifies the area for residential purposes.

Policy Statement

When considering planning applications for development other than a dwelling in a residential zone (excluding schools, Commercial Vehicle Parking, Home-base businesses, home occupations, Industry-Cottage and Hobby Farm uses), the following should be taken into consideration when determining the suitability of the use:

(1) Location

1. Proposed uses shall generally be located where they abut, are opposite or are in close proximity to (and can be directly viewed from) an existing or proposed Regional, District or Local Centre.
2. Proposed uses shall generally be located within 250m of any public transport bus route or 500m of a train station, measured in a straight line from any part of the route to any part of the lot.
3. Proposed uses shall generally be restricted to 'Primary Distributor' and 'District Distributor' roads and shall generally not be located on 'Regional Distributor', 'Local Distributor' or 'Access' roads.

Title	Non Residential Uses in Residential Zones
Policy Number (Governance Purpose)	LPP 1.17



Attachment 6 Non-Residential Uses in Residential Zones

(2) Built Form

1. The built form of proposals shall be consistent with the height, bulk, scale and intensity of surrounding residential dwellings to ensure that the locality maintains a residential character.
2. Proposed buildings shall address the street by way of major openings, entries and provide high levels of passive surveillance.
3. Building setbacks (street and side) and heights shall be as per the residential code of the land in accordance with the Residential Design Codes of WA.

(3) Vehicle Parking

1. Vehicle parking should generally be contained on-site to avoid the likelihood of street and verge parking associated with the use.
2. Vehicle parking areas should generally be to the rear of the lot and screened from view of neighbouring residential dwellings. Some parking in the front setback of the building may be suitable.

(4) Traffic

1. Applications shall generally be accompanied by a Traffic Impact Study or Assessment to demonstrate that the surrounding road network can accommodate any additional traffic generated by the proposal. Proposals that generate traffic beyond the capacity of the surrounding road network shall generally not be supported.

(5) Noise

1. Applications shall generally be accompanied by a Noise Impact Assessment to demonstrate that the proposed use will not generate an unreasonable level of noise that may negatively impact on the amenity of neighbours.

(6) Landscaping

1. Proposals shall be compatible with the landscape and environmental qualities of the locality.
2. Quality semi-mature Landscaping should be installed to provide screening and buffers to non-residential uses.

(7) Signage

1. Signage for proposals shall be minimal and avoid the use of pylon signs, roof mounted signs, illuminated signs and any other sign that may detract from the amenity of residential neighbours.

Title	Non Residential Uses in Residential Zones
Policy Number (Governance Purpose)	LPP 1.17



Attachment 6 Non-Residential Uses in Residential Zones

2. Signage for proposals shall accord with Council’s Local Planning Policy 3.7 ‘Signage’.

(8) Lighting

1. Lighting of the building and any car parking area shall be designed and constructed to ensure that it complies with AS4282 Control of the obtrusive effects of outdoor lighting and does not detract from the amenity of neighbours.

(9) Waste

1. Waste storage and collection shall be considered as part of the proposal. Any proposed bin storage area shall be designed and located so that it does not detract from the amenity of neighbours.

Strategic Link:	Town Planning Scheme No. 3
Category	Planning - Town Planning & Development
Lead Business Unit:	Statutory Planning
Public Consultation: (Yes or No)	Yes
Adoption Date: (Governance Purpose Only)	10 November 2022
Next Review Due: (Governance Purpose Only)	November 2024
ECM Doc Set ID: (Governance Purpose Only)	7633670

Title	Signs & Advertising
Policy Number (Governance Purpose)	LPP 3.7



Attachment 7 Signs and Advertising

Policy Type

Local Planning Policy

Policy Purpose

This policy should be read in conjunction with the City of Cockburn's Town Planning Scheme No. 3 (TPS 3), any of the City's specific Design Guidelines and Signage Local Laws.

The purpose of this policy is to ensure that the display of signs and advertisements on properties does not adversely impact upon the amenity of the area while providing appropriate exposure of activities or services. The objectives of the policy are:

- (1) To ensure that signage and advertising does not detract from the streetscape or amenity of the area.
- (2) To avoid the proliferation of signage in commercial areas.
- (3) To avoid an abundance of signs on individual sites and buildings.
- (4) To ensure that signs only relate to the services and products on the site.
- (5) To encourage the rationalisation of advertising signs on individual premises.
- (6) To encourage the incorporation of advertising signs into the design consideration of buildings.

Policy Statement

(1) Application

1. This policy applies to all advertisements as defined within *Planning and Development (Local Planning Schemes) Regulations 2015*. All references here within to the words 'sign' or 'signage' shall also be read as advertisement.
2. In accordance with Clause 61 of the *Planning and Development (Local Planning Schemes) Regulations 2015*, development approval is not required where signage meets all of the following:
 - i. Meets the criteria of the 'General Development Provisions' in Part (2) below;
 - ii. Meets the definition and provisions of the 'Permissible Signs and Standards' table in Part (3) below;
 - iii. Is not erected or installed within 1.5m of any part of a crossover or street truncation;
 - iv. Is erected within the boundaries of a freehold lot;
 - v. Is not proposed on land reserved (partially or wholly) under the MRS;
 - vi. The works are not located within a heritage-protected place.

Title	Signs & Advertising
Policy Number (Governance Purpose)	LPP 3.7



Attachment 7 Signs and Advertising

3. Where signage does not meet the criteria set out in Part 1b above, development approval is required. Development applications shall be determined in accordance with clause 67 of *Planning and Development (Local Planning Schemes) Regulations 2015*.
4. Development approval is required for all signage which meets the expectations of the table 'Signage that Requires Development Approval' in Part (4) below. The 'expectations' column of the table provides guidance on what the City would generally accept in relation to each type of sign, however an assessment against clause 67 of the *Planning and Development (Local Planning Schemes) Regulations 2015* is still required.
5. Where assessing an application for development approval the City may, at its discretion, request the provision of a Signage Strategy detailing how signage is to be integrated into the overall development on the site.
6. Where a property is subject to specific Design Guidelines which contain provisions inconsistent with this policy, the Design Guidelines shall prevail.
7. Signs proposed on land reserved (partially or wholly) under the MRS shall be referred to the relevant Public Authority in accordance with the relevant WAPC Instrument of Delegation.
8. The maximum signage area contained in the policy refers to an aggregate of signage.

(2) General Development Provisions

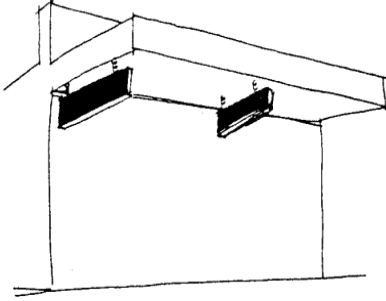
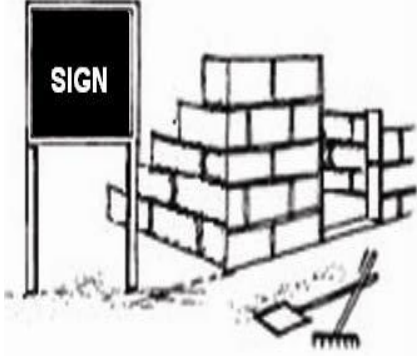

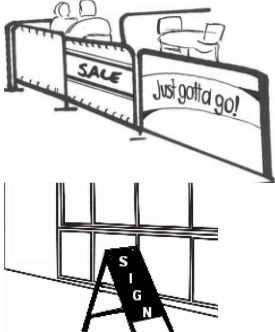
1. *All Signs shall:*
 - a) Not pose a threat to public safety or health.
 - b) Only relate to services and products on the subject site.
 - c) Not consist of inappropriate or offensive language or material.
 - d) Not protrude over Council property, including footpaths (unless allowed under a verandah or attached to a fascia) or neighbour's property.
 - e) Be in accordance with the requirements of Australian Standard AS 4282 - 2019 "Control of the Obtrusive Effects of Outdoor Lighting";
 - f) Not comprise flashing, intermittent or running lights;
 - g) Have any boxing or casing in which it is enclosed, constructed of incombustible material;
 - h) Have its electrical installation constructed and maintained to the satisfaction of Western Power Corporation or the appropriate electricity supply authority and in accordance with the S.A.A. Code 3000-1991;
 - i) Be maintained to operate as an illuminated sign where proposed as such.

Title	Signs & Advertising
Policy Number (Governance Purpose)	LPP 3.7



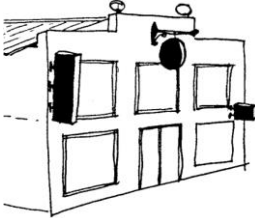
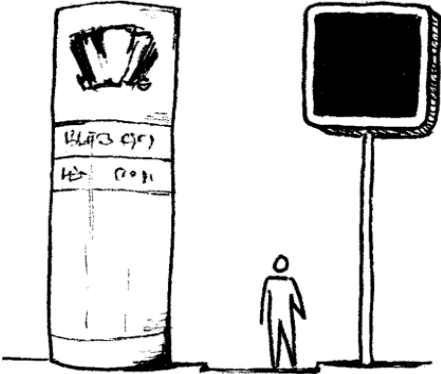


Attachment 7 Signs and Advertising

(3) Standards

	<u>Sign Type</u>	<u>Definition</u>	<u>Provisions</u>
1.	Awning Sign (Below) 	A sign fixed to or suspended from the underside of a verandah, balcony or awning.	a) Maximum size of 6m ² . b) Minimum headway/clearance of 2.75m.
2.	Construction Sign 	A sign which is displayed for the duration of the construction period which displays the project or contractors details relating to the site.	a) Maximum size of 6m ² for single/grouped dwellings. b) Maximum size of 12m ² for multiple dwellings, commercial development not within centre zoned land and industrial development. c) Maximum size of 20m ² for centre zoned development. d) In place for the duration of construction/development works only.
3.	Fence Sign 	Any sign attached to a fence.	a) Maximum size of 6m ² per street frontage. b) Maximum of one sign per street frontage on any one lot.
4.	Ground Based Sign 	A sign which is not attached or otherwise affixed to a building and no portion of which is higher than 1.2m above natural ground level.	a) Maximum size of 6m ² . b) Maximum of one sign per tenancy on a lot. c) Where incorporated in a barrier that distinguishes an outdoor dining area, clause b above is varied to allow up to three signs per tenancy on a lot.

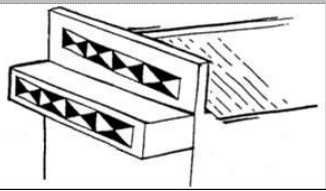



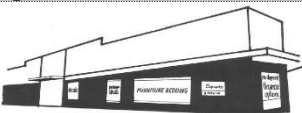
Title	Signs & Advertising
Policy Number (Governance Purpose)	LPP 3.7

Attachment 7 Signs and Advertising

	<u>Sign Type</u>	<u>Definition</u>	<u>Provisions</u>
5.	Projecting Sign 	A sign which projects from a wall of the building below the eaves or ceiling height.	<ul style="list-style-type: none"> a) Maximum size of 6m². b) Minimum headway/clearance of 2.75m. c) Shall not project more than 1m from the wall.
6.	Pylon Sign 	A sign supported by one or more piers and not attached to a building.	<ul style="list-style-type: none"> a) Maximum size of 6m² per street frontage. b) Maximum height of 3m. c) On any one lot, maximum of one sign per frontage, or one sign per 100 metres of frontage; whichever is the greater. d) Setback a minimum distance of half its own overall height to the front property boundary. e) Setback 6m from any other sign erected on the same lot.
7.	Real Estate Sign 	A sign advertising the display, sale or lease of the property.	<ul style="list-style-type: none"> a) Maximum size of 6m². b) Limited to one sign per lot.
8.	Residential Property Sign 	A sign affixed to, or located on, a Residential property, and includes signs relating to Home Occupations and approved Home Businesses.	<ul style="list-style-type: none"> a) Maximum size of 0.2m². b) Maximum height of 0.5m where the sign is a single line of letters fixed to the facade of a building. c) Only used for the purpose of identifying the name of the dwelling and/or nature of a Home Occupation or

Title	Signs & Advertising
Policy Number (Governance Purpose)	LPP 3.7


Attachment 7 Signs and Advertising

	<u>Sign Type</u>	<u>Definition</u>	<u>Provisions</u>
			approved Home Business operating from the dwelling.
9.	Roof Sign 	A sign which is affixed to the fascia or parapet, or forms part of a projection above the eaves of the building.	a) Maximum size of 6m ² . b) Not located within 0.5m of either end of the external walls of the building.
10.	Safety/Warning Signs 	A sign which displays safety or warning messages relating to a potential hazard on or around a site.	a) Necessary to comply with occupational, health and safety requirements.
11.	Temporary Signs (for events) 	A sign which advertises an upcoming event.	a) Maximum size of 6m ² . b) Exhibited up to a maximum of four (4) weeks prior to the event advertised and removed by the end of the day following the event.
12.	Variable Message Sign 	A sign which is permanent or portable and displays electronically generated variable messages.	a) Used by public authorities for public purposes only.
13.	Wall Sign 	A sign which is affixed to the external part of a wall of the building but does not project above the lowest point of the eaves or ceiling of the building.	a) Maximum size of 6m ² . b) Not exceeding 10% in area on any one wall. Note: If the background colour of the sign matches the balance of the colour of the wall on which it is located, then the area of the sign shall be determined by measuring



Title	Signs & Advertising
Policy Number (Governance Purpose)	LPP 3.7



Attachment 7 Signs and Advertising

	<u>Sign Type</u>	<u>Definition</u>	<u>Provisions</u>
			around the words of the sign.
14.	Window Sign	Sign which is affixed to either the interior or exterior of the glazed area of a window.	<ul style="list-style-type: none"> a) Maximum size of 6m². b) Not exceeding more than 25% of the overall area of glazing of the individual façade. c) Lettering shall not exceed 1m in height.
			

(4) Signage that Requires Development Approval

	<u>Sign Type</u>	<u>Definition</u>	<u>Expectations</u>
1.	3D Replica Model or Shape/Object	A three dimensional replica object or shape used as an advertising device and designated to replicate or copy a real world reject or shape. The replica may be freestanding or attached to a building or other sign.	<ul style="list-style-type: none"> a) Maximum volume of 4m³. b) Maximum height of 5m. c) Must not be erected for more than 4 weeks in any 12 month period.
			
2.	Advisory Sign	A sign erected on land which has been approved for subdivision, advertising the lots for sale.	<ul style="list-style-type: none"> a) Does not exceed 20m². b) One sign per subdivision. c) Shall be removed from the site within two years or when 80% of the lots in the subdivision have been sold, whichever comes first. d) A bond received by the City to cover any costs associated to the removal of Advisory signage
			

Title	Signs & Advertising
Policy Number (Governance Purpose)	LPP 3.7



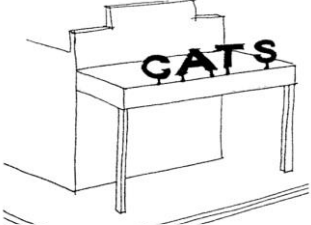
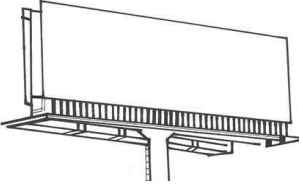
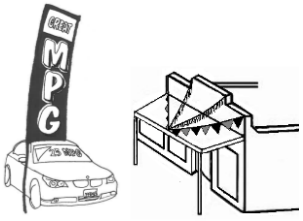

Attachment 7 Signs and Advertising

	<u>Sign Type</u>	<u>Definition</u>	<u>Expectations</u>
			(particularly where signage includes lighting and any water features).
3.	Any sign on a Heritage Place	Any sign located in a heritage-protected place.	<ul style="list-style-type: none"> a) Shall contribute to the significance of a Heritage Place and existing signs should be retained and maintained. b) Shall not dominate a Heritage Place. Instead, new signs should be placed where they would have traditionally been placed, and should be limited to a level consistent with traditional coverage. c) Shall not be located where they would obscure or detract from a feature of the building. d) Shall be located so as to ensure views to individually significant signs are maintained. e) Shall not result in physical damage to any significant fabric. Signage should be readily removable, with careful consideration given in regards to the method of installation, in order to ensure that the original material can be reinstated. f) If included on the State Heritage Register, shall be referred to the Heritage Council of Western Australia for comment. g) Shall not require reproduction of historic advertising styles. Rather,

Title	Signs & Advertising
Policy Number (Governance Purpose)	LPP 3.7




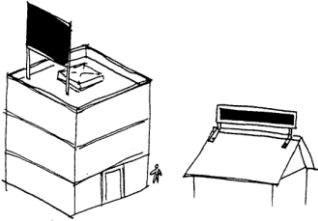
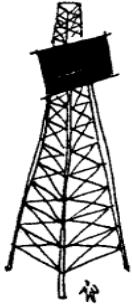
Attachment 7 Signs and Advertising

	<u>Sign Type</u>	<u>Definition</u>	<u>Expectations</u>
			<p>respectful but clearly modern design is preferred for new signs.</p> <p>h) Colours should be appropriate to the Heritage Place and in some instances which may require departure from standard company advertising colours.</p> <p>e)</p>
4.	Awning Sign (Above)	A sign located above the outer fascia of a verandah, balcony or awning.	<p>a) Maximum area of 6m².</p> <p>b) Lettering shall not exceed 1m in height.</p>
			
5.	Billboard Sign	A sign fixed to a free-standing structure that is not a building and has one or more supports.	<p>a) Billboard signs are generally not supported unless the City deems that the sign has a public/community purpose</p>
			
6.	Bunting/Flag/Tethered	A single or group of flags or material that may be freestanding or attached to a rope or line stretched between two or more points.	<p>a) Bunting, flags and tethered signs are generally not supported unless the City deems the sign has a public/community purpose</p>
			
7.	Estate Entry Signage	A sign which serves to announce the entry to an estate or new residential development.	<p>a) Temporary only.</p> <p>b) A bond being received, where the estate entry signage is located on land vested to the Crown and/or the City, to cover any costs associated to the removal of the signage (particularly where signage includes</p>
			

Title	Signs & Advertising
Policy Number (Governance Purpose)	LPP 3.7



Attachment 7 Signs and Advertising

	<u>Sign Type</u>	<u>Definition</u>	<u>Expectations</u>
			lighting and any water features).
8.	Product Display Sign 	An object which is displayed for the purposes of advertisement; or an advertisement sign which is attached to or placed on a vehicle (car, truck, boat, trailer, caravan, machinery, whether moveable or not).	a) Product display signs are generally not supported within the City unless the sign has a public/community purpose.
9.	Roof Sign (Above) 	A sign erected on or attached to the roof of a building.	a) Not permitted if there is a roof sign on the same lot. b) Shall not project higher than 2m above the roof or 50% of the height of the building to which it is attached (whichever is higher).
10.	Tower Sign 	A sign affixed to, or placed on an open structural mast or tower.	a) Maximum area of 6m ² . b) Where applicable a minimum headway/clearance of 2.75m.

Title	Signs & Advertising
Policy Number (Governance Purpose)	LPP 3.7



Attachment 7 Signs and Advertising

Strategic Link:	Town Planning Scheme No. 3
Category	Planning - Town Planning & Development
Lead Business Unit:	Statutory Planning
Public Consultation: (Yes or No)	Yes
Adoption Date: (Governance Purpose Only)	10 November 2022
Next Review Due: (Governance Purpose Only)	November 2024
ECM Doc Set ID: (Governance Purpose Only)	4518397

Title	Cockburn Central North (Muriel Court) Structure Plan – Design Guidelines
Policy Number (Governance Purpose)	LPP4.2



Attachment 8 Cockburn Central North (Muriel Court) Structure Plan - Design Guidelines

Policy Type

Local Planning Policy

Policy Purpose

To provide site specific design guidelines for Development Area 19 (DA19) that will assist in the delivery of high quality and functional built environment.

All applications for subdivision and development shall have due regard with the Design Guidelines.

Policy Statement

(1) Introduction

These Design Guidelines shall be read in conjunction with the City of Cockburn Town Planning Scheme No.3 (TPS 3), Residential Design Codes, Cockburn Central North (Muriel Court) Local Structure Plan and other applicable Local Planning Policies and Local Development Plans.

(2) Vision

The Muriel Court Local Structure Plan area will be characterised as a sustainable, contemporary, high quality, vibrant, urban village which is an extension of the Cockburn Central Town Centre.

(3) Objectives

The objectives of the Design Guidelines are:

Physical

- Promote a safe, comfortable and easily accessible environment for residents and visitors;
- Ensure that buildings address and complement public spaces by providing transparent and engaging interfaces between the street and the public realm;

Social

- Promote a strong sense of place and community;
- Promote walking, cycling and public transport use through appropriately designed streets and buildings;
- Provide for a mix of housing opportunities to cater for a diverse community;
- Ensure good access to public open space for residents and visitors;

Title	Cockburn Central North (Muriel Court) Structure Plan – Design Guidelines
Policy Number (Governance Purpose)	LPP4.2



Attachment 8 Cockburn Central North (Muriel Court) Structure Plan - Design Guidelines

Economic

- Promote a viable local centre that supplies the daily needs of the local residents;
- Facilitate a development intensity that complements the Cockburn Central Town Centre and Cockburn Central West.

(4) Local Development Plans

Any subdivision application that proposes to create lots suitable for single house (including terrace-style) or grouped dwelling development in the Local Structure Plan (LSP) area will require the preparation of a Local Development Plan (LDP) for approval by the City to guide development outcomes. LDPs may address:

- Boundary setbacks;
- Building Heights;
- Vehicle Access;
- Open Space;
- Fencing;
- Built Form requirements;
- Roof Form and Design;
- Storage; and
- Any other design element as agreed to by the City.

(5) Minimum Residential Densities and Staging

In accordance with TPS 3 (DA19 provisions), as a minimum, the following zones are required to achieve density at the equivalent of 75% of greater of the density code applicable to the zone:

Density Code	Min Land Area (Lot) to Yield 75%	Land Area to Yield 100%
R25	466	350
R40	293	220
R60	200	150
R80 & R160	160	120

Achieving the 75% minimum residential density provision for multiple dwellings is calculate based on the single house/grouped dwelling yield.

The 75% minimum residential density requirement may be achieved via a staged approach which can be demonstrated as part of any application. Staged development shall demonstrate how future stages address:

- Minimum density requirements;
- Earthworks;
- Levels;
- Drainage;
- Road connections;
- Any other relevant information.

Title	Cockburn Central North (Muriel Court) Structure Plan – Design Guidelines
Policy Number (Governance Purpose)	LPP4.2



Attachment 8 Cockburn Central North (Muriel Court) Structure Plan - Design Guidelines

(6) Noise Attenuation

To address potential noise sources in the area, an Acoustic Report shall accompany applications for residential development and subdivision in the following locations:

- All development adjacent to the Kwinana Freeway;
- All development abutting the Mixed Business zone;
- All development incorporating a mix of uses;
- All development incorporating residential uses adjacent to North Lake Road.
- All noise attenuation and noise management shall be in accordance with Council’s Local Planning Policy LPP 1.12 Noise Attenuation.

(7) Vehicle Access – North Lake Road

Reciprocal vehicle access arrangements and shared vehicle access points are specified for lots abutting North Lake Road. Please refer to Council’s Local Planning Policy LPP 5.6 Vehicle Access.

(8) Low Density Zone (R25) Standards

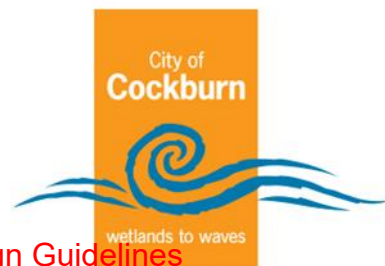
(a) As per R-Codes and LDP.

(9) Primary Controls for Medium Density Zone (R40 & R60) and High Density Zone (R80 & R160)

- (a) Single House and Grouped Dwellings – As per R-Codes and LDP.
(b) Multiple Dwellings (Apartments):

	R40	R60	R80	R160
Building height (storeys) Minimum	Two storeys	Two storeys	Two storeys	Two storeys
Building height (Storeys) Maximum	Three storeys	Four storeys	Four storeys	No maximum (refer to Jandakot Airport Obstacle Limitation Surfaces)
Boundary wall height (storeys)	As per R-Codes (R40)	Two storeys	As per R-Codes (R80)	Two storeys
Minimum primary and secondary street setbacks	Primary - 2m Secondary – 1m	Primary - 2m Secondary – 1m	As per R-Codes (R80)	As per R-Codes (R160)
Minimum side setbacks	As per R-Codes (R40)	As per R-Codes (R60)	As per R-Codes (R80)	As per R-Codes (R160)
Minimum rear setback	As per R-Codes (R40)	As per R-Codes (R60)	As per R-Codes (R80)	As per R-Codes (R160)
Plot Ratio	0.7	1.0	As per R-Codes (R80)	As per R-Codes (R160)

Title	Cockburn Central North (Muriel Court) Structure Plan – Design Guidelines
Policy Number (Governance Purpose)	LPP4.2



Attachment 8 Cockburn Central North (Muriel Court) Structure Plan - Design Guidelines

All other provisions as per R-Codes and any variations to the provisions above shall be assessed using the relevant element objectives in the R-Codes as they related to apartment design.

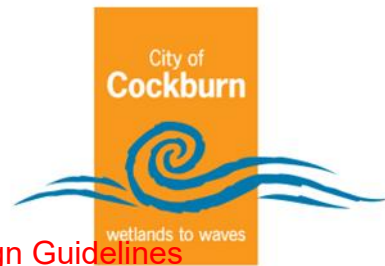
(10) Mixed Business & Local Centre Standards

	<i>Restricted Use – Office/Residential</i>	Restricted Use – Non-Residential	Local Centre
Building height (storeys) Minimum	Three storeys	Nil	Two storeys
Building height (Storeys) Maximum	No maximum (refer to Jandakot Airport)	No maximum (refer to Jandakot Airport)	Four Storeys
Boundary wall height (storeys)	As per R-Codes (R160)	As per BCA	Two storey
Minimum primary and secondary street setbacks	Subject to design in accordance with LPP 5.6 Vehicle Access.	Subject to design in accordance with LPP 5.6 Vehicle Access.	Nil
Minimum side setbacks	3m	As per BCA.	Nil
Minimum rear setback	3m	10m	Nil
Plot Ratio	N/A	N/A	As per R-Codes (R80) – Residential Component

All other provisions as per R-Codes and any variations to the provisions above shall be assessed using the relevant element objectives in the R-Codes as they related to apartment design.

Strategic Link:	Town Planning Scheme No.3
Category	Planning – Town Planning & Development
Lead Business Unit:	Development Assessment and Compliance
Public Consultation: (Yes or No)	Yes
Adoption Date: (Governance Purpose Only)	10 November 2022
Next Review Due: (Governance Purpose Only)	November 2024
ECM Doc Set ID: (Governance Purpose Only)	4517094

Title	Cockburn Central North (Muriel Court) Structure Plan – Design Guidelines
Policy Number (Governance Purpose)	LPP4.2



Attachment 8 Cockburn Central North (Muriel Court) Structure Plan - Design Guidelines

APPENDIX 1 - STREETS

The following sets out the City's specific streetscape requirements to be adopted by developers when designing and planning civil works. The City will refer to this section when assessing civil drawings. Developers are encouraged to engage with the City's Engineering and Parks Team early in the civil design process to discuss the below and further site specific design considerations prior to lodging civil drawings for approval.

Objectives for the design of public spaces in Cockburn Central North

Designs that seek to:

- Contribute to a vibrant, pedestrian friendly community that promotes use of public spaces;
- Provide the functional and aesthetic requirements required to support medium and high density development of which is expected to occur across the precinct;
- Promote the design of streets and public open spaces that will allow users and residents to connect to the local environment. This includes accommodating street trees consistent with the scale of development being proposed on adjoining lands.

Design process

Developers and applicants are advised that the City will be seeking evidence that streetscape civil drawings have responded to:

- The Cockburn Central North Structure Plan (Muriel Court Structure Plan) including but not limited to the design requirements set out within the Local Water Management Strategy;
- Site specific conditions including: Site context, topography and geotechnical conditions, environmental constraints and opportunities, and the retention of significant trees, and;
- Sustainable design measures including water quality and management.

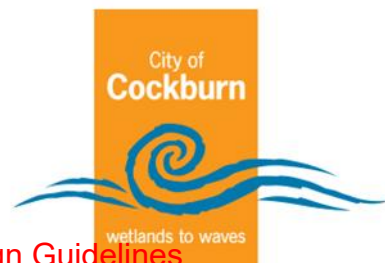
STREET PROFILES

The following details set out the City's requirements for the design of new roads and associated infrastructure within the precinct.

Semple Court (25.2 metres)

- Semple Court will be designed as an Integrator B Road, as defined by Liveable neighbourhoods, with a 25.2m road reserve;
- The design speed will be 50 km/hr and provide for up to 15,000 vehicles per day;
- Road pavement widths will be sufficient to provide for a traffic lane, segregated bicycle lane and on-street parking;
- 1.5m wide footpaths on both sides of the road which are adjacent property boundaries (0.3m offset);
- Within the local centre, the footpaths will be wider and constructed between the property boundary and the kerb; and
- Street lighting and tree planting in central median;
- Median width 4m; and

Title	Cockburn Central North (Muriel Court) Structure Plan – Design Guidelines
Policy Number (Governance Purpose)	LPP4.2



Attachment 8 Cockburn Central North (Muriel Court) Structure Plan - Design Guidelines

- See Appendix 2 for material selections.

Muriel Court (25.2m – East of Semple Court Realignment)

- Semple Court will be designed as an Integrator B Road, as defined by Liveable neighbourhoods, with a 25.2m road reserve;
- Muriel Court is designed for 50 km/hr and will provide for up to 15,000 vehicles per day;
- See attachment 2 for required intersection types along Muriel Court;
- Road pavement widths will be sufficient to provide for a traffic lane, segregated bicycle lane and on-street parking;
- 1.5m wide footpaths on both sides of the road which are adjacent property boundaries (0.3m offset);
- Within the local centre, the footpaths will be wider and constructed between the property boundary and the kerb;
- Street lighting and tree planting in central median;
- Median width 4m; and
 - See Appendix 2 for material selections.

Muriel Court (24.4m – West of Semple Court Realignment)

- Muriel Court will be designed as a Neighbourhood Connector Road, as defined by Liveable Neighbourhoods, within a 24.4m road reserve;
- Muriel Court is designed for 50 km/hr and will provide for up to 15,000 vehicles per day;
- 1.5m wide footpaths on both sides of the road which are adjacent property boundaries (0.3m offset);
- Segregated bicycle lane for the entirety of the length of Muriel Court;
- Street lighting in a minimum 2m wide central median; and
- See Appendix 2 for material selections.

18 metre wide roads

- These roads are designed as wide access streets suitable for higher density residential areas and provide for on street parking and street tree planting;
- Street pavement 6m with one verge 6.2m and the other 5.8m. The wider verge on one side will accommodate sewer and stormwater drainage;
- 1.5m wide footpaths on both sides of the road which are adjacent to property boundary (0.3m); and
- 2.1m wide on-street car parking bays.

15 metre wide roads

- These roads are for lower density residential areas and some on-street parking (on the carriageway), which provide speed control;
- 4.5m verges and 6m pavement;
- Street trees are to be provided in the road reserve;
- 1.5m wide footpaths on both sides of the road which are adjacent to property boundary (0.3m offset); and
- 2.1m side on-street car parking bays.

Title	Cockburn Central North (Muriel Court) Structure Plan – Design Guidelines
Policy Number (Governance Purpose)	LPP4.2



Attachment 8 Cockburn Central North (Muriel Court) Structure Plan - Design Guidelines

16 metre wide road

- As per 15m wide roads, except that verges are 5m wide each;
- 1.5m wide footpaths on both sides of the road which are adjacent property boundaries (0.3m offset);
- For lots that abut public open space the road reserve is 14.5m; and
- The street profile is the same, except on the public open space side the verge which is 3.5m and contains a Dual Use Path (2.1m) and no car parking.

Laneways

- New laneways may be required to provide access to new lots created through subdivision.
- Road Design speed of 10 km/h;
- 6m wide road pavements with no footpaths with 1x0.5m setbacks for lighting at regular spacing as illustrated in Figure below; and
- Require traffic calming where length is deemed to warrant it.



Title	Cockburn Central North (Muriel Court) Structure Plan – Design Guidelines
Policy Number (Governance Purpose)	LPP4.2

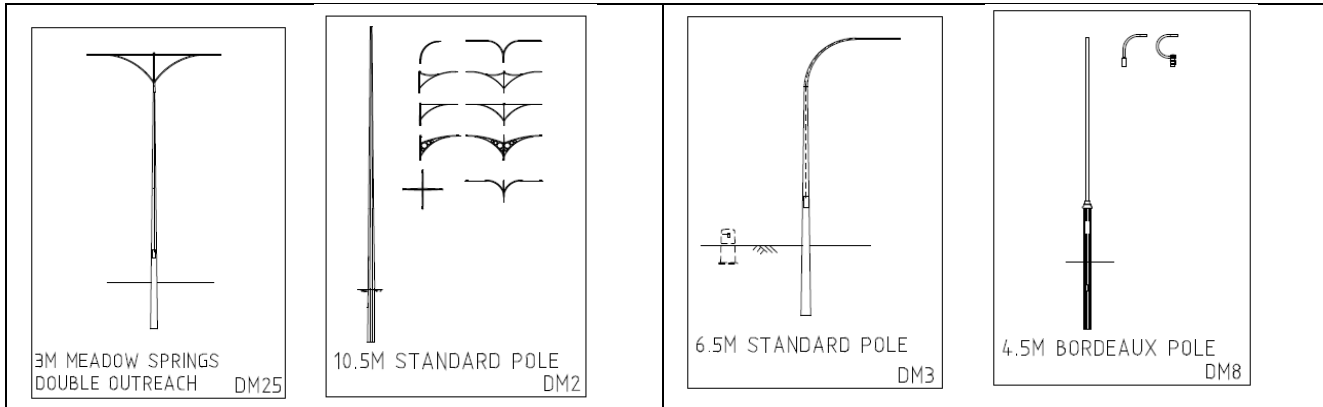


Attachment 8 Cockburn Central North (Muriel Court) Structure Plan - Design Guidelines

APPENDIX 2 - MATERIAL SELECTIONS

Street lighting – precinct wide

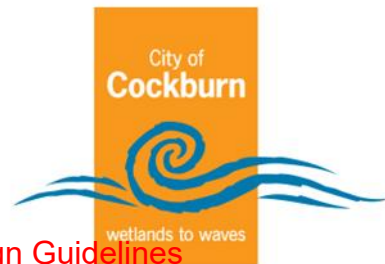
<u>Street name/type</u>	<u>Type</u>	<u>Western Power item</u>
Muriel Court/Semple Court	Double outreach pole	DM25_10.5m Standard Pole, MEADOW SPRINGS 3m DOR, Parkville Luminaire, EBONY
Residential Access streets	6.5m standard pole	DM2_STANDARD POLE 6.5m, LED Luminaire, EBONY
Laneways	4.5m standard pole	DM3 Bourdeaux Pole 4.5m, LED Luminaire, EBONY




Muriel Court road reserve materials and standard requirements

<u>Component</u>	<u>Material/colour</u>
Travel lane	<ul style="list-style-type: none"> Black asphalt
Cycle lane	<ul style="list-style-type: none"> Red asphalt and green at intersections. White lines
Footpath	<ul style="list-style-type: none"> Exposed light grey aggregate at corners. Grey concrete footpaths. Exposed aggregate colour: Preston Beach form Boral, or similar to be approved by the City of Cockburn. Bollards to be included on corners while at same time meeting City requirements for pedestrian ramps.
Round about	<ul style="list-style-type: none"> Black asphalt with MRWA intersection mix
3 and 4-way intersections	<ul style="list-style-type: none"> Black asphalt with linemarking and signage
Tactile pavers at pram ramps and medians	<ul style="list-style-type: none"> Charcoal 60mm tactile pavers
On-street parking bays/Intersection threshold treatment	<ul style="list-style-type: none"> Charcoal interlock pavers
Median finishes	<ul style="list-style-type: none"> Semi mountable kerbing 600mm concrete apron to the outer, inside edge of the median: concrete to be finished with dark red ochre faux paving. Remaining garden bed to have trees planted max. of 6 -8m apart. Chunky pine bark mulch to remaining unpaved median area.

Title	Cockburn Central North (Muriel Court) Structure Plan – Design Guidelines
Policy Number (Governance Purpose)	LPP4.2



Attachment 8 Cockburn Central North (Muriel Court) Structure Plan - Design Guidelines

<u>Component</u>	<u>Material/colour</u>
Tree nibs (between car embayments)	<ul style="list-style-type: none"> • Tree nibs to have flush beam edging (to allow rain water flow). • Nibs to be minimum of 1.2m wide at road edge. • Trees to have root barrier installed to furthest edge of tree nib at 600mm deep. • Trees between car embayments to have tree surrounds installed as per the City of Cockburn standard detail. • Wheel stops to be installed either side of a tree on in-car embayments to prevent cars damaging trees. 

For material selections for streets other than Muriel Court please contact the City's Engineering and Parks teams.

Street trees

Road	Location	Species selection
Muriel Court	<ul style="list-style-type: none"> • Centre line planting • Verge both sides or in nibs 	<ul style="list-style-type: none"> - Claret Ash (Fraxinus Raywoodii) - Chinese elm (Ulmus Parivolia)
15m access streets	<ul style="list-style-type: none"> • Select at time of civil approvals by Parks 	Applicants choice subject to City approval

Title	Cockburn Central North (Muriel Court) Structure Plan – Design Guidelines
Policy Number (Governance Purpose)	LPP4.2



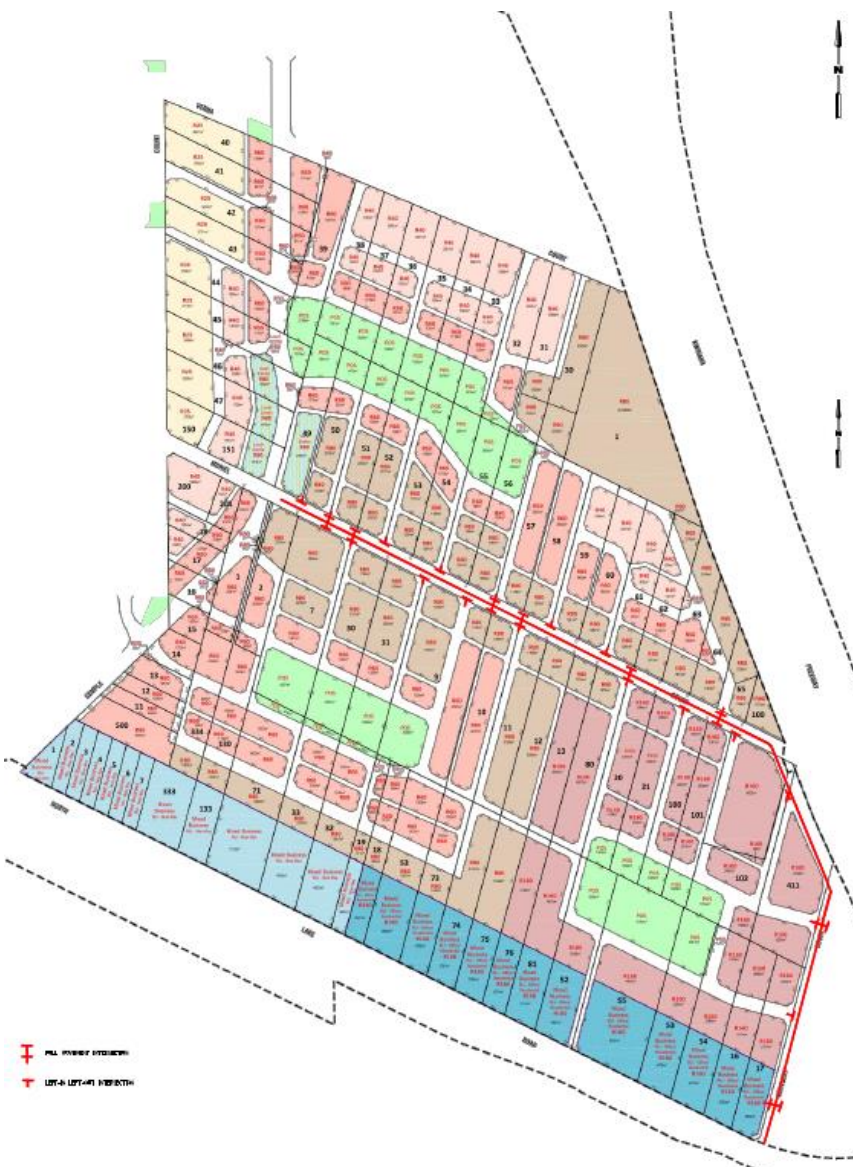
Attachment 8 Cockburn Central North (Muriel Court) Structure Plan - Design Guidelines

APPENDIX 3 – MURIEL COURT AND KNOCK PLACE INTERSECTION TREATMENTS

In 2018 the City engaged GTA Consultants to undertake a review of the Muriel Court Traffic Impact Assessment due to revised traffic generation numbers expected across the Muriel Court precinct. This recognises several structure plan amendments have cumulatively impacted on dwelling yield and therefore also traffic generation numbers.

The addendum is to be read alongside the original Muriel Court traffic impact assessment undertaken in 2008. The full report can be accessed on the Muriel Court webpage [https://www.cockburn.wa.gov.au/Building-and-Development/Town-Planning-and-Development/Current-Development-Projects/Cockburn-Central-North-\(Muriel-Court\)](https://www.cockburn.wa.gov.au/Building-and-Development/Town-Planning-and-Development/Current-Development-Projects/Cockburn-Central-North-(Muriel-Court))

Within the TIA Addendum at Attachment 6 and provided below is the City’s preferred and now adopted option for the design of intersection treatments along Muriel Court and Knock Place.



Title	Newmarket Precinct - Design Guidelines
Policy Number (Governance Purpose)	LPP 4.3



Attachment 9 Newmarket Precinct - Design Guidelines

Policy Type

Local Planning Policy

Policy Purpose

To provide site specific design guidelines for the 'Newmarket Precinct' as identified in the Cockburn Coast District Structure Plan (DSP). All applications for subdivision and development shall have due regard with the Design Guidelines. These Design Guidelines shall be read in conjunction with the City of Cockburn Town Planning Scheme No.3 (TPS 3), Residential Design Codes, and other applicable Local Planning Policies and Local Development Plans.

The objectives of the Design Guidelines are:

- To encourage a diverse population that contributes to the interest and vitality of the precinct and the DSP area generally by providing a genuine mix of dwelling types to cater for a range of living options;
- To promote the redevelopment of the Cockburn coast into a vibrant and sustainable environment that integrates living, working and leisure opportunities;
- To encourage a residential and mixed use focus, with ground floor office and commercial opportunities along Cockburn Road and Rockingham Road, and to encourage mixed use development in areas where it is considered that the market could, at some time, sustain some commercial use;
- To encourage lower level of car dependence for those travelling to and within the Cockburn Coast area by promoting public and non-motorised transport options;
- To encourage the foundation for a highly connected, legible, and active street environment that is safe, and weather protected.;
- To ensure that any development that occurs within the Newmarket Precinct deals with both the designation and distribution of height across the Precinct in a comprehensive manner;
- To promote innovative, high quality development that contributes to a lively streetscape;
- To ensure the efficient and effective upgrade of infrastructure to support orderly development and meet the demands arising from new development.

Policy Statement

(1) Land Use

1.1 Mixed Use

Mixed use developments which are pedestrian oriented at ground floor should be provided adjacent to Cockburn Road and Rockingham Road to encourage opportunities for commercial use and/or home based business 'designed in' to developments.

Title	Newmarket Precinct - Design Guidelines
Policy Number (Governance Purpose)	LPP 4.3



Attachment 9 Newmarket Precinct - Design Guidelines

1.2 Residential Convertible dwellings

In relation to ground floor dwellings fronting Cockburn Road and Rockingham Road, as a minimum, 3.6m floor to ceiling should be provided. This will assist in facilitating the conversion of ground floor (street) frontage to non-residential use when the demand arises for such product as Cockburn Coast develops. Other considerations to be contemplated in the design and development of new buildings include:

- (a) 'Back of house' access for servicing to enable conversion to active 'lifestyle' type use (food and beverage establishments);
- (b) Making provision for future mechanical services (flues and exhaust vents) to enable active use;
- (c) The design, location and/or size of wet areas and toilet facilities (public building requirements);
- (d) Non load bearing walls to allow future changes to internal floor layouts;
- (e) Internal layouts which facilitate self-containment of the ground floor for independent use; and
- (f) Potential future changes in use taken into consideration in the provision of parking.

1.3 Local Activity Node

It is intended that the local activity nodes will contain the majority of Cockburn Coast's retail space in a street based form, and large mall based retail with large expanses of car parking will not be supported. Built form within the local activity node will be expected to have a strong interface with Cockburn Road through nil to minimum setbacks to the lot boundary.

(2) Transport

2.1 Road Network

- (a) The higher order road layout should be in accordance with the DSP.
- (b) Any development seeking alterations and/or additions to the lower order road network must be supported by detailed design work as would normally take place in preparing a local structure plan. This detailed design work should demonstrate that proposed alterations and/or additions are consistent with best practice planning outcomes, and align with the intent of the District Structure Plan. This detailed design work should have regard for the function of Cockburn Road and the primary regional road (Cockburn Coast Drive), and ensure both the higher and lower order road networks are not prejudiced in any way.
- (c) To ensure appropriate permeability, development on Lot 2 Bellion Drive, Hamilton Hill (Vol 1300 Folio 625) will require the ceding of land and construction of an additional road network connection between Boyd Crescent and Bellion Drive at the time of development or following the construction of Cockburn Coast Drive (whichever comes first). It is expected that the land and construction of this road would be provided by the landowner.
- (d) Provision of a traffic impact assessment may be required in support of development applications.

Title	Newmarket Precinct - Design Guidelines
Policy Number (Governance Purpose)	LPP 4.3



Attachment 9 Newmarket Precinct - Design Guidelines

2.2 Pedestrian and Cyclist Movement

- (a) Development should provide a positive contribution through pedestrian-oriented uses at ground level, with consideration for non-motorised transport options, particularly along Cockburn Road.
- (b) Development should facilitate close interaction with the pedestrian activity along footpaths. Buildings should be designed to focus on pedestrian and cyclist amenity and safety, while accommodating on street parking and slow vehicle speeds in certain locations.
- (c) Provision of end-of-trip facilities is required for commercial uses.

2.3 Car Parking

- (a) Where the local government has either provided or has made firm proposals for providing public car parking in the vicinity of the land subject of the application, a cash-in-lieu payments for a reduction in onsite parking in accordance with the provisions of TPS 3 may be considered.
- (b) Where possible parking should be located in a basement or part basement arrangement (depending on the topography of the land).
- (c) Onsite parking should be provided behind building frontages or screened from public view.
- (d) Grade level car parking areas should be suitably landscaped, and this is particularly relevant where adjoining buildings look into or across the rear of a development site.
- (e) Access to onsite parking should be easily identifiable and suitably signed.
- (f) Consideration should be given to onsite servicing and waste management at the development application stage.

(3) Built Form

3.1 General

- (a) All development should be 'urban' in form where it meets the public domain, characterised by nil street setbacks.
- (b) Active building frontages are encouraged, and the number of doors and windows open to the street should be maximised.
- (c) Awnings and street trees should be provided for weather protection wherever possible.
- (d) To further assist the function of the pedestrian environment, all building and tenancy entrances should be well located and clearly defined through the use of elements distinctive to the architectural style of the development.
- (e) Development should express strong architectural themes, demonstrating variation, distinctiveness, high visual interest, sustainability and climate responsiveness.
- (f) Development should address public streets and other public areas, and should be an attractive and functional component of the Precinct and Cockburn Coast area as a whole.
- (g) Development should maintain consistent street setbacks and street edge configurations.

Title	Newmarket Precinct - Design Guidelines
Policy Number (Governance Purpose)	LPP 4.3



Attachment 9 Newmarket Precinct - Design Guidelines

- (h) All visible walls should be articulated, or otherwise architecturally treated, in order to create visual interest and to avoid a broad expanse of featureless wall.

3.2 Primary Controls

<u>Control</u>	<u>SU24</u> (Mixed Use/R60)	<u>SU25</u> (Residential R60)	<u>SU27</u> (Mixed Use R160)	<u>SU29</u> (Local Activity Node R60)
Building Height (storeys)	Abutting Cockburn & Rockingham Roads – 4-5 Other – 3-4	3-4 (17m)	4-5 (21m)*	4-5 (21m)*
Boundary Wall Height (storeys)	1	1	2	1
Minimum Primary & Secondary Street Setback	Nil (ground floor commercial) 2m (Residential)	2m	Nil	Nil (ground floor commercial) 2m (Residential)
Minimum Side Setback	2m	3m	Nil	2m
Minimum Rear Setback	3m	3m	3m	3m
Plot Ratio	Abutting Cockburn & Rockingham Roads – 2.0 Other - 0.8	0.8	2.0	0.8

Note: Maximum heights (metres) are the total heights of the physical building (including services such as lift over-runs, water tanks and heating/cooling equipment that may add to the building height). Calculated as 3.5m for each floor (floor-to-floor measurement) and an additional 3m for buildings above 12m in height (buildings above 12m may need to provide services such as lift over-runs, water tanks and heating/cooling equipment that should be factored into the total height of the building).

*can only be supported if a 3m terrace setback or loft level is provided.

3.3 Development Abutting Cockburn Coast Drive and Rollinson Road Link

- (a) Cockburn Coast Drive and the Rollinson Road link (east of Cockburn Road) will not accommodate direct road access, however buildings should be oriented and designed to provide an outlook to the road and Beeliar Regional Park.
- (b) Use of balconies at upper levels is encouraged to provide surveillance of Beeliar Regional Park, while providing residents with access to the views and vistas of the scenic surrounds.

2.4 Landmark & Gateway Sites

- (a) Only one landmark site and one gateway site will be considered within the precinct.
- (b) The landmark site should be located at the south western corner of the intersection the Cockburn Coast Drive and Rockingham Road as an entry statement to the project area. It should be at a strategic location

Title	Newmarket Precinct - Design Guidelines
Policy Number (Governance Purpose)	LPP 4.3



Attachment 9 Newmarket Precinct - Design Guidelines

focused along the prominent ridgeline which forms the visual backdrop to the Precinct and broader Cockburn Coast area.

- (c) The gateway site should be located on the north-east side of the intersection between Cockburn Road and future Rollinson Road extension to Cockburn Coast Drive. This road link will form one of the key entrances into the Cockburn Coast area.
- (d) The landmark site development is permitted to be up to 16 storeys (and not exceeding 49m in height).
- (e) The gateway site development is permitted to be up to eight storeys (and not exceeding 32m in height).
- (f) Notwithstanding the timing of development, the appropriateness of the design of development will be assessed in the context of the ultimate streetscape it will form part of.
- (g) To achieve variety in the built form development should comprise a base (i.e. low rise podium), main body and coronation (top), articulated by architectural elements.
- (h) Gateway development located on the northeast side of the intersection between Cockburn Road and the future Rollinson Road extension should address that intersection in an appropriate manner.
- (i) Development should endeavour to incorporate an area for public benefit, such as plazas, arcades or public viewing platform, providing a demonstrable benefit to the general public as a result of the development.

(4) Standards of Development

It is envisaged that conditions will be imposed on subdivision and development applications within the Newmarket Precinct requiring proponents to undertake upgrades to infrastructure and streetscapes in accordance with the City's Engineering guidelines and standards. This will typically include (where relevant) the following works as they relate to the development:

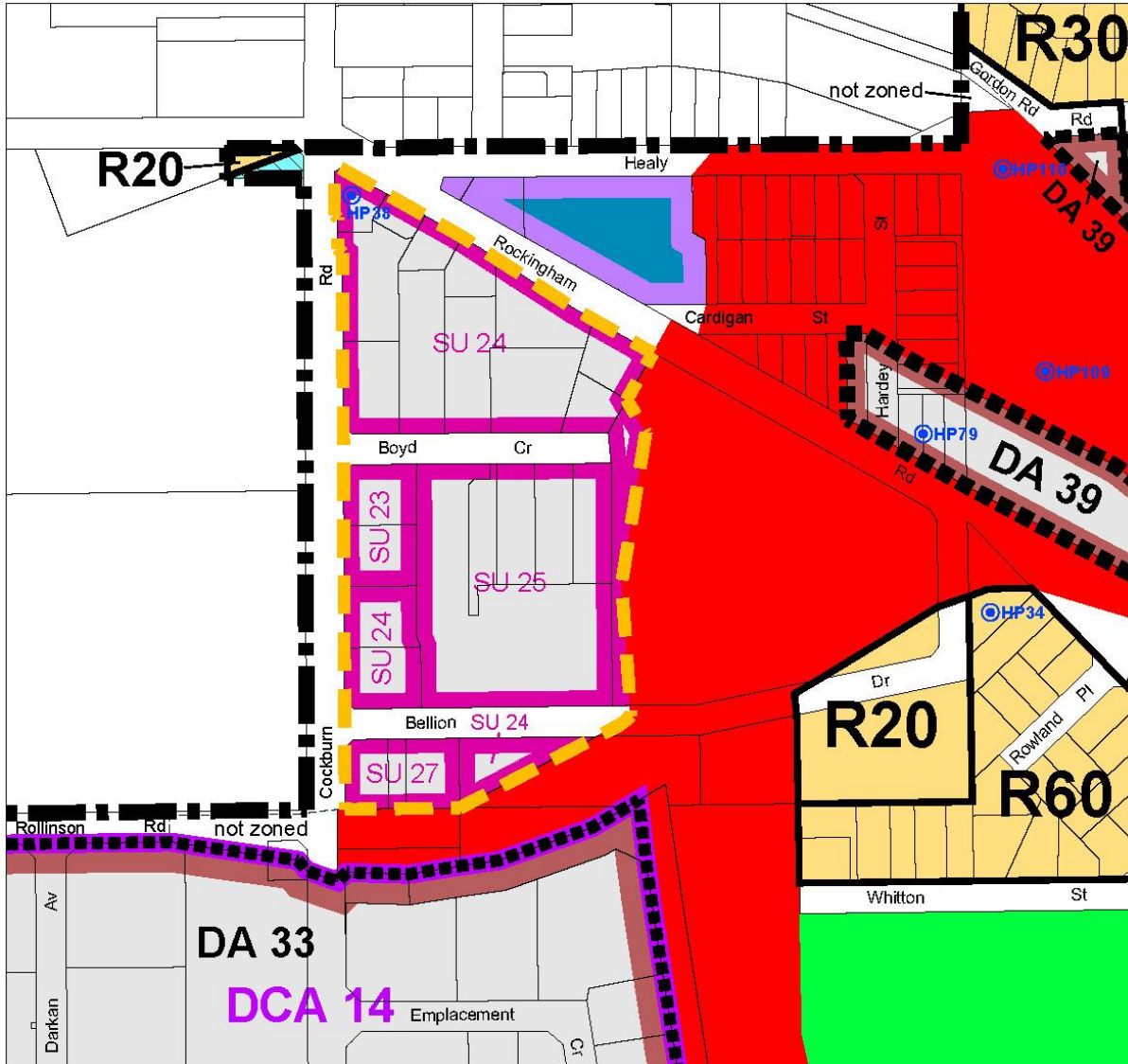
- Streets/roads abutting the development site (including kerbing, draining and resurfacing);
- Service, utility and drainage upgrades;
- Streetscape elements abutting the development site (such as pavement, lighting, trees, furniture);
- Footpaths and dual use-paths abutting the development site;
- Traffic management devices;

Title	Newmarket Precinct - Design Guidelines
Policy Number (Governance Purpose)	LPP 4.3



Attachment 9 Newmarket Precinct - Design Guidelines

APPENDIX A – NEWMARKET PRECINCT LOCATION PLAN



<p>General</p> <ul style="list-style-type: none"> Scheme Boundary Residential Density Codes Heritage Place Special Control Areas Development Area Development Contribution Area Region Reserves Parks & Recreation Primary Regional Roads 	<p>Local Reserves</p> <ul style="list-style-type: none"> Lakes & Drainage Local Road <p>Zones</p> <ul style="list-style-type: none"> Residential Mixed Business Development Special Use 	<ul style="list-style-type: none"> Newmarket Precinct Area
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Title	Newmarket Precinct - Design Guidelines
Policy Number (Governance Purpose)	LPP 4.3



Attachment 9 Newmarket Precinct - Design Guidelines

Strategic Link:	Town Planning Scheme No.3
Category	Planning – Town Planning & Development
Lead Business Unit:	Development Assessment and Compliance
Public Consultation: (Yes or No)	Yes
Adoption Date: (Governance Purpose Only)	10 November 2022
Next Review Due: (Governance Purpose Only)	November 2024
ECM Doc Set ID: (Governance Purpose Only)	4517282

Title	Heritage Conservation Design Guidelines
Policy Number (Governance Purpose)	LPP 4.4



Attachment 10 Heritage Conservation Design Guidelines

Policy Type

Local Planning Policy

Policy Purpose

The City of Cockburn Local Government Inventory (LGI) identifies places within the City of Cockburn that have cultural heritage significance. The compilation of a Local Government Inventory is a requirement of Clause 45 of the *Heritage of Western Australia Act 1990*. Those places on the LGI with the greatest heritage significance are also included on the Heritage List pursuant to the City of Cockburn's Town Planning Scheme No. 3 (TPS 3).

The City of Cockburn's LGI includes a significant tree list, and these trees are protected under the City of Cockburn's TPS 3.

The purpose of this Policy is to:

- (1) Set out development control principles for places on the Heritage List established pursuant to TPS 3, and the LGI.
- (2) Provide further direction on the development control principles contained within State Planning Policy 3.5 Historic Heritage Conservation (2007).
- (3) Provide improved certainty to landowners and the community about the development control principles for heritage conservation and protection.
- (4) Set out guidelines for modifications to trees included on the Significant Trees List.

The key objectives of this Policy are:

- (1) To ensure that works, including conservation, restoration, alterations, additions, changes of use and new development, respect the heritage significance associated with heritage places;
- (2) To encourage opportunities for interpretation where it can enhance understanding and enjoyment of heritage places, and strengthen the relationships between the community and its heritage.
- (3) To ensure that any works to trees included on the Significant Tree List do not endanger the trees survival.

This policy applies to places entered on the Heritage List pursuant to TPS 3, and places on the City of Cockburn LGI where specified.

Definitions:

In this Policy the following definitions apply, in addition to those terms defined in TPS 3 and Residential Design Codes of Western Australia (R-Codes):

Conservation Plan means a document that details how to identify and look after the significant cultural values of a place. Its preparation involves systematic consideration, recording and monitoring of actions and decisions relating to all aspects of managing a

Title	Heritage Conservation Design Guidelines
Policy Number (Governance Purpose)	LPP 4.4



Attachment 10 Heritage Conservation Design Guidelines

place. The Heritage Council of WA provides guidelines for the preparation of conservation plans.

Heritage place means a building, structure, site, area of land or other physical element valued for its cultural (or historic) heritage significance together with its associated contents and surrounds, and that is included on the State Register of Heritage Places, the City of Cockburn Heritage List, and/or the City of Cockburn LGI.

Heritage significance means the aesthetic, historic, social and scientific values of a place for past, present or future generations.

Interpretation means all the ways of presenting the heritage significance of an item. Interpretation may be a combination of the treatment and fabric of the item; the use of the item; the use of interpretive media, such as events, activities, signs and publications, or activities, but is not limited to these.

Setting means the area around a heritage place, which may include the visual catchment.

Significant Fabric means all the physical material of the place including components, fixtures, contents, and objects that contribute to the heritage significance of the place.

Significant Trees means trees that are included on the Significant Tree List (contained within the LGI) for their significance, which includes characteristics such as outstanding aesthetic significance, horticultural value, historic value, and/or unique location or context.

Policy Statement

- (1) Development Control Principles for Places on the Heritage List (Management Category A and B)

Places on the City of Cockburn Heritage List are those of highest heritage value, and the following policy provisions shall apply to these places:

1. External Alterations and Extensions

(a) General Provisions

- (i) Alterations and additions to a heritage place should not detract from the heritage significance and should be compatible with the siting, scale, architectural style and form, materials and external finishes of the place.
- (ii) Alterations and additions to a heritage place should involve the least possible change to the significant fabric.
- (iii) Alterations and additions should sit well within the significant fabric rather than simply copying it, and new work that mimics the original should be avoided.
- (iv) New work should be easily distinguishable from the significant fabric, except where the proposal constitutes restoration work of original fabric.

Title	Heritage Conservation Design Guidelines
Policy Number (Governance Purpose)	LPP 4.4



Attachment 10 Heritage Conservation Design Guidelines

- (v) Alterations and additions should respect the original roof pitch and roof form.
- (vi) Alterations and additions should not obscure or alter elements that contribute to the heritage significance of the place.
- (vii) Walls and fences in the front setback should be complementary to the heritage place in terms of materials, finishes, textures and colours and appropriate to its architectural style.
- (viii) Where there is a Conservation Plan for a heritage place all proposed development should address the policies contained within the Conservation Plan.
- (ix) Substantial modifications to the place may require an archival record (as a condition of development approval), to be prepared in accordance with the Heritage Council of WA guidelines.

(b) Upper Storey Additions and Modifications

- (i) Upper storey additions should generally be sited and massed so they are visually recessive from the place's main frontage to ensure that the heritage place is the dominant element in the streetscape. On corner sites the visibility and impact of additions will be assessed from both streets.
- (ii) Upper storey additions or modifications should be designed to minimise the impact on the original roofline, and to retain an appreciation for the original form of the building.

(c) Openings and Doors

- (i) New openings in the principal elevation (addressing the primary street) that will be visible from the street should be avoided. If openings are proposed they should be proportional in size relative to original openings of the heritage place and consistent in terms of materials, finishes, textures and colours (appropriate to its architectural style).

(d) Landscaping Elements

- (i) Where landscape elements such as plantings or hard landscape treatments form part of the heritage significance of a place, or are important to its setting, all proposed extensions and modifications should be designed and sited to minimise the impact on these elements.
- (ii) All new landscaping, external works and site elements should be well considered and respectful to the heritage significance of the place.

2. Internal Alterations

- (a) Alterations to the interior of a heritage place to suit a current and compatible future use will be supported where the proposal does not compromise the heritage significance of the place, as follows:

Title	Heritage Conservation Design Guidelines
Policy Number (Governance Purpose)	LPP 4.4



Attachment 10 Heritage Conservation Design Guidelines

- (i) Ideally the original internal layout should be retained, however where original internal walls or features are proposed to be removed or modified these changes should be managed to allow evidence of the original layout to be read (for example by retention of wall “nibs” as evidence of the location of a former wall), to retain a sense of the original use of the space(s).
 - (ii) Where new internal finishes are proposed there should be careful consideration given to retaining evidence of original materials and finishes.
- (b) Internal alterations that are reversible without compromising the heritage significance of the place will generally be acceptable, and the onus rests with the applicant to demonstrate that this reversal is achievable.
3. Change of Use
- (a) Adaptive reuse of heritage places may be supported provided:
 - (i) The proposed use(s) will not impact negatively on the amenity of the surrounding area.
 - (ii) Any required modifications do not substantially detract from the heritage significance of the place and are consistent with the provisions of this policy.
 - (iii) The use is consistent with TPS 3 and other relevant Council policies.
 - (b) Where there is a Conservation Plan for a heritage place any proposed new use(s) will be assessed on the basis of the recommendations contained within the Conservation Plan.
 - (c) Where possible, evidence of the original use of a building should be retained, and in some circumstances interpretation may be appropriate to help understand the former use where it is not readily apparent (refer to 7.0).

4. New Buildings/Structures

New buildings, structures and other features that are located within the curtilage of a heritage place have the potential to impact on the heritage significance. Accordingly the following provisions are applicable:

- (a) Any proposed buildings, structures or hardstanding (including car parking) should not detract from the setting of the heritage place.
- (b) Where new buildings or structures are proposed and they are visible from the street and/or other public places, they should take into account the character of the existing streetscape by having regard to the rhythm, orientation, setbacks, height, and proportions of existing buildings.
- (c) Where possible existing views of a heritage building(s) from the street should be preserved to acknowledge the contribution heritage places make to the streetscape.

Title	Heritage Conservation Design Guidelines
Policy Number (Governance Purpose)	LPP 4.4



Attachment 10 Heritage Conservation Design Guidelines

- (d) New buildings or structures should be designed and located in a way that does not overwhelm or dominate the heritage building(s) which should remain the dominant building(s) on the site, and they should be understated relative to the existing heritage building(s).
- (e) Wherever possible, new buildings, structures or hardstanding areas (including car parking) should be designed and sited to avoid having a negative impact on original mature landscaping, garden areas, driveways and other landscaping features where they are considered to form part of the setting of the heritage place, and/or contribute to the heritage significance.
- (f) New buildings should not directly copy the style and design of the heritage buildings, and should not attempt to look like old buildings. Rather they should complement the original fabric and design characteristics of the heritage building(s) in terms of its bulk, style, materials, colour scheme and form, which could include contrasting, contemporary building(s).
- (g) Where there is a Conservation Plan for a heritage place any proposals for new buildings, structures or hardstanding areas (including car parking) should address the policies contained within the Conservation Plan.

5. Demolition

- (a) Demolition of a whole building on the Heritage List will generally not be supported.
- (b) Consideration of a demolition proposal for a place on the Heritage List will be based on the following:
 - (i) The heritage significance of the place.
 - (ii) The feasibility of restoring or adapting it, or incorporating it into new development.
 - (iii) The extent to which the community would benefit from the proposed redevelopment.
- (c) Where structural failure is cited as justification for demolition the onus rests with the applicant to provide a clear justification for demolition, and evidence should be provided from a registered structural engineer that the structural integrity of the building has failed to the point where it cannot be rectified without the removal of a majority of its significant fabric and/or prohibitive costs.
- (d) Partial demolition of a building on the Heritage List may be supported provided that:
 - (i) The part(s) to be demolished do not.
 - (ii) The proposed demolition will not have a negative impact on the significant fabric of the place.
 - (iii) Sufficient fabric is retained to ensure structural integrity during and after development works, and the onus rests with the applicant to demonstrate that this is achievable.

Title	Heritage Conservation Design Guidelines
Policy Number (Governance Purpose)	LPP 4.4



Attachment 10 Heritage Conservation Design Guidelines

- (e) If demolition of a heritage place is considered appropriate an archival record will be required as a condition of development approval, to be prepared in accordance with the Heritage Council of WA guidelines.
- (f) Demolition of ancillary buildings or structures that do not relate to the heritage significance of the place will generally be acceptable.
- (g) Where full or partial demolition is supported this may be subject to appropriate interpretation to acknowledge the cultural heritage significance of the heritage place (refer to 7.0).

6. Relocation of Buildings/Structures

- (a) In the majority of cases the physical location of a place is an important part of its heritage significance, therefore the relocation of a building or other component of a place on the Heritage List is generally unacceptable except in the following circumstances:
 - (i) This is the sole practical means of ensuring its survival.
 - (ii) It can be demonstrated that these components of the heritage place already have a history of relocation, or were designed to be readily relocated.
 - (iii) Its relocation forms part of a proposal for a new use or development on the site, and is fundamental to retention of the place on the same site.

7. Minor Works, Repairs, and Restoration

Pursuant to TPS 3, all development affecting a place on the Heritage List requires development approval, and this includes minor works such as replacement of roofing, gutters, downpipes. This is to ensure that these works do not have a negative impact on the heritage significance of the place, and accordingly the following policy provisions apply:

- (a) Where there is a Conservation Plan for a heritage place all restoration works will be guided by the Conservation Plan.
- (b) Where proposals include the replacement of materials it should be “like for like”, matching the original as closely as possible with regard to the materials, colours, and textures.
- (c) External repainting should match the original paint colours wherever possible, or should reflect a complementary palette of colours from the same era from which the property derives its heritage significance.
- (d) Replacement of materials should take into consideration the original method of fixing.
- (e) Where restoration is being carried out, works should be based on historic photographs, plans or other records that indicate the former state of the building or place during the era identified in the LGI as of most importance to the heritage significance of the property.
- (f) Routine maintenance does not require development approval. This includes the following:

Title	Heritage Conservation Design Guidelines
Policy Number (Governance Purpose)	LPP 4.4



Attachment 10 Heritage Conservation Design Guidelines

- (i) Cleaning gutters and downpipes (as opposed to replacing deteriorated gutters and downpipes).
- (ii) Repainting previously painted surfaces in the same colour scheme.
- (iii) Refixing existing loose roof sheeting using a “like for like” method of fixing (as opposed to installing new or different roof sheeting), with the exception of emergency repairs that are temporary in nature.

NB: If there are any questions regarding what constitutes routine maintenance, the City’s Planning Services should be consulted.

(2) Development Control Principles for Management Category C Places

The following policy provisions apply to places included on the LGI and identified as management category C (Significant).

1. Alterations, Extensions or Changes of Use
 - (a) Where alterations or extensions are proposed consideration should be given to ensuring these modifications do not detract from the heritage values of the place, and retention of original fabric is encouraged where feasible.
 - (b) Substantial modifications to the place may require an archival record (as a condition of development approval), and the archival record should be prepared in accordance with the Heritage Council of WA guidelines.
2. Demolition
 - (a) Retention of the building or place is encouraged; however demolition may be supported, subject to the consideration of heritage significance together with other relevant planning issues.
 - (b) An archival record will be required as a condition of development approval for demolition, and the archival record should be prepared in accordance with the Heritage Council of WA guidelines.
 - (c) Consideration should be given to the inclusion of interpretation of the heritage place (refer to 7.0).

(3) Development Control Principles for Management Category D Places

The following policy provisions apply to places included on the LGI and identified as management category D (Some significance).

1. Demolition
 - (a) Retention of the building or place is encouraged; however, demolition may be supported subject to the preparation of an archival record which will be required as a condition of development approval for demolition. The archival record should be prepared in accordance with the Heritage Council of WA guidelines.

(4) Significant Trees

Under TPS 3 planning approval is required prior to the removal, destruction of and/or interference with any tree included on the Significant Tree List, and as such the following policy provisions apply:

Title	Heritage Conservation Design Guidelines
Policy Number (Governance Purpose)	LPP 4.4



Attachment 10 Heritage Conservation Design Guidelines

1. Significant Trees may be pruned as part of routine maintenance in accordance with the International Society of Arboriculture standards, provided the pruning would not reduce the tree's height or crown or diameter, alter the trees general appearance, increase the tree's susceptibility to insects or disease, or otherwise increase its risk of mortality.
2. The removal of a Significant Tree will only be supported where it is necessary to protect public safety or private or public property from imminent danger and the onus is on the applicant to demonstrate that this is the case. This may require the submission of a report prepared by a suitably qualified arborist.
3. Proposals for substantial pruning to a Significant Tree may require the submission of an arborist report prepared by a suitably qualified consultant demonstrating that the proposal is acceptable and will not endanger the tree's survival or fore-shorten its life expectancy.

(5) Structure Plans and Subdivision Proposals

1. Subdivision proposals for heritage places should be designed to retain an appropriate setting for any elements which contribute to its heritage significance. This includes the retention of original garden areas, landscaping features or other features that are considered essential to the setting of the heritage place or its heritage significance.
2. Subdivision proposals that indicate the required demolition, partial demolition or modification to a place on the Heritage List or State Register of Heritage Places will not be supported without a Heritage Impact Statement accompanying the subdivision proposal. This is to be prepared in accordance with the Heritage Council of WA guidelines.
3. Where a structure plan is proposed for land that includes a heritage place(s) the structure plan should demonstrate how matters of heritage significance will be appropriately addressed.
4. Where a structure plan area includes more than one heritage place, or includes a heritage place that comprises a number of buildings or features the City may require the preparation of an overall heritage strategy to be included with the structure plan report. This should demonstrate how heritage issues will be addressed; outline principles to be addressed in later planning stages; and include recommendations for interpretation (refer to 7.0).
5. Consideration should also be given to how future development of the subdivided land is likely to affect the identified heritage significance of the heritage place, particularly its setting.

Title	Heritage Conservation Design Guidelines
Policy Number (Governance Purpose)	LPP 4.4



Attachment 10 Heritage Conservation Design Guidelines

(6) Applications for Planning Approval for Places on the Heritage List

1. In addition to the information required by TPS 3, the following provides a guide for accompanying material and information that may be required to be submitted with planning applications for places on the Heritage List.
2. For larger and more complex development proposals, a Heritage Impact Statement should be submitted that identifies how the heritage significance of the place will be affected by the proposed works or future use. The statement should be prepared by a heritage professional, and should be consistent with the Heritage Council of WA's guidelines.
3. If a proposal affects a place that is entered on the State Register of Heritage Places the City may require the preparation of a Conservation Plan, which is to be prepared by a qualified heritage professional, and consistent with the Heritage Council of WA's guidelines.
4. Where proposed extensions and alterations involve modifications to external areas and features of a place that form part of its heritage significance or are important to its setting a site landscaping plan may be required, demonstrating how the impact will be managed, and this should be included in the Heritage Impact Statement where relevant.
5. Where a Conservation Plan exists for a Heritage place, the development application should include information regarding how the conservation policies and any urgent works identified in the Conservation Plan will be addressed.
6. Where structural failure is cited as justification for demolition of a place on the Heritage List the onus rests with the applicant to provide a clear justification for demolition, and evidence should be provided from a registered structural engineer that the structural integrity of the building has failed to the point where it cannot be rectified without the removal of a majority of its significant fabric and/or prohibitive costs.

(7) Interpretation and Interpretation Plans

Interpretation can enhance understanding and enjoyment of heritage places, and it can strengthen and sustain the relationships between the community and its heritage. Interpretation can be an integral part of the experience of a heritage place, particularly where the heritage significance of the place is not readily apparent. Accordingly the following policy provisions are applicable:

1. Opportunities for the interpretation, commemoration and celebration of significant associations between people and a place should be investigated and implemented wherever possible. In particular, the City may require the preparation of interpretative material as a condition of development approval for the following proposals:

Title	Heritage Conservation Design Guidelines
Policy Number (Governance Purpose)	LPP 4.4



Attachment 10 Heritage Conservation Design Guidelines

- (a) Major redevelopment that involves substantial modifications to a heritage place or modifications that will negatively impact on the heritage significance of the place.
 - (b) Changes of use for a heritage place, particularly where the original use will no longer be readily apparent.
 - (c) Proposals that will result in the heritage significance of the place not being readily apparent, and which could be explained and enhanced by interpretation.
 - (d) Proposals where there is the opportunity for the re-use of hardware or artefacts that are associated with the former use in interpretive material.
 - (e) Proposals that will substantially impact on the heritage significance of the place.
 - (f) Demolition (full or partial) of a heritage place.
2. An interpretation plan may be required where the proposal involves the substantial redevelopment of a major site, such as a commercial or industrial site, particularly where there may be more than one heritage place affected by the proposal.

Strategic Link:	Town Planning Scheme No. 3
Category	Planning - Town Planning & Development
Lead Business Unit:	Development Assessment and Compliance
Public Consultation: (Yes or No)	Yes
Adoption Date: (Governance Purpose Only)	10 November 2022
Next Review Due: (Governance Purpose Only)	November 2024
ECM Doc Set ID: (Governance Purpose Only)	4517607

Title	Naval Base Holiday Park Heritage Area
Policy Number (Governance Purpose)	LPP 4.5



Attachment 11 Naval Base Holiday Park Heritage Areas

Policy Type

Local Planning Policy

Policy Purpose

Naval Base Holiday Park Heritage Area comprises six rows of campsites and 178 holiday shacks settled on the mainland, along Cockburn Road in Henderson.

The Naval Base Heritage Area has a continuous history as a holiday destination since 1933, and has been used by generations of families from Cockburn and the wider area, and there are a number of extended families that have a long association with the site.

It is significant as a good example of the holiday camps which used to exist along the Western Australian coastline but have disappeared in the face of development.

Overall, there is a uniformity of scale, with many of the shacks being within the original size limit of 16' x 17' (approximately 5.2m x 5.2m).

The original shacks comprise a caravan with a permanent walled annexe extension. Subsequently, permanent shacks have been built, in theory with a maximum sized building of 16' x 17'. Most of the shacks are a conglomeration of assorted building fabric (often recycled from other buildings), and vary in their condition. Historically there has been an 'unwritten rule' that states that extensions to shacks must only be to the east or west so as not to block views to the ocean.

The shacks have no internal running water and no externally supplied electricity. Residents cook and power refrigerators with gas, use solar panels for electrical power and get their water from nearby communal taps. Two large ablution blocks are available for the residents.

There is little to no vegetation on the site but some of the shacks are surrounded by lawn.

Although individual shacks have changed since 1933, as a holiday destination the overall site has a high level of integrity and authenticity.

The Naval Base Holiday Park Heritage Area is designated under the Heritage Protection provisions of City of Cockburn Town Planning Scheme No. 3 (TPS 3).

Archaeological Potential

The Naval Base Holiday Park Heritage Area is also associated with the attempts at settlement by Thomas Peel in 1829. Consequently, it is possible that there are archaeological remains at the site which may warrant investigation at a future time.

Title	Naval Base Holiday Park Heritage Area
Policy Number (Governance Purpose)	LPP 4.5



Attachment 11 Naval Base Holiday Park Heritage Areas

Application:

This policy applies to all development within Reserve 24308, as shown in Appendix A.

The purpose of this policy is:

- (1) To set out a statement of significance for the Naval Base Holiday Park Heritage Area, and to clearly identify the characteristics that are essential to the heritage significance;
- (2) To set out design guidance for development within the Naval Base Holiday Park Heritage Area to ensure conservation of the heritage significance of the area.

Objective:

The key objective of this Policy is to ensure that development within the Naval Base Holiday Park Heritage Area respects the heritage significance of the area by having due regard to the characteristics that have been identified in this Policy as essential to the heritage significance.

Policy Statement

(1) Statement of Significance

Naval Base Holiday Park has been a holiday destination since 1933, and has been used by generations of families from Cockburn and the wider area. It has exceptional social historical value as a popular and well used holiday destination.

Naval Base Holiday Park is a good example of holiday camps which used to exist along the Western Australian coastline but have disappeared in the face of development. As a holiday destination the overall site has a high degree of authenticity.

Although the condition of individual shacks varies, as a whole the shacks at Naval Base Holiday Park have a uniformity of scale that creates a unique landscape, and gives the place significant aesthetic appeal, especially when combined with the coastal setting.

Individual shacks within the Heritage Area have changed over time. The design and construction of the shacks demonstrates the resourcefulness, versatility and creativity of the shack builders and their occupants.

The evolution of the shacks is considered to be one of the distinguishing characteristics of the Heritage Area, and it is acknowledged that individual shacks and the overall site plan will continue to evolve over time. However, it is considered important that this occurs in a manner that does not adversely impact on the heritage significance of the Heritage Area.

For this reason it is not considered that there is any distinction in the level of contribution made by individual shacks to the heritage significance of the Heritage

Title	Naval Base Holiday Park Heritage Area
Policy Number (Governance Purpose)	LPP 4.5



Attachment 11 Naval Base Holiday Park Heritage Areas

Area. Rather, it is considered that the following overall characteristics of the Heritage Area are essential to conserving its heritage significance:

1. The ongoing use of the site as a holiday destination;
2. General uniformity of scale of the shacks;
3. Modest scale and simple informal 'holiday shack' character of the structures;
4. General uniformity of siting and spacing between the shacks, noting that the future spacing will have to meet other regulatory requirements.

(2) Development of Shacks within the Heritage Area

1. Any extensions or modification to existing shacks need to meet the requirements of the Building Code of Australia.
2. Shacks should be of a small, modest scale with regards to their height and the size of the building footprint. They should not exceed a maximum building footprint of 5.2m x 5.2m, with one additional 1.8m wide verandah permitted on one side of the shack where it is open on three sides.
3. Additional ancillary structures such as outbuildings (sheds) and carports will generally not be supported.
4. Shacks should be constructed of lightweight materials that are indicative of the informal 'holiday shack' character.
5. Development of new shacks needs to be consistent with the overall site plan and be contained within the boundaries of their designated site area.

(3) Other Development within the Heritage Area

1. Any other development within the Heritage Area is to respect the heritage significance of the area by having due regard to the characteristics that have been identified as essential to conserving its heritage significance.
2. New buildings or structures within the Heritage Area should be designed and located in a way that does not overwhelm or dominate the shacks.
3. New buildings or structures within the Heritage Area should complement the modest scale and simple informal 'holiday shack' design characteristics of the shacks in terms of bulk, style, materials, and form.
4. The site of the Naval Base Holiday Park Heritage Area may have archaeological potential, since the area is associated with an early attempt at settling the area by Thomas Peel. Therefore any proposed earthworks, excavation or development outside existing designated shack sites should be subject to an archaeological investigation prior to the commencement of any works.

Title	Naval Base Holiday Park Heritage Area
Policy Number (Governance Purpose)	LPP 4.5



Attachment 11 Naval Base Holiday Park Heritage Areas

APPENDIX A - LOCATION PLAN

NAVAL BASE HOLIDAY PARK HERITAGE AREA



Title	Naval Base Holiday Park Heritage Area
Policy Number (Governance Purpose)	LPP 4.5



Attachment 11 Naval Base Holiday Park Heritage Areas

Strategic Link:	Town Planning Scheme No. 3
Category	Planning - Town Planning & Development
Lead Business Unit:	Development Assessment and Compliance
Public Consultation: (Yes or No)	Yes
Adoption Date: (Governance Purpose Only)	10 November 2022
Next Review Due: (Governance Purpose Only)	November 2024
ECM Doc Set ID: (Governance Purpose Only)	4517732

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Policy Type

Local Planning Policy

Policy Purpose

The Cockburn Coast Design Guidelines for the Robb Jetty and Emplacement Precinct have been prepared to guide the development and urban form (including subdivision) of Robb Jetty Local Structure Plan and Emplacement Local Structure Plan (LSP) areas.

The Design Guidelines will guide the creation of a quality development that ensures the design principles of the Robb Jetty and Emplacement LSP's are achieved.

Policy Statement

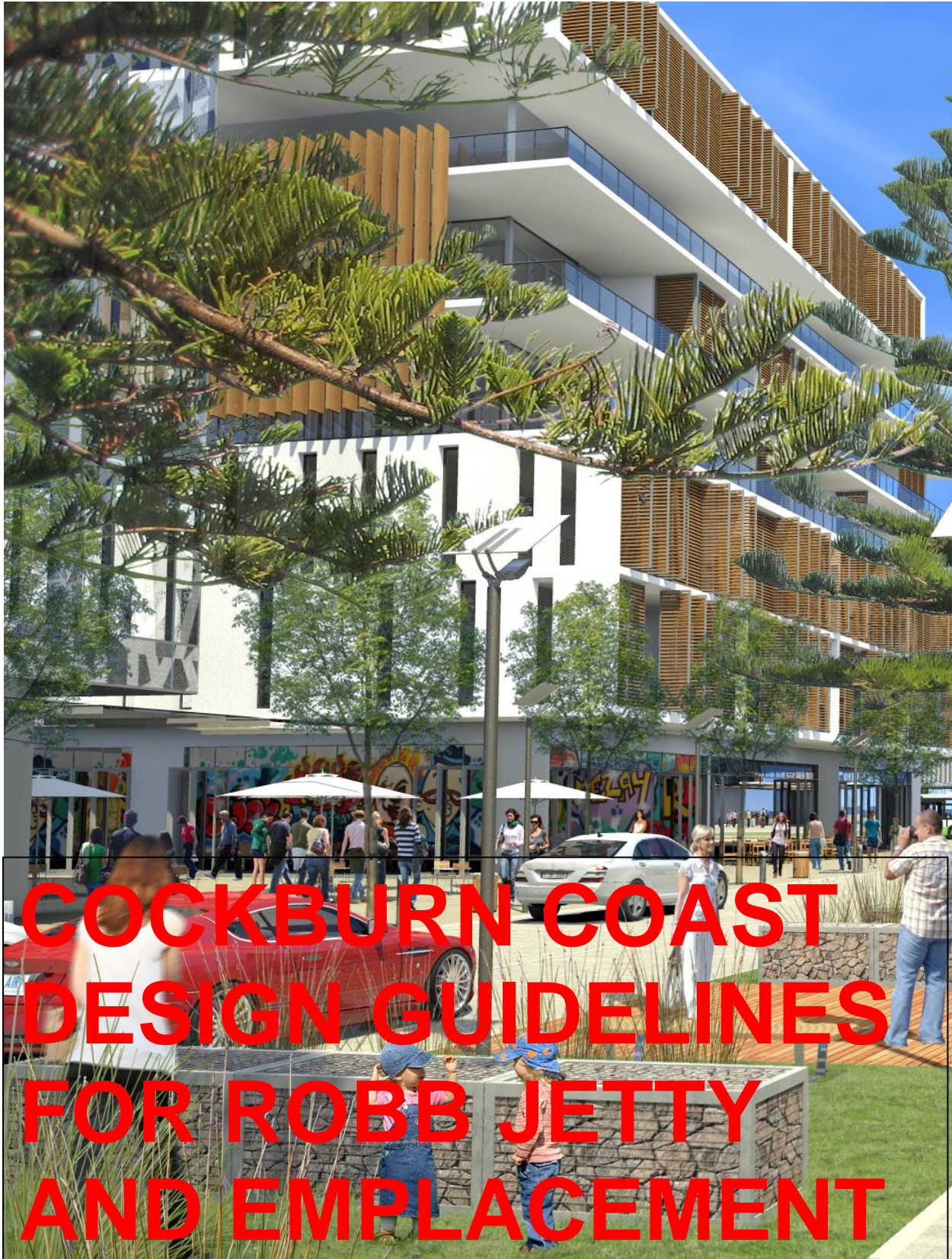
Appendix 1 contains the Cockburn Coast Design Guidelines for the Robb Jetty and Emplacement Precinct.

Development applications will be assessed under the Design Guidelines in conjunction with the Residential Design Codes of Western Australian (R-Codes), the approved structure plan, an approved local development plan and any other relevant local planning policy.

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts



Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Appendix 1: LPP 4.6 Cockburn Coast Design Guidelines for Robb Jetty and Emplacement

1.	Background	4
1.1	Introduction	4
1.2	Vision for Cockburn Coast	4
1.3	Context	6
1.4	Approach	7
1.5	Objectives	7
1.6	Purpose	8
1.7	Design Guideline Policy Area	8
1.8	Relationship to Relevant Planning Documents	9
1.9	Relationship to the Robb Jetty LSP and Emplacement LSP	9
1.10	Guideline Framework	9
1.11	Discretion	10
1.12	Definitions	10
1.13	Development Process	10
2.	Typology Specific Guidelines	11
2.1	Activity Centre – Main Street Typology	13
2.2	Mixed Use – Cockburn Road Typology	18
2.3	High Density Residential Typology	24
2.4	Medium Density Residential Typology	30
2.5	Mixed Residential Typology	35
3.	General Provisions	43
3.1	Built Form Requirements	43
3.1.1	Building Height	43
3.1.2	Facades	44
3.1.3	Roof Form	46
3.1.4	Lighting	46
3.1.5	Acoustics and Vibration	47
3.1.6	Active Edges and Street Relationship	48
3.1.7	Heritage Considerations	48
3.2	Service Infrastructure and Access	50
3.2.1	Internal Access	50
3.2.2	Parking	50
3.2.3	Parking Location and Access	50
3.2.4	Sleeved Parking	51
3.2.5	End of Trip Facilities	51
3.2.6	Site Services	52
3.3	Sustainability Requirements	53
3.3.1	Sustainable Travel	53
3.4	Laneways	53
3.4.1	Residential and Commercial Laneways	53
3.5	Communal Open Space	55

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

1. Background

1.1 Introduction

The Cockburn Coast Design Guidelines for the Robb Jetty and Emplacement precincts (henceforth referred to as the Design Guidelines) have been prepared to guide the development and urban form (including subdivision) of *Robb Jetty Local Structure Plan* (Robb Jetty LSP) and *Emplacement Local Structure Plan* (Emplacement LSP) areas. The design guidelines are focused on the creation of a quality development that ensures the design principles of the Robb Jetty and Emplacement LSP's are achieved.

The design guidelines will bring to fruition a lively and sustainable urban centre set amongst dense residential development. The design guidelines introduce standards for development to create the intended character and amenity within the Robb Jetty LSP and Emplacement LSP areas. Although some of the criteria are mandatory, the general approach is to provide a series of broad principles for development to follow while allowing flexibility in design outcomes over the project life span.

The design guidelines are a performance orientated assessment tool. Each design element is expressed as a design objective and one or more assessment criteria. Where a stated assessment criterion is proposed to be varied, development must demonstrate that it meets the related design objective. In this way a performance approach to design and assessment is facilitated.

The design guidelines are divided into two main sections:

Typology Specific Guidelines

A series of built form typologies are established in defined areas where specific guideline provisions apply that may expand on or vary the general provisions.

General Provisions

Contain the design guideline general provisions which are applicable to all development.

1.2 Vision for Cockburn Coast

Capitalising on a rare opportunity, these design guidelines set out to inform the development of an exciting mixed use community that celebrates the best of the Western Australian coastal lifestyle.

Cockburn Coast will be different from its neighbouring suburbs; it will be a place that offers choice and variety of living, recreation and working opportunities. Core to the success of the redevelopment is a well-connected Bus Rapid Transit (BRT) system which is intended to link the development to its surrounding areas. As well as connecting the design guideline area to its surrounds, this system will provide an internal system of movement which encourages more sustainable personal transportation choices.

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

The City of Cockburn's *Cockburn Coast District Structure Plan* (DSP) and *Cockburn Coast District Structure Plan Part 2* (DSP2) nominates three local structure plan areas being Robb Jetty, Emplacement and Power Station. Each of these areas is distinct in character and function. These design guidelines introduce standards for development to create the intended character and amenity within the Robb Jetty and Emplacement LSP areas following a detailed local structure planning process.

Robb Jetty LSP Area

The Robb Jetty LSP area forms the north-western portion of the site and stretches from Rollinson Road in the *north*, to the Parkland Corridor in the south and Cockburn Road in the east. The area stretches west of Robb Road but excludes the beach.

The Robb Jetty LSP area will contain elements of mixed use development along significant road links including Cockburn Road an activity centre main-street, and a Mixed Business' precinct, but is otherwise set aside for medium to high density residential development. The area will also house supporting community facilities in the form of the two storey urban primary school and the area's key active playing field. A coastal character is proposed to complement the adjacent foreshore and areas of open space contained within it.

The BRT public transport alignment is set to pass through the heart of the area and be well connected to Fremantle and the rapidly emerging Cockburn Central. A variety of small but connected public spaces will offer a range of experiences from the quiet to the communal, the sheltered to the open and the organic to the formal.

Emplacement LSP Area

The Emplacement LSP area forms the north-east portion of the project area and stretches from the northern boundary of the master plan area, to the middle parkland corridor to the south, to Cockburn Coast Drive in the east, and Cockburn Road in the west.

The distinct character of the Emplacement LSP area is a product of its elevated topography and this landform influences how it shall be treated. Development will be responsive to the topography and shall aim to retain as much of the existing natural character of the site as possible. The Emplacement LSP area will be predominantly mixed use in its north, residential in its south, and contain the east-west linear parks, providing strong connections from Beeliar Park and through Robb Jetty LSP area to the coastal foreshore.

The Emplacement LSP area will be the new highpoint, a manufactured horizon line that offers the opportunity for a new architectural topography and an integrated landscape of nature and built form.

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6

Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

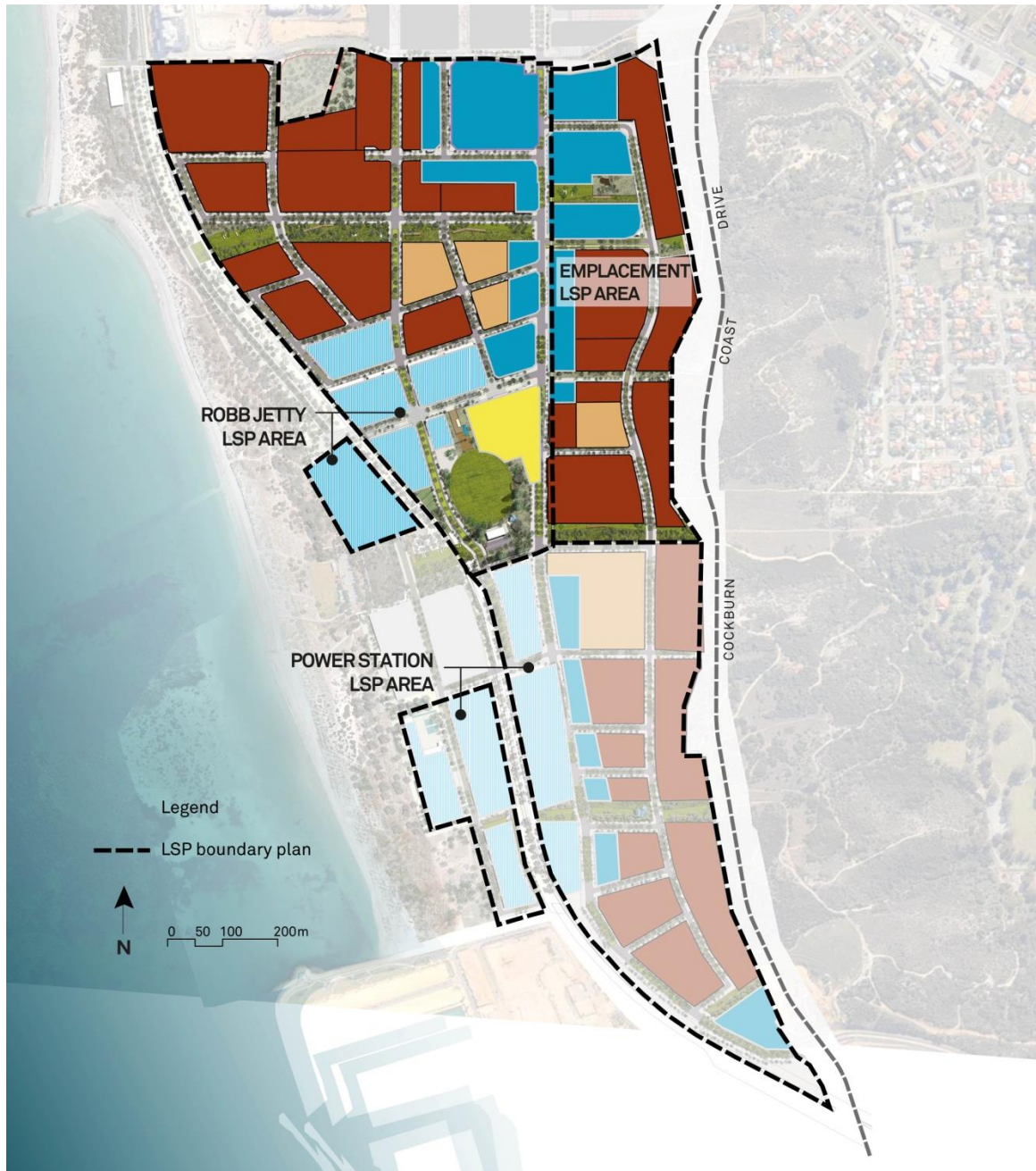


Figure 01_Cockburn Coast Local Structure Plan areas

1.3 Context

The design guidelines complete a complex process of strategic planning to capitalise on the opportunity for redeveloping Cockburn Coast identified in the Western Australian Planning Commission’s strategic planning document *‘Directions 2031 and Beyond’*. The adoption of the DSP and later DSP2 2012 served to solidify the recognition of the Cockburn Coast’s potential and identifies a number of key drivers and opportunities that underpin the vision and intent of the DSP and DSP2. Following an amendment (Amendment 89) to the City of Cockburn Town Planning Scheme No. 3 (The Scheme), which aligns the City of Cockburn’s (the City) planning framework with that as proposed in the DSP and

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

DSP2, local structure plans were produced for the Robb Jetty LSP and Emplacement LSP areas which establishes a development agenda and expands on the foundations of the DSP and DSP2.

These design guidelines bring to fruition a vision established and carried forward through a number of strategic planning documents and processes.

1.4 Approach

The DSP established a vision which remains relevant to the ongoing planning of Cockburn Coast:

“To create a vibrant, landmark destination that is connected, integrated, diverse and accessible.”

The vision seeks to create a place that offers new and exciting living, employment and recreation opportunities, whilst providing an appropriate level of compatibility and support for adjoining residents and existing enterprises in the area. These design guidelines are set to establish this vision by creating a sustainable community that celebrates the area’s past as well as taking on creative ideas, innovation and development. Cockburn Coast will be an easily accessible place, with an integrated transit system offering contemporary lively cafes, restaurants, shops, residential and commercial areas, tourism, cultural and recreation activities.

Integral to the vision of Cockburn Coast is the intention to establish a new benchmark for sustainable urban development. This means creating a place where people not only want to live and work today, but also in the future. Sustainable communities cater to the different needs of all its residents; they are safe and inclusive and offer equality of opportunity, they are sensitive to their environment and contribute to a high quality of life.

1.5 Objectives

The development of Cockburn Coast is guided by a number of key objectives or drivers which will bring to fruition the vision of a sustainable landmark destination. These objectives have influenced the preparation of the design guidelines and underpin their purpose, being to:

- create a hierarchy of coastal nodes providing for the needs of local residents and visitors alike;
- create physical and emotional links between the urban environment and the coast allowing the coastal experience to translate into the urban setting;
- provide attractive, pedestrian-oriented streets and public spaces that create an environment for positive community engagement and business exchange;
- enable buildings and public realm to engage with pedestrians and facilitate a comfortable and safe urban environment;
- allow for activation at ground level by retail and hospitality uses in key streets identified by the Local Structure Plans;
- optimise residential development potential whilst maintaining the intended character of the Cockburn Coast;
- minimise the impact of car parking on the pedestrian experience and quality of the public realm;

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

- create a sustainable environment that allows for the implementation of green infrastructure; and
- promote the use of sustainable modes of transport and a health way of living through active engagement with the urban environment.

1.6 Purpose

These design guidelines have been prepared to guide development within the Robb Jetty LSP and Emplacement LSP areas under the Scheme. Implementation of the guidelines will ensure the realisation of Cockburn Coast as an urban environment providing both local and district centre activity centres.

1.7 Design Guideline Policy Area

These design guidelines apply to the area of land within the Robb Jetty LSP and Emplacement LSP, henceforth referred to as the policy area. The policy area is bound by:

- Rollinson Road to the north;
- South Fremantle Power Station and the Western Power Switchyard to the south;
- Beeliar Regional Park to the east; and
- The foreshore reserve to the west.



Figure 02_Design Guidelines Policy Area

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

1.8 Relationship to Relevant Planning Documents

The design guidelines are adopted under the provisions of section 4 of the Planning and Development Regulations 2015 Procedures for Making Local Planning Policy. The provisions of these design guidelines vary the requirements of the State Planning Policy 7.3 Residential Design Codes (R-Codes). Where these design guidelines are silent the provisions of the R-Codes and relevant local planning policies apply. It should be noted that the plot ratios of the R-Codes are varied and plot ratio will not form part of the assessment criteria for proposals in these precincts.

These design guidelines should be read in conjunction with the Scheme, the Robb Jetty LSP, the Emplacement LSP, any relevant Local Development Plan (LDP) and the R-Codes. In determining any application for development approval, the City will utilise these design guidelines in conjunction with the Scheme, any relevant LDP and policies.

1.9 Relationship to the Robb Jetty LSP and Emplacement LSP

The Robb Jetty LSP and Emplacement LSP set out a number of development objectives relating to the DSP2 redevelopment area. In particular they establish land use, movement, activity, urban form and resource enhancement development standards to ensure Cockburn Coast operates as an effective urban environment.

These design guidelines build upon both LSPs and provide more detailed guidance on development standards in the form of an adopted local planning policy.

1.10 Guideline Framework

The detailed design guidelines contained in the General Provisions section are set out with the following framework:

Design Objective:

Statements outlining the design philosophy and intent of the assessment criteria. It is mandatory for development to meet the design objective.

Assessment Criteria:

Standards that sets out the specific criteria to satisfy an associated design objective. Compliance with the applicable assessment criteria will achieve the design objective. However individual criteria are not mandatory and alternative solutions for complying with the design objective will be considered on a performance basis subject to supporting evidence.

The typology specific section of the design guidelines contains character statements. The character statements guide both the design objective and assessment criteria and as such, all development shall be consistent with the relevant character statement.

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

1.11 Discretion

An important provision within the design guidelines is the opportunity for the applicant or owner to meet the design objective through an alternative solution.

The City may approve a development application or Local Development Plan where the applicant or owner has departed from the recommended assessment criteria. Variations may be considered where, in the City's opinion, the applicant or owner has demonstrated that the alternative solution is consistent with the Robb Jetty LSP or Emplacement LSP where relevant and meets the design objective. Variations will be considered where a proposal does not include an affordable housing component, but will be considered more favourably where it does.

Where a development proposal is determined to be inconsistent with a design objective in a manner that may impact on the public realm or adjoining properties then the proposal may be refused or referred to Council for determination.

Where the applicant or owner has provided a sufficient affordable housing component, a relaxation of the assessment criteria may be considered where the alternative solution is consistent with the relevant LSP and meets the design objective. These design guidelines provide further guidance on those criteria considered suitable for variation.

Each application for development approval will be assessed on an individual basis and the approval of an alternative solution will not set a precedent for other developments.

1.12 Definitions

Noise Sensitive Premises (as defined in the Environmental Protection (Noise) Regulations 1997) includes premises occupied solely or mainly for residential or accommodation purposes, and premises used for the purpose of a hospital, sanatorium, educational establishment, public worship, aged care or child care.

Commercial Laneway includes any laneway within the mixed use or activity centre typology areas as set out by these design guidelines.

All definitions included in the R-Codes are applicable to land affected by these Design Guidelines.

1.13 Development Process

Owners, developers and/or agents are encouraged to arrange pre-application meetings with the City's Planning Department prior to lodgement of a formal development application. Once a development application is lodged, it will be assessed by the City to verify it meets all applicable design objectives and assessment criteria.

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts



Activity Centre – Main Street Typology



Mixed Use – Cockburn Road Typology



High Density Residential Typology



Medium Density Residential Typology

2. Typology Specific Guidelines

The policy area is divided into a number of built form typologies each with their own distinct character and function. There are also a number of landmark and gateway sites identified by the built form typology location plan. These sites are to be developed with a diverse and active facade to facilitate way finding and reflect the natural hierarchy and land use of the area.

Activity Centre - Main Street Typology

Development in this area addresses and activates the identified pedestrian oriented “main” street whilst a high quality public realm creates a comfortable place in which locals meet and conduct business. This area provides a key link between the ocean and urban environment as well as providing for the retail and local service needs of the local community.

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Mixed Use - Cockburn Road Typology

A range of retail and commercial functions complemented by residential development are to be accommodated within this mixed use area. The presence of Cockburn Road informs the scale and built form of development and necessitates the promotion of an active ground floor.

High Density Residential Typology

The most intensely developed residential typology to afford the greatest access to the proposed bus rapid transit system. High density residential development is to create a new skyline in Cockburn Coast.

Medium Density Residential Typology

Providing a mix of housing opportunities near the Activity Centre, this typology will feature soft landscape public realm and contemporary urban development ranging from terrace housing to medium scale apartment style buildings.

Mixed Residential Typology

A special urban precinct designed around exploring contemporary urban forms, featuring a mix of housing opportunities, with living choices ranging from high density apartments to compact terrace housing, punctuated by innovative shared access streets and open spaces.

Mixed Business Typology

Facilitating a transition from the existing industrial businesses to lighter industrial or commercial uses that do not have a detrimental impact on the amenity of future surrounding residential uses. All new built form proposals shall address an interface that is compatible and complementary with surrounding existing or future residential or mixed use development.

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

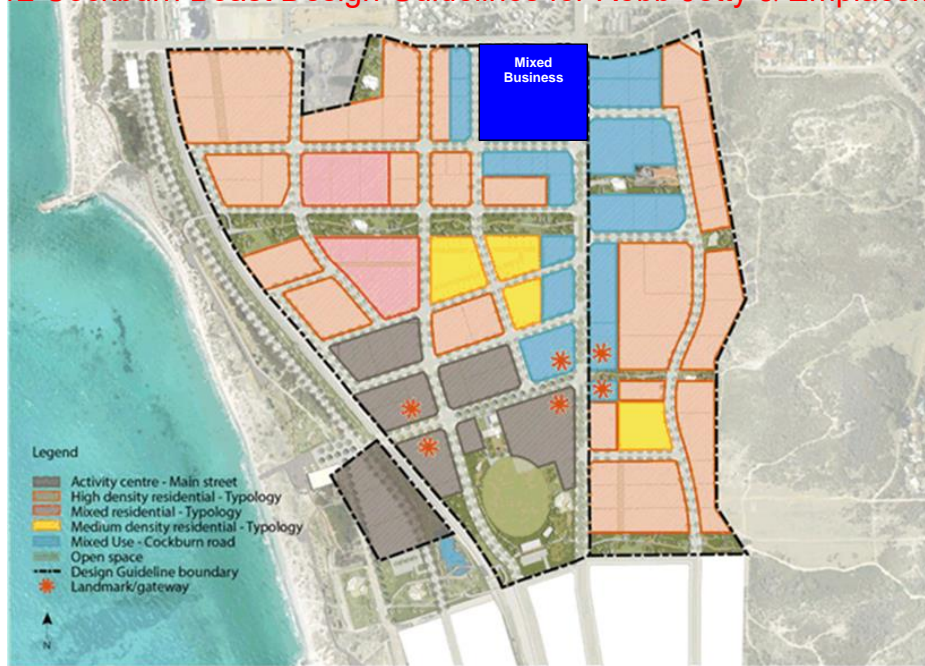


Figure 03_Built Form Typologies

2.1 Activity Centre – Main Street Typology

The activity centre typology is primarily a place for local residents and businesses, a walkable village that is intimate in scale and ‘soft’ in character. The beach comes to the main street and a variety of small but connected public spaces offer a range of experiences from the quiet to the communal. Buildings and land use will facilitate the creation of a central shopping and activity zone resulting in a walkable community hub.

The Main Street provides a convenient and inviting local shopping experience intended to be serviced by a rapid bus transit system. Street trading and active retail is concentrated in the western portion of the area creating a vibrant community hub. A diverse and contiguous streetscape will be developed with civic, business and retail services ensuring a suitable business mix. The oval and park within the activity centre typology represents the traditional village green and is therefore the focus of active recreation at Cockburn Coast. It is a place to be shared harmoniously by many for diverse purposes.

The built form is encouraged to take advantage of the abundant natural assets and create a comfortable outdoor environment that encourages social interactions in a relaxed and personal environment. Future built form should embody the feeling of seamless transition, from indoor to outdoor, from formal to informal, from exposed to protected. Respectful of nature, built form should reflect the natural characteristic of the vegetation and landscape.

Buildings generally of 5 to 8 storeys in height will promote a pedestrian friendly place through podium style built form and a focus on ground floor activation. Development embodies a warm architectural finish through the use of natural materials, whilst street awnings, wide footpaths and soft landscaped edges create a sense of intimacy and shelter pedestrians.

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Opportunities for laneway development enhance and celebrate the distinctive environment by reflecting the neighbourhood character whilst allowing for it to be developed as a secondary small street. Laneways containing commercial uses will be characterised by small scale tenancies, evolving over time to provide an intimate and unique experience.



Figure 04_Activity Centre built form typology

Building Setbacks

Design Objective

- I. Building setbacks create tightly framed streetscapes and public open spaces
- II. Building setbacks help create highly urban streetscapes

Assessment Criteria

- i. Building setbacks are to be in accordance with the following table

Setback	Street (minimum and maximum)	Side/Rear (minimum m)	Laneway (minimum)	Public Open Space (minimum)
Levels 1-5	Nil	Nil	Nil	4m to wall and 2m to balconies (cantilevered/Light weight only)

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Levels 6+	5m to wall and 2m to balconies (cantilevered/light weight only)	3m	3m	5m to wall and 2m to balconies (cantilevered/light weight only)
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* Where there is a commercial laneway the minimum setback above 3 storeys should be a distance equivalent to the width of lane unless a variation to the assessment criteria outlined in clause 3.4.1(ii)c of the general provisions is granted

Table 01_ Building Setbacks for Activity Centre

- ii. The public open space setback area shall include space for landscaping and if necessary an outdoor living area. Where additional outdoor living area is to be provided, the additional outdoor living area shall be absorbed into the building space (i.e. building shall cantilever over the outdoor living area)
- iii. Projections are permitted within the 4m setback to public parkland to maximum of 2m into the setback area
- iv. Balconies will be supported within the nil setback on levels 1-5 where a substantial facade is provided to ensure a continuous built form
- v. Balconies for Levels 6+ proposed to be setback between 2-5m shall be lightweight/cantilevered only

Building Articulation

Design Objective

- I. To ensure that building facades add positively to the public realm and its interest. Building articulation will encourage interaction with the street and passive surveillance of adjacent spaces
- II. To promote a pedestrian scale of buildings at street level
- III. The building design shall demonstrate an appropriate level of articulation to avoid building bulk appearing excessive
- IV. Building articulation will express a vibrant and modern design aesthetic

Assessment Criteria

- i. Permanent blank walls are not permitted to any street frontage. Major openings are required to provide for surveillance and interaction with the public realm
- ii. For commercial street level frontages a minimum of 80% of the frontage shall be glazed. For the street frontage for all upper floors a minimum of 40% of the frontage shall be glazed
- iii. Mixed use buildings should provide separate entries for non-residential and residential uses for legibility of pedestrian access
- iv. The facade detail may be simplified on loading areas, parapet walls and walls to 'back of house' areas
- v. Corner buildings are to address both frontages through the provision of:
 - a) distinct roof form at corners;
 - b) variation in materials and colours; and
 - c) varied balcony treatments.

Building Levels

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Design Objective

- I. To ensure development maintains a positive relationship with the street such that pedestrian movement, sight lines and streetscape character are maximised
- II. To allow for the safe use of ceiling fans for cooling

Assessment Criteria

- i. Floor to floor heights on the ground floor should be 4.5m to allow for commercial use. Apartments shall achieve minimum floor to ceiling levels as outlined in Clause 4.3 of the Residential Design Codes Volume 2 – Apartments.
- ii. All other floors shall maintain a 3.1m floor to floor height for residential use and a 3.6m floor to floor height for commercial use
- iii. The ground floor should be flush with the adjacent footpath at the boundary
- iv. All development is to achieve a minimum finished floor level of +3.8AHD to ensure development takes into account coastal erosion and accretion patterns. Non habitable rooms and the provision of basement parking are exempt from the finished floor level stated above

Awnings

Design Objective

- I. To encourage a pedestrian scale of development
- II. To provide shelter from environmental conditions
- III. To encourage a seamless flow of the use and function of a building from internal to external
- IV. To maintain a safe separation between passing traffic and awnings

Assessment Criteria

- i. Awnings over footpaths are to be provided for no less than 80% of the primary and secondary street frontage. This requirement does not apply to laneways
- ii. The vertical clearance of awnings shall be consistent and generally 3.2m from pavement level
- iii. Awnings shall project 3.5m from the building line except where this results in a setback between to the awning and the outer edge of the road pavement of less than 0.6m
- iv. Adjoining awnings are to form continuous coverage over the footpath
- v. Awnings are to be provided with non-structural veranda posts along the Robb Jetty Main street. In this respect awnings are to be suspended by cantilevered construction and not use load bearing posts

Building Height

Design Objective

- I. Building heights help create a compact urban built environment
- II. Consistent building heights create a recognisable urban character
- III. Building heights mean the Activity Centre Typology area is highly visible from a distance
- IV. Building heights do not visually overwhelm the streetscape
- V. Building heights avoids continual overshadowing of the streetscape

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Assessment Criteria

- i. Building heights shall be in accordance with the Building Height Plan (Figure 14)
- ii. Development shall be a minimum of three storeys and six storeys (depending on site)

Building Materials

Design Objective

- I. To encourage a style of development that is consistent with the coastal location
- II. To provide for a consistency in the standard of finish and materials throughout Cockburn Coast.
- III. To foster a sense of place through an identifiable character and style of development

Assessment Criteria

- i. Extensive use of concrete tilt panels is discouraged. Where concrete tilt panels are used, they shall be integrally coloured (colour tinted concrete)
- ii. Moulded textures imprinted in the external surfaces of any concrete panels are encouraged
- iii. Painted finishes and rendered textures over concrete panels are not permitted
- iv. The use of natural materials such as stone, timber and other such natural products is encouraged in both interior and exterior finishes

Open Space

Design Objective

- I. To ensure that development provides an attractive and engaging interface with the public open space
- II. To maximise the potential for passive surveillance

Assessment Criteria

- i. Where an area of public open space is provided the surrounding development must address the open space by maximising passive surveillance from habitable rooms; buildings must front onto the open space through placement of doors, windows and balconies to create a safe and comfortable pedestrian environment

Landmark Sites

Design Objective

- I. To encourage a sense of place and identity
- II. To increase the legibility of place
- III. To demarcate the natural hierarchy of an area by identifying those places which are of significance

Assessment Criteria

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

- i. Sites in key locations have been nominated as landmark sites as shown in Figure 04 Built Form Typologies shall:
 - a) Promote prominent architectural form on corner elements to provide a reference point in the built form and landscape;
 - b) Encourage additional height elements where appropriate to create a point of difference with the balance of the development area and demarcate points of entry and prominence; and
 - c) Variations to setback requirements will be considered in order to create prominent feature elements.

Fencing

Design Objective

- I. To ensure that fencing does not detract from the function and appearance of the streetscape

Assessment Criteria

- i. Fencing is not permitted forward of the building line adjacent to the primary or secondary street frontage
- ii. The interface between private lots and the public open space may be fenced to a maximum height of 1.2m from natural ground level, but must be visually permeable above a height of 1.0m above natural ground level



Figure 05_Typical cross section for activity centre development

2.2 Mixed Use – Cockburn Road Typology

Cockburn Road is the main arterial road through Cockburn Coast and the policy area. Cockburn Road will be the focus of a mixed use form of development allowing for commercial, residential and retail uses. An active ground floor through retail and commercial uses will be encouraged with primarily residential development occupying the upper levels. The impact of the busy Cockburn Road will be softened by landscaping and an active footpath. Alfresco dining opportunities will be encouraged and facilitated by the built forms and land uses.

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
 The Mixed Use – Cockburn Road Typology as shown in Figure 06 below, applies to the Mixed Use and zone as shown on the approved Robb Jetty Local Structure Plan.



Figure 06_Mixed Use built form typology

Building Setbacks

Design Objective

- I. Building setbacks promote tightly framed streetscapes and public open spaces
- II. Building setbacks help create highly urban streetscapes

Assessment Criteria

- i. Building setbacks are to be in accordance with the following table

Table 02_ Building Setbacks for Mixed Use development

Setback	Street (minimum and maximum)	Side/Rear (minimum)	Laneway (minimum)	Public Open Space (minimum)
Levels 1-3	Nil	Nil	Nil	4m to wall and 2m to balconies (cantilevered/light weight only)

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Levels 4+	5m to wall and 2m to balconies (cantilevered/light weight only)	3m	3m	5m to wall and 2m to balconies (cantilevered/light weight only)
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* Where there is a commercial laneway the minimum setback above 3 storeys should be a distance equivalent to the width of lane unless a variation to the assessment criteria outlined in clause 3.4.1(ii)c of the general provisions is granted

- ii. Buildings shall be setback 4m from any boundary adjoining public parkland. This setback area shall include space for landscaping and if necessary an outdoor living area. Where additional outdoor living area is to be provided, the additional outdoor living area shall be absorbed into the building space (i.e building shall cantilever over the outdoor living area)
- iii. Projections are permitted within the 4m setback to public parkland to maximum of 2m into the setback area
- iv. Balconies will be supported within the nil setback on levels 1-5 where a substantial facade is provided to ensure a continuous built form

Building Articulation

Design Objective

- I. To ensure that building facades add positively to the public realm and its interest. Building articulation will encourage interaction with the street and passive surveillance of adjacent spaces
- II. To promote a pedestrian scale of buildings at street level
- III. The building design shall demonstrate an appropriate level of articulation to avoid building bulk appearing excessive
- IV. Building articulation will express a vibrant and modern design aesthetic

Assessment Criteria

- i. Permanent blank walls are not permitted to any street frontage. Major openings are required to provide for surveillance and interaction with the public realm
- ii. For commercial street level frontages a minimum of 80% of the frontage shall be glazed. For the street frontage for all upper floors a minimum of 40% of the frontage shall be glazed
- iii. Mixed use buildings should provide separate entries for non-residential and residential uses for legibility of pedestrian access
- iv. The facade detail may be simplified on loading areas, parapet walls and walls to 'back of house' areas
- v. Corner buildings are to address both frontages through the provision of:
 - a) distinct roof form at corners;
 - b) variation in materials and colours; and
 - c) varied balcony treatments.

Building Levels

Design Objective

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

- I. To ensure development maintains a positive relationship with the street such that pedestrian movement, sight lines and streetscape character are maximised
- II. To allow for the safe use of ceiling fans for cooling

Assessment Criteria

- i. Floor to floor heights on the ground floor should be 4.5m to allow for commercial use of the ground floor
- ii. All other floors shall maintain a 3.1m floor to floor height for residential use and a 3.6 metre floor to floor height for commercial use. Apartments shall achieve minimum floor to ceiling levels as outlined in Clause 4.3 of the Residential Design Codes Volume 2 – Apartments.
- iii. The ground floor should be flush with the adjacent footpath at the boundary
- iv. All development is to achieve a minimum finished floor level of +3.8AHD to ensure development takes into account coastal erosion and accretion patterns. Non habitable rooms and the provision of basement parking are exempt from the finished floor level stated above

Awnings

Design Objective

- I. To encourage a human scale of development
- II. To provide shelter from environmental conditions
- III. To encourage a seamless flow of the use and function of a building from internal to external
- IV. To maintain a safe separation between passing traffic and awnings

Assessment Criteria

- i. Awnings over footpaths are to be provided for no less than 80% of the primary and secondary street frontages. This requirement does not apply to laneways
- ii. The vertical clearance of awnings shall be consistent and generally 3.2m from pavement level
- iii. Awnings shall project 3.5m from the building line except where this resulting in a setback between to the awning and the outer edge of the road pavement of less than 0.6m
- iv. Adjoining awnings are to form continuous coverage over the footpath
- v. Any veranda post provided to an awning shall be non-structural. In this respect awnings are to be suspended by cantilevered construction and not use load bearing posts

Building Height

Design Objective

- I. Building heights help create a compact urban built environment
- II. Consistent building heights create a recognisable urban character
- III. Building heights do not visually overwhelm the streetscape
- IV. Building heights avoids continual overshadowing of the streetscape

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Assessment Criteria

- i. Building shall be in accordance with the Building Height Plan (Figure 14)
- ii. Development shall be a minimum of three storeys

Building Materials

Design Objective

- I. To encourage a style of development that is consistent with the coastal location
- II. To provide for a consistency in the standard of finish and materials throughout Cockburn Coast
- III. To foster a sense of place through an identifiable character and style of development

Assessment Criteria

- i. Extensive use of concrete tilt panels is discouraged. Where concrete tilt panels are used, they shall be integrally coloured (colour tinted concrete)
- ii. Moulded textures imprinted in the external surfaces of any concrete panels are encouraged
- iii. Painted finishes and rendered textures over concrete panels are not permitted
- iv. The use of natural materials such as stone, timber and other such natural products is encouraged in both interior and exterior finishes

Open Space

Design Objective

- I. To ensure that development provides an attractive and engaging interface with the public open space
- II. To maximise the potential for passive surveillance

Assessment Criteria

- i. Where an area of public open space is provided the surrounding development must address the open space by maximising passive surveillance from habitable rooms; buildings must front onto the open space through placement of doors, windows and balconies to create a safe and comfortable pedestrian environment

Landmark Sites

Design Objective

- I. To encourage a sense of place and identity
- II. To increase the legibility of place
- III. To demarcate the natural hierarchy of an area by identifying those places which are of significance

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Assessment Criteria

- i. Sites in key locations have been nominated as landmark sites as shown in Figure 06 Built Form Typologies. Development on Landmark Sites shall:
 - a) Promote prominent architectural form on corner elements to provide a reference point in the built form and landscape;
 - b) Encourage additional height elements where appropriate to create a point of difference with the balance of the development area and demarcate points of entry and prominence; and
 - c) Variations to setback requirements will be considered in order to create prominent feature elements.

Fencing

Design Objective

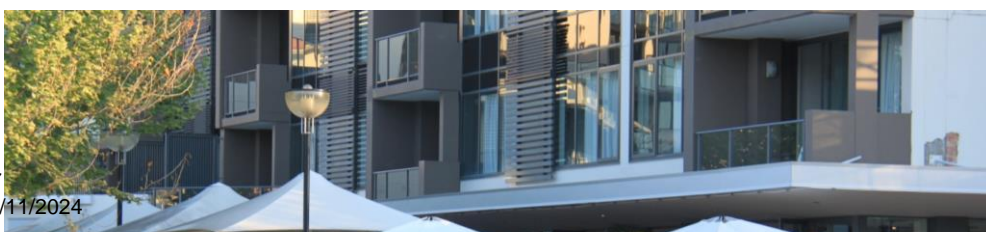
- I. To ensure that fencing does not detract from the function and appearance of the streetscape

Assessment Criteria

- i. Fencing is not permitted forward of the building line to the primary and secondary street frontages
- ii. The interface between residential development and the public open space may be fenced to a maximum height of 1.2m from natural ground level, but must be visually permeable above a height of 1.0m above natural ground level



Figure 07_Typical cross section for mixed use development



Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Mixed use development will encompass active street edges that create a comfortable pedestrian environment

2.3 High Density Residential Typology

High density housing opportunities along the Emplacement escarpment and within the Robb Jetty LSP area will create a new skyline for the Cockburn Coast. A manufactured horizon line of apartment buildings six to eight storeys in height will offer the opportunity for a new architectural topography and an integrated landscape of nature and built form. Residents will enjoy the expansive views but also the sense of containment and grounding in the environment. Facades and balconies shade and veil occupants whilst the ground level public realm is internalised and places focus on the residential communities' common interest.

Landscaped front setbacks and tree lined verges will combine to create a soft and comfortable urban setting for apartment buildings. Pocket parks and integrated greenery with built form create a calming natural feel throughout the area despite the intensity of development, acting as a backyard space and providing a link to the coast.

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6

Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts



Figure 8_High Density built form typology

Building Setbacks

Design Objective

- I. Building setbacks frame streetscapes and public open spaces
- II. Building setbacks accommodate landscaping which slightly widen and softens the streetscape

Assessment Criteria

- i. Building setbacks are to be in accordance with the following table

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Setback	Street (minimum)	Side/Rear (minimum)	Laneway (minimum)	Public Open Space (minimum)
Levels 1-3	3m	Nil	Nil	4m to wall and 2m to balconies (cantilevered/light weight only)
Levels 4+	5m to wall and 2m to balconies (cantilevered/light weight only)	3m	3m	5m to wall and 2m to balconies (cantilevered/light weight only)

Table 03_ Building Setbacks for high density residential development

- ii. Buildings shall be setback 4m from any boundary adjoining public parkland. This setback area shall include space for landscaping and if necessary an outdoor living area. Where additional outdoor living area is to be provided, the additional outdoor living area shall be absorbed into the building space (i.e. building shall cantilever over the outdoor living area)
- ii. Projections are permitted within the 4m setback to public parkland to maximum of 2m into the setback area

Building Articulation

Design Objective

- I. To ensure that building facades add positively to the public realm and its interest. Building articulation will encourage interaction with the street and passive surveillance of adjacent spaces
- II. To promote a pedestrian scale of buildings at street level
- III. The building design shall demonstrate an appropriate level of articulation to avoid building bulk appearing excessive
- IV. Building articulation will express a vibrant and modern design aesthetic

Assessment Criteria

- i. Permanent blank walls are not permitted to any street frontage. Major openings are required to provide for surveillance and interaction with the public realm
- ii. The facade detail may be simplified on loading areas, parapet walls and walls to 'back of house' areas
- iii. Built form is to address parks, pedestrian access ways and in particular laneways by providing windows, balconies and suitable facade articulation facing these areas. These elevations are to match the design quality of the dwellings primary street elevation
- iv. Corner buildings are to address both frontages through the provision of:
 - a. distinct roof form at corners;
 - b. variation in materials and colours; and
 - c. varied balcony treatments

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Building Levels

Design Objective

- I. To ensure development maintains a positive relationship with the street such that pedestrian movement, sight lines and streetscape character are maximised
- II. To allow for the safe use of ceiling fans for cooling

Assessment Criteria

- i. All development shall maintain a minimum floor to floor height of 3.1m. Apartments shall achieve minimum floor to ceiling levels as outlined in Clause 4.3 of the Residential Design Codes Volume 2 – Apartments.
- ii. All development is to achieve a minimum finished floor level of +3.8AHD to ensure development takes into account coastal erosion and accretion patterns. Non habitable rooms and the provision of basement parking are exempt from the finished floor level stated above
- iii. Where residential dwellings are proposed on the ground floor adjacent to a street or public open space, a grade separation from 0.5m to 1.2m between the finished floor level of the ground floor and the adjacent street or public open space is encouraged in order to create a visual distinction between the public and private space

Building Height

Design Objective

- I. Building heights will respond to the pedestrian scale, urban character, intended dwelling density, land use mix as well as the natural topography of the area
- II. The built form of an area shall provide a pedestrian scaled street interface with taller upper floors setback from the street alignment
- III. The built form shall minimise overshadowing to adjacent streets and public spaces

Assessment Criteria

- i. Development shall be in accordance with the Building Height Plan (Figure 14)
- ii. Development shall be a minimum of three storeys

Building Materials

Design Objective

- I. To encourage a style of development that is consistent with the coastal location
- II. To provide for a consistency in the standard of finish and materials throughout Cockburn Coast
- III. To foster a sense of place through an identifiable character and style of development

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Assessment Criteria

- i. Extensive use of concrete tilt panels is discouraged. Where concrete tilt panels are used, they shall be integrally coloured (colour tinted concrete)
- ii. Moulded textures imprinted in the external surfaces of any concrete panels are encouraged
- iii. Painted finishes and rendered textures over concrete panels are not permitted
- iv. The use of natural materials such as stone, timber and other such natural products is encouraged in both interior and exterior finishes

Open Space

Design Objective

- I. To ensure that development provides an appropriate interface with the public open space
- II. To maximise the potential for passive surveillance

Assessment Criteria

- i. Where an area of public open space is provided the surrounding development must address the open space by maximising passive surveillance from habitable rooms; buildings must front onto the open space through placement of doors, windows and balconies to create a safe and comfortable pedestrian environment

Fencing

Design Objective

- I. To ensure that the provision of fencing does not detract from the function and appearance of the streetscape

Assessment Criteria

- i. The interface between private lots and the public open space may be fenced to a maximum height of 1.2m from natural ground level, but must be visually permeable above a height of 1.0m above natural ground level
- ii. Fencing shall generally not be permitted in the primary or secondary street setback areas

Landscaping

Design Objective

- I. To ensure an attractive streetscape environment
- II. To aid the sustainability of a building through the provision of permeable surface

Assessment Criteria

- i. The front setback area shall include provision for elements of soft landscaping
- ii. In ground landscaping is preferred over shallow landscaping above basements
- iii. Paving that is contiguous with foot paths and other paving in the public realm shall be of the same style and materials, matching exactly wherever possible

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

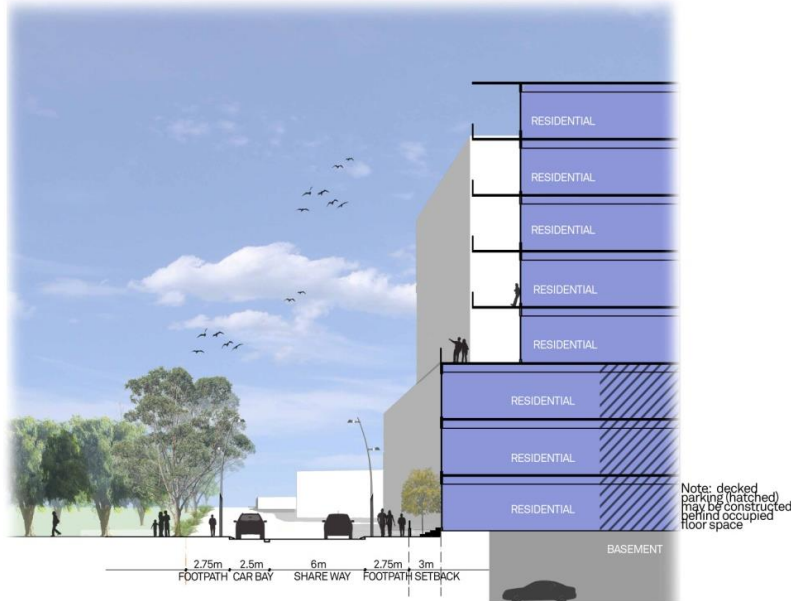


Figure 9_Typical cross section of high density residential adjoining road

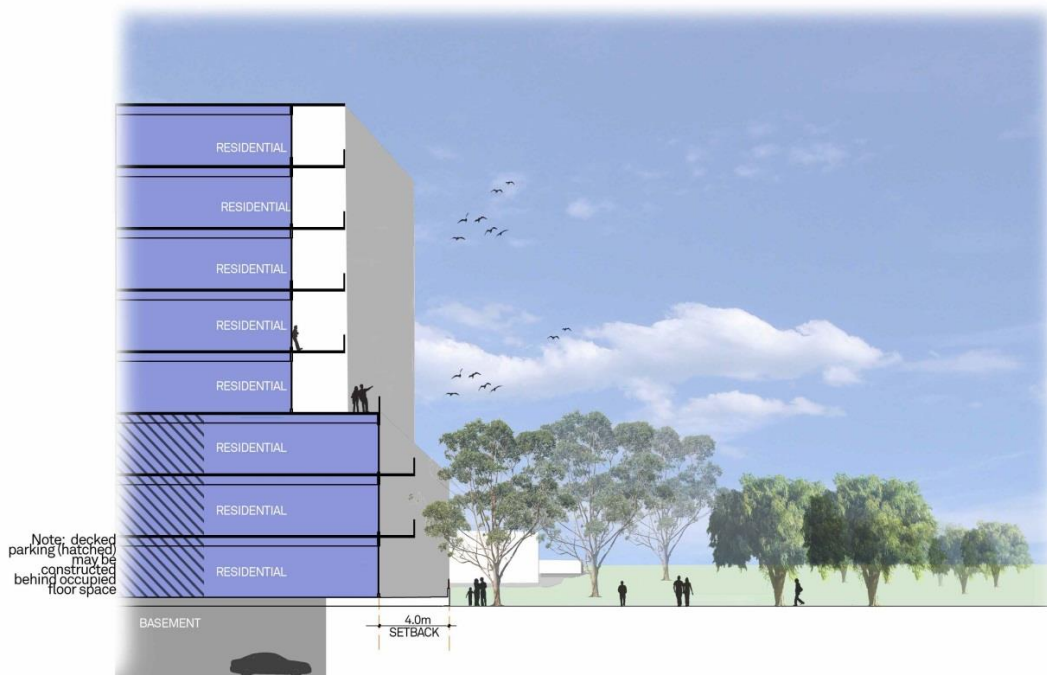


Figure 10_Typical cross section of high density residential directly adjoining public open space

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts



Figure 11_Typical cross section for high density residential development



High density Residential Development showing the use of natural materials in the facade and a provision of high quality building articulation in keeping with the objectives of these design guidelines

2.4 Medium Density Residential Typology

The Robb Jetty area provides an important medium density housing area. Leafy streets and small softly landscaped front setbacks will combine to create a comfortable urban setting for contemporary apartment buildings. Future built form will embody a seamless transition from indoor to outdoor, from formal to informal , from exposed to protected. Built form will be respectful of nature and

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts reflect the natural characteristics of the vegetation and landscape within Cockburn Coast.



Figure 12_Medium Density built form typology

Building Setbacks

Design Objective

- I. Building setbacks create intimate streetscapes
- II. Building setbacks accommodate landscaping which slightly widen and softens the streetscape

Assessment Criteria

- i. Building setbacks are to be in accordance with the following table

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Setback	Street (minimum)	Side (minimum)	Laneway (minimum)	Public Open Space (minimum)
Levels 1-3	2m (primary) 1m (secondary)	Nil	Nil	3m to wall and 2m to balconies (cantilevered/light weight only)
Levels 4+	5m to wall and 2m to balconies (cantilevered/light weight only)	3m	3m	5m to wall and 2m to balconies (cantilevered/light weight only)

Table 04_ Building Setbacks for medium density residential development

- ii. Buildings shall be setback 3m from any boundary adjoining public parkland. This setback area shall include space for landscaping and if necessary an outdoor living area. Where additional outdoor living area is to be provided, the additional outdoor living area shall be absorbed into the building space (i.e. building shall cantilever over the outdoor living area)
- iii. Projections are permitted within the 3m setback to public parkland to maximum of 2m into the setback area

Building Articulation

Design Objective

- I. To ensure that building facades add positively to the public realm and its interest. Building articulation will encourage interaction with the street and passive surveillance of adjacent spaces
- II. To promote a pedestrian scale of buildings at street level
- III. The building design shall demonstrate an appropriate level of articulation to avoid building bulk appearing excessive
- IV. Building articulation will express a vibrant and modern design aesthetic

Assessment Criteria

- i. The facade detail may be simplified on loading areas, parapet walls and walls to 'back of house' areas
- ii. Built form is to address parks, pedestrian access ways and in particular laneways by providing windows, balconies and suitable facade articulation facing these areas. These elevations are to match the design quality of the dwellings primary street elevation
- iii. Balconies are encouraged but shall not run continuously along the facade. Separate individual balconies are appropriate
- iv. The primary frontage shall provide pedestrian access to the major entry (front door) of the building(s)
- v. Corner buildings are to address both frontages through the provision of:
 - a. distinct roof form at corners;
 - b. variation in materials and colours; and
 - c. varied balcony treatments.

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts Roof Form

Design Objective

- I. The roof form should be designed as a contemporary and integrated architectural structure as befits this unique metropolitan coastal location

Assessment Criteria

- i. Use of skillion roofs and modern materials is actively promoted
- ii. Use of pitched roofs and dark tiles is discouraged
- iii. Lighting or similar features may be used to accentuate the roofscape and provide a positive architectural feature at night
- iv. Flat roofs are acceptable where concealed behind a building parapet.

Building Levels

Design Objective

- I. To ensure development maintains a positive relationship with the street such that pedestrian movement, sight lines and streetscape character are maximised
- II. To allow for the safe use of ceiling fans for cooling

Assessment Criteria

- i. All development shall maintain a minimum floor to floor height of 3.1m. Apartments shall achieve minimum floor to ceiling levels as outlined in Clause 4.3 of the Residential Design Codes Volume 2 – Apartments.
- ii. All development is to achieve a minimum finished floor level of +3.8AHD to ensure development takes into account coastal erosion and accretion patterns. Non habitable rooms and the provision of basement parking are exempt from the finished floor level stated above
- iii. Where residential dwellings are proposed on the ground floor adjacent to a street or public open space, a grade separation from 0.5m to 1.2m between the finished floor level of the ground floor and the adjacent street or public open space is encouraged in order to create a visual distinction between the public and private space

Building Height

Design Objective

- I. Building heights will respond to the pedestrian scale, urban character, intended dwelling density, land use mix as well as the natural topography of the area
- II. The built form of an area shall provide a pedestrian scaled street interface with taller upper floors setback from the street alignment
- III. The built form shall minimise overshadowing to adjacent streets and public spaces

Assessment Criteria

- i. Building shall be in accordance with the Building Height Plan (Figure 14)
- ii. Development shall be a minimum of three storeys, with the exception of Lots 235-239 and 247-259 where the minimum height is two storeys as shown in a

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

relevant Local Development Plan. For the purposes of assessing the number of storeys, a loft can be considered as a third storey, provided the building design gives the appearance of three storeys from the primary street frontage

- iii. Single storey development shall not be supported

Building Materials

Design Objective

- I. To encourage a style of development that is consistent with the coastal location
- II. To provide for a consistency in the standard of finish and materials throughout Cockburn Coast
- III. To foster a sense of place through an identifiable character and style of development

Assessment Criteria

- i. Extensive use of concrete tilt panels is discouraged. Where concrete tilt panels are used, they shall be integrally coloured (colour tinted concrete)
- ii. Moulded textures imprinted in the external surfaces of any concrete panels should also be applied
- iii. Painted finishes and rendered textures over concrete panels are not permitted
- iv. Warm exterior finishes are encouraged through the use of natural materials such as stone, timber, and other such natural products

Open Space

Design Objective

- I. To ensure that development provides an appropriate interface with the public open space
- II. To maximise the potential for passive surveillance

Assessment Criteria

- i. Where an area of public open space is provided the surrounding development must address the open space by maximising passive surveillance from habitable rooms; buildings must front onto the open space through placement of doors, windows and balconies to create a safe and comfortable relationship to the public open space

Fencing

Design Objective

- I. To ensure that fencing does not detract from the function and appearance of the streetscape

Assessment Criteria

- i. The interface between private lots and the public open space may be fenced to a maximum height of 1.2m from natural ground level, but must be visually permeable above a height of 1m above natural ground level.

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Landscaping

Design Objective

- I. To ensure an attractive streetscape environment
- II. To aid the sustainability of a building through the provision of permeable surface

Assessment Criteria

- i. The front setback area shall include provision for elements of soft landscaping
- ii. In ground landscaping is preferred over shallow landscaping above basements, particularly in front setback areas which provides the opportunity for tree planting



Figure 13_ Typical cross section for medium density built form typology

2.5 Mixed Residential Typology

The Robb Jetty Precinct encourages the exploration of innovative approaches to higher density living, emphasising the need to reflect the Cockburn Coast vision of offering choice and variety of living and working opportunities in a form and density that supports the planned Bus Rapid Transit (BRT) system. The Mixed Residential typology area addresses the ‘missing middle’ by offering a range of medium density housing typologies. This Typology area evolved from an intensive, design-led, exploration of ground-breaking urban forms. It is intended that the urban form will be unique in the overall precinct and present a point of difference to development elsewhere in the Cockburn Coast district.

In addition to mid-rise apartment living around the western and southern edges, the area will feature a diverse mix of high quality compact single housing

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

options, around a public realm design that blurs the lines between streets and open space. Streets will be designed with visual amenity, landscape and pedestrian movement assuming equal importance to the private vehicle. As a compact, high-density area the built form will be close, special setbacks will apply, and the relationship between private living space and public open space (POS) will be strong.

As a special precinct, the City is prepared to vary some requirements of the Robb Jetty Local Structure Plan (LSP), to facilitate the exploration of creative ideas, on the basis that planning for the area will undergo a rigorous process of consultative, design-led enquiry, including but not limited to, the following steps:

1. Vision definition, explaining how the precinct will differ from other precincts in the Robb Jetty LSP area;
2. An integrated design process, involving qualified architects, landscape architects, urban designers, and civil engineers;
3. Independent design input or peer review from an independent expert in urban design or architecture, such input to be formally documented;
4. Regular engagement with technical staff from the City of Cockburn and Department of Lands, Planning and Heritage, in the form of workshops and briefings, from the initial formation of a design vision to completion of a masterplan concept;
5. Preparation of a masterplan which illustrates a cohesive design that blends the built form, movement systems, open space and servicing, including, but not limited to, the following key elements:
 - a) An overall urban form - in terms of dwelling styles, streets and open space – that displays a unique innovation precinct to address the ‘missing middle’ and provide a range of living choices that will present a new living choice to the precinct;
 - b) Illustration of proposed dwelling forms, building height and massing;
 - c) Demonstration that the dwelling yield identified in the Robb Jetty LSP is maintained;
 - d) Demonstration that the plan will offer a community benefit outcome equal to, or better than, the urban pattern envisaged under the Robb Jetty LSP;
 - e) Demonstration that the plan will not compromise the planned function or amenity of adjoining typology areas; and
 - f) An implementation strategy that ensures the public realm design will be developed in accordance with the plan and that the built form will be delivered in an orderly and progressive manner.

It is also required that any development application in the Mixed Residential Typology area is to be considered by the Design Review Panel (DRP)

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6

Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts



Figure 14 Mixed Residential built form typology

Building Setbacks

Design Objective

- I. Building setbacks frame streetscapes and public open spaces
- II. Building setbacks accommodate landscaping which slightly widens and softens the streetscape

Assessment Criteria

- i. Building setbacks are to be in accordance with the following table

Setback	Primary Street (minimum)	Secondary (minimum)	Laneway (minimum)	Public Open Space – Northern Drainage Swale (minimum)	Public Open Space – Central Spine (minimum)
Levels 1-3	3m	Nil	Nil	3m to wall and 2m to balconies (cantilevered/light weight only)	2m to wall and nil to balconies (cantilevered/light weight only)
Levels 4+	5m to wall and 2m to	3m to wall and 2m to balconies	3m	5m to wall and 2m to balconies (cantilevered/light weight only)	4m to wall and 2m to balconies (cantilevered/light weight only)

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Setback	Primary Street (minimum)	Secondary (minimum)	Laneway (minimum)	Public Open Space – Northern Drainage Swale (minimum)	Public Open Space – Central Spine (minimum)
	balconies (cantilevered/ light weight only)	(cantilevered/ light weight only)		t weight only)	t weight only)

Table 05_ Building Setbacks for mixed residential development

- ii. Buildings shall be setback from any boundary adjoining public parkland as specified in the above table. This setback area shall include space for landscaping, and if necessary, an outdoor living area. Where an additional outdoor living area is to be provided, the additional outdoor living area shall be absorbed into the building space (i.e. building shall cantilever over the outdoor living area)

Building Articulation

Design Objective

- I. To ensure that building facades add positively to the public realm and its interest. Building articulation will encourage interaction with the street and passive surveillance of adjacent spaces
- II. To promote a pedestrian scale of buildings at street level
- III. The building design shall demonstrate an appropriate level of articulation to avoid building bulk appearing excessive
- IV. Building articulation will express a vibrant and modern design aesthetic

Assessment Criteria

- i. Permanent blank walls are not permitted to any street frontage. Major openings are required to provide for surveillance and interaction with the public realm
- ii. The facade detail may be simplified on loading areas, parapet walls and walls to ‘back of house’ areas
- iii. Built form is to address parks, pedestrian access ways and in particular laneways by providing windows, balconies or suitable facade articulation facing these areas. These elevations are to match the design quality of the dwellings primary street elevation
- iv. Corner buildings are to address both frontages through the provision of:
 - a. distinct building form at corners;
 - b. variation in materials and colours; and
 - c. varied balcony treatments

Roof Form

Design Objective

- I. The roof form should be designed as a contemporary and integrated architectural structure as befits this unique metropolitan coastal location

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Assessment Criteria

- i. Use of skillion roofs and modern materials is actively promoted
- ii. Use of dark tiles is discouraged
- iii. Lighting or similar features may be used to accentuate the roofscape and provide a positive architectural feature at night
- iv. Flat roofs are acceptable where concealed behind a building parapet

Building Levels

Design Objective

- I. To ensure development maintains a positive relationship with the street such that pedestrian movement, sight lines and streetscape character are maximised
- II. To allow for the safe use of ceiling fans for cooling

Assessment Criteria

- i. All development shall maintain a minimum floor to floor height of 3.1m. Apartments shall achieve minimum floor to ceiling levels as outlined in Clause 4.3 of the Residential Design Codes Volume 2 – Apartments.
- ii. All development is to achieve a minimum finished floor level of +3.8AHD to ensure development takes into account coastal erosion and accretion patterns. Non habitable rooms and the provision of basement parking are exempt from the finished floor level stated above
- iii. Where residential apartments are proposed on the ground floor adjacent to a street or public open space, a grade separation from 0.5m to 1.2m between the finished floor level of the ground floor and the adjacent street or public open space is encouraged in order to create a visual distinction between the public and private space
- iv. Where single dwellings are proposed on the ground floor adjacent to a street or public open space, a transition between the public and private realm is encouraged to create a visual distinction,

Building Height

Design Objective

- I. Building heights will respond to the pedestrian scale, urban character, intended dwelling density, land use mix as well as the natural topography of the area
- II. The built form of an area shall provide a pedestrian scaled street interface with taller upper floors setback from the street alignment
- III. The built form shall minimise overshadowing to adjacent streets and public spaces

Assessment Criteria

- i. Development shall be in accordance with the Building Height Plan (Figure 14)
- ii. A Local Development Plan shall be prepared to stipulate more specific height limits for designated sites within the area

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Building Materials

Design Objective

- I. To encourage a style of development that is consistent with the coastal location
- II. To provide for a consistency in the standard of finish and materials throughout Cockburn Coast
- III. To foster a sense of place through an identifiable character and style of development

Assessment Criteria

- i. Extensive use of concrete tilt panels is discouraged. Where concrete tilt panels are used, they shall be integrally coloured (colour tinted concrete)
- ii. Moulded textures imprinted in the external surfaces of any concrete panels are encouraged
- iii. Painted finishes and rendered textures over concrete panels are not permitted
- iv. The use of natural materials such as stone, timber and other such natural products is encouraged in both interior and exterior finishes

Open Space

Design Objective

- I. To ensure that development provides an appropriate interface with the public open space
- II. To maximise the potential for passive surveillance

Assessment Criteria

- i. Where an area of public open space is provided the surrounding development must address the open space by maximising passive surveillance from habitable rooms; buildings must front onto the open space through placement of doors, windows and balconies to create a safe and comfortable pedestrian environment

Fencing

Design Objective

- I. To ensure that the provision of fencing does not detract from the function and appearance of the public realm

Assessment Criteria

- i. The interface between private lots and the public open space may be fenced to a maximum height of 1.2m from natural ground level, but must be visually permeable above a height of 1m above natural ground level
- ii. For apartment development, fencing shall generally not be permitted in the primary or secondary street setback areas

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Access and Parking

Design Objective

- I. The design and location of vehicle crossovers should minimise the impact on the pedestrian amenity of the street environment
- II. Provide sufficient visitor parking for the proposed development precinct
- III. Promote street design that creates a shared environment for pedestrian, cyclists and vehicle movement

Assessment Criteria

- i. Open carports are permitted for terrace style or single dwellings where it is considered to improve streetscape character and surveillance, provided that storage and drying areas are suitably screened from public view
- ii. Tandem parking will be permitted where two or more parking bays are provided for the exclusive use of the same dwelling
- iii. Car park entries, driveway crossovers, service areas and bin refuse collection points should be constructed from materials consistent with the road pavement and landscape design treatments of the of the adjacent street
- iv. Where terrace style or single residential lots are proposed the required visitor parking provision shall be located in streets that are immediately adjacent, or in close proximity, to the development

Waste Management

Design Objective

- I. Services required for the function of all proposed development shall be appropriately considered

Assessment Criteria

- i. Building design shall demonstrate compliance with an approved integrated Waste Management Strategy or as otherwise approved by the City
- ii. Rubbish bin storage areas shall be integrated into the development and located to minimise the impact on adjoining residences and the public realm

Landscaping

Design Objective

- I. To ensure an attractive streetscape environment
- II. To aid the sustainability of a building through the provision of permeable surface

Assessment Criteria

- i. The front setback area shall include provision for elements of soft landscaping
- ii. In ground landscaping is preferred over shallow landscaping above basements
- iii. Paving that is contiguous with foot paths and other paving in the public realm shall be of the same style and materials, matching exactly wherever possible

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

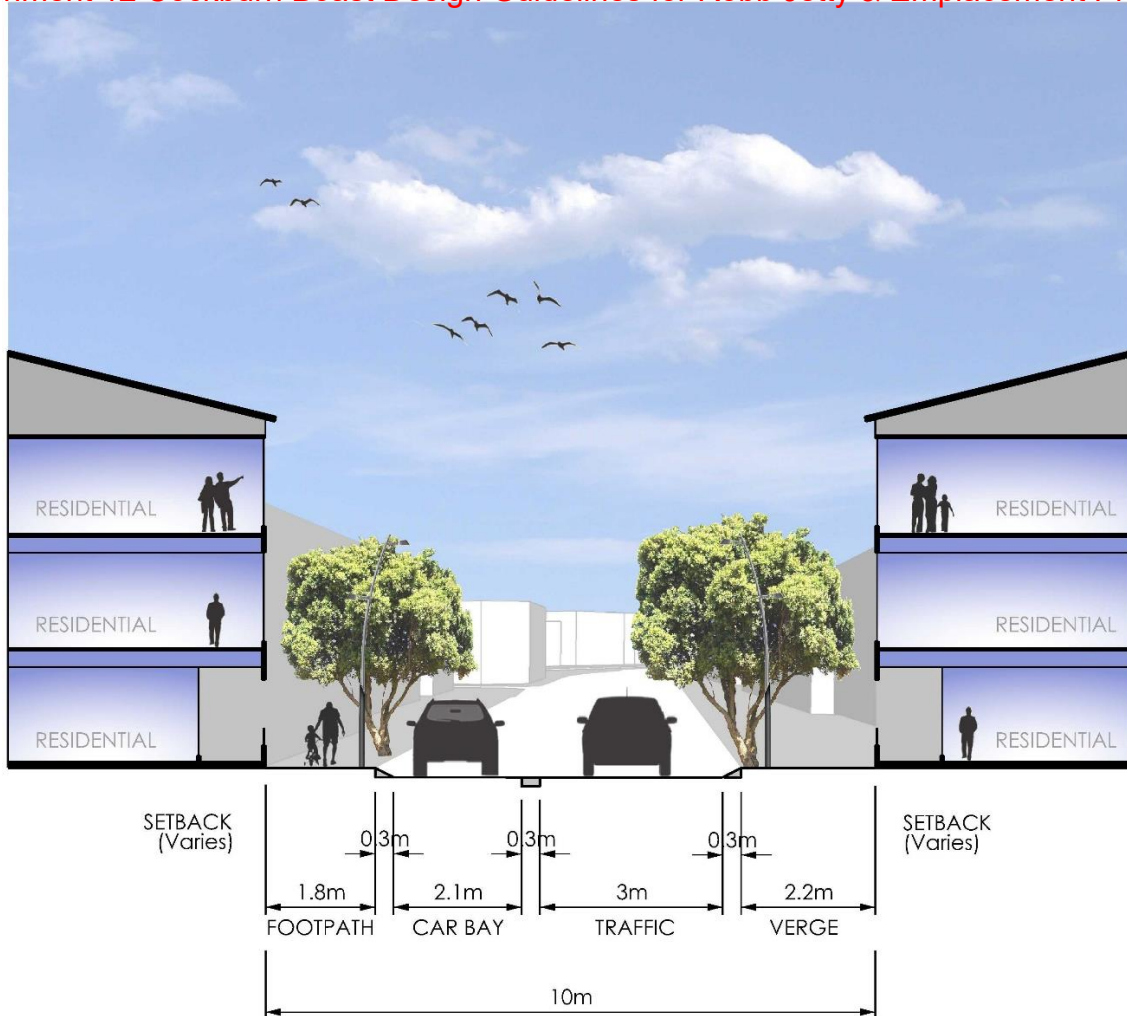


Figure 16_Typical cross section of Mixed Residential adjoining road



Mixed Residential Development showing the use of natural materials in the facade and a provision of high quality building articulation in keeping with the objectives of these design guidelines

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

3. General Provisions

3.1 Built Form Requirements

Built form should provide a pedestrian scale and define streets and public spaces whilst contributing towards creating an urban presence. The built form will contribute towards the intended streetscape character and typology. Taking cues from the natural assets of the site building height responds to site topography, maximising views to the ocean particularly for residential development.

For private open space, visual privacy, storage for grouped and multiple dwellings requirements refers to the relevant section of the R-Codes. For private open space for single houses refer to the relevant Local Development Plan (LDP) or if not specified in the LDP refer to the City of Cockburn's Local Planning Policy LPP 1.16 Single House Standards for Medium Density Housing in the Development Zone. Other elements not listed in the relevant LDP or LPP 1.16 shall be assessed as per the Residential Design Codes.

3.1.1 Building Height

Design Objective

- I. Building heights will respond to the pedestrian scale and urban character of Cockburn Coast, intended dwelling density and land use mix as well as the natural topography
- II. The built form of an area shall provide a pedestrian scaled street interface with taller upper floors setback from the street alignment
- III. The built form shall minimise overshadowing to adjacent streets and public spaces

Assessment Criteria

- i. Heights to be in accordance with the typology specific built form requirements and the Building Height Plan below in Figure 15
- ii. The proposed Primary School may be approved with a minimum of two storeys

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

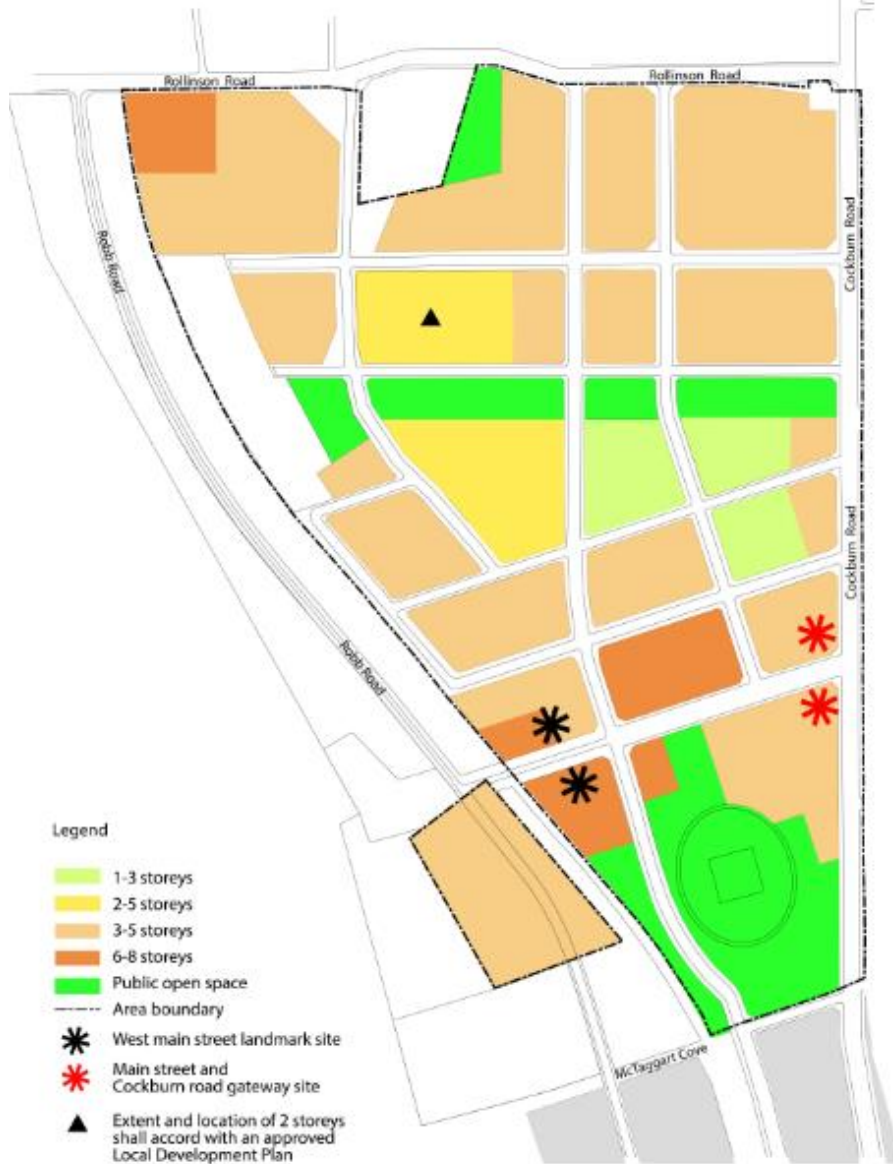


Figure 15 Building Height Plan

3.1.2 Facades

Design Objective

- I. Building facades add significantly to the public realm and its interest. A vibrant and modern design aesthetic for Cockburn Coast will require the provision of visually engaging building exteriors which encourage interaction with the street and passive surveillance of adjacent spaces

Assessment Criteria

- i. Fenestration, entrances, balconies and awnings shall be provided in a manner that creates visual cohesiveness, interest and interaction with the public realm
- ii. An exposed parapet or boundary wall must have the same standard of finish as the primary facade. Detailing for permanently exposed

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

- blank walls shall include texture, patterns or suitable alternatives to the finish of the wall to address the objective
- iii. External ducting, air conditioners, plants, pipes, lift over-runs, service doors and similar building services must be screened from public view or adjacent property and incorporated into the building at the initial design stage
 - iv. Ground floor lobbies shall be clearly delineated, well lit and safe to access
 - v. Facade design shall address crime prevention through environmental design (CPTED) principles



Buildings shall provide a break up of bulk and scale through articulated facades



Building facades are to be finished with fine grain architectural elements

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

3.1.3 Roof Form

Design Objective

- I. The roof form as seen from the street or adjoining sites should be designed to make a contemporary and positive architectural contribution to the streetscape and skyline. Where appropriate the roof form can be designed to enhance the architecture and contribute to creating local landmarks through the use of integrated architectural form and detailing

Assessment Criteria

- i. Roof designs must conceal roof plant and equipment including lift over run structures from view from the public realm and street level
- ii. Lighting or similar features may be used to accentuate the roofscape to provide a feature at night
- iii. Flat roofs are acceptable where concealed behind a building parapet
- iv. Flat roof areas that are accessible concealed behind a building parapet and provide a minimum dimension of 2.4m may be used as an outdoor living area or communal open space, however shall not count towards the private open space area requirement.

3.1.4 Lighting

Design Objective

- I. To ensure perceived and actual safety for all users of the area is achieved by providing lighting around public spaces that allows for a high degree of visibility of pedestrians at all times

Assessment Criteria

- i. Lighting to be integrated into built form to highlight architectural features
- ii. Ensure inset spaces, access, egress and signage is well lit
- iii. Lighting is to be incorporated into building awnings over the footpath and building entrances

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6

Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts



Innovative lighting built into the facade of a building can contribute to an activated and interesting façade

3.1.5 Acoustics and Vibration

Design Objective

- I. To facilitate a sustainable mixed use environment where a variety of land uses can co-exist
- II. To ensure appropriate noise intrusion and noise emission mitigation measures are incorporated into building design and construction and where necessary, building refurbishment

Assessment Criteria

- i. Design of Noise sensitive premises must be give consideration to the following:
 - a) the identification of existing/potential environmental noise sources;
 - b) development orientation and layout taking into account the location of existing/potential environmental noise sources;
 - c) the location of bedrooms away from noise sources;
 - d) the location of balconies and windows away from noise sources;
 - e) the use of built form (blade walls, etc) to screen noise sources; and
 - f) the use of building design elements (balcony balustrades, decorative screens, etc) to provide some reduction in noise impact on windows.
- ii. Notifications are required to be applied to the created land title and any subsequent strata titles of any noise sensitive premises pursuant to section 70A of the Transfer of Land Act 1893, together with section 165 of the Planning and Development Act 2005 to inform prospective land owners and residents of the likelihood of higher noise levels associated within the inner city environment
- iii. An acoustic and vibration (as deemed required in the local structure plan) report and associated plans are required detailing compliance with

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

the above design objectives and assessment criteria for noise sensitive and commercial developments. The report is to be prepared by a qualified and experienced acoustic consultant and submitted as part of a DA and should address the requirements of State Planning Policy 5.4 Road and Rail Transport Noise and Freight Considerations in Land Use Planning (and associated guidelines), the City of Cockburn's Local Planning Policy LPP 1.12 Noise Attenuation.

3.1.6 Active Edges and Street Relationship

Design Objective

- I. The activation of streets and other publicly accessible spaces are fundamental to providing an attractive and safe pedestrian environment throughout Cockburn Coast
- II. All development must be designed to activate streets and laneways. This can be achieved by utilising major openings to residential and commercial land uses, alfresco dining areas, pedestrian shelters and legible building entries to create a vibrant, diverse and safe environment

Assessment Criteria

- i. Passive surveillance of communal areas and public spaces shall be integrated into building design, providing for overlooking of the street, public space and communal open space
- ii. Pedestrian entrances are to be highly visible
- iii. Ground floor non-residential frontages should be designed as shop fronts with no less than 80% of the shop front glazed with clear glass
- iv. Car park entries are to be located appropriately to avoid disruption of the pedestrian experience
- v. Inactive ground floor uses are to be avoided within the Activity Centre and Mixed Use areas particularly on the Robb Jetty Main Street and surrounding the identified landmark development sites

3.1.7 Heritage Considerations

Design Objective

- I. Development of site adjacent to a heritage place shall be respectful of the recognised cultural heritage significance; and should not adversely affect the heritage significance

Assessment Criteria

- i. New buildings adjacent to a Heritage Place should conform with the provisions of the City's Heritage Conservation Guidelines policy to ensure that they respect the heritage significance of the place
- ii. Any new work adjacent to a significant tree should not affect the appearance or health of the tree

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6

Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts



Ground floor commercial land uses will provide active street edges

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

3.2 Service Infrastructure and Access

Service infrastructure and access arrangements are an important part of allowing development to function effectively. However, these elements can often create unsightly urban environments and therefore appropriate treatment and coordination of these elements is required to make them an integral part of new development

3.2.1 Internal Access

Design Objective

- I. Internal access within street blocks to perform as one coordinated and efficient movement network

Assessment Criteria

- i. Internal access ways servicing development to be designed to facilitate adjoining development and where logical allow for reciprocal access arrangements

3.2.2 Parking

Design Objective

- I. Development will encourage and support alternative modes of transport to the car by limiting and screening the provision of car parking on site

Assessment Criteria

- i. Vehicle crossovers for non-residential development are required to be built underneath the building or provide design elements above the crossover to reduce the street impact and pedestrian environment
- ii. Reciprocal use of commercial car parking bays for uses within a comprehensive development with different peak usage requirements (such as restaurants and offices) may be considered
- iii. Residential parking is to be provided in accordance with the City of Cockburn Town Planning Scheme No.3 and the Residential Design Codes of Western Australia.

3.2.3 Parking Location and Access

Design Objective

- I. The number of vehicle crossovers into a development is to be minimised to create a pedestrian friendly environment
- II. Parking is to be located so as minimise the visual impact on the public realm

Assessment Criteria

- i. All on site car parking facilities are to be concealed from public view to ensure car parking does not dominate streetscapes or create conflict with pedestrian and vehicle movement

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

- ii. Car parking entry is to be subservient to pedestrian entries and shall address street spaces, building returns and recesses
- iii. Where terrace style or single residential lots are proposed vehicle access must be provided at the rear of the dwellings
- iv. Car parking is to be concealed from public view by habitable frontages, or high quality landscaping along minor/secondary streets
- v. Parking facilities should not be visible from public open space
- vi. Where garage doors service only one dwelling they should be no wider than 6m

3.2.4 Sleeved Parking

Design Objective

- I. To screen multi storey car parks from the public realm and to provide active frontages to the street

Assessment Criteria

- i. All multi storey car parking structures should be sleeved by development to ensure car parking is screened from view of the public realm
- ii. Sleeve above ground car parking structures with other uses, such as offices, residential and retail
- iii. Where it is not possible for car parking structure to be screened any car parking structures that contain three or more levels must be appropriately designed and screened from adjacent or nearby buildings and the street through the use of innovative wall detailing, decorative screening, patterning and vegetation

3.2.5 End of Trip Facilities

Design Objective

- I. To encourage the use of bicycles, walking and other alternative means of transport to reduce the use of private motor vehicles and contribute to public health

Assessment Criteria

- i. Provision of adequate bicycle and change room facilities. Secure lockers, bicycle storage and showers shall be provided within buildings
- ii. Developments are to be provided with end of trip facilities in accordance with the following table

Commercial	1 Secure bicycle storage per 150m ² of Net Lettable Area (NLA); and
Accessible showers	There must be a minimum of two female and two male showers, located in separate changing rooms, for the first 10 bicycle parking bays. Additional shower facilities to be provided at a rate

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

	of one male and one female shower for every 10 bicycle parking bays or part thereof.
Changing facilities	Including secure lockers at 1.5 for each bicycle parking bay.
Visitor Bicycle Storage	A minimum of 1 space per 750m ² of NLA. Located and signed near the main public entrance to the building.
Residential	Bicycle parking facilities for multiple dwellings, short stay accommodation and serviced apartments shall be provided at a minimum of 1 bay per unit.

Table 05_ End of trip facility provision rates



End of trip facilities



Use of screening can minimise the impact of parking structures

3.2.6 Site Services

Design Objective

- I. Services and related elements required for the function of the building shall be appropriately screened or integrated into the building design

Assessment Criteria

- i. Air-conditioning units must not be visible from the streets and laneways
- ii. Service pipes and wired services are to be concealed from public view
- iii. All meters to be contained within development lots to the requirements of the appropriate authorities
- iv. Provide secure and accessible facilities for mail delivery

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

- v. Commercial utility and waste storage areas are to be screened or located behind buildings and not visible from public view and residential apartments
- vi. Fire booster cabinets and associated infrastructure are to be discretely designed into development and must not dominate any frontage

3.3 Sustainability Requirements

Integral to the sustainability of the development will be the provision of affordable housing and facilities to encourage alternative modes of transport to the private car. This will promote a healthy lifestyle that encourages people to actively engage with the urban environment and create a robust and diverse community

3.3.1 Sustainable Travel

Design Objective

- I. To reduce greenhouse gases through the reduction of motorised transport to and from Cockburn Coast and encourage residents and site visitors to improve their physical health through walking, cycling or other physically active forms of transport either solely or in combination with public transport

Assessment Criteria

- i. Demonstrate that pedestrians and cyclists have been prioritised within the development
- ii. Surface finishes of all driveways and pathways to be safe and comfortable for pedestrians and cyclists
- iii. Grade changes between private and public spaces to be complementary and accessible

3.4 Laneways

3.4.1 Residential and Commercial Laneways

Design Objective

- I. To create unique and attractive built form and character along laneways through sensitive and innovative design
- II. To encourage activity and interaction between public laneways and adjacent private uses at the ground level
- III. To reinforce the primary function of laneways as key service and vehicle access spaces within the development
- IV. Encourage development to provide highly articulated and well detailed facades that create visual interest, particularly at the lower levels
- V. Encourage development to orientate windows and balconies to overlook lane ways

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

Assessment Criteria

- i. Residential Laneways
 - a) For lots with a laneway frontage of 8m or greater, pedestrian access to the laneway from the lot should be provided
 - b) Buildings are to provide an elevation to the laneway that is articulated and similarly detailed to the front façade

- ii. Commercial Laneways
 - a) Laneways within the activity centre and mixed used zones are encouraged to be activated at ground floor level, but shall not be done so to the detriment to the activation of the primary or streetscape facade of the building
 - b) Buildings shall maintain a nil setback to the laneway for the first three storeys
 - c) The minimum setback above 3 storeys should be a distance equivalent to the width of the lane, unless it can be demonstrated that a lesser setback protects the quality of the pedestrian space at ground level including:
 - a. by maintaining or providing greater access to sunlight;
 - b. by maintaining or providing greater wind protection; and
 - c. by avoiding a sense of enclosed space.
 - d) Buildings are to provide an elevation to the laneway that is articulated and similarly detailed to the front facade
 - e) Development should contain a door which addresses the laneway or is accessed via its own pedestrian access gate



Activated laneways encourage vitality and interaction between public laneways and adjacent private uses

Title	Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts
Policy Number (Governance Purpose)	LPP 4.6



Attachment 12 Cockburn Coast Design Guidelines for Robb Jetty & Emplacement Precincts

3.5 Communal Open Space

Design Objective

- i. To provide an adequate area of quality communal open space for multiple dwelling developments which will enhance residential amenity and provide opportunities for soft landscape areas.

Assessment Criteria

- i. Communal open space is provided for multiple dwellings in accordance with Clause 3.4 of the Residential Design Codes Volume 2 – Apartments.

Strategic Link:	Town Planning Scheme No. 3
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ECM Doc Set ID: (Governance Purpose Only)	4518689

Title	Phoenix Activity Centre Design Guidelines
Policy Number (Governance Purpose)	LPP 4.7



Attachment 13 Phoenix Activity Centre Design Guidelines

Policy Type

Local Planning Policy

Policy Purpose

The Phoenix Revitalisation Strategy identified a vision for the Phoenix Activity Centre to evolve into a town centre, and the Phoenix Activity Centre Structure Plan provides further guidance for development.

These Design Guidelines set out development control policy measures to achieve the key objectives of the Revitalisation Strategy and the Activity Centre Structure Plan.

The purpose of this Policy is to set out design guidelines for the Phoenix Activity Centre, which is divided into three precincts as follows:

1. Phoenix Core Precinct
2. Mixed Use Precinct
3. Northern Precinct

This Policy should be read in conjunction with the Phoenix Activity Centre Structure Plan.

Policy Statement

Definitions:

Active Frontage means street frontages where there is an active visual engagement between people in the street and those on the ground floors of buildings.

Amenity means all those factors which combine to form the character of an area and includes the present and likely future amenity. An area of high amenity could be described as a comfortable and pleasant immediate environment, located within agreeable surroundings.

Articulation means the breaking up of a building façade into individual elements to provide a modulated effect aimed at enhancing individual building identity, variety and interest through the use of such elements as window projections, balconies, awnings, minor recesses and/or projections of walls or parts of walls.

Built Form means the configuration of the aggregate form of all buildings, structures, etc., which make up the physical environment of a locality.

Bulk means the size, or mass, of a building within its built form context.

Character means the essential combination of the public and private domains. Every property, public place or piece of infrastructure and the way it is used by the public, makes

Title	Phoenix Activity Centre Design Guidelines
Policy Number (Governance Purpose)	LPP 4.7



Attachment 13 Phoenix Activity Centre Design Guidelines

a contribution, whether large or small. It is the cumulative impact of all these contributions that establishes neighbourhood character.

Context means the specific character, quality, physical, historic and social context of a building's setting and may, according to circumstances, be a group of buildings, a part of a street, whole street, part of a town or the whole town.

Legibility means a street and movement system designed to provide a clear sense of direction and connection, giving definite signals regarding the spatial layout and geography of an area.

Public Realm means areas of a town which belong to the community as a whole. This refers to spaces that are physically accessible to the public, and those aspects of other spaces that are visible from physically accessible spaces. It incorporates features such as streets, parks, squares, community buildings and the street facades of other buildings.

Scale means the size of a building and its relationship with its surrounding buildings or landscape.

Streetscape means the total visual impression gained from any one location within a street including the natural and man-made elements and is made up of the appearance of and the relationships between buildings in terms of design, scale, materials, colours, finishes, signs, external furniture, paving materials for roads, footpaths and landscaping.

Passive Surveillance means the presence of passers-by or the ability of people to be seen in public spaces from surrounding windows, decks, balconies or the like. "eyes on the street" provided by local people going about their daily activities.

(1) General policy objectives

1. To create a high quality and safe pedestrian environment along Rockingham Road in the Phoenix Activity Centre.
2. To create a new sense of place with high-quality and dynamic building and landscape design and landmark development sites.
3. To create an activity centre with a readily identifiable character.
4. To facilitate safe and convenient pedestrian and cyclist movement within the Activity Centre, resolving vehicle and pedestrian/cyclist conflict points.
5. To create an Activity Centre that is highly legible.
6. To create attractive, active frontages that provide visual interest and contribute to pedestrian and cyclist safety and comfort.
7. To ensure that signage is not visually obtrusive, does not result in excessive visual clutter; and does not hinder passive surveillance.

Title	Phoenix Activity Centre Design Guidelines
Policy Number (Governance Purpose)	LPP 4.7



Attachment 13 Phoenix Activity Centre Design Guidelines

8. To ensure that signage is compatible with the scale, design and visual character of the building and activity centre.
9. To provide adequate opportunities for commercial advertising to support and encourage business activity.
10. To encourage an increase in pedestrian and cycling trips by maximising the convenience, safety and appeal of these modes of travel.
11. To create safe, functional and attractive car parking areas that allow for landscaping, and facilitate safe and convenient pedestrian and cyclist movement.
12. To encourage landmark development features which are integrated with buildings, and which improve legibility within the activity centre.
13. To utilise artworks to create community identity; improve inactive frontages; improve legibility; and provide functional infrastructure for pedestrians and cyclists.
14. To promote the integration of wayfinding with urban design, landscaping, architectural design and public art.
15. To encourage mixed use development and a diversity of land uses.

(2) General Provisions

1. Signage
 - (a) Development should include clearly identifiable entry point(s) for customers/pedestrians in the building design, rather than relying on signage.
2. Lighting
 - (a) Development should make provision for the location of external lighting, to include the lighting of commercial building facades for public safety purposes and to add variety, interest and character to the development at night.
 - (b) Lighting should be even and consistent to avoid shadows and glare, and should be provided to increase safety and security along important pedestrian pathways.

(3) Phoenix Core Precinct

1. Movement
 - (a) Development Applications are to be accompanied by a Pedestrian and Cyclist Movement Plan demonstrating how pedestrian and cyclist

Title	Phoenix Activity Centre Design Guidelines
Policy Number (Governance Purpose)	LPP 4.7



Attachment 13 Phoenix Activity Centre Design Guidelines

connectivity can be accommodated safely and conveniently, addressing the following:

- (i) Demonstrate improvements to legibility, permeability and pedestrian safety along and connecting with Rockingham Road.
 - (ii) Inclusion of separate pedestrian path(s) on the vehicle access ramp from Rockingham Road to secure safe pedestrian movement, where the ramp is retained as part of a redevelopment proposal, or a proposal for expansion of the floorspace.
 - (iii) Demonstrate how car parking areas include safe and convenient pedestrian routes to key destinations, both from car parking bays, bus stops, and from the wider pedestrian network. This may require a decrease in the number of parking spaces to allow for improved accessibility and/or improved landscaping.
 - (iv) Provision of high quality, safe, secure and accessible end-of-trip facilities for cyclists.
- (b) All development applications for the Phoenix Shopping Centre site that propose expansion of floorspace, or extensions or modifications to car parking areas or vehicle access ramps, must address the following matters:
- (i) Demonstrate improvements to the servicing area on Rockingham Road.
 - (ii) Minimising the width and impact of vehicle crossovers on the pedestrian environment if possible, given that they serve to disconnect the pedestrian environment, reduce pedestrian comfort and increase potential conflict between vehicles and cyclists and pedestrians.
 - (iii) Improving the inactive frontage.
 - (iv) Improving the interface with the bus stop.
- (c) Improve pedestrian connectivity through the Phoenix Shopping Centre site, and to the main entries of the shopping centre.
- (d) Demonstrate improvements to the servicing area on March Street which reduce negative impacts on residential amenity.

2. Built Form

- (a) Proposed buildings or extensions/major modifications to buildings are to address street frontages (and internal roads) and maximise opportunities for passive surveillance of streets, car parking areas, and areas used by pedestrians and cyclists.

Title	Phoenix Activity Centre Design Guidelines
Policy Number (Governance Purpose)	LPP 4.7



Attachment 13 Phoenix Activity Centre Design Guidelines

- (b) Proposed buildings or extensions/major modifications to buildings are to be appropriately detailed at ground level so that they create an attractive, safe and interesting environment for occupants and pedestrians alike.
- (c) Ground floor non-residential frontages fronting Rockingham Road or primary pedestrian linkages are to be designed as shop fronts with no less than 70% glazing. Buildings fronting other public areas shall be glazed for a minimum of 50%. Glazing percentages are to apply from between 0.9m and 2.1m above footpath/street level.
- (d) Where an active frontage cannot be achieved, for example servicing/loading areas, it should be demonstrated how the frontage has been designed to contribute to a high quality, safe and attractive street environment by:
 - (i) Minimising the length and height of blank walls, and
 - (ii) Articulating blank walls through the creative application of complementary materials, avoiding large continuous masses of the same finish; and/or the provision of appropriately integrated structural features, lighting, street furniture, artworks, or display windows where possible.
- (e) Vehicle ingress and egress, loading facilities and building services should be designed so that they do not detract from a high quality, safe and attractive public realm.
- (f) Wherever possible development should use built form to signify entry points rather than relying on signage elements (including 'signage' structures) to serve as a landmark to the shopping centre, and/or to signify the location of entrances to the shopping centre.
- (g) Wherever possible the opportunity to "sleeve" large scale retail and/or to introduce outwards-facing uses to avoid blank walls is strongly encouraged. Buildings at ground level should contain activities that positively contribute (either passively or actively) to the public domain.
- (h) Ensure where appropriate that development complements and corresponds to neighbouring or abutting built form through consideration of form, detail and application of materials.

3. Visual Impact

- (a) Development must demonstrate the visual impact of development from the surrounding area, particularly from the residential area to the east of the activity centre.
- (b) Buildings and structures should present well from all angles as some may be visible from a considerable distance.

Title	Phoenix Activity Centre Design Guidelines
Policy Number (Governance Purpose)	LPP 4.7



Attachment 13 Phoenix Activity Centre Design Guidelines

- (c) Development should demonstrate that consideration has been given to the vista and pedestrian connectivity from Orleans Street, including ensuring the following is achieved:
 - (i) The establishment of safe and attractive pedestrian connectivity;
 - (ii) ensuring development is not obtrusive when viewed from Orleans Street

4. Signage

- (a) All development applications should be accompanied by information regarding the location and scale of signage.
- (b) Signage should be:
 - (i) Be contained within the boundary of the lot
 - (ii) Be easy to read and provide a clear message
 - (iii) Only advertise goods and services that relate to the business on which the sign is located
- (c) Signage is to be an integrated part of the building/site, and should be compatible with the scale, design and visual character of the building/site, and should not result in visual clutter.
- (d) Signage is not to cover a large proportion of the shopfront window, or prevent passive surveillance.
- (e) Signage and signage structure should not be used in isolation to signify entry points to development.

5. Landscaping

- (a) Development proposals should include the retention of existing landscaping on March Street to provide a buffer with the residential development on the eastern side of the road.

(4) Mixed Use Precinct

1. Objectives

- (a) To create a vibrant mixed use environment that facilitates a diversity of uses.
- (b) Promote a diversity of housing types within the activity centre structure plan area.
- (c) To encourage rationalisation of signage to reduce visual clutter and enhance the streetscape.

Title	Phoenix Activity Centre Design Guidelines
Policy Number (Governance Purpose)	LPP 4.7



Attachment 13 Phoenix Activity Centre Design Guidelines

- (d) To encourage mixed use development whereby non-residential uses can be accommodated at the ground floor.
- (e) To ensure residential amenity and security is protected in mixed use environments.
- (f) To protect the residential amenity of dwellings on adjacent 'Residential' zoned land.
- (g) To ensure high quality built form in the 'Mixed Use' zone that provides interest to the street, and promotes passive surveillance.
- (h) To ensure new development in the 'Mixed Use' zone achieves visual cohesiveness.
- (i) To ensure buildings are robust and adaptable to allow for future use changes so that the mixed use area can evolve over time to meet the needs of businesses and the community.
- (j) To ensure that buildings give additional prominence to street corners by using landmark features which will create visual interest in the streetscape and assist with legibility.
- (k) To ensure buildings contribute positively to the public realm by creating visual interest, facilitating passive surveillance of streets and spaces used by the public, and contributing to pedestrian comfort.
- (l) To ensure safe vehicle access and egress to Rockingham Road, and improve pedestrian safety and comfort by:
 - Rationalising vehicle crossovers to Rockingham Road generally where possible.
- (m) To improve the pedestrian amenity of Rockingham Road through the introduction of a landscaping strip.

2. General Built Form Provisions

- (a) Development should incorporate non-residential ground floor uses that promote activity and informal surveillance of the street and have facades that add interest and vitality to the public realm.
- (b) Where commercial uses are not considered viable in the short term, ground floor tenancies should be designed to be adaptable for future commercial uses, including incorporating a minimum ground floor tenancy height of 3.9 metres above the finished ground floor level. In this respect a maximum building height of 10m will be accepted to accommodate this requirement.

Title	Phoenix Activity Centre Design Guidelines
Policy Number (Governance Purpose)	LPP 4.7



Attachment 13 Phoenix Activity Centre Design Guidelines

- (c) Where the ground floor is designed accordingly the plot ratio of the ground floor will be in addition to that of the residential development, calculated in accordance with the Residential Design Codes.
- (d) To ensure that building facades are architecturally interesting the upper levels of buildings should be articulated to break-up long sections of plain facades. This should include at least three of the following features:
 - (i) Major openings;
 - (ii) Different colours, materials or textures;
 - (iii) Indentations and extrusions which break the building into individual elements;
 - (iv) Protruding balconies;
 - (v) Awnings over windows.
- (e) All building levels should be clearly defined through the use of colours, materials and detailing.
- (f) The upper level(s) of buildings should be designed to promote informal surveillance of the street through the use of balconies and/or large windows.
- (g) Upper floor windows of buildings should be largely unobscured to promote passive surveillance.
- (h) Ground floor non-residential frontages fronting Rockingham Road or primary pedestrian linkages are to be designed as shop fronts with no less than 70% glazing. Buildings fronting other public areas shall be glazed for a minimum of 50%. Glazing percentages are to apply from between 0.9m and 2.1m above footpath/street level.
- (i) Commercial buildings should address the street in a traditional manner with windows facing the street and clearly defined entry points that are visible from the street. To achieve this entry points should generally include at least two of the following features:
 - (i) Appropriately scaled signage above the entry door;
 - (ii) Indentation of the entry point, with recessed entrances truncated at an angle to the pedestrian route of no less than 60 degrees;
 - (iii) Highlighting the entry point through the use of different materials.
- (j) Buildings should be designed so that services do not project above the specified maximum height of the building, and should be screened from view. To ensure adaptability of buildings this should include consideration of larger servicing unit requirements for other uses (such as restaurants) so that these can be accommodated should the building undergo a future change of use.

Title	Phoenix Activity Centre Design Guidelines
Policy Number (Governance Purpose)	LPP 4.7



Attachment 13 Phoenix Activity Centre Design Guidelines

- (k) To articulate street corners to provide visual interest and assist with legibility, new buildings located on corner sites should include:
 - (i) Architectural roof features that protrude above the normal roof line;
 - (ii) Increased parapet heights with additional detail, colour and textures; and/or
 - (iii) An increased number of storeys at the street corner.
- (l) New buildings with parapets should include indentations; additional modulation, and/or variation in parapet heights and designs so as to provide additional interest to the street.
- (m) In mixed use developments commercial uses should be separated from residential uses by being located on separate floors of a building to ensure the amenity and security of residents and commercial tenants.
- (n) Development applications are to be accompanied by design and documentation of 'back-of-house' services, including ducting and vents. To ensure the robustness and adaptability of buildings this should consider a general and basic overview of potential 'back-of-house' services for food businesses (such as ducting and vents allowing for the mechanical ventilation of kitchen areas, and 'grease traps').
- (o) Buildings adjacent to Rockingham Road are to be a minimum of two storeys in height, with single storey commercial buildings to be assessed on their merits against the objectives of the policy.
- (p) Garages facing Rockingham Road will not be supported.

3. Parking and Movement

- (a) Where new building(s) are proposed within the 'Phoenix Mixed Use Development and Access Precinct' they are required to be setback in accordance with the R60 coding.
- (b) At-grade car parking areas should be landscaped with suitable trees at the rate of one tree per 6 bays. The chosen trees should provide shade, improve amenity and assist in visual screening from above. The car park should also be appropriately lit for after-dark use.
- (c) Any new multi-storey car parks should incorporate interactive street frontages, such as shops or other uses that promote activity, where possible. These can be 'sleeved' along the street frontages of the car park structure.

Title	Phoenix Activity Centre Design Guidelines
Policy Number (Governance Purpose)	LPP 4.7



Attachment 13 Phoenix Activity Centre Design Guidelines

- (d) Where car parking levels (including undercroft levels) are visible from a street or public space, high quality architectural detailing shall be incorporated into the façade of all floors.
- (e) Development proposals should include only one vehicle crossover, to be placed where there is no street tree.
- (f) Wherever possible the finished level of buildings/tenancies should match that of the adjacent footpath so that continuous access is provided from the pedestrian footpath into each commercial tenancy and a consistent streetscape is achieved on Rockingham Road.
- (g) Services should be located away from the street and towards the rear of the site to minimise impact on the pedestrian environment.
- (h) Safe and comfortable pedestrian access shall be provided from the parking area to the entry point of the proposed development and to all street frontages.
- (i) For developments that include parking at the rear of the building pedestrian access between the street and car parking area is to be provided.

4. Servicing

- (a) Bin and service enclosures are to be screened and located away from visually prominent parts of the site. Wherever possible services should be designed to visually integrate into buildings, rather than be a separate element.
- (b) Development will need to conform to the City's Local Planning Policy related to Waste Management Plans in Multiple Unit development.

5. Change of use proposals for dwellings

- (a) Where the existing dwelling or building is the subject of a change of use proposal (to be accommodated within an existing dwelling/building), the following provisions will apply:
 - (i) Only one vehicle crossover is to be utilised, with any existing additional crossovers to be removed unless safe access and egress cannot be accommodated otherwise.
 - (ii) Existing large crossovers are to be reduced in size.
 - (iii) Car parking areas are to be designed so that vehicles can exit onto Rockingham Road in forward gear.

6. Landscaping

- (a) A comprehensive landscaping plan is required for the front setback area and verge, demonstrating an appropriate and attractive mix of

Title	Phoenix Activity Centre Design Guidelines
Policy Number (Governance Purpose)	LPP 4.7



Attachment 13 Phoenix Activity Centre Design Guidelines

hard paving and in-ground planting, provided that the plantings maintain openness to the building to ensure a visible and safe entrance, and create no potential entrapment areas.

- (b) Water-sensitive design planting principles will be encouraged.
- (c) Opportunities should be taken to include simple pedestrian amenities such as seats and shade/shelter.

(5) Northern Precinct

1. Objectives

- (a) To ensure buildings contribute positively to the public realm by creating visual interest, facilitating passive surveillance of streets and spaces used by the public, and contributing to pedestrian comfort.
- (b) To facilitate safe, comfortable pedestrian and cyclist movement, particularly in a north south direction through the activity centre.
- (c) To improve legibility for pedestrians throughout the precinct.
- (d) To ensure safe and legible vehicle access and egress throughout the precinct, particularly onto Rockingham and Phoenix Road.

2. General Built Form Provisions

- (a) New buildings or proposed modifications to existing buildings should include clearly identifiable pedestrian entry point(s).

3. Parking and Movement

- (a) Development must demonstrate how safe and convenient pedestrian movement from the street footpaths and car parking areas to building entry points are facilitated.

4. Signage

- (a) All applications for development are to be accompanied by a plan showing location and details of any proposed signage.

5. Landscaping

- (a) Development proposals should include landscaping plans that provide detail of plant species and maintenance.

Title	Phoenix Activity Centre Design Guidelines
Policy Number (Governance Purpose)	LPP 4.7



Attachment 13 Phoenix Activity Centre Design Guidelines

Strategic Link:	Town Planning Scheme No. 3
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Title	Local Development Plans
Policy Number (Governance Purpose)	LPP 5.5



Attachment 14 Local Development Plans

Policy Type

Local Planning Policy

Policy Purpose

Local Development Plans (LDP's) are a simple tool to facilitate the implementation of desirable variations or design objectives in a consistent manner. Facilitation is achieved by the LDP taking into account specific considerations and/or objectives and applying them on a site by site basis to achieve a consistent built form for a specified area. LDP's are created either through subdivision of land involving lots under 260m² or through the requirements of Town Planning Scheme No. 3 (TPS 3) under Structure Plans that call for LDP's to address specific built form aspirations for a location.

LDP's may be prepared by the City or the owner of the land and consist of a one page document with a plan of one or more lots annotated with various notes indicating variations preapproved by the LDP or desired outcomes for development on the subject lots.

TPS 3 lists matters for inclusion in a LDP created through the structure planning process. The Western Australian Planning Commission's (WAPC) operation policy for land subdivision Liveable Neighbourhoods (LN) originally introduced the concept of LDP's. LN sets out the scope of LDP's and provides a template which should be used in preparing a LDP.

However the list of information required does not provide guidance on the purpose and rationale for content of a LDP. There are also additional considerations that a LDP may address and these include matters such as neighbourhood character, occupant/resident safety and sustainability.

LDP's were primarily designed to facilitate specific variations to the requirements of the Residential Design Codes of Western Australia (R-Codes) to enable a design objective to be met. Such objectives may include solar orientation and access to sunlight maximisation or creating a more intimate streetscape with reduced front setbacks being permitted via the LDP.

On commercial and multi residential sites a LDP may also address building height, scale, bulk and the relationship of the mass of larger structures to the street and public spaces or between building structures and sites.

The purpose of this policy is to ensure adequate planning and design considerations are incorporated into LDP's for the purpose of achieving responsive development outcomes where particular built form outcomes are desired or required to be addressed.

Title	Local Development Plans
Policy Number (Governance Purpose)	LPP 5.5



Attachment 14 Local Development Plans Policy Statement

(1) LDP's whether required as a condition of subdivision approval or via a Structure Plan in a Development Area under TPS 3 are to demonstrate how they will address these objectives:

- achieve high quality, responsive development outcomes;
- integration of landform and site context responses;
- contribute to a strong sense of place, community and local identity; and
- facilitate sustainable dwelling design.

In particular where a LDP proposes variations to the R-Codes or TPS 3 development standards the LDP is required to demonstrate how the variations are consistent with the above objectives.

Proposed LDP's are to address the following matters (where applicable, as determined in consultation with the City).

1. Site Planning

- (a) Primary street setback variations;
- (b) Secondary street setback variations;
- (c) Setbacks to Rights of Way, common driveways or similar;
- (d) Ground and upper floor side and rear setbacks
- (e) Location and extent of boundary walls where they vary from the R-Codes
- (f) The location and setback of garages
- (g) Setbacks from open space reserves, rights of way or the like
- (h) Visual privacy setbacks where design objectives may result in overlooking of sensitive areas.
- (i) Building Height variations.

2. Dwelling Orientation - for the purpose of addressing or emphasising particular lot frontages, corner locations, local environmental conditions (prevailing winds/solar access) or views (immediate/distant).

3. Open Space - Outdoor Living Areas positioned to maximise passive environmental design opportunities (solar access and cross ventilation), and interactive development edges to streetscape environments or public open space.

4. Safe and Engaging Street and Public Space Frontages

- (a) Largely permeable street/public edge fencing and walls, providing for high levels of community interaction and visibility. This includes both dwelling elevations and boundary fencing (front and rear – if the property adjoins public open space).
- (b) Visually permeable fencing (with few exceptions), limited in height to maximise visual relationships between public and private spaces.

Title	Local Development Plans
Policy Number (Governance Purpose)	LPP 5.5



Attachment 14 Local Development Plans

5. Parking and Access

- (a) Safe and functional, unobtrusive on-site parking relative to adjoining development and the streetscape environment. On-site parking identified as 'preferred' or 'designated' to achieve these requirements.
- (b) On-site parking setback 4.5m where possible from the front or side lot boundary, catering for visitor parking in tandem (whilst diminishing the impact of parking related structures).
- (c) Vehicle access where available to be via a rear or side laneway (1st), secondary street (2nd) or primary street (3rd) in order of preference.
- (d) Garages and carports setback at least 0.5m from laneways which have a width of 6m or less or where the need arises (depending on laneway width) for safe access/egress.
- (e) Garage/carports to laneways limited to 6m in width or as per the R-Codes, whichever is the lesser requirement.
- (f) Street elevations are not to be dominated by carports and garages. In all instances, these elements are to be set behind the main alignment of the dwelling at least 0.5m where fronting a primary or secondary street.

6. Sustainability

- (a) Building envelopes prescribed wherever possible, to maximise the potential to access winter sun for warmth and natural lighting.
- (b) Building envelopes prescribed wherever possible, to maximise the potential to access prevailing winds for ventilation and summer cooling.

7. Noise from Road and Rail Transport

- (a) Sound attenuating measures or particular design requirements may be required for particular sites and/or locations to mitigate noise from transport corridors. For residential development, 'Quiet House Design' requirements to address a noise source/s and supported by noise assessments may be required.
- (b) Noise assessments and the submission of mitigation details where the planned use of land has the potential be impacted on by noise sources defined in State Planning Policy 5.4 Road and Rail Transport Noise and Freight Considerations in Land Use Planning are required

8. Expiry

LDP's are envisaged under the Planning and Development (Town Planning Schemes) Regulations 2015 to expire after a period of 10 years. However it is expected that a LDP remain in place until development of the subject area has

Title	Local Development Plans
Policy Number (Governance Purpose)	LPP 5.5



Attachment 14 Local Development Plans

been completed, After which time the LDP need no longer apply, this is particularly the case for single and grouped housing development.

(2) Process:

LDP's are to be presented to the City for determination in accordance with the requirements of a Subdivision Approval or under the requirements of TPS 3 where facilitated by a Structure Plan and a Development Area. An application for a LDP is to include a copy of the LDP consistent with the Liveable Neighbourhoods template and all supporting reports and information are to be provided.

The relevant application fee for a LDP is required to be paid prior to assessment commencing.

Strategic Link:	Town Planning Scheme No. 3
Category	Planning - Town Planning & Development
Lead Business Unit:	Development Assessment and Compliance
Public Consultation: (Yes or No)	Yes
Adoption Date: (Governance Purpose Only)	10 November 2022
Next Review Due: (Governance Purpose Only)	November 2024
ECM Doc Set ID: (Governance Purpose Only)	4514879

Title	Vehicle Access
Policy Number (Governance Purpose)	LPP 5.6



Attachment 15 Vehicle Access

Policy Type

Local Planning Policy

Policy Purpose

When land adjacent to major/arterial/distributor/important roads is developed for more intensive uses the resulting additional traffic generated by such uses, can cause conflict, especially where pre-existing traffic -volumes are high. This can create dangerous and unattractive road environments.

In these situations, a coordinated approach to vehicle access is required to ensure that development does not introduce any undesirable impacts on the safe and efficient movement for motorists, heavy vehicles operators, public transport users, pedestrians and cyclists.

The purpose of the Policy provides a framework for the planning and development of safe and efficient movement of motorists, public transport users, pedestrians and cyclists, where such a coordinated approach to vehicle access is required.

The Policy will be used by the City of Cockburn to guide the assessment of applications for development, subdivision and Local Development Plans, where a Vehicle Access Plan has been prepared.

The overall objectives for the policy are to:

- (1) Provide for safe and efficient movement of motorists, public transport users, pedestrians and cyclists;
- (2) Provide for safe and efficient movement of waste management and other service vehicles;
- (3) Minimise the potential for conflict between through and local traffic;
- (4) Provide visually attractive road environments; and
- (5) Provide for reasonable property access that is direct, convenient and safe.

Policy Statement

(1) Vehicle Access Policy Plans:

1. Vehicle Access Policy Plans are to be prepared by the City of Cockburn in consultation with relevant stakeholders, which may include Main Roads WA, the Department of Planning and landowners of affected properties.
2. Vehicle Access Policy Plans are to be adopted by Council for inclusion as an Appendix to the Vehicle Access Policy.

Title	Vehicle Access
Policy Number (Governance Purpose)	LPP 5.6



Attachment 15 Vehicle Access

3. The access arrangements in Vehicle Access Policy Plans shall be constructed to public road standards and should consider the following design features to ensure traffic safety and efficiency:
 - a) road reserve widths;
 - b) intersection location and spacing;
 - c) intersection type;
 - d) traffic control type (i.e traffic signals, roundabout, signage);
 - e) median location and breaks;
 - f) the number, location and design of vehicle access points to adjoining private properties;
 - g) the size and length of vehicles that will be using the access;
 - h) the volume of traffic that will be using the access;
 - i) road lighting;
 - j) mechanisms for organising and securing shared use of crossovers;
 - k) servicing of properties by waste management and other commercial vehicles; and
 - l) provision of cyclist and pedestrian facilities and integration with the adjacent path network. .

4. Vehicle Access Policy Plans are to consider pedestrian and cyclist movement patterns and key desire lines, particularly where residential development exists or is planned in proximity to the Vehicle Access Policy Plans area.

5. Vehicle Access Policy Plans are to be prepared with reference to the relevant local/state/national policies, guidelines and Australian Standards as determined appropriate by the Local Government.

(2) Vehicle Access Controls:

1. Crossovers

- a) Control over the location, design and number of crossovers will be exercised by the responsible authority to reflect the arrangements shown on a Vehicle Access Policy Plan.

- b) There will be a presumption against the creation of new vehicle crossovers across the primary frontage of the lot where alternative access is, or could be made available, to secondary roads and laneways, or via easements in gross arrangements to a nominated crossover as shown on the Vehicle Access Policy Plan.

- c) Where a vehicle crossover is permitted under a Vehicle Access Policy Plan, conditions may be imposed on the width and design of the crossover to ensure adequate visibility and to provide for the safe and convenient movement of vehicles entering and leaving the road.

2. Easement in Gross

Title	Vehicle Access
Policy Number (Governance Purpose)	LPP 5.6



Attachment 15 Vehicle Access

- a) Where indicated on a Vehicle Access Policy Plan, the City of Cockburn will require as a condition of development or subdivision, easements in gross in the form of a public access easement on land titles. The easements in gross are granted for the City of Cockburn to maintain public access across the subject land to the side streets and crossover access points as delineated on the Vehicle Access Policy Plan. These easements in gross on land titles are to be provided at the cost of the developer or landowners of the subject land. The treatment of the area covered by the easement in gross shall be designed and treated to the satisfaction of the City to ensure consistency across lots.
- b) In determining development applications where a Vehicle Access Policy Plan requires the provision of a right-of-carriageway and where such access is not available through adjacent properties to a dedicated road, consideration may be given for retention of a temporary access crossover until such time as alternative access is available via the right-of-carriageway system.
- c) Provisions for the closure of the temporary crossover shall be provided by way of legal agreement.

3. Parking/Circulation

- a) All applications for development approval on land subject to a Vehicle Access Policy Plan will be required to provide for traffic to enter and leave the site in a forward direction and to comply with the City of Cockburn's car parking and on-site vehicle circulation requirements, as detailed in the City's Town Planning Scheme No. 3 (TPS 3). This requirement is intended to ensure that there is no traffic congestion on-site which would interfere with the safety and free flow of traffic along the primary road.

4. Variation Clause

- a) The precise location of rights of carriageways or crossovers delineated on a Vehicle Access Policy Plan may be varied subject to the agreement of the City of Cockburn provided that the purpose and the intent of the Vehicle Access Policy Plan is maintained.
- b) A detailed assessment may be required in the form of a traffic management plan, a traffic impact assessment, a road safety audit, and/or traffic modelling may be required where a variation to a Vehicle Access Policy Plan is proposed. The form and scope of the required traffic assessment will be subject to guidance and agreement by the City of Cockburn.
- c) This policy document may be updated with agreement from the City of Cockburn.

Title	Vehicle Access
Policy Number (Governance Purpose)	LPP 5.6



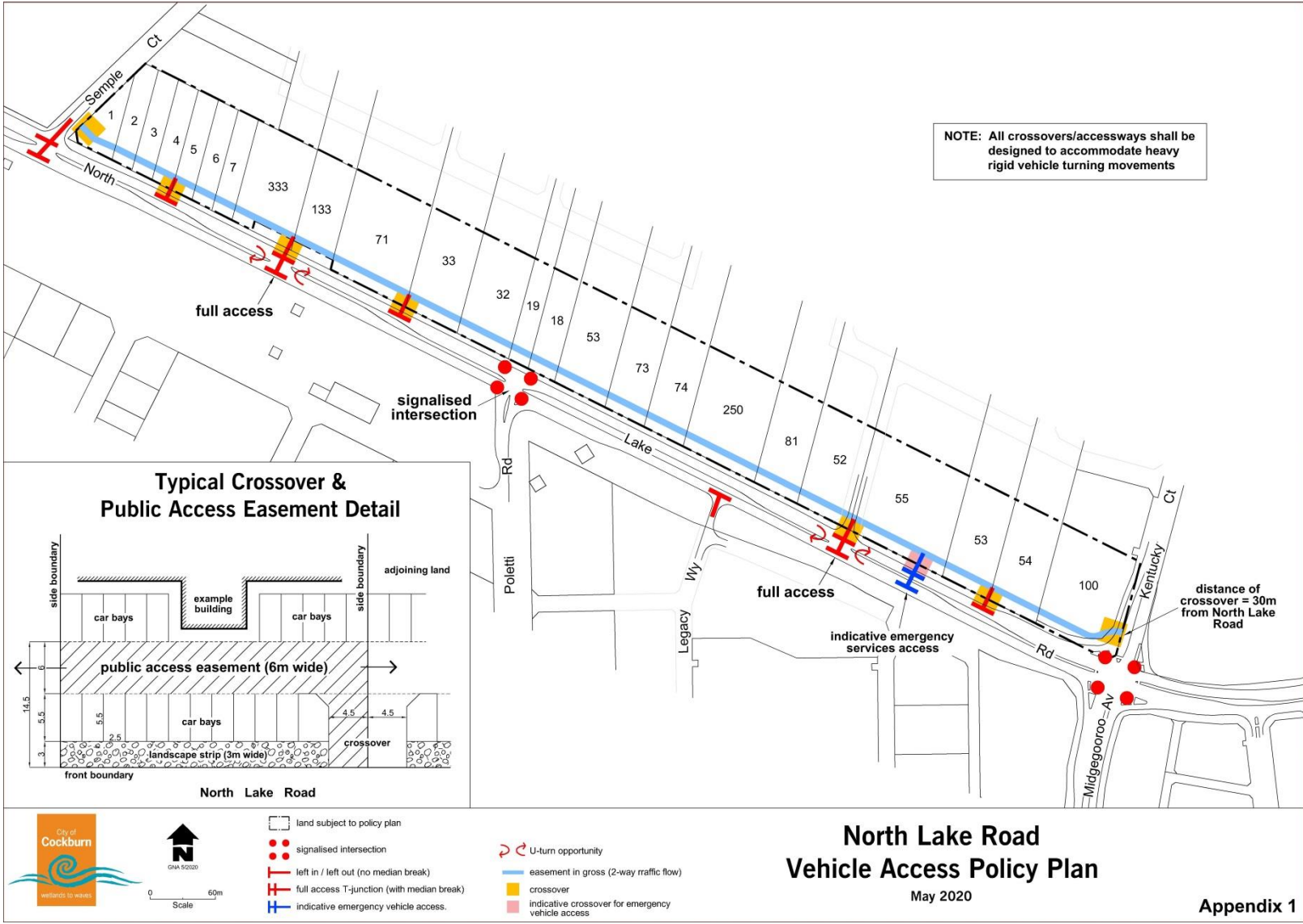
Attachment 15 Vehicle Access

- d) With specific regard to the Verde Drive Access Plan consideration will be given to the exact location of crossovers and need for an easement (West of Solomon Road) as development occurs. This recognises the likely need to reconfigure long narrow lots and the likely timing of various developments will likely be staged over time. The City at such time(s) will ensure the staging of development in this area is provided with suitable access arrangements and will use discretion by having due regard to the objectives of this policy and the intent of the Plan illustrated at Appendix 2.

Title	Vehicle Access
Policy Number (Governance Purpose)	LPP 5.6



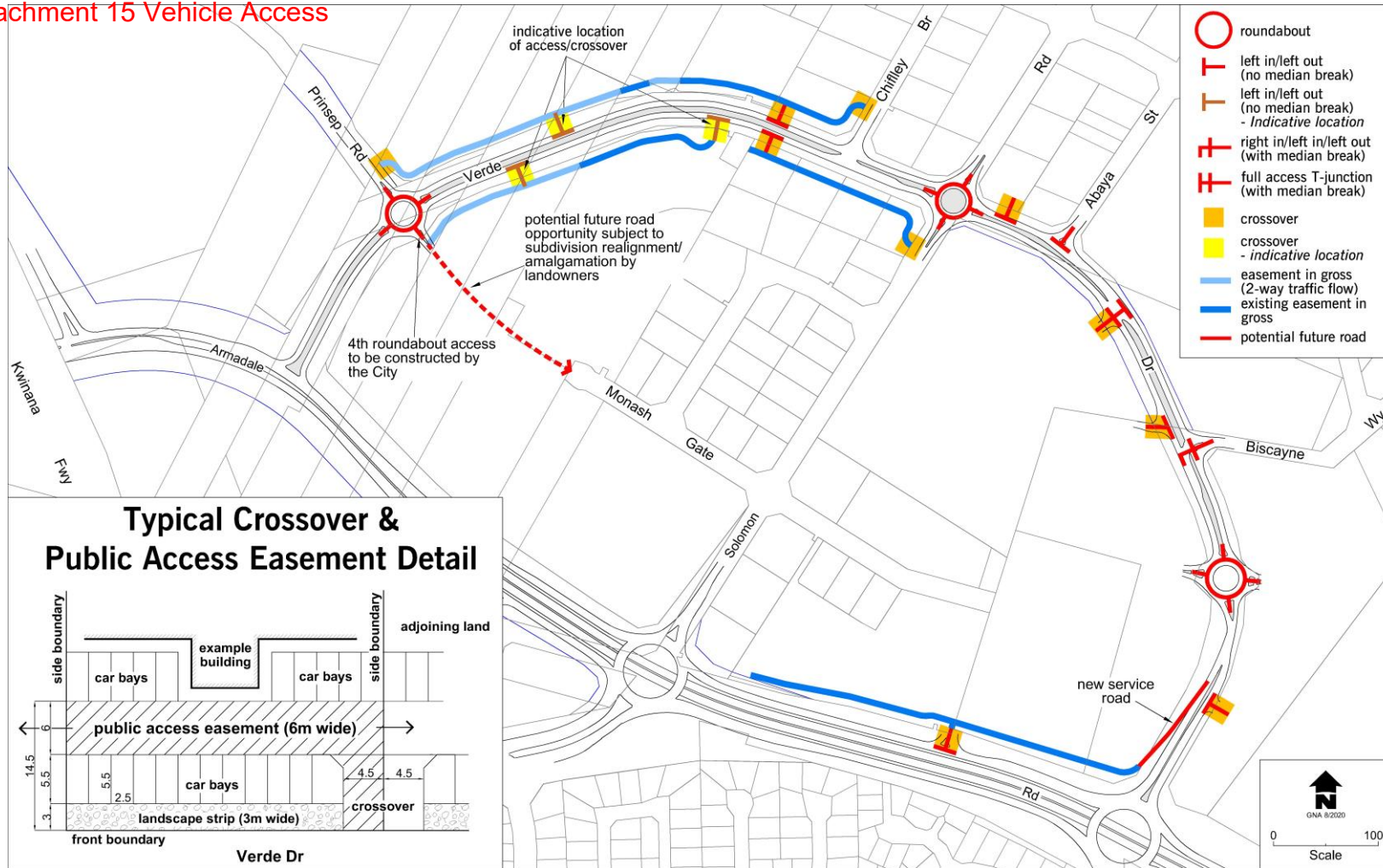
Attachment 15 Vehicle Access



Title	Vehicle Access
Policy Number (Governance Purpose)	LPP 5.6



Attachment 15 Vehicle Access



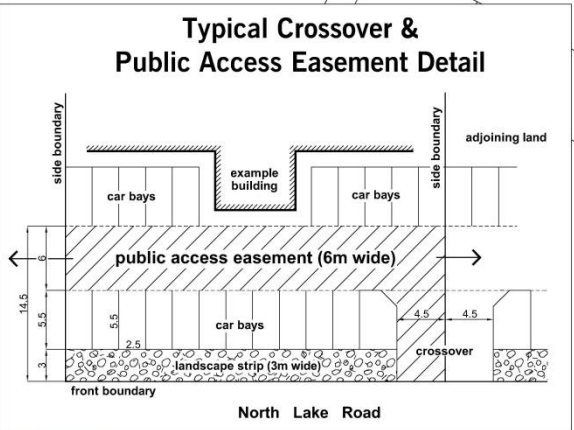
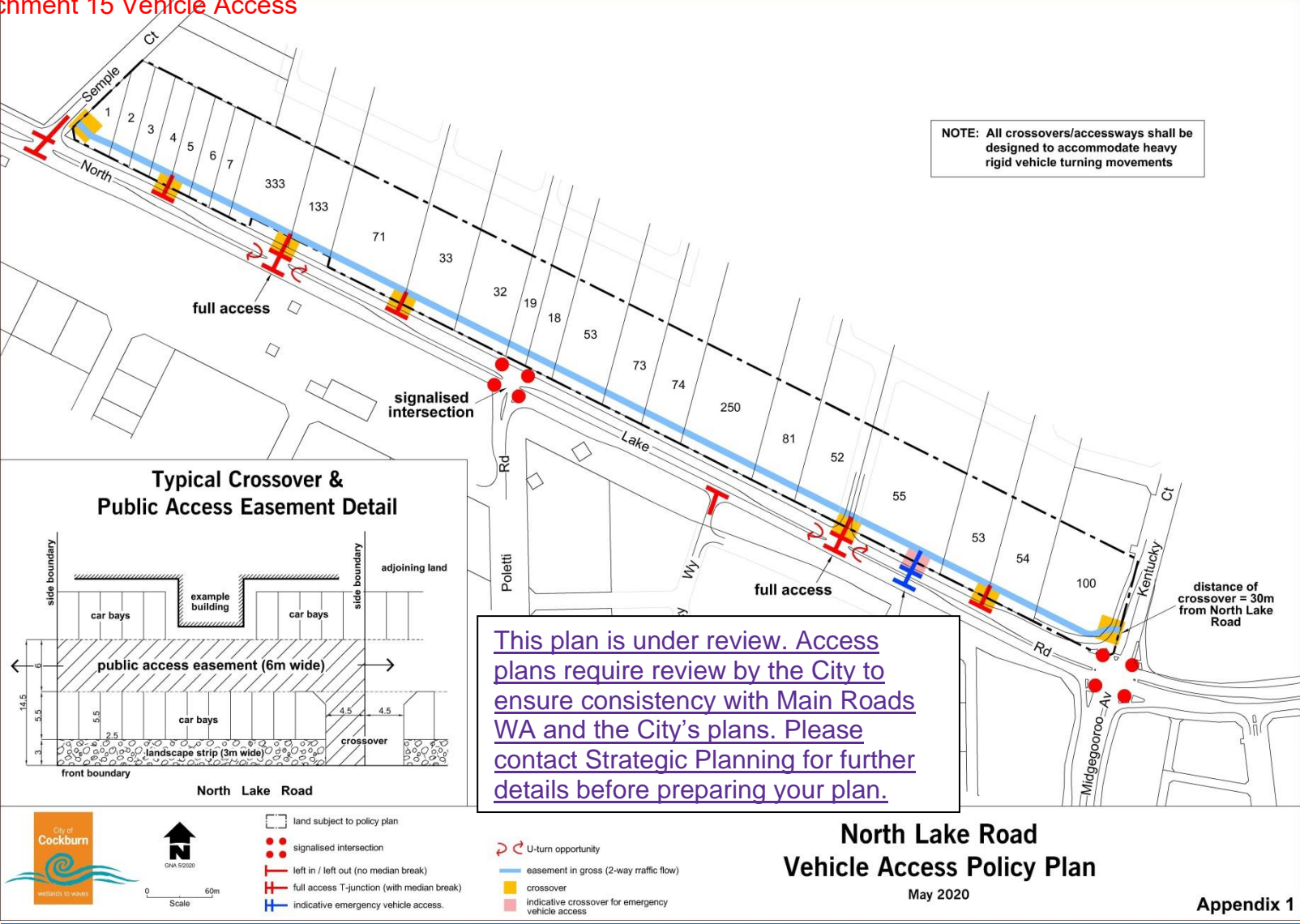
Verde Drive Vehicle Access Policy Plan

Appendix 2
Aug 2020

Title	Vehicle Access
Policy Number (Governance Purpose)	LPP 5.6



Attachment 15 Vehicle Access



This plan is under review. Access plans require review by the City to ensure consistency with Main Roads WA and the City's plans. Please contact Strategic Planning for further details before preparing your plan.

Scale: 0 to 60m

- land subject to policy plan
- signalised intersection
- left in / left out (no median break)
- full access T-junction (with median break)
- indicative emergency vehicle access.
- U-turn opportunity
- easement in gross (2-way traffic flow)
- crossover
- indicative crossover for emergency vehicle access

North Lake Road Vehicle Access Policy Plan

May 2020

Appendix 1

Title	Vehicle Access
Policy Number (Governance Purpose)	LPP 5.6



Attachment 15 Vehicle Access

Strategic Link:	Town Planning Scheme No. 3
Category	Planning - Town Planning & Development
Lead Business Unit:	Development Assessment and Compliance
Public Consultation: (Yes or No)	Yes
Adoption Date: (Governance Purpose Only)	10 November 2022
Next Review Due: (Governance Purpose Only)	November 2024
ECM Doc Set ID: (Governance Purpose Only)	4517299

Title	Sea Containers
Policy Number (Governance Purpose)	LPP 5.8



Attachment 16 Sea Containers

Policy Type

Local Planning Policy

Policy Purpose

Sea Containers (also known as ISO Containers and Shipping Containers) have become available in increasing numbers on the second hand market from transport agencies and Sea Container repair companies in recent years.

Sea Containers afford a cheap, secure method of storing items or goods and are becoming increasingly popular for this purpose. Sea Containers are made in varying sizes, but are generally 2.4m x 2.4m² and range in length from 3m to 12m. The most common length is the 6m (20 feet) version. As Town Planning Scheme No. 3 (TPS 3) contains no guidance on this matter, this policy will ensure the use of sea containers is managed effectively.

The purpose of this policy is to guide and control the use of Sea Containers within the City of Cockburn.

Policy Statement

- (1) Sea containers are not permitted within the boundaries of any Residential zoned land within the City other than for a temporary period (maximum 18 months) in association with building construction subject to obtaining planning approval from the City. In this respect only one container is permitted at any one time. The City may condition the external upgrade to the façade of the sea container, where deemed necessary.
- (2) One (1) Sea Container may be permitted to be kept on Rural, Rural Living or Resource zoned land, subject to obtaining planning approval from the City and the following conditions:
 1. The container shall be no longer than 6m (20 feet).
 2. The container is to be located behind the dwelling on-site and within the Building Envelope (if one exists). The container is also to be screened from view of the street, including secondary streets, and adjoining properties.
 3. The container shall be set back from the side and rear boundary in accordance with the setback requirements of the TPS 3 for land zoned Rural, Rural Living and Resource zone.
 4. The container shall be painted in a colour that is similar to or complementary to the colour of existing buildings on the property, or the prevailing landscape and/or upgraded to the satisfaction of the City.

Title	Sea Containers
Policy Number (Governance Purpose)	LPP 5.8



Attachment 16 Sea Containers

5. Sea Containers are to be used for storage purposes only, that is, the container shall not be used for any commercial, industrial or habitable purpose.
 6. Sea containers will not be permitted on land where there is no existing dwelling or where there is no dwelling under construction.
- (3) Sea Containers on land zoned Local Centre, District Centre, Regional Centre, Mixed Business, Light & Service Industry and Industry zones may be kept within an approved yard subject to obtaining planning approval from the City and the following conditions:-
1. The container(s) shall not be located within car parking areas or within the front or secondary street building setback area.
 2. The container(s) shall be screened from view of the street, including secondary streets.
 3. The container(s) shall be painted in a colour that is similar to or complementary to the colour of existing buildings on the property and/or upgraded to the satisfaction of the City.
 4. As per the order of the Executive Director, Public Health WA, Sea Containers may not be used for the purpose of storing food products unless they are modified to comply with the Health (Food Hygiene) Regulations 1993.
 5. Refrigerator motors and other cooling devices must be modified to ensure that the noise emitted from the unit complies with the *Environmental Protection (Noise) Regulations 1997*.
- (4) Development Applications for Sea Containers in a Development Zone will be assessed and determined on their merits taking into TPS 3 and the content of any Structure Plan that may apply.
- (5) Refrigerated Sea Containers are not permitted on Residential, Rural, Rural Living or Resource zoned land anywhere in the City.
- (6) Sea containers are not permitted to be used for or in conjunction with advertising signs or the like.
- (7) The City has the right to instruct a landowner to remove a sea container from land in the City if any or all of the above conditions, or any other conditions associated with a planning approval for a sea container have not been addressed.
- (8) Sea Containers are not permitted to be placed on public reserve or other public or Crown land within the City unless all relevant approvals are obtained through the Engineering Directorate

Title	Sea Containers
Policy Number (Governance Purpose)	LPP 5.8



Attachment 16 Sea Containers

(9) Sea Containers and skip bins are permitted only within the road reserve to facilitate minor home renovations or landscaping works subject to the following conditions:

1 Sea Containers are permitted to be located on the verge adjacent to the property where the request has been received by the Engineering Directorate subject to the following:

1. Sea Containers size – 6m (20 foot) (max)
2. 14 day period (max)
3. Not to be located on a footpath (where possible)
4. Not to impede the vision of adjacent property owners
5. Not to impede the street tree (where possible)

(2) Skip Bins are permitted to be located on the verge adjacent to the property where the request has been received by the Engineering Directorate subject to the following:

1. Skip Bin Size – 8m³ (max)
2. 14 day period (max)
3. Not to be located on a footpath (where possible)
4. Not to impede the vision of adjacent property owners
5. Not to impede the street tree (where possible)

Strategic Link:	Town Planning Scheme No. 3
Category	Planning - Town Planning & Development
Lead Business Unit:	Development Assessment and Compliance
Public Consultation: (Yes or No)	Yes
Adoption Date: (Governance Purpose Only)	10 November 2022
Next Review Due: (Governance Purpose Only)	November 2024
ECM Doc Set ID: (Governance Purpose Only)	4516539

Title	Subdivision & Development - Street Trees
Policy Number (Governance Purpose)	LPP 5.18



Attachment 17 Subdivision & Development - Street Trees Policy Type

Local Planning Policy

Policy Purpose

The City of Cockburn (City) manages over 35,000 street trees which benefit our City and suburbs in many diverse ways. These include:

- Providing pedestrians and cyclists with cooler and more attractive access ways throughout the City;
- Improving air quality;
- Providing habitat for native fauna;
- Reducing heat island effect; and
- Enhancing the character and attractiveness of our suburbs.

The City recognises the importance of trees in contributing to the health and well-being of our communities and is therefore committed to:

- Preserving the City's Urban Forest through tree protection and maintenance programs;
- Increasing the number of street trees on verges through proactive planting programs and plantings associated with the subdivision and development of land; and
- Increasing the canopy coverage throughout the City.

The purpose of this policy is to increase the number of street trees in the City's road reserves in new and infill areas, and provide a framework for their installation and management.

This supports Council's *Strategic Community Plan 2020 – 2030*, and the specific objectives to "address climate change" and "protection and enhancement of our natural areas, bushland, parks and open spaces". Council's objective will be measured and become achieved through the provision of new street trees, associated with subdivision and/or development within the City.

This policy also reflects community feedback which lists streetscape appearances as a high community priority in Cockburn

Policy Statement

(1) Background

The policy has due regard to, and should be read in conjunction with, the following Western Australian Planning Commission (WAPC) State Planning Policies (SPP) and operational policies:

- SPP No. 3.0 (Urban Growth and Settlement)
- SPP No. 7.3 (Residential Design Codes)
- Liveable Neighbourhoods (WAPC 2009)

Title	Subdivision & Development - Street Trees
Policy Number (Governance Purpose)	LPP 5.18



Attachment 17 Subdivision & Development - Street Trees

This policy has due regard to and should be read in conjunction with the following City of Cockburn Policies:

- LPP 1.2 'Residential Design Guidelines'
- AEW1 'Street Verge Improvements'
- SEW1 'Maintenance of Verge/ Public Open Space Following Residential Subdivision'
- City of Cockburn – Good Design Guide – Commercial Lot Development
- City of Cockburn – Verge Development Guide – Residential Verges

(2) Application

This policy applies to land which is zoned Residential, Regional Centre, District Centre, Local Centre, Mixed Business, Mixed Use, Light and Service Industry and Industry.

This policy is pursuant to City of Cockburn Town Planning Scheme No. 3 (TPS 3).

(3) Provisions

3.1 Requirement for street trees:

Street trees are required at both a subdivision and development stage, as follows:

3.1.1 Subdivision:

Once an applicant, subdivider, developer or landowner as the case may be (hereafter referred to as "proponent") receives a subdivision approval from the Western Australian Planning Commission which involves civil works, a condition will require that they must first prepare detailed engineering/civil works drawings for submission and approval by the City, prior to the commencement of works.

For the purposes of approving the civil /engineering drawings, this policy requires these drawings to indicate the provision of street trees to be planted on both sides of all streets within the subdivision application area, and how they will be maintained.

3.1.2 Development:

An applicant, developer or landowner as the case may be (hereafter referred to as "proponent") receiving a development approval from the City of Cockburn (except those for two grouped dwellings or a Single (R-Code) House), will receive a condition imposed on such approval for the requirement to install and maintain street tree/s where the adjoining verge has an inadequate amount of street trees.

Title	Subdivision & Development - Street Trees
Policy Number (Governance Purpose)	LPP 5.18



Attachment 17 Subdivision & Development - Street Trees

3.2 Quantity of trees required:

Street trees are to be provided at the rate listed below.

3.2.1 Subdivisions:

One tree per lot or in the case of lots less than a 10m width, at a rate to be determined by the City.

3.2.2 Development subject to the R-Codes (except those for two grouped dwellings or a Single (R-Code) House):

One street tree per dwelling, or in the case of lots less than 10m of linear lot width to a public road reserve, at a rate to be determined by the City.

3.2.3 All other development:

One tree per 10m of linear lot width to a public road reserve or at a rate to be determined by the City where agreed upon between the applicant and the City.

3.3 Options for clearance:

In order to satisfy the conditions of the WAPC subdivisional approval or City of Cockburn development approval noted above, the following two options are to be used:

3.3.1 Proponent provides contribution payment to City of Cockburn who then takes responsibility for planting and management.

In this option, the proponent pays the City \$600 per street tree required. This payment must be receipted by the City prior to the practical completion certification of the civil works for subdivision or issue of building permit for development.

In receiving this contribution, the City will purchase, install and maintain all street trees. Installation will occur at the most optimal time in the opinion of the City.

3.3.2 Proponent provides the City with a bond for the value of planting and maintaining street trees.

For this option a minimum of ten (10) street trees or more must be required. This option allows the proponent to bond the value of the planting and maintenance of street trees required whilst allowing the applicant to obtain the practical completion certification of the civil works for subdivision or the building permit for development.

Title	Subdivision & Development - Street Trees
Policy Number (Governance Purpose)	LPP 5.18



Attachment 17 Subdivision & Development - Street Trees

The City will return the bond, only in one amount, once all bonded street trees have been planted and maintained consistent with any approvals.

3.4 Prior to Occupation

For all intents and purposes, where a condition of development approval requires the planting of the trees prior to occupation of the development, in lieu of prior to the issue of the building permit, the City acknowledges that neither option A or B above is applicable or required to clear such condition.

3.5 Species, maintenance and longevity

Street trees must comprise of species which will mature to a sufficient size and canopy, and be located such as to provide sufficient shading of the street verge area, to the satisfaction of the City. The City may require such engineering or development drawings to demonstrate this detail on the relevant plans to ensure no conflict with road infrastructure or services.

3.6 All existing or proposed street trees shall be protected from adjoining development in accordance with Australian Standard AS4970-2009 via maintenance of a *Tree Protection Zone* and any relevant measures to ensure protection of the asset accordingly.

Strategic Link:	Town Planning Scheme No. 3
Category	Planning - Town Planning & Development
Lead Business Unit:	Development Assessment and Compliance
Public Consultation: (Yes or No)	Yes
Adoption Date: (Governance Purpose Only)	10 November 2022
Next Review Due: (Governance Purpose Only)	November 2024
ECM Doc Set ID: (Governance Purpose Only)	5670114