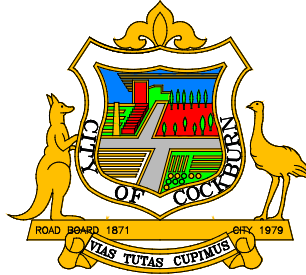


CITY OF COCKBURN



ORDINARY COUNCIL

AGENDA PAPER

FOR

THURSDAY, 13 SEPTEMBER 2012

CITY OF COCKBURN

SUMMARY OF AGENDA TO BE PRESENTED TO THE ORDINARY COUNCIL MEETING TO BE HELD ON THURSDAY, 13 SEPTEMBER 2012 AT 7:00 PM

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CITY OF COCKBURN

AGENDA TO BE PRESENTED TO THE ORDINARY COUNCIL MEETING TO BE HELD ON THURSDAY, 13 SEPTEMBER 2012 AT 7:00 PM

1. DECLARATION OF MEETING

2. APPOINTMENT OF PRESIDING MEMBER (If required)

3. DISCLAIMER (To be read aloud by Presiding Member)

Members of the public, who attend Council Meetings, should not act immediately on anything they hear at the Meetings, without first seeking clarification of Council's position. Persons are advised to wait for written advice from the Council prior to taking action on any matter that they may have before Council.

4. ACKNOWLEDGEMENT OF RECEIPT OF WRITTEN DECLARATIONS OF FINANCIAL INTERESTS AND CONFLICT OF INTEREST (by Presiding Member)

5. APOLOGIES AND LEAVE OF ABSENCE

6. ACTION TAKEN ON PREVIOUS PUBLIC QUESTIONS TAKEN ON NOTICE

7. PUBLIC QUESTION TIME

8. CONFIRMATION OF MINUTES

8.1 (OCM 13/09/2012) - ORDINARY COUNCIL MEETING - 09/08/2012

RECOMMENDATION

That Council confirm the Minutes of the Ordinary Council Meeting of 9 August 2012, as a true and accurate record.

COUNCIL DECISION

9. WRITTEN REQUESTS FOR LEAVE OF ABSENCE

10. DEPUTATIONS AND PETITIONS

11. BUSINESS LEFT OVER FROM THE PREVIOUS MEETING (If adjourned)

12. DECLARATION OF COUNCILLORS WHO HAVE NOT GIVEN DUE CONSIDERATION TO MATTERS IN THE BUSINESS PAPER

13. COUNCIL MATTERS

13.1 (OCM 13/09/2012) - MINUTES OF CHIEF EXECUTIVE OFFICER PERFORMANCE & SENIOR STAFF KEY PROJECTS APPRAISAL COMMITTEE - 16 AUGUST 2012 (CC/C/017) (S CAIN) (ATTACH)

RECOMMENDATION

That Council receive the Minutes of the Chief Executive Officer Performance and Senior Staff Key Projects Appraisal Committee dated 16 August 2012, as attached to the Agenda, and adopt the recommendations contained therein.

COUNCIL DECISION

Background

The Chief Executive Officer Performance and Senior Staff Key Projects Appraisal Committee met on 16 August 2012. The minutes of that meeting are required to be presented to Council and its recommendations considered by Council.

Submission

N/A

Report

The Committee recommendations are now presented for consideration by Council and, if accepted, are endorsed as the decisions of Council. Any Elected Member may withdraw any item from the Committee meeting for discussion and propose an alternative recommendation for Council's consideration. Any such items will be dealt with separately, as provided for in Council's Standing Orders.

The independent confirmation, as requested under the Committee Recommendation at Item 17.1 of the Minutes, was provided by the CEO to all Committee Members via email on 24 August 2012.

Strategic Plan/Policy Implications

Governance Excellence

- To maintain a professional, well-trained and healthy workforce that is responsive to the community's needs.

Budget/Financial Implications

Committee Minutes refer.

Legal Implications

Committee Minutes refer.

Community Consultation

N/A

Attachment(s)

Minutes of the Chief Executive Officer Performance and Senior Staff Key Projects Appraisal Committee 16 August 2012 are provided to the Elected Members as confidential attachments.

Advice to Proponent(s)/Submissioners

The CEO and Senior Staff have been advised that this item will be considered at the 13 September 2012 OCM.

Implications of Section 3.18(3) Local Government Act, 1995

Committee Minutes refer.

14. PLANNING AND DEVELOPMENT DIVISION ISSUES

14.1 (OCM 13/09/2012) - AMENDMENTS TO DETAILED AREA PLAN AND JETTY DESIGN GUIDELINES FOR STAGE 4A PORT COOGEE, NORTH COOGEE - PREPARED BY: TAYLOR BURRELL BARNETT - PROPONENT: CONSOLIDATED MARINE DEVELOPMENTS AND ANCHORAGE INDUSTRIES PTY/LTD (PS/A/001) (T WATSON) (ATTACH)

RECOMMENDATION

That Council:

- (1) approve the amended Detailed Area Plan and Jetty Design Guidelines presented for Stage 4C Port Coogee, North Coogee, prepared by Taylor Burrell Barnett for Consolidated Marine Developments and Anchorage Industries Pty Ltd, pursuant to the provisions contained under Clause 6.2.15 of the City of Cockburn Town Planning Scheme No. 3; and
- (2) advise the applicant accordingly.

COUNCIL DECISION

Background

At its 12 June 2008 meeting the Council approved the original Detailed Area Plan (DAP) and Jetty Design Guidelines for Stage 4A within the Port Coogee development area. Stage 4A provides for low density residential development (R25) and forms one of the 'Water Based Residential' precincts within Port Coogee. The approved DAP and Guidelines provide development direction for 36 single residential lots within the stage, including 18 waterside lots on the southern side of the subdivision. The 18 lots forming the northern edge of the subdivision abut foreshore public open space.

Whilst the DAP is primarily aimed at guiding builtform outcomes on all lots, the Guidelines deal with the control and development of jetties and related structures in mooring envelopes at the rear of the water side lots. To date, the DAP and Jetty Design Guidelines have been successfully applied in the assessment and determination of several

Development Application/Building Licence proposals lodged with the City.

Council at its March 2012 meeting approved an amended DAP and Guidelines for the Stage. The owners of two adjoining waterside lots made application to the Western Australian Planning Commission (WAPC) to adjust the alignment of the common boundary dividing the lots. In granting approval to the subdivision, the WAPC imposed the recommended condition of the City requiring "*Revised Detailed Area Plans to be prepared for all lots within the proposed subdivision.*"

A further approval to subdivide two lots in the Stage into three lots was recently granted by the WAPC (1 August 2012). The same condition requiring the DAP to be amended was imposed on the approval. Taylor Burrell Barnett (TBB) have again been engaged to revise the applicable DAP and Jetty Design Guidelines (having produced the original DAP in consultation with the City i.e. on behalf of Australand).

Submission

The DAP and Jetty Design Guidelines have been amended to reflect the revised lot layout involving the land in question (formerly Lots 909 and 910, now Lots 21, 22 and 23 Chelydra Point). The new lots each have a 10 metre frontage to Chelydra Point (previously 15.0 metres wide).

The changes proposed include the following:

1. Alternate setback arrangements for the lots given their reduced frontage.
2. The inclusion of a note encouraging 4.5 metre garage setbacks on the lots (given their reduced frontage and the inability to incorporate parking perpendicular to the street i.e. within a large basement or ground floor parking area as some owners have).
3. The amendment of the DAP to include reference to the lots (21, 22 and 23) where relevant or necessary i.e. in conjunction with the other lots comprising the subdivision (and referred to in the DAP and Design Guidelines).
4. The inclusion of the lots to the 'Typical Setback Requirements' box for Lot 50 (included as part of the original amendment to the DAP).
5. The amendment of both the DAP and Jetty Design Guidelines to show a reduced mooring envelope width (12.0 metres reduced to 8.0 metres).

All other aspects of the DAP and Jetty Design Guidelines remain the same.

Report

The amendments and changes made to the DAP and Jetty Design Guidelines have been made by TBB in consultation with the City. The involvement of TBB to facilitate the amendments is fortunate given their involvement in the production of the first DAP for Stage 4A. Maintaining the integrity of the documents and the principles that underlie their content is important to ensuring the built environment develops in an orderly and proper manner.

As stated by Taylor Burrell Barnett:

“The modifications to the DAP for proposed Lots 21, 22 and 23 have been deliberately configured to maintain original development principles and lot interface requirements contained on the approved detailed area plan. This was a primary objective within the modification process, thereby limiting any impact on properties neighbouring Lots 21 and 23.”

Bearing the above in mind, approval of the amended DAP and Jetty Design Guidelines is recommended. Approval is in accordance with the provisions of 6.2.15 of the Scheme. The provisions identify planning considerations to be included in a DAP (and Design Guidelines) and the process for adopting such (Clause 6.2.15.8 provides scope for a DAP to be amended).

Where a DAP/Guidelines may affect landowners other than the owner/s of the subject land, the City may undertake consultation. In the subject instance, the status quo is generally maintained where the new lots interface with adjoining lots. Specifically, the controls that apply are essentially the same as they currently exist. That is, there is no material impact anticipated on the adjoining lots by virtue of the proposed changes.

Strategic Plan/Policy Implications

Demographic Planning

- To ensure the planning of the City is based on an approach that has the potential to achieve high levels of convenience and prosperity for its citizens.
- To ensure development will enhance the levels of amenity currently enjoyed by the community.

Budget/Financial Implications

N/A

Legal Implications

Town Planning Scheme No. 3
Planning and Development Act 2005

Community Consultation

No consultation has been undertaken.

Attachment(s)

1. Location/Structure Plan
2. Detailed Area Plan
3. Jetty Design Guidelines

Advice to Proponent(s)/Submissioners

The Proponent(s) have been advised that this matter is to be considered at the 13 September 2012 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

14.2 (OCM 13/09/2012) - INITIATION OF CITY OF COCKBURN TOWN PLANNING SCHEME NO. 3 PROPOSED AMENDMENT NO. 97 (93097) (C CATHERWOOD)

RECOMMENDATION

That Council

(1) in pursuance of Section 75 of the *Planning and Development Act 2005* ("Act") , initiate an amendment to City of Cockburn Town Planning Scheme No. 3 ("Scheme") for the purposes of:

1. Providing additional clarification as to the methodology of the calculation for development contributions for Development Contribution Plan 13 into Schedule 12 as follows (additional words shown in bold text):

Method for calculating contributions	The City's Plan for the District identifies the needs that impact on the Development Contribution Plan. The contributions outlined in this plan have been derived based on the need for the facilities generated by the additional development in the Development Contribution Plan. This calculation excludes the demand for a facility that is generated by the current population in existing dwellings.
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	<p>Contributions shall be calculated on the basis of the number of new lots and/or dwellings created. Existing dwellings on a lot or lots to be subdivided or developed will be exempt from the contribution. Land required for public roads, public open space, drainage and other uses not including residential development will not be assessable. Where a lot may have further subdivisional potential, for example as a grouped dwelling site, contributions will be sought at the next development approval stage where additional dwellings or lots are created.</p> <p>Contributions applying to development of aged or dependant person's dwellings or single bedroom dwellings shall be calculated on the number of dwelling units permitted prior to the application of the variations permissible under clause 6.1.3.A3.1 of State Planning Policy Residential Design Codes.</p> <p>Notwithstanding the definition of 'lot' listed in Schedule 1, for the purposes of calculating cost contribution liability within DCA13, the term lot will be inclusive of green title, survey strata and built strata subdivisions.</p>
<p>(2) note as the amendment is in the opinion of Council consistent with Regulation 25(2) of the <i>Town Planning Regulations 1967</i> ("Regulations"), the amendment be referred to the Environmental Protection Authority ("EPA") as required by Section 81 of the Act, and on receipt of a response from the EPA indicating that the amendment is not subject to formal environmental assessment, be advertised for a period of 42 days in accordance with the Regulations.</p>	

COUNCIL DECISION

Background

Development Contribution Plan 13 ("DCP13") was included in City of Cockburn Town Planning Scheme No. 3 ("Scheme") via Amendment No. 81, gazetted in August 2011 and relates to community infrastructure.

Community infrastructure is the land, structures and facilities which help communities and neighbourhoods function effectively. This includes facilities such as sporting and recreational facilities,

community centres, child care and after care centres, libraries and cultural facilities. They are often highly valued by their communities and add greatly to the overall quality of life by providing opportunities for physical activity and social interaction.

It is widely accepted that the use of community facilities has a direct correlation to the number of people using them. This is clear in the intent and basis of the relevant State Planning Policy 3.6 Development Contributions for Infrastructure as well as the City's DCP13.

What has also become clear with some applicants, being quite a complicated matter, is that they require some further clarity in the wording of Council's Scheme to ensure they understand the intent behind DCP13 and their cost contribution liability is as clear to them as possible. This is the purpose of this amendment.

Submission

N/A

Report

To introduce clarity to the Scheme wording, in particular the methodology described for DCP13 within Schedule 12, some additional words are recommended. These are minor in nature, and do not affect the statutory application of DCP 13 - rather it is to ensure that applicant's understand their obligations clearly in respect of DCP 13.

The first insertion at paragraph two ensures even if the sentence is read in isolation, rather than the context of the whole paragraph, it is clear either lots or dwellings are used in the calculation of the cost contribution liability.

The insertion at the end of the table section clarifies the use of the term 'lot' is inclusive of strata and green title allotments. This again is consistent with the intent of both the SPP and Section 6.3 of the Scheme which discusses development contributions more broadly. It also rectifies a potential concern with the definition of the term 'lot' within Schedule 1 of the Scheme.

It is recommended that Council initiate the amendment, as a way of ensuring that the Scheme is as clear as possible for applicants reading the Scheme and ascertaining their obligations in respect of land use and development within the City.

Strategic Plan/Policy Implications

Demographic Planning

- To ensure the planning of the City is based on an approach that has the potential to achieve high levels of convenience and prosperity for its citizens.
- To ensure development will enhance the levels of amenity currently enjoyed by the community.

Infrastructure Development

- To construct and maintain community facilities that meet community needs.

Budget/Financial Implications

N/A

Legal Implications

These will provide additional clarity for applicants, developers and landowners and their representatives.

Community Consultation

In accordance with the *Town Planning Regulations 1967* consultation is to be undertaken subsequent to the local government adopting the Scheme Amendment and the Environmental Protection Authority (EPA) advising that the proposal is environmentally acceptable. This requires the amendment to be advertised for a minimum of 42 days.

Attachment(s)

N/A

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

14.3 (OCM 13/09/2012) - CLOSURE OF PORTION OF ROAD LOCATION: MARABOO WHARF, NORTH COOGEE OWNER: PORT CATHERINE DEVELOPMENTS PTY LTD APPLICANT: TAYLOR BURRELL BARNETT (6004074) (L GATT) (ATTACH)

RECOMMENDATION

That Council

- (1) requests the Minister for Lands to close a small triangular portion of Maraboo Wharf, North Coogee in accordance with Section 58 of the *Land Administration Act 1997*;
- (2) supports the land resulting from the road closure being purchased by the adjoining landowner (Port Catherine Developments Pty Ltd) as per the normal procedures of the *Land Administration Act 1997*; and
- (3) advise the applicant of this decision accordingly.

COUNCIL DECISION

Background

A request has been received on behalf of the adjoining landowner to close a small triangular portion of Maraboo Wharf, North Coogee which will be amalgamated with the development to the south to provide an additional site area to accommodate a proposed marine services building and cafe/restaurant. The purpose of this report is to consider this request

Submission

By way of letter, Taylor Burrell Barnett requested that the City initiate the closure of the portion of Maraboo Wharf, North Coogee (refer to Attachment 1).

The subject area is a 41m² triangular portion of the road reserve which will be amalgamated with the adjoining development to the south to provide an additional site area to accommodate a proposed marine services building and cafe/restaurant (refer to Attachment 2).

The proponent has agreed in writing to meet all costs associated with the proposed road closure, a copy of which is provided within Attachment 3.

Report

Maraboo Wharf is within the Port Coogee development and was approved via a previous conditional subdivision – only a portion of the approved road has been constructed.

The City advertised the road closure in the local newspaper on 24 July 2012 and no submissions have been received. It represents a very minor proposal, effectively seeking to rationalise a small piece of the road reserve to enable efficient development of land to take place. All service providers have been contacted and there were no objections to the closure.

An application for subdivision has recently been submitted to the Western Australian Planning Commission (WAPC) proposing to create Lots 1 and 2 via the subdivision of Lot 9096 including the small portion of closed road. A copy of the sketch from this application, which provides details of how the road is proposed to be configured, is at Attachment 4.

It is recommended that Council support the request, and write to the Minister for Lands requesting formal closure of the portion of Maraboo Wharf in accordance with Section 58 of the *Land Administration Act 1997*.

Strategic Plan/Policy Implications

Demographic Planning

- To ensure the planning of the City is based on an approach that has the potential to achieve high levels of convenience and prosperity for its citizens.
- To ensure development will enhance the levels of amenity currently enjoyed by the community.

Governance Excellence

- To conduct Council business in open public forums and to manage Council affairs by employing publicly accountable practices.

Budget/Financial Implications

N/A

Legal Implications

To be undertaken in accordance with Section 58 of the *Land Administration Act 1997*.

Community Consultation

Advertised on 24 July 2012, in accordance with Section 58 of the *Land Administration Act 1997*. No objections have been received.

Attachment(s)

1. Letter of request from Taylor Burrell Barnett
2. Location Plan
3. Letter confirming Port Catherine Developments Pty Ltd will pay all associated costs.

Advice to Proponent(s)/Submissioners

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 13 September 2012 Ordinary Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

**14.4 (OCM 13/09/2012) - PROPOSED STRUCTURE PLAN LOCATION:
LOT 39 GAEBLER ROAD, HAMMOND PARK OWNER: NIKOLA AND
PAMELA BASKOVICH APPLICANT: GRAY & LEWIS LANDUSE
PLANNERS (SM/M/063) (R SERVENTY) (ATTACH)**

RECOMMENDATION

That Council:

- (1) adopts the Schedule of Submissions;
- (2) adopts the Structure Plan for Lot 39 Gaebler Road, Hammond Park prepared by Grey & Lewis Landuse Planners as shown within Attachment 2, subject to the below modification:
 1. an annotation being added to the Structure Plan requiring a Detailed Area Plan to be approved by the City prior to subdivision approval over the two proposed lots, north of Weetman Street and immediately adjacent to Lot 42 Frankland Avenue, Hammond Park in order to ensure they adequately address the future POS on Lot 42 Frankland Avenue.
- (3) advise the Western Australian Planning Commission, the proponent and submissioners of Council's decision accordingly;
- (4) advise the proponent of the Structure Plan that Council has currently re-advertised Amendment No. 28, which will introduce developer contribution arrangements for infrastructure within the Hammond Park and Wattleup Development Areas. Landowners subdividing to create residential allotments will be required to make contributions in accordance with Amendment No. 28 once it becomes operational; and
- (5) advise the proponent that Development Contribution Area 13 - Community Infrastructure is now in operation under the Scheme. Landowners subdividing to create residential allotments and/or developing grouped/multiple dwellings will therefore be required to make contributions in accordance with the development contribution plan requirements.

COUNCIL DECISION

Background

The Structure Plan for Lot 39 Gaebler Road, Hammond Park has been submitted to the City on behalf of the landowner to guide future residential subdivision.

The Proposed Structure Plan is effectively an extension of the urban development occurring immediately east and south of the subject site as part of the 'Hammond Rise Estate' and also the approved future residential development to the south east of the subject site. This development is occurring in accordance with the following Structure Plans:

	Council Adoption Date	WAPC Adoption Date
Hammond Park Local Structure Plan	8 November 2007	4 March 2010
Lots 43 & 44 Frankland Avenue Structure Plan	10 November 2011	3 February 2012
Lot 40 Gaebler Road Local Structure Plan	14 November 2011	7 March 2012

The subject site as shown in Attachment 1 is located on the southern side of Gaebler Road, between Barfield Road and Frankland Avenue, Hammond Park.

Submission

The Proposed Structure Plan has been submitted by Grey & Lewis Landuse Planners on behalf of Nikola and Pamela Baskovich.

Report

The subject site is zoned 'Urban' under the Metropolitan Region Scheme ("MRS") and 'Development (DA9)' under City of Cockburn Town Planning Scheme No. 3 ("Scheme"). Pursuant to Clause 6.2.4 and Schedule 11 of the Scheme, a structure plan is required to be prepared and adopted prior to any subdivision and development of land within DA9 occurring.

The Proposed Structure Plan (shown in Attachment 2) will facilitate the development of the majority of this land for residential purposes at a density of R25, with the remainder of the land being identified for public open space and road reserve. The Structure Plan will result in the creation of 20 front-loaded lots.

Southern Suburbs District Structure Plan – Stage 3

The subject site is located within the Southern Suburbs District Structure Plan – Stage 3 ("SSDSP3") area as shown in Attachment 3. The SSDSP3 identifies that this site should meet an urban density

target of 12 dwelling units per hectare. The Proposed Structure Plan has a density of approximately 16 dwelling units per net hectare and therefore exceeds the targets set by the SSDSP3 however it should be noted that the SSDSP3 was adopted in 2005 and has moderate targets in comparison to recent similar documents. SSDSP3 is in the final stages of review, with a separate report on this agenda dealing with the review outcome.

This Proposed Structure Plan is consistent with the reviewed SSDSP3 document.

Public Open Space

The SSDSP3 requires all landowners to provide their 10% public open space ("POS") requirement within their own landholding. A 1,201m² area of POS is proposed as part of the Structure Plan, which accounts for 10% of the Development Area. The POS is located immediately abutting POS created as part of the 'Hammond Rise Estate', thus creating a square park, just under 3,000m² in area.

Two of the residential lots abutting Lot 42 Frankland Avenue, Hammond Park, north of Weetman Street, are highly likely to abut POS containing a Conservation Category Wetland Buffer once the Lot 42 Frankland Avenue is structure planned and developed. Therefore, to ensure that the future dwellings on these lots appropriately address the POS it is recommended that an annotation be added to the Structure Plan requiring a Detailed Area Plan to be approved by the City prior to subdivision approval.

It should also be noted that this Proposed Structure Plan represents a Stage 1 proposal for the subject site. A further Structure Plan will be submitted into the future, and will need to specifically respond to the constraints posed by the adjoining Conservation Category Wetland.

Local Water Management Strategy

Generally a Structure Plan should be supported by an approved Local Water Management Strategy ("LWMS"). However, the Department of Water has previously advised the City of Cockburn that due to the size of the site, a full LWMS would not be required to support the Structure Plan. It was concluded that supporting information would still need to be provided in the Structure Plan to demonstrate that stormwater can be managed in accordance with best practice. The supporting document provided within the Structure Plan was deemed satisfactory to the Department of Water as noted in correspondence dated 15 June 2012.

Consultation

The Proposed Structure Plan was referred to the WAPC for comment in accordance with Clause 6.2.7.2 of the Scheme as it proposes the subdivision of land. No comments were received from the WAPC and as such the City proceeded to advertise the proposal for public comment.

The Structure Plan was advertised for public comment for a period of 21 days ending on 7 August 2012 in accordance with the Scheme requirements. Four submissions were received from government authorities and servicing agencies and one submission from an adjoining landowner. All submissions were supportive of the Proposed Structure Plan. The submissions that were received are set out and addressed in detail within the Schedule of Submissions (Attachment 4).

Conclusion

The Proposed Structure Plan is consistent with the SSDSP Stage 3 and surrounding residential development. The design of the Structure Plan conforms to Liveable Neighbourhoods principles and integrates with the adjacent road network and lot layout in a logical manner. It is therefore recommended that Council adopt the Structure Plan subject to an annotation being added requiring a Detailed Area Plan to be approved by the City over the two proposed lots, north of Weetman Street and immediately adjacent to Lot 42 Frankland Avenue, Hammond Park. The Detailed Area Plan will require they are designed to address the future POS on Lot 42 Frankland Avenue.

Strategic Plan/Policy Implications

Demographic Planning

- To ensure the planning of the City is based on an approach that has the potential to achieve high levels of convenience and prosperity for its citizens.
- To ensure development will enhance the levels of amenity currently enjoyed by the community.

The Planning Policies which apply to this item are:

SPD4 'Liveable Neighbourhoods'
APD4 'Public Open Space'

Budget/Financial Implications

N/A

Legal Implications

N/A

Community Consultation

The Proposed Structure Plan was open for community consultation for a period of 21 day ending on 7 August 2012. The proposed Structure Plan was advertised in the newspaper. Five submissions were received and all were supportive.

Attachment(s)

1. Site Location
2. Proposed Lot 39 Gaebler Road Local Structure Plan
3. Southern Suburbs District Structure Plan – Stage 3
4. Schedule of Submissions

Advice to Proponent(s)/Applicant

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 13 September 2012 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil

14.5 (OCM 13/09/2012) - ACQUISITION PORTIONS OF LOT 6 AND 50 HAMMOND ROAD, COCKBURN CENTRAL AND ASSOCIATED ROAD CLOSURES FOR THE CREATION OF RE-ALIGNED HAMMOND ROAD / BEELIAR DRIVE INTERSECTION - OWNER CITY OF COCKBURN -- APPLICANT CITY OF COCKBURN (450012) (K SIM) (ATTACH)

RECOMMENDATION

That Council

- (1) acquire 4430 square metres of Lot 6 Hammond Road for a purchase price of \$1,478,400 exclusive of GST;
- (2) acquire 535 square metres of Lot 50 Hammond Road for purchase price of \$117,000 exclusive of GST;
- (3) meet all costs associated with the surveying and application for new certificates of title for 1 and 2 above;
- (4) dedicate portions of Lots 6, 50 and 77 as road reserve pursuant to Section 56 of the *Land administration Act 1997*;
- (5) request that the Minister for Lands close the redundant road reserve portions of Hammond Road and include the land into adjoining Lots 146 and 147 pursuant to Section 58 of the *Land Administration Act 1997*, subject to there being no objection at the conclusion of the statutory 35 day advertising period; and
- (6) indemnify the Minister for Lands against reasonable costs incurred in considering and granting the request in 5 above

COUNCIL DECISION

Background

The intersection of Hammond Road and Beeliar Avenue, Cockburn Central is in the form of two staggered 'T' intersections, approximately 140 metres apart. This layout, created in 1993 was at the time considered an appropriate layout to deal with the then reasonably low traffic volumes. However with the advent of time and major

development, the intersection no longer functions effectively and requires redevelopment in order to address known safety and congestion problems.

The intersection has one of the highest frequencies of reported crashes in the City, with 115 crashes reported to have occurred at the intersection in the 5-year period to the end of 2011. The majority of those crashes were rear-end crashes involving vehicles turning left out of both legs of Hammond Road, which can be a difficult movement because of the limited availability of safe gaps in traffic approaching along Beeliar Drive during busy periods. This is made more difficult when motorists turning left out of one leg of Hammond Road want to immediately cross the 2 through traffic lanes to turn right into the other leg of Hammond Road.

To improve safety at the intersection a Black Spot Program fund grant has been approved to combine the two T-intersections as a four-way intersection by realigning the northern section of Hammond Road, and installing traffic signals there to control traffic movements. This will dramatically improve safety at the intersection by enabling motorists to negotiate the intersection under signal control. This does however require the purchasing of privately owned land, in order to affect the new design.

The purpose of this report is to consider the required land for purchase.

Submission

Valuation reports have been prepared by Licensed Valuer McGees Property on behalf of the City and from Licensed Valuer Glenn Miller Property Consultants on behalf of Walter Lenz, the owner of Lot 6 Hammond Road.

Report

An initial plan was prepared by the City's Engineering Department depicting how the road pavement in Hammond Road could be diverted through Lots 50, 6 and 77 to create a new four way intersection at Beeliar Drive. From this initial plan the bounds of the land required for the new road reserve was drawn. The plan also determined the extent of the existing road reserve that would become redundant.

The land requirement plan was sent to the owners of Lots 6 and 50 Hammond Road for comment. Both owners requested minor modification to facilitate access to future commercial projects that are either planned or envisaged on their respective properties.

The area of concern centred on how a common access point could be achieved for the benefit of each of the properties.

A modified plan incorporating the owner's requests plus further refinement by the Engineering department has now been finalized. The plan addresses the difficult road geometry and drainage requirements of the deviation.

There is a minor land requirement (194 square metres) from the western most portion of Lot 77 Beeliar Drive. The owner of Lot 77 Beeliar Drive was also sent a copy of the initial plan. The area required for the new road has not changed with the most recent plan. The owner of Lot 77 has in the mean time made application for a fuel outlet / convenience store on the site. The plan attached to that application acknowledges the location of the future road widening.

Surveying and amendments to the title of Lot 77 Beeliar Drive will be dealt with in the same manner as those to create new boundaries and balance titles for Lots 6 and 50 Hammond Road.

The owner of Lot 6 requested that the City of Cockburn pay for an independent valuation by a licensed valuer of his choosing. Glenn Miller Licensed Valuer as the Licensed Valuer nominated by the owner of Lot 6 has provided a valuation that reflects the requirements of a compulsory acquisition pursuant to the *Land Administration Act 1997*.

The report prepared by Licensed Valuer Wayne Srhoy for the City and that prepared by Glenndin Miller for the owner of Lot 6 differ in that the rate per square metre for the road land has been determined to be \$260 by Wayne Srhoy and \$300 per square metre by Glenndin Miller.

The report by Glenndin Miller included an additional 10% solatium and an amount of \$15,000 being severance damage pursuant to section 241(7) (a) of the *Land Administration Act 1997*.

The report prepared by Glenndin Miller is dated June 2012 whereas the report by Wayne Srhoy is dated January 2012. This fact could account for the difference in the square metre rate. Discussions with the owner of Lot 6 left officers in no doubt that he would only accept payment that reflected that determined by his Licensed Valuer.

It was deemed to be expedient to accept the valuation by the owner's Licensed Valuer in order to commence construction in 2012. This is considered to only feasible way to affect this required road upgrade poste haste.

The owner of Lot 6 has a long standing approval to construct a tavern on Lot 6. Due to funding uncertainties for this project the City has not

been able to give the owner of Lot 6 a clear undertaking as to when the project was to commence. The owner has delayed his development and agreed to modify the plans to show the tavern development adjacent to the re-aligned Hammond Road. Given the owner of Lot 6 has suffered delays and there being no guarantee that further negotiation and possible legal action would reduce the purchase price for the land, it is deemed to be expedient to accept the valuation by the owner's Licensed Valuer. Construction is due to commence in the latter half of 2012.

Pursuant to provisions of the *Land Administration Act 1997* the road closures were advertised on 13 August 2012 in the *West Australian*. To date there have been no objections to the proposal.

Following Council's resolution seeking closure of the two portions of road reserve a request will be forwarded to the Minister for Lands via the Department for Regional Development and Lands (DRL). In order for the closed road land to be included into Lots 146 and 147 (land owned in freehold by the City) the land will need to be purchased from the State of Western Australia by the City of Cockburn. This transaction will be concurrent with the dedication of the land acquired from Lots 6, 50 and 77 to road purposes.

Depending on land valuations undertaken by DRL for both the closed road portions and the land to be dedicated the cost of the closed road land is expected to be a nominal amount.

Although the future consolidated portion of land comprising the closed road portions and Lots 146 and 147 will have an area of approximately 4,500 square metres it has an irregular shape and is encumbered by stormwater sumps that cater for run-off from existing and new road pavements. There is scope in the future to enhance this site. This land has been included into Scheme Amendment 90 which on gazettal will render the land as a Development Zone.

Strategic Plan/Policy Implications

Demographic Planning

- To ensure the planning of the City is based on an approach that has the potential to achieve high levels of convenience and prosperity for its citizens.

Transport Optimisation

- To construct and maintain roads which are convenient and safe for vehicles, cyclists and pedestrians.

Budget/Financial Implications

The Municipal Budget contains \$2.394m for purchase of the land and construction on the intersection. The funds for this project come from municipal fund - \$1.074m and Land Development Reserve - \$1.32m. As part of the land transaction, the City estimates an amount of land will become available for development and sale post construction. The estimate at this stage is still yet to be quantified as the final design of the intersection has not been completed nor the exact amount of the surplus land.

Legal Implications

Provisions of the Land Administration Act 1997 refer.

Community Consultation

Section 58 of the *Land Administration Act 1997* require proposed road closures to be advertised in a newspaper circulating in the district and for a 35 day period for the receipt of objection.

Attachment(s)

1. A plan of the proposed land acquisition and road closures has been attached
2. Road upgrade plan

Advice to Proponent(s)/Submissioners

The Proponent(s) have been advised that this matter is to be considered at the 13 September 2012 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

14.6 (OCM 13/09/2012) - PROPOSED STRUCTURE PLAN - LOTS 3 AND 4 WEST CHURCHILL AVENUE, MUNSTER - OWNER: ESTATE OF EUNICE SANTICH - APPLICANT: DYKSTRA PLANNING (SM/M/069) (C HOSSEN) (ATTACH)

RECOMMENDATION

That Council

- (1) in pursuance of Clause 6.2.9.1 of City of Cockburn Town Planning Scheme No. 3 ("Scheme") adopts the Structure Plan for Lots 4 and 4 West Churchill Avenue, Munster;
- (2) subject to compliance with (1) above, in pursuance of Clause 6.2.10.1 of the Scheme, the Structure Plan be sent to the Western Australian Planning Commission for endorsement;
- (3) endorse the Schedule of Submissions prepared in respect to the Structure Plan;
- (4) advise the landowners within the Structure Plan area and those who made a submission of Council's decision accordingly;
- (5) advise the proponent that Development Contribution Area 13 - Community Infrastructure is now in operation under the Scheme. Landowners subdividing to create residential allotments and/or developing grouped/multiple dwellings will therefore be required to make contributions in accordance with the development contribution plan requirements; and
- (6) write to the Department of Health advising of the decision of Council.

COUNCIL DECISION

Background

The purpose of this report is to consider for adoption the Proposed Structure Plan for Lots 3 and 4 West Churchill Avenue, Munster ("subject land"). The Proposed Structure Plan seeks to provide for

urban development of the subject land, comprising a range of densities and reservations as well as the associated structural elements to facilitate an urban outcome.

The Proposed Structure Plan has been advertised for public comment and also referred to authorities for comment. This report now seeks to specifically consider the Proposed Structure Plan for adoption, in light of the advertising process and assessment by officers.

Submission

Dykstra Planning has lodged the proposal for the subject land.

Report

Planning Background

The subject land area is 2.1875 hectares in size and generally bound by the existing road network, the existing residential development to the east and a portion of the Woodman Point Waste Water Treatment Plant ("WPWWTP") Buffer. Attachment 2 contains a location plan.

The subject area is zoned 'Urban' and Urban Deferred' under the Metropolitan Region Scheme ("MRS") and 'Development' under City of Cockburn Town Planning Scheme No. 3 ("Scheme"). The subject land is also located within Development Contribution Area No. 6 ("DCA 6") and No. 13 ("DCA 13").

The portion of Lot 4 West Churchill Avenue affected by the WPWWTP Buffer has been excluded from the subject land.

Pursuant to Clause 6.2.4 and Schedule 11 of the Scheme, a structure plan is required to be prepared and adopted to guide future subdivision and development.

Proposed Structure Plan

The Proposed Structure Plan as shown in Attachment 3 provides for residential development, with an area of Public Open Space ("POS") and an associated road network. Based on the design, approximately 27 single residential lots and 4 grouped/multiple housing sites will eventuate.

The Proposed Structure Plan satisfies density objectives, POS requirements and provides a suitable road network.

Residential Density

Proposed densities allow for the provision of variety in lot sizes and dwelling types and are conducive to the density found in the surrounding residential developments. Densities range from R25 to R60; with R25 being the predominant zoning.

Directions 2031 and Beyond (“Directions 2031”) and Liveable Neighbourhoods (“LN”) promote 15 dwellings per gross hectare as the 'standard' density for new greenfield development in urban areas.

Medium and higher residential densities are generally considered to be appropriate adjacent to areas of POS as these areas offer a high level of amenity and convenience. Medium densities also provide increased opportunity for passive surveillance and capitalise on the amenity values of such open space areas.

Lots fronting Fiore Court are coded R25. The base coding of R25 will, considering the lot sizes and width, allow for orderly residential development that is consistent with the existing dwellings on Fiore Court. The allowance for a R40 density bonus triggered by the amalgamation of lots was removed post advertising following concerns raised by existing residents of Fiore Court.

An area of R60 zoned land, being approximately 2,173m² in size, is identified in the south west portion of the subject site. This area and how it will interface with the WPWWTP Buffer will be discussed in detail later in this report. Land coded R60 will be required to be developed at a minimum of 2 stories and require a Detailed Area Plan to ensure that an optimal built form outcome is achieved.

An area of R40 zoned land is identified adjoining the area of POS, as per Element 3 and Element 4 of Liveable Neighbourhoods. The inclusion of higher densities around areas of POS encourages overlooking and safety. A Detailed Area Plan will be required over all land zoned R40 to ensure that such issues are addressed to the satisfaction of the City.

In general the lot yield and proposed densities are consistent with those outlined in Directions 2031 and the Outer Metropolitan Perth and Peel Sub-Regional Strategy.

Public Open Space

The Structure Plan provides for a total of 2,228m² of POS, approximately 10% of the overall area. A small drainage swale, of approximately 179m², or 0.8% of the subject area, is proposed within the area of POS.

An indicative footpath is shown linking the extension of Gicha Close and West Churchill Avenue through the proposed area of POS. This connection is deemed important to the wider pedestrian connectivity of the area. The exact location of the path through the area of POS will be determined at a later stage.

The provision of POS in the Proposed Structure Plan is in keeping with the requirements of Liveable Neighbourhoods.

Interface with land affected by the WPWWP Buffer

A portion of both lots within the subject site are affected by the WPWWTP Buffer. The Buffer as defined in 1996 extends between 1.65km and 1.8km to the east across Lake Coogee. Council has previously advised of its aspiration for the operations of the WPWWTP to be improved so that the extent of the buffer can be reduced to (at most) the eastern foreshore of Lake Coogee. The WAPC, Department of Planning, Minister for Planning and Environmental Protection Authority have all formally responded to the City advising of the current status of the WPWWTP Buffer and decision making in respect of its future. Copies of this correspondence is provided in Attachment 5.

This Proposed Structure Plan is consistent with the statutory application of the buffer as it currently exists. This is on the basis of not providing for residential development within the WPWWTP Buffer.

Land broadly between West Churchill Avenue and the extension of Fiore Court, which is affected by the Buffer, has been indicated as being for POS. The inclusion of POS within the WPWWTP Buffer and Kwinana Air Quality Buffer has previously been approved by Council.

Land in the southern portion of Lot 4 West Churchill Avenue is excluded from the Proposed Structure Plan area. However, this excluded land must form part of a lot not wholly contained within the WPWWTP Buffer. A serif arrangement for these lots is annotated on the Structure Plan map. Council will not support the creation of a whole lot contained within the Buffer and any future subdivision of this land must accurately reflect the intention of the Structure Plan. The creation of lots wholly contained within the WPWWTP Buffer is not an acceptable practice to be contemplating given the issues currently associated with the WPWWTP Buffer.

The Proposed Structure Plan indicates that this requirement of Council has been met. The lots shown as R60 include the portion of land inside the buffer which must stay connected to the R60 lots as per the requirements of the Structure Plan.

The Proposed Structure Plan outlines possible uses for this land. These being uses which do not constitute habitable living such as vehicle access, car parking, storage space and collection of refuse, all associated with the future complex that may be built on the R60 coded portion of land. The full utilisation of this land by such uses would lessen the likelihood that the area would fall into disrepair and lowering the amenity of the wider neighbourhood. Additional requirements that the R60 coded lots be developed to at least 2 stories will also contribute to this land being utilised at a higher level. A Detailed Area Plan will be required for the entire site prior to development.

The provision of a Detailed Area Plan for this site is deemed a high importance to ensure that any future R60 development appropriately addresses the issue of the buffer and also the existing lower density residential land surrounding it.

Access

The movement network of the Proposed Structure Plan is broadly an extension of the existing road network. Gicha Close will be extended westward approximately 130m ending in a cul-de-sac. No changes are proposed to Fiore Court and West Churchill Avenue.

The proposed extension of Gicha Close will take the distance between Syndicate Link and the cul-de-sac to approximately 190m with 20 single residential lots and two grouped lots gaining access of the extended road.

The transport impact assessment provided as an attachment to the Proposed Structure Plan notes that although the length of the cul-de-sac is beyond the desirable length, the expected traffic volumes are below those seen as desirable for a cul-de-sac arrangement.

The design of the Proposed Structure Plan is not envisaged to lead to an adverse outcome in respect of traffic generation.

Community Consultation

The Proposed Structure Plan was advertised for public comment from 26 June 2012 to 17 July 2012. The Proposed Structure Plan was advertised to nearby and affected landowners published in the Cockburn Gazette for 21 days and also referred to relevant government authorities.

In total 10 submissions were received for the Proposed Structure Plan, including:

- 3 from adjoining landowners.
- 1 from the registered proprietor of the subject land.
- 6 from government agencies.

Two submissions from adjoining landowners raised concerns regarding the nature of the proposed density along the northern side of Fiore Court. Specifically, the potential for R40 type development to occur through the amalgamation of the lots as shown on the advertised Structure Plan. Concerns went to amenity, car parking, traffic and noise. The Structure Plan has thus been amended to remove the split coded R25/40 from lots fronting Fiore Court. The lots are now shown as R25 with the potential for only one dwelling on each lot. This is considered to address the issues raised in submissions.

A submission was received from the Department of Health objecting to the Proposed Structure Plan, specifically their concerns went to:

1. Proximity of residential development to the WPWWTP Buffer.
2. Proximity of residential development to operating market gardens.
3. General health implications of 1 and 2 above.

The Department of Health's submission does not reflect how the Department of Planning and WAPC have viewed and applied decision making in respect of the WPWWTP Buffer. The buffer currently exists as a line which residential development is not permitted to take place within. To now start viewing residential development outside the buffer as not appropriate, would create an unworkable and improper application of the planning process and Scheme, and would not constitute orderly and proper planning. It needs to also be emphasised that residential development already exists up to the extent of the buffer. To therefore consider this Structure Plan as inappropriate (in line with the view of the Department of Health) would clearly raise questions as to the appropriateness of all other residential development in the Munster area outside the buffer which has been created recently.

Previous advice on the buffer is that an expansion of the boundary is not expected, nor would this be contemplated for any support by the City. The view of the Council is to aspire to have the buffer restricted to the eastern foreshore of Lake Coogee.

Therefore as the subject land of the Structure Plan is zoned 'Urban' under the MRS and the development proposed is entirely consistent with that zoning, it is considered appropriate. This is consistent with the views taken previously by the Department of Planning and WAPC in permitting subdivision for residential development throughout the Munster locality up to (but not within) the buffer.

The advice from the Department of Health appears to be inconsistent with all other advice given. Council and the WAPC have approved residential development, including medium density development, directly adjacent to the WPWWTP Buffer.

An additional item has been included in the officer recommendation for officers to write to the Department of Health to request an explanation to clarify their position on this matter.

Guidance on how to deal with issues arising from the proximity of residential development to operating market gardens is outlined in the WAPC's Planning Bulletin 63 Section 5.0 – Policy Measures which notes a number of options available to decision makers for dealing with proposed residential development on 'Urban' zoned land adjoining operating market gardens. The Department of Health's submission states that no residential development should occur within 300m of an operating market garden. As per Planning Bulletin 63 such a requirement only applies where residential development occurs on land zoned 'Rural' under the MRS.

The objections from the Department of Health are therefore noted but not supported.

All of the submissions that were received are set out and addressed in the Schedule of Submissions (Attachment 4).

Conclusion

It is recommended that Council adopt the Structure Plan for Lots 3 and 4 West Churchill Avenue, Munster and pursuant to Clause 9.2.10 of the Scheme refer it to the Western Australian Planning Commission for their endorsement.

Strategic Plan/Policy Implications

Demographic Planning

- To ensure the planning of the City is based on an approach that has the potential to achieve high levels of convenience and prosperity for its citizens.
- To ensure development will enhance the levels of amenity currently enjoyed by the community.

Budget/Financial Implications

N/A

Legal Implications

Clause 6.2.9.1 of the Scheme requires Council to make a decision on the application within 60 days from the end of the advertising period of such longer period as may be agreed by the applicant. The advertising period concluded on 17 July 2012.

Community Consultation

In accordance with Clause 6.2.8 of the City's Scheme, public consultation was undertaken from 26 June 2012 to 17 July 2012. This included a notice in the Cockburn Gazette, letters to landowners within the Structure Plan area, adjoining landowners and State Government agencies.

Analysis of the submissions has been undertaken within the 'Report' section above, as well as the attached Schedule of Submissions (Attachment 4).

Attachment(s)

1. Location and Context Plan
2. Aerial Locality Plan
3. Proposed Local Structure Plan
4. Schedule of Submissions
5. Letters from Minister for Planning; Department of Planning; Office of the EPA

Advice to Proponent(s)/Submissioners

The proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 13 September Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

14.7 (OCM 13/09/2012) - PURCHASE OF PORTION OF LOT 58 ERPINGHAM ROAD, HAMILTON HILL AND DEDICATION OF PUBLIC ACCESS WAY LAND AS ROAD PURSUANT TO SECTION 56 OF THE LAND ADMINISTRATION ACT 1997 LOCATION: LOT 65 ERPINGHAM ROAD, HAMILTON HILL OWNER / APPLICANT: CITY OF COCKBURN (2200882) (LGATT) (ATTACH)

RECOMMENDATION

That Council

- (1) delegate authority to the CEO to negotiate the purchase of the small portion of Department of Housing's Lot 58 Erpingham Road, Hamilton Hill (being approximately 306sqm), for the purpose of creating legal access to the City's land at Lot 65 Erpingham Road, Hamilton Hill;
- (2) request that the Minister for Regional Development and Lands dedicate pedestrian access way Lot 155 on Plan 11049 as Road Reserve pursuant to Section 56(1) of the *Land Administration Act 1997*;
- (3) indemnify the Minister for Regional Development and Lands against reasonable costs incurred in considering and granting this request; and
- (4) note the planned subdivision design for Lot 65 Erpingham Road and actions being taken to proceed with the development of the land.

COUNCIL DECISION

Background

By way of Amendments No. 38 and 76 to City of Cockburn Town Planning Scheme No. 3, the City previously supported the rezoning of the following land parcels:

- Lot 65 Erpingham Road from 'Parks and Recreation Reserve' to 'Residential' with a density code of R40;

- The pedestrian access way running north-south between Lots 65, 460 and 58 Erpingham Road from 'Local Road' to 'Residential' with a density code of R40.

These specific rezonings had links to the Phoenix Rise Master Plan and the Phoenix Central Revitalisation Strategy. These have both created a framework to develop areas of land within the Phoenix Rise area, associated with upgrades to the public realm in partnership with the Department of Housing and Satterley Property Group.

In respect of the land development components, the City's Land Management Strategy was adopted by Council in March 2011, which programmed various landholdings for development. Lot 65 Erpingham Road, Hamilton Hill is identified for development this financial year (2012/13) and Council's budget has made available funds to achieve the development of the subject land.

The purpose of this report is to affect statutory decisions of Council to enable the development to begin.

Submission

N/A

Report

Lot 65 Erpingham Road, Hamilton Hill was rezoned to provide for residential development under the Scheme. A location plan is provided at Attachment 1.

The development and subdivision of this land forms a key aspect of the Phoenix Rise Master Plan. The Phoenix Rise Master Plan was prepared and advertised in 2005, and adopted in 2006 in which the subsequent zoning changes were based. The Master Plan incorporates a number of primary elements that focus on achieving the key objectives of reducing crime, increasing safety and revitalisation of the area overall. A copy of the Master Plan and summary is attached in Attachment 2.

The City's Land Management Strategy 2011 – 2016 identifies Lot 65 Erpingham Road, Hamilton Hill as the next land parcel for development. This has been supported through the current financial year budget for Council to enable development to proceed.

The City's officers have been progressing the development for the past few months in line with the endorsed program, involving negotiating with BP Australia for an informal road access across the BP pipeline for the City's rubbish trucks to be able to access/egress from the future

subdivision of Lot 65 Erpingham Road. Also locating the pipeline for the sewer design and surveying the land to create the future subdivision plans. The propose subdivision plan is provided in Attachment 3.

As the City's land is currently land locked it has been necessary to create legal access to the lot via Erpingham Road. City's Officers have initiated negotiations with the Department of Housing to purchase a portion of their Lot 58 Erpingham Road. When purchased the portion of the Department's land will be amalgamated with the adjoining Public Access Way (PAW) to create the legal access road to the City's future subdivision. Department of Housing have provided in principle agreement to the excision of the portion of their land, provided it does not interfere with their aged care facility.

This report seeks Council resolution to delegate authority to the CEO to negotiate the purchase of the portion of the Department's land and to support the PAW land being dedicated to road reserve which will be amalgamated with the Department's land to create the legal road access to the City's future development.

Separate to this, the City has prepared a letter to surrounding landowners advising them of the timeline and planning of the redevelopment of Lot 65 Erpingham Road. As Lot 65 currently comprises a playground and some limited remnant vegetation and trees, it is important that communication with all landowners be undertaken to make them aware of the planned redevelopment. A copy of the proposed letter is at Attachment 4.

Importantly the redevelopment seeks to affect the ongoing implementation of the Phoenix Rise Master Plan, which is aimed at a broad rejuvenation of the Phoenix Rise area. In terms of access, while a physical connection will exist between Erpingham Road and Eliza Court, this will not be permitted to operate as a public thoroughfare, instead being limited to access by residents and the City's rubbish trucks servicing the area.

Strategic Plan/Policy Implications

Demographic Planning

- To ensure the planning of the City is based on an approach that has the potential to achieve high levels of convenience and prosperity for its citizens.
- To ensure development will enhance the levels of amenity currently enjoyed by the community.

Governance Excellence

- To conduct Council business in open public forums and to manage Council affairs by employing publicly accountable practices.

Budget/Financial Implications

Budget requirements for the planned redevelopment of Lot 65 Erpingham Road are provided by the current 2012/13 budget. These cover costs including land acquisition and the development of the resulting subdivision itself.

Legal Implications

Provisions of the Land Administration Act 1997 refer.

Community Consultation

Past consultation has already been undertaken with the adjoining owners regarding the proposed changes to Lot 65 Erpingham Road, which have now been formally gazetted. The City has also prepared a letter to surrounding landowners advising them of the timeline and planning of the redevelopment of Lot 65 Erpingham Road. As Lot 65 currently comprises a playground and some limited remnant vegetation and trees, it is important that communication with all landowners be undertaken to make them aware of the planned redevelopment.

Attachment(s)

1. Location Plan
2. Master Plan and Summary
3. Sketch and Subdivision Plan
4. Copy of letter sent to landowners

Advice to Proponent(s)/Applicant

N/A.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

14.8 (OCM 13/09/2012) - ADDITIONS AND ALTERATIONS TO EXISTING WAR MEMORIAL PARK - LOCATION: 38912R (LOT 594) ARMADALE ROAD, BANJUP - OWNER: CITY OF COCKBURN - APPLICANT: GRIFFITHS ARCHITECTS (5514436) (T CAPPELLUCCI) (ATTACH)

RECOMMENDATION

That Council recommends that the application be approved by the Western Australian Planning Commission (WAPC), for additions and alterations to the existing War Memorial Park at No. 38912R (Lot 594) Armadale Road, Banjup, in accordance with the attached plans and subject to the inclusion of the following conditions and advice notes:

CONDITIONS

1. The proposed new timber post and rail fencing is to be in the same material, finish, texture and colour as the existing fencing.
2. The vehicle parking area shall be sealed, kerbed, drained and line marked in accordance with the approved plans and specifications certified by a suitably qualified practicing Engineer to the satisfaction of the City.
3. The proposed vehicle crossover being constructed in accordance with the City of Cockburn specifications.
4. The existing and new trees landscaping proposed for the site must be reticulated or irrigated and maintained to the satisfaction of the City.
5. The installation of outdoor lighting is to be provided within the vicinity of the proposed granite memorial block. This is to be in accordance with the requirements of Australian Standard AS 4282-1997: 'Control of the Obtrusive of Outdoor Lighting'.
6. An additional plaque to remember the men of Banjup who served their country in wars subsequent to World War I being provided on-site, in a location and design agreed upon by the Banjup Residents Group and Cockburn RSL to the satisfaction of the City.

ADVICE NOTE

1. This is a Planning Approval only and does not remove the responsibility of the applicant/owner to comply with all relevant building, health and engineering requirements of

the City, with any requirements of the City of Cockburn Town Planning Scheme No. 3, or the requirements of any other external agency.

2. Access and facilities for disabled persons is to be provided in accordance with the requirements of the Building Code of Australia.
3. Routine maintenance does not require development approval. This includes the following:
 - (i) repainting previously painted surfaces in the same colour scheme.

If there are any questions regarding what constitutes routine maintenance, the City's planning services should be consulted.

COUNCIL DECISION

Background

The City was approached by Main Roads WA earlier this year requesting information about the Banjup Memorial Park. Their specific enquiry related to their plans for the upgrade of Armadale Road, and how this may impact on the Memorial site.

From the outset the City advised Main Roads WA of the importance of the Memorial park, and the proposals which potentially impacted on the Memorial would be of great community interest. Effectively, the City advised Main Roads WA of the need to approach this issue in a consultative manner, and as a minimum, engage suitably qualified heritage expertise to identify any impacts on the Memorial Park and determine how they could be managed.

The subject site is included on the City's Heritage List and is subsequently protected by the City's Town Planning Scheme No. 3. It has a 'Category B - Considerable significance' management classification under the City's Local Government Inventory which prescribes the following:

- *“Very important to the heritage of the locality.*
- *Conservation of the place is highly desirable.*
- *Any alterations or extensions should be sympathetic to the heritage values of the place.”*

A search of City’s records has confirmed that site was originally vested to the City by the Governor of Western Australia on 2 August 1988 under Section 33 of the then *Land Act 1933*. The purpose of the vesting is stated as being for a “War Memorial”.

As mentioned above the City received a request from Main Roads WA to modify the boundaries of the subject site, in order to address the future land requirements needed for the future widening of Armadale Road.

The City subsequently advised Main Roads WA that it will not support the excision of the land until such time that a comprehensive study is undertaken to determine the suitability or otherwise of the proposed works.

In this regard, the City advised Main Roads WA that a development application will need to be lodged with the City for approval and will need to include a detailed heritage assessment of the site. As such, given the above has now been received through a formal development application, the proposed works to the War Memorial Park are now being considered.

As the proposed development will take place on land reserved under the Metropolitan Region Scheme (MRS) for ‘Regional Reserve – Parks and Recreation’, the approval of the Western Australian Planning Commission (WAPC) is required in accordance with the Public Works Act and Planning and Development Act 2005.

Therefore, the City of Cockburn has forwarded the application to the WAPC for determination by the WAPC in accordance with Part IV – Development Division 1, Clause 29 (1) of the MRS.

It should be noted that the City considers the overall support of the Cockburn Returned and Services League Sub Branch and the wider Banjup community being of key importance. As such the input of the Returned and Services League (RSL) and the Banjup Residents Group has been sought as part of this application and as a result of the sites associated importance, has been referred to Council before a recommendation to the WAPC is provided.

Submission

N/A

Report

A search of City's records has confirmed that site was originally vested to the City by the Governor of Western Australia on 2 August 1988 under Section 33 of the then *Land Act 1933*. The purpose of the vesting is stated as being for a "War Memorial".

As mentioned above the City received a request from Main Roads WA to modify the boundaries of the subject site, in order to address the land requirements needed for the future widening of Armadale Road.

The City subsequently advised Main Roads WA that it will not support the excision of the land until such time that a comprehensive study is undertaken to determine the suitability or otherwise of the proposed works.

In this regard, the City advised Main Roads WA that a development application needed to be lodged with the City for development approval and would require a heritage assessment of the site and impacts.

As the proposed development will take place on land reserved under the Metropolitan Region Scheme (MRS) for 'Parks and Recreation', the approval of the Western Australian Planning Commission (WAPC) is required.

Further the works are not considered to fall under the Public Works act or exemptions for Public Authorities as the scope of the works are not directly related to the construction of roads by MRWA and are not being conducted by the vested authority (City of Cockburn) or in accordance with a Management Plan approved by the WAPC.

On receipt of the application the City forwarded the application to the WAPC for determination in accordance with Part IV – Development Division 1, Clause 29 (1) of the MRS.

In assessing the application the City considered the support of the Cockburn Returned and Services League Sub Branch and the wider Banjup community being of primary importance. As such the input of the Returned and Services League (RSL) and the Banjup Residents Group has been sought as part the City's assessment. Due to the site's social importance and heritage significance, the matter has been referred to Council for a recommendation to the WAPC to be provided.

As part of the application, a Heritage Impact Statement for the modifications to the park has been prepared and is an attachment to this Report (Attachment 2) given the site is included in the City of

Cockburn's Municipal Inventory 'Category B' being of considerable significance.

The proposal involves the removal of 5 metres of the memorial from the front of the lot to provide a larger distance between the widened Armadale Road and the park. In addition, the rear fence will be removed and the park extended 7 metres to the northern site boundary.

In order to mitigate the effects of the modification, a number of improvements are proposed to enhance the social and heritage values of the place. The alteration works involve the following:

1. A more substantial memorial, located towards the rear of the reserve, designed and developed in consultation with the Cockburn RSL Sub Branch.
2. A flag pole and new trees (of suitable species) to 'stand guard' of the new memorial - tree selection to be in accordance with the City's 'Grow Local Plants Guide'.
3. Safer access/egress to the site including driveway and parking area as part of the new upgraded Armadale Road design;
4. Suitable lighting to signify and highlight the prominence of the site.
5. Upgraded fencing.
6. Demonstration that any reduction in the size of the site being suitably offset by improvements to its overall aesthetic value and function.
7. Suitable seating and resting facilities.

In addressing the above, the proposed application has provided a new granite memorial block, 1.5 metres high, at the rear of the site, flanked by two (2) new trees and a new steel flagpole. The new trees are to replace the removal of the front trees which are need to be removed due to the road widening. Flooded Gums (*Eucalyptus Rudis*) have been selected from the City's "Grow Local Plants" guide as they grow to a tall height and do well in the area.

Consultation was undertaken by Griffiths Architects for MRWA with the Cockburn RSL local sub branch to ascertain their aspirations for the place in order to retain the social values associated with the Memorial. As confirmed by the Cockburn RSL when asked to provide comments on the proposal after being referred the application by the City, the RSL requested a new granite memorial block to be included as part of the new development. As such, the memorial will have the original dedication plate fixed to the new block with commemorative inscriptions to be devised by the RSL, which the RSL has confirmed.

In addition, the flagpole proposed for the rear of the site will be used by the RSL and by the community for remembrance ceremonies which

currently is not the case and will enhance the value of the place and reinforce its purpose as a memorial.

The modification of moving the memorial away from Armadale Road by approximately 5 metres is a result of ensuring the natural bush context of the place will be preserved due to the Memorial Park protruding into the Armadale Road reserve. During pre-lodgement discussions, the City of Cockburn requested the provision of safer access and egress to the site via a driveway and small parking area which is now proposed. The provision of two (2) car parking bays in the road reserve is seen as being suitable given the low intensity of use and likely need for parking at the site.

The proposed works are considered to improve the safety of and access to the site and will also enhance the memorial's visibility from Armadale Road which will result in it being a more recognisable place of remembrance.

In regard to lighting, no details have been provided as to how this will be addressed in order to highlight the prominence of the site given that the site is not powered. However, it is considered important that the site does provide lighting to highlight the prominence of the site and as such, a condition has been recommended that around the proposed new granite memorial block, lighting is to be provided.

The fencing locations are to be modified by this proposal. As a result, new fencing is proposed for the front and rear of the site. Rotting timbers will be replaced and the fence painted to match the existing colour scheme. The new fencing will match the timber post and rail fencing, and is compatible with the values of the site.

Given the modification to the site layout, improvements have been proposed to ensure the heritage value and function of the place is enhanced. As outlined earlier in this report, two (2) new trees, a new flagpole, new granite memorial block and relocation of park bench away from Armadale Road have been provided. These improvements and additions are seen as potentially enhancing the social heritage values of the place and providing necessary upgrades in order to promote the importance and use of the site.

Conclusion

Through the retention of the majority of the trees on-site, a commemorative block that is more substantial than the existing dedication plaque and the introduction of a flagpole to the rear of the lot, the overall scale of development proposed for the site is seen as enhancing the popularity and awareness of the place compared to the site currently.

The proposed additions and alterations to the site are seen as enhancing the site's significance. In addition, the Cockburn RSL, State Heritage Office and Banjup Residents Group have all indicated support of the proposed works.

In light of the above, it is considered that the proposed modification of the lot boundary and improvements proposed for the War Memorial Park is able to be supported as it is generally consistent with the provisions of the City's APD64 – Heritage Conservation Design Guidelines.

It is recommended that Council recommends the application be approved by the WAPC, subject to conditions set out in the proposed recommendation to address the above matters.

Strategic Plan/Policy Implications

Demographic Planning

- To ensure the planning of the City is based on an approach that has the potential to achieve high levels of convenience and prosperity for its citizens.
- To ensure development will enhance the levels of amenity currently enjoyed by the community.

Budget/Financial Implications

The costs will be borne by Main Roads, WA.

Legal Implications

Town Planning Scheme No. 3
Planning and Development Act 2005
State Administrative Tribunal Regulations
State Register of Heritage Places

Community Consultation

Given the City considers the overall support of the Cockburn RSL Sub Branch and the wider Banjup community, the proposed works have been referred to the Cockburn RSL Sub Branch and the Banjup Residents Group as well as the RSL WA Branch and the State Heritage Office for comment.

A copy of the schedule of submissions is detailed in attachment 5.

Attachment(s)

1. Existing and Proposed Location Plan
2. Photos of existing site
3. Heritage Impact Statement
4. Schedule of Responses from consulted Organisations

Advice to Proponent(s)/Submissioners

The Proponent and those organisations who were consulted to provide comments on the proposal have been advised that this matter is to be considered at the 13 September 2012 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

14.9 (OCM 13/09/2012) - EIGHTEEN (18) MULTIPLE DWELLINGS - LOCATION: 509L (LOT 509) CARRINGTON STREET, HAMILTON HILL - OWNER: BETTY JUNE MATTHEWS - APPLICANT: DEVELOPTIONS (2200638) (T CAPPELLUCCI) (ATTACH)

RECOMMENDATION

That Council:

- (1) grant planning approval for eighteen (18) multiple dwellings at No. 509L (Lot 509) Carrington Street, Hamilton Hill, in accordance with the attached plans and subject to the following conditions and advice notes:

CONDITIONS

1. Prior to the application for a Building Permit, revised plans are to be submitted showing the first and second floor balconies on the northern elevation, in accordance with part 7.4.1 of the Residential Design Codes with a permanent obscure material and be non-openable to a minimum of 1.6 metres above the finished floor level. A permanent obscure material does not include a self-adhesive material or other material that is easily removed.
2. Prior to the application for a Building Permit, details of a proposed security gate and fencing towards Wheeler Road and Carrington Street entrances to the site is to be provided.
3. Prior to the issue of a Building Permit, a construction

management plan is to be prepared and submitted to the satisfaction of the City. The plan should address the following items:

- a) Access to and from the site.
 - b) Delivery of materials and equipment to the site.
 - c) Storage of materials and equipment on the site.
 - d) Parking arrangements for contractors and subcontractors.
 - e) Management of construction waste.
 - f) Other matters likely to impact on the surrounding properties.
4. Prior to the issue of a Building Permit, a materials schedule shall be submitted to and approved in writing by the City. The schedule should include reference to the materials proposed and include their finish and colour. The development shall be constructed in accordance with the approved materials schedule.
 5. Outdoor lighting is to be provided to pathways, communal open space and car-parking areas, the details of which are to be shown on the plans submitted for building permit approval to the satisfaction of the City.
 6. Landscaping including verge planting is to be installed, reticulated and/or irrigated in accordance with the approved plan and maintained thereafter to the satisfaction of the City. The landscaping shall be implemented prior to the occupation of the dwellings hereby approved. Any species which fail to establish within a period of 12 months from planting shall be replaced to the satisfaction of the City.
 7. Visitor parking bays (as shown on the approved Site Plan) are to be located and constructed to the City's specifications with the cost to be fully borne by the applicant/owner and made available for use by visitors to the site at all times.
 8. All service areas (bin stores) and service related hardware, including antennae, satellite dishes, air-conditioning units etc, being suitably located away from public view and/or screened, the details of which are to be provided to the satisfaction of the City of Cockburn prior to the issue of a Building Permit.
 9. The parking bays, driveways and points of ingress and

egress to the parking bays and subject site shall be sealed, kerbed, drained and line marked in accordance with the approved plans and specifications certified by a suitably qualified practicing Engineer to the satisfaction of the City.

10. Crossovers and bin pads are to be located and constructed to the City's specifications. Copies of specifications are available from the City's Engineering Services.
11. Walls, fences and landscape areas are to be truncated within 1.5 metres of where they adjoin vehicle access points where a driveway and/or parking bay meets a public street or limited in height to 0.75 metres.
12. The premises must clearly display the street number.
13. All stormwater being contained and disposed of on-site to the satisfaction of the City.
14. During the construction phase, no activities causing noise and/or inconvenience to neighbours being carried out after 7.00pm or before 7.00am, Monday to Saturday, and not at all on Sunday or Public Holidays.
15. The development site must be connected to the reticulated sewerage system of the Water Corporation before commencement of any use.
16. Arrangements being made to the satisfaction of the Chief Executive Officer for the pro-rata subdivider contributions towards those items listed in the City of Cockburn Town Planning Scheme No. 3 for Development Contribution Area 13 – Community Infrastructure.
17. The surface finish of the boundary wall abutting the adjoining lots is to be finishes and maintained to the satisfaction of the City.
18. All storage rooms abutting proposed car parking bays are to have their doors opening inwards.
19. During the construction phase, measures to the satisfaction of the City are to be undertaken to prevent sand and or dust blowing onto adjoining properties in accordance with an approved Dust Management Plan (the subject of Condition 20)

20. Vehicle access to and from the subject site is restricted to Wheeler Road with no vehicle access between the subject site and Carrington Street being permitted.
21. Prior to occupation of the development hereby approved, the proposed eight (8) covered bicycle stands are to be provided in close proximity to the entrance of the building and thereafter maintained to the satisfaction of the City.
22. The development to comply with the provisions of the approved Waste Management Plan dated received 15 August 2012.
23. Earthworks over the site and batters must be stabilised to prevent sand or dust blowing, and appropriate measures shall be implemented within the time and in the manner directed by the City in the event that sand or dust is blown from the site.
24. Adequate refuse bins shall be provided to service the development and bins are to be screened from view to the satisfaction of the City before the development is occupied or used.
25. No earthworks shall encroach onto the Carrington Street road reserve.
26. No stormwater drainage shall be discharged onto the Carrington Street road reserve.

ADVICE NOTES

1. This is a Planning Approval only and does not remove the responsibility of the applicant/owner to comply with all relevant building, health and engineering requirements of the City, with any requirements of the City of Cockburn Town Planning Scheme No. 3, or the requirements of any other external agency.
2. In relation to Condition 4, the schedule of materials, finishes and colours must be directly related to the information and details shown in the approved perspective.
3. Condition 13 requires the on-site storage capacity be designed to contain a 1 in 20 year storm of a 5 minute duration. This is based on the requirements to contain

surface water by Building Codes of Australia.

4. Where the obligation for payment of developer contributions has been met by a previous approval, such as subdivision, condition 16 will be deemed to have been complied with.
 5. In regards to Condition No. 17, the surface finish of the boundary walls of the adjoining lots should be to the satisfaction of the adjoining landowner and to be completed as part of the building licence. In the event of a dispute the boundary wall must be constructed with a clean or rendered finish to the satisfaction of the City.
 6. Condition No. 18 has been applied in order to ensure that storage room doors do not open into car spaces as per the Australian Standards.
 7. With reference to Condition 24, external bin enclosure(s) must be and of an adequate size to contain all waste bins, at least 1.8 m high, fitted with a gate and graded to a 100mm diameter industrial floor waste with a hose cock, all connected to sewer. Further information can be obtained from the City's Environmental Health Service.
 8. As this property is in close proximity to the possible future intersection of Roe Highway and Carrington Street, it is recommended that a transport noise assessment is undertaken and any noise amelioration measures or quite house design principles recommended in this assessment be included in the final design and construction of these units.
 9. The installation of outdoor lighting (if proposed) shall be in accordance with the requirements of Australian Standard AS 4282-1997: 'Control of the Obtrusive of Outdoor Lighting'.
- (2) notify the applicant and those who made a submission of the Council's decision.

COUNCIL DECISION

Background

Zoning:	MRS: Urban
	TPS3: Mixed Business – R60
Land use:	Multiple Dwellings
Lot size:	1629m ²
Use class:	D

The subject site is located at No. 509L Carrington Street in Hamilton Hill and contains an existing older building previously used as a dance studio as well as an existing car park. The site is zoned 'Mixed Business' however as per Clause 5.8.3 (b) of the Town Planning Scheme No. 3, the residential zoning for the site is R60. The site has vehicular access from Wheeler Road and contains a fall of approximately 3 metres from east to west. The subject site is also in close proximity to several commercial developments including the 'Local Centre' and 'Mixed Business' zoned areas on the corners of Carrington Street and Rockingham Road to the south of the site.

The application was initially submitted to the City on the 22 December 2011 for seventeen (17) multiple dwellings over two-storeys. However, the City had been involved in pre-lodgement meetings with the applicant well before the application was submitted for Approval. One of the main issues resolved as part of the pre lodgement process was vehicular access. As Carrington Street is a Primary Regional Road Reserve (Category 3) the applicant was advised to contact the Department of Planning for further advice regarding suitable access arrangements from Carrington Street. The result was that the Department of Transport recommended that all vehicle access to the subject site be from Wheeler Road.

After the application was initially submitted, additional details, reports and modifications were provided before the subject development was initially put on advertising to the directly affected neighbours on the 24 April 2012. The application was advertised as a result of variations proposed to upper floor side setbacks to the northern and southern abutting properties of No. 62 Wheeler Road and No. 402 Carrington Street.

As a result of this consultation, an objection was received from the Port School site. The prominent reason for the objection was the school's concerns of the developments ground and upper floor outdoor living areas/balconies being orientated towards the north facing courtyard of the school which is the predominant meeting point of the school and as a result, substantial noise is generated from this area.

As a result of the school's objection, the applicant was informed of the subject issue and prepared amended plans accordingly to ensure the

balconies on the upper floors facing the school were screened accordingly. However, given the application was brought to the attention of Councillors by the Principal of the Port School, an on-site meeting was conducted on the 31 May 2012. In attendance at the on-site meeting were the applicants, the Principal of the Port School, representatives from the City's Statutory Planning department and the Mayor with a few Councillors.

The result of this on-site meeting was that the applicants had to revise their plans in order to reduce the prominence of outdoor living areas and balconies facing the school site.

The applicants then prepared modified plans dated stamped 20 July 2012. Given these new plans also proposed variations to the other northern property at No. 62A Wheeler Road, a meeting was organised at the City's officers on the 31 July 2012 with all three (3) adjoining landowners, the applicants, the City's officers, the Mayor and Councillor Lee-Anne Smith in attendance. The meeting provided the opportunity for the applicants to explain the proposed modified application (detailed in the submission section of this report) as well as allowing the adjoining landowners to provide any comments they wished to raise on the new proposal.

Given the previous consultation with neighbours conducted, all the adjoining landowners were sent out copies of the modified plans and given 14 days to provide their final comments on the development. As such, this application has been referred to Council for determination as the proposal constitutes a 'D' use in the City's Town Planning Scheme No.3 which was advertised to nearby landowners and two (2) objections to the proposal were received.

Submission

The applicant seeks approval to construct eighteen (18) multiple dwellings. The development comprises of two (2) buildings consisting of ground floor car parking for the residential units, four (4) ground floor units, ten (10) first floor units and four (4) second floor units, drying areas, store rooms, bicycle parking, communal drying courtyard, bin storage as well as ample visitor parking.

The intent of the development is to provide affordable housing options to both first home buyers and local residents of Hamilton Hill. The development is intended to be sold to the market or to the Department of Housing and works as part of the Affordable Housing Strategy providing housing diversity and options to the area other than single and grouped housing typologies.

After the on-site meeting conducted on the 31 May 2012, the applicants had no further discussions with the neighbouring properties relating to the design until the meeting held at the City's offices on the 31 July 2012 with all affected parties. Given the response to the on-site meeting and on-going consultation with the City's Planning Officers, the plan has been revised with the following being a summary of the main changes to the design which have occurred:

1. Four dwellings on the ground floor instead of eight with the previous design, significantly less boundary walls therefore less bulk on the boundary and no boundary walls on the southern side.
2. Dispersion of the bulk of the first floor development over the entire development site rather than just towards the Carrington Street frontage which includes two separate buildings.
3. The introduction of a second floor at the two street frontages to provide better interaction with the streets at both ends of the development site.
4. Activation of the Wheeler Street frontage for better streetscape and urban design presentation.
5. Additional (18th) Ground floor adaptable living unit to Wheeler Street frontage to provide housing choice to those who require universal accessibility.
6. No courtyards facing the Port School to maximise privacy and lessen the potential for acoustic disruption of the school onto our development.
7. Introduction of a communal open space area for the benefit of all residents on site.
8. Skillion roof style to minimise the overall height of the building and lessen the bulk and scale of the development.
9. One (1) vehicle parking bay has now been provided per unit including one (1) adaptable bay and adequate and compliant visitor parking bays.

The proposal does not comply with ground floor setbacks to the southern boundary. The final plans proposed, as presented at the meeting held at the City's offices on the 31 July 2012, were advertised to the directly adjoining landowners for comment. Three (3) submissions were received with two (2) objections and therefore, the subject development is being referred to Council for determination given multiple dwellings is a 'D' discretionary use within a mixed business zone.

Report

The following section provides a discussion of the various issues affecting the proposal.

Statutory Framework

Town Planning Scheme No. 3

The subject site is zoned Mixed Business under the City's Town Planning Scheme No. 3 (TPS 3) and multiple dwellings are a 'D' use which means that *"the use is not permitted unless the local government has exercised its discretion and has granted planning approval."* Council therefore has the discretion to issue planning approval for the proposed development.

State Planning Policy 3.1 - Residential Design Codes of Western Australia

The proposal has been assessed under the Residential Design Codes of Western Australia (R-Codes) which were modified in November 2010 to incorporate provisions for multiple dwellings in areas with a coding of R30 or greater. This section of the R-codes provides development assessment criteria for multiple dwellings.

The dwellings have been assessed against the residential provisions of the R-Codes and are deemed to comply with acceptable development provisions with the exception of the ground floor setback of the stairs/stores/bin storage on the southern boundary. The stairs/stores and bin storage proposes a 1.2 metre setback. The total length of the wall is 57.41 metres and the height of wall is less than 3.5 metres, with no major openings and therefore the required setback is 1.5 metres. However, the proposed minor setback variation is considered acceptable as it satisfies the performance criteria of Clause 7.1.4 of the R-Codes in the following ways:

1. Does not impact on the amount of sunlight available and ventilation for buildings and open space associated with them.
2. The articulation in the wall allows for the moderation the visual impact of the building bulk.
3. This wall is predominantly adjacent to a neighbouring driveway.
4. Adjoining properties to the south have access to direct sun.
5. The proposal complies with the R-Code requirement relating to privacy.

In addition, with the first and second floor balconies within the cone of vision of the northern properties at No. 64A Wheeler Road and No. 402 Carrington Street, a condition has been recommended requiring the balconies to be screened in accordance with part 7.4.1 of the Residential Design Codes with a permanent obscure material and be non-openable to a minimum of 1.6 metres above the finished floor level.

Neighbour Consultation

The proposal was advertised to the three (3) adjoining landowners considered to be potentially affected by the proposal. Three (3) responses were received consisting of two (2) objections and one (1) non-objection. The planning issues raised during the consultation process are detailed in attachment 8, schedule of submissions.

Traffic

Given the nature of the site, a request for a traffic impact study was requested to ensure that the City is provided with adequate information to assess the transport related impacts of the development.

The development fits into the WAPC Transport Assessment Guidelines for Developments category of having a moderate impact, for which a transport statement should be provided. The impact of the proposal on local traffic has been assessed by the City's Engineers who consider that the existing road network can accommodate additional traffic movements resulting from the proposed development. This is due to the scale and number of expected vehicle movements generated by the residential dwellings.

In addition, as the site abuts a Red Primary Regional Road (Carrington Street) the application was referred to Main Roads for comments. As noted earlier in this Report, the applicant prior to lodging the application for approval with the City contacted the Department of Transport who informed them that future vehicle access from Carrington Street would not be available to service the proposed site. Given therefore the site is accessed from Wheeler Road, the development was considered acceptable by Main Roads subject to conditions and an advice note which have been considered.

Built Form

The proposed development consists of two (2) buildings within the site with it being split between two and three storeys. The heights of the proposed site are compliant with the R-Code requirements for multiple dwellings site zoned R60, with a maximum external wall height allowed of 9 meters and top of concealed roof height of 10 metres above natural ground level.

The contemporary facade of the buildings, in particular towards the Wheeler Road elevation, which comprises a select range of attractive external wall surface treatments that will provide articulation and interest to the streetscape, balconies assisting in providing passive surveillance of the Wheeler Road streetscape and will make an attractive addition to the streetscape.

Conclusion

The proposal for a three-storey multiple dwelling development consisting of a eighteen (18) dwellings is considered to provide additional dwellings within close proximity to the Hamilton Hill Activity Centre. The proposal has been assessed on its merits and is supported for the following reasons:

1. The development component complies with the requirements of the Residential Design Codes.
2. The proposal is consistent with the State Government's Directions 2031 document which promotes density nearby designated activity centres.
3. The proposal is considered to increase vibrancy and activity to a site currently occupied by a vacant building.
4. The proposal will provide a housing type (apartments) which is relatively uncommon in the area adding to a diversity of housing and residents within the Phoenix precinct.
5. The proposal is considered to enhance surveillance of the area.
6. Only highlight windows and screened balconies are provided to the northern elevation, therefore no visual privacy issues are prevalent.
7. The bulk and scale of the building is mitigated through the use of skillion roof design and through the provision of two buildings with the third level only being provided to the front sections of both buildings.
8. Traffic generated by the development is not considered excessive and is considered to be adequately accommodated within the existing road network.

In light of the above, it is recommended that Council approve the application, subject to the conditions confirmed in the officer's recommendation to address the above matters.

Demographic Planning

- To ensure the planning of the City is based on an approach that has the potential to achieve high levels of convenience and prosperity for its citizens.
- To ensure development will enhance the levels of amenity currently enjoyed by the community.

Budget/Financial Implications

N/A

Legal Implications

Town Planning Scheme No 3
Planning and Development Act 2005
State Administrative Tribunal Regulations

Community Consultation

In accordance with Council's Town Planning Scheme No. 3, Clause 9.4, the original proposal was initially advertised to the northern and southern neighbouring properties of No. 62 Wheeler Road and No. 402 Carrington Street for comment given the variations proposed for side setbacks on the proposed upper floor of the subject development. As a result of this initial consultation, an on-site meeting was conducted on the 31 May 2012 with the applicants, the City's officers, select Councillors and the Mayor, along with the Principal of the adjoining Port School, Mr Barry Finch.

As a result of this on-site meeting, the applicant then lodged amended plans dated stamped 12 July 2012 to address the City's and Port Schools concerns. With these new plans submitted, given variations where now proposed towards the other northern property at No. 64A Wheeler Road, a meeting was held at the City's officers on the 31 July 2012 with all three (3) adjoining land owners, the City's officers, the applicants, the Mayor and one (1) councillor. The meeting gave the opportunity for the applicants to explain the changes they have made from the initial application as well as provide an opportunity for the adjoining landowners to view the modified plans and get an understanding of the proposed development.

As agreed upon at this meeting, the City sent copies of the new plans to each of the three (3) neighbouring properties for them to provide comments on. This resulted in the City receiving three (3) submissions with two (2) objections received. A copy of the schedule of submissions is detailed in attachment 8.

In addition, as the proposed development abuts a Red Primary Regional Road (Carrington Street) the application was referred to Main Roads for comment. As access to the site is from Wheeler Road, this development was acceptable to Main Roads subject to conditions and an advice note, which have been included in the recommended conditions. Details of the submission have been included in attachment 8.

Attachment(s)

1. Site Plan
2. Overshadowing Plan
3. Landscaping Plan
4. Floor Plans
5. Elevations
6. Existing Site Plans
7. Waste Management Plan
8. Traffic Report
9. Applicants Response to Final Submissions
10. Schedule of Submissions

Advice to Proponent(s)/Submissioners

The Proponent(s) and those landowners who lodged a submission on the proposal have been advised that this matter is to be considered at the 13 September 2012 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

14.10 (OCM 13/09/2012) - TWO CONSULTING ROOMS - LOCATION: LOT 453 (NO. 3/1) MACQUARIE BOULEVARDE, HAMMOND PARK - OWNER: BUCKOS WA PTY LTD ANORS - APPLICANT: ERWIN BIEMEL AND ASSOCIATES (6006998) (R SIM) (ATTACH)

RECOMMENDATION

That Council :

- (1) refuse to grant approval to commence development for an additional consulting room at Tenancy 3 No.1 Macquarie Boulevard for the following reasons:
 1. The proposed development does not comply with the car parking standards of Town Planning Scheme No.3.
- (2) advise the owners of 1 Macquarie Boulevard of the extent of the existing onsite parking shortfall in writing.

COUNCIL DECISION**Background**

The proposal seeks to modify an existing approval (DA12/0554) for a Medical Consulting Room at 3/1 Macquarie Boulevard Hammond Park. The Change Of Use to medical consulting room for Unit 3 was approved under delegated authority by the City on 19 July 2012 as the development was within the extent of officer delegation. The applicant chose not to apply for two consulting rooms at the time to ensure that the use could commence without needing to wait for a determination at a Council meeting. Approval to increase the number of consulting rooms to two is beyond Officer delegation due to the size of the variation to car parking standards at the shopping centre.

Submission

The applicant now seeks to modify the existing approval to increase the number of practitioners from 1 to 2. The previous planning approval contained a condition which restricted the number of practitioners operating from the approved consulting room at any one time to 1 practitioner as there was no delegation to issue an approval which further increased the car parking shortfall. At the time the applicant only sought approval for a single consulting room to allow the application to be determined under delegated authority.

Report

Lot 453, No. 1 Macquarie Boulevard is zoned 'Local Centre' under The City of Cockburn Town Planning Scheme No. 3. The site consists of a supermarket and 5 specialty shops and was approved by Council as part of development application consisting of a supermarket, shops, tavern and drive through bottle shop on Lots 453 and 454, No.'s 1 and 2 Macquarie Boulevard. (Refer OCM 13/11/2008 Minute Number 3833). The supermarket and 5 specialty shops at Lot 453 No. 1 Macquarie Boulevard was approved by Council with an onsite provision of 69 parking spaces in lieu of the required 101 parking spaces under the provisions of the scheme.

On 24 March 2011, the City approved a change of use from shop to consulting rooms under delegated authority at tenancy 2a 1 Macquarie Boulevard, which has a floor space of 100m². The parking standard for the use class 'shop' under the Scheme is 1 parking bay per 12m² of

GLA, while the use class 'consulting room' attracts a parking standard of 5 parking bays per medical practitioner or consulting room. Given that this change of use was approved with a condition restricting the number of medical practitioners to 1 practitioner at any given time, the overall parking shortfall at 1 Macquarie Boulevard was reduced from 32 to 28 parking bays.

An application for a change of use from shop to consulting room was approved under delegated authority by the City for tenancy 3, on 19 July 2012 which has a floor space of 83m². A condition restricting the number of medical practitioners operating from the subject site at any given time to 1 medical practitioner was also imposed, which further reduced the onsite parking shortfall to 26 bays. Should an additional medical practitioner be approved to operate from the tenancy 3, the onsite parking shortfall at 1 Macquarie Boulevard would increase to 29 parking bays.

An assessment of the current onsite parking provision at Lot 453, No 1 MacQuarie Boulevard and Lot 454, No 2 MacQuarie Boulevard is as follows:

LOT 453, NO. 1 MACQUARIE BOULEVARD HAMMOND PARK		
APPROVED USE	REQUIRED PARKING SPACES	CAR SPACES
TENANCY 1 SUPERMARKET (800M ² NLA)	1 CAR SPACE: 12M ² NLA	66.7 CAR SPACES
TENANCY 2A (HAMMOND PARK FAMILY PRACTICE) (CONSULTING ROOM – 1 PRACTITIONER 100M ² NLA)	5 CAR SPACES PER MEDICAL PRACTITIONER	5 CAR SPACES
TENANCY'S 2 (PHARMACY) (131M ² NLA SHOP/PHARMACY)	1 CAR SPACE PER 12M ² NLA	10.9 CAR SPACES
TENANCY 3 (PROPOSED CONSULTING ROOMS WITH 2 PRACTITIONERS APPROX 77M ² NLA)	5 CAR SPACES PER PRACTITIONER	10 CAR SPACES
TENANCY 4 (SHOP/CAFE 100M ² NLA)	1 CAR SPACE PER 12M ² NLA	8.3 CAR SPACES
	TOTAL REQUIRED: 101 CAR SPACES	TOTAL PROVIDED: 69 CAR SPACES

LOT 454, NO. 2 MACQUARIE BOULEVARD HAMMOND PARK		
TAVERN (306M2 SEATED, 122M2 STANDING)	1 CAR SPACE PER 4 SEATS (SEATED AREA)	77 CAR SPACES
	1 CAR SPACE PER 2M2 NLA (STANDING AREA)	61 CAR SPACES
SHOP (DRIVE THROUGH BOTTLE SHOP 176M2)	1 CAR SPACE PER 12M2 NLA	14.7 CAR SPACES
	TOTAL REQUIRED: 152 CAR SPACES	TOTAL PROVIDED: 101 CAR SPACES

The City's APD 10; *Discretion to modify development standards – non residential development* permits variations to the acceptable development standard with regard to matters such as onsite parking of up to 20% to be approved under delegated authority. Any applications for development such as a change of use which result in a car parking variation of 20% or more are required to be determined by Council. The current proposal DA12/0660 will increase the total parking shortfall to 29 parking spaces. As the variation exceeds 20% Council determination is required.

Despite the reciprocal parking arrangement between the Tavern and Shopping Centre sites, there is a substantial car parking variation. Given that there is a large existing onsite parking shortfall, the expansion of the centre to two practitioners will exacerbate the car parking shortfall further and this is contrary to the objective of providing well serviced accessible retail centres.

The applicant has provided letters of support from the adjoining tenancies at 1 Macquarie Boulevard, the Fremantle Health Service and members of the immediate surrounding community in Hammond Park.

Letters and a multi party signature letter with around 250 supporting signatures have been lodged with the application and are at Attachment 2.

In light of the above, the proposal is recommended for refusal due to the extent of non compliance with Town Planning Scheme No.3 car parking requirements at the Centre.

Strategic Plan/Policy Implications

Demographic Planning

- To ensure the planning of the City is based on an approach that has the potential to achieve high levels of convenience and prosperity for its citizens.
- To ensure development will enhance the levels of amenity currently enjoyed by the community.

Lifestyle and Aspiration Achievement

- To identify community needs, aspirations, expectations and priorities for services that are required to meet the changing demographics of the district.

Budget/Financial Implications

N/A

Legal Implications

N/A

Community Consultation

N/A

Attachment(s)

1. Application plans
2. Site plan
3. Letters of support

Advice to Proponent(s)/Submissioners

The Proponent(s) have been advised that this matter is to be considered at the 13 September, Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

14.11 (OCM 13/09/2012) - TWO GROUPED DWELLINGS - LOCATION: 19 TANUNDA ROAD, COOGEE - OWNER: ALAN MARIN & JACKIE PECOTIC - APPLICANT: ANTHONY RECHICHI ARCHITECTS (3316858) (T. CAPPELLUCCI) (ATTACH)

RECOMMENDATION

That Council

- (1) grant approval for two grouped dwellings at No. 19 (Lot 78) Tanunda Road, Coogee, in accordance with the attached plans and subject to the following conditions and advice notes:

CONDITIONS

1. Walls, fences and landscaped areas are to be truncated within 1.5 metres of where they adjoin vehicle access points where a driveway and/or parking bay meets a public street or limited in height to 0.75 metres.
2. All stormwater being contained and disposed of on-site to the satisfaction of the City.
3. During the construction phase, no activities causing noise and/or inconvenience to neighbours being carried out after 7.00pm or before 7.00am, Monday to Saturday, and not at all on Sunday or Public Holidays.
4. The proposed crossover must be located and constructed in accordance with the City's requirements.
5. The dwellings must be connected to the reticulated sewerage system of the Water Corporation prior to occupation.
6. The surface finish of the boundary walls abutting adjoining lots and reserves are to be either face brick or rendered the same colour as the external appearance the dwellings.
7. The boundary walls to the adjoining Public Open Space reserve shall be finished with an anti graffiti coating to the satisfaction of the City.
8. *Arrangements being made to the satisfaction of the Chief Executive Officer for the pro-rata subdivider contributions towards those items listed in the City of Cockburn Town Planning Scheme No. 3 for Development Contribution Area 13 – Community Infrastructure.*

9. Any vegetation removed or damaged on Rotary Park reserve during construction is to be reinstated to the satisfaction of the City.
10. *Pedestrian access between Rotary Park and Tanunda Close shall not be restricted during construction.*

ADVICE NOTES

1. This is a Planning Approval only and does not remove the responsibility of the applicant/owner to comply with all relevant building, health and engineering requirements of the City, with any requirements of the City of Cockburn Town Planning Scheme No. 3, or the requirements of any other external agency.
 2. In regard to Condition 2, the City requires the on-site storage capacity be designed to contain a 1 in 20 year storm of a 5 minute duration. This is based on the requirements to contain surface water by Building Codes of Australia.
 3. Where the obligation for payment of developer contributions has been met by a previous approval, such as subdivision, Condition No. 5 will be deemed to have been complied with. The principles and administrative requirements for Development Contribution Plans are set out in Part 6.3 of the City of Cockburn's Town Planning Scheme No. 3. Further information may be found at: www.cockburn.wa.gov.au/communityinfrastructure
 4. This development has been assessed and approved as 'grouped dwellings' and should not be construed as an approval to subdivide the land which will be assessed if and when an application is referred from the Western Australian Planning Commission.
- (2) advise those who made submissions on the proposal of the Council's decision.

COUNCIL DECISION

Background

Zoning:	MRS: Urban
	TPS3: Residential R20
Land use:	Grouped Dwellings
Lot size:	920m ²
Use class:	P

Submission

The applicant seeks approval to construct two grouped dwellings with a 2.5 story profile. The proposed new dwellings result in a maximum top of wall (roof over) height of 8.14 metres and 8.21 metres above ground level respectively which is a variation to Policy.

The proposed variations were advertised to surrounding landowners. One (1) submission was received which objected to impacts on views.

Report

Coogee Residential Height Requirements Policy APD 53

The Coogee Residential Height Requirements Policy APD 53 was prepared to guide the height of residential development in the suburb of Coogee and was adopted by Council on 14 August 2008. The policy states that:

“Maximum building height of residential development shall be limited to:

- (i) Top of wall (roof over) - 7m*
- (ii) Top of Wall (parapet) – 8m*
- (iii) Top of pitched roof – 10m”*

The policy states that building heights for residential development shall be limited to those specified in the policy and that any proposal that exceeds the requirements is to be advertised for public comment.

Building Height

As mentioned above, the proposal will result in two dwellings both with a portion of their wall height exceeding the maximum wall height by 1.14 metres (front unit) and 1.21 metres (rear unit).

The top of wall heights exceed Council’s policy for the front dwelling on the street (northern) and west (common driveway side) elevations and

the rear dwelling on the east (side), north (internal) and west (driveway) elevations.

Despite the wall height variation both dwellings are compliant with R-Codes boundary setback requirements except for the wall on the eastern boundary, abutting the Rotary Hill public open space reserve, where both dwellings have variations from the second floor as well as boundary wall length variations.

Overshadowing is fully compliant with the R-codes as only 2.7% (27m²) of the site area of No. 16 King Street is overshadowed where the R-codes permit up to 25% (or 250m²) to be overshadowed as of right.

The site has a very significant slope as the site rises approximately 5m from the front up to the rear and across the site. The steep gradient and cross fall of the land means that the wall height variations are limited to the western portion of the site for both dwellings along with the maximum occurring on the northern elevations for each dwelling.

Both dwellings have generally been designed to respect the natural ground levels existing on-site by incorporating ground floor levels below existing natural ground levels. The visual impression will be of the natural level of the site being maintained by stepping and retaining the development in sections across the site.

Building Side and Rear Setbacks

The application proposes side setback variations from the second floors of both dwellings to the eastern side boundary abutting Rotary Park. The second floor wall consisting of the Activity/Linen/Stair wall is setback 3 metres from the boundary in lieu of the required 4.3 metres as required by the R-Codes due to a major opening window proposed from the activity room overlooking the park.

The setback would comply without the windows overlooking the park, however it is considered beneficial to have windows to active living spaces overlooking public spaces in the interest of crime prevention. Passive surveillance of public space has been shown to improve to safety and security of public spaces. The side setback variations are therefore considered compliant with the Performance Criteria of Clause 6.3.1 of the R-Codes.

Garage Setback to Primary Street

The proposed front dwelling's garage is accessed from the common property access leg and is setback 1.65 to 2.6 metres from the street in lieu of the required 3 metres as per the R-Codes as well as being forward of the ground floor building line of the dwelling.

Given that the garage is a half basement due to the slope of the land and the 2.5 storey front from elevation, the impact of the garage setback variation is negligible and in context with the form of development proposed.

Front Elevation

The facade presented to the street beneath a low pitched gabled roof is a contemporary form consisting of a contrast between solid and void spaces in the building's massing plus a vertical layering of the building bulk. There is also a mixture of material with large expanses of glass and rendered block work providing articulation and interest to the streetscape.

In addition, the low roof pitch assists in reducing visual impact of the height and remains compatible with the existing streetscape. This has been achieved through the utilisation of materials and colours that maintain a consistency with adjoining properties, which themselves have predominately pitched rooves while the adjoining grouped dwellings to the west have concealed rooves.

Buildings on Boundary

The application proposes a parapet wall to each dwelling on the eastern boundary abutting the Rotary park. Both parapet walls are 10.21 metres in length, exceeding the maximum length required by the R-Codes for an R20 zoned lot (9 metres) up to one side boundary, for a single dwelling.

Given both parapet walls abut a parks and recreation reserve, the variation in regards to the length of each wall is considered minor and the walls will have no significant adverse effect on the amenity of the area in that they will not overshadow not over tower any development. The park also rises steeply away from the dwelling to the crest of the hill and the Rotary lookout point which is some 11m higher than the highest point of the subject site.

Visual Privacy

Both dwellings show balconies at the first floor level facing toward the west side neighbouring lot being 17A and 17 Tanunda Road.

The balconies are setback 5.6m which is less than the 7.5 metre cone of vision for balconies. This indicates that the potential for overlooking of a sensitive area needs to be assessed.

In this respect only the first floor balcony to the front unit potentially overlooks a sensitive area on the adjoining site. The courtyard of 17B Tanunda is within a direct line of sight of the balcony to the proposed front dwelling. As such the proposal was advertised to the neighbours for comment. However, no comments were received from the owners of either 17A or 17B Tanunda Road.

On detailed assessment the courtyard to 17b Tanunda Road is significantly lower than the proposed balcony and is roofed with a translucent polycarbonate sheet patio which is likely to block direct overlooking of the courtyard from the proposed balcony. The eastern facades of No's 17A and 17B Tununda Road also contain no major openings to the existing dwellings and therefore there are no other overlooking concerns raised by the proposal.

Given the lack of objection and the mitigating factor of the roofed courtyard and level difference no screening to the balcony to the front dwelling is considered necessary.

Conclusion

Despite a number of variations each individually is considered relatively minor and within the intent and objectives of the R-Codes, Town Planning Scheme 3 and the relevant Planning Policy. In light of the above, the proposed maximum top of wall (roof over) height for the front dwelling on the front elevation and the rear dwelling on the western elevation are considered to not adversely impact on the amenity of the surrounding area given that the total height is well within the maximum permitted under Policy. It is therefore recommended that Council approve the application subject to the conditions listed above.

Strategic Plan/Policy Implications

Demographic Planning

- To ensure the planning of the City is based on an approach that has the potential to achieve high levels of convenience and prosperity for its citizens.
- To ensure development will enhance the levels of amenity currently enjoyed by the community.

Budget/Financial Implications

N/A

Legal Implications

Town Planning Scheme No 3
Planning and Development Act 2005
State Administrative Tribunal Regulations

Community Consultation

In accordance with Planning Policy APD53, the proposal was advertised to the surrounding five (5) neighbours for comment. One neighbour responded consisting of one (1) objection being received from the owners of No. 16 King Street which is the property touching the SW corner of the subject site. The submission does not object to the height but impact on views and raises several other matters as observations.

Attachment 5 contains the submission and Officer's response. However two points raised in the submission are addressed here.

The submission raises a range of general concerns about the nature of infill development and building scale and bulk in Coogee. It should be noted that the overall density coding of Coogee at R20 is classified as Low Density. The topography and natural features of the area make the views that can be obtained highly valuable and sought after. High value development of scale and bulk often accompanies areas with such features but ultimately the form of development will remain consistent with the R-20 Density Code over the area.

The submission also raises the potential for impact on views from Rotary Hill lookout. It is apparent that the proposed top of roof height at 8.4m above the highest ground level on the site of approximately 34m (42.4m AHD total) is still 1m below the ground level at the lookout which is 43.4m AHD. And as stated above the top of roof height to the rear unit is below the 10m maximum roof height permitted under Policy.

Attachment(s)

1. Site, Landscaping and Overshadowing Plans
2. Floor plans
3. Elevations
4. Variations to Height Limits
5. Location plan
6. Schedule of Submissions

Advice to Proponent(s)/Submissioners

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 13 September 2012 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

14.12 (OCM 13/09/2012) - DRAFT SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN - STAGE 3 AND SCHEME AMENDMENT NO. 28 - LOCATION: HAMMOND PARK - OWNER: VARIOUS - APPLICANT: N/A (SM/M/017 & 93028) (R COLALILLO) (ATTACH)

RECOMMENDATION

That Council

- (1) endorse the Schedule of Submissions prepared in respect of the Draft Southern Suburbs District Structure Plan - Stage 3 and Amendment No. 28 to City of Cockburn Town Planning Scheme No. 3;
- (2) adopt the Draft Southern Suburbs District Structure Plan - Stage 3 ("SSDSP3"), for the purpose of providing a guiding document to inform the preparation of future Local Structure Plans ("LSP's") within the District Structure Plan area, subject to the following modifications:
 1. SSDSP3 Spatial Map (Figure 7)
 - (a) Deleting the notional linear public open space spine abutting the future Rowley Road Primary Regional Road reservation on Lots 13 and 51 Rowley Road and Lot 301 Barfield Road, Hammond Park;
 - (b) Rewording Plan Note 1 to the following -
'Future residential development shall not directly abut Rowley Road. Future local structure planning is to demonstrate a suitable interface treatment (e.g. enlarged service road design with fronting residential development as a minimum) being provided to the future Rowley Road freight access route.'
 - (c) Adding 'subject to Main Roads WA approval' to the end of Plan Note 2;

- (d) Adding 'This may be further rationalised through subsequent local structure planning to determine how this specific access is created.' to the end of Plan Note 3 ;
- (e) Reducing the size of the extent of the 'Central Precinct' to only include Lots 114, 123 and 124 Wattleup Road, Hammond Park;
- (f) Adding interface/buffer roads to the southern boundary of Harry Waring Marsupial Reserve and northern boundary of Frankland Reserve;
- (g) Adding a major shared path along the existing Wattleup Road alignment;
- (h) Removing the roundabout at the intersection of Hammond Road and Wattleup Road intersection and indicating this as a traffic light controlled intersection.

2. SSDSP3 Report

- (a) Adding text to section '6.7.1 Rowley Road' stating that any interim construction or upgrades to Rowley Road not directly related to subdivision/development within the SSDSP3 area is the responsibility of the State and reiterating the City's preference for Main Roads WA to construct/upgrade Rowley Road using 'quiet grade' asphalt similar to that used in the construction of the Kwinana Freeway;
- (b) Rewording section '5.6.4 Neighbourhood/Local POS - ongoing planning' to reflect that POS design details at the LSP stage are only anticipated to be conceptual and as a minimum in terms of future maintenance, the City would expect LSP's to outline an approximate unit rate per hectare for maintenance of proposed POS areas; and
- (c) Adding to section '5.3.3 Shared Paths/Connections' reference to a major shared path being required to be provided along the existing and realigned portions of Wattleup Road.

- (3) forward a copy of the endorsed SSDSP3 (as modified) to the Western Australian Planning Commission ("WAPC") for information purposes only;
- (4) advise landowners within the SSDSP3 area, affected landowners, and affected public authorities of the adoption of the District Structure Plan as modified; and
- (5) adopt Scheme Amendment No. 28 for final approval as set out in Attachment 4, subject to the following modifications:
 1. Rewording provision 1 point 1 within the 'Infrastructure and administrative items to be funded' section of DCA's 9 and 10 as follows

'Construction of one carriage way comprised of two-lanes for Hammond Road and where the reserve width is less than 40 metres wide, kerbing to the verge side of the carriageway shall be provided';
 2. Rewording provision 1 point 6 within the 'Infrastructure and administrative items to be funded' section of DCA's 9 and 10 as follows

'Land and infrastructure associated with the drainage of Hammond Road'; and
 3. Deleting provision 3 within the 'Method for calculating contributions' section of DCA's 9 and 10 being -

'In relation to those portions of properties that have been identified on the Southern Suburbs District Structure Plan - Stage 3 for the purpose of a primary school, if these sites are not developed for school purposes in the future a proportional contribution to the regional drainage and road infrastructure will be required'.
- (6) once modified, ensure the amendment documentation be signed and sealed and then submitted to the Western Australian Planning Commission along with details of the steps taken to advertise the amendment, with a request for the endorsement of final approval by the Hon. Minister for Planning.

COUNCIL DECISION

Background

The Southern Suburbs District Structure Plan - Stage 3 ("SSDSP3") was adopted by Council in 2005 to guide future residential subdivision and development in the southern portion of Hammond Park. The 2005 adoption of the plan has enabled subdivision within the northern part of the subject area to proceed (e.g.) Hammond Rise estate. However, since the original SSDSP3 was adopted, a number of factors have led to the need for the City to undertake a review of the plan. This review was completed and led to the development of the (revised) Draft SSDSP3 which Council at its meeting held 8 March 2012 resolved to advertise for the purposes of providing a guiding document to inform the preparation of future LSP's within the subject area.

In order to cater for the future development of the SSDSP3 area, there is a need for development contributions towards the following regional infrastructure:

- Hammond Road extension - including widening and upgrading of existing Frankland Avenue
- Southern Lakes Main Drainage Scheme ('SLMDS') - including construction of a groundwater control point (swale) on Gaebler Road (within Lot 412 Gaebler Road).

To facilitate the collection of development contributions for the above regional infrastructure, Scheme Amendment No. 28 proposes to introduce two new development contributions areas within the locality, these being DCA 9 and DCA 10. Scheme Amendment No. 28 was originally adopted by Council at its 11 August 2005 Ordinary Council Meeting; however, the Amendment has not yet received final approval from the WAPC.

Since Amendment No. 28 was originally initiated in 2005, assessment by officers of the Department of Planning ("DoP") and a number of associated contributing factors have led to the need for the City of Cockburn ("City") to modify the Amendment provisions and readvertise the proposal in accordance with the direction given by the DoP in February 2012. In addition, a comprehensive review of the associated SSDSP3 has been undertaken to reflect the modified Amendment provisions. In particular, the following factors have influenced the requirement to update Amendment No. 28 and the SSDSP3:

- relocation of the future Mandogalup Train Station south of Rowley Road;
- Rowley Road designated as a strategic freight route;
- extension of the Kwinana Industrial (including Air Quality) Buffer;
- retention of Hammond Road extension alignment through Frankland Reserve.

In addition to the above, the timing of the update meant that it was necessary to ensure Amendment 28 and SSDSP3 were consistent with all current government strategic planning initiatives including State Planning Policy 3.6 - Development Contributions for Infrastructure ("SPP 3.6"), Directions 2031 and beyond, Liveable Neighbourhoods - Edition 4 etc.

The area covered by the Draft SSDSP3 and Amendment No. 28 is shown within Attachment 1.

Both the Draft SSDSP3 and Amendment No. 28 were advertised for public comment for 42 days, ending on 25 July 2012. The purpose of this report is for Council to consider the Draft SSDSP3 and Amendment No. 28 for final adoption in light of submissions received on the proposals.

Submission

N/A

Report

Southern Suburbs District Structure Plan - Stage 3

The Draft SSDSP3 is provided within the attachments to this report and includes the following components:

1. A Draft District Structure Plan report.
2. The Draft District Structure Plan spatial map.

The Draft SSDSP3 provides an overall planning framework to guide future LSP's, given the fragmented nature of landownership which exists.

The Draft SSDSP3 has been prepared on Liveable Neighbourhoods design principles, and allows for future development of the land for urban purposes in a coordinated manner. It aims to provide for an appropriate response to the environmental characteristics of the land, as well as providing a mixture of residential densities based around a highly permeable neighbourhood design.

It is consistent with the WAPC's Directions 2031 and Beyond: Metropolitan Planning Beyond the Horizon ("Directions 2031") as it provides for a range of residential densities and housing types. Medium to high density housing will be located in high amenity areas, such as adjacent to neighbourhood centres and public open space reserves.

The Draft District Structure Plan retains the existing major road network, while providing for a new east-west road connection between Hammond Road and Barfield Road. An interconnecting network of shared paths will also be provided, utilising the existing road network and new local roads. New pedestrian links will also provide connections to the proposed neighbourhood centres and new areas of public open space.

The public open space strategy adopted by the Draft District Structure Plan, balances environmental, recreational and drainage objectives through an integrated open space, conservation and drainage network.

Scheme Amendment No. 28

As mentioned previously, Scheme Amendment No. 28 has been subject to various modifications at the direction of the DoP. The specific modifications to original Development Contribution Plans (“DCP”) and Development Areas (“DA”) provisions proposed by the 2005 version of Amendment No. 28 are as follows:

DCA 9 - Hammond Park	
Modification	Justification
Specification of method of calculating cost contributions, period of operation, priority and timing for the provision of infrastructure and review process	SPP3.6 was gazetted in November 2009 and prescribes the principles underlying development contributions and the form, content and process for the preparation of a development contribution plan under a local planning scheme. As such, the DCP has been modified to adhere to the requirements of SPP3.6.
Removal of requirement to contribute towards construction of Rowley Road between the Kwinana Freeway and Hammond Road reservation	Planning Control Area No. 95 has been designated over the alignment of Rowley Road and the Western Australian Planning Commission (“WAPC”) has purchased some of the land required for the road. Given this road will become a freight route servicing the Latitude 32 industrial development and the proposed outer harbour, it is not appropriate for developers within the SSDSP3 area to contribute to the construction of Rowley Road. Acquisition, funding, design and construction of Rowley Road are therefore the responsibility of the State Government given the road will be a ‘Primary Regional Road’.
Addition of requirement to contribute towards ‘full earthworks’ in relation to Hammond Road upgrading	This requirement has been added in order to maintain consistency with DCP 10 and is a standard requirement relating to the upgrading and widening of regional roads.

<p>Removal of requirement to make 1% POS cash-in-lieu contribution toward purchase of Lots 41 Gaebler Road and 42 Frankland Avenue, Hammond Park</p>	<p>The Department of Planning (“DoP”) advised that the inclusion of POS as a DCP item cannot be supported as conservation assets are not POS and the <i>Planning and Development Act 2005</i> does not provide for cash-in-lieu to be expended on the acquisition of conservation assets. In terms of the Conservation Category Wetland and its associated buffer, State Government requirements will dictate the need for this to be ceded to the crown free of cost in addition to POS requirements.</p>
<p>Addition of provision requiring contributions to be made toward the relocation of servicing infrastructure, where required.</p>	<p>This requirement has been added in order to maintain consistency with DCP 10 and is a standard requirement relating to the development and/or upgrading of infrastructure in greenfield areas.</p>

DCA 10 - Wattleup	
Modification	Justification
<p>Specification of method of calculating cost contributions, period of operation, priority and timing for the provision of infrastructure and review process</p>	<p>SPP3.6 was gazetted in November 2009 and prescribes the principles underlying development contributions and the form, content and process for the preparation of a development contribution plan under a local planning scheme. As such, the DCP has been modified to adhere to the requirements of SPP3.6.</p>
<p>Removal of requirement to contribute towards construction of Rowley Road between the Hammond Road reservation and Lot 81 Wattleup Road</p>	<p>Planning Control Area No. 95 has been designated over the alignment of Rowley Road and the WAPC has purchased some of the land required for the road. Given this road will become a freight route servicing the Latitude 32 industrial development and the proposed outer harbour, it is not appropriate for developers within the SSDSP3 area to contribute to the construction of Rowley Road. Acquisition, funding, design and construction of Rowley Road are therefore the responsibility of the State Government given the road will be a ‘Primary Regional Road’.</p>
<p>Removal of requirement to make 1% POS cash-in-lieu contribution toward purchase of Lots 41 Gaebler Road and 42 Frankland Avenue, Hammond Park</p>	<p>The Department of Planning (“DoP”) advised that the inclusion of POS as a DCP item cannot be supported as conservation assets are not POS and the <i>Planning and Development Act 2005</i> does not provide for cash-in-lieu to be expended on the acquisition of conservation assets. In terms of the Conservation Category Wetland and its associated buffer, State Government requirements will dictate the</p>

	need for this to be ceded to the crown free of cost in addition to POS requirements.
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DA 26 - Rowley Road	
Modification	Justification
Removal of Provision 4. Mixed Business/Local Centre design requirements	Section 5.8 of the SSDSP3 details the requirements for the development of the neighbourhood centre and neighbourhood nodes within the Development Area. The design requirements will be further refined through the Structure Plan process.
Removal of Provision 5. Design Guidelines for development precincts	Sections 5.1 and 5.4 of the SSDSP3 provide guidance with regards to the City's objectives for the design of future subdivision and development. Future Structure Plans and Detailed Area Plans will provide further opportunities to refine the character of the Development Area.
Removal of Provision 6. Buffer requirements for Market garden and intensive horticulture uses	The majority of the sites listed within the provision have ceased operation and buffer requirements are detailed within Section 4.1.1 of the SSDSP3 and the WAPC's Planning Bulletin 63. The refinement of buffers and/or mitigation measures will be determined through the Structure Plan process.
Removal of Provision 7. 1% POS cash-in-lieu contribution for Lots 41 Gaebler Road and 42 Frankland Avenue, Hammond Park	The DoP advised that the inclusion of POS as a DCP item cannot be supported as conservation assets are not POS and the <i>Planning and Development Act 2005</i> does not provide for cash-in-lieu to be expended on the acquisition of conservation assets. Landowners will be required to provide 10% POS as per section 5.6.2 of the SSDSP3 and Liveable Neighbourhoods. In terms of the Conservation Category Wetland and its associated buffer, State Government requirements will dictate the need for this to be ceded to the crown free of cost in addition to POS requirements.
Removal of Provision 8. Rowley Road noise wall requirements	The SSDSP3 specifies that future residential development shall not directly abut Rowley Road. Future Structure Plans must demonstrate a suitable interface treatment being provided to the future Rowley Road freight access route, which includes how development will comply with the associated State Planning Policy 5.4 covering noise road impacts. This will need to include a detailed noise assessment which demonstrates how compliance can be achieved. The

	construction of noise walls specific to Rowley Road will be the responsibility of the State Government given Rowley Road's designation as a strategic freight route. SPP5.4 also specifies requirements with regards to future freight routes and sensitive land uses.
Removal of Provision 9. Kwinana Freeway and Railway noise amelioration requirements	Future Structure Plans for land in the vicinity of the Kwinana Freeway will need to demonstrate compliance with SPP5.4. It is noted this provision was developed prior to the gazettal of SPP5.4.

DA 27 - Wattleup Road	
Modification	Justification
Removal of Provision 4. Mixed Business/Local Centre design requirements	Section 5.8 of the SSDSP3 details the requirements for the development of the neighbourhood centre and neighbourhood nodes within the Development Area. The design requirements will be further refined through the Structure Plan process.
Removal of Provision 5. Design Guidelines for development precincts	Sections 5.1 and 5.4 of the SSDSP3 provide guidance with regards to the City's objectives for the design of future subdivision and development. Future Structure Plans and Detailed Area Plans will provide further opportunities to refine the character of the Development Area.
Removal of Provision 6. Buffer requirements for Market garden, poultry farm, orchard, turf farm and intensive horticulture uses	The majority of the sites listed within the provision have ceased operation and buffer requirements are detailed within Section 4.1.1 of the SSDSP3 and the WAPC's Planning Bulletin 63. The refinement of buffers and/or mitigation measures will therefore be determined through the Structure Plan process.
Removal of Provision 7. 1% POS cash-in-lieu contribution for Lots 41 Gaebler Road and 42 Frankland Avenue, Hammond Park	The DoP advised that the inclusion of POS as a DCP item cannot be supported as conservation assets are not POS and the <i>Planning and Development Act 2005</i> does not provide for cash-in-lieu to be expended on the acquisition of conservation assets. Landowners will be required to provide 10% POS as per section 5.6.2 of the SSDSP3 and Liveable Neighbourhoods. In terms of the Conservation Category Wetland and its associated buffer, State Government requirements will dictate the need for this to be ceded to the crown free of cost in addition to POS requirements.
Removal of Provision 8.	The SSDSP3 specifies that future

Rowley Road noise wall requirements	residential development shall not directly abut Rowley Road. Future Structure Plans must demonstrate a suitable interface treatment being provided to the future Rowley Road freight access route, which includes how development will comply with the associated State Planning Policy 5.4 covering noise road impacts. This will need to include a detailed noise assessment which demonstrates how compliance can be achieved. The construction of noise walls specific to Rowley Road will be the responsibility of the State Government given Rowley Road's designation as a strategic freight route. SPP5.4 also specifies requirements with regards to future freight routes and sensitive land uses.
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In addition to the above modifications, the southern boundaries of DCA's 9 and DCA 10 and DA's 26 and 27 have been modified on the proposed Scheme zoning map to reflect Planning Control Area 95 - Rowley Road extension.

Refer to Attachment 4 for a full copy of the Amendment No. 28 document including the above modifications. Attachment 5 contains the Draft Cost Contribution Schedules for DCA's 9 and 10. As noted within the revised cost contribution schedule, the costs associated with developer contributions within the SSDSP3 area are summarised following:

DCA 9 (Hammond Park)		DCA 10 (Wattleup)	
Lots <u>within</u> SLMDS catchment area (i.e. all lots <i>except</i> Lots 51 Rowley Rd & 301 Barfield Rd)	Lots <u>outside</u> SLMDS catchment area (i.e. Lots 51 Rowley Rd & 301 Barfield Rd)	Lots <u>within</u> SLMDS catchment area (i.e. Lots 1, 2, 110 & 111 Wattleup Rd)	Lots <u>outside</u> SLMDS catchment area (i.e. all lots <i>except</i> Lots 1, 2, 110 & 111 Wattleup Rd)
\$28,841.75/ha	\$28,053.82/ha	\$29,816.88/ha	\$29,028.95/ha

Based on a typical average of approximately 15 lots per hectare, contributions will range from \$1870 to \$1988 per lot on average. This is considered an appropriate cost magnitude for the development. It should be noted that these figures do not include additional developer contributions which are required to be made in accordance with Development Contribution Plan 13 - Community Infrastructure.

The cost contribution schedules for DCA 9 and DCA 10 will be required to be updated no later than 90 days from the date of gazettal of Scheme Amendment No. 28 in accordance with the provisions of SPP3.6.

Community Consultation Outcomes

The Draft SSDSP3 and Amendment No. 28 were advertised for public comment for a period of 42 days, concluding on 25 July 2012. A total of 26 submissions were received, with 15 submissions of support or support subject to conditions or modifications and 11 submissions expressing concerns or objecting.

All submissions have been outlined and addressed in detail in the Schedule of Submissions (Attachment 6). The key issues that have been raised are summarised below.

Central Precinct

A number of submissions objected to the requirement for a comprehensive LSP to be submitted for the area notated as the 'Central Precinct'. It was originally considered appropriate by the City that a comprehensive local structure plan be required for the central neighbourhood centre and adjacent development and to not consider individual structure plans in this location due to the need to ensure the suitable integration of development. Given the concerns outlined by the submissioners including the separation of the identified precinct by Hammond Road and the Neighbourhood Centre being located within only two to three landholdings, it is recommended that the 'Central Precinct' be reduced to only include Lots 114, 123 and 124 Wattleup Road.

Linear POS Abutting Rowley Road

A submission was received raising concerns in relation to the proposed linear POS spine adjacent to Rowley Road within the south east of the project area. The City identified the requirement for this POS area as a potential mitigation measure for noise and visual amenity associated with the future Rowley Road strategic freight route. Alternatives to the linear POS design have been provided by the submissioner including examples of how an enlarged service road with appropriate landscaping can provide adequate buffering and amenity to future residences. It is therefore considered appropriate to delete reference to the linear POS on the SSDSP3 spatial map and modify the associated notation accordingly. The design of this interface will be carefully considered at the LSP stage and the notation will stipulate the need for an appropriate design response. This will need to comply with the

associated SPP 5.4, which dictates noise requirements and how these requirements will need to influence the final response in respect of Local Structure Plan designs.

Interface Roads to Conservation Reserves

The DEC raised concerns with the Draft SSDSP3 not prescribing the need for interface roads abutting large areas of conservation such as Frankland Reserve and Harry Waring Marsupial Reserve. Although the Draft SSDSP3 report and map detailed the need for Bushfire Management Plans to be prepared and influence design at the LSP stage it is recommended that the SSDSP3 spatial map be modified to include roads as appropriate separation measures adjacent to these conservation areas. This is seen as a positive change to embrace within the document.

Barfield Road Link

Submissions were received which raised concerns with the need, timing and practicalities of the proposed vehicular and pedestrian link for Barfield Road to connect with development to the south in the Town of Kwinana. The main issue of contention being the timing for the construction of the upgraded of Rowley Road not being known, and whether Main Roads WA will provide for such a link. This would therefore have implications in terms of timing of development adjoining the link.

Despite these concerns the City maintains that a future linkage to residential development and the Mandogalup Train Station to the south of the SSDSP3 area is important and desirable. This is also supported by the need for adequate access to be provided for future residents in the Mandogalup residential cell to access the future High School in the SSDSP3 area. The Town of Kwinana has also reiterated its support for this link given future residential development which is planned for the Mandogalup area in line with the Town's 'Eastern Residential Intensification Concept'.

At the District Structure Plan level, there is merit to protect future grade separated access at this point, however it is noted that this may be further rationalised through subsequent local structure planning to determine how this specific access is created, design and functions. It is recommended the associated notation will be modified to reflect this.

Other Modifications to Draft SSDSP3

The following minor modifications to the Draft SSDSP3 are recommended as a result of comments received during advertising -

- Adding a major shared path along the existing Wattleup Road alignment.
- Adding 'subject to Main Roads WA approval' to the end of Plan Note 2 in relation to the requirement for interim and possible future access to Rowley Road.
- Adding text to section '6.7.1 Rowley Road' stating that any interim construction or upgrades to Rowley Road not directly related to subdivision/development within the SSDSP3 area is the responsibility of the State and reiterating the City's preference for Main Roads WA to construct/upgrade Rowley Road using 'quiet grade' asphalt similar to that used in the construction of the nearby Kwinana Freeway.
- Rewording section '5.6.4 Neighbourhood/Local POS - ongoing planning' to reflect that POS design details at the LSP stage are only anticipated to be conceptual and as a minimum in terms of future maintenance, the City would expect LSP's to outline an approximate unit rate per hectare for maintenance of proposed POS areas.
- Removing the roundabout from the intersection of Hammond Road and Wattleup Road based on the requirement for traffic lights at this intersection.

Hammond Road extension

Some concern was raised in relation to the extent to which DCA's 9 and 10 would be contributing to the upgrading and extension of Hammond Road. It is therefore recommended that the wording of the Scheme provision outlined in Amendment No. 28 be modified to reflect the reality that the contributions will cover the construction of one carriage way comprised of two-lanes for the future Hammond Road which is in accordance with SPP3.6.

Contribution requirements for school sites

An objection was received in relation to the requirement for a non-government primary school to provide contributions in accordance with the proposed DCA. The basis for the proposed exemption was that the site was not a 'development' site in the traditional sense and would be providing a valuable social and community use. This is not considered appropriate by the City as the subject site will be receiving direct benefit from the infrastructure covered by the DCA's and the nature of the development is not reason enough to warrant an exemption from contributing to the DCA. It is also noted that development within the area would not be possible without the regional drainage network being

in place and there is no evidence that confirms that patrons of the school site will not be using the upgraded/widened Hammond Road.

DCA's are ultimately intended to provide for the equitable sharing of the costs of infrastructure and administrative costs between owners/developers, ensure that cost contributions are reasonably required as a result of the subdivision and development of land in the development contribution area and coordinate the timely provision of infrastructure.

The State Government High School (within DCA9) and Primary School (within DCA10) sites will be subject to DCA contribution requirements as they too will be receive benefit from the infrastructure associated with the DCA. A review of the previous proposed DCA9 and DCA10 provisions showed that these sites were originally to be exempt from the requirement to contribute. However it should be noted that this was based on the framework available prior to the implementation of SPP3.6.

If an exemption were to be afforded to the school sites then, despite the reality of the demand they will be creating, then the additional costs arising from the exemptions will be borne by all other landowners and their associated DCA contributions will be far greater. This scenario would fail the test of 'Need and Nexus' and 'Equity' in terms of SPP 3.6.

It is therefore recommended that provision 3 within the 'Method for calculating contributions' section of DCA's 9 and 10 being 'In relation to those portions of properties that have been identified on the Southern Suburbs District Structure Plan - Stage 3 for the purpose of a primary school, if these sites are not developed for school purposes in the future a proportional contribution to the regional drainage and road infrastructure will be required' be deleted. This will ensure consistency with SPP3.6.

Other Modifications to Draft SSDSP3

The following minor modifications to the Draft SSDSP3 are recommended as a result of comments received during advertising:

- In the interests of further clarity is it recommended that provision 1 point 6 within the 'Infrastructure and administrative items to be funded' section of DCA's 9 and 10 be reworded as follows -
- 'Land and infrastructure associated with the drainage of Hammond Road'.

Conclusion

It is recommended that Council adopt the SSDSP3 for the purposes of providing a guiding document to inform the preparation of future Local Structure Plans within the Southern Suburbs Stage 3 area, subject to the modifications outlined in this report and the recommendation.

Amendment No. 28 sets the framework for the coordinated development of the Southern Suburbs Stage 3 area in a manner that ensures the objectives of the SSDSP3 are achieved. It is therefore recommended that Council adopt the amendment for final approval, subject to modifications outlined in this report and the recommendation and refer the modified amendment to the WAPC for final consideration.

Strategic Plan/Policy Implications

Demographic Planning

- To ensure the planning of the City is based on an approach that has the potential to achieve high levels of convenience and prosperity for its citizens.

Infrastructure Development

- To provide an appropriate range of recreation areas that meets the needs of all age groups within the community.

Natural Environmental Management

- To ensure development of the district is undertaken in such a way that the balance between the natural and human environment is maintained.

Budget/Financial Implications

Having DCP's in place will enable infrastructure costs which benefit groups of landowners to be shared equitably. Without such DCP's in place, it is likely that either the first subdividing landowner/s will need to fund the infrastructure in its entirety, or potentially the City will be requested to fund. Either scenario is unappealing due to inequity arguments.

Accordingly, a DCP is seen as the most appropriate mechanism to fund common infrastructure. The current rate is identified with the attached Draft Cost Contribution Schedules.

Subdivision and development within the SSDSP3 area is also subject to the requirements of the City's Development Contribution Plan 13 - Community Infrastructure.

Legal Implications

Planning and Development Act 2005
 City of Cockburn Town Planning Scheme No. 3
Town Planning Regulations 1967

It is proposed to adopt the Draft District Structure Plan as a guiding document. It is important this distinction is made from a LSP, given the way in which the Scheme deals with a LSP as an extension to the statutory requirements of the Scheme.

Community Consultation

Community consultation was carried out for a period of 42 days, from 13 June to 25 July 2012. The proposals were advertised in the newspaper, on the City's website and letters were sent to affected landowners and government/servicing authorities in accordance with the Scheme requirements. A landowners' information evening was also held on the 11 July 2012. A total of 25 submissions were received.

Analysis of the submissions has been undertaken within the 'Report' section above, as well as the attached Schedule of Submissions.

Attachment(s)

1. Location Plan
2. Draft Southern Suburbs District Structure Plan - Stage 3 Map
3. Draft Southern Suburbs District Structure Plan - Stage 3 Report
4. Scheme Amendment No. 28 Report
5. Draft Cost Contribution Schedules - DCA 9 and DCA 10
6. Schedule of Submissions

Advice to Proponent(s)/Submissioners

Those who lodged a submission on the proposal/s have been advised that this matter is to be considered at the 13 September 2012 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

15. FINANCE AND CORPORATE SERVICES DIVISION ISSUES

15.1 (OCM 13/09/2012) - LIST OF CREDITORS PAID - JULY 2012 (FS/L/001) (N MAURICIO) (ATTACH)

RECOMMENDATION

That Council receive the List of Creditors Paid for July 2012, as attached to the Agenda.

COUNCIL DECISION

Background

It is a requirement of the Local Government (Financial Management) Regulations 1996, that a List of Creditors be compiled each month and provided to Council.

Submission

N/A

Report

The List of Accounts for July 2012 is attached to the Agenda for consideration. The list contains details of payments made by the City in relation to goods and services received by the City.

Strategic Plan/Policy Implications

Governance Excellence

- To conduct Council business in open public forums and to manage Council affairs by employing publicly accountable practices.

Budget/Financial Implications

N/A

Legal Implications

N/A

Community Consultation

N/A

Attachment(s)

List of Creditors Paid – July 2012.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

15.2 (OCM 13/09/2012) - STATEMENT OF FINANCIAL ACTIVITY AND ASSOCIATED REPORTS - JULY 2012 (FS/S/001) (N MAURICIO) (ATTACH)

RECOMMENDATION

That Council receive the Statement of Financial Activity and associated reports for July 2012.

COUNCIL DECISION

Background

Regulations 1996 prescribes that a local government is to prepare each month a Statement of Financial Activity.

Regulation 34(2) requires the Statement of Financial Activity to be accompanied by documents containing:–

- (a) details of the composition of the closing net current assets (less restricted and committed assets);

- (b) explanations for each material variance identified between YTD budgets and actuals; and
- (c) any other supporting information considered relevant by the local government.

Regulation 34(4)(a) prescribes that the Statement of Financial Activity and accompanying documents be presented to Council within 2 months after the end of the month to which the statement relates.

The regulations require the information reported in the statement to be shown either by nature and type, statutory program or business unit. The City chooses to report the information according to its organisational business structure, as well as by nature and type.

Financial Management Regulation 34(5) requires Council to annually set a materiality threshold for the purpose of disclosing budget variance details. To this end, Council has adopted a materiality threshold variance of \$100,000 from the corresponding base amount for the 2012/13 financial year.

Submission

N/A

Report

Closing Funds

The City's closing municipal position of \$78.0M was \$8.5M higher than the revised YTD budget target of \$69.5M. This is mainly comprised of underspending within the City's capital program, which is typical for July. The operating result has also contributed \$3.9M to this variance.

The budgeted year end closing funds position (currently at \$29k) will fluctuate throughout the year as it is impacted by various Council decisions and minor system adjustments and corrections. Details of these are outlined in Note 3 to the financial report.

Operating Revenue

Operating revenue at \$76.0M is tracking ahead of budget by \$1.5M. Key factors are \$0.8M of above budget Waste Services fees & charges revenue and \$0.5M of Human Services grants also ahead of budget.

Details of material variances are disclosed in the agenda attachment.

Operating Expenditure

Operating expenditure (including depreciation) is under budget overall by \$2.4M. This tends to be the case each July as all efforts at this time of year are focused towards finalising the accounts for the previous year. Suppliers also tend to focus on invoicing for the previous financial year with \$1.8M of the variance coming from materials and contracts.

With the exception of Waste Service (\$28k over budget), all service units are well within their budgetary limits for operational expenditure. Parks has the greatest surplus variance at \$0.5M.

Details of material variances within the service units are disclosed in the agenda attachment.

The following table shows budgetary performance from a nature or type perspective:

Nature or Type Classification	Actual	Amended Budget	Variance to Budget
	\$	\$	%
Employee Costs	\$2.86M	\$3.19M	10.4%
Materials and Contracts	\$1.29M	\$3.16M	59.2%
Utilities	\$0.25M	\$0.37M	33.0%
Insurances	\$0.99M	\$1.02M	3.6%
Other Expenses	\$0.80M	\$0.56M	-41.9%
Depreciation (non cash)	\$1.59M	\$1.85M	13.9%

Given the early stage of the year, percentage variances appear significant but will only become more meaningful and relevant as the year progresses.

Capital Program

The City's capital budget has hardly incurred expenditure at \$0.6M of the YTD budget of \$6.3M. Several significant projects were back invoiced up to June 30 with July invoicing being compromised. However, August should show more solid expenditure numbers.

The more significant project spending variances are disclosed in the attached CW Variance analysis report.

Turning to capital income, settlement of land sales for the Grandpre Crs and Bourbon St developments came in at \$1.3M for the month, \$1.1M ahead of the YTD budget. These proceeds will be transferred to the City's Land Development and Investment Fund Reserve in August.

Cash & Investments

Council's cash and current/non-current investment holdings reduced to \$76.0M (from \$83.9M at the end of June). This still reflects a healthy cash position ahead of major capital spending on projects such as the Cockburn Integrated Community Facilities and Coogee Beach Integrated Community Facility. This balance is expected to rise to over \$100m at the peak of the collection of the City's rates in Aug/Sep.

\$62.5M of the total cash and investment holding represents the City's cash reserves.

Another \$7.0M of the cash position represents funds held for other restricted purposes such as bonds, restricted grants and capital infrastructure contributions. The remaining balance of \$6.5M represents the cash and investment component of the City's working capital, available to fund ongoing operations and the municipal funded portion of the capital program. At this stage, this mainly comprises the carried forward municipal funding for the uncompleted works program from 2011/12.

The City's investment portfolio made an annualised return of 5.68% for the month, versus the benchmark BBSW performance of 3.5%.

The majority of investments held continue to be in term deposit (TD) products placed with highly rated APRA (Australian Prudential Regulation Authority) regulated Australian banks. These are mainly invested for terms of between three and six months, as this is where the value lies in the yield curve.

Whilst the Reserve Bank has reduced interest rates over the past several months by 75 basis points the City's investment strategy of rolling over TD's for six monthly terms has somewhat buffered the City's investment performance from a sudden fall. The 2012/13 budget has been premised on a reduced investment earnings potential as interest rates are likely to continue facing some downward pressure and the balance of funds held will diminish as a result of the large capital works budget.

Description of Graphs and Charts

There is a bar graph tracking Business Unit operating expenditure against budget. This provides a very quick view of how the different units are tracking and the comparative size of their budgets.

The Capital Expenditure graph tracks the YTD capital spends against the budget. It also includes an additional trend line for the total of YTD actual expenditure and committed orders. This gives a better

indication of how the capital budget is being exhausted, rather than just purely actual cost alone.

A liquidity graph shows the level of Council's net current position (adjusted for restricted assets) and trends this against previous years. This gives a good indication of Council's capacity to meet its financial commitments over the course of the year.

Council's overall cash and investments position is provided in a line graph with a comparison against the YTD budget and the previous year's position at the same time.

Pie charts included show the break-up of actual operating income and expenditure by nature and type and the make-up of Council's current assets and liabilities (comprising the net current position).

Strategic Plan/Policy Implications

Governance Excellence

- To conduct Council business in open public forums and to manage Council affairs by employing publicly accountable practices.

Budget/Financial Implications

Other material variances identified of a permanent nature (ie. not due to timing issues) may impact on Council's final budget position (depending upon the nature of the item).

Legal Implications

N/A

Community Consultation

N/A

Attachment(s)

Statement of Financial Activity and Associated Statements – July 2012.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

15.3 (OCM 13/09/2012) - OBJECTION TO IMPOSITION OF DIFFERENTIAL (COMMERCIAL) RATES (FS/T/001) (S DOWNING) (ATTACH)

RECOMMENDATION

That Council

- (1) dismiss the objection; and
- (2) advise the objector of Council's decision.

COUNCIL DECISION

Background

The City has received an objection to the 2012/13 rates assessments for 17 Winterfold Road Hamilton Hill and 316 Rockingham Road Spearwood as per section 6. of the Local Government Act.

Submission

N/A

Report

The sole ratepayer of 17 Winterfold Road, Hamilton Hill (a former house modified to accommodate 30 persons and 316 Rockingham Road Spearwood, a former doctors surgery (and commercial premises) has been modified to accommodate 38 persons) has objected to the rates assessment under section 6.76 (1)(b) of the Local Government Act:

6.76. Grounds of objection:

- (1) A person may, in accordance with this section, object to the rate record of a local government on the ground —
 - (a) that there is an error in the rate record —
 - (i) with respect to the identity of the owner or occupier of any land; or

(ii) on the basis that the land or part of the land is not rateable land;

or

(b) if the local government imposes a differential general rate, that the characteristics of the land recorded in the rate record as the basis for imposing that rate should be deleted and other characteristics substituted.

The ratepayer is not objecting to part 1(a) above but rather to part (b) under differential rates. This is because the characteristics of the property have determined that the properties are commercial properties. The properties have planning approval for 30 and 38 rooms and to operate as lodging houses. This is a commercial concern not to be confused with a rental residential investment property. This is more akin to a one star suburban hotel. This is business venture. It might meet the market by providing lower cost accommodation but it is not a charity or not for profit business. Hence the characteristics of a lodging house which are akin to a low cost "hotel".

As such the Council has applied the commercial rate in the dollar to the valuation provided by the Valuer General. No objection has been lodged with the Valuer General nor does there appear to be one going to be lodged.

The objector has advised that the Synergy charge electricity at residential tariffs rather than commercial tariffs. Electricity tariffs are similar for small businesses as for residential eg. our buildings. The City has similar tariffs for some of its buildings to residential homes.

Questions raised by Mr Townes:

1. Lodging house is private rental accommodation not a commercial concern.

Response:

This is a business of a lodging house, it is not a rental property. It involves multiple individuals renting rooms, using a communal living area and have provided a linen service. The applicant is the planning application makes note of occupancy targets and strong occupancy rates.

2. The lodging house does not charge GST.

Response:

The City can't offer a comment on individual tax positions of ratepayers.

3. Synergy charge residential tariffs.

Response:

The City has some of its buildings on similar tariffs.

4. Why should residents of lodging house pay commercial rates and not residential rates.

Response:

The Council is not rating the residents, it is rating the owner of the property who runs a commercial lodging house. As a commercial entity, the rates form a deductible expense against income derived from renting out rooms.

The reason why the objection has been rejected is that the basis of the objection claims the lodging house properties are not commercial, a key characteristic. The properties exist to make a profit as they are not charities or not for profit facilities.

The outcome of an objection under section 6.76 (1) (b) is if the characteristics are incorrectly recorded, the objection succeeds in changing the characteristics. The characteristics lead to the conclusion that the two properties are commercial in nature.

Legal advice received from Mr Denis Mcleod of McLeods (a copy is attached) confirms this position.

The outcome of the rejection of the objection could be a challenge in the State Administration Tribunal, a position Mr McLeod believes could also defend.

Strategic Plan/Policy Implications

N/A

Budget/Financial Implications

The following table indicates the difference between residential and commercial rates:

Property	GRV	Current Rates	Residential
		Commercial	Rates
Healy Road Hamilton Hill	\$78,000	\$5,708.82	\$3,790.02
Rockingham Road Spearwood	\$103,740	\$7,592.73	\$5,040.73
Cost per room per week - Healy		\$3.66	\$2.43
Cost per room per week - Rockingham		\$3.84	\$2.55

Legal Implications

6.77. Review of decision of Local Government on objection:

Any person who is dissatisfied with the decision of a local government on an objection by that person under section 6.76 may, within 42 days (or such further period as the State Administrative Tribunal, for reasonable cause shown by the person, allows) after service of notice of the decision, apply to the State Administrative Tribunal for a review of the decision.

Community Consultation

N/A

Attachment(s)

1. Letter from the objector Mr Jason Townes
2. Legal Advice from Mr Denis McLeod of McLeods (provided to Elected Members under separate confidential cover)

Advice to Proponent(s)/Submissioners

The Proponent has been advised that this matter is to be considered at the September 2012, Ordinary Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

16. ENGINEERING AND WORKS DIVISION ISSUES

16.1 (OCM 13/09/2012) - TENDER NO. RFT 13/2012 - ELECTRICAL SERVICES (INDUSTRIAL, COMMERCIAL & DOMESTIC) (RFT 13/2012) (P CRABBE) (ATTACH)

RECOMMENDATION

That Council accepts a panel arrangement for tender number RFT 13/2012 – Electrical Services (Industrial, Commercial & Domestic) consisting of the following Tenderers:

1. Northlake Electrical Pty Ltd;
2. JF Covich & Co Pty Ltd;
3. Pearmans Electrical & Mechanical Services Pty Ltd ;

for a period of three (3) years for an estimated total Contract value of \$1,210,325 GST Inclusive (\$1,100,295 GST Excl.) in accordance with the submitted Schedule of Rates and additional schedule of rates for additional services and determining variations.

COUNCIL DECISION**Background**

The current Electrical Services Contract (No. RFT 34/2007) is due to expire on 31st December 2012 with the existing Contractor providing for much of the City's facility requirements. Additionally it was identified that Parks requirements for bore pump and reticulation electrical works has increased in value over the previous three years to a point where these works are now also required to be included within a service contract.

The schedule of rates for these works will enable the Capital Works and Operational programmes to be delivered according to approved schedules. Both Facilities and Parks business units are required to service Capital Works programs and Operational programmes through performing a significant number of electrical projects during each financial year that require the utilisation of service providers to perform a range of services.

The electrical services required include:

- Maintenance, repair and servicing of existing & new electrical installations.
- New minor and major services installations – design, supply & testing.
- Removal of redundant installations as required.
- Consumer mains, Switchboards including alterations to existing.
- Submains cabling, distribution boards, earthing.
- Wiring, cables, enclosures and supports.
- Lighting, internal, external, street & flood (including piles, foundations, fittings and lamps).
- Testing of electrical equipment, emergency lighting and exit signs and completing relevant documentation.
- Metered pump cubicles, Bore pump controls.
- Underground electric supply.
- Access panels; and all other miscellaneous items.

The tender specifications have been written for the purpose of seeking suitably qualified, skilled and licensed Contractors capable of offering

diverse, reliable and effective electrical services to meet the City's needs.

The Request for tender (RFT) called for submissions from electrical contractors for a period of three (3) years with Principal instigated options to extend the period for a subsequent one (1) year period and up to an additional twelve (12) months after that to a maximum of five (5) years. A specification and tender document was prepared in conjunction with Procurement Services and tenders were subsequently called.

Tender Number RFT 13/2012 Electrical Services (Industrial, Commercial & Domestic) three (3) Year Contract was advertised on Wednesday, 4th July 2012 in the Local Government Tenders section of "The West Australian" newspaper.

The tender was also displayed on the City's e-Tendering website between the 4th July and 19th July 2012.

Submission

Tenders closed at 2:00 p.m. (AWST) on Wednesday 19th July 2012 with tender submissions being received from the following seventeen (17) companies:

	Company Name	Trading Name
1	Leedenn & Lorson Pty Ltd ATFT Graessin Family Trust	Elexacom
2	JF Covich & Co Pty Ltd	
3	Pearmans Electrical & Mechanical Services Pty Ltd	
4	Enviro Contracting Pty Ltd	
5	M & IE Holdings Pty Ltd	
6	Northlake Electrical Pty Ltd	
7	David Holden Pty Ltd	Holdens Electrical Contracting
8	Measurement Control Engineering Pty Ltd	
9	EAMCO Pty Ltd	EOS Electrical
10	Interlec (WA) Pty Ltd	Interlec Pty Ltd
11	Datatel Communications Pty Ltd	
12	Cockburn Group P/l	
13	KRE Electrical Pty Ltd	
14	Mondale Pty Ltd	Electek
15	Nilsen WA Pty Ltd	
16	Selectro Services Pty Ltd	
17	Thai Pacific Products Pty Ltd	Longmont

Report

Evaluation Criteria

1. Tender submissions were assessed against the following criteria:

Evaluation Criteria	Weighting Percentage
Demonstrated Experience	25%
Key Personnel Skills and Experience	15%
Tenderer's Resources	10%
Response Times	10%
Local Area Knowledge	10%
Tendered Price – Estimated Lump Sum	30%
TOTAL	100%

Tender Intent / Requirement

The purpose of this Tender was to select experienced, competent and reliable Electrical Contractors to perform Electrical Services (Industrial, Commercial & Domestic) within the City of Cockburn.

Evaluation Panel

The Evaluated panel consisted of the following City of Cockburn Officers.

1. Philip Crabbe – Facilities & Plant Manager (Chair);
2. Anton Lees – Manager Parks & Environment; and
3. Robert Avard – Manager Community Services

Scoring Table

Tenderer's Name	Percentage Score		
	Non-Cost Evaluation	Cost Evaluation	Total
	70%	30%	100%
Northlake Electrical Pty Ltd**	39.89	28.60	68.49
J F Covich & Co Pty Ltd**	39.92	26.22	66.14
Pearmans Electrical & Mechanical Services Pty Ltd**	38.57	27.13	65.70

Elexacom	40.50	22.98	63.48
Nilsen WA Pty Ltd	34.67	28.54	63.21
Datatel Communications Pty Ltd	33.91	28.66	62.57
Enviro Contracting Pty Ltd	33.84	28.16	62.00
Interlec Pty Ltd	37.52	23.53	61.05
KRE Electrical Pty Ltd	26.27	30.00	56.27
Electek	30.52	24.75	55.27
M & IE Holdings Pty Ltd	30.08	22.85	52.93
Selectro Services Pty Ltd	26.71	26.11	52.82
EOS Electrical	27.90	23.33	51.23
Measurement Control Engineering Pty Ltd	20.40	26.59	46.99
Holdens Electrical Contracting	26.33	20.38	46.71
Longmont	8.07	17.96	26.03

** Recommended Submissions

Evaluation Criteria Assessment

Demonstrated Experience

All sixteen (16) tenderers provided varying levels of detail relating to their demonstrated experience, track record of achieving outcomes and organisational structure. Northlake Electrical Pty Ltd, JF Covich & Co Pty Ltd, Pearmans Electrical & Mechanical Pty Ltd and Elexacom clearly demonstrated to the evaluation panel that they had the required level of experience to perform the tasks specified.

All Tenderers provided substantial documentation of similar works performed for other Local Government Authorities and private corporations with a focus on project management and achieving outcomes.

Longmont, Holdens Electrical Contracting, KRE Electrical, Measurement Control Engineering and Selectro Services only provided minimal documentation relating to some of the elements required within demonstrated experience, thus impacting on their score for this criteria.

Longmont failed to provide suitable levels of documentation on issues arising from previous works and demonstrated competency with regard to energy efficient products and referees thus having a direct bearing on their score for this criteria.

Key Personal Skills and Experience

Northlake Electrical Pty Ltd, JF Covich & Co Pty Ltd, Pearmans Electrical & Mechanical Pty Ltd, Datatel Communications Pty Ltd, Electek, Elexacom, Enviro Contracting Pty Ltd, Interlec Pty Ltd and Nilsen WA Pty Ltd all clearly demonstrated they had the necessary skills and experience in their respective key personnel.

Fifteen (15) of the tenderers provided documentation of personnel that had performed works in similar projects including well documented curriculum vitae's.

Longmont did not provide sufficient documentation regarding their key personnel and their experience which is reflected in their score for this criteria.

Tenderer's Resources

Fifteen (15) of the Tenderers provided documentation on the resources required to perform works. However, Northlake Electrical Pty Ltd, JF Covich & Co Pty Ltd, Pearmans Electrical & Mechanical Pty Ltd, Datatel Communications Pty Ltd, Elexacom, and Nilsen WA Pty Ltd all clearly demonstrated to the evaluation panel that they had the appropriate quantity of resources and contingency measures in place to perform the required services.

Longmont did not provide adequate details of their resources to allow the panel to determine their ability to supply and sustain the resources required for the services.

Response Times

Fifteen (15) of the Tenderers provided documentation on response times relating to emergency call outs and programmed maintenance requirements and demonstrated to the evaluation panel that they would be able to perform the works under the Contract.

Longmont did not provide any documentation on response times.

Local Area Knowledge

All Sixteen (16) Tenderers demonstrated degrees of knowledge of the Cockburn area to the evaluation panel.

Six (6) of the Tenderers – JF Covich & Co Pty Ltd, Northlake Electrical Pty Ltd, Nilsen WA Pty Ltd, Pearmans Electrical & Mechanical Pty Ltd, Interlec Pty Ltd and Elaxacom demonstrated a good to high level of local area knowledge to the evaluation panel.

Tendered Price

The Tender required the submission of a schedule of rates for licensed electricians, apprentice electricians and trade assistant to be used to provide the required services.

The price schedule completed by each Tenderer was tallied based on an indicative total number of hours of work coupled with indicative value of materials to provide a single total price which was then scored against each Tender submission.

Summation

The tender submissions received from:

1. Northlake Electrical Pty Ltd;
2. JF Covich & Co Pty Ltd; and
3. Pearmans Electrical & Mechanical Services Pty Ltd

Have been assessed by the evaluation panel as being the most advantageous to Council to perform the Electrical Services (Industrial, Commercial & Domestic) contract based on the following:

- Significant demonstrated experience in performing works of similar scale.
- A range of personnel that have the experience to undertake the wide range of works required.
- The required resources to complete the works.
- The required measures in place to perform emergency callouts and programmed maintenance along with very sound knowledge of the Cockburn area.
- The tendered prices submitted are considered fair and reasonable for the wide range of works to be performed.

A panel of three (3) contractors would also provide the most beneficial and functional mechanism to complete various levels of electrical works offering maximum flexibility according to predetermined timeframes within current and future capital and Operational programmes. Therefore the panel recommends the Council award a Panel Contract to these Three (3) Tenderers.

Strategic Plan/Policy Implications

Infrastructure Development

- To maintain and improve our existing building and parks electrical infrastructure in a safe and reliable manner for public use, whilst continuously looking at products with high levels of energy efficiencies.

Lifestyle and Aspiration Achievement

- To deliver our services and to manage resources in a way that is cost effective without compromising quality.

Governance Excellence

- To conduct Council business in open public forums and to manage Council affairs by employing publicly accountable practices.

Budget/Financial Implications

The required level of funding is allocated for each financial year for Electrical Services (Industrial, Commercial and Domestic) under operational and capital works expenditure.

The schedule of rates submitted will be utilised in the budgeting process to determine the funding required for individual projects.

The table below details indicative expenditure for Electrical Services (industrial, commercial and domestic) over the past five (5) years.

Financial Year	Indicative Turnover (inc GST)
2006/07	\$399,000
2007/08	\$468,000
2008/09	\$548,000
2009/10	\$571,000
2010/11	\$642,000
Total 2006 to 2011	\$2,280,000
Five Year Average (per annum)	\$525,600

Legal Implications

Section 3.57 of the Local Government Act 1995 and Part 4 of the Local Government (Functions and General) Regulations 1996 refers.

Community Consultation

N/A

Attachment(s)

The following Confidential Attachments are provided under a separate cover:

1. Consolidated Score Sheet
2. Compliance Criteria
3. Tendered Prices

Advice to Proponent(s)/Submissioners

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 13 September 2012 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil

16.2 (OCM 13/09/2012) - TEMPORARY CLOSURE OF INTERSECTION OF HAMMOND ROAD AND FERTITO ENTRANCE IN SUCCESS TO THE PASSAGE OF VEHICLES - APPLICANT: CITY OF COCKBURN (ES/R/002) (J KIURSKI & C MACMILLAN) (ATTACH)

RECOMMENDATION

That Council in accordance with Section 3.50 of the Local Government Act 1995, approves the temporary closure of the intersection Fertito Entrance and Hammond Road for a period of up to three (3) months from 1 October 2102, subject to:

1. There being no substantial objection received as a result of advertising in a local newspaper.
2. There being no substantial objection from service authorities, emergency service or adjoining owners.
3. An approved traffic management plan to monitor and control traffic movements due to the closure.
4. The proponent being fully responsible for public liability and damages arising from works; and managing detours.
5. Alternate access to be clearly signed.

COUNCIL DECISION

Background

Widening Hammond Road from Russell Road to Bartram Road is a project approved in FY 2011/12 and carried forward in the 2012/13 road construction budget. Project design and relocation of existing Telstra and Water Corporation services have been completed and the construction work is ready to commence.

The proposed construction works include road widening between Russell Road and Bartram Road and reconstruction of existing intersections Hammond Rd/Awl Way and Hammond Road/Fertito. The intersection of Hammond Road and Fertito Entrance is to be reconstructed and left in and left out access is to be provided.

Submission

N/A

Report

City requires approval for the temporary road closure of Fertito Entrance at the intersection Hammond Road from 1st October 2012 to 31st December 2012 to complete the Hammond Road project.

The following construction options were explored by the Engineering team prior to settling on the fully isolated Hammond Road and Fertito Entrance intersection closure option:

- Half road closure of Hammond Road at intersection with Fertito Entrance

This option would not be feasible due to current intersection configurations and level difference between existing and proposed pavement. This may have been an alternative if the proposed pavement was wholly to one side of the existing pavement. This option is impracticable.

- Retention of Hammond Road – Fertito Entrance Intersection

The option of leaving the existing Fertito Entrance intersection in use with access to the side track is not practicable from a construction point of view. This option has a greatly increased risk of accident. The intersection is already regarded as a high risk exit due to the speed environment on Hammond Road.

While there would be speed restriction in place by way of signage, these are not physical restrictions. Driver behaviour in unfamiliar or altered traffic conditions poses a high risk of accident, most likely through speed, in this case. The best way to reduce or avoid this risk is to eliminate the hazard. This is done by closing Firtito Entrance for approximately 12 weeks.

Closure of Fertito Entrance will require the re-routing of traffic to Tangaroo Boulevard/Hammond Road roundabout (the plan of the proposed traffic diversion attached). These diversions would increase traffic on Tangaroo Boulevard, which could be inconvenient for residents but is manageable and the preferred scenario. A Traffic Management Plan has been prepared which will show how access will be maintained throughout the closure period. The City will maintain closure for the shortest period of time possible to minimise disruption.

A concept plan of the proposed road closure and detour is attached to the Agenda.

Strategic Plan/Policy Implications

Demographic Planning

- To ensure development will enhance the levels of amenity currently enjoyed by the community.

Transport Optimisation

- To construct and maintain roads which are convenient and safe for vehicles, cyclists and pedestrians.

Budget/Financial Implications

The cost of the Hammond Road widening and associated work is in the annual budget allocations for road construction capital works budgets – CW 2365.

Legal Implications

Section 3.50 of the Local Government Act.

Community Consultation

To be advertised in a local newspaper and service authorities, emergency services and adjoining owners advised.

As Fertito Entrance is not a bus route, a consultation with the Public Transport Authority or Southern Coast Transit is not required.

Attachment(s)

Plan for proposed realignment and closure of intersection.

Advice to Proponent(s)/Applicant

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

17. COMMUNITY SERVICES DIVISION ISSUES

17.1 (OCM 13/09/2012) - ADOPTION OF COMMUNICATION STRATEGY 2012-2017 (CC/P/002) (S SEYMOUR-EYLES) (ATTACH)

RECOMMENDATION

That Council:

1. adopts the Communication Strategy 2012-2017, as attached to the agenda; and
2. includes any financial implications for the plan for consideration in Council's Strategic and Annual Budget planning documents.

COUNCIL DECISION

Background

The City of Cockburn's Communication Strategy 2006-2012 was adopted to take account of changing customer expectations and the increased use of technology in the field of communications.

Some outcomes include the introduction of a Contact Centre in 2008, which now takes and completes 80% of calls to the City and the availability of the majority of public documents on the City's website,

which has resulted in corporate documents becoming more “reader friendly” over time.

The 2006-2012 strategy has now run its course with all outcomes completed or otherwise attended to, as detailed in Appendix 1 of the new Draft Communications Strategy 2012-2017. The next five years presents new communications challenges for the City.

The City’s population continues to grow and much of the City is under development. Periods of great change will create a higher level of more complex communications requirements and issues.

Report

The City undertakes comprehensive annual community perception surveys, annual KPI customer satisfaction surveys and focus groups with residents on the subject of communications. The 2012-2017 Communications Strategy has been developed using:

- The results of this community research.
- A more contemporary approach to public relations.
- More attention to alignment with other strategies, plans, policies and charters (as detailed on p8 of the 2012-2017 Strategy).

This focus has identified the following principles to guide the strategy: Communications will:

- be accessible
- be honest
- be transparent
- be customer-focused
- be accountable
- present a more “human” side to council
- tackle the “hard” issues.

The updated strategy seeks to:

- Address the best and most sustainable ways for the City to communicate the breadth of facilities, events and services the organisation provides to its diverse Community;
- Provide a framework for communicating the City’s current and future strategic plan objectives;
- Inform stakeholders on areas which are of concern to them;
- Consider ways to increase two-way dialogue, and improve consultation and engagement between the City and its key stakeholders to improve the perceptions of transparency, honesty and trust; and,

- Build on the strengths of existing and emerging communication channels.

This updated strategy includes measureable actions, linked to the following ten communication outcomes:

1. To develop staff knowledge of City activity, the City's vision and future direction, through internal Communications.
2. To improve the City's community consultation and engagement processes.
3. To improve the community's knowledge of the City's Vision for the area.
4. To improve stakeholder awareness of the City's services and community facilities.
5. To keep stakeholders up-to-date with the status of major projects and issues.
6. To promote City-run events.
7. To improve the perception of local government through the continual improvement of customer service.
8. To ensure that the City of Cockburn primary brand is used consistently across all marketing communication and service points.
9. To achieve continual improvement in the perception of the City as a local government and the elected members as its leaders.
10. To ensure that communications material is accessible.

Each action identifies the estimated time for completion, the officer responsible, and the estimated budget implication. The strategy will be reviewed each year and updated in 2017.

Strategic Plan/Policy Implications

Governance Excellence

- To conduct Council business in open public forums and to manage Council affairs by employing publicly accountable practices.
- To maximise use of technology that contributes to the efficient delivery of Council's services.

Budget/Financial Implications

All actions which require additional Municipal resources will need to be considered by Council through its strategic and annual budget process. The majority of resource implications are relatively minor in nature, but will still require Council consideration for them to be progressed.

Legal Implications

N/A

Community Consultation

1. Annual Community Perceptions Surveys
2. Annual Customer Satisfaction KPI Surveys
3. Focus Groups August 2011
4. Internal Communications (staff) Survey – July 2012

Attachment(s)

1. Draft Strategy
2. Results of Focus Groups August 2011

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil

17.2 (OCM 13/09/2012) - AMENDMENT TO CITY OF COCKBURN LOCAL LAW - CONSTRUCTION OF FIRE BREAKS (FILE RS/J/002) (R AVARD) (ATTACH)

RECOMMENDATION

That Council pursuant to Section 3.12(4) of the Local Government Act, 1995:

- (1) adopt the City of Cockburn (Local Government Act) Amendment Local Law, 2012, as attached to the Agenda after having considered all submissions, incorporated appropriate amendments and determined that the Local Law is not significantly different from what was originally proposed; and
- (2) authorise the signing by the Mayor and Chief Executive Officer and affixing of the Common Seal to the Local Law.

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COUNCIL DECISION

Background

The City of Cockburn Local Laws, 2010, Sec 2A, stipulates the dates by which firebreaks and fire mitigation measures on land within the district must be completed and maintained. The relevant sections of the local laws are as follows:

Construction of Firebreaks

“2A.2 Construction of Firebreaks

All owners and occupiers of land within the district shall clear flammable matter from the land in accordance with the following requirements:

- (1) As to land which is 2032m² or less in area, or which is zoned “Residential” under the town planning scheme, the owner or occupier is to remove all the flammable matter from the whole of the property, except living trees, shrubs, plants under cultivation and lawns, by slashing or mowing the matter to a height of not more than 50 millimetres, or otherwise to the satisfaction of the local government or an authorised person, and the property is to be maintained to the standard so stated in this subsection for the duration of the period 1 October to 31 May each year.
- (2) As to land, which is greater than 2032m² in area, shall have a trafficable firebreak three (3) metres in width cleared to mineral earth subject to the following requirements:
 - (a) immediately inside all external boundaries of the land;
 - (b) immediately surrounding buildings (if any) situated on the land;
 - (c) immediately surrounding all fuel dumps and ramps (if any) on the land; and
 - (d) in any event, clear the firebreaks to the satisfaction of the local government or an authorised person.
- (3) In reference to subsection (2) all firebreaks must be cleared by the owner or occupier of the land on or before 30 November in any year, and thereafter be maintained by the owner or occupier

clear of flammable mater up to and including 31 March in the following year.

- (4) Where and owner occupier of land fails or neglects to comply with this Part of the Local Laws within the time specified, an authorised person may with such employees and/or contractors, vehicles and machinery as the authorised person deems necessary enter upon the land and do all such things as necessary to comply with this Local Law and may recover costs and expenses of doing so as a due debt from the owner or occupier of the land pursuant to the Act, in addition to any penalty which might be imposed.

Variation to Fire prevention Measures

2A.3. (1) If for any reason an owner or occupier considers it impractical to clear firebreaks in accordance with subsection (2) of section 2A.2, the owner or occupier may apply in writing to Council or an authorised person no later than 31 October in any year for approval to construct a firebreak in an alternative position on the land.”

Any person who fails to comply with any provisions of this Section commits an offence and a penalty shall be prescribed by the Bushfire Act 1954. The maximum fine is \$5,000.

At its meeting of April 2012 Council resolved as follows

- (1) proceed to amend the City of Cockburn (Local Government Act) Local Laws, 2010 Section 2A, as follows:
1. Clause 2A.1 – Delete the words after “between” and insert “1 November in any year until 31 May in the year following”.
 2. Clause 2A.2 (1) – Delete “1 October” and insert “1 November”.
 3. Clause 2A.2 (3) – Delete ‘30 November’ and insert ‘1 November’ and delete “31 March” and insert “31 May”.
 4. Clause 2A.3 (1) – Delete “31 October” and insert “1 October”.
- (2) in accordance with Section 3.12 of the Local Government Act, 1995, the above amendments, as shown in the attachment to the Agenda, be advertised for a minimum period of 6 weeks.

Submission

In accordance with the requirements of the Local Government Act the proposed changes to the Local Law was advertised in the Government Notices section of the West Australian on the 18th April 2012. An article was also placed in the Cockburn Soundings of June 2012 advising of the proposed change to the Local Law and copies sent to the relevant Ministers for consideration.

Formatting comments were received from the Minister for Local Government; Heritage, Citizenship and Multicultural Interest, and have now been incorporated into the proposed amendments.

Submissions were received from the Banjup Residents Group and Mr Stephen Dobson in a private capacity.

Report

The purpose of the amendment is to standardise the dates within the City of Cockburn for fire mitigation measures to be installed on specified land.

The effect of the amendment is to change the dates applicable to the installation of fire prevention measures to vacant (urban) and rural properties within the District.

The term firebreak is used for and includes the clearing of land to prevent fires such as required on land of less than 2032m².

At its meeting of 6 December 2011 the Cockburn Bushfire Reference Group supported the fire break period starting on the 1st November of each year. There was, however, no record in the minutes of the agreed date for the end of the required firebreak period as identified by Mr Dobson in his submission.

Representatives of the Banjup Residents Group (Inc) gave a presentation to the Cockburn Bushfire Advisory Reference Group at its meeting of 24 July 2012. There were a number of concerns raised by the Banjup Residents Group in relation to proposed changes to the fire break period. The key concerns of the Banjup Residents Group as presented to the Reference Group are as follows:

1. Lack of notice and consultation of the proposed changes with the Banjup Residents Group.

Council consideration of the changes to the firebreak period was following initial consultation with the Cockburn Bushfire Advisory Reference Group at its meeting of 6 December 2012. Changes to Local

Laws require public advertising and consultation, and hence the initial consideration by Council was the start of the process to recommend a change the firebreak period. The proposed changes affect all property owners within the City of Cockburn and no community group was considered more than any other.

2. The Banjup Residents Group believes that there is insufficient evidence that any wild fires have been caused by firebreaks not being in place prior to the existing firebreak season.

There is anecdotal evidence that the climate in the area in which Perth is located is becoming drier and hence the risk of wild fires is increasing. Firebreaks and the clearing of blocks are a mitigation strategy and are considered effective steps to reduce destructive wild fires and need to be taken with the onset of drier conditions.

3. The rainfall and temperatures for the Jandakot Airport Bureau of meteorology monitoring station for the past 16 years do not support a change to the firebreak period.

It is evident from the figures provided by the Banjup Residents Group that rainfall from year to year is highly variable and attempting to draw conclusions on rainfall figures over a short period of 16 years is not highly representative. December remains a month of very low rainfall notwithstanding that in some years there is an increase in rainfall. November is generally getting drier with again some years of higher rainfall. The fact remains that in any summer month in the Perth metropolitan area there may be unseasonal rainfall that can cause regrowth and the need for repeat firebreak clearing. Similarly, the rainfall in April and May is highly variable.

4. Fire break contractors have not been contacted in relation to the proposed changes to the firebreak season.

Firebreak contractors have been advised by the Chief Bushfire Officer and members of the Volunteer Bushfire Brigades that Council was prepared to consider a change to the firebreak installation season. The advice is that the contractors are able to operate within the requirements of the proposed changes to the Local Law. As the contacted contractors operate in the south metropolitan area of Perth it is plausible that the City of Cockburn, by having an earlier start date to its firebreak season, will allow the contractors to complete firebreaks prior to the adjoining local authorities which have a start date of 1 December. With the generally drier climate there are fewer areas in the District that are wet in November and should there be such areas, a variation can be readily obtained through the City. In summary, it is considered there are very few areas in the District that could not have firebreaks installed by 1 November in an 'average' rainfall season.

5. Standardising dates.

The Banjup Residents Group argue that there is no need to 'standardise' the firebreak period across the City as they believe that there is no confusion between the residential areas having a firebreak period that runs from the 31 October to the 31 May and in rural area from the 30 November to the 31 March. It is the view of the Reference Group that this is confusing for owners of properties in the City of Cockburn. The proposed change which aligns the requirements for property owners in both urban and rural areas in line is clear.

The start of the season being proposed (1 November) is not the same as the adjoining local authorities, but it does allow for consistency across the City and for firebreaks to be in place prior to the requirements of adjoining local authorities. It is worth noting that the City of Gosnells requires all land not General Rural or Special Rural to be compliant all year round.

The following table shows the dates for the firebreak season for adjoining local authorities that are similar to the City of Cockburn (ie having significant residential areas and rural/semi rural areas.)

Local Authority	Start	End	Area applicable
Current Cockburn	1 October	31 May	Residential area.
	30 November	31 March	Rural areas
Proposed Cockburn	1 November	31 May	All areas
Gosnells	30 November	30 April	General rural and special rural
	Fire breaks or land cleared all year	All year	All other areas (residential, commercial etc)
Kwinana	1 st December	31 st March	All areas

Armadale	30 November	14 March	All area
Rockingham	30 November	31 May	All areas
Mandurah	17 November	31 May	All areas

The Bushfire Advisory Reference Group unanimously resolved to support the fire break period for all areas within the City of Cockburn to be from the 1st November of one year to the 31st May of the following year.

Should Council resolve to amend the Local Law as proposed, there will be an impact on the inspection regime.

Currently, the appointed ranger carries out the inspections in the residential areas first, then moves to the rural areas when these areas are due to have their firebreaks in place. As the residential properties are less of a fire risk than the rural properties, it is proposed that the inspection be focussed on the rural areas and known high risk areas in the residential areas (such as large undeveloped properties and reserves). The inspection of small undeveloped residential lots will be performed as a result of complaints and by observation of rangers on normal patrol duties.

The long standing process in relation to 'fire break' infringements is as follows:

- Inspections are commenced by the appointed ranger for the various areas on the due dates (ie. 1 October and the 1 December of each year).
- If the owner's fire breaks were non-compliant in any of the last 3 years, they are issued an infringement and given 14 days to comply. If the property owner has been compliant before they are given 14 days to comply. The property is reinspected and if not compliant a contractor is brought for the work to be done and the ranger makes another inspection.

Where a property owner is not compliant and a contractor has to be brought in the ranger visits the property at least 3 times. This process requires a ranger to be taken off regular duties and to be put on fire break duties for the best part of 6 months. Further, the process takes so long that there are properties without compliant fire breaks well into February of each year, which is an obvious fire risk. To allow for the fire break inspections to be done more quickly, additional staff would be required unless the system can be streamlined. The technology used

by the appointed ranger allows for ready identification of property owners and the ability to generate correspondence. The primary reason for the time taken to carry out fire break inspections is the number of inspections required each year.

The risks of wild fires are well publicized and owners of properties are well informed through rates notices and public advertising of their responsibilities in relation to fire mitigation measures they must take on their properties. It is strongly recommended that the City put the responsibility to adhere to the requirements of the Fire Order on the owner, and it not be the responsibility to continually remind the owner of their legal responsibilities.

In summary, it is proposed that the following procedure be put in place in relation to fire break inspections in the district.

1. The City of Cockburn Local Law 2010 be amended to require all properties in the district to have fire mitigation requirements in place from the 1 November of each year to 31 May of the following year.
2. Authorised Officers carry out inspections of all properties in rural and semi rural areas and other known high risk areas in the District to ensure compliance with the requirements of the Local Law related to fire mitigation. All property owners found not to be compliant will be issued an infringement. Property owners can appeal against the infringement by application to the City of Cockburn's Rangers and Community Safety Services Manager. If they are unsuccessful in this appeal, they have recourse through the courts.
3. Firebreak and fire mitigation requirements in residential areas be monitored through reports from other parties and by observation by rangers while performing their usual duties. Following the initial firebreak inspections in rural areas, adherence to the requirements will be done through monitoring and observation by Rangers during their normal duties.

It is recommended that Council adopt the Local Law amendment as per Attachment 1 (Proposed Amendment – City of Cockburn (Local Government Act) Amendment Local Law 2012 – Firebreaks), as it does not significantly differ from what was originally proposed. The Local Law amendment can then be forwarded to the Minister of Local Government, Heritage, Citizenship and Multicultural Interests and following gazettal, to the Parliamentary Joint Standing Committee on Delegated Legislation for review.

Strategic Plan/Policy Implications

Infrastructure Development

- To construct and maintain parks and bushland reserves that are convenient and safe for public use, and do not compromise environmental management.

Governance Excellence

- To conduct Council business in open public forums and to manage Council affairs by employing publicly accountable practices.
- To provide effective monitoring and regulatory services that administer relevant legislation and local laws in a fair and impartial way.

Budget/Financial Implications

N/A

Legal Implications

The *Local Government Act, 1995*, section 3.12, states the requirements to be followed for proposed amendments to local laws.

Community Consultation

The amendment to a local Law requires public advertising and notification. The Cockburn Bushfire Volunteer Reference Group has advised on the proposals and their views are reflected in the report.

Attachment(s)

1. Proposed amendment – City of Cockburn (Local Government Act) Amendment Local Law 2012 – Firebreaks
2. Banjup Resident`s Group (Inc) Submission

Advice to Proponent(s)/Submissioners

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the September, 2012, Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

17.3 (OCM 13/09/2012) - TS COCKBURNS REQUEST TO RELOCATE TO KENT STREET SES BUILDING (FILE 22022027) (R AVARD) (ATTACH)

RECOMMENDATION

That Council defer consideration of the leasing of the Kent Street premises until the October 2012 Council meeting and advise the Cockburn TS Navel Cadets that it supports an arrangement which will allow the building to be used by both the TS Cockburn and the Spearwood Girl Guides or similar group.

COUNCIL DECISION

Background

The Cockburn Voluntary State Emergency Services (SES) Building on 15-19 Kent St, Spearwood WA 6163; a portion of Lot 7 on Plan P1764, will soon become vacant due to the relocation of the SES to the new facility on Poletti Road in Cockburn Central. The current conditions of the facilities have contributed to the need for the SES to relocate.

Submission

The City received a letter on 18 August 2012 from Lieutenant Hickey (ANC), Commanding Officer of Training Ship Cockburn. The letter conveying the units request to relocate to the SES building in Kent Street, Spearwood is attached.

Report

As detailed in the Phoenix Revitalisation Strategy, adopted by Council on the 14 May 2009, the SES building in Kent Street has been planned to be demolished and for the area to be returned to recreational space to compliment the adjoining lots of Beale Park and to meet the City's increasing need for recreational space. Beale Park is one of the City's largest and most used active reserves. As per the Sport and Recreation Strategic Plan, adopted in 2010, the upgrade would allow the reserve to accommodate premier league soccer; any additional open space would assist with this provision.

Currently the Cockburn Navel Cadets operate out of the South Coogee Agricultural Hall on Rockingham Road in Munster which they feel is isolated from the community and is not safe for the Cadets to travel to and from without supervision. For this reason most cadets travel some distance by private vehicle. Entering and exiting the facility is also a concern for some parents. The Unit's increasing assets have also meant that offsite storage is used to store the unit's water craft. The unit has prepared a comprehensive rationale supporting their proposed relocation and have also provided a letter of support from the Royal Australian Navy. There is scope for TS Cockburn Naval Cadets to share space provided that there is a secure lockable area for their equipment.

While the Cadets' proposal does have merit, the facility in question is in poor condition although still deemed structurally acceptable. The buildings are aging and do not contribute positively to the amenity of the area. A building inspection has been conducted and has shown that the facility is structurally acceptable although cracking to the western and southern external wall does present an area of structural concern. The steel lintels over the windows and door openings on the western and southern elevations are displaying signs of extensive corrosion. The down pipes are in poor condition and rusted out with downpipes missing and draining onto the soil. The structural condition survey recommends the following remediation works:

- Replace corroded lintels.
- Replace damaged brickwork.
- Replace damaged and missing downpipes.
- Connect downpipes to sub-surface drainage or install soak-wells at least 2 meters from the building.

There would be a need to install a sub meter to allow power usage to be identified for the building should it be leased.

It is estimated that these repairs and the sub meter would cost approximately \$30,000.00 to bring the facility up to tenantable standard.

Based on the attached report, the City's increasing need for recreation space and the planned upgrade for Beale Park, it is recommended that the facility is decommissioned in the future in accordance with the requirements to meet the recreation needs on Beale Park In the mean time however the building could be leased for a period of say 3 years as an interim arrangement.

The Spearwood Girl Guides have operated from the Joe Cooper Recreation Centre for many years and currently have 42 members aged between 5 and 17 years. Community group HALO currently

occupy a large part of the Joe Cooper building with other groups such as boxing. It is reasonable to suggest that the Girl Guides activities and their clientele are not readily compatible with other users of Joe Cooper. It is proposed that the Spearwood Girl Guides be given the opportunity to share space within the Kent Street premises as they have shown an interest in this proposal. The ability to share is quite practical as there is an enormous amount of secure storage space suitable for the Navel Cadets and numerous rooms in the building. The Spearwood Girl Guides meet on a Thursday night and the TS Cockburn on a Friday night.

To allow time to negotiate a share arrangement between the TS Cockburn and the Spearwood Guides it is proposed that this matter be deferred until the October meeting of Council.

Strategic Plan/Policy Implications

Infrastructure Development

- To construct and maintain community facilities that meet community needs.

Lifestyle and Aspiration Achievement

- To facilitate and provide an optimum range of community services and events.
- To identify community needs, aspirations, expectations and priorities for services that are required to meet the changing demographics of the district.

Budget/Financial Implications

The recommendation of the report will not result in any significant financial implications. Should Council resolve to proceed with a lease it would anticipate that the lease terms and conditions would be similar to that which applies for the use of the South Coogee Agricultural where the lease fee is a peppercorn and all out goings are funded by the Navel cadets.

An allocation could be made in the 2013/14 budget to address the immediate temporary maintenance issues with the building.

Legal Implications

N/A

Community Consultation

The Kent Street Building has been used for many years by the Cockburn Volunteer Emergency Service. The use of the building by the TS Cockburn navel cadets should it proceed would have little change to the impact on the local area.

Attachment(s)

Documentation supporting the relocation of TS Cockburn.

Advice to Proponent(s)/Submissioners

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the September, 2012 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

17.4 (OCM 13/09/2012) - TENDER RFP 01/2012 - LEASE LAND - AGED FRIENDLY DEVELOPMENT COOLBELLUP, WA (RFP 01/2012) (GBOWMAN) (ATTACH)

RECOMMENDATION

That Council

- (1) accept the proposal submitted by Bethanie Group Incorporated, for Proposal No. RFP 01/2012 - Lease Land - Aged Friendly Development, Coolbellup WA;
- (2) enter into negotiations with the Bethanie Group Inc regarding an agreement to lease in accordance with the request for proposal for the two (2) hectare lease site situated at the corner of Cordelia and Coolbellup Avenues in Coolbellup; and
- (3) require that the key terms and conditions of the agreement to lease be considered by Council at a later meeting.

COUNCIL DECISION

Background

At its Ordinary Council meeting in May 2011, (Minute No 4510), Council resolved in part to:

“(8) request that the Minister for Regional Development and Lands make available for purchase by the City of Cockburn an unencumbered 2 ha of portion of Crown Reserve 38243 (former Koorilla Primary School) utilising provisions of the Government Guidelines (Public Open Space) Policy, Disposal of Section 152 Planning and Development Act 2005 Reserves. The unencumbered 2 ha portion of Crown Reserve 38243 is to be consistent with that shown on the Koorilla School Site Structure Plan;

Subsequent to the Council decision, approval has been gained from the Minister to proceed in accordance with this decision.

The intent was for the City to seek proposals from suitably qualified and experienced not for profit organisations to develop the site inclusive of a Residential Aged Care facility and affordable Independent seniors living units. The successful respondent be required to enter into a lease agreement with Council with all costs for the development to be borne by the respondent. The expectation was for the successful respondent to source capital works funds to construct at its own cost a minimum of one (100) hundred beds of Residential Aged Care accommodation, and a suitable number of seniors independent living units with at least twenty percent of the independent seniors living units deemed to be affordable to low income households. The successful respondent would also have to demonstrate that they are able to manage both facilities into the future with priority of access provided to Coolbellup and Cockburn residents.

Consequently, request for proposals for the proposed Lease Area were sought from not for profit organisations who could demonstrate that they are suitably qualified and experienced in the construction and operational management of affordable Seniors Independent Living Units, and Residential Aged Care facilities.

Request For Proposal Number RFP 01-2012 - Lease Land - Aged Friendly Development, Coolbellup WA was advertised on Wednesday 30 May 2012 in the Local Government Tenders section of “The West Australian: newspaper. It was also displayed on the City’s E-Tendering website between the 30 May and 26 June 2012.

Submission

Request for Proposals closed at 2:00 p.m. (AWST) on Tuesday 26 June 2012 and two (2) submissions were received from:

1. Bethanie Group Inc
2. Southern Cross Care (WA) Inc

Report

Evaluation Criteria

Evaluation Criteria	Weighting Percentage
Company Profile	20%
Key Personnel Skills and Experience	10%
Interpretation, Methodology, Implementation	40%
Financial Position	30%
TOTAL	100%

Request For Proposal Intent/Requirements

The City of Cockburn sought proposals from suitably qualified and experienced not for profit organisations interested in a leasing opportunity, which requires the successful Respondent to establish an Age Friendly accommodation development on the lease site.

The two (2) hectare site is in the Coolbellup Town Centre precinct located on the corner of Cordelia and Coolbellup Avenues, Coolbellup WA and is adjacent to residential, medical, retail, commercial, and community facilities. The City required an outward facing high quality development to be achieved on the site.

The City sought innovative proposals that will provide a minimum requirement of a one hundred (100) bed Residential Aged Care facility and affordable independent seniors living units with at least twenty (20) percent proposed to be affordable to low income households.

The successful respondent was required to demonstrate that they have suitably qualified staff who have experience in project managing a significant capital works development in the order of approximately fifty (50) Million dollars.

The respondents were also required to provide a concept plan of the site with sufficient detail and a construction cost estimate from a quantity surveyor. The concept plan needed to provide the orientation

of the buildings, car parking, entry and exit routes, and outdoor spaces, the number of residential aged care beds and the number of independent seniors living units. They also had to provide details of the location and number of affordable independent living units for low income households.

The successful respondent would have to demonstrate that they are able to source capital and operational funding, and have experience in managing both types of facilities into the future. The successful respondent was also required to demonstrate that they would provide priority access to Coolbellup and Cockburn residents to the proposed accommodation.

Due to tenure issues with the site the land will not be available for transfer to the City for approximately twelve (12) months. The agreement to lease provides the opportunity to select the preferred respondent and commence the planning and design stage of the development prior to the lease site being transferred which will reduce the construction timeline significantly. An agreement to lease will also provide the successful respondent the ability to commence negotiations with funding bodies such as the Department of Housing and the Department of Health and Ageing.

Evaluation Panel

The tender submissions were evaluated by:

1. Gail Bowman- Manager Human Services
2. Andrew Trosic- Manager Strategic Planning
3. Jill Zumach- Human services Operations Manager

Scoring Table - Combined Totals

Respondent's Name	Percentage Score		
	Non-Cost Evaluation	Cost Evaluation	Total
	100%	0%	100%
Bethanie Group Inc **	84.44%	0%	84.44%
Southerncross Care (WA) Inc	73.89%	0%	73.89%

** Recommended Submission

The evaluation panel recommends the Bethanie Group Inc submission.

Evaluation Criteria Assessment

Company Profile

Both submissions were considered to have relevant organisational structure, not for profit status, relevant experience and staff availability. Bethanie Group Inc demonstrated more experience as a Growth Housing provider for the Department of Housing.

Key Personnel Skills and Experience

Both submissions outlined that they had sufficient, and suitably qualified and experienced personnel to undertake their proposal.

Interpretation, Methodology, and Implementation

Bethanie Group Inc provided a superior outward facing, concept plan highly suited to the site. The Bethanie Group Inc proposal exceeded the City's minimum requirements for twenty (20) percent Affordable Independent Living Units for low income households by providing thirty three (33) percent, and their submission also exceeded the minimum requirements for one hundred (100) Residential Aged Care Beds through provision of one hundred and twelve (112) beds. The proposal was innovative and provided demonstrated community benefits. They also clearly demonstrated that they would provide priority access to Coolbellup and Cockburn Residents. Bethanie Inc provided a clear methodology regarding how they would ensure that thirty three percent of the independent living units would be affordable to low income households.

Southerncross Care (WA) Inc proposal did not commit to the minimum one hundred (100) bed Residential Aged Care Development through provision of only sixty (60) beds, but exceeded the twenty (20) percent affordable Independent living unit criteria. The methodology regarding how the proponent would ensure the rent is affordable to low income households for the term of the proposed lease was not demonstrated. Both proponents demonstrated that they had previous experience in the construction and management of similar facilities.

Financial Position

Bethanie Group Inc provided a capital works project plan that demonstrated when and from what source capital works funding would be secured within the required timeframe for each stage of the five (5) year development.

Both proponents demonstrated their ability to gain Commonwealth funding for the Residential Aged Care Development they proposed.

Both proponents demonstrated that their organisation's financial position was stable and viable.

Summation

Both compliant tenders are considered to have the capacity to meet the City's requirements as detailed in the specifications as well as comply with the General and Special Conditions of Contract as stated in the request for proposal document.

Bethanie Group Inc provided the best assessment against the selection criteria. Bethanie Group Inc provided a superior concept plan for the site and their proposal exceeded all the minimum requirements sought by the City. Consequently, their tender should be supported and it is therefore recommended that the City commence negotiations for the agreement to lease.

Strategic Plan/Policy Implications

Demographic Planning

- To ensure development will enhance the levels of amenity currently enjoyed by the community.

Lifestyle and Aspiration Achievement

- To facilitate and provide an optimum range of community services and events.
- To identify community needs, aspirations, expectations and priorities for services that are required to meet the changing demographics of the district.

Budget/Financial Implications

All terms and conditions including the rent component of the lease will be considered by Council at a later meeting.

Legal Implications

Section 3.57 of the Local Government Act 1995 and Part 4 of the Local Government (Functions and General) Regulations 1996 refers.

Community Consultation

Community consultation was undertaken during the Structure Plan process. No further community consultation was required.

Attachment(s)

The following Confidential Attachments are provided under a separate cover:

1. Consolidated Evaluation Panel Score Sheet
2. Adopted Structure Plan for lease area
3. Lease area
4. Advertisement authorisation

Advice to Proponent(s)/Submissioners

Those who lodged a proposal submission have been advised that this matter is to be considered at the September 2012 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

18. EXECUTIVE DIVISION ISSUES

19. MOTIONS OF WHICH PREVIOUS NOTICE HAS BEEN GIVEN

20. NOTICES OF MOTION GIVEN AT THE MEETING FOR CONSIDERATION AT NEXT MEETING

21. NEW BUSINESS OF AN URGENT NATURE INTRODUCED BY COUNCILLORS OR OFFICERS

22. MATTERS TO BE NOTED FOR INVESTIGATION, WITHOUT DEBATE

23. CONFIDENTIAL BUSINESS

24 (OCM 13/09/2012) - RESOLUTION OF COMPLIANCE (SECTION 3.18(3), LOCAL GOVERNMENT ACT 1995)

RECOMMENDATION

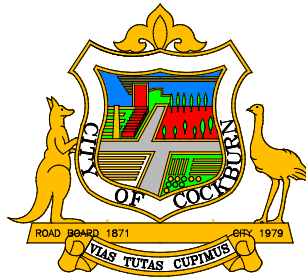
That Council is satisfied that resolutions carried at this Meeting and applicable to items concerning Council provided services and facilities, are:-

- (1) integrated and co-ordinated, so far as practicable, with any provided by the Commonwealth, the State or any public body;
- (2) not duplicated, to an extent Council considers inappropriate, services or facilities as provided by the Commonwealth, the State or any other body or person, whether public or private; and
- (3) managed efficiently and effectively.

COUNCIL DECISION

25. CLOSURE OF MEETING

CITY OF COCKBURN



ORDINARY COUNCIL

ATTACHMENTS

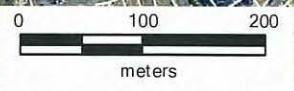
FOR

THURSDAY, 13 SEPTEMBER 2012

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**Lots 21, 22 and 23
(Formerly Lots 909 and 910)**



**Amended DAP and Jetty
Design Guidelines Stage 4A
PORT COOGEE**

PRINTED ON:
Tuesday, 28 August 2012

SCALE = 1:6207

DISCLAIMER - The City of Cockburn provides the information contained herein and bears no responsibility or liability whatsoever for any errors, faults, defects or omissions of information contained in this document.



Detailed Area Plan R-Code Variation

The District Town Planning Scheme and R-Codes are varied as described below:

R-CODING
Density Coding is R25

SCHEME AND RESIDENTIAL DESIGN CODE VARIATIONS

The requirements to consult with adjoining or other land owners to achieve a variation to the R Codes is not required where the design complies with the following standards.

DESIGN ELEMENTS

- All dwellings must be located within the building envelopes depicted on the DAP.
- Only non-habitable lightweight shadecover and gazebos structures are permitted within the rear setback (refer Dwelling Setbacks section to determine required setback). Structures shall not exceed a maximum height of 3m above finished lot level. Building materials, form and colour palette shall be complementary to the design of the dwelling.
- No sheds or storage structures are permitted in the rear setback. All storage areas shall be within the designated building envelope and incorporated into the main dwelling.
- No blank walls are permitted on boundaries of corner lots 906, 923, 940 and 948, walls that interface with POS shall be articulated and also meet the following design element criteria. Balconies are encouraged.
- Dwellings shall address the Primary Street (Chelydra Point), Public Open Space (POS) and the waterway (where applicable) by way of design, fenestration, entry and must contain major opening(s) to a living area and/or master bedroom.
- All dwellings abutting POS shall be suitably designed and orientated to ensure passive surveillance. Dwellings shall have one or more major opening(s) to a habitable room (being a lounge or living room and not to include bedrooms) and an outdoor living area facing the Public Open Space.
- Any exposed parapet wall on a common boundary shall be suitably finished to match the external walls of the dwelling, unless otherwise agreed with the adjoining property owner.
- At least one balcony greater than 10 m² is to be provided to the Primary Street elevation of each dwelling.
- A balcony with an area of 10 m² or greater may be included as part of the minimum open space requirement equal to the area of that balcony.
- A maximum site cover measuring 70% of the dry lot area is permitted.
- Element 6.8.1 of the R Codes applies to major openings facing side boundaries outside the rear setback area only. Rear setback areas (abutting POS and/or the waterway) are exempted from the requirements of Element 6.8.1.

DWELLING SETBACKS

- Setbacks for development shall be in accordance with the following:
(Except where specified below all other setbacks shall be in accordance with the R-Codes)
- The dwelling setback from the primary street shall be a minimum of 1.5m and maximum of 3m.
- A balcony (complying with the specified Design Elements criteria) is permitted to project over the dwelling/garage to within 0.5m of the front lot boundary.
- Side**
 - Dwelling upper levels shall be setback 2.0m from the western lot boundary beyond 15m of the front boundary (where indicated on the DAP).
 - A nil setback is permitted for walls on the western lot side boundary (where indicated on the DAP) for a maximum length determined by the required front and rear setbacks.
 - A nil setback is permitted for walls on the eastern lot side boundary (where indicated on the DAP) for a length of 15m from the front boundary less the required front setback
 - For Lots 21-23 and 50 only, dwelling upper levels shall be setback 2.0m from one side boundary only (where indicated on the DAP) for a length of 15m from the front boundary less the required front setback.
 - A nil setback will be permitted for one side boundary only, this may be nominated from the options displayed on the DAP. No combination of nil setbacks will be permitted except for lots 21-23, 50, 906, 923, 940, and 948.
- Rear**
 - On lots 924-940 and 948 the minimum rear dwelling setback shall be 3m. Any lightweight structure (as described under Design Elements) shall be setback a minimum of 2m from the rear boundary.
 - On lots 21-23, 50-51 and 906-923, the minimum rear dwelling setback shall be 4m from the dry lot boundary (as defined on the DAP). Any lightweight structure (as described under Design Elements) shall be setback a minimum of 2m from the rear dry lot boundary.

DWELLING HEIGHT

- A maximum building height of 2 storeys (+loft) with top of roof pitch at 10m above the finished lot level is permitted, roof terraces and all lightweight structures (highest point) are permitted.
- Non-habitable levels, partially or wholly below finished lot level shall not be deemed storeys and shall be excluded from the height calculation.

GARAGES AND VEHICULAR ACCESS

- The garage shall not dominate the front elevation of the dwelling. To comply with this requirement, the garage shall be setback a minimum of 0.5m behind the proposed dwelling setback. Consideration may be given to other means by which the requirement for garages not to dominate the front elevation can be achieved.
- For lots with 10m frontage, a garage setback of 4.5m from the Primary Street is encouraged to provide off street visitor parking.
- Development above garages may overhang the garage front setback requirements and extend out to the dwelling setback.
- Garage openings shall be limited to total maximum width of 6m in total.
- Carports are not permitted.

FENCING

- For lots 21-23, 50-51 and 906-923, fence height to the dry lot boundary (as defined on the DAP) shall be a maximum of 1.2m above the rear setback lot level (RL+1.25m). The fence must be visually permeable and incorporate elements such as glass panels, battens, screens, shutters or louvres.
- For lots 21-23, 50-51 and 906-923, the side boundary fence within the rear setback (4m) shall be a maximum height of 1.2m above the rear setback lot level (RL+1.25m).
- For Lots 50 and 51, the rear common boundary (4.0m east west section) fence shall be a maximum height of 1.8m and suitably finished to match external walls of the dwelling upon Lot 50.
- For lots 924-940 and 948, the side boundary fence within the rear setback (3m) shall be a maximum height no greater than the rear fence.
- For lots 924-940 and 948, fencing to the POS is provided by the developer. No modifications are to be affected apart from maintenance and repair in materials that are substantially identical with those used in the original construction.
- All other fencing to lot boundaries shall be in accordance with specifications detailed in Port Coogee Guidelines - Appendix C.

FINISHED LOT LEVELS

- Finished ground floor levels shall be no greater than 0.5m above the finished lot level, no brick build-up or additional fill is permitted for dwelling construction.
- All habitable rooms shall have a minimum floor level of RL2.5m AHD.
- For lots 21-23, 50-51 and 906-923, the finished lot level of RL1.25m AHD within the 4m rear building setback exclusion area shall be maintained, except for the specific purpose of access steps where earthworks shall be within 0.5m of the finished lot level (RL1.25m AHD).
- For lots 21-23, 50-51 and 906-923, all works proposed within the 4m rear building setback area (building exclusion zone) combined with potential pedestrian loading allowance, shall not exceed a total loading of 5kPa (0.5 Tonne/m²). As such, the total combined outcome (including, but not limited to, any proposed lightweight structure, landscaping works and pedestrian loading) within this area must be considered to enable engineering certification of final loading.
- For lots 21-23, 50-51 and 906-923, no excavation (including for the purposes of access steps or landscaping) within the 4m rear building setback area shall be below RL0.75m AHD to preserve the geogrid soil reinforcement.
- All applications to Council are to include a site feature survey with levels shown relative to AHD.

RETAINING WALLS

- Construction including load bearing walls on a nil setback must comply with engineering requirements associated with any retaining/feature walls present.
- Dwellings are generally to be setback 1.0m from retaining/feature walls for single storey dwellings and 1.5m to double storey dwellings. Certification for load bearing walls must be obtained from an independent practising Structural Engineer in relation to final dwelling proximity to retaining/feature wall.

DRAINAGE

- The stormwater drainage system design requires that all stormwater shall be managed by soakage within the lot. Discharge to the waterway is not permitted.

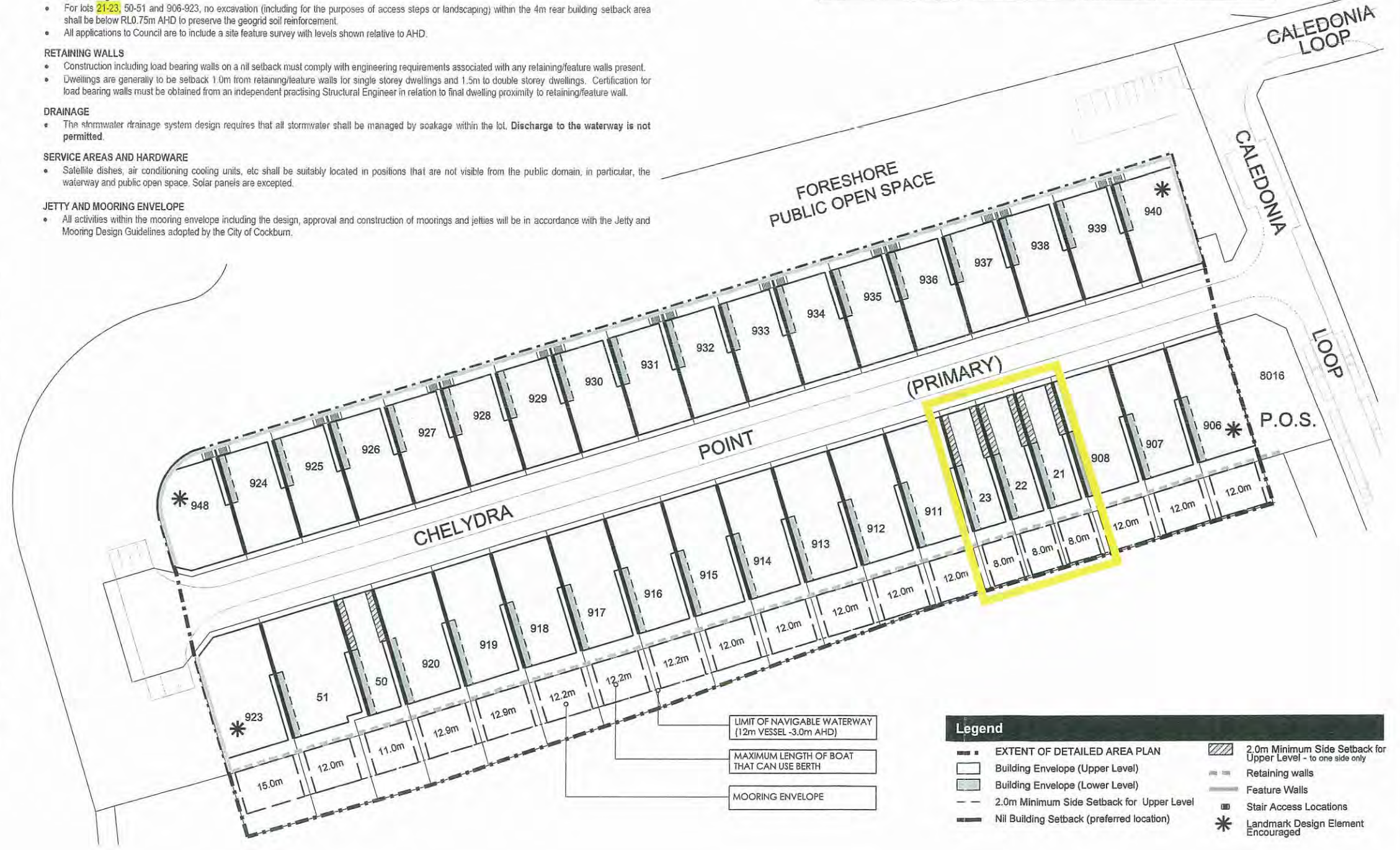
SERVICE AREAS AND HARDWARE

- Satellite dishes, air conditioning cooling units, etc shall be suitably located in positions that are not visible from the public domain, in particular, the waterway and public open space. Solar panels are excepted.

JETTY AND MOORING ENVELOPE

- All activities within the mooring envelope including the design, approval and construction of moorings and jetties will be in accordance with the Jetty and Mooring Design Guidelines adopted by the City of Cockburn.

Location Plan



Legend

- EXTENT OF DETAILED AREA PLAN
- Building Envelope (Upper Level)
- Building Envelope (Lower Level)
- 2.0m Minimum Side Setback for Upper Level
- Nil Building Setback (preferred location)
- 2.0m Minimum Side Setback for Upper Level - to one side only
- Retaining walls
- Feature Walls
- Stair Access Locations
- Landmark Design Element Encouraged

Detailed Area Plan - Peninsula Lots (Stage 4A) - (Sheet 1 of 2)
PORT COOGEE

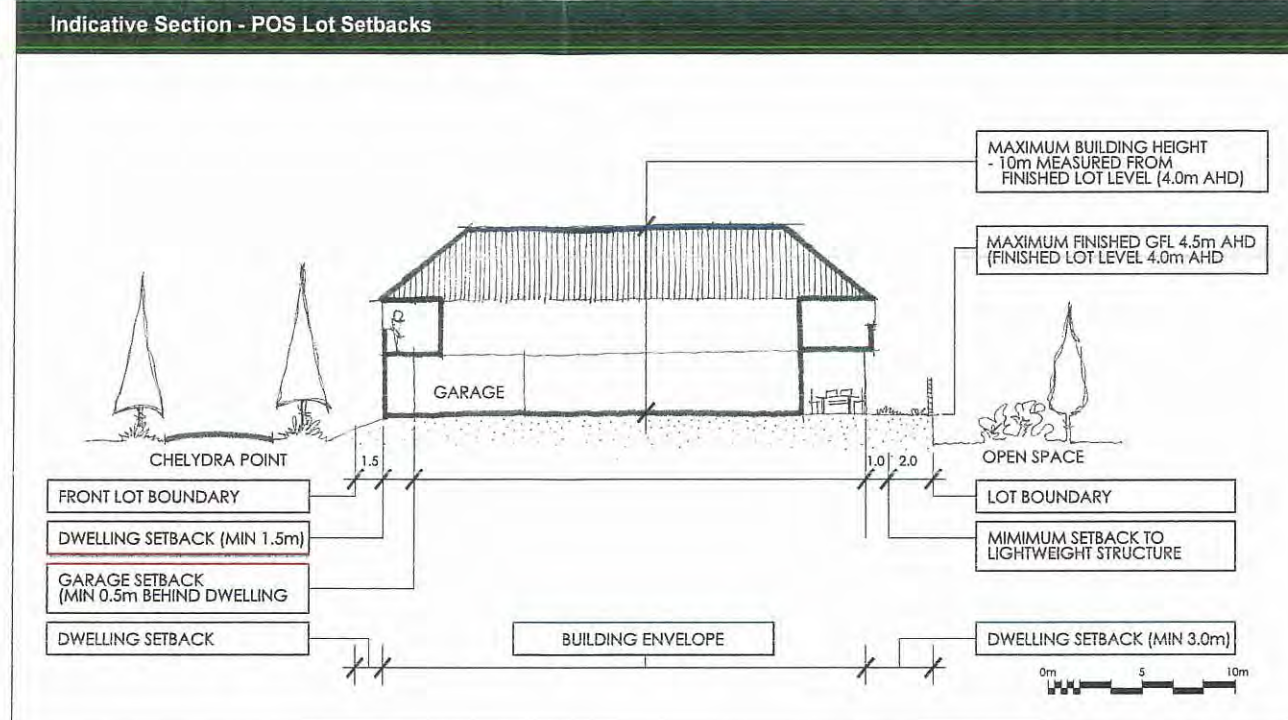
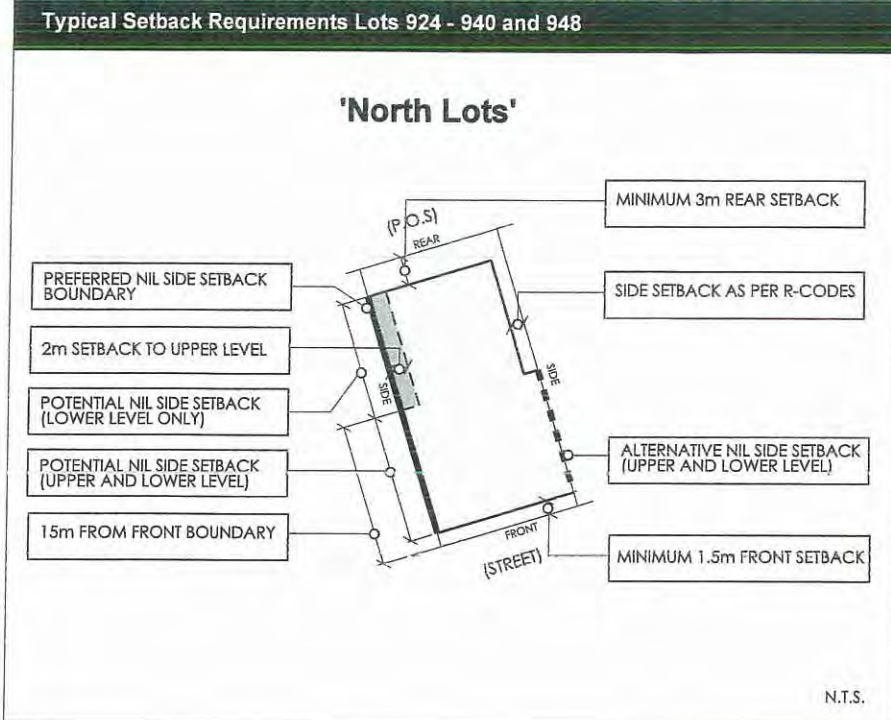
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date: 31/07/2012
projection: PCG 94

designed: MB
checked: WH
drawn: NM

scale: 1:1200@A3
0 12 24m

Taylor Burrell Barnell Town Planning & Design
187 Roberts Road Subiaco Western Australia 6008
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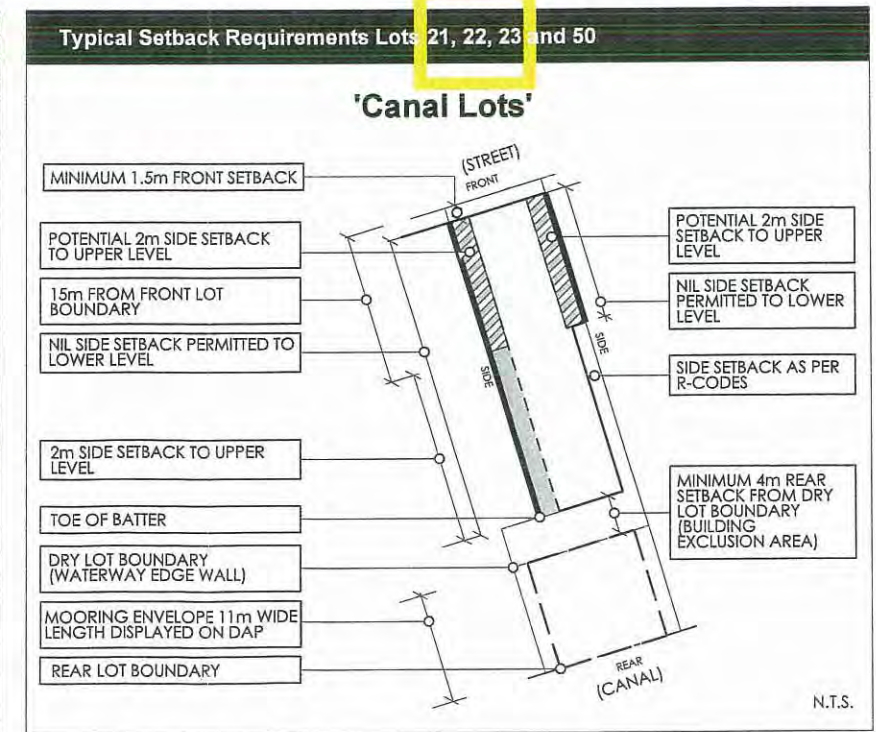
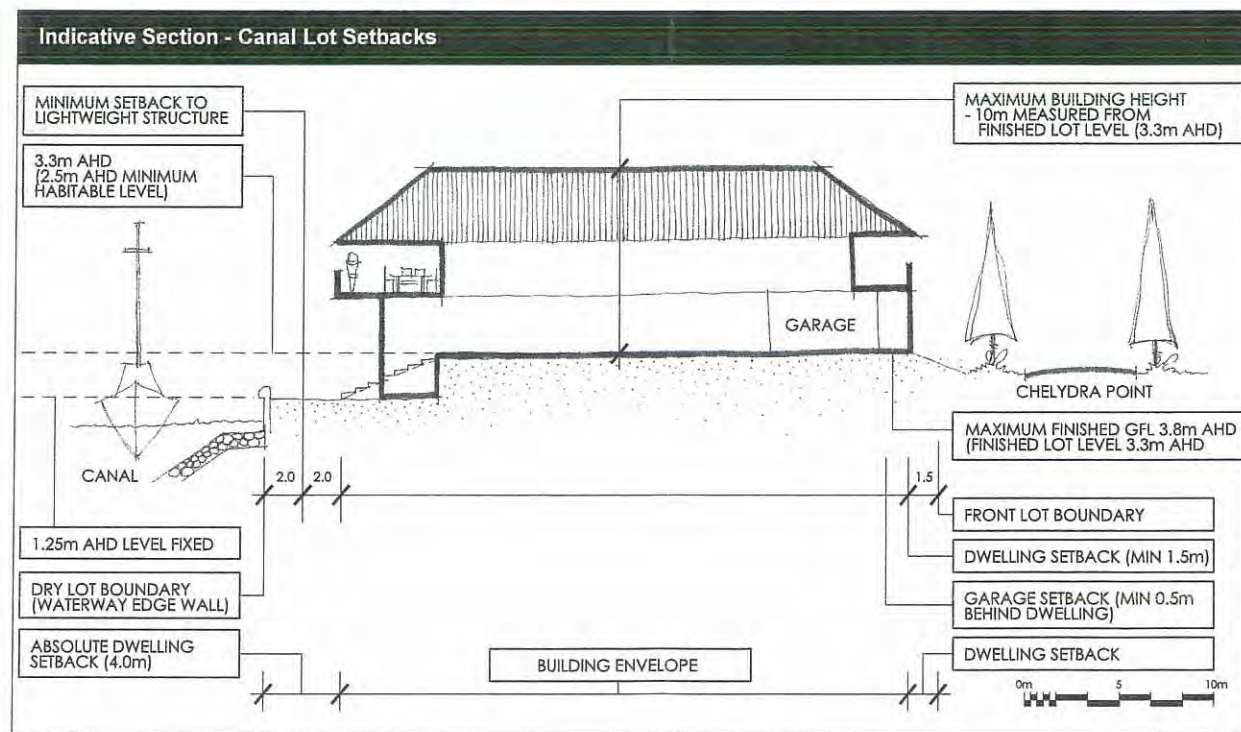
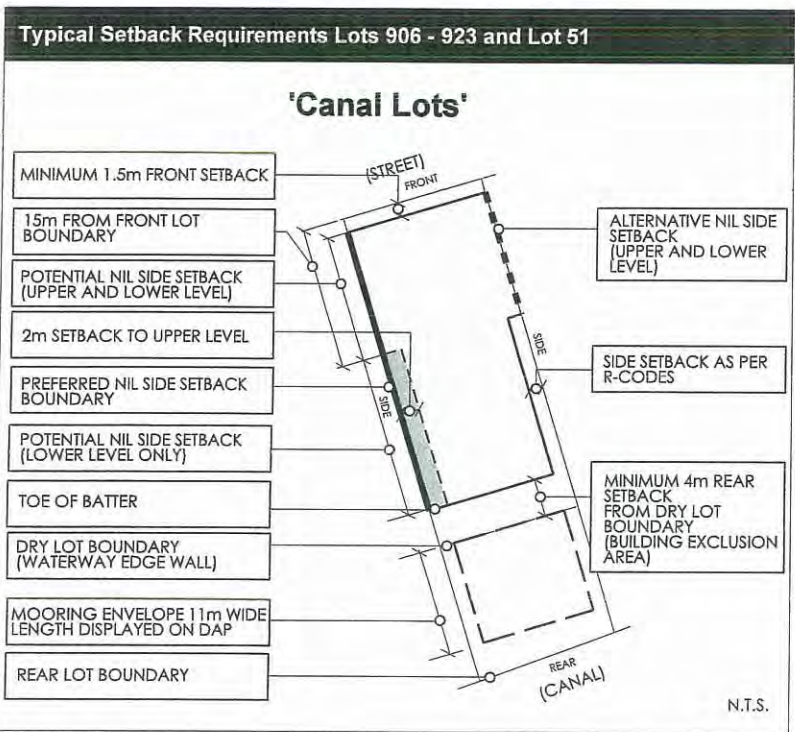


Endorsement Table

This Detailed Area Plan has been adopted by Council and signed by the Principal Planner:

Principal Planner _____

Date _____



**PORT COOGEE
JETTY DESIGN GUIDELINES**

for

PENINSULA LOTS

**STAGE 4A
AUGUST 2012**

PORT COOGEE – JETTY DESIGN GUIDELINES

1. Purpose

The purpose of these guidelines is to provide Lot Owners with sufficient information such that Jetties can be built adjacent to waterfront Lots in accordance with the Developer's requirements. These guidelines shall be adhered to by all Lot owners that propose to construct a Jetty and/or moor a vessel within their allotted Mooring Envelope. The construction and use of any Jetty or Pontoon and Lot Owners activities upon a Lot shall also be in accordance with City of Cockburn Jetties, Waterways and Marina Local Law.

2. Approval Process

All proposed Pontoon and Jetty arrangements must conform to these guidelines and all relevant Australian Standards. The following items are required:

- A planning application shall be prepared and submitted to the City of Cockburn for approval prior to construction/erection of any Pontoon or Jetty within the Jetty/Pontoon Envelope of a Lot.
- A building licence application shall be prepared and submitted to the City of Cockburn for approval prior to the construction of any Pontoon or Jetty. All applications must be supported by certified structural engineering details for the proposed construction.
- All Jetties and Pontoons require a jetty licence from the Department of Transport (DoT) before construction. Lot Owners shall be responsible for obtaining and paying for the jetty licence and any associated fees.
- Any modification to constructed Jetties or Pontoons requires approval from the City of Cockburn and the DoT prior to the proposed modification being undertaken.
- A building licence must be obtained prior to the construction of Mooring Piles.
- Waterway Edge Walls are not to be altered, extended or removed without the prior written approval of the Developer or the Waterways Manager.
- Pontoons and Jetties shall be constructed in accordance with the design guidelines and requirements set out herewith.
- Limits apply on the size of boats that may enter the waterways and be moored at a Lot. Maximum permitted vessel sizes through the navigable areas of Port Coogee Marina are shown on the attached layout plan (Appendix A).
- The Waterways Manager reserves the right to install navigational aids and/or signage within or adjacent to individual Mooring Envelopes. In this case the Lot will contain an easement to allow access for these to be installed and maintained by the Waterways Manager.
- Proposed Pontoons and Jetties shall not interfere with or obstruct any existing or proposed navigation aids and/or signage. All Jetty designs shall be suitably certified by structural engineers or recognised suppliers.
- Submissions for approvals shall include details of proposed materials and colour schemes.

PORT COOGEE – JETTY DESIGN GUIDELINES

3. Definitions

AHD means Australian Height Datum.

Bank Seat means a structure installed behind the Waterway Edge Wall on a Lot which provides a foundation for fixing Gangways to the shore.

Beam means the greatest width of the vessel including all permanent attachments.

Boat Draft means the vertical distance from the deepest part of the boat hull to the waterline.

Boat Length means the length overall measured between extremes, including bowsprits and stern davits/marlin boards.

Developer means Port Catherine Developments Pty Ltd and such other entity or authority that is from time to time charged with responsibility for managing the waterways adjacent to or near the Lots.

Gangway means a structure that provides pedestrian access between a fixed jetty or shore and a Jetty or Pontoon.

Jetty means a fixed (non-floating) structure providing landing, docking or mooring facilities.

Jetty/Pontoon Envelope is the area within which a Jetty or Pontoon and associated Gangway may be constructed and wholly contained.

Lot means lots 911 to 920 on Deposited Plan 58338 and lots 906 to 908 and lot 923 on Deposited Plan 58344 and lots 50 and 51 on Deposited Plan 70898 and lots 21 to 23 on Deposited Plan 73147.

Lot Owner means the registered proprietor from time to time of a Lot.

Mooring Envelope means the area of a Lot designated for the mooring of vessels.

Mooring Pile means piles used to secure a vessel by attaching the vessel to the piles, ensuring that the vessel does not drift from the Mooring Envelope.

Pontoon means a floating platform or similar structure providing landing, docking or mooring facilities.

Waterway Edge Wall means the revetment wall separating the land filled part of a Lot from the submerged part of the Lot together with any associated retaining wall constructed within the Lot.

Waterways Manager means the party charged with responsibility for managing the waterways adjacent to or near the Lots.

LAT is the lowest astronomical tide.

HAT is the highest astronomical tide.

4. Restrictions

- Jetties or Pontoons placed perpendicular to the Lot are **not** permitted.
- Jetties or Pontoons (including gangways) shall **not** damage or impose any additional load on the waterway edge wall.
- Gangways shall be a minimum of 0.9 metres and a maximum of 2.0 metres wide and Lot Owners are **not** permitted to deck the area between the vertical edge of the Revetment Wall and the Pontoon or Jetty.

PORT COOGEE – JETTY DESIGN GUIDELINES

- Dry storage of vessels is **not** permitted on Jetties or pontoons or on those parts of the Lots immediately adjacent to the waterway.
- Boat lifting devices or structures may not be installed on or adjacent to Jetties or pontoons or on those parts of the Lots which are adjacent to a waterway.
- Jetties may not be roofed (permanently or temporarily) or have structures built upon them (including, but not limited to, patios, pergolas, shade sails and tarpaulins).
- Installation of sewage pump-out or refuelling systems is **not** permitted on Jetties or pontoons constructed within Lots.
- Lighting is permissible (subject to approval by City of Cockburn) and shall be in full accordance with the latest publication of AS1158.
- Lot Owners shall be responsible for ensuring that there is sufficient depth of water in which to moor their vessel based on the geometry of the Waterway Edge Wall. Details can be seen in Appendix B.
- The maximum length of a vessel moored at a Jetty or Pontoon shall be determined by the length of the Mooring Envelope. All vessels shall be fully contained within the Mooring Envelope. Further details on the Peninsula Mooring Envelopes are included in Appendix C.

5. Requirements

5.1 *Jetties and Pontoons:*

- All Jetties and pontoons shall be 'L' or 'T' type with a suitable safe Gangway from the Lot.
- All Jetties, pontoons and Gangways must be located within the Jetty/Pontoon Envelope.
- Two permissible options for Jetty/Pontoon types (A and B) are shown in the attached figures (Figure 1 and 2 in Appendix B). Designs for Jetties and pontoons within Lots shall adhere to one of these options. The options are: piled floating Pontoon and non-piled floating Pontoon (typically only suitable for boats with a beam less than 3.8m). Refer to Appendix B and C for further details on Jetty types and permissible dimensions.
- The length of the Jetty including any supporting structure may be the full length of the Jetty/Pontoon Envelope, but typically the Jetty length shall be no greater than 80% of the moored vessel's length (AS3962).
- Lot Owners shall ensure that the construction of the Jetty and Gangway does not compromise the integrity of the revetment and Lot Owners shall be responsible for the reinstatement of the revetment after any construction works.
- Gangway, Jetty and Pontoon dimensions shall be in full accordance with the most recent publication of AS3962.
- Use of the vertical edge of the Revetment Wall must not defer from the original intended use as a retaining wall. It is the responsibility of the Lot Owner to obtain approval from a structural engineer and to install any additional Bank Seats required for fixing the Gangways (and struts if Type B).
- with the Lot Owner.

PORT COOGEE – JETTY DESIGN GUIDELINES

5.2 *Revetment Wall:*

- The Waterway Edge Wall lies fully within the Lots and responsibility for maintenance and ensuring the integrity of the Waterway Edge Wall remains with the Lot Owner.
- The Lot Owner shall not obstruct the “u-shape” opening within the vertical edge of the Revetment Wall which has been constructed for the purposes of allowing drainage.
- Details of the Waterway Edge Wall construction are shown in Appendix D. The attention of the Lot Owners is brought to the concrete works below ground that form part of the Revetment Wall construction. The design of the Jetty shall take due consideration of the Revetment Wall construction.
- A depression shall be maintained on the dry side of the Lot behind the top of the vertical edge of the Retaining Wall to maintain drainage. The depression shall be 4.0m wide parallel to the Waterway Edge Wall and shall be at a level of +1.25m AHD.

5.3 *Canal Waterways:*

- Navigable areas and restrictions to boat size are shown in the attached Layout Plan (Appendix A).
- Boat owners shall adhere to all rules and management controls of the Marina Management and, signage and restrictions within the navigable areas and public marina.

5.4 *Supply and Construction:*

- Construction shall be by a recognised contractor with previous experience of the installation of similar works.
- pontoons shall be from a recognised supplier and all products shall meet the appropriate Australian Standards (For concrete – AS 3600; Steel – AS 4100 and Aluminium – AS/NZS 1664).

5.5 *Piling:*

- Mooring Piles and Mooring Envelopes may not be roofed (permanently or temporarily) or have structures built upon them (including, but not limited to, patios, pergolas, shade sails and tarpaulins)
- All Mooring Pile construction, protective coating and installation shall be in accordance with AS2159, “Piling – Design and Installation”.
- Where steel Mooring Piles are installed, they shall be structural steel sections to AS3679.1, AS3679.2 and AS1163 manufactured in accordance with AS3678.
- All materials shall be suitable for the marine environment.
- Protective coatings shall be applied strictly in accordance with manufacturers instructions, with due allowance made for the minimum drying and curing times between successive coats.
- Driven steel Mooring Piles shall be coated from cut off level to at least 2 metres below seabed (or compacted fill) level.
- Adjacent Lot Owners may consider sharing an installed Mooring Pile. The sharing of these Mooring Piles will be by agreement between the Lot Owners and the location of the Mooring Pile shall be wholly within one or other of the Lot boundaries. Ownership of the Mooring Pile will be by the Lot Owner of the Lot in which the Mooring Pile is located.

PORT COOGEE – JETTY DESIGN GUIDELINES

- All Mooring shall be installed wholly within Lot boundaries. Mooring Piles not installed as part of a pontoon system shall be located 0.3m in from the Lot boundary and 0.3m in from the waterway edge of the Mooring Envelope as shown in Figures 1 and 2 of Appendix B. Mooring Piles may not be located within the navigable waterway area of Lots.
- Mooring Piles shall have a finish level (measured as the top of the pile cap) not exceeding +2.5mAHD.

5.6 Materials:

- All materials used in the construction of Jetties and Pontoons shall comply with the relevant Australian Standard (For concrete – AS 3600; Steel – AS 4100 and Aluminium – AS/NZS 1664) and shall be suitable for use in the marine environment.

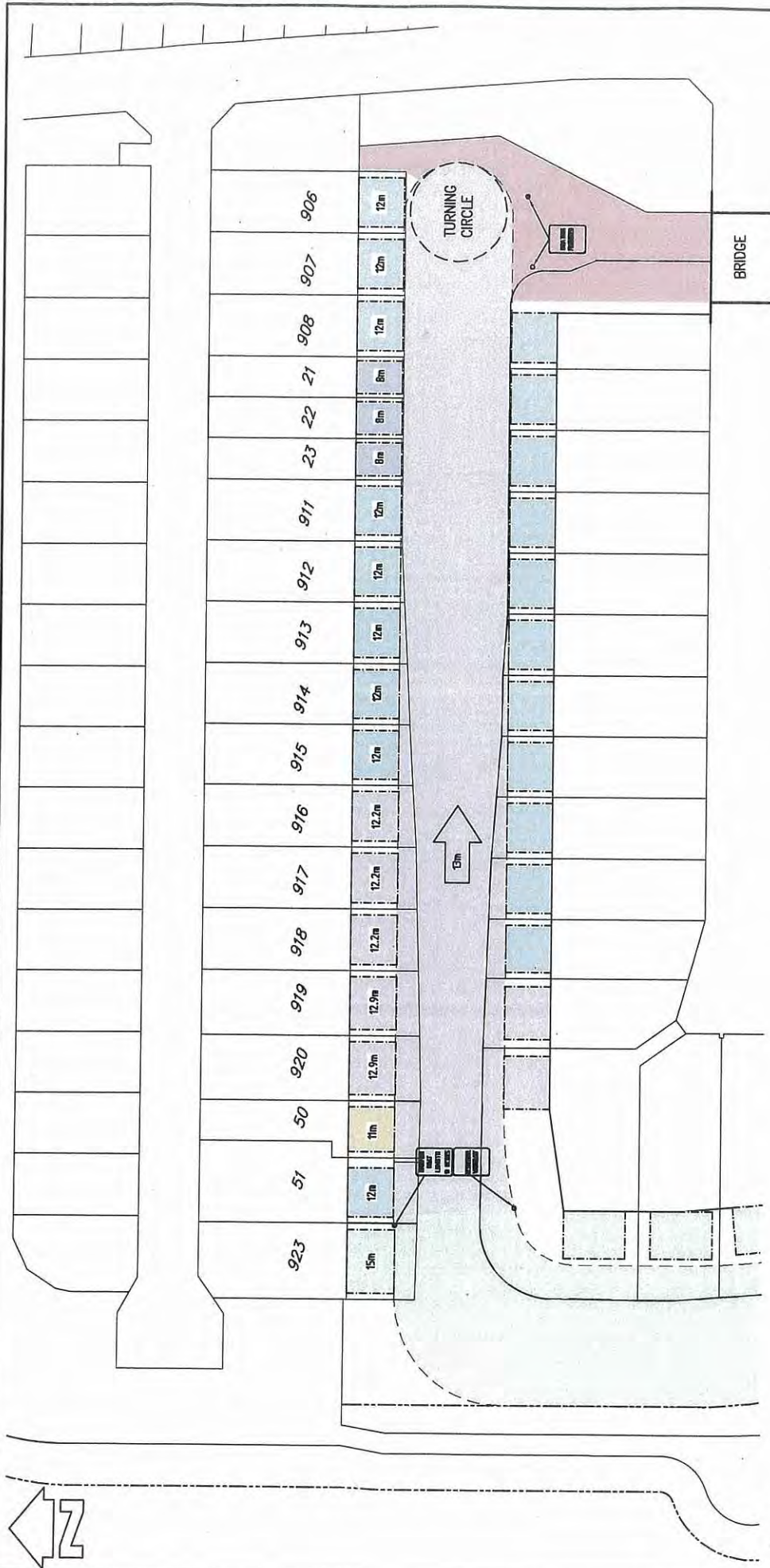
6. Mooring Options

Stage 4A

- Generally Lots may have floating mooring types, at the discretion of the Lot Owner.
- All Lots can have mooring type A or B.
- Refer to Appendices B and C for details.

Appendix A - Layout Plan

AT CORRECT SCALE THIS IS 100 mm



AT CORRECT SCALE THIS IS 100 mm

LEGEND

- MAXIMUM BOAT LENGTH 15m
- MAXIMUM BOAT LENGTH 13m
- MAXIMUM BOAT LENGTH 12m
- MAXIMUM BOAT LENGTH 11m
- MAXIMUM BOAT LENGTH 8m
- BOATING PROHIBITED
- MOORING ENVELOPE
- CADASTRAL BOUNDARY
- NAVIGABLE WATER
- MAXIMUM LENGTH OF BOAT THAT CAN USE BERTH 12m

m p rogers & associates pl
 coastal and port engineers

Suite 1, 128 Main Street
 Osborne Park 6017
 Western Australia
 t: +61 8 9254 6600
 f: +61 8 9254 6699
 admin@coastalports.com.au

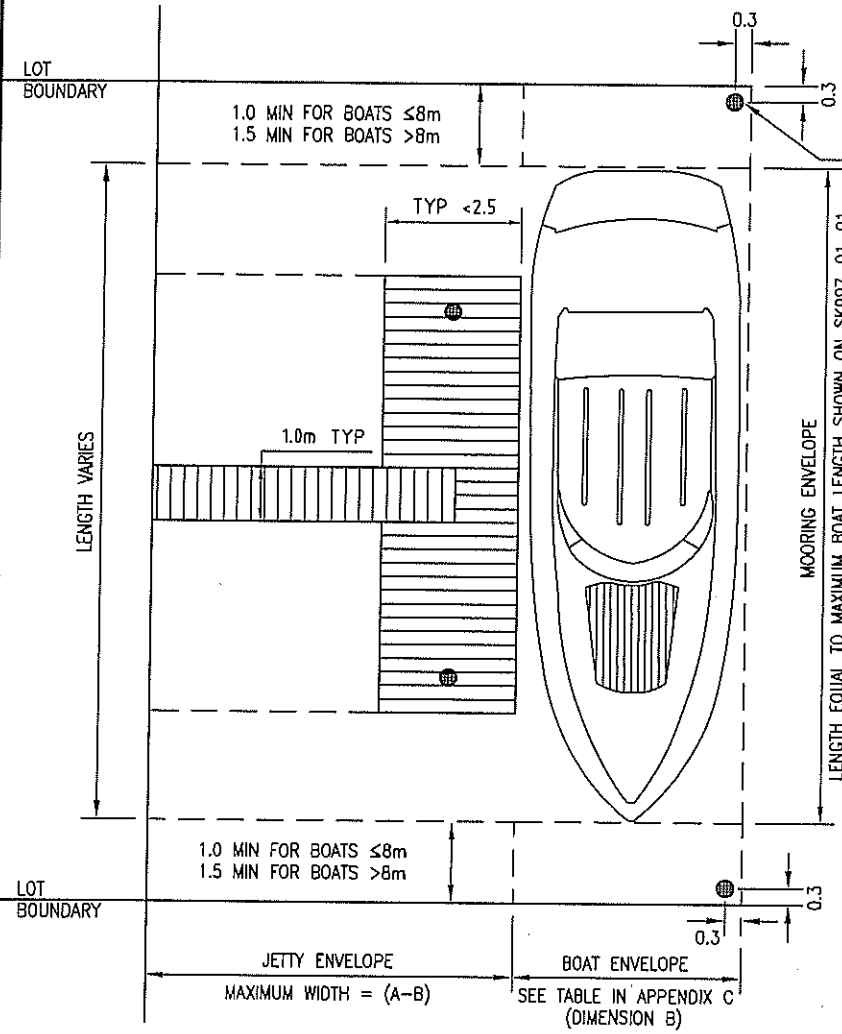
Navigation & Mooring Envelopes for Stage 4A - Peninsula
 PORT COOGEE

SCALE
 AT AS 1:1000

August 2012
 SK997-01-01
 SK997-01-01A

Appendix B – Permissible Mooring Types

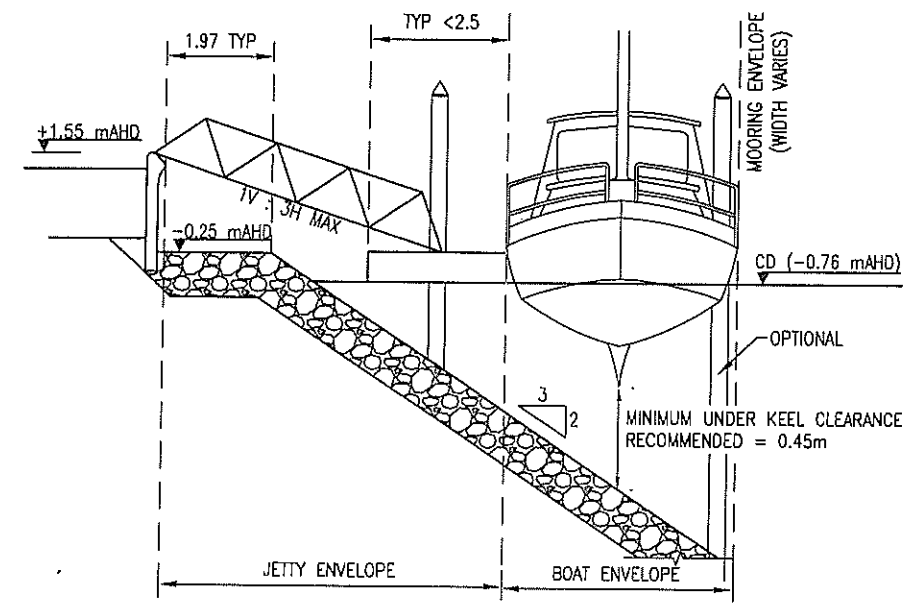
AT CORRECT SCALE THIS IS 100 mm



MOORING & FENDER PILE BY OWNER (OPTIONAL) TO BE LOCATED AS SHOWN

NOTES:

1. TO BE READ IN CONJUNCTION WITH SK997-D1-01.
2. MINIMUM BOAT DRAFT MUST BE CONSIDERED IN DESIGN OF PONTOON WIDTH.
3. AN UNDER KEEL CLEARANCE OF 0.45m IS RECOMMENDED AT LAT.
4. NO LOAD FROM THE PONTOON OR GANGWAY SHALL BE TRANSFERRED THROUGH THE VERTICAL RETAINING WALL.
5. SUPPORT FOUNDATIONS AND ANY ANCHOR BRACING TO BE IN ACCORDANCE WITH PERMISSIBLE LOADING LIMITS BEHIND THE VERTICAL RETAINING WALL.



INDICATIVE ONLY

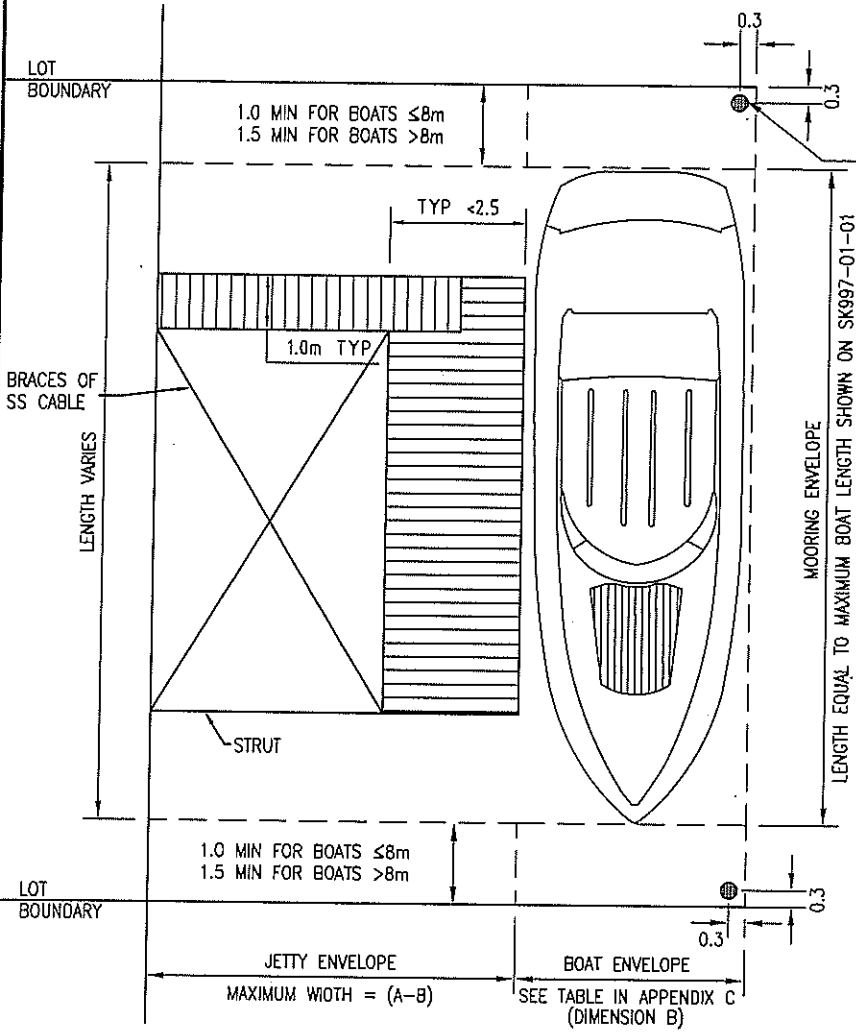
m p rogers & associates pl
coastal and port engineers

Jetty Type A
PORT COOGEE

August 2012
SK997-01-02

SK9970102A

AT CORRECT SCALE THIS IS 100 mm

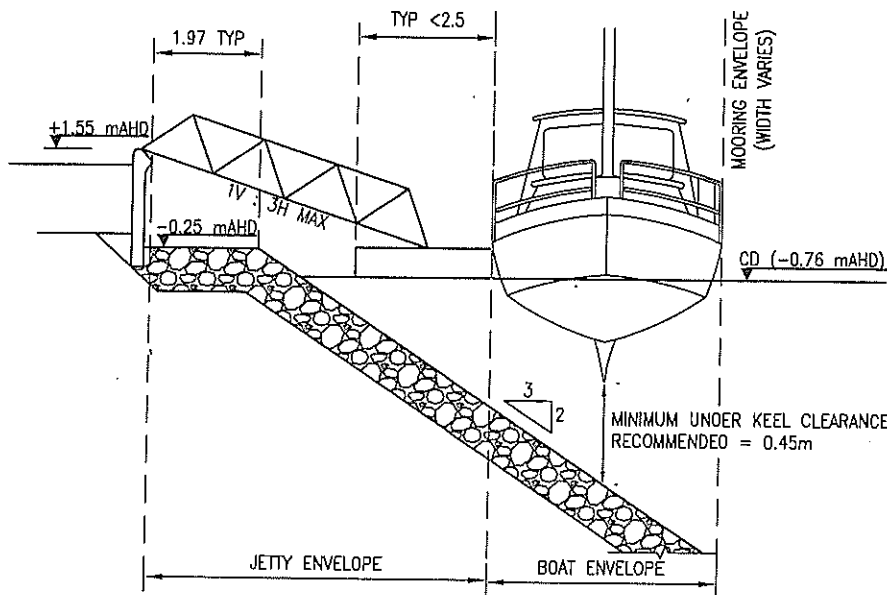


MOORING & FENDER PILE BY OWNER (OPTIONAL) TO BE LOCATED AS SHOWN

NOTES:

1. TO BE READ IN CONJUNCTION WITH SK997-01-01.
2. MINIMUM BOAT DRAFT MUST BE CONSIDERED IN DESIGN OF PONTOON WIDTH.
3. AN UNDER KEEL CLEARANCE OF 0.45m IS RECOMMENDED AT LAT.
4. NO LOAD FROM THE PONTOON OR GANGWAY SHALL BE TRANSFERRED THROUGH THE VERTICAL RETAINING WALL.
5. SUPPORT FOUNDATIONS AND ANY ANCHOR BRACING TO BE IN ACCORDANCE WITH PERMISSIBLE LOADING LIMITS BEHIND THE VERTICAL RETAINING WALL.

AT CORRECT SCALE THIS IS 100 mm



INDICATIVE ONLY

m p rogers & associates pl
coastal and port engineers

Jetty Type B
PORT COOGEE

August 2012
SK997-01-03

SK997-01-03A

PORT COOGEE – JETTY DESIGN GUIDELINES

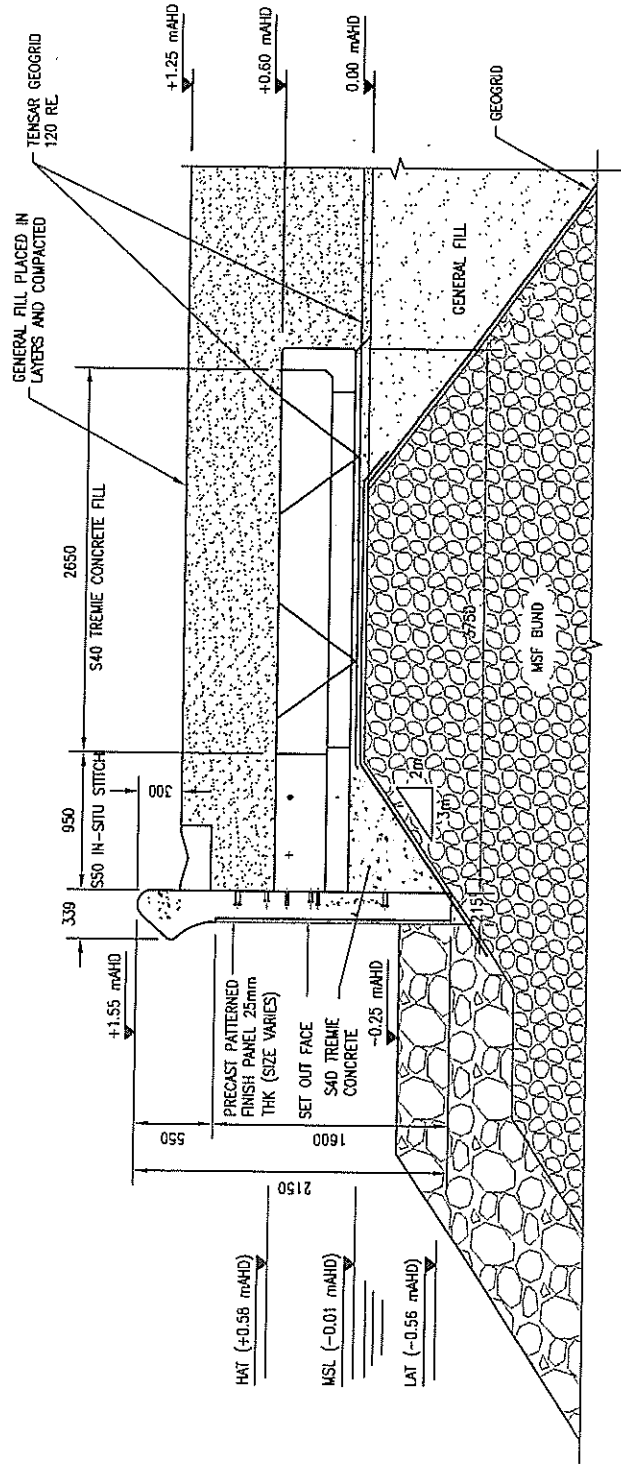
Appendix C – Stage 4A Mooring Types

Jetty Type	LOT																			
	906	907	908	21	22	23	911	912	913	914	915	916	917	918	919	920	50	51	923	
Type A - PILED FLOATING																				
Max Permissible Boat Length (m)	12	12	12	8	8	8	12	12	12	12	12	12.2	12.2	12.2	12.9	12.9	11	12	15	
Max Permissible Boat Beam ² (m)	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.5	
Max Boat Draft (m)	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	2.0	
Type B - NON-PILED FLOATING																				
Max Permissible Boat Length (m)	12	12	12	8	8	8	12	12	12	12	12	12.2	12.2	12.2	12.9	12.9	11	12	15	
Max Permissible Boat Beam ² (m)	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.5	
Max Boat Draft (m)	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	1.8	2.0	
MAXIMUM DIMENSIONS (REFER FIGURES IN APPENDIX B)																				
A – Mooring Envelope Width ¹ (m)	11.15	11.15	11.15	11.15	11.15	11.15	11.15	11.15	11.15	11.15	11.15	11.15	11.15	11.15	11.15	11.15	11.15	11.15	11.5	
B – Boat Envelope Width ² (m)	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.2	4.5	

1. The boat and jetty MUST be fully contained within the Mooring Envelope.
2. Maximum boat beam is based on typical dimensions for a power boat. Sail boats are typically narrower.

Appendix D – Details of Revetment Wall Construction

AT CORRECT SCALE THIS IS 100 mm



AT CORRECT SCALE THIS IS 100 mm

NOTE:
WALL DESIGNED AND CONSTRUCTED BY OTHERS

m p rogers & associates pl
coastal and port engineers

Suite 1, 128 Main Street
Osborne Park 6017
Western Australia
t: +61 8 9254 6600
f: +61 8 9254 6699
admin@coastsandports.com.au

Details of Edge Wall Construction
PORT C00GEE

SCALE
AT 1:50

August 2012
SK997-01-04

SK9970104A

Our Ref: 90/057 KS:ct

22 June 2012

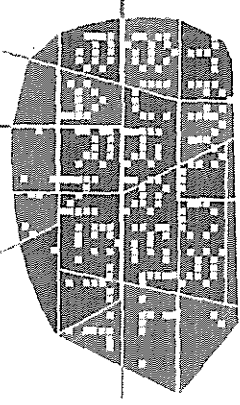
Attention: Lee Gatt

City of Cockburn
PO Box 1215
BIBRA LAKE WA 6163

Taylor Burrell Barnett Town Planning & Design

RECEIVED at: CITY OF COCKBURN
File # Subiaco PO Box 6108 Stirling East Western Australia 6108
x-ref:
Action Officer: L Gatt 26 JUN 2012 CC
Retention: D: BOX Manual File Archive FILE
LOOSE / ATTACHED File please!

Telephone (08) 9382 2911 Facsimile (08) 9382 2912



Indcliffe Processing Pty Ltd ABN 74 631 437 975

Dear Sir

PROPOSED ROAD CLOSURE-- PORTION OF MARABOO WHARF, NORTH COOGEE

Taylor Burrell Barnett, on behalf of our client Port Catherine Developments Pty Ltd, submits this request for the closure of a 41m² portion of the Maraboo Wharf road reserve, North Coogee. Maraboo Wharf is within the Port Coogee development and was approved via previous conditional subdivision (WAPC Ref: 140561) – only portion of the approved road has currently been constructed and gazetted.

Please find attached a plan detailing the proposed road closure. It is intended that the 41m² portion of closed road will be amalgamated with adjoining development land to the south to provide additional site area to accommodate a proposed marine services building and cafe/restaurant, in respect of which Council has recently granted approval to commence development.

An application for subdivision has recently been submitted to the Western Australian Planning Commission (WAPC) (WAPC Ref: 146293) proposing to create Lots 1 and 2 via the subdivision of Lot 9096 and the proposed closed road. The Form 1A has been signed by State Land Services.

We trust that this application contains sufficient information for Council to initiate the tasks required to permanently close this 41m² portion of the Maraboo Wharf road reserve. Should you require any further information, please do not hesitate to contact the undersigned on 9382 2911.

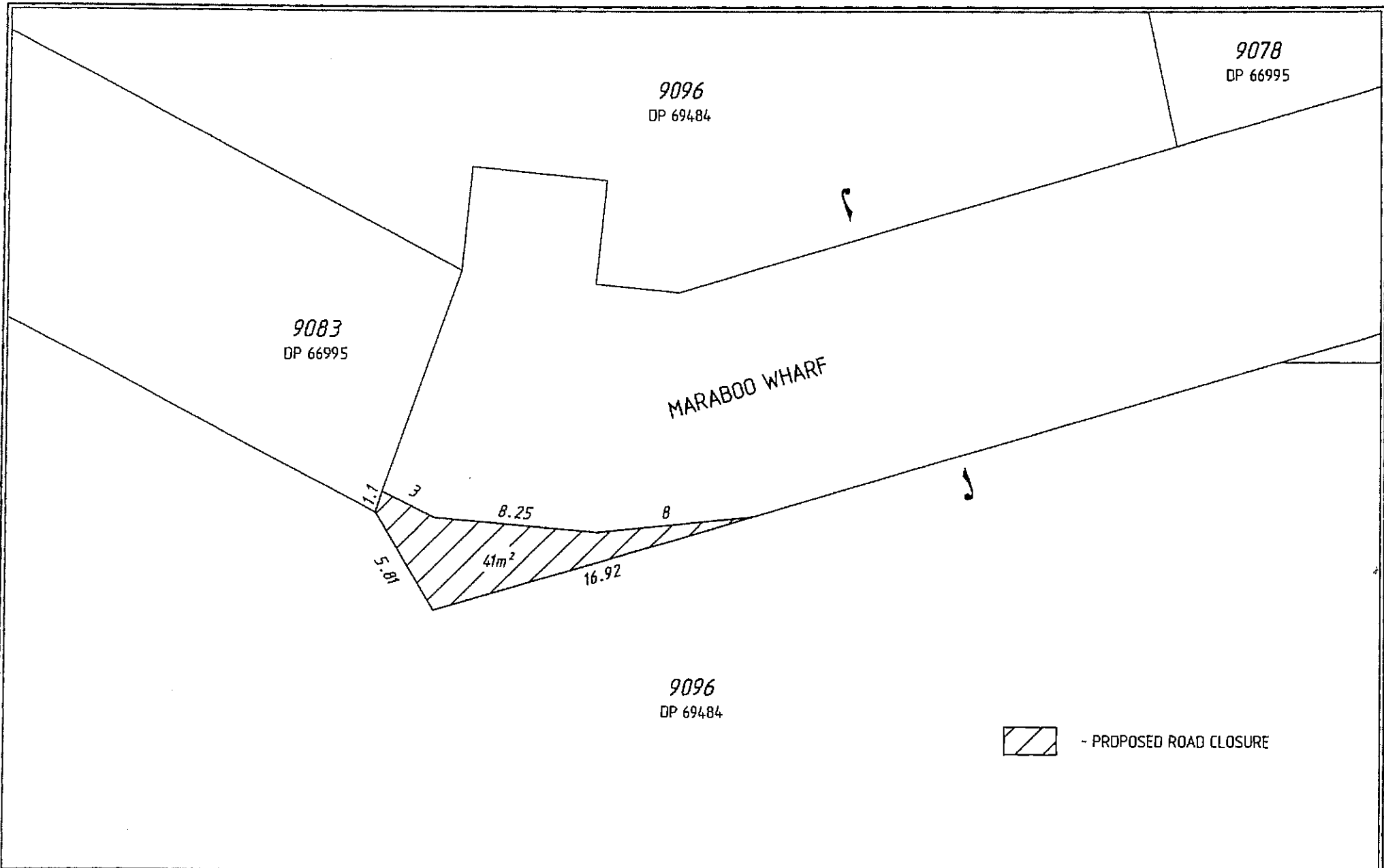
Yours faithfully
TAYLOR BURRELL BARNETT

KATHERINE SHIRLEY
PLANNER

CC: Danielle Williams – Australand Holdings Ltd
Scott Anderson – McMullen Nolan Group Pty Ltd

\$750.00

RECEIVED
WITH
PAYMENT....



Rev	Description	Drawn	Date	Checked

SCALE 1:250 @ ALL
 ALL DISTANCES ARE IN METRES
 For a true copy to all registered owners of this plan, send it to the Planning Section at the above address.

DATE: 21/06/2012
 LOCATION: 92657-1067-A



All areas and dimensions shown on this plan are subject to survey and boundary verification. All details of boundaries referred to on this plan shall have the status of this plan are not guaranteed and are subject to a title only.

McMULLENOLAN GROUP
 Surveying Excellence

2012/06/12
 92657-1067-A

**PORT COOGEE
 MARABOO WHARF ROAD CLOSURE
 North Coogee**

Project Prop: Staff Andersen Date: 2012/06/12
 92657-1067-A

Our Ref: 90/057 KS:ct

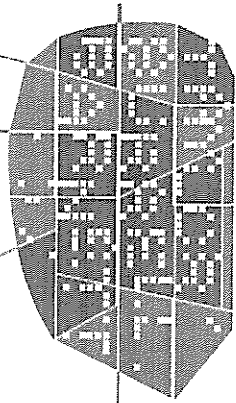
13 August 2012

Attention: Lee Gatt

Chief Executive Officer
City of Cockburn
PO Box 1215
BIBRA LAKE WA 6965

107 Hobsons Road Seaside PO Box 2145 Seaside East Western Australia 6150

Taylor Burrell Barnett Town Planning & Design
Telephone (08) 9382 2311 Facsimile (08) 9382 4385 <http://www.tbndesign.com.au>



2012/08/13 10:51:43 AM

Dear Sir

PROPOSED ROAD CLOSURE-- PORTION OF MARABOO WHARF, NORTH COOGEE

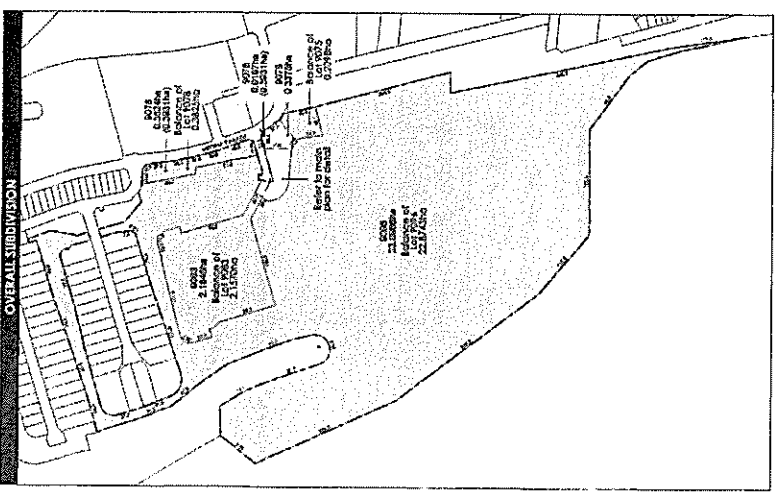
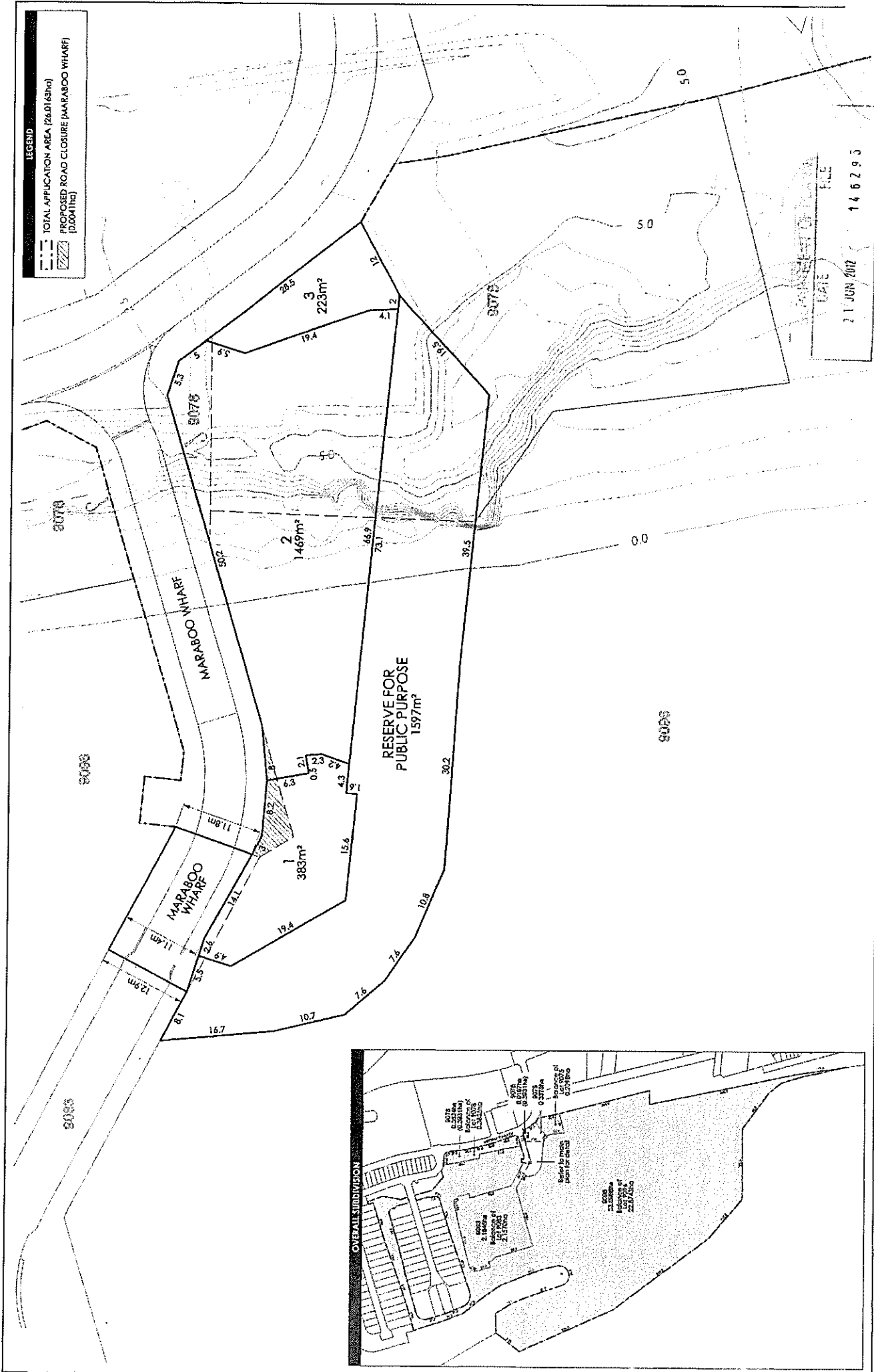
In respect of our request of 22 June 2012 to close a 41m² portion of the Maraboo Wharf road reserve, we are writing to confirm that Port Catherine Developments Pty Ltd will pay all costs directly associated with the road closure process.

Should you wish to further discuss any of the above, please do not hesitate to contact the undersigned.

Yours faithfully
TAYLOR BURRELL BARNETT

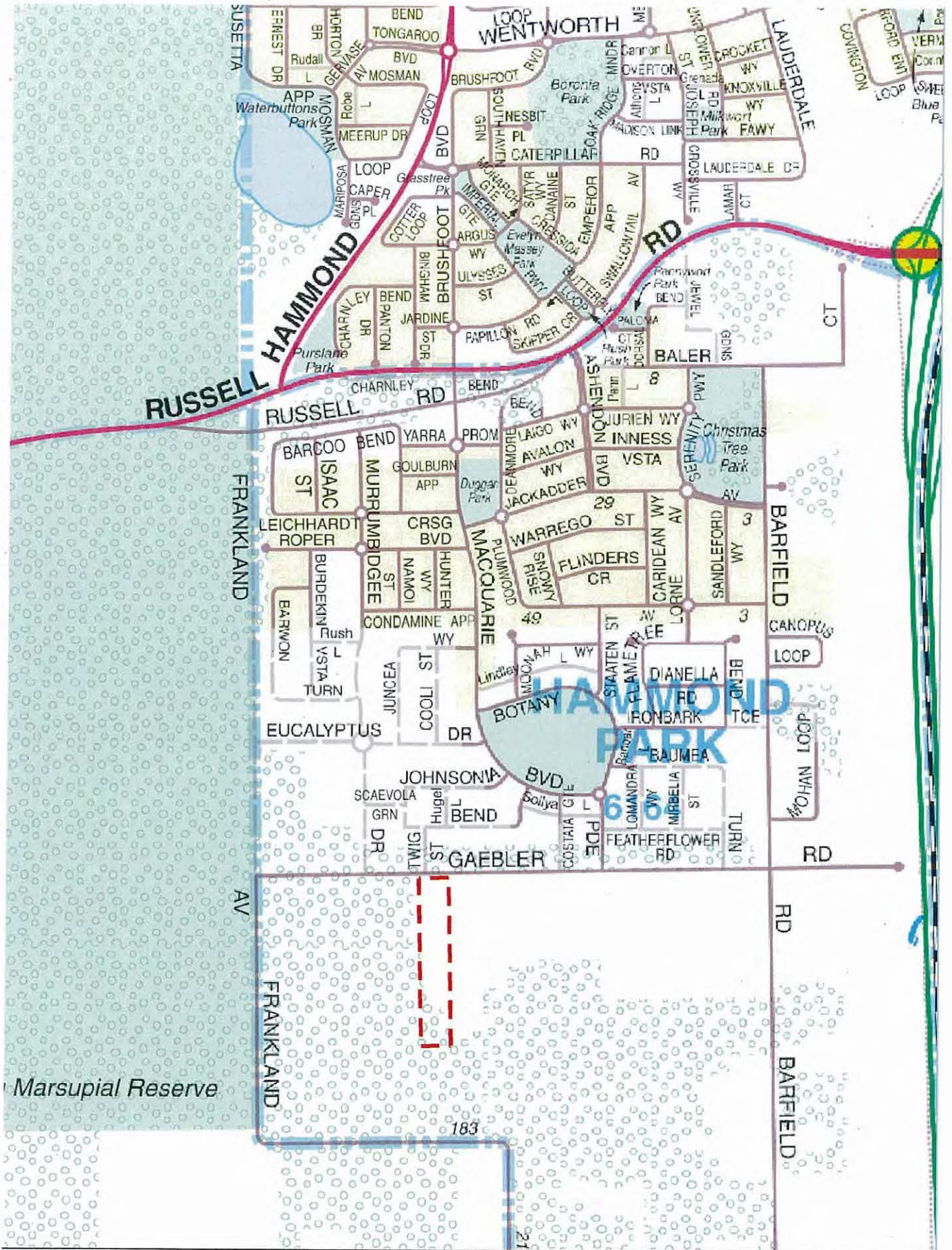
KATHERINE SHIRLEY
PLANNER

CC: Danielle Williams – Australand Holdings Ltd



PROJECT	146293
DATE	21 JUN 2012
FILE	146293
SCALE	1:500
PROJECT	146293
DATE	21 JUN 2012
FILE	146293
SCALE	1:500

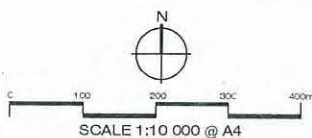
Plan of Subdivision
 LOTS 907/5, 907/8, 908/3 AND 909/6 MEDINA PARADE, NORTH COOGEE
 AN AUSTRALAND HOLDINGS PROJECT



LOCATION PLAN
LOT 39 GAEBLER ROAD
HAMMOND PARK

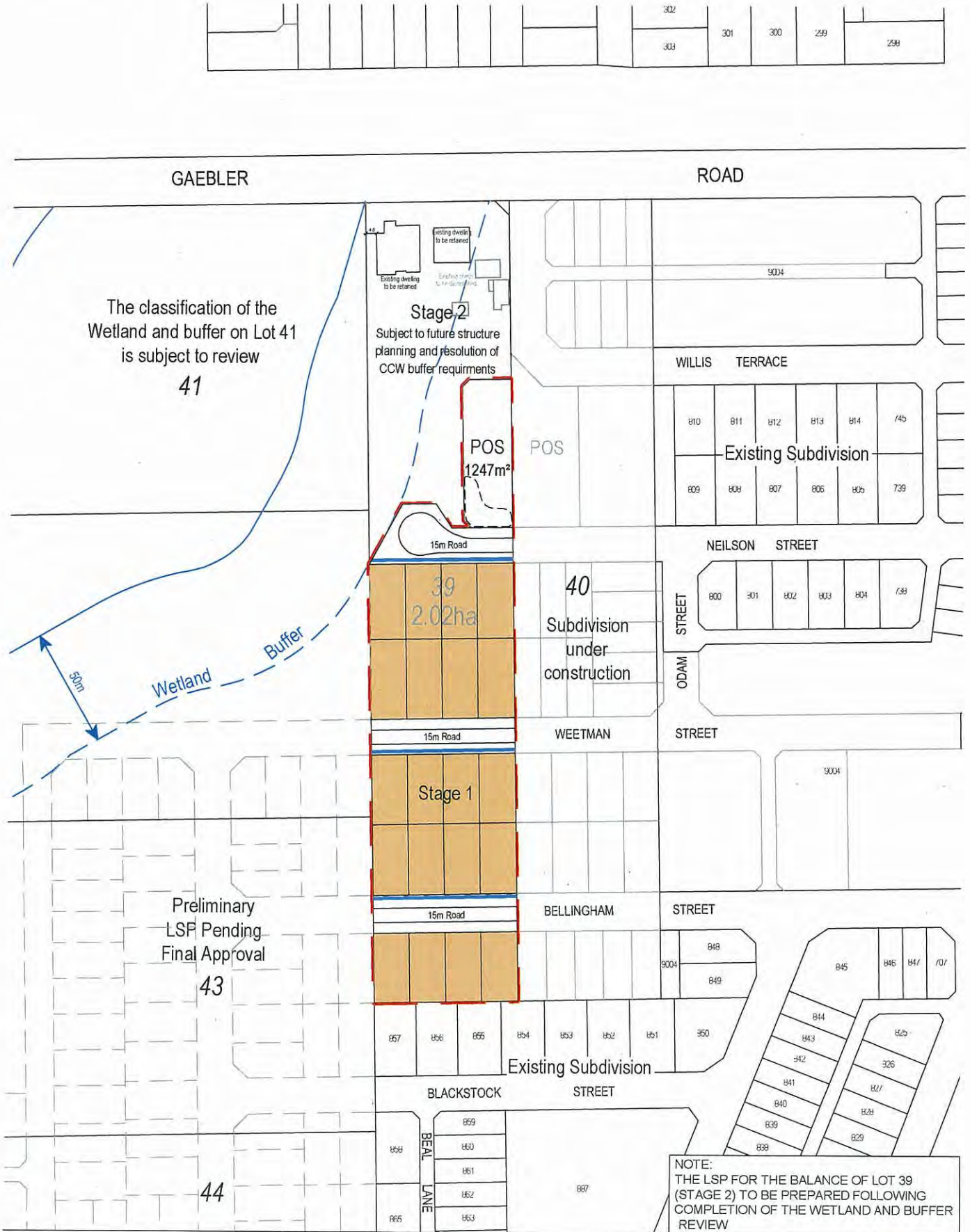
JOB REFERENCE: 100846 DATE: 02.12.2011

THE DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS COMMISSIONED AND IN ACCORDANCE WITH THE TERMS OF ENGAGEMENT FOR THE COMMISSION. UNAUTHORISED USE OF THIS DOCUMENT IN ANY FORM WHATSOEVER IS PROHIBITED.



GRAY & LEWIS
LAND USE PLANNERS

Figure 1
 Suite 5, 2 Hardy Street
 South Perth, WA 6151
 T (08) 9474 1722
 F (08) 9474 1172
 perth@graylewis.com.au



NOTE:
THE LSP FOR THE BALANCE OF LOT 39
(STAGE 2) TO BE PREPARED FOLLOWING
COMPLETION OF THE WETLAND AND BUFFER
REVIEW

LOCAL STRUCTURE PLAN

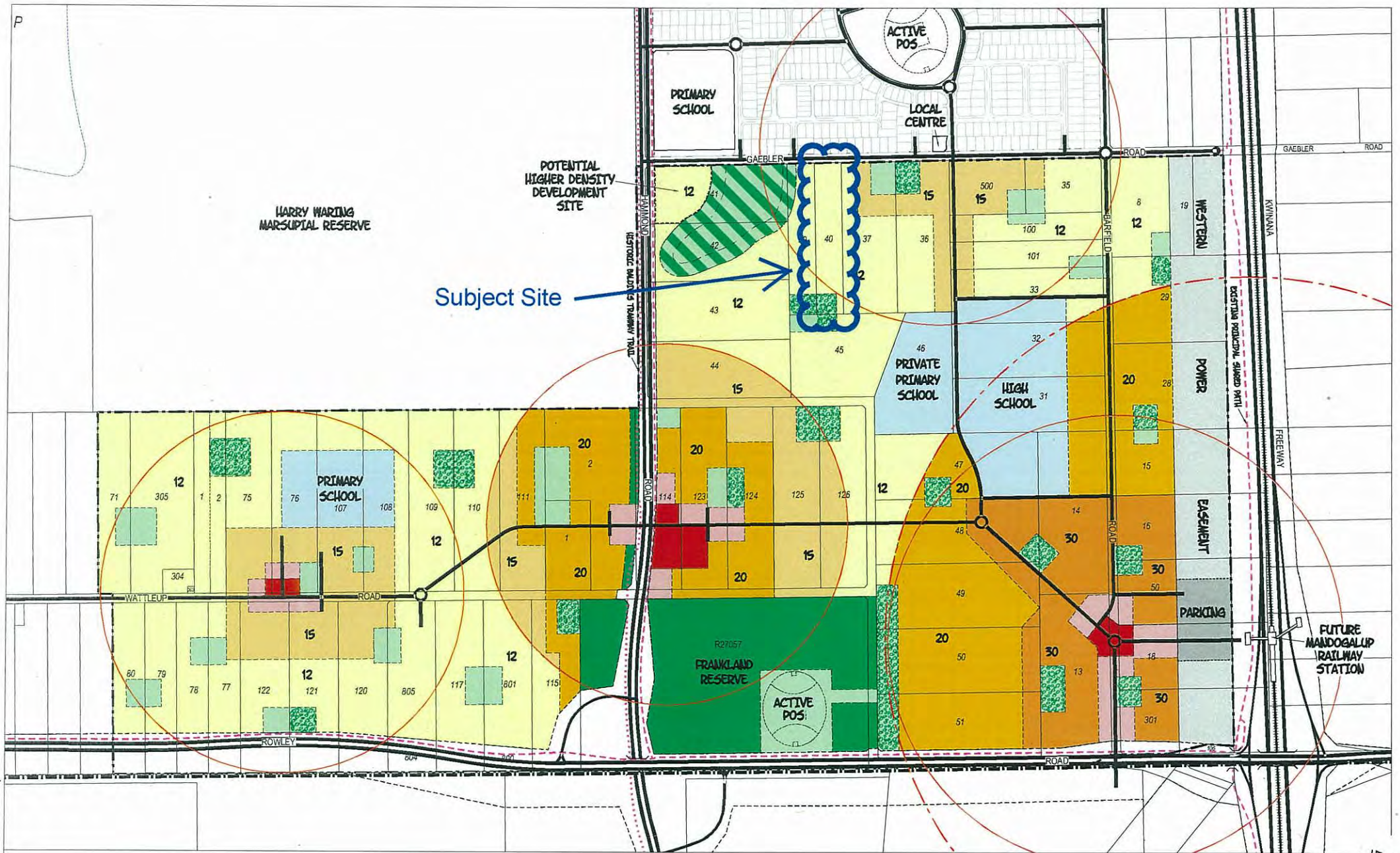
Pt LOT 39 GAEBLER ROAD
HAMMOND PARK

- LEGEND
- LOCAL STRUCTURE PLAN AREA
 - EXISTING CADASTRAL BOUNDARY
 - FOOTPATHS
 - RESIDENTIAL R25 ZONE
 - PUBLIC OPEN SPACE (POS)



Plan 1

GRAY & LEWIS
LAND USE PLANNERS
Suite 5, 2 Hardy Street
South Perth, WA 6151
T (08) 9474 1722
F (08) 9474 1172
perth@graylewis.com.au



Subject Site

POTENTIAL HIGHER DENSITY DEVELOPMENT SITE

HARRY WARING MARSUPIAL RESERVE

SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN

- STAGE 3, HAMMOND PARK / WATTLEUP



- Residential - Urban Density Target:
- 30 30 DU's / ha
 - 20 20 DU's / ha
 - 15 15 DU's / ha
 - 12 12 DU's / ha

- Local Centre (Pedestrian Based Retail)
- Mixed Business / Commercial / Home Based Business
- Community Facilities / Schools etc
- Western Power Easement

- Key Local Open Space Areas:
- POS - Bush
 - POS - Other
 - Conservation
 - Regional Open Space

- Railway Station Car Park
- Major Shared Path
- Historic Baldvis Tramway Trail
- 400m Walkable Catchment
- 800m Walkable Catchment To Railway Station

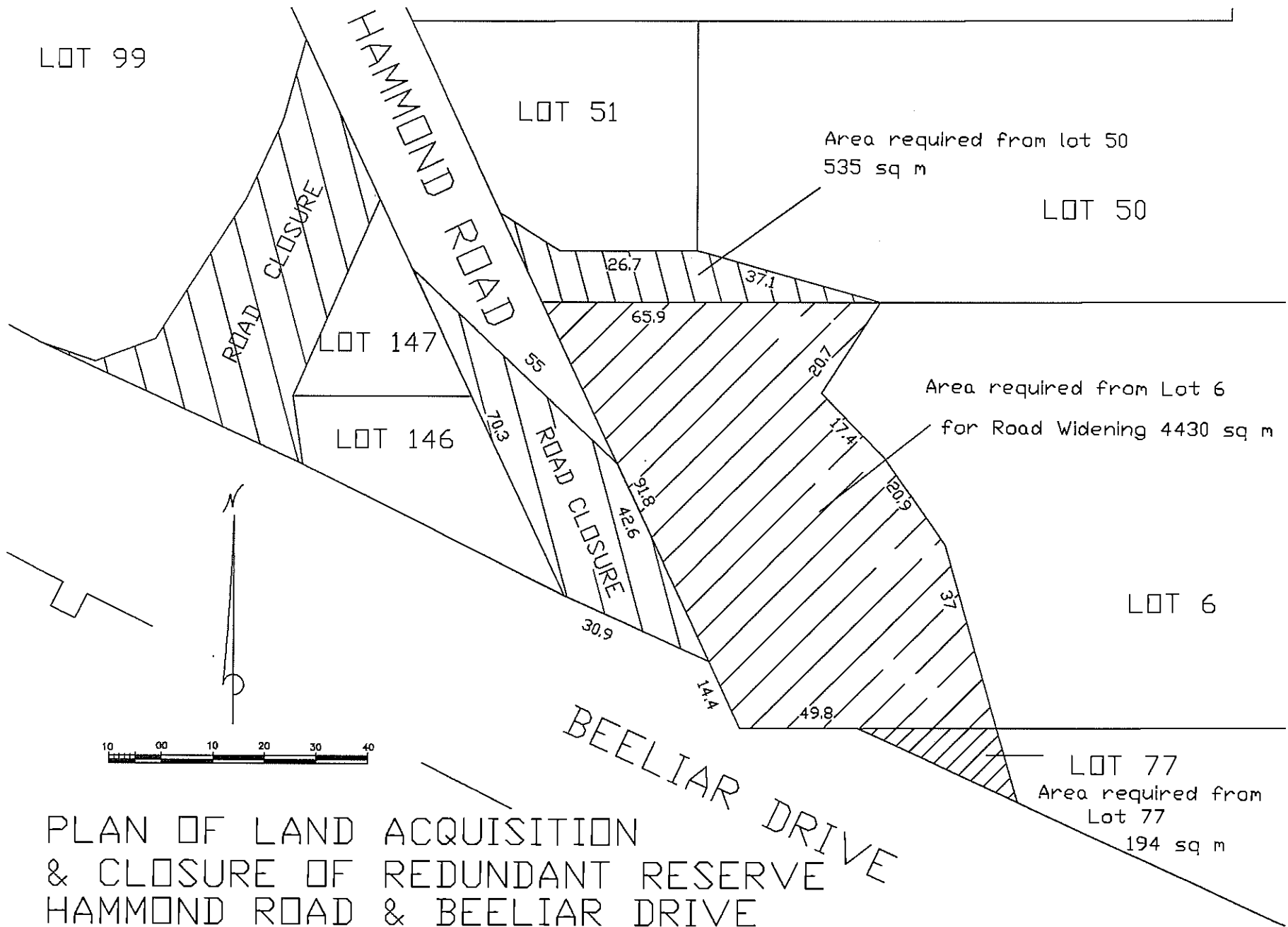
SCHEDULE OF SUBMISSIONS

STRUCTURE PLAN PROPOSAL – LOT 39 GAEBLER ROAD, HAMMOND PARK

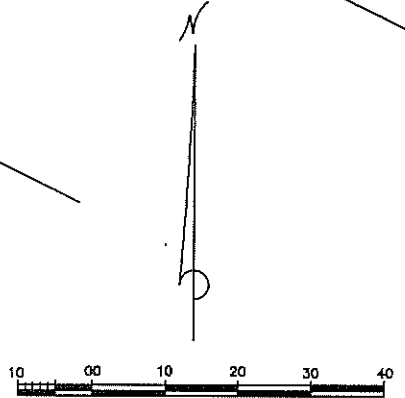
NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
1	Graeme Morris Richard Noble Level 1, 189 Hay Street SUBIACO WA 6008	Support We received your referral for the Structure Plan for lot 39 yesterday. It has been circulated within the office and I advise that we have no comment to make on the proposal thank you.	Noted.
2	Norm Walkerden Telstra, Forecasting & Area Planning Locked Bag 2525 PERTH WA 6001	Support Thank you for the above advice. At present, Telstra Corporation Limited has no objection; however, this lot will be serviced by NBN Co. Any network extension that may be required for any development within the area concerned, the owner/developer will have to submit an application before construction is due to start to NBN Co. Please dial 1100 (Dial before You Dig) for location of existing services.	Noted.
3	Lyndall Ford Department of Indigenous Affairs PO Box 3153 EAST PERTH WA 6892	Thank you for your correspondence dated 16 July 2012. I have reviewed the relevant information and I can confirm that there are currently no known sites registered with the Department of Indigenous Affairs (DIA) within the work areas you have outlined. DIA has released Cultural Heritage Due Diligence Guidelines (Guidelines) to assist you in your planning for future works. A copy of the Guidelines is attached. You can also find the Guidelines on the DIA website at: http://www.dia.wa.gov.au/Documents/HeritageCulture/Heritage%20management/AH A Due Diligence Guidelines.pdf Should cultural material or new sites be discovered either while undertaking the project or through any heritage surveys you may carry out, there is an obligation under section 15 of the AHA to record and lodge site information with the DIA so that we can update our information and place these sites on the Register. Further information about site recording and the AHA generally can be found at the DIA Website at the following links. http://www.dia.wa.gov.au/en/Heritage-and-Culture/ http://www.dia.wa.gov.au/en/Section-18-Applications/Heritagemanagement/Reporting-a-Site/ If you have any questions, please contact me on 6551 8110.	Noted.

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>ANTHROPOLOGICAL AND ARCHAEOLOGICAL ORGANISATIONS IN WESTERN AUSTRALIA FOR ADVICE ON CONSULTANTS TO UNDERTAKE ABORIGINAL HERITAGE ASSESSMENT SURVEYS Please note: The Department of Indigenous Affairs (DIA) does not accept any responsibility for the choice of consultant or outcomes. For advice on the conduct of surveys, including legislative requirements, contact the Department of Indigenous Affairs or refer to the web page at www.dia.wa.gov.au.</p> <p>Anthropological Society of Western Australia Inc. - Professional Section CONTACT: An alphabetical list of consultants who are members of ASW A can be found at the ASWA website under the heading Consultants: http://www.anthropologywa.org/list.html.</p> <p>Australasian Association of Professional and Consulting Anthropologists and Archaeologists Inc. CONTACT: A list of consultants who are members of AAPCAA can be found at the AAPCAA website: http://www.aapcaa.asn.au/Consultancy.htm Australian Association of Consulting Archaeologists Inc. - W A Chapter.</p> <p>(Document attached: Register of Aboriginal Sites and Heritage Places)</p>	
4	Brett Dunn, Department of Water PO Box 332 MANDURAH WA 6210	<p>Support</p> <p>Thank you for the referral of the above Local Structure Plan (LSP) received with correspondence dated 16 July 2012. The Department of Water (DoW) has reviewed the proposal and wishes to provide the following advice:</p> <p>Better Urban Water Management</p> <p>Consistent with the Department of Planning (DoP's) Better Urban Water Management (BUWM) document and the policy measures outlines in State Planning Policy 2.9 the proposed Local Structure Plan should be supported by a Local Water Management Strategy (LWMS) prior to the approval of the proposed Local Structure Plan.</p> <p>The DoW has previously agreed with the City of Cockburn that due to the size of the site, a full LWMS would not be required to support the LSP. It was concluded that supporting information would still need to be provided in the LSP to demonstrate that storm water can be managed in accordance with best practice. The supporting document, Lot 39 Gaebler Rd, Hammond Park- Engineering Services and Drainage Report, Revision C (Groundwork Consulting Engineers, June 2012) provided within the LSP was deemed satisfactory to the DoW, as noted in correspondence dated 15 June 2012. Accordingly, the Department has no objections to the proposed LSP for Lot 39 Gaebler Road, Hammond Park.</p>	Noted.

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>An Urban Water Management Plan (UWMP) will be required as a <i>condition of subdivision</i> in the future, in accordance with Better Urban Water Management (WAPC, 2008), and will provide a greater level of information for water management strategies and infrastructure to be implemented on the site.</p> <p>If you wish to discuss the above further please contact Shelley McSpirit at the DoW's Mandurah Office on (08) 9550 4222.</p>	
5	<p>Stefan De Han, Department of Environment and Conservation PO Box 1167 Bentley Delivery Centre WA 6983</p>	<p>Support</p> <p>I refer to your letter of 16 July 2012 requesting the Department of Environment and Conservation's (DEC) comments on the Local Structure Plan (LSP) to facilitate residential development of a portion of Lot 39 Gaebler Road, Hammond Park. DEC advises it has no objection to the proposed structure plan however provides the following comments.</p> <p>It is noted that the northern portion of the site abuts a wetland mapped in the Geomorphic wetlands dataset as a Conservation category wetland (CCW). CCWs are wetlands with high ecological values and are the highest priority wetlands for protection. The protection of the CCW requires the retention of an adequate buffer. In the absence of a site specific buffer study a generic minimum 50 metre wetland buffer distance is recommended to provide long-term protection of wetland value and function. It is noted that while a 50 metre setback has been allocated in the current structure plan design, there is a provision for a further stage which may encroach this buffer. DEC would expect to be consulted regarding any further proposal to develop within the buffer and would generally not support any development including drainage within the buffer, in accordance with current government policy and guidance. As a condition of future subdivision DEC would also recommend that access from the development into the adjacent wetland and buffer is controlled through fencing and signage.</p> <p>DEC is aware there are some areas of remnant vegetation within the LSP area. However on review of the structure plan report it is clear that this vegetation is degraded and of minimal fauna habitat value. DEC however supports the retention of remnant vegetation within the adjacent Lots 41 - 44 Frankland Avenue in areas of conservation open space. Although these lots fall outside the cutiettt LSP, DEC's request is-consistent with recent comments made on the Southern Suburbs District Structure Plan.</p> <p>Thank you for the opportunity to comment on this structure plan. Please contact Jacqui Maguire at DEC's Swan Coastal District on 9303 77 43 should you have any further queries regarding this advice.</p>	Noted.

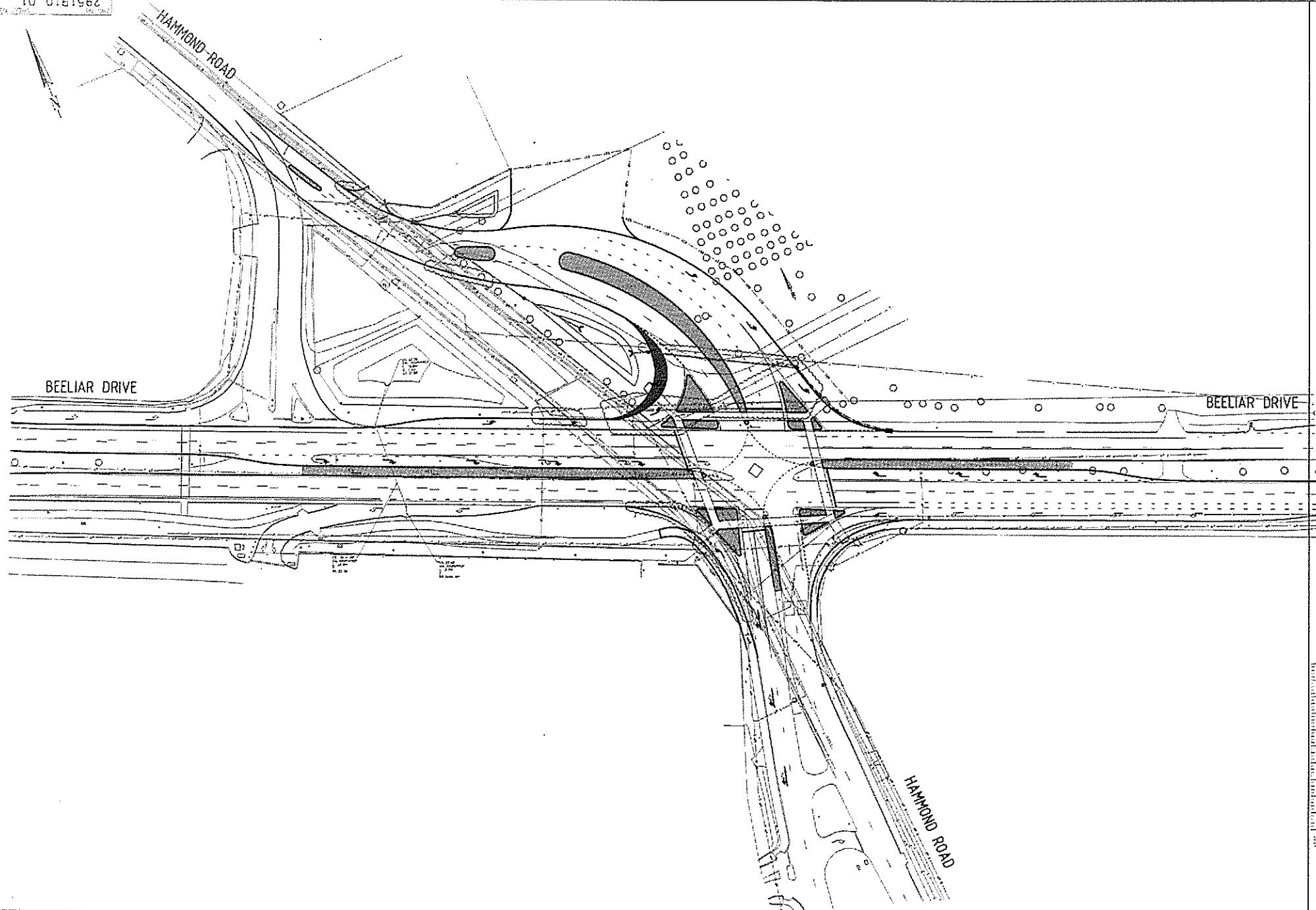


PLAN OF LAND ACQUISITION
 & CLOSURE OF REDUNDANT RESERVE
 HAMMOND ROAD & BEELIAR DRIVE
 COCKBURN CENTRAL



OCM 13/9/2012 Item 14.. 5 Attach 1

2951B10 01



NO	DATE	DESCRIPTION	APPROVED
1	11-01-12	ISSUED FOR REVIEW	
2	11-08-12	REVISED TO INCLUDE PROPOSED TRAFFIC SIGNAL	
3	11-08-12	REVISED TO INCLUDE PROPOSED TRAFFIC SIGNAL	

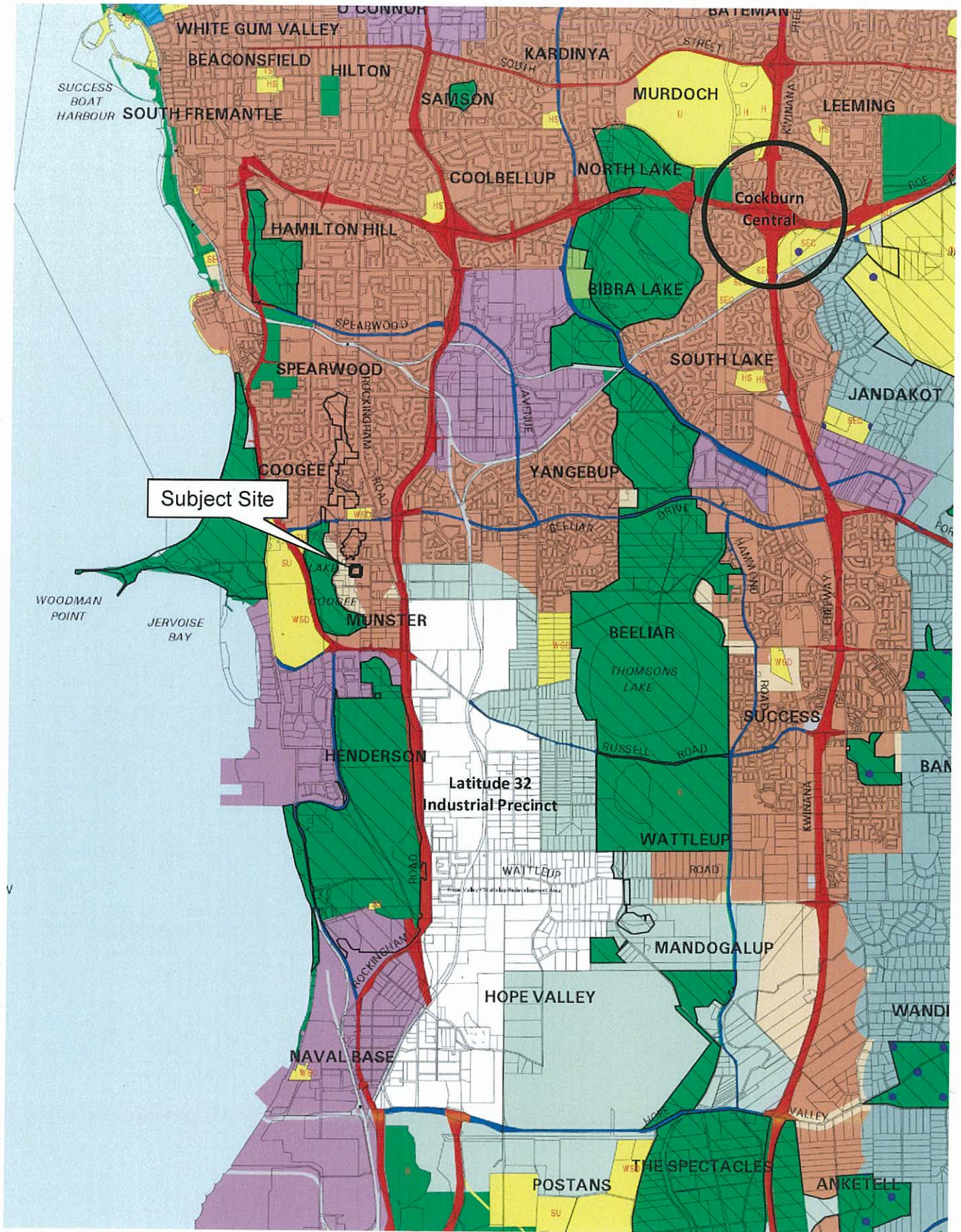
CITY OF COCKBURN
 9 COLEVILLE CRESCENT, SPEARWOOD, WA 6103
 PHONE: (08) 9411 3444 FAX: (08) 9412 5333

**BEELIAR DRIVE
 HAMMOND ROAD INTERSECTION
 PROPOSED TRAFFIC SIGNAL**

DESIGNED	DR & SM	APPROVED	SCALE	1:200	DATE	11/08/12
DRAWN	SM & SM					
CHECKED						
			SHEET NO.		2951B10 01 B	

Scale: 1:200

Attach 2




Subdivision, Zoning, Structure
Planning, Developers Planning,
Design, Advocacy

2083 Albany Highway,
Kalgoorlie WA 6111

T 0895 1842
F 0895 1843
admin@dykstra.com.au

Location & Context Plan

Lots 3 and 4 West Churchill Ave Munster

April 2012

N



not to scale

Figure 1

Note:

- This document may only be used for the purpose for which it was commissioned and in accordance with the Terms of Engagement.
- The dimensions, areas and number of lots are subject to survey and also the requirements of all authorities.

12943CPF1-120117-A



Subdivision, Planning, Structure
 Planning, Development Planning,
 Design, Advocacy
 2363 Albany Highway,
 Karridale, WA 61111
 T: 09495 1947
 F: 09425 1840
 admin@dykstra.com.au

Aerial Locality Plan

Lot 3 & 4 West Churchill Avenue

MUNSTER

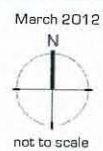
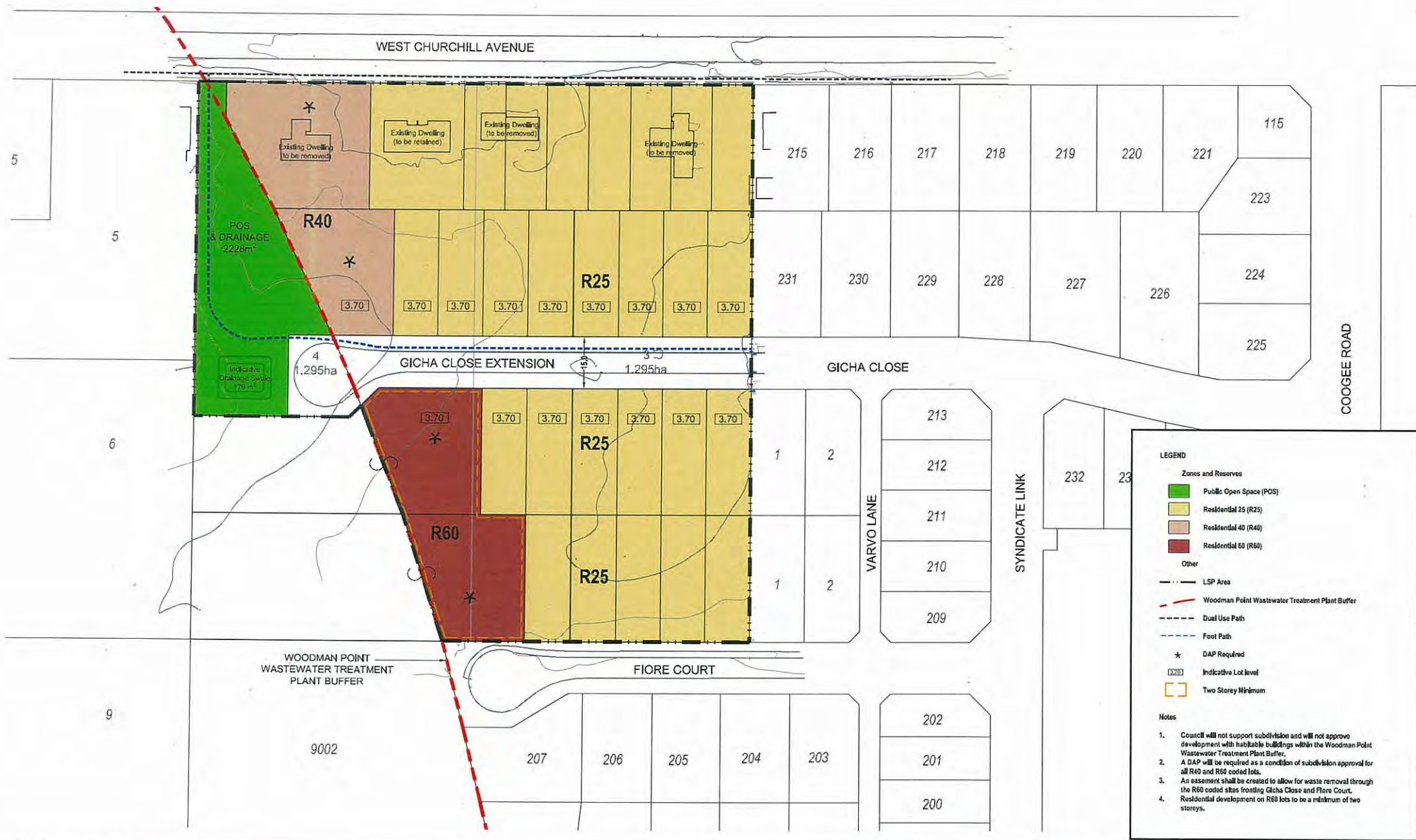


Figure 2

- Notes:
- This document may only be used for the purpose for which it was commissioned and in accordance with the Terms of Engagement.
 - The dimensions, areas and number of lots are subject to survey and see the requirements of all authorities.

12950 ALP/F2-120426-A



LEGEND

Zones and Reserves

- Public Open Space (POS)
- Residential 25 (R25)
- Residential 40 (R40)
- Residential 60 (R60)

Other

- LSP Area
- Woodman Point Wastewater Treatment Plant Buffer
- Dual Use Path
- Foot Path
- DAP Required
- Indicative Lot level
- Two Storey Minimum

Notes

- Council will not support subdivision and will not approve development with habitable buildings within the Woodman Point Wastewater Treatment Plant Buffer.
- A DAP will be required as a condition of subdivision approval for all R40 and R60 coded lots.
- An easement shall be created to allow for waste removal through the R60 coded sites fronting Gicha Close and Fiore Court.
- Residential development on R60 lots to be a minimum of two storeys.

LOCAL STRUCTURE PLAN
 Lots 3 & 4 West Churchill Avenue
 MUNSTER

DP
 DYKSTRA
 PLANNING

Subdivision, Rezoning, Structure Planning, Development Planning, Design, Advocacy

2953 Albany Highway,
 Kalamacott, WA 6111

T: 9495 1947
 F: 9495 1946
 admin@dykstra.com.au

7 August 2012

Figure 3

Notes:

- This document may only be used for the purpose for which it was commissioned and in accordance with the Terms of Engagement.
- The dimensions, areas & number of lots are subject to survey and also the requirement of all authorities.

1:1000 @ A3

129504.SPF3&4-120807

Attach 3

SCHEDULE OF SUBMISSIONS

STRUCTURE PLAN PROPOSAL – LOT 3 & 4 WEST CHURCHILL AVENUE, MUNSTER

NO	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
1	Mr Robert Santich 17 Loris Way KARDINYA WA 6163	<p>Support</p> <p>I am writing to respond to your notice that appeared in the Cockburn Gazette advertising for submissions on the proposed structure plan relating to Lots 3 & 4 West Churchill Avenue, Munster.</p> <p>I am the registered proprietor of the subject land and hereby provide my full support to the structure plan and associated report.</p>	Support Noted.
2	<ul style="list-style-type: none"> Details to be kept confidential 	<p>Objection</p> <p>Being such a small cul-de-sac and a mother of 3 children the increased traffic in the cul-de-sac could be dangerous and noisy being that on the left side of Fiore court there are only 5 homesites and on the left there are a proposed number of 8.</p> <p>The smaller sites could decrease the value of our property as being smaller they will most likely be rentals. As well as the aesthetic look of fibre court once the smaller properties have been built, it will look ridiculous to have such big home sites on one side and such smaller sites on the other.</p> <p>All the residents on the left have small children and we are all concerned about this development.</p> <p>Second submission received: 17/07/12</p> <p>As a private citizen and owner of an affected property.</p> <p>With impending release of such small lots and the ability to have 2x homes per site, the increased traffic alone in such a small cul-de-sac would be a major concern. Parking would be a problem with the potential of 6x cars per site if each building was 3 x bedrooms.</p> <p>We also have 3 children who play outside and ride their bikes often. Cars reversing would be a danger. Our property value would likely be decreased and what is a nice family cul-de-sac would be ruined.</p> <p>We bought our property because of the beautiful and very quiet location and also the size of the land. We have built a beautiful large family home and to have multiple home sites opposite in such smaller sizes has to decrease the value of our property.</p>	<p>Existing lots fronting the southern portion of Fiore Court are zoned R20, 630m² and 20m in width. The two existing lots on the northern side of Fiore Court are zoned R20, 570m² and 16m wide. The proposed lots are 13m wide and 476m².</p> <p>The size, width and density of the propped lots are in keeping with the density requirements of Directions 2031. The base coding of R25 is a low residential zoning and will not impact negatively on the amenity, streetscape or building bulk of Fiore Court.</p> <p>It is acknowledged that allowing development to the level of R40 along Fiore Court may of lead to an alteration of the streetscape away from the existing traditional feel of the street as well as issues with amenity.</p>

Attach 4

NO	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>The traffic would substantially increase as well as noise. The street would look mis-matched with large home sites on one side and multiple smaller sites on the other. The option also for a potential buyer to buy 2 x sites and put four buildings on it is very disappointing. The sites are too small for this in comparison to the size of the cul-de-sac. The traffic impact alone related to this potential is mind boggling.</p> <p>Our street would be ruined with beautiful bigger home sites on one side and smaller on the other side.</p>	<p>Post advertising the split coding has been removed from Fiore Court. The 5 proposed lots are now coded R25 and development will therefore be restricted to single residences due to the size of the proposed lots.</p> <p>The development of R60 lots at the end of Fiore Court will allow for the full development of WPWWTP Buffer impacted land and will feasibly lead to a lowering of blighted land in the immediate area.</p>
3	<p>Craig & Rebecca Newman 11 Fiore Court MUNSTER WA 6166</p>	<p>Objection</p> <p>As a young family (3Y/O and 37 weeks pregnant) we have invested everything into the future health and safety of our children. We saw the opportunity to move from cramped inner suburbs with high traffic volumes, noise and disconnected community, to the safety of a quiet cul-de-sac with enough land to grown and like minded neighbours.</p> <p>To date our community and specifically our street have proven to embrace the same ideas.</p> <p>Although this proposal structure constitutes a small pocket in the immediately developing area, it would obviously impose substantial change to the immediate surrounding lots, most notably to our street and more specifically to our home and family.</p> <p>Given the existing plan and development it was not and remains unreasonable to assume such a disruptive and discontinuous capitalist venture would be allowed to violate our community and incidentally our future prospects.</p> <p>As it stands the proposed structure will cause a visual, physical, financial and community divide to our street, and stands only to provide immediate profit to people operating without ties or regard to our community.</p>	<p>Existing lots fronting the southern portion of Fiore Court are zoned R20, 630m² and 20m in width. The two existing lots on the northern side of Fiore Court are zoned R20, 570m² and 16m wide. The proposed lots are 13m wide and 476m².</p> <p>The size, width and density of the propped lots are in keeping with the density requirements of Directions 2031. The base coding of R25 is a low residential zoning and will not impact negatively on the amenity, streetscape or building bulk of Fiore Court.</p> <p>It is acknowledged that allowing development to the level of R40 along Fiore Court may lead to</p>

NO	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
			<p>an alteration of the streetscape away from the existing traditional feel of the street as well as issues with amenity.</p> <p>Post advertising the split coding has been removed from Fiore Court. The 5 proposed lots are now coded R25 and development will therefore be restricted to single residences due to the size of the proposed lots.</p> <p>The development of R60 lots at the end of Fiore Court will allow for the full development of WPWWTP Buffer impacted land and will feasibly lead to a lowering of blighted land in the immediate area.</p>
4	<p>Lauren Leslie 16B West Churchill Avenue MUNSTER WA 6166</p>	<p>We would like the proposed footpath to be moved to the opposite side of the park. Also the two R40 blocks the houses to built in the further sides of the blocks not overlooking the parks.</p>	<p>The exact location of the footpath will be determined at a later stage. However locating the footpath closer to the R40 coded lots is a desirable outcome so as to clearly delineate the public realm from the private realm.</p> <p>The location of dwellings on R40 lots will largely be dictated by a Detailed Area Plan, to be prepared as part of the subdivision process. However, it should be noted that the location of habitable areas is preferred to be in a location where overlooking of the area of POS is possible. This is in</p>

NO	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
			<p>keeping with Liveable Neighbourhood principles. The minimum distance from the R40 coded land to 16B West Churchill Avenue is 8m therefore overlooking should be minimal and not invasive on the existing residents.</p> <p>Comments are therefore noted though not supported.</p>
5	Lauren Taylor, State Heritage Office	<p>Support</p> <p>Thank you for your referral received 13 July 2012 for the abovementioned proposal.</p> <p>I wish to advise that we have no comment in relation to the proposal, as it does not appear to impact upon any place of State cultural heritage significance.</p>	Comment Noted.
6	Brett Dunn Department of Water PO Box 332 MANDURAH WA 6210	<p>Thank you for the referral of the above Local Structure Plan (LSP) received with correspondence dated 12 July 2012. The Department of Water (DoW) has reviewed the proposal and wishes to provide the following advice: Better Urban Water Management</p> <p>Consistent with the Department of Planning (DoP's) Better Urban Water Management (BUWM) document and the policy measures outlines in State Planning Policy 2.9 the proposed Local Structure Plan should be supported by a Local Water Management Strategy (LWMS) prior to the approval of the proposed Local Structure Plan.</p> <p>The DoW has previously advised that due to the size of the site, an Urban Water Management plan (UWMP) would be deemed appropriate to support the LSP instead of a LWMS.</p> <p>The DoW has previously reviewed an UWMP (Shawmac, May 2012) for this site and found the document unsatisfactory in its current form. Comments were issued to the proponent identifying required modifications to the UWMP, in correspondence dated 4 July 2012. Accordingly the LSP should not be finalised prior to the endorsement of a satisfactory UWMP by the DoW and the City of Cockburn.</p> <p>If you wish to discuss the above further please contact Shelley McSpirit at the DoW's Mandurah Office on (08) 9550 4222.</p> <p>Second submission received via email 25/07/12 – 12/33051</p>	<p>Note second submission clarifying the Department of Waters response to the Local Structure Plan.</p> <p>Due to the size of the site the LSP will only require an Urban Water Management Plan at Subdivision Stage and none at the Local Structure Plan Stage.</p>

NO	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>Thank you for your e-mail, we wish to clarify expectations of water management requirements for the Local Structure Plan (LSP) for Lot 3 and 4 West Churchill Avenue, Munster. Please consider this advice an update to the Department of Water's (DoW's) comments provided in correspondence dated 18 July 2012.</p> <p>It had previously been agreed with the proponent, and in consultation with the City's Sabbir Hussien, that given the proposed LSP is small in nature, on a relatively unconstrained site, and intends to link to pre-existing drainage infrastructure, a Local Water Management Strategy was not considered necessary to support a Local Structure Plan (LSP).</p> <p>It was concluded that, supporting information should be included in LSP application to demonstrate the site layout (i.e. land set aside for POS/drainage) could adequately manage stormwater, and that sufficient capacity existed to manage water from the site.</p> <p>An Urban Water Management Plan (UWMP) will be required as a <i>condition of subdivision</i> in the future, in accordance with Better Urban Water Management (WAPC, 2008), and will provide a greater level of information for water management strategies and infrastructure to be implemented on the site.</p> <p>Though the information regarding drainage provided within the LSP report is not comprehensive, it is considered adequate as enough land has been provided for drainage/POS to manage the sites stormwater. Accordingly, the Department has no objections to the proposed LSP for Lot 3 and 4 West Churchill Avenue, Munster.</p> <p>I apologise for the initial confusion. The proponent had lodged a draft UWMP with the Department, and a subdivision application had been received from the Western Australian Planning Commission all at the same time as the City of Cockburn's referral for the LSP had been received. Thus there was some initial procedural confusion.</p>	
7.	Sally McGann, Department of Indigenous Affairs PO Box 3153 EAST PERTH WA 6892	<p>I write with reference to your letter dated 12 July 2012 regarding the above listed Proposed Structure Plan (the Plan). The Department of Indigenous Affairs (DIA) has conducted a review of the information submitted within your letter and I can confirm that there are no sites as currently mapped on the Register of Aboriginal Sites within the Plan area.</p> <p>All persons employed or engaged in the Plan area should be made aware of their obligations under the <i>Aboriginal Heritage Act, 1972</i> (AHA). Should cultural or skeletal material be discovered during the project, work should cease immediately and the site should be recorded and the DIA notified. Additional information on the AHA can be found on the DIA website, under Heritage and Culture. http://www.dia.wa.gov.au/Heritage--Culture/</p>	Comment Noted.

NO	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>The following is a link to the Due Diligence Guidelines, which parties operating within the Plan area could use to assess their risk with regard to the AHA: http://www.dia.wa.gov.au If you require any further information please contact Sally McGann on 6551 8075 or Sally.McGann@dia.wa.gov.au.</p>	
8.	<p>Ashis Parajuli, Main Roads WA PO Box 6202 EAST PERTH WA 6892</p>	<p>Support</p> <p>Thank you for your letter dated 12 July 2012 requesting Main Roads comments on the above proposed structure plan.</p> <p>Main Roads has no objection to the proposed Structure Plan.</p> <p>If you require any further information please contact Ashis Parajuli on (08) 9323 4286. In reply please quote file reference 04/11588-07 (D12#221474).</p>	<p>Comment Noted.</p>
9.	<p>Stefan de Haan Department of Environment and Conservation PO Box 1167 Bentley Delivery Centre WA 6983</p>	<p>Support</p> <p>I refer to your letter of 12 July 2012 requesting the Department of Environment and Conservation's (DEC) comments on the structure plan to facilitate residential development of a portion of Lot 4 and all of Lot 3 West Churchill Avenue, Munster. DEC advises it has no objection to the proposed structure plan however provides the following comments.</p> <p>Woodman Point Waste Water Treatment Plant Buffer : DEC notes that a portion of Lot 4 is currently zoned in the Metropolitan Region Scheme as Urban Deferred. This is due to the provision of an 800m odour buffer to the Woodman Point Waste Water Treatment Plant (WWTP), which precludes residential development until issues surrounding the odour emissions from the WWTP are resolved. It appears however that the current structure plan design does not include any residential lots within this portion of Lot 4 and DEC therefore has no objections to the current structure plan design.</p> <p>Conflicting Land Uses: As noted at Section 5.3.3 of the structure plan, there is a current operational flower market garden immediately abutting the southern boundary of Lot 4. DEC therefore advises that the Western Australian Planning Commission's Planning Bulletin 63 - <i>Policy for Dealing with Potential Conflicts Between Residential Subdivision and Market Gardens in East Wanneroo</i> should be considered in assessing any future subdivision within 300 metres of the operating market garden.</p> <p>Thank you for the opportunity to comment on this structure plan. Please contact Jacqui Maguire at DEC's Swan Coastal District on 9303 77 43 should you have any further queries regarding this advice.</p>	<p>Comments Noted.</p>

NO	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
10.	Jim Dodds, Department of Health PO Box 8172 Perth Business Centre WA 6849	<p>Thank you for your letter dated 12 July 2012 requesting comment from the Department of Health (DOH) on the above proposal. The DOH provides the following comment:</p> <p>1. Water and Sewerage Indicative developments to the density of R25 to R60 are required to connect to reticulated sewerage by the <i>Government Sewerage Policy - Perth Metropolitan Region</i>.</p> <p>2. Toxicological Evaluation The proposed local structure plan suggests that amalgamation of lots with split coded densities do not require a Detailed Area Plan (DAP). The DOH is of the opinion that the development and subdivision requirements of these split coded lots should fall under the higher densities requirement. Therefore R40/R25 coded lots will not only have the R40 code activated but require a DAP. The DOH does not support high residential densities in areas adjoining the Woodman Point Waste Water Treatment Plant (WPWWTP) buffer. Should the buffer be extended, sensitive receptors within the subject area will be affected by spray drift, dust and odour issues. The DOH reiterates that there should be no part of residential properties or areas used for various residential purposes including open spaces within the buffer area.</p> <p>The DOH also does not endorse the notification of the presence of or possible future health impacts on land sale contracts within 300m buffer zones of market gardens as there should be no development within this area unless the land has been assessed and remediated to remove any potential health impacts.</p> <p>3. Mosquito-borne Disease Control Programs and Services The subject land is within 3km of mosquito dispersal distance from saltmarsh mosquito breeding sites near Lake Coogee and surrounding waterways. Mosquitoes will disperse from these sites to the subject land under favourable environmental conditions. Additionally, there is the potential for mosquitoes to breed in on-site infrastructure and constructed water bodies if they are poorly designed or maintained.</p> <p>Developments in locations within close proximity to mosquito breeding habitat (either on-site or off-site) present a significantly high risk to future residents contracting the debilitating diseases, Ross River virus (RRV) disease and Barmah Forest virus (BFV) disease. For this reason, careful planning and consideration must be given to the design and location of 'liveable communities'.</p> <p>It is the recommendation of the Department of Health that:</p>	<p>Objection noted however not supported.</p> <p>The lots coded R25/R40 have been altered following community consultation to R25. As such no DAP will be necessary.</p> <p>The Department of Health's submission does not reflect how the Department of Planning and WAPC have viewed and applied decision making in respect of the WPWWTP Buffer. The buffer currently exists as a line which residential development is not permitted to take place within. To now start viewing residential development outside the buffer as not appropriate, would create an unworkable and improper application of the planning process and Scheme, and would not constitute orderly and proper planning. In the past Council has adopted and the WAPC has endorsed a number of Local Structure Plans within the locality where residential uses directly abutting the WPWWTP Buffer.</p> <p>Previous advice on the buffer is that an expansion of the boundary is not expected, nor would this be contemplated for any support by the City. Moreover previous advice from</p>

NO	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<ul style="list-style-type: none"> • The City of Cockburn ensures they have sufficient resources to continue mosquito management into the future; • The proponent must ensure proposed infrastructure and site works do not create additional mosquito breeding habitat as follows: <ul style="list-style-type: none"> • Changes to topography resulting from earthworks (e.g. the installation of pipelines, footpaths, roads etc) must prevent run-off from creating surface ponding as it may become mosquito breeding habitat; and • Water tanks and other water-holding containers must be sealed or screened to prevent mosquito access and breeding. Regular monitoring for mosquito larvae and treatment with larvicide may also be required; • Public open space with limited vegetation should be located between the mosquito breeding sites and residential areas to create an area that is refractory to mosquito dispersal, thereby reducing the number of mosquitoes impacting residents; and • The proponent should use built form design measures (insect screening on doors and windows and screened outdoor enclosures) in the construction of accommodation and recreational areas to protect future residents from mosquitoes. <p>4. Structure Plan Content</p> <p>This proposal provides the City of Cockburn the opportunity to minimise the land use conflicts and incompatible activities which is the most common issue in land use and development. Health impacts draw many attentions in those issues and hence, should be appropriately and adequately addressed at this level.</p> <p>Health concerns about buffers are not limited to the industries and infrastructures. Consideration must be given to the need for adequate buffers to protect residents from lifestyle and public health impacts such as mosquitoes, spray drift from agricultural activities or odour for example. You should consider incorporating Health Impact Assessment (HIA) and/or Public Health Assessment (PHA) principles in your decision making process. For your information and guidance, you may access the relevant information at the following sites: HIA- http://www.public.health.wa.gov.au/2/1400/2/health_risk_assessment.pm Should you have queries or require further information please contact Vic Andrich on 9388 4978 or victor.andrich@health.wa.gov.au.</p>	<p>the Department of Water and the Department of Planning has advised that upgrades to the WPWWTP will likely see the affect on nearby residents reduced going forward.</p> <p>Council has consistently stated that it will not support new habitable dwellings being located within the buffer and this proposal continues that trend.</p> <p>An additional item has been included in the officer recommendation for officers to write to the Department of Health to request an explanation to clarify their position on this matter.</p> <p>With regard to the location of the local structure plan in relation to operating market gardens. The approach undertaken in this proposal is consistent with that outlined in the WAPC's Planning Bulletin No. 63.</p> <p>Concerns around the proximity of the site to mosquito breeding grounds noted.</p>



Government of Western Australia
Department of Planning

Your ref: 3400024
Our ref: DP/11/00295/6
Enquiries: Neil Fox
Telephone: (08) 6551 9291

Mr Andrew Trosic
Manager Strategic Planning
City of Cockburn
PO Box 1215
BIBRA LAKE DC WA 6965

Dear Mr Trosic

WOODMAN POINT WASTE WATER TREATMENT PLANT ODOUR BUFFER

I refer to your letter of 24 April 2012 in regard to the Woodman Point Waste Water Treatment Plant (WPWWTP) Odour Buffer, written to the Chairman: Western Australian Planning Commission (WAPC), and the Director General, Department of Planning. It is noted that it is very similar in content to your letter of the same date written to the Minister for Planning; Culture and the Arts; Science and Innovation.

Although you have written to both the Chairman and the Director General, I respond on behalf of both as Acting Director General.

As outlined in Minister Day's letter of 6 June 2012, it is important to note that odour is acknowledged to be the key issue associated with the WPWWTP. The Water Corporation is responsible for the operation of the plant, and is required to operate the plant in terms of the requirements stipulated by the Department of Environment and Conservation (DEC).

The Water Corporation implements an Odour Improvement Plan in order to reduce the impacts of the plant's operations. The Stage 1 odour control upgrade works at the plant were completed in July 2009, with the objective of achieving a 50% odour reduction.

The Water Corporation released the report *Results of the Odour Monitoring and Modelling Program (2010)*, for comment. The Water Corporation has now finalised its report in order to assess the success of the Stage 1 odour control upgrade works and this was issued to the DEC to close out the works approval for the upgrade.

The Water Corporation advised the WAPC in March 2012 that it had finalised its odour monitoring and modelling report, which recommends the retention of the existing 750 metre odour buffer.

Although the Odour Improvement Plan has resulted in the reduction of odour, it cannot guarantee that there will not be odours from the plant. The report indicates that there will still be an odour impact extending to roughly the eastern edge of the urban deferred land and accordingly, that the current buffer should be retained.

I understand that your Council would like to see the buffer reduced in extent, but it is important to bear in mind that a particular process needs to be followed before the review of the buffer can be finalised. It is likely that the Water Corporation's report will be an important input into the decision making process, and that any decision made by the WAPC will be guided by the precautionary principle.

Postal address: Locked Bag 2506 Perth WA 6001 / Street address: 140 William Street Perth WA 6000
Tel: (08) 6551 9000 / Fax: (08) 6551 9001 / corporate@planning.wa.gov.au / www.planning.wa.gov.au
ABN 79 051 750 680
wa.gov.au

I trust that this advice will be of assistance, but should you wish to discuss the matter any further please do not hesitate to contact Neil Fox, A/Director: Metropolitan South West (email: neil.fox@planning.wa.gov.au, telephone: 6551 9291).

Yours sincerely

A handwritten signature in black ink, appearing to read 'Neil Thomson', with a long horizontal flourish extending to the right.

Neil Thomson
A/Director General

3.1.8 / 2012



**Minister for Planning; Culture & the Arts; Science & Innovation
Government of Western Australia**

Our Ref: 33-16966
Your Ref: 3400024

RECEIVED at: CITY OF COCKBURN	
File #:	<u>3400024</u>
x-ref:	
Action Officer	CC
<u>A. TRO</u>	7 JUN 2012
Retention	DAYBOX
	Manual File
	Archive FILE
LOOSE / ATTACHED	File please:

Mr Andrew Trosic
Manager Strategic Planning
City of Cockburn
PO Box 1215
BIBRA LAKE DC WA 6965

Dear Mr Trosic

WOODMAN POINT WASTE WATER TREATMENT PLANT ODOUR BUFFER

Thank you for your letter of 24 April 2012 regarding the Woodman Point Waste Water Treatment Plant (WPWWTP) Odour Buffer.

I note the City of Cockburn (City) Council's resolution on 12 April 2012 regarding the odour buffer.

From a reading of Council's resolution, it is apparent that while Council has a position for the extent of the buffer in the Lake Coogee area, it may not be fully mindful of the process which needs to be followed in order to finalise the determination of the buffer in this area.

The WPWWTP serves a large portion of the metropolitan region, and the *Review of the Kwinana Air Quality Buffer – Position Paper 2008* (the 2008 Review) highlighted the fact that the plant is likely to increase its capacity as growth and development extends southwards.

The 2008 Review identified the potential need for a 750 metre buffer encompassing urban deferred land (private land) east of Lake Coogee, as well as the public purposes reserve north of the plant.

Odour is acknowledged to be the key issue associated with the plant. The Water Corporation is responsible for the operation of the plant, and is required to operate the plant in terms of the requirements stipulated by the Department of Environment and Conservation (DEC).

11th Floor, 2 Havelock Street, West Perth, Western Australia 6005
Telephone: +61 8 6552 6200 Facsimile: +61 8 6552 6201 Email: Minister.Day@dpc.wa.gov.au

The Water Corporation implements an Odour Improvement Plan in order to reduce the impacts of the plant's operations. The Stage 1 odour control upgrade works at the plant were completed in July 2009, with the objective of achieving a 50% odour reduction.

The Water Corporation released the report *Results of the Odour Monitoring and Modelling Program (2010)*, for comment. The Water Corporation has now finalised its report in order to assess the success of the Stage 1 odour control upgrade works and this was issued to the DEC to close out the works approval for the upgrade.

The Water Corporation advised the WAPC in March 2012 that it had finalised its odour monitoring and modelling report, which recommends the retention of the existing 750 metre odour buffer.

Although the Odour Improvement Plan has resulted in the reduction of odour, it cannot guarantee that there will not be odours from the plant. The report indicates that there will still be an odour impact extending to roughly the eastern edge of the urban deferred land and accordingly, that the current buffer should be retained.

Now that the report has been received, it will be necessary for the Department of Planning to submit a recommendation on the extent of the buffer to the WAPC after consideration by the Western Trade Coast Industries Committee. In addition to this, the Water Corporation's advice together with advice from DEC, the Department of Health, and the City will be considered when the WAPC determines any request submitted by a landowner to lift the urban deferred zone in favour of an urban zone.

I am aware that concern has been expressed by a number of stakeholders and members of the public at the length of time taken to finalise the buffer in this area. However, it should be emphasised that the determination of the need for, and the extent of, the buffer could only be completed once the odour monitoring and modelling report was finalised.

Should you wish to discuss the points made in the Council's resolution further, please do not hesitate to contact Mr Neil Fox at the Department of Planning (email: neil.fox@planning.wa.gov.au, telephone: 6551 9291).

Yours sincerely



JOHN DAY
MINISTER FOR PLANNING;
CULTURE AND THE ARTS;
SCIENCE AND INNOVATION

06 JUN 2012

*(As you may be aware,
 I met with Member for
 Cockburn and Deputy Mayor
 on this matter yesterday.)*



GOVERNMENT OF
WESTERN AUSTRALIA

Office of the Environmental Protection Authority

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RECEIVED at: CITY OF COCKBURN	
File #:	<u>3400024</u>
x-ref:	
Action Officer	CC
<u>A.TRO.</u>	21 JUN 2012
Retention	DAYBOX
	Manual File
	Archive FILE
LOOSE / ATTACHED	File please:

Mr Andrew Trosic
Manager Strategic Planning
City of Cockburn
PO Box 1215
BIBRA LAKE DC WA 6965

Your ref 3400024
Our Ref A497171
Enquiries Amy Sgherza 6467 5424

Dear Mr Trosic

WOODMAN POINT WASTE WATER TREATMENT PLANT ODOUR BUFFER

Thank you for your letter dated 24 April 2012 in which you provide Council's position to support landowners in reducing the Woodman Point Waste Water Treatment Plant (WPWWTP) odour buffer back to the eastern edge of Lake Coogee. In response to this the Office of the Environmental Protection Authority (OEPA) makes the following comments.

The OEPA understands that the Department of Planning (DoP) is preparing a recommendation to the Western Australian Planning Commission (WAPC) regarding the adequacy of the current buffer. The DoP's recommendation to the WAPC will be informed by advice received from the Water Corporation, Department of Environment and Conservation (DEC), Department of Health and the City of Cockburn, and will be used to consider the application to lift the urban deferred zone in the buffer area to urban.

It is the OEPA's strong preference for amendments to the City of Cockburn's Local Planning Strategy and scheme, where it relates to the buffer definition, to be aligned with the outcome of the DoP's process and ultimately the decision of the WAPC. Hence, it is recommended that the amendments to the local planning strategy, as set out in Council's resolution, should not be initiated until the WAPC has considered the advice from the relevant agencies and made a decision on the matter.

In relation to your comment about the scientific justification of maintaining the current odour buffer, the DEC is best placed to provide information on the technical aspects of studies undertaken to date by the Water Corporation and it will continue to provide this advice to the DoP during the buffer review.

However, it should also be noted that the WPWWTP serves a large portion of the metropolitan region, and the *Review of the Kwinana Air Quality Buffer – Position paper 2008* (2008 review) highlighted that the plant is likely to increase its capacity as growth and development extends southwards. The 2008 review identified the potential need for a 750 metre buffer encompassing urban deferred land east of Lake Coogee.

With respect to your question about the role of the EPA, I can advise that the EPA undertakes the environmental impact assessment (EIA) of significant proposals and planning schemes referred to it under Part IV of the Environmental Protection Act 1986 (the EP Act). The EPA also prepares Strategic Advice to Government under section 16(e) of the EP Act and this tends to be related to a specific project or environmental issue. As you are aware, in 2006 the EPA was requested to provide advice to the then Minister for Environment on the buffer requirements for the Water Corporation's WPWWTP by way of section 16(e) (EPA Bulletin 1240).

With respect to the *Environmental Protection (Kwinana) (Atmospheric Wastes) Environmental Protection Policy 1999* (EPP), the then Minister for Environment directed the EPA not to undertake a review of the Kwinana EPP and this was gazetted in November 2010. The decision of the Minister was on the basis of consultation and EPA advice, where the EPA advised that the continuation of the current Kwinana EPP is appropriate and does not warrant conducting a statutory review. However, the Minister may choose to direct the EPA to undertake a review of the Kwinana EPP within a period of seven years from the date of the gazettal.

Should you wish to further discuss the above matter, please contact Amy Sgherza on 6467 5424 in the first instance.

Yours sincerely



Anthony Sutton
Director
Assessment and Compliance Division

19 June 2012

LOT 65 ERPINGHAM ROAD, HAMILTON HILL





- Shops
- Primary School
- Dry Landscaped POS
- Active & Enhanced POS
- Bushland
- Landscaped Entry Statement
- Entry Treatment to Pavement
- Treatment to Pavement at Traffic Control Devices
- Streetscape Treatment
- Street Trees
- "Phoenix Rise" Boundary
- POS Dual Use Paths
- Road Links
- Development Areas
- Detailed Area Plan Required to Guide Future Development
- 400m Walkable Catchment

MASTER PLAN

PHOENIX RISE - NEW LIVING PROJECT

MASTER PLAN - NEW LIVING PROJECT - PHOENIX RISE (SOUTHWELL)

Location

Southwell is located within the suburb of Hamilton Hill and is bounded by Blackwood Avenue and Forrest Road to the north, Stack Road to the east, Phoenix Road to the south and Rockingham Road to the west.

The area is very well positioned being within close proximity of Fremantle, being close to employment centres such as O'Connor, Bibra Lake, Jandakot and Henderson, close to Jandakot Airport and also nearby to health and educational facilities at Murdoch.

Attributes

The location of the area with regard to the local, district and regional facilities listed above and proximity to the Perth Central Business District is a key attribute for Southwell.

The area also benefits from an abundance of land available for recreational purposes, the convenience of having a small local shopping centre, a primary school, a range of housing choices (traditional homes, villa style homes and aged persons dwellings) and a number of community facilities such as halls and activity centres.

An oil pipeline also traverses the site from the south-east to the north-west. This corridor provides an opportunity to enhance pedestrian linkages (parts of the corridor already contain pathways), subject to agreement from the landowner and approval by relevant authorities.

Constraints

The design of the Southwell area is one that was based on a historic style of planning.

The intent of this planning was to create a unique high quality urban and recreational environment for residents. Land lots were created that directly abutted public open space so that residents could enjoy a parkland setting and could walk freely along landscaped corridors.

It was intended to also create a quiet street environment, comprising a series of culs-de-sac. As the cul-de-sac street pattern restricted not only the movement of vehicles but also that of pedestrians, a need was created for a system of 'Pedestrian Access Ways' (PAW's) designed to allow pedestrians reasonably direct access throughout Southwell.

Although at the time of its inception this planning was considered to be at the forefront of urban design, the actual result of this design has been very different to that intended. This is in part due to the nature of many homes having two frontages, a street frontage and a rear parkland frontage. Successful development of the area was thus contingent upon homes being built that addressed both the street and overlooked the parkland, as opposed to conventional homes that are designed to address the street but have a private, fenced back yard.

The result of this planning in Southwell, has been as follows:

- Homes have not been designed and oriented to overlook abutting recreational areas and solid rear fencing has been constructed to provide privacy for residents. Thus the majority of Southwell's public and community spaces, including the primary school, are surrounded by closed solid fencing, not open roadways;
- As a result of the above, there is little or no surveillance of public open space areas;
- This lack of surveillance means that people are less likely to feel comfortable being within that space;
- With less people using the public open space the safety and also physical maintenance of these areas has diminished;
- The lack of surveillance of public open space combined with some blocks having solid rear fencing directly adjacent to public open space areas also poses a home security issue; and,
- The pedestrian access ways throughout Southwell are on the whole too narrow and indirect to provide residents with a feeling of safety.

The design of Southwell had thus shaped the social fabric of the area, as described above. It has also shaped the physical fabric of the area, as because the road system is characterised by a number of culs-de-sac, vehicle movement throughout the area is constrained. This is particularly noticeable with a lack of easy access to the primary school from the eastern portion of the area and a lack of access to the shopping centre from the western portion of the area. Linkages north-south through the area are also constrained.

Planning of new urban areas has changed considerably over time to address these design and safety issues. In fact, the Western Australian Planning Commission no longer support the provision of 'Pedestrian Access Ways' (PAW's) of the size of those within Southwell (3 to 4 metres in width) and of a windy nature (where you cannot see from one end to the other).

The Commission are also opposed to the widespread use of a cul-de-sac street pattern due to the movement constraints it places on a community.

The Southwell community has been previously consulted with regard to the issues that are of concern to residents. Many of the issues raised relate to the aspects outlined above regarding the design of the area. These concerns are described further below.

Previous consultation with a 'representative group' of the Southwell community, revealed the following issues as being of concern to residents of the area:

- Environmental aesthetics (including presentation of some areas of public open space);
- Lack of personal security/home security (related to the lack of surveillance and orientation of homes to public open space areas);
- Graffiti;
- Fear in the community;
- Lack of community spirit; and,
- A lack of a sense of place/community pride.

In a door to door survey of residents in November 2003 the following two issues were revealed as being the overwhelming concerns of the community:

- Crime and safety; and,
- Anti-social behaviour/graffiti and vandalism.

Careful design of the urban environment can assist in addressing the above issues. The key objective of the Master Plan for Phoenix Rise (Southwell) is to encourage a sense of place and pride in the community. By addressing surveillance of public spaces the plan aims to improve public safety, reduce the likelihood of crime, encourage the use of parklands and thus create positive community interaction.

Master Plan Proposals

The Master Plan incorporates a number of primary elements that focus on achieving the key objectives of reducing crime and increasing safety.

Connection of the road system and creation of more usable pedestrian pathway corridors of an 'open' nature is important to enhance access and safety within Southwell.

By reducing the amount of rear fencing that abuts the public open space and oil pipeline corridor and providing for development of new blocks of land and redevelopment of existing blocks/homes to overlook these areas, the safety of both the public and private spaces can be enhanced.

With homes orientated to look over the public areas, residents will feel more comfortable being within these spaces and the reduction of rear boundary fencing abutting these areas, combined with the houses overlooking them, will assist in reducing the opportunity for crime. Reduction of the potential for crime and creation of parkland areas that are more open and useable will encourage community interaction, leading to community pride and the creation of a strong sense of place for Southwell. This will then enhance the environmental, social and economic value of the area.

The attached Master Plan shows a number of Precincts and describes the proposals within these Precincts.

The plan aims to provide and enhance street linkages throughout the area to allow far better access to the primary school and shopping centre and to provide on street movement for both vehicles and pedestrians, thereby facilitating the closure of all but two pedestrian access ways.

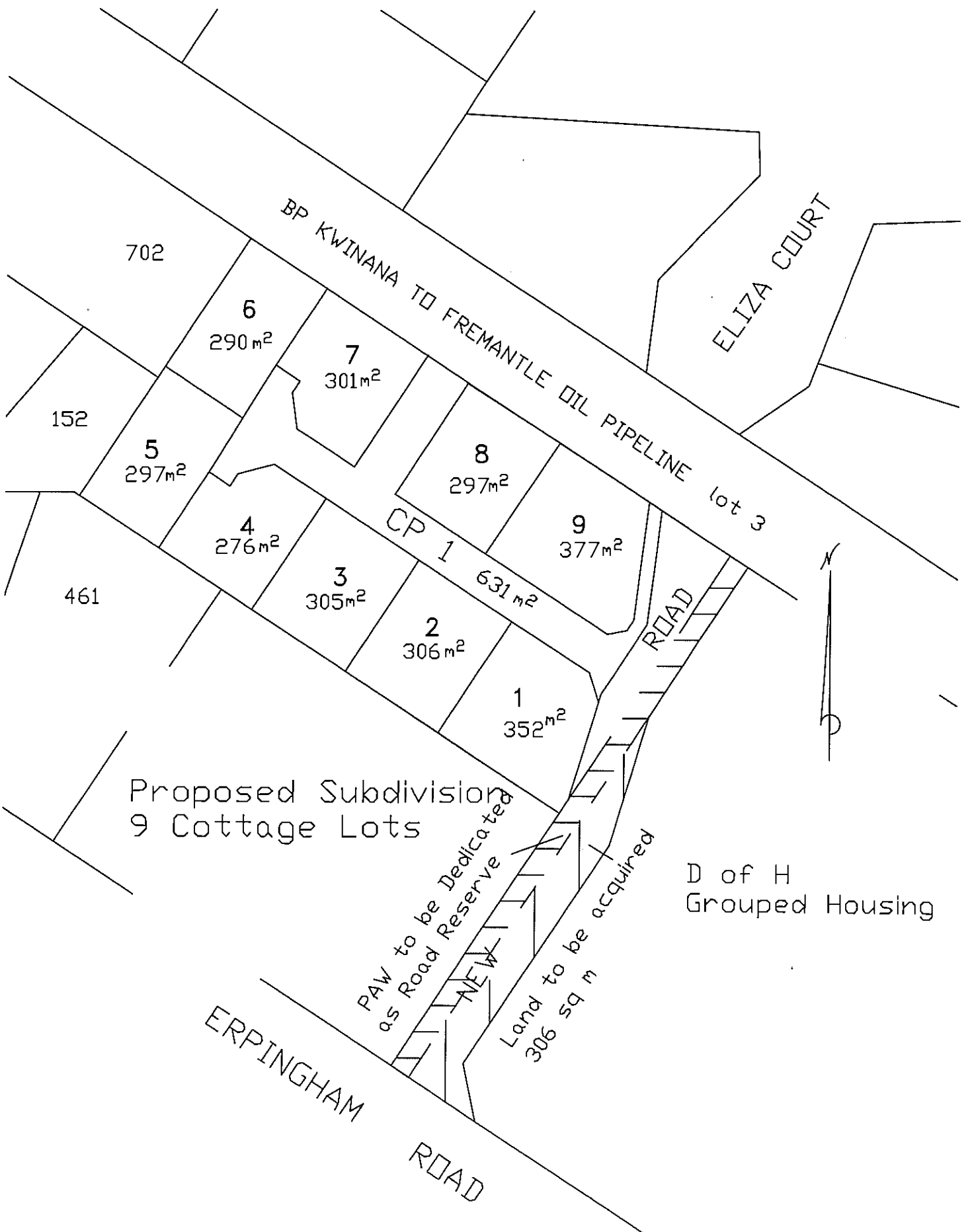
Allowing for residents to walk on streets and within 'open' parkland areas rather than within narrow fenced pedestrian access ways will significantly improve the feeling of safety and security.

The plan proposes to rationalise some areas of existing public open space and to create residential development within these areas. The public open space areas to be redeveloped are those that are currently underutilised and/or hidden from general public view. Redevelopment of these 'hidden' areas reduces the opportunity for crime and results in the creation of new homes 'overlooking' the parkland areas that will also increase surveillance and thus encourage use of these areas.

The plan also shows areas that are proposed to be subject to future Detailed Area Plans. The purpose of these plans will be to guide owners through the possible future development of their land to encourage the siting and orientation of homes to overlook abutting parklands where possible. Appropriate redevelopment over time will not only increase surveillance and security of public and private spaces, it will also add value to the area.

The Sykes Place Precinct also seeks to significantly increase safety of the area, by introducing new development to overlook the parkland and primary school. It also introduces new roadways to improve linkages between the eastern/northern parts of Southwell, the primary school and western portions of the area.

The Master Plan also shows a number of potential sites for the creation of a new community building. The existing community building site is not ideally located and redevelopment of this land can provide for better surveillance over the oil pipeline corridor (and pathways within this corridor). A new community building site adjacent to Southwell Crescent will be more accessible by public transport, will have higher exposure to passing traffic and will be located on a main distributor road rather than a local street. There are three alternative site options shown for this facility and further research and negotiations will be required to determine the most appropriate location.



Inquiries: Andrew Trosic – 9411 3675
Our Ref: 2200882 12/

August 2012

Mail Merge

DEVELOPMENT OF LOT 65 ERPINGHAM ROAD, HAMILTON HILL (ELIZA PARK)

The City is currently progressing the development of Lot 65 Erpingham Road, Hamilton Hill. This land is shown following:



This land was rezoned for residential development in 2007, via Amendment No. 38 to City of Cockburn Town Planning Scheme No. 3. Similar to the recent subdivision of Council owned land in Grandpre Crescent, the subdivision of this land forms a key aspect of the Phoenix Rise Master Plan. The Master Plan incorporates a number of primary elements that focus on achieving the key objectives of reducing crime, increasing safety and revitalisation of the area overall. A copy of the Master Plan and summary is attached for your information, showing the land in question at Attachment 1.

The subdivision and development of Council owned land within the Phoenix Rise Master Plan forms a key part of the Master Plan. Lot 65 Erpingham Road represents the next land parcel identified for development. The Phoenix Rise Master Plan was prepared and advertised in 2005, and adopted in 2006 which the subsequent zoning changes were based. A copy of the adopted Council report is attached at Attachment 2.

As a part of the development the City will relocate the existing playground to Goodchild Park, which is approximately 400 metres to the north east. Existing grass trees will be relocated to other parks in the Phoenix Rise area, focussed upon the creation of appealing public parks which form an important component of the Master Plan.

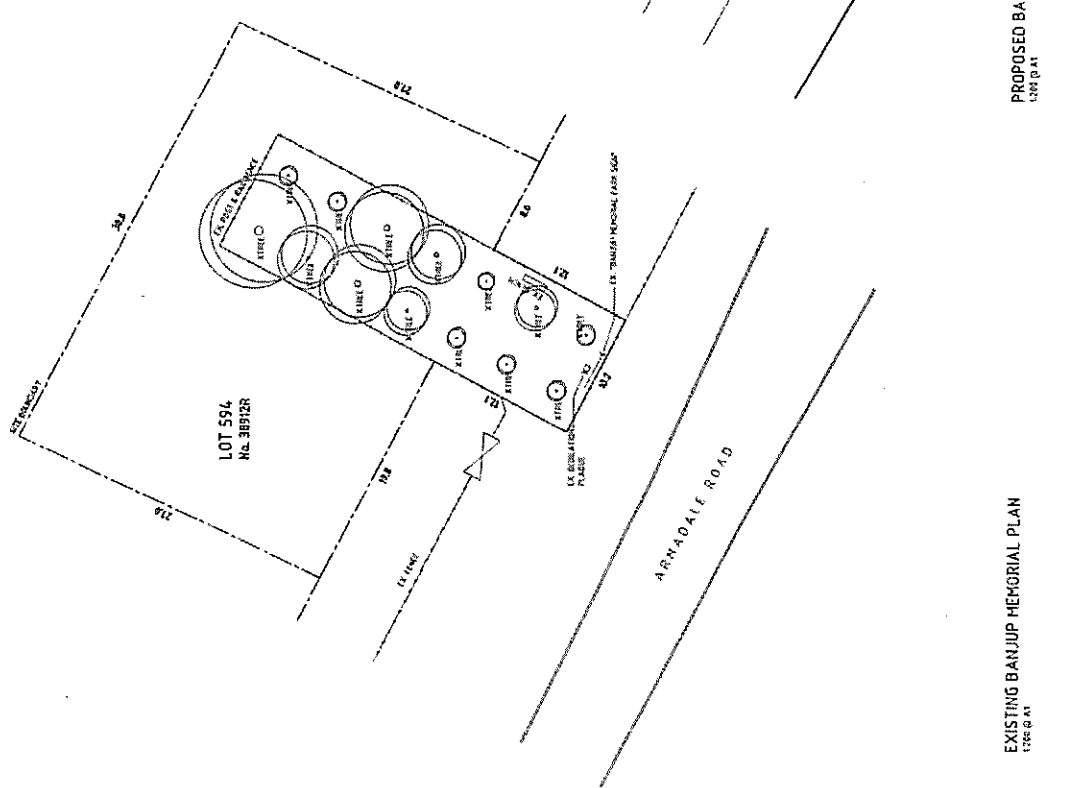
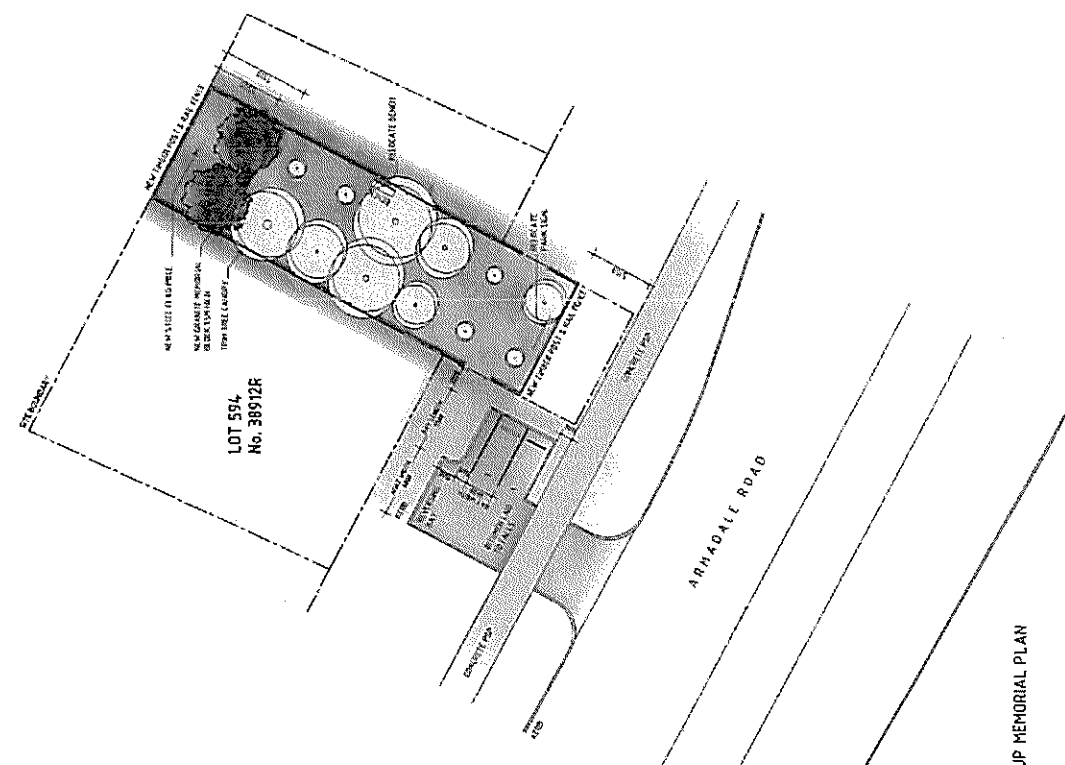
The two gum trees on the subject land will need to be removed to progress the construction of the subdivision, however the City has and continues to undertake plantings of trees in other surrounding areas to offset the removal of these trees.

If you have any comments in relation to this information please contact the undersigned on the above number.

Andrew Trosic
MANAGER STRATEGIC PLANNING

Enc: Attachment 1 - Phoenix Rise Master Plan and Summary
Attachment 2 - Copy of Council Resolution

- GENERAL NOTES**
1. NEW TREES TO BE PLANTED AS SHOWN ON THIS PLAN.
 2. LANDSCAPE TO BE MAINTAINED AS SHOWN ON THIS PLAN.
 3. EXISTING PLANTING TO BE MAINTAINED AS SHOWN ON THIS PLAN.
 4. EXISTING PLANTING TO BE MAINTAINED AS SHOWN ON THIS PLAN.
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 10. EXISTING PLANTING TO BE MAINTAINED AS SHOWN ON THIS PLAN.
- ABBREVIATIONS**
- D5 EXISTING DRIVE
- D6 EXISTING DRIVE
- D7 EXISTING DRIVE
- D8 EXISTING DRIVE
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- D10 EXISTING DRIVE
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Griffiths Architects

BANJUP MEMORIAL PARK

PROPOSED BOUNDARIES TO MAINTAIN EXISTING PLANTING TO BE MAINTAINED AS SHOWN ON THIS PLAN.

PROJECT NO.	DATE	SCALE
12/2011	13/9/2012	A.01

DESIGNER: GRIFFITHS ARCHITECTS

CLIENT: GRASSLANDS CITY COUNCIL

PROJECT: BANJUP MEMORIAL PARK

SCALE: A.01

PROPOSED BANJUP MEMORIAL PLAN
13/09/2012

EXISTING BANJUP MEMORIAL PLAN
13/09/2012







Report

Project Name:	Banjup War Memorial
Project Address:	Armadale Road Banjup
Project Number:	12146 HIS
Date:	8 June 2012

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PROPOSED MODIFICATIONS TO BANJUP WAR MEMROIAL,-LOT 594 RESERVE 38912 ARMADALE ROAD, BANJUP

HERITAGE IMPACT STATEMENT

This Heritage Impact Statement (HIS) evaluates the impact of the alterations to the Banjup Memorial Park, required to accommodate the proposed widening of Armadale Road. This HIS is part of a development application to the City of Cockburn and has been prepared in accordance with the requirements of the State Heritage Office standard brief for HIS.

The Banjup Memorial Park was constructed in 1920 and includes an avenue of 14 trees to commemorate the men from Banjup who served in World War I. It is included in the City of Cockburn's Municipal Inventory (LGI 002) as a Category B of Considerable Significance. The place has important social value for the local community and the Cockburn RSL.

The proposal includes fence relocation and the removal of the two southernmost trees, and the planting of two new trees to the land at the rear of the lot to accommodate the widening of Armadale Road. A number of improvements to the place are proposed to mitigate the effects imposed by this.

SUMMARY

The Banjup Memorial Park is listed in the City of Cockburn's Local Government Heritage Inventory (LGI 002) as a Category B of Considerable Significance. As such it is protected under the Town Planning scheme of the City of Cockburn and requires that a development application is made. This HIS is intended to provide a brief description of the development application and includes:

- a summary description of the proposed works; and,
- the response to heritage values and possible heritage impacts.

The Banjup Memorial Park is located on Lot 594, in the Rose Shanks Reserve near the intersection of Armadale and Warton Roads. It is surrounded by a white timber post and rail fence; 10.2 metres wide by 32 metres in length. A number of trees have required replacement over the years

Banjup War Memorial-Heritage Impact Statement.

Griffiths Architects

June 2012

so that now there is an assortment of tree species at varying ages. These include Jarrah (*Eucalyptus marginata*), Tuart (*Eucalyptus gomphocephala*), Grevillea and Red Flowering Gum (*Corymbia ficifolia*).

The memorial has become increasingly popular in recent years as more people become aware of its significance. This was particularly evident at 2012 ANZAC day service, which attracted approximately 150 people. Currently the memorial faces onto Armadale Road, with the southern portion of the fence very close to the road. Memorial service attendees stand between the memorial fence and the road for dawn service. The existing memorial plaque is small and set very low, so that few attendees can see it at services.

As part of the roadwork and due to the increasing recognition of the memorial it was considered that alterations undertaken to the memorial could include additional work to enhance the heritage values and the safety of the place.

Consultation with the Cockburn RSL was undertaken to ascertain the work required to mitigate the effects of the relocation works and retain the values of the place. The memorial will be shifted approximately 5 metres away from the road which will require the removal and planting of 2 trees. It is proposed to refurbish the existing white timber post and rail fence as it is currently in very poor condition. To enhance the social values of the memorial a new flagpole is to be installed at the rear of the place to service remembrance ceremonies and a more significant granite monument constructed to contain the original dedication plaque.

To provide safer access and egress to the sight, a new hard pull up zone with a small parking area is proposed.

There are no negative heritage impacts on the place as the park is to be upgraded to enhance the existing social values. From a cultural heritage point of view this project can be supported.

INTRODUCTION

Main Roads are undertaking road works to widen Armadale Road. A portion of the Banjup Memorial Park protrudes into the Armadale Road Reserve and it is proposed to relocate the front 5m of the park and re-vest it to the rear of the lot.

Purpose

This Heritage Impact Statement was commissioned to examine the possible heritage impacts of the development.

Background

Banjup Memorial Park was assessed by the City of Cockburn for the Local Government Inventory in 2009. A proposal to alter a heritage place requires a development application.

Key Documents

The key documents referred to in the development of this Heritage Impact Statement include: -

City of Cockburn Local Government Inventory LGI# 002

Proposed Modifications to Banjup Memorial Park Correspondence May 2012, City of Cockburn.

Griffiths Architects schematic design drawings dated November 2011.

A1.01 Site Plan

DESCRIPTION

Historic Overview

The Banjup Memorial Park was established in 1920 and initially comprised an avenue of 14 trees. A memorial plaque that reads 'Banjup Memorial Reserve' was dedicated to those men from Banjup who had enlisted in World War I indicating whether they had died or were wounded.

Physical Evidence

The memorial is located on Lot 594, in Rose Shanks Reserve near the intersection of Armadale and Warton Roads. It is surrounded by a white timber post and rail fence; 10.2 metres wide by 32 metres in length. As a number of trees have required replacement over the years, there is an assortment of tree species at varying ages, including Jarrah (*Eucalyptus marginata*), Tuart (*Eucalyptus gomphocephala*), Grevillea and Red Flowering Gum (*Corymbia ficifolia*). A dedication plate listing the men who served is located on a concrete plinth at the southern portion of the lot.

SIGNIFICANCE

The Municipal Inventory's assessment of significance has been accepted as the basis for the HIS.

The document states:

Banjup Memorial Park is associated with those men from the district who enlisted in World War I, and has social significance for the RSL and the descendents of those men commemorated.

Banjup Memorial Park has aesthetic value as a designed small park set within a natural bush area.

DEVELOPMENT PROPOSAL

Planning Guidance

Local Planning Policy, *Heritage Conservation Design Guidelines APD64* provides a layer of guidance. Its objectives are:-

- To ensure that works, including conservation, restoration, alterations, additions, changes of use and new development, respect the heritage significance associated with heritage places;
- To encourage opportunities for interpretation where it can enhance understanding and enjoyment of heritage places, and strengthen the relationships between the community and its heritage.
- To ensure that any works to the trees included on the Significant Tree List do not endanger the trees survival.

Significant fabric

The significance for the park is in the existence of the place itself and what it symbolises. The dedication plate is the most significant physical element within the place.

Proposed Demolition

The extent of proposed demolition is within the first 5 metres of the Armadale frontage. It includes the removal of two trees and the front portion of the timber post and rail fence. Two new trees will be planted in the land to the rear to replace the removal of the front trees. Flooded Gums (*Eucalyptus Rudis*) have been selected from the City's "Grow Local Plants" guide as they grow to a tall height and do well in the area

The timber post and rail fence will be refurbished and new portions constructed to match the existing so that the overall dimensions of the place will remain as they are at present.

Site Context

Banjup Memorial Park is located within the Rose Shanks Reserve near the intersection of Armadale and Warton Roads. It is situated in a large natural bush setting with the south boundary fronting the busy Armadale Road.

Armadale road functions as the sole entry to the memorial. The memorial is highly visible from the road at slow speeds and to pedestrians.

Streetscape

The Banjup Memorial Park will retain its visibility from Armadale Road. The memorial will be moved away from the road by approximately 5 metres so the natural bush context of the place will be preserved.

The proposed modifications are intended to improve the safety of the site but will also enhance its visibility from the road, to allow the location to be recognised as a place for remembrance. A flagpole located to the rear of the site and a new large granite commemorative block will allow the place to be readily identifiable from the road. A larger memorial will be a better focus for services.

HERITAGE IMPACT

Impact on Stated Values

For the purposes of the Heritage Impact Statement, the statement of significance in the Municipal Inventory has been used. Working through the local values, the following points should be noted:-

Value

Banjup Memorial Park is associated with those men from the district who enlisted in World War I, and has social significance for the RSL and the descendents of those men commemorated.

Impact

Consultation was undertaken with the Cockburn RSL local sub branch to ascertain their aspirations for the place in order to retain the social values associated with the memorial. The RSL expressed a strong desire for a new granite memorial block to be included as part of the development. A 1.5m high granite block with a commemorative inscription will be constructed and located to the rear of the lot in order to enhance the social significance of the place. The original dedication plate will be fixed to the new block and commemorative inscriptions are being devised by the RSL. A flagpole will be located to the rear of the site as part of the proposal so that the place can be used by the RSL and by the community for remembrance ceremonies. These works will enhance this value.

Value

Banjup Memorial Park has aesthetic value as a designed small park set within a natural bush area.

Impact

The relocation of the memorial 5 metres back from its current location will lessen the encroachment of the busy road and allow the context of the natural bush setting to be retained. The bench will be moved away from the disturbances of the road. The surrounding white post and rail fence will be refurbished as part of the works and the canopies of the trees pruned in order to enhance this value.

Positive Heritage Impacts

The positive heritage impacts include the retention of the majority of the trees, the provision of more substantial commemorative block and the inclusion of a flagpole to the rear of the lot. The beneficial outcome is that the provision of these new elements will allow the park to be properly used as a memorial and will promote the increasing popularity and community awareness of the place.

The development serves to promote the continued use of the place as a memorial park.

Negative Heritage Impacts and Mitigation

The negative impacts of the road widening on the place are to be mitigated by enhancing the heritage social values.

As native trees generally don't prosper after being transplanted, and since the place has a history of trees being replaced, the planting of two new trees to the rear is considered to be preferable. There are no significant elements that are impacted.

In any event, mitigation includes the retention of the significant items and their ongoing care, as well as underpinning the continuing use of the place as a memorial park, together with enhancements as noted above.

CONCLUSION

There are no negative heritage impacts on the physical cultural environment and all significant values are protected. The proposed road widening of Armadale road is not in conflict with the recommendations provided by the RSL and City of Cockburn and the modest upgrade of the park is beneficial for the ongoing use of the place. Aesthetic and social values are maintained and enhanced, so that the development is regarded as acceptable in cultural heritage terms.



PHILIP GRIFFITHS FRAIA RIBA M.ICOMOS
ABWA Reg.No. 1071
for Griffiths Architects

SCHEDULE OF RESPONSES

PROPOSED ADDITIONS AND ALTERATIONS TO EXISTING WAR MEMORIAL PARK – NO. 38912R (LOT 594) ARMADALE ROAD, BANJUP

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
1	State Heritage Office PO Box 7479 Cloisters Square WA 6850	<p>SUPPORT</p> <p>We have reviewed the application and as the place is not in the State Register of Heritage Places, there is no requirement for us to provide comment. We are happy for the City to determine the application in accordance with its Scheme.</p> <p>The use of a Heritage Impact Statement (HIS) is highly commended, and should give good guidance to the City when assessing and determining the application.</p> <p>If this is a HIS commissioned by the City, would you be happy for us to use this proposal as an example in our Local Heritage Assessment Workshops? We are always looking for good examples to show other Local Government staff.</p>	<p>Noted.</p> <p>In regards to the Heritage Impact Statement, after the application is determined at Council, the City will liaise with the applicants to confirm if they are happy for the Heritage Impact Statement provided to be passed on to the State Heritage Office.</p>
2	RSL WA Branch Incorporated PO Box 3023 East Perth WA 6892	<p>SUPPORT</p> <p>The Returned & Services League of Australia WA Branch Incorporated have no objection or comment in respect to the modifications to the memorial.</p>	<p>Noted.</p>

3	Cockburn RSL Sub Branch PO Box 309 HAMILTON HILL WA 6963	<p>SUPPORT</p> <p>Thank you for the plans of the Banjup Memorial, The City of Cockburn RSL has no objections to the proposals and I have attached the wording to be affixed to the granite plinth above the original plaque.</p>	<p>Noted.</p> <p>A condition has been recommended that an additional plaque is provided on-site in consultation with the Banjup Residents Group and Cockburn RSL Sub Branch.</p>
4	Mr. Ian Thurston Banjup Residents Group 207 Liddelow Road BANJUP WA 6164	<p>SUPPORT</p> <p>Thank you for seeking the views of Banjup residents concerning Main Roads proposed modifications to the Banjup Memorial Park should Armadale Road be widened.</p> <p>The Banjup Residents Group has sought the views of its members, of the Jandakot Volunteer Fire Brigade who conduct an ANZAC Day ceremony at the Park, and of Steve Treeby, a descendent of some of the men commemorated at the Park. Steve planted a replacement tree at the Park on ANZAC Day 2011. With a couple of reservations, Banjup residents are supportive of the proposed modifications to the Park.</p> <p>We suggest that the Park be modified as proposed but with an additional plaque to remember the men of Banjup who served their country in wars subsequent to World War I. We would welcome being consulted on the design of the new plaque.</p>	<p>Noted.</p> <p>In regards to an additional plaque being provided, the City has recommended a condition that an additional plaque is provided on-site in consultation with the Banjup Residents Group and Cockburn RSL Sub Branch.</p>

PROPOSED SITE PLAN

Sheet 2 of 8

Site Coverage
 Floor Area = 936.45m²
 Site Area = 1497m²
 Site Cover = 55.18%

This site has been given a 50% 17% 14% wind class allocation

PAVING AREAS
 Area: 1128.83m²
 Perimeter: 537.07m

R60

Developptions



CONTOUR & FEATURE SURVEY

406 CARRINGTON STREET, HAMILTON HILL

CITY OF COCKBURN

Job Number: 11/5120

Client : ROLSTON

TITLE DETAILS

LOT 509 ON DIAGRAM 59387
 CERTIFICATE OF TITLE:
 Vol 1569 Folio 28

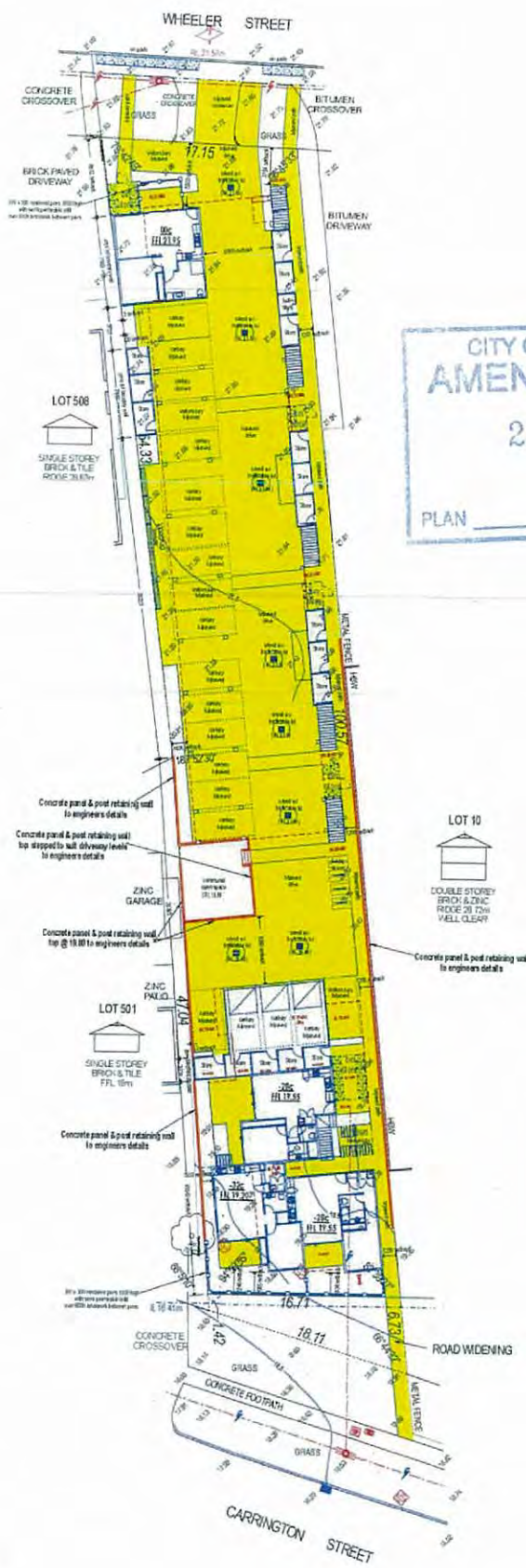
Area : 1629m²

LEGEND

	Sewerage line with invert level		Spot height in metres
	Sewerage manhole		Power pole
	Water meter		Telephone pit
	Gully pit & bin		Street Sign
	Significant tree		Stormwater drain
	High back wall		Overhead power
	Fibre optic		Sleep connection

**CITY OF COCKBURN
 AMENDED PLAN
 20 JUL 2012**

PLAN _____ OF _____



Drafted by: SJ
 Scale 1:200 on A1
 Date 23/08/2011

SURVEYOR SITE INSPECTION

SITE FEATURES	COMMENTS
Electricity	Overhead Connection
Gas meter	Yes (not sighted)
Water meter	Yes (sighted)
Sewerage Connection	Yes (sighted)
Tele Com	Yes
Road	Bitumen
Kerb	Concrete: Semi-mountable
Crossover	Concrete
Driveway	Bitumen
Dwelling	Split Level
Dwelling Foundation	Concrete
Vehicle Parking	Bitumen Caspax
Eaves	None
Fencing	Metal and Fibro in Poor Condition
Vegetation	Well kept
REPEG	NOT COMPLETED

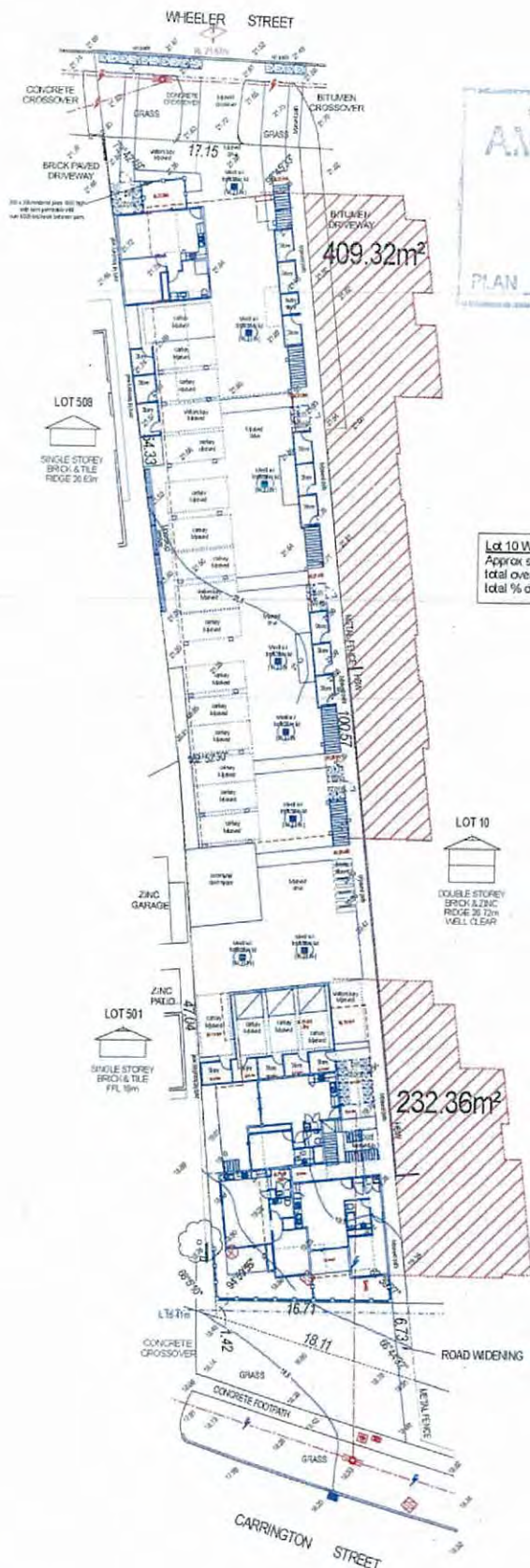
1) THE POSITION OF FEATURES AND LEVELS ARE PLOTTED FROM EXISTING FENCES AND CAN'T BE RELIED UPON FOR SETTING OUT OF BUILDINGS. 2) A RE-ESTABLISHMENT OF THE CADASTRAL BOUNDARIES IS REQUIRED TO DEFINE ACCURATE POSITION. 3) CONSULT LEGAL ADVICE ON EASEMENTS, ENCUMBRANCES AND CAVEATS THAT MAY APPEAR ON THE CERTIFICATE OF TITLE. 4) LEVELS ON ADJOINING PROPERTIES ARE APPROXIMATE DUE TO ACCESS RESTRICTIONS. 5) SERVICES PLOTTED AS VISUALLY SEEN ON SITE AND ARE APPROXIMATE. 6) SEWER POSITION AND LEVELS FROM WATER CORPORATION PLANS. 7) CHECK ON SITE DATUM AND LEVELS PRIOR TO CONSTRUCTION. 8) DIAL BEFORE YOU DIG TO CHECK LOCATION OF UNDERGROUND SERVICES. 9) BEWARE OF OVERHEAD POWER LINE HAZARDS. 10) CONSULT PPS ON ANY ANOMOLY BEFORE DESIGN AND CONSTRUCTION.

OVERSHADOW SITE PLAN

sheet 4 of 8

R60

Developments



CITY OF COCKBURN
AMENDED PLAN
 20 JUL 2012
 PLAN _____ OF _____

CONTOUR & FEATURE SURVEY

406 CARRINGTON STREET,
 HAMILTON HILL

CITY OF COCKBURN

Job Number: 11/5120

Client : ROLSTON

TITLE DETAILS

LOT 509 ON DIAGRAM 59387
 CERTIFICATE OF TITLE:
 Vol 1569 Folio 28

Area : 1629m²

LEGEND

- Temporary benchmark, nail & plate in bitumen RL 21.57m AHD
- Sewerage line with invert level
- Sewerage manhole
- Water main
- Gully pit & drain
- Significant tree
- High brick wall
- Fibro fence
- 10m AHD height contour
- Spot height in metres
- Power pole
- Telephone pt
- Street Sign
- Storm water drain
- Overhead power
- Sewer connection

Lot 10 Wheeler Rd
 Approx site Area : 2736m²
 total overshadowing area: 641.68m²
 total % overshadowing: 23.45%

Drafted by: SJ
 Scale 1:200 on A1
 Date 23/08/2011

SURVEYOR SITE INSPECTION

SITE FEATURES	COMMENTS
Electricity	Overhead Connection
Gas meter	Yes (not sighted)
Water meter	Yes (sighted)
Sewerage Connection	Yes (sighted)
Tele Com	Yes
Road	Bitumen
Kerb	Concrete: Semi-mountable
Crossover	Concrete
Driveway	Bitumen
Dwelling	Split Level
Dwelling Foundation	Concrete
Vehicle Parking	Bitumen Carpark
Eaves	None
Fencing	Metal and Fibro in Poor Condition
Vegetation	Well kept
REPEG	NOT COMPLETED



1) THE POSITION OF FEATURES AND LEVELS ARE PLOTTED FROM EXISTING FENCES AND CAN'T BE RELIED UPON FOR SETTING-OUT OF BUILDINGS. 2) A RE-ESTABLISHMENT OF THE CADASTRAL BOUNDARIES IS REQUIRED TO DEFINE ACCURATE POSITION. 3) CONSULT LEGAL ADVICE ON EASEMENTS, ENCUMBRANCES AND CAVEATS THAT MAY APPEAR ON THE CERTIFICATE OF TITLE. 4) LEVELS ON ADJOINING PROPERTIES ARE APPROXIMATE DUE TO ACCESS RESTRICTIONS. 5) SERVICES PLOTTED AS VISUALLY SEEN ON SITE AND ARE APPROXIMATE. 6) SEWER POSITION AND LEVELS FROM WATER CORPORATION PLANS. 7) CHECK ON SITE DATUM AND LEVELS PRIOR TO CONSTRUCTION. 8) DIAL BEFORE YOU DIG TO CHECK LOCATION OF UNDERGROUND SERVICES. 9) BEWARE OF OVERHEAD POWER LINE HAZARDS. 10) CONSULT PPS ON ANY ANOMOLY BEFORE DESIGN AND CONSTRUCTION.

Plot Ratio = 1187.83m²
 Max Plot Ratio = 1187.90m²

external colours & finishes
 painted masonry main coloured castor, pale grey
 painted masonry window coloured, masonry
 and face coloured, white
 galvalume coloured masonry
 painted aluminium coloured, white
 white stone coloured castor, white
 window frames optional



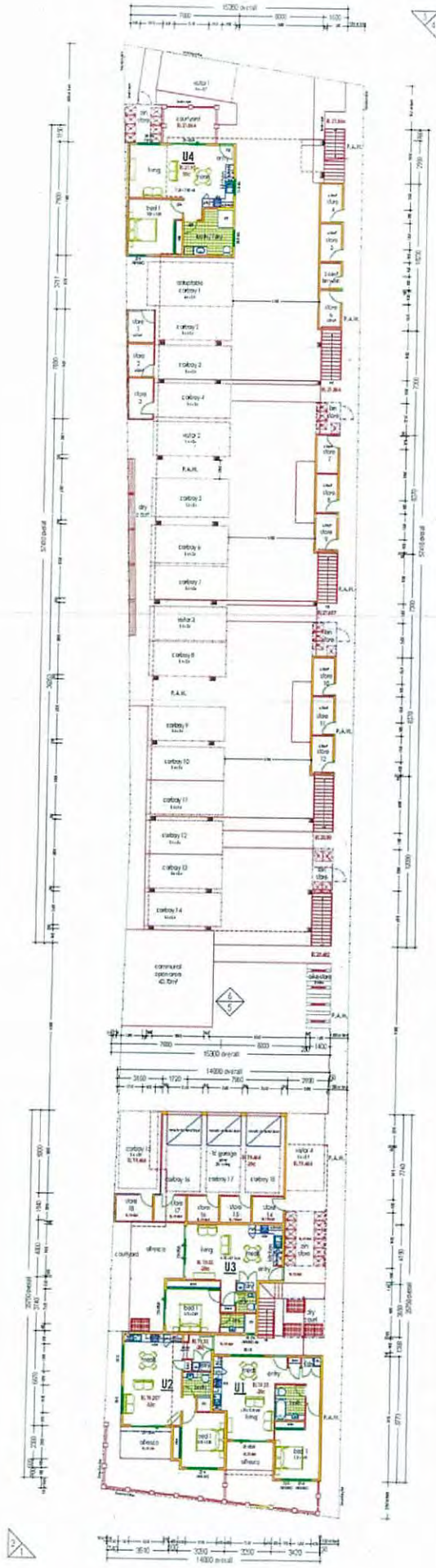
- U4 - 1x1**
Internal Living: 56.41m²
- U3 - 1x1**
Internal Living: 50.00m²
- U2 - 1x1**
Internal Living: 50.07m²
- U1 - 1x1**
Internal Living: 50.26m²

GROUND FLOOR PLAN
 scale 1:100 (m AG)

Signature 1: _____
 Signature 2: _____
 Name: _____
 Title: _____

Approved: _____
 Date: _____

Client: _____
 Project: _____
 Date: _____



CITY OF COCKBURN
AMENDED PLAN
 20 JUL 2012
 PLAN _____ OF _____

Plot Ratio = 1187.83m²
 Max Plot Ratio = 1187.90m²

external colours & finishes
 external walls: coloured brick - dark grey
 external windows: coloured aluminium - maximum
 roof: light grey, coloured aluminium
 gutters: large, coloured aluminium
 floors: terrazzo, coloured aluminium
 entry doors: coloured aluminium
 external landscaping: green lawn

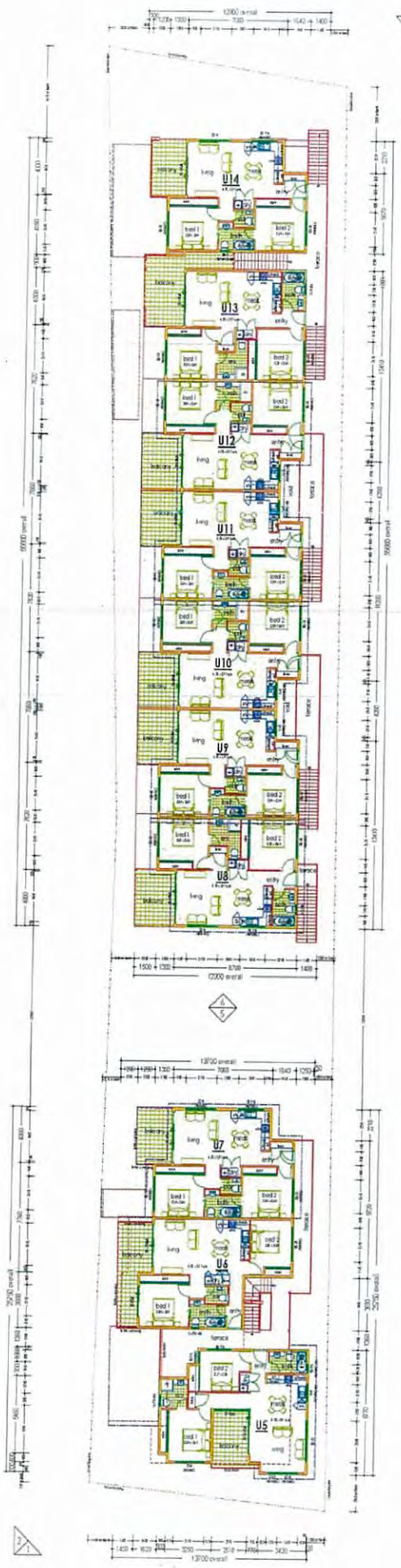


- U14 - 2x1**
 Internal Living: 65.05m²
 Balcony: 10.31m²
- U13 - 2x2**
 Internal Living: 68.88m²
 Balcony: 13.95m²
- U12 - 2x1**
 Internal Living: 65.05m²
 Balcony: 11.54m²
- U11 - 2x1**
 Internal Living: 65.05m²
 Balcony: 11.54m²
- U10 - 2x1**
 Internal Living: 65.05m²
 Balcony: 11.54m²
- U9 - 2x1**
 Internal Living: 65.05m²
 Balcony: 11.54m²
- U8 - 2x2**
 Internal Living: 68.88m²
 Balcony: 11.33m²
- U7 - 2x1**
 Internal Living: 65.05m²
 Balcony: 10.11m²
- U6 - 2x1**
 Internal Living: 65.00m²
 Balcony: 14.79m²
- U5 - 2x2**
 Internal Living: 66.55m²
 Balcony: 10.30m²

FIRST FLOOR PLAN
 scale 1:500 on A2

Signature 1: _____
 Signature 2: _____
 Witness: _____
 Date: _____

Scale: 1:500
 Date: 01/10/12
 City: Perth
 State: WA
 Project: _____
 Drawing: _____
 Revision: _____



CITY OF COCKBURN
 AMENDED PLAN
 20 JUL 2012
 PLAN _____ OF _____

Plot Ratio = 1187.83m²
 Max Plot Ratio = 1187.90m²

external colours & finishes
 zarfati emila man colorand color. Pali gny
 zarfati emila man colorand color. Pali gny
 zarfati emila man colorand color. Pali gny
 zarfati emila man colorand color. Pali gny



- U15 - 2x1**
 Internal Living: 45.03m²
 Balcony: 10.11m²
- U16 - 2x1**
 Internal Living: 45.05m²
 Balcony: 11.20m²
- U17 - 2x1**
 Internal Living: 45.37m²
 Balcony: 10.50m²
- U18 - 2x1**
 Internal Living: 44.09m²
 Balcony: 14.27m²

SECOND FLOOR PLAN
 Scale: 1:100 or A3

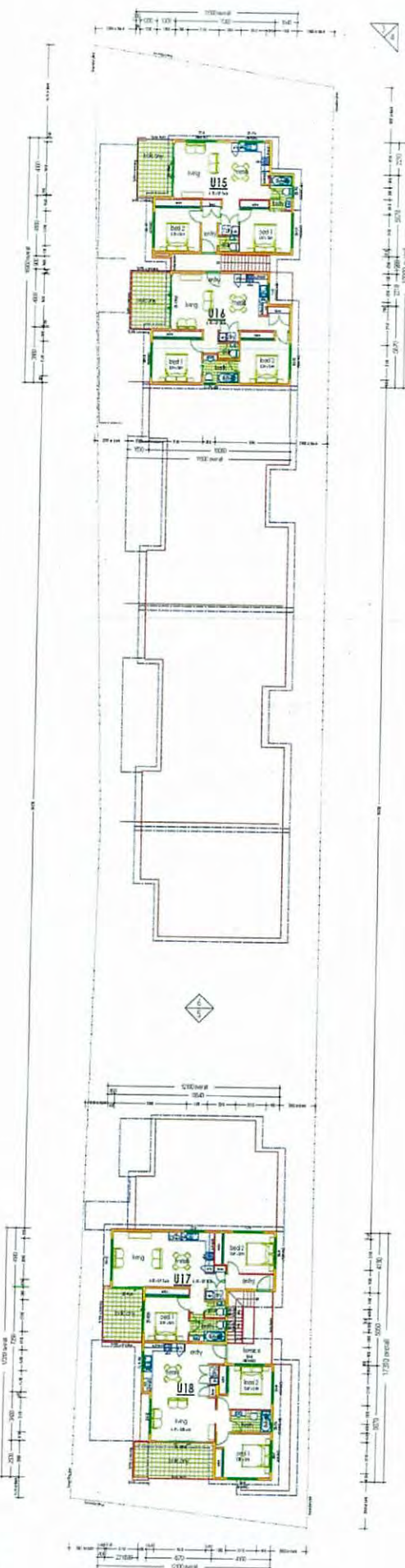
Outlook
 R60
 BETTERLIFE
 Developptions

Signed by: _____
 Signed by: _____
 Witness: _____
 Date: _____

Checked: _____
 Approved: _____
 Date: _____

Project: _____
 Site: _____

C:\2014
 Day: 1/1/2014
 12:46
 Copyright © 2014
 10/10/14



CITY OF COCKBURN
 PREPARED PLAN
 20 JUL 2012
 _____ OF _____

Plot Ratio = 1187.83m²
 Max Plot Ratio = 1187.99m²

external colours & finishes
 concrete masonry - sandstone color - Ash grey
 sandstone masonry - light brown - masonry
 roof - grey - metal
 aluminium - silver
 window frames - white
 window frames - black
 window frames - white



ELEVATION 1 - Carrington Street



ELEVATION 1 - Carrington Street Screen Wall



ELEVATION 5



ELEVATION 3 - Wheeler Street



ELEVATION 3 - Wheeler Street Screen Wall



ELEVATION 6

CITY OF COCKBURN
 AMENDED PLAN
 20 JUL 2012
 PLAN _____ OF _____

ELEVATIONS
 scale 1:100 on A2

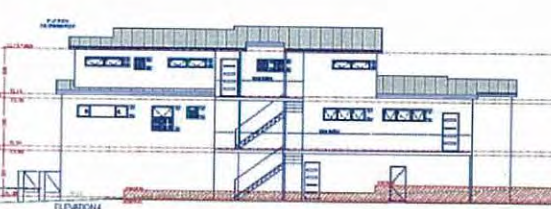
Ourlook
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 R60
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BETTERLIFE
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 Date: 20/07/2012

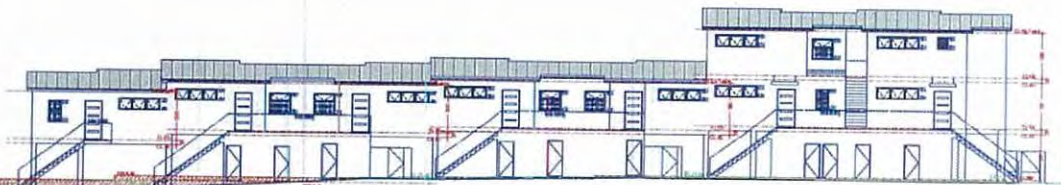
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ELEVATION 2



ELEVATION 4



EXISTING SITE PLAN
sheet 1 of 6

Develop Options



CONTOUR & FEATURE SURVEY

406 CARRINGTON STREET,
HAMILTON HILL

CITY OF COCKBURN

Job Number: 11/5120

Client: ROLSTON

TITLE DETAILS

LOT 509 ON DIAGRAM 59387

CERTIFICATE OF TITLE:

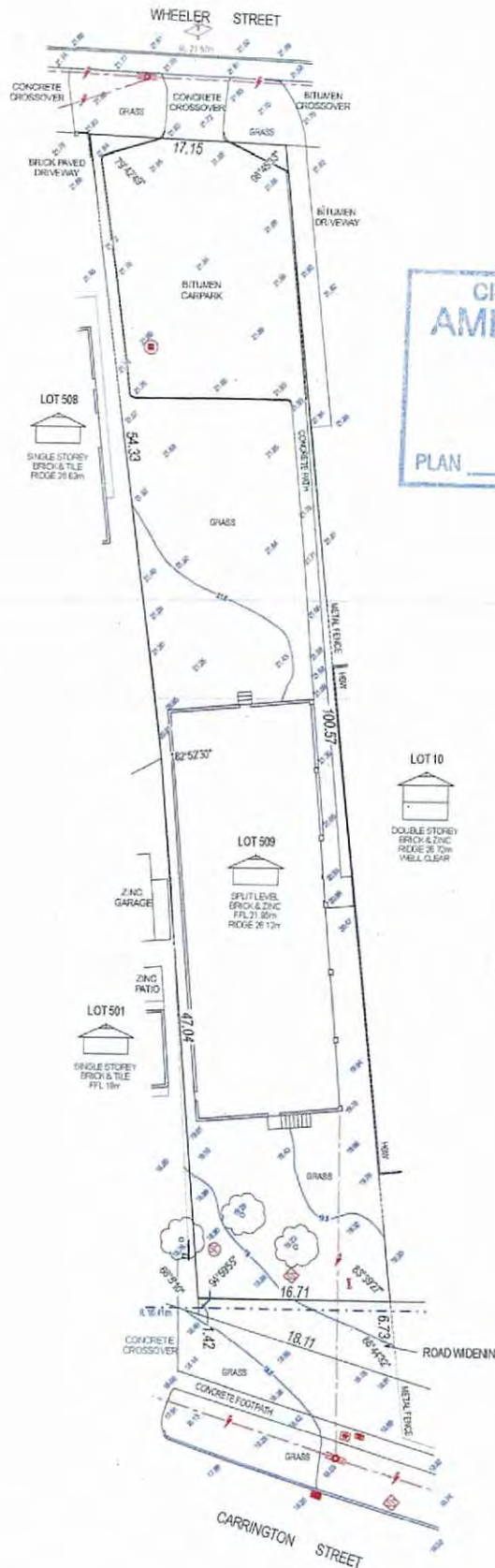
Vd 1569 Folio 28

Area: 1629m²

LEGEND

Temporary benchmark nail & plate in datum RL 21.57m AHD	AHD height contour
Sewerage line with invert level	Spill height in metres
Sewerage manhole	Power pole
Water meter	Telephone pit
Gully pot drain	Street Sign
Significant tree	Storm water drain
HDV High back wall	Overhead power
Fero fence	Sewer connection

CITY OF COCKBURN
AMENDED PLAN
20 JUL 2012
PLAN _____ OF _____



Drafted by: SJ
Scale 1:400 on A3
Date 23/08/2011

SURVEYOR SITE INSPECTION

SITE FEATURES	COMMENTS
Electricity	Overhead Connection
Gas meter	Yes (not sighted)
Water meter	Yes (sighted)
Sewerage Connection	Yes (sighted)
Tele Com	Yes
Road	Bitumen
Kerb	Concrete: Semi-mountable
Crossover	Concrete
Driveway	Bitumen
Dwelling	Spill Level
Dwelling Foundation	Concrete
Vehicle Parking	Bitumen Carpark
Eaves	None
Fencing	Metal and Fibro in Poor Condition
Vegetation	Well kept
REPEG	NOT COMPLETED



1) THE POSITION OF FEATURES AND LEVELS ARE PLOTTED FROM EXISTING FENCES AND CAN'T BE RELIED UPON FOR SETTING OUT OF BUILDINGS. 2) A RE-ESTABLISHMENT OF THE CADASTRAL BOUNDARIES IS REQUIRED TO DEFINE ACCURATE POSITION. 3) CONSULT LEGAL ADVICE ON EASEMENTS, ENCUMBRANCES AND CAVEATS THAT MAY APPEAR ON THE CERTIFICATE OF TITLE. 4) LEVELS ON ADJOINING PROPERTIES ARE APPROXIMATE DUE TO ACCESS RESTRICTIONS. 5) SERVICES PLOTTED AS VISUALLY SEEN ON SITE AND ARE APPROXIMATE. 6) SEWER POSITION AND LEVELS FROM WATER CORPORATION PLANS. 7) CHECK ON SITE DATUM AND LEVELS PRIOR TO CONSTRUCTION. 8) DIAL BEFORE YOU DIG TO CHECK LOCATION OF UNDERGROUND SERVICES. 9) BEWARE OF OVERHEAD POWER LINE HAZARDS. 10) CONSULT PPS ON ANY ANOMOLY BEFORE DESIGN AND CONSTRUCTION.

WASTE MANAGEMENT PLAN

LOT 509, CARRINGTON STREET, HAMILTON HILL



GRA REF 7220 19 DECEMBER

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5.0 WASTE MANAGEMENT PLAN SUMMARY – RESIDENTIAL WASTE 7

WASTE MANAGEMENT PLAN » SPEARWOOD » 19 DEC 2011 » GRA REF 7220 » 7220_IIDE001R_RM_WASTE MANAGEMENT

1.0 Objective and Background

1.1 Objective

The City of Cockburn require a Waste Management Plan (WMP) to be included as part of a development application. This WMP has been prepared to fulfil this condition.

The objective of this plan is to ensure that waste disposal is undertaken effectively, efficiently and sustainably, minimising the effects on the community and the environment during both construction and operation of the development keeping in line with legislation and company policy.

The Plan addresses both design features and operational controls required to ensure that the plan can be implemented effectively.

1.2 Background

The land owner has requested Greg Rowe and Associates to prepare a waste management plan for a residential multiple dwellings development at Lot 509 (406) Carrington Street, Hamilton Hill. The proposed development is located in the City of Cockburn. The building has been designed by Outlook Designs.

At the time of preparing this plan, the proposed development consisted of 18 residential units. Of the residential units, 14 are two-bedroom units with two bathrooms each and four 1 bedroom 1 bathroom dwellings

The subject site is 1629m² and is zoned Mixed Use R60.

2.0 Communication

2.1 Construction

- » As part of the construction phase a waste management consultant will be appointed and all site and company Waste Management Policies will be explained to subcontractors during contract negotiation and compliance with these policies is included in subcontractor contracts. A copy of those Waste Management policies will accompany a building licence application. We will be tendering the construction of the building and the Tender assessment scoring will be weighted in favour of contractors with waste minimisation strategies.
- » Compliance will be policed by the managing contractor and developer during construction to ensure contractual obligations are met.

2.2 Occupation

- » The occupants of the development will be made aware of the Waste Management Plan and the role they are required to fulfil. This documentation will be included in the handover pack given to owners at time of purchase/lease with the waste management consultant providing this information upon lodgement of a building licence.
- » The eventual Body corporate will be informed of the obligations to the up keep of the system. This will be included in their contract. The body corporate will continue to inform occupants of their obligations or any modifications to the system.

3.0 Waste Disposal

3.1 Construction

During construction a skip bin will be provided on site for waste produced on site during the course of construction and serviced as required. Preferred waste management contractors provide off site sorting and recycling to minimise landfill waste. These contractors will be preferentially used where practical. Sub contractors will be responsible for dumping all waste they generate into the skip bin. This will be policed by site management. Subcontractors are encouraged to use products with lower waste output.

Waste water generated during wash down and clean-up of equipment used for brickwork and plastering has the potential to be high pH and to be toxic to aquatic flora and fauna. To minimise the impacts associated with the clean-up of such equipment, the developer shall ensure that wastewaters are disposed of in accordance with DEC Guidelines. This shall be communicated to all personnel during induction.

- » Used solvents and paints are to be stored in the site sea container and removed by a licensed contractor as required.
- » All excess lime or cement is to be removed from the site by the person who brought it on to site.

All subcontractors will be notified of their responsibility to maintain site cleanliness and adhere to waste management policies during construction. These obligations will be included in all subcontractor contracts.

3.2 Occupation

3.2.1 Waste Flow

Waste is generated by the apartment occupiers. This waste is separated into different waste types by the occupiers and transported by hand to the bin store, where it is placed in the relevant bin. The Caretaker will manage the bins in the bin storage area to ensure that one bin is full before the next one is used by the apartment occupiers. The caretaker will then present only the full bins outside the bin storage room on collection day. The City of Cockburn collection service empties the bins into the waste disposal truck where it is removed from site for processing.

3.2.2 Bin Storage

The bin storage area is located on the ground floor, see drawing (Site Plan) and (Ground floor Plan), set back sufficiently from the entrance to ensure is not in public view by a screen wall of 1.8metres in height but still easily accessible for both residents, from the lobby. Collection services via bin presentation pads are located within the site and the verge.

Adequate road frontage to Wheeler Street provides an area to present their bins.

It is acknowledged that the City's objective is to plan the service so that waste trucks never have to reverse at all.

This has been achieved by presenting the bins along the Wheeler Street frontage.

The bin store area will have concrete slab floor with falls to drains. A hose cock will also be included to facilitate washout of bins and washout of the area. The walls of the bin store will be sealed and painted in a light colour to facilitate washout. The caretaker will be responsible for washing waste and recycling bins. The bin storage and wash down area will be constructed from concrete and block work walls with gates and access to car parking areas and

lobby and lit when in use. Signage will be installed at entry/egress points stating "DANGER" and "NO STANDING". The Bin store and wash down areas will be open to the air and therefore ventilated.

4.0 Waste Management Plan

4.1 City Requirements

The City of Cockburn has reviewed the current Development Application and has requested more information regarding the proposed waste management arrangements. The City's Waste Minimisation, Storage and Collection in Multi Unit Developments Policy have been provided by the City of Cockburn.

A Summary of the City's minimum requirements relating to Waste Storage and Collection in multi storey residential buildings are:

1. A Waste Management Plan detailing the waste movement through the development.
2. If residents share bins, then details on how and who presents these bins on collection day.
3. Details on the estimated waste and recycling volumes generated / unit.
4. An adequate bin storage area equal to 1m²/ unit.
5. A bin storage area that is convenient for residents and verge presentation.
6. Details on verge bin presentation pads to correspond to demand.

The Waste Manager, Lyall Davieson, has advised that all relevant conditions are to be addressed in order to comply with the City's Waste, Planning and Health Department requirements

4.2 Waste Sources and Volumes Generated

This section shows how the development will deal with the following requirements specified by the City of Cockburn;

- » Adequate storage shall be provided to contain all waste and recycled material generated on the premises for at least 1 week.
- » The minimum area shall be 1 square metre per residential unit.
- » In addition to a detailed floor plan showing the size and location of the Bin Storage Area, a Waste Management Plan shall be provided showing compliance with the City's requirements.

4.2.1 Residential Waste

Waste audit data shows that high-rise multi-unit dwellings (MUDs) produce between 4 kg and 7 kg of garbage per week, and about 3 kg per week of recyclables. This is around one third that produced from single dwellings (free-standing houses). The City of Cockburn normally provides 240-litres of garbage capacity and 240-litres of recyclables capacity per single dwelling per week. As the figures above show, this capacity is well in excess of that required for MUDs. The residential bin capacity required for this development would be one 240-litre garbage bin and one 240-litre recycling bin per three units. Other councils typically provide this capacity for households in MUD developments and it will be enough for the likely quantities produced per residential unit in this case.

Residential waste in this instance will be separated into general and recyclable products and stored in the associated receptacles.

4.3 Storage Area Required

The City of Cockburn has indicated that the bin storage areas at this development must be adequate 'to contain all waste and recycled material generated on the premises for at least 1 week.' The following calculations have been made with this condition in mind.

4.3.1 Residential Waste

The current plans show 18 units in the development. If the City provided one 240-litre garbage bin and one 240-litre recycling bin per three units, this would amount to 6 garbage bins and 6 recycling bins. One 240-litre bin occupies a footprint of 0.45m². The total space occupied by 12 bins would be 5.4m². Allowing as much area again for access and manoeuvring bins means approximately 18m² of bins storage area would be required which equates to 1m² per unit.

To comply with the Policy, the residential bin areas have been separated to provide for easy access for residents as a priority and as such the proposed plans illustrate a pedestrian access point through the garage areas and out to a communal bin storage area for all eighteen units. The bins will be moved by one person on bin day and collected the same day. The bins will be located on the bin collection pads on the verge and on site and removed after collection, and to be stored in their original location.

A bulky goods storage area is located adjacent to the bins within the bin storage area with a space of approximately 3m²

4.4 Other City Requirements

This section deals with how the development complies with the City's following requirements:

1. A Waste Management Plan detailing the waste movement through the development.
2. If residents share bins, then details on how and who presents these bins on collection day.
3. Details on the estimated waste and recycling volumes generated / unit.
4. An adequate bin storage area equal to 1m²/ unit.
5. A bin storage area that is convenient for residents and verge presentation.
6. Details on verge bin presentation pads to correspond to demand.

The proposed plans illustrate that a clear pathway can be made from the outside of the building from the garage / parking areas or within the building via access from the garage / lobby areas. The location of the bin storage areas are out of view to the street as required by the Residential Design Codes 2010, and are located in a convenient location for future residents.

The location of the bin storage area is in a location in one larger area near the front driveway entrance. This is for the ease of the caretaker in moving the bins onto the driveway and verge for weekly collection. The location is within reasonable distance for all residence to dispose of waste utilising external access to the southern side of the building.

The volumes created by residential dwellings in a multiple dwelling format are considered to be 1/3 of a single dwelling or between 4-7kg per week per unit and 3kg per week for recycling.

Our proposed plans demonstrate that we are now providing bin storage of 1m² per unit and a bulky goods area besides the bin storage for larger item storage.

Bin presentation pads are now indicated on the associated site plan indicating that 4 x 240litre rubbish bins and 4 x 240 litre recycling bins can be collected from the verge on collection day.

5.0 Waste Management Plan Summary – Residential Waste

As there are 18 residential units, the bin storage area is designed to 18m² + 3m² of bulky goods storage space. There is adequate bin collection area located on the verge with a management strategy for the requirement of one person to be commissioned with the responsibility of removing and replacement of bins on collection day.

The proposed Strata Management plan which will form part of the strata title for this development will incorporate this Waste Management Plan and any changes to this plan must be approved by the City of Cockburn.

ROLSTON
406 CARRINGTON STREET, HAMILTON HILL

TRAFFIC STATEMENT

March 2012



PO Box Z5578
Perth WA 6831
0413 607 779 Mobile

Issued on	06-03-2012	Amendment	Date
Version	V1		
Reference	668		

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- 2.0 THE SITE AND SURROUNDING ROAD NETWORK
- 3.0 TRAFFIC GENERATION AND DISTRIBUTION
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- 5.0 ACCESS
- 6.0 PARKING
- 7.0 PEDESTRIANS, CYCLISTS AND PUBLIC TRANSPORT

1.0 EXECUTIVE SUMMARY

Riley Consulting has been commissioned through Greg Rowe and Associates to prepare a traffic statement for the proposed residential development at 406 Carrington Street, Hamilton Hill. The analysis undertaken in this report indicates the following:

- The previous uses of the site required the provision of a 16 bay car park accessed from Wheeler Road. The site has previously generated traffic movements to Wheeler Road and the surrounding road network.
- The development proposes the construction of 17 dwellings with access to Wheeler Road. The expected traffic generation of the development is 79 movements per day. Considered to the previous land uses, the proposed development is considered to increase local traffic movements by just 15 trips per day. The proposed development is considered to have no significant impact to the surrounding road network.
- Access to Wheeler Road can be achieved in a safe and appropriate manner. It is expected that Wheeler Road will continue to operate as a quiet residential street. The proposed development will not affect the amenity of the street.
- Parking in accordance with current standards and requirements can be provided on the site.
- There are no reasons to suspect that the proposed development would have a negative impact to local road safety.

2.0 THE SITE AND SURROUNDING ROAD NETWORK

The subject site is located at 406 Carrington Street in the suburb of Hamilton Hill. Figure 1 shows the location of the site. Roads of importance to the proposed development are discussed below.

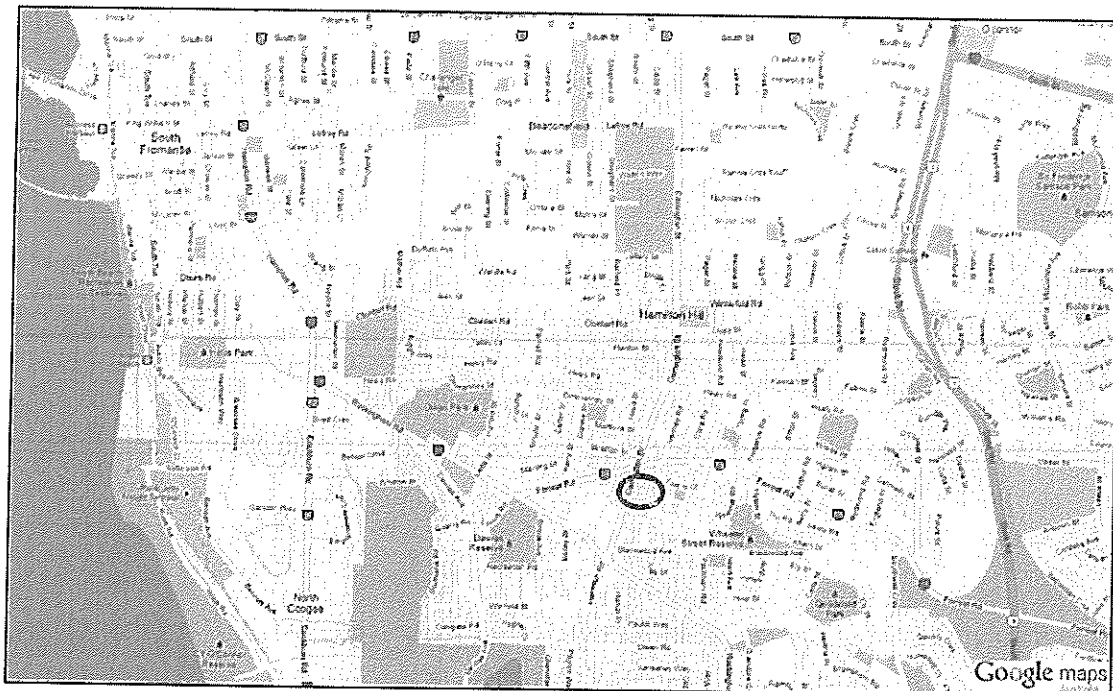


Figure 1 Location of the Subject Site

Carrington Street

Carrington Street is classified as a district distributor A road in the Main Roads *Functional Road Hierarchy*. It is constructed with an 8 metre wide carriageway set within a 20 metre road reservation. Traffic signals are provided at the intersection of Carrington Street and Forrest Road.

Forrest Road

Forrest Road is classified as a district distributor B road in the Main Roads *Functional Road Hierarchy*. It is generally constructed as a two lane road with medians and some turning pockets. The road is provided in a 20 metre road reservation.

Wheeler Road

Wheeler Road is a local residential access street. It is constructed with a 6.5 metre (approximate) wide carriageway in a typical 20 metre road reservation. Wheeler Road runs

parallel to Forrest Road and provides access for about 80 residential lots. Both ends of Wheeler Road are controlled by give way lines to Forrest Road.

Traffic data is not available for Wheeler Road, but based on about 38 residential lots needing access to Forrest Road from the western end of Wheeler Road, a daily traffic flow of less than 300 vehicles would be expected.

Current Traffic Data

Traffic data has been sourced for the local road network from Main Roads Western Australia (MRWA). Figure 2 shows the current traffic data from annual counts, and the stop line flows from the traffic signals at the intersection of Carrington Street and Forrest Road.

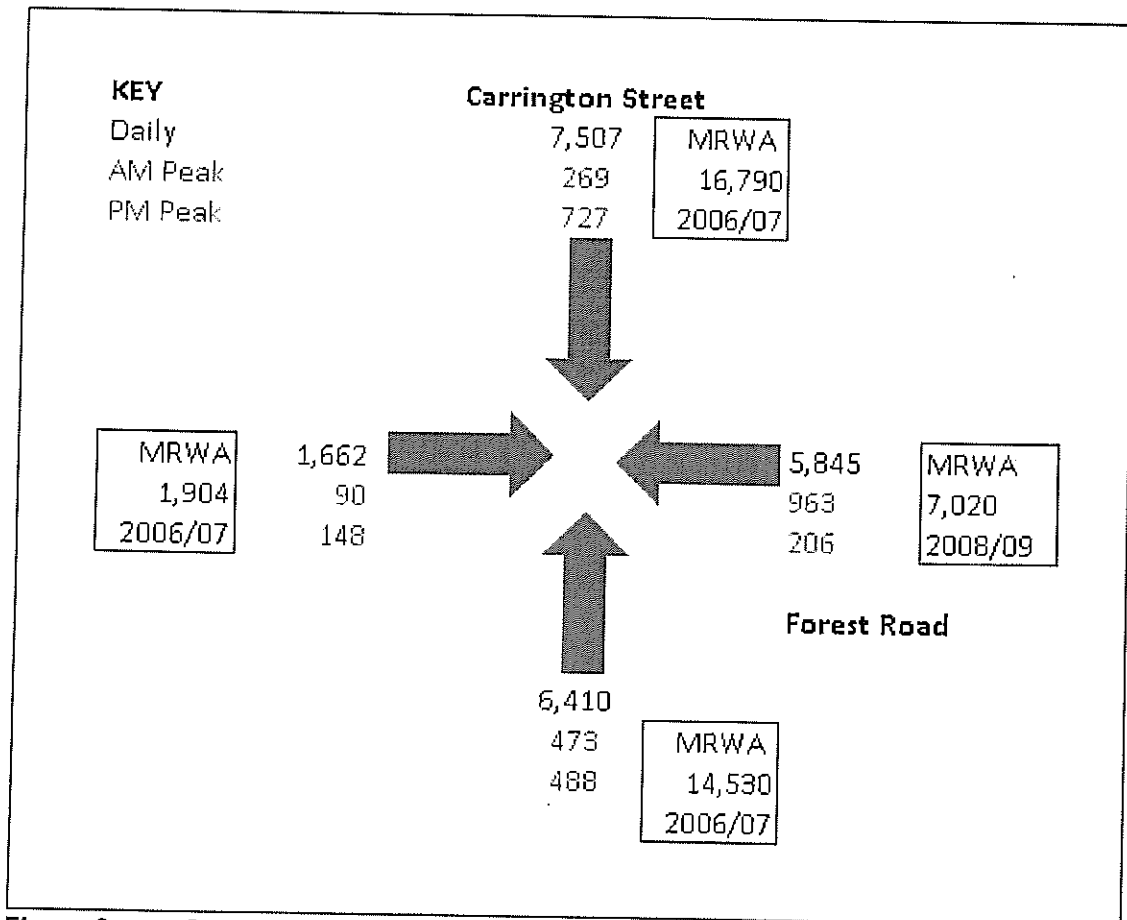


Figure 2 Current Traffic Flows

3.0 TRAFFIC GENERATION AND DISTRIBUTION

The site has previously been used as a dance school and has a 16 bay car park provided off Wheeler Road. Traffic data for the previous use is unknown, but it would be expected that as a viable business several dance classes would be held each day. On the basis of 16 parking bays and a feasible a demand of 2 classes per day, at least 64 vehicle movements per day could have been expected, excluding staff trips. During a weekend the level of attraction could have been significantly higher.

Previous land uses could have generated over 64 movements per day.

The Proposed Development

It is proposed to develop the site to provide 17 dwelling units comprising of 14 x 2 bed units and 3 x 1 bed units. Reference to the RTA *Guide to Traffic Generating Developments* suggests that a 2 bed unit can be expected to generate 4 to 5 trips per day and a single bed unit 2 to 3 trips per day. On this basis the proposed development can be expected to generate:

- 14 x 2 bed units @ 5 trips = 70 trips per day
- 3 x 1 bed units @ 3 trips = 9 trips per day

Based on recognised trip generation sources, the proposed development can be expected to generate 79 vehicle movements per day.

The development is expected to generate 79 vehicle movements per day.

Assuming that the previous land uses generated about 64 vehicle movements per day, it can be seen that the proposed redevelopment of the site will increase local traffic movements by (79 – 64) 15 vehicle movements per day.

The development will increase local traffic movements by about 15 trips per day.

Distribution

Residents of the development can be expected to use their cars to access work, shopping and entertainment facilities located in Perth and Fremantle. It would be expected that the generated traffic would head in the following directions:

- North on Carrington Street 50%
- East on Forrest Road 35%
- South on Carrington Street 10%
- West on Forrest Road 5%

Forrest Road west is restricted to left-in / left-out at Rockingham Road.

Figure 3 shows the anticipated traffic movements of the proposed development.

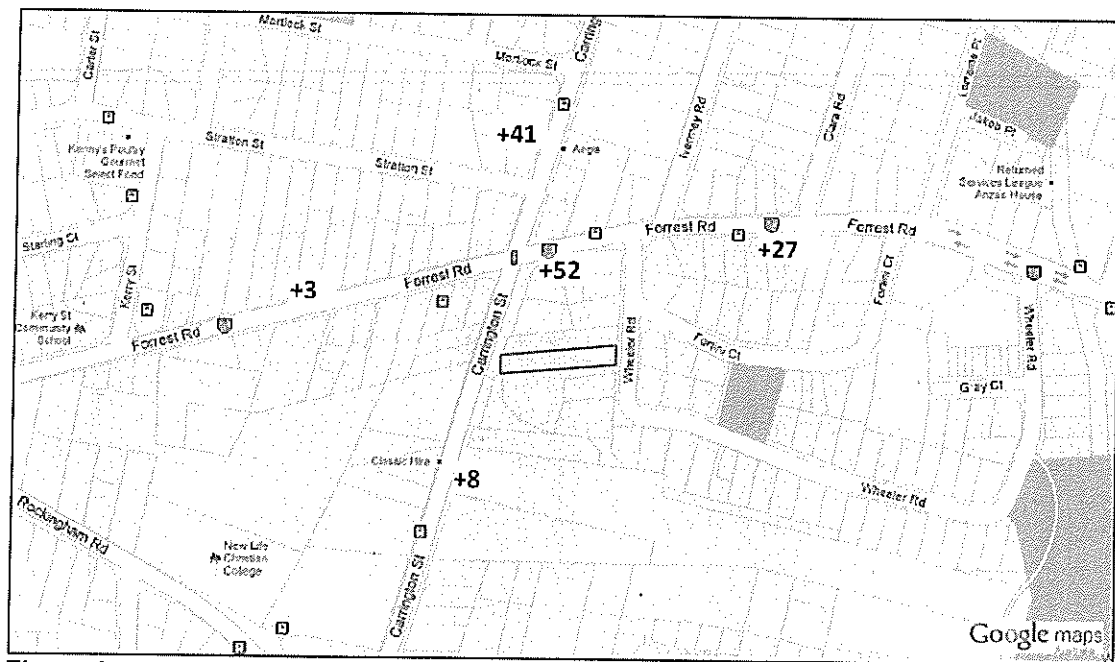


Figure 3 Expected Traffic Movements of Full Development

4.0 TRAFFIC IMPACT

The expected traffic generation of the proposed development will be about 79 vehicle movements per day, based on recognised trip generation source documents. With the previous land uses taken into consideration, local traffic movements could be increased by about 15 trips per day. This level of increased traffic movement would be considered as insignificant.

Local traffic increases are considered to be insignificant.

MRWA traffic data for the adjacent road network is reasonably old, being from 2006 to 2009, but recent traffic data from the traffic signals at Carrington Street / Forrest Road indicates that traffic volumes in the locality have not changed. Based on these traffic volumes, Table 1 considers the possible increase based on the traffic generated from the proposed development.

Table 1 Traffic Impacts with Natural Growth 2021

Road	Volume 2021	Development	%
Carrington Street north	16,790	+41	<1%
Forrest Road east	7,020	+52	<1%
Carrington Street south	14,530	+8	<1%
Forrest Road west	1,904	+3	<1%
Wheeler Road	@300	+79	+27%

Table 1 shows that the proposed development will increase local traffic movements by less than 1% of the current daily traffic flow.

It is acknowledged in traffic engineering practice that traffic flows on any road will vary by +/- 5% during the week. Over the course of the year the fluctuation may be significant with variations of 25% or more. It is therefore considered that where a road is subject to a traffic flow change of less than +/-5% no significant impact would occur.

The proposed development is considered to have no traffic impact.

5.0 ACCESS

Access to the site is taken to Wheeler Road from a single cross-over. The access is in a similar position to the current access to the 16 bay car park. Figure 4 shows an extract of an aerial photograph for Wheeler Road showing how the existing access relates to the road.



Figure 4 Site Access

Visibility of over 100 metres is provided to the intersection of Wheeler Road / Forrest Road and visibility of about 80 metres is provided along Wheeler Road past the 90° bend shown. Austroads recommends that visibility of 90 metres should be provided on roads of 50kph. As the bend on Wheeler Road will limit traffic speeds to <40kph a visibility requirement of 67 metres would be required.

Appropriate visibility is achieved at the proposed site access.

Figures 5 and 6 show the visibility along Wheeler Road.



Figure 5 Looking South

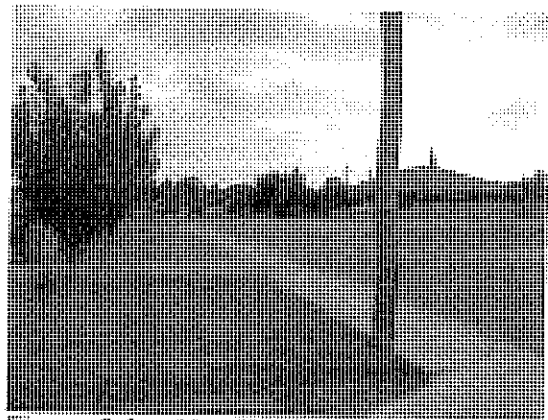


Figure 6 Looking North

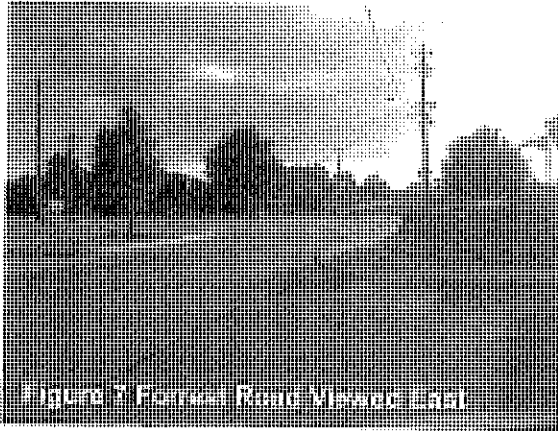
Large vehicle access

Large vehicle access would not be expected on the site. Garbage collection for the 17 units would be expected from Wheeler Road. As a quiet residential street on-street garbage collection is considered appropriate. A waste management plan has already been provided to the City of Cockburn.

Road Safety

All access and car parking is provided from Wheeler Road. As Wheeler Road is a quiet residential street with low daily traffic flows, there are no indications that the anticipated traffic increases would affect the current operation of the street. On-street resident parking to Carrington Street would not be expected, although pedestrian access is provided to this street.

At present there is an existing cross-over from Carrington Street to the property and historically the verge has been used to accommodate car parking. It would be considered rare that visitors to the proposed units would park on Carrington Street, if residents advise there is parking off Wheeler Road. Occasional parking on Carrington Street is not considered to be an issue as an 8 metre wide carriageway is provide for 2 traffic lanes and excellent visibility exists in both directions. Ample width exists for traffic to pass a stationary car. This is only likely to occur outside of peak periods by delivery vehicles.



There is a restriction to the visibility for traffic entering Forrest Road at the Wheeler Road intersection.

Figure 7 shows the current view east along Forrest Road. It is estimated that this visibility is in the order of 50 metres to 60 metres, but noting that the City has recently introduced medians to Forrest Road, it can be expected that an assessment of the

visibility has been undertaken and considered acceptable.

Future Road network Changes

It is noted that the City of Cockburn had previously suggested that permanent visitor parking on the verge of Carrington Street is not to be allowed as it may be removed when the Roe Highway interchange is constructed, assuming that Carrington Street is widened.

Figure 8 shows an extract of the preferred Roe Highway alignment which terminates at Stock Road. Access to the Highway will be provided to Phoenix Drive. On this basis, widening of Carrington Street would probably not be justified as traffic would use Stock Road and Winterfold Road to access areas to the north, or Phoenix Drive to access areas to the south and local areas.

However, this is no longer an issue as all parking is provided on-site.

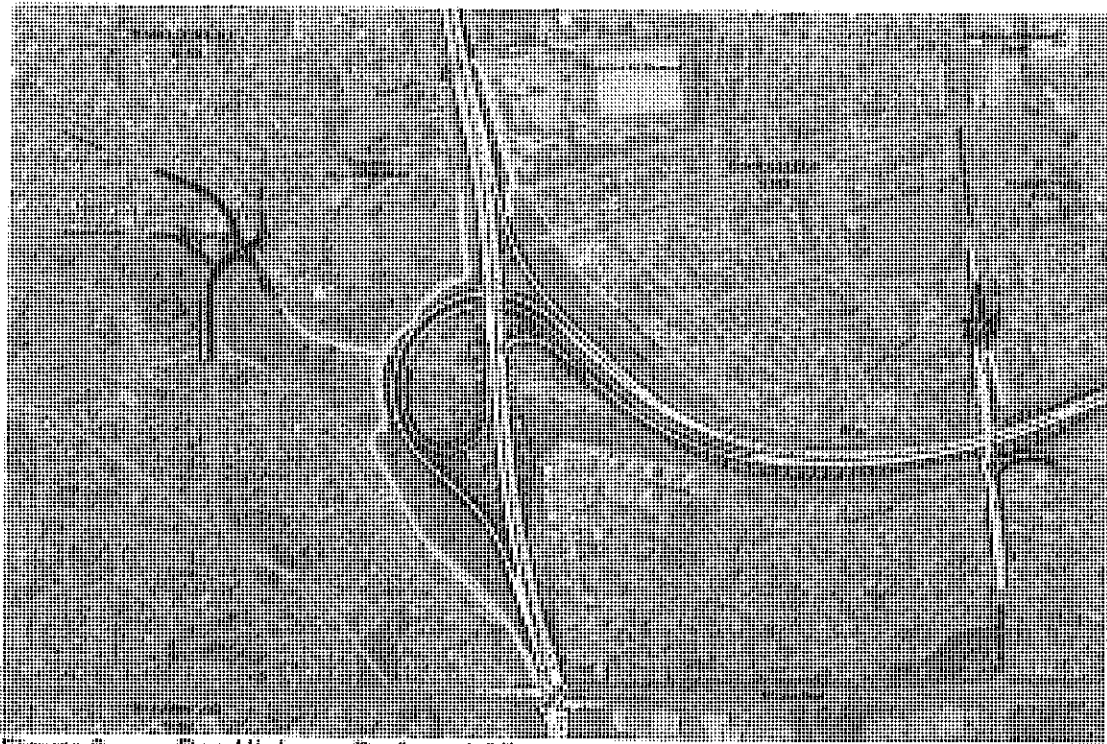


Figure 8: Ece Highway Preferred Alignment

6.0 PARKING

Car parking for multiple dwellings is required to be provided as set out by the Residential Design Codes (R-codes). The site is located within 250 metres 4 bus stops providing a total of 6 services. The site is considered to accord with column A of the table show in Section A3.1 of the R-Codes.

A3.1 As a minimum requirement on-site parking spaces are to be provided in accordance with the following, rounded up to the nearest whole number.

Location	A	B
Small (<75 sq m or 1 bedroom)	0.75 per dwelling	1 per dwelling
Medium (75-110 sq m)	1 per dwelling	1.25 per dwelling
Large (>110 sq m)	1.25 per dwelling	1.5 per dwelling
Visitors	0.25 per dwelling	0.25 per dwelling

A = within 800 metres of a train station or within 250m of a high frequency bus route.

B = not within 800 metres of a train station or not within 250m of a high frequency bus route.

A3.2 In addition to the above, 1 bicycle space to each 3 dwellings for residents; and 1 bicycle space to each 10 dwellings for visitors, and designed in accordance with AS2090.3.

All units within the proposed development have a floor area of less than 75m². All units can be considered as small. On the basis of 17 small apartments the car parking requirement will be:

- 17 x 0.75 bays 12.75bays
 - Visitor bays @ 0.25 4.25 bays
-
- 17 bays

In total 17 bays would be required based on section A3.1 of the R-codes.

17 bays are required for the proposed development¹.

The ground floor plan for the proposed development shows that 19 car parking bays can be provided. This equates to 2 bays more than set out by the table shown in section A3.1.

The proposed development provides a surplus of 2 parking bays.

From a technical perspective, the proposed development provides car parking in accordance with current planning requirements. However, in reality the development is able to provide one parking bay for each unit and two designated visitor bays on-site. Additional visitor parking can occur on-street.

¹ There may be some argument that each component of the table in section A3.1 should be rounded up. However this is not the case as the table deals with applying fractions of parking bays to developments. It is the total number of bays as a result of the table that is required to be rounded up.

On-street Parking

It has been shown that the proposed development can provide car parking in accordance with the R-Codes. However, in reality there may be a need for some occasional parking to occur on-street. It is noted that on-street parking to Carrington Street, whilst permissible, is not desirable.

Wheeler Road is a quiet residential street with an expectation of less than 300 vehicle movements per day. During peak periods, this equates to one vehicle every 2 minutes, the majority of which will be heading in the same direction. It is evident therefore that there is limited need to provide 2 clear traffic lanes to Wheeler Road. On this basis it is considered acceptable for parking to occur on-street with little interruption to other road users.

Wheeler Road is limited to the residential 50kph speed limit. Local intersections and the road alignment can be expected to result on an operating speed of 30kph – 40kph. With a slow speed environment and good visibility, on-street parking can occur in a safe and appropriate manner.

Occasional on-street parking would not be considered detrimental to local road safety.

Bicycle Parking

The City of Cockburn's Town Planning Scheme No 3 requires that 1 cycle bay per four units is to be provided for residents and 1 cycle bay per 16 units should be provided for visitors. On this basis the proposed development should provide $(17 / 4 + 17 / 16)$ 5.3 bicycles bays.

The R-codes require that 1 bicycle bay per 3 dwellings be provided for residents and 1 bicycle bay per 10 dwellings be provided for visitors. Therefore a total of $(17 / 3 + 17 / 10)$ 7.3 bicycle bays should be provided.

The architects plan shows that 8 bike stores are provided on the site which accords to current planning guidelines.

Bicycle parking is provided in accordance with current requirements

7.0 PEDESTRIANS, CYCLISTS AND PUBLIC TRANSPORT

Local streets are provided with at least one footpath. The footpath to Wheeler Road is provided to the western side, the side of the proposed development. Forrest Road has a footpath to the south side and a partial footpath to the north side. Carrington Street has a footpath provided to both sides.

There is a local shopping centre located about 250 metres to the east with a supermarket and bottleshop. Whilst the shopping centre is within easy walking distance, there is no direct route to the centre. Pedestrian access can only be achieved from Forrest Road or Wheeler Road. The local area is not considered attractive from a walking environment perspective. A fence has been introduced in the middle of the footpath to the south side of Forrest Road. The resulting footpath width is likely to restrict parents with prams / pushchairs.

It is noted that the bus stops on Carrington Street are not provided with shelters or medians to assist pedestrian crossing movements.

Access to bus stops on Forrest Road can be achieved in a safer environment as median islands have been provided. However, the median island to the west of Wheeler Road (providing access to eastbound buses) is not provided with dropped kerbs and is not to an acceptable standard to accommodate parents with prams.

Narrow cycle lanes have been introduced to Forrest Road east of Iverney Road. There are limited cycle facilities in the locality and many roads are provided as four-lane undivided roads, so are not suited to cycling.

Figure 9 shows the bus routes passing close to the subject site. There are bus stops located on both Carrington Street and Forrest Road.

Developtions

21st August 2012

City of Cockburn
Troy Cappelucci Senior Statutory Planning Officer
9 Coleville Crescent
SPEARWOOD WA 6163

Development Application Lot 509 Carrington Street, Hamilton Hill
Response to submissions, Community Consultation

Dear Troy,

Thank you for providing an opportunity to respond to submissions received during the advertising of revised plans for development of 18 multiple dwellings at the abovementioned address.

As you are aware we have been working with the City of Cockburn to develop plans suitable for redevelopment of the old dance studio site since late 2011. We are currently contracted to purchase the site and have encountered numerous design challenges with this site, including the narrowness of the site and restricted access to Carrington Street by the Department of Transport. As such we have attended a meeting on site with Councillors, the Port School and neighbours to hear what our neighbours have to say, we have taken on board these comments and have spent considerable investment in time and money into re-drafting a plan which makes commercial sense, as well as providing a solution which encompasses the concerns of the neighbours.

The site is zoned Mixed Use with the Residential medium density coding of R60. We originally were proposing a mixed use development with ground floor office; however the restrictions on access from Carrington Street make a commercial element unviable.

We have heard the comments from the Port School in relation to potential noise; however, we are aware that this will only be during school hours, not in the evening and not on the weekends or school holidays. We do believe that a residential land use next to a school is appropriate, and if anything it is our development that should be concerned about impact rather than the other way around. All potential purchasers will be made aware of the presence of the school and I will encourage them to speak with the school if they have any concerns.

The original plan was not acceptable to Mr Finch the Port School Principle who had concerns regarding courtyards directly abutting the Port School courtyards. Through our revised plans we removed all courtyards to this elevation and created an access way to the units. The units are now orientated to the north with only entrances and stairs to the units facing the Port School.

We have also separated the development into two buildings to move the bulk of the development to being predominantly abutting the school courtyard, now to being dispersed to the Wheeler Street elevation with a gap in the centre of the site.

We have also been made aware by the selling agent, Mr Peter Cave from Brockway Keys, that the Port School has since approached that agent with the intention of wanting to buy the site. The school is aware that we are purchasing the site under contract. We are therefore sceptical of the School's intentions, whether they are genuine or if they are aimed at delaying or stopping our bid to purchase the site.

The new plans have provided additional positive elements to the site and these include the following:

1. Two separate buildings, now with better surveillance to Wheeler Street, there are now units directly facing Wheeler Street, under the previous plan only a car park was viewed from this elevation.
2. Ground floor adaptable unit for person(s) with disability or impairment, again with passive surveillance opportunities to increase security to Wheeler Street.
3. Communal open space to the development.
4. Additional on-site car parking, one bay per unit as required by the Residential Design Codes.
5. Adequate on-site visitors parking, as per compliance with the Residential Design Codes.

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6. Courtyards removed from Southern elevation, better for solar access and also reduces noise from school to main habitable living areas and courtyards.
7. We have provided an adequate degree of dwelling mix with a combination of 1, 2 bedroom units with 1 and 2 bathrooms.
8. We are providing for diversity in housing typology to follow a demographic trend in single person households.
9. We are providing for an affordable housing solution, providing entry level housing in a location which is within close proximity to Fremantle, Cockburn Central, and public transport and employment opportunities.
10. Our development is a keystone example for urban infill development which will be proposed throughout Hamilton Hill once the revitalisation strategy is complete.
11. Three storeys are proposed, this is contained to the street frontages of the site, providing passive surveillance opportunities, and is centralised to the width site in what is a very narrow, long block.
12. We have provided a skillion roof form to minimise the height, bulk and scale impact of the building. Because the building is relatively narrow, there are minimal bulk impacts from an urban design and streetscape perspective.
13. The neighbouring sites are identified (to the North) for rezoning to R40, this will allow multiple dwellings up to two storeys. The School could be redeveloped (to the South) with similar scale development up to R60 and three levels in height as per the R Codes.
14. All visual privacy for neighbours is protected; we are using fixed angled louvers to allow sunlight but to block the view into neighbour's courtyards, all in compliance with the Residential Design Codes.
15. Security has been raised a major concern with the existing building and burglaries taking place. Our development will provide a blockage to through pedestrian traffic, currently people can walk through the site, and this will not be accessible once the development is complete. We will be providing security gates and fencing to the satisfaction of the City of Cockburn.

We have incorporated a design which complies predominantly with the Residential Design Codes; we have only sought minor variations to setbacks due to the narrowness of the site and the requirements to incorporate essential facilities, store rooms, bike parking, drying courtyards and on-site provision for parking with limited access.

In direct response to submissions received:

Port School

- Number of Dwellings.

The proposal complies with Part 7 and Table 4 of the Residential Design Codes.

- Setback to Southern elevation

The entire development has been re-designed to accommodate the concerns of the Port School. The areas to the south are now non-habitable areas, i.e. no courtyards or areas for entertaining or areas where people would spend more than a few minutes, primarily it is an access corridor.

The stairways and access to these units comply with the Residential Design Codes; we are not sure what else we can do to appease the school in this matter. Screening to stairwells is not a Code requirement nor do we see it as appropriate given the short amount of time people spend entering and exiting their dwelling.

The use to the south is non-residential; as such the R Codes in terms of visual privacy in its applicability is questionable.

- Overshadowing

If the adjoining southern site were residential then Clause 7.4.2 of the R codes would apply. The adjoining site is an educational facility and not residential.

In any case if the adjoining site were also developed for R60 Residential Development then in accordance with Clause 7.4.2 A2 (Acceptable Development Criteria) then we would comply as we do not overshadow more than 50% of the adjoining southern site.

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We have separated the buildings to disperse any potential shadow impact on the School. A portion of their courtyard will be in shadow (as it is now with the existing dance studio) at the winter solstice, however (if this were a residential property being overshadowed) it would remain compliant. It is questionable whether or not the R Code applies where the land use adjoining is not residential in nature.

Mrs Procino (Northern Neighbour Wheeler Street)

We have listened to Mrs Procino's concerns and have the following comments:

- Balconies

The proposed northern balconies will not overlook her backyard and will be screened in accordance with the Residential Design Codes. We will not face them south as we would not comply with solar access requirements which would increase the carbon footprint for the development as well as increase the requirement for heating in winter and cooling in summer.

- Three Storey aspect

As seen on the Wheeler Street elevation, the three storey element is centralised to the width site, the site is narrow and the three storey element is contained to the street frontages only and is not at full height for the entire length of the lot. The skillion roof design reduces the bulk and scale issues as seen from the street.

- Use of existing car parking

Our original plan used the existing car park, the car park currently allows for cars to be parked perpendicular; however the current aisle width, as determined by Australian Standards and confirmed by the City of Cockburn engineering department, does not comply. We have therefore dispersed the parking throughout the site; this is also providing for sheltered and more convenient parking, of which all is covered. We do not wish to alter the parking configuration as it has taken considerable time and effort to confirm it as suitable.

- Northern Boundary Walls

There are two sections of boundary wall, one section is for the adaptable living unit and the other is to store rooms. Each individual wall complies with height and length requirements. These walls will be finished in a rendered or face brick finish.

- Security Fencing / gate

We are happy to provide security gates and fencing to reduce the access capabilities for the general public through from Carrington to Wheeler Street.

We believe that the increased number of residents will increase the surveillance opportunities and potentially reduce crime in the immediate vicinity. With more people being present and the elimination of dark and lurking place opportunity we believe that the re-development of this site will increase the appeal of the local area.

Please note that we are currently exploring diversity in building materials to produce energy efficient 8 star energy rated development, this will also potentially reduce the development build time and minimise the disruption to the immediate neighbourhood. The planning application indicates standard building materials; it is unlikely that the built form will be altered by utilising the SIPS product that we are currently investigating.

Please note that we are also investigating the option of partnership with the Department of Housing in accordance with the State Government's Shared Equity Scheme, which would see the provision and sale of affordable housing to eligible home buyers at a discounted rate, if this is successful it will contribute to the provision of affordable housing within the City of Cockburn, this has positive impacts on the affordable housing and rental crisis currently being experienced throughout Perth.

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We look forward to working with the City of Cockburn to achieve a planning approval for this site, if you have any questions please feel free to phone me Ross Minett on 0403 577 422 or email developtions@live.com

Kindest Regards,

A handwritten signature in black ink, appearing to read 'Ross Minett', with a large, stylized flourish at the end.

Ross Minett
Director
B.A Urban and Regional Planning

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developtions@live.com

SCHEDULE OF SUBMISSIONS

PROPOSED EIGHTEEN (18) MULTIPLE DWELLINGS – NO. 509L (LOT 509) CARRINGTON STREET, HAMILTON HILL

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
1	Nicvira Nominees Pty Ltd 10 Sunset Court SPEARWOOD WA 6163	<p>SUPPORT</p> <p>No objection to the proposed development.</p>	Noted.
2	Mrs Gaetana Procino 64A Wheeler Road HAMILTON HILL WA 6163	<p>OBJECTION</p> <p>I object the application for the following reasons:</p> <ol style="list-style-type: none"> 1. Balconies to face south not north towards my property for the three storey part. 2. Move the three-storey dwellings in the middle of the site, not near my property. 	<p>In regards to the Balconies, a condition has been recommended that the balconies within the cone of vision on No. 64 A Wheeler Road will be required to be screened in accordance with the R-Codes.</p> <p>If the balconies were orientated towards the south rather than the north, the development will more than likely not comply with solar access requirements and also result in an increased requirement for heating in winter and cooling in summer</p> <p>With the aspect of moving the three-storey portions of the development, the particular element of interest is the three storey element that is adjacent to Mrs Procino's property towards Wheeler Road. Three-storeys are an allowed height as per the R-Codes for multiple dwelling sites zoned R60. In addition, this proposed three-storey building also</p>

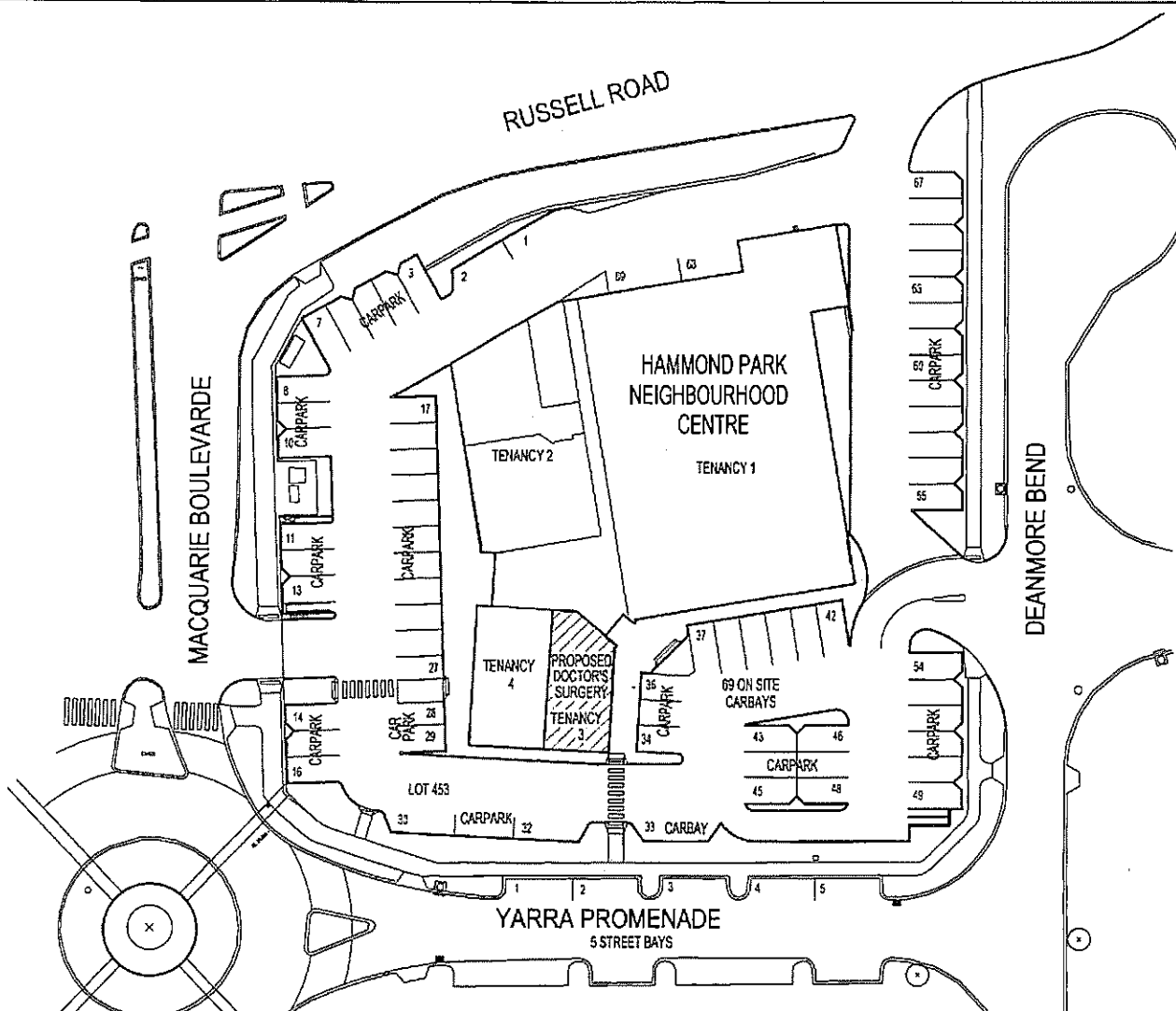
NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>3. Use existing car park for parking, and move development towards Carrington Street.</p> <p>4. North boundary walls to comply with R-Codes.</p>	<p>complies with upper floor setbacks and overshadowing requirements as per the R-Codes and the skillion roof design ensures the building will not present any bulk and scale issues when viewed from Wheeler Road or the northern properties adjoining the site.</p> <p>With using the existing car park, the applicant, in consultation with the City's Engineering department came to the conclusion that the current aisle width of the car park does not comply with current Australian Standards.</p> <p>As a result, the applicants have decided to not use the existing car park and disperse the provided car parking for residents and visitors throughout the site. This has resulted in sheltered and more convenient parking which is covered and easily accessible for each unit rather than some units having to walk from the western (Carrington Street) end of the site to access their car.</p> <p>With the northern boundary walls abutting the northern property, there are three (3) boundary walls proposed. As per Table 4 of the R-Codes, for multiple dwellings zoned R60, boundary walls are allowed to be a maximum height of 3.5 metres, average of 3 metres and in terms of length, not exceed two-thirds of the length of the boundary.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>5. Security gates to be provided so not anyone can access development".</p>	<p>Given the proposed wall heights vary from 2.82 metres to 3 metres, and the total length of boundary walls on the northern boundary is 17.89 metres which is less than two-thirds of the length of the northern boundary, the northern boundary walls comply with R-Code requirements. In addition, a condition has been recommended that the surface finish of the boundary walls abutting the northern property is to be finished and maintained to the satisfaction of the City.</p> <p>A condition has been recommended that a security gate is to be provided from the Wheeler Road vehicular entrance and pedestrian paths to the site as well as from the Carrington Street entry via the pedestrian path. This will ensure access to the subject site is only available for residents and visitors and will not allow pedestrians to walk through the site to access both Wheeler Road and Carrington Street.</p> <p>In addition, the proposed new development will provide additional surveillance of the streetscape through providing dwellings with major openings and upper floor balconies orientated towards Wheeler Road.</p>
3	<p>Port School Mr Barry Finch 62 Wheeler Road HAMILTON HILL WA 6163</p>	<p>OBJECTION</p> <p>The changing of the outdoor areas to the Northern aspect has been greatly appreciated; unfortunately there are still some major areas of concern.</p>	<p>Noted.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p><i>Number of dwellings</i></p> <p>We are concerned with the overall size of the development and would like clarification as to how a block with an R60 rating can have 18 units.</p> <p><i>Limited setback from the boundary and the close proximity of the access stairs to our northern courtyard</i></p> <p>The terrace/entry for units 17 and 18 on the second floor is an area big enough to congregate and directly overlook our northern courtyard.</p> <p>Units 6 and 7 on the first floor (1.2 metre setback) are nearly close enough for the tenant to shake hands with students.</p> <p>I feel that the potential for ongoing negative interaction is too great a risk and therefore strongly recommend that the areas screened or relocated to a central location.</p>	<p>The development complies with Part 7 and Table 4 of the Residential Design Codes in regards to multiple dwellings as it complies with the maximum plot ratio allowed of 0.7, proposed 0.7, as well as open space and height requirements for an R60 zoned property.</p> <p>With the setback to the southern boundary, towards the Port School site, the entire development for the site has been re-designed to accommodate the concerns of the Port School from the on-site meeting conducted on the 31 May 2012.</p> <p>The application now proposes towards the south boundary no habitable areas, i.e. no courtyards or areas for entertaining or areas where people would spend more than a few minutes. The southern area of the site is primarily now an access corridor.</p> <p>The stairways and access to these units comply with the Residential Design Codes as they are not habitable areas and therefore are not required to be screened for visual privacy requirements. The City does not see any reason to request screening from the</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p><i>Extensive shading over our northern courtyard and building</i></p> <p>Over \$80,000 has been spent on redeveloping our northern courtyard to give students a warm and sunny environment in winter. This development would keep the west half of the courtyard in permanent shade in winter affecting the amenity for the students and the passive solar heating for our classrooms.</p> <p>In conclusion all of the above issues are of concern with the proximity of the terraces of units 6, 7, 17, 18 and the lack of screening as an issue that will cause grief in the future. On this note I am seeking advice from Council as to the next process for appeal.</p>	<p>stairwells towards the school site in this instance.</p> <p>The school site is not a residential property or zoned Residential therefore Clause 7.4.2 of the R-Codes does not apply in this instance.</p> <p>The applicants have modified the plans to address the schools concerns and have separated the buildings to disperse any potential shadow impact on the School.</p>
4	Main Roads PO Box 6202 EAST PERTH WA 6892	<p>SUPPORT</p> <p>Thank you for your letter of 10 February 2012 inviting Main Roads comments on the proposed eighteen (18) multiple dwellings at the above location.</p> <p>As access to this site is from Wheeler Road, this development is acceptable to Main Roads subject to the following conditions:</p> <p>1: No earthworks shall encroach onto the Carrington</p>	<p>Noted.</p> <p>The conditions and advice notes recommended by Main Roads have been included as conditions as part of the Officer's recommendation.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>Street road reserve; and</p> <p>2. No stormwater drainage shall be discharged onto the Carrington Street road reserve.</p> <p><u>Advice to the Applicant:</u></p> <ul style="list-style-type: none"> - As this property is in close proximity to the possible future intersection of Roe Highway and Carrington Street, it is recommended that a transport noise assessment is undertaken and any noise amelioration measures or quite house design principles recommended in this assessment be included in the final design and construction of these units. <p>Should Council disagree with or resolve not to include as part of its conditional approval any of the above conditions or advice to the applicant, would you please advise Main Roads.</p> <p>Also, would you please forward a copy of Council's final determination on this proposed development quoting file reference 04/11993-15 (D12#62923).</p>	



SITE PLAN
SCALE 1:500

TENANCY 3 NET AREA:
83.5 sqm APPROX.

PROPOSED DOCTORS SURGERY
AT LOT 453 MACQUARIE BLD. (TENANCY 3)
HAMMOND PARK

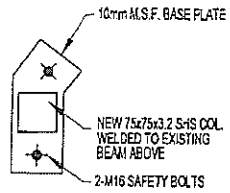


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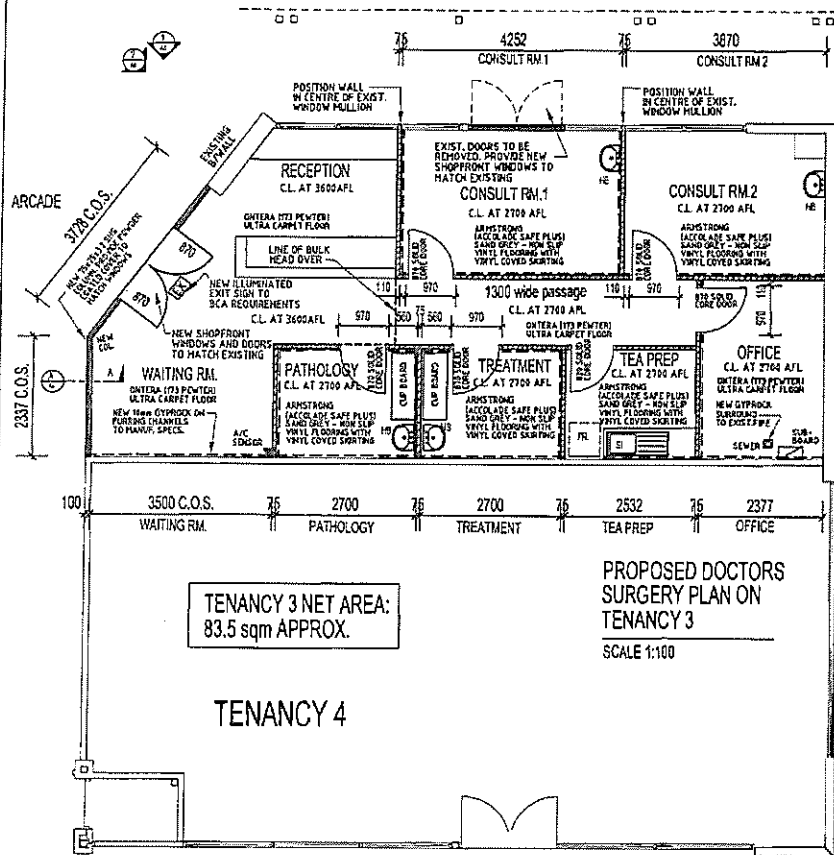
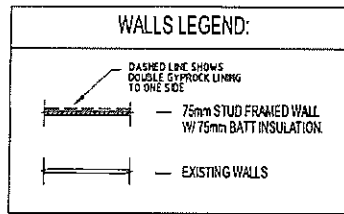
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Date:	9 May 2012	Drawn:	D.MORALES
Checked:	E.BIEMEL	Drwg. No.	A1

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OCM 13/9/2012 Item 14.10 Attach 1



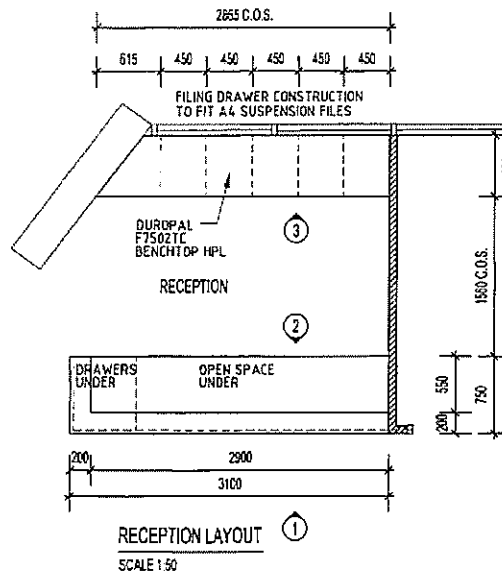
NEW COLUMN DETAIL
SCALE 1:10



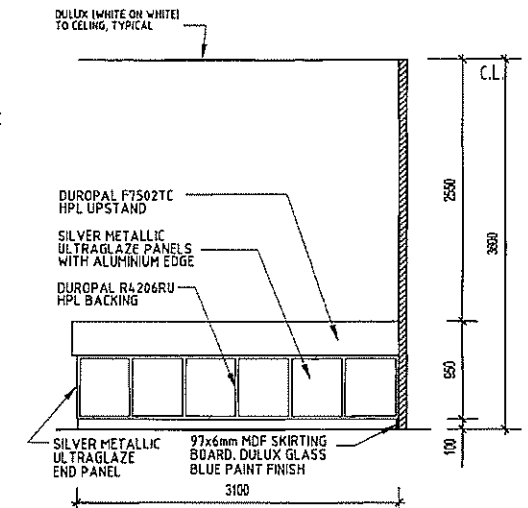
TENANCY 3 NET AREA:
83.5 sqm APPROX.

TENANCY 4

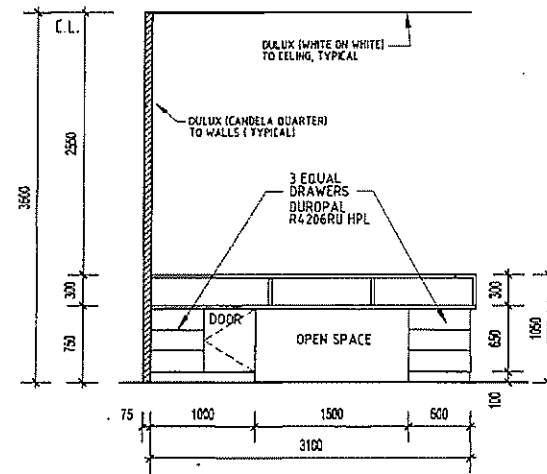
**PROPOSED DOCTORS SURGERY
AT LOT 453 MACQUARIE BLD. (TENANCY 3)
HAMMOND PARK**



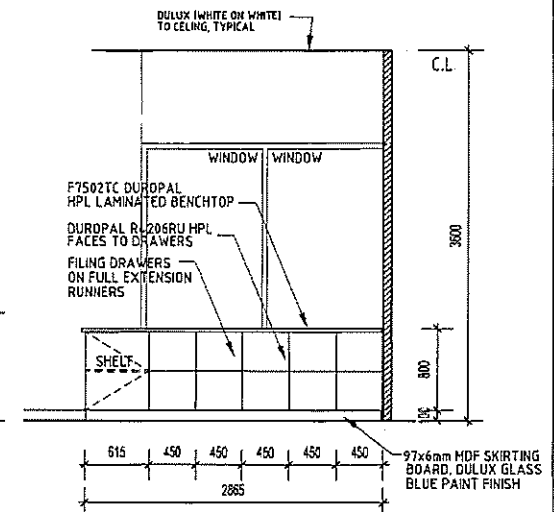
RECEPTION LAYOUT
SCALE 1:50



ELEVATION - 1
SCALE 1:50



ELEVATION - 2
SCALE 1:50

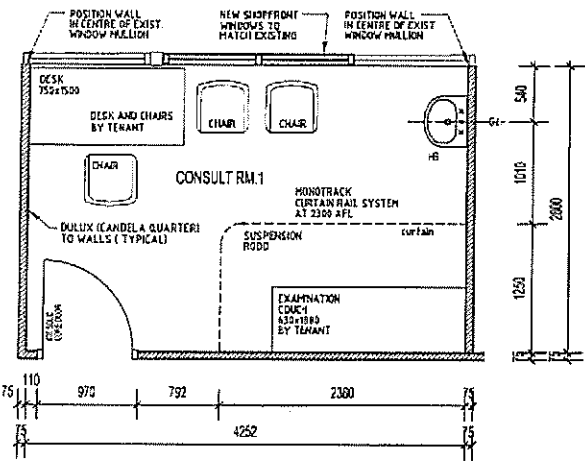


ELEVATION - 3
SCALE 1:50

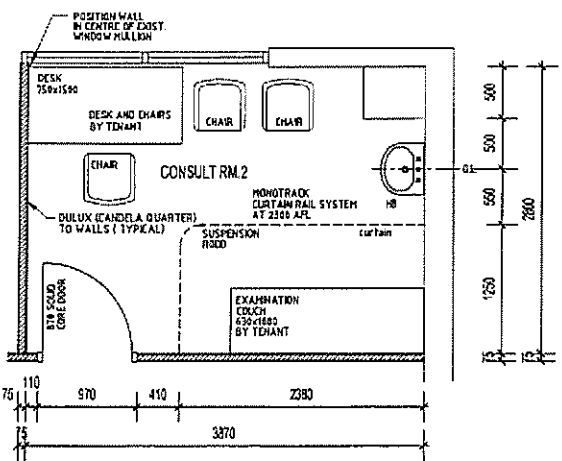
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Drawn:	D.MORALES	Checked:	E.BIEMEL
Checked:	E.BIEMEL	Drwg. No.	A2

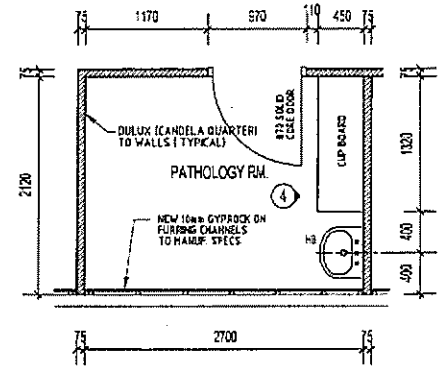
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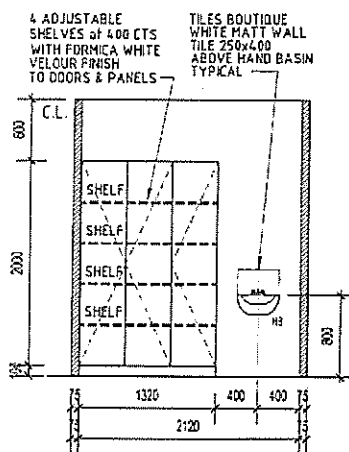
CONSULT RM. 1 LAYOUT
SCALE 1:50



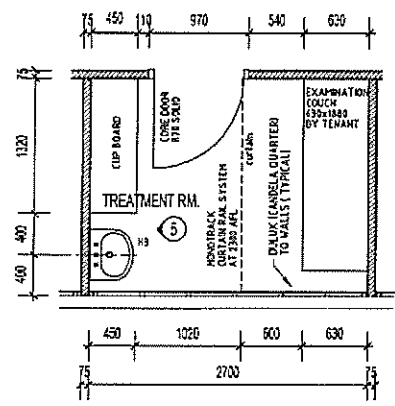
CONSULT RM. 2 LAYOUT
SCALE 1:50



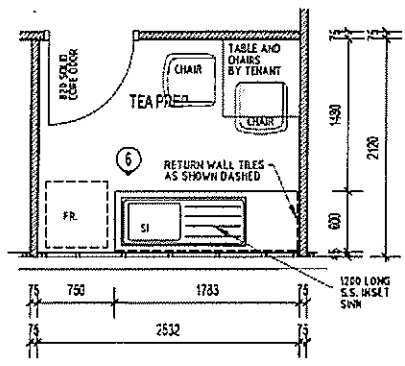
PATHOLOGY RM. LAYOUT
SCALE 1:50



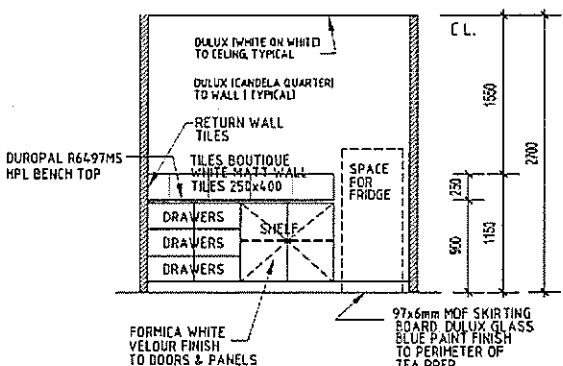
ELEVATION - 4
(ELEVATION - 5 OPP. HAND)
SCALE 1:50



TREATMENT RM. LAYOUT
SCALE 1:50



TEA PREP. LAYOUT
SCALE 1:50



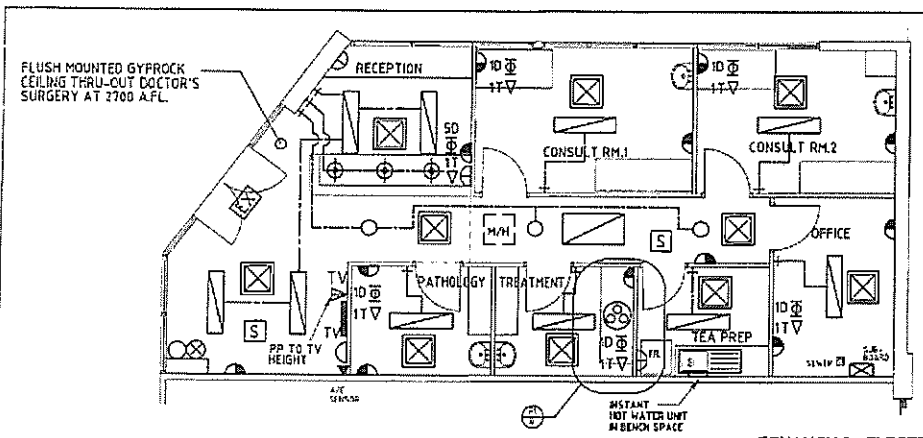
ELEVATION - 6
SCALE 1:50

PROPOSED DOCTORS SURGERY
AT LOT 453 MACQUARIE BLD. (TENANCY 3)
HAMMOND PARK

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Scale:	AS SHOWN	Job No.	92283
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Drawn:	O.MORALES	Checked:	E.BIEMEL
Checked:	E.BIEMEL	Drwg. No.	A3

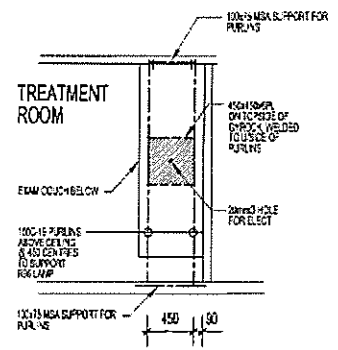
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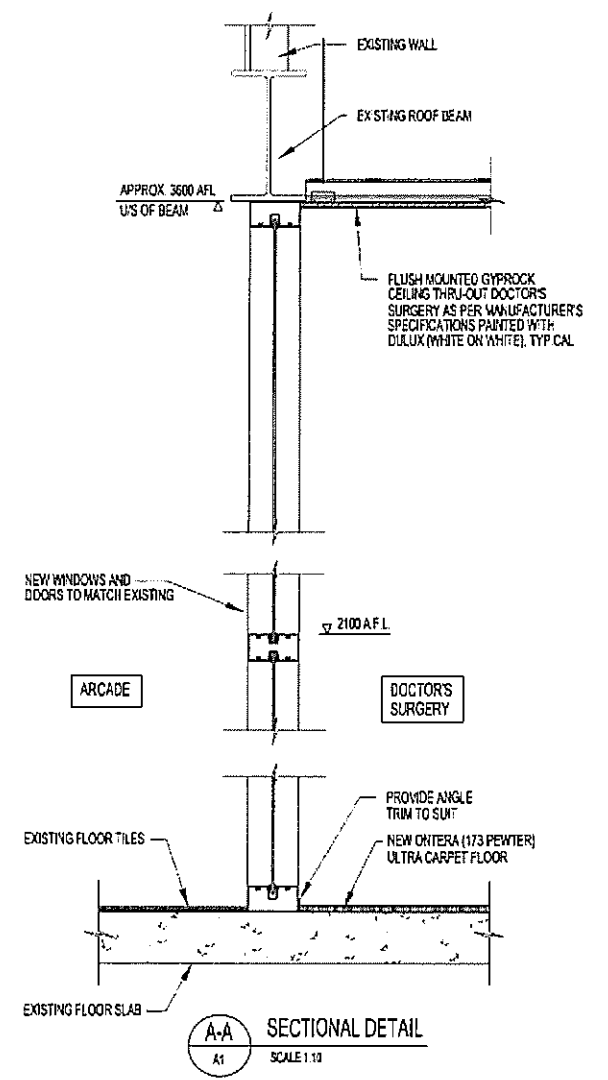
SYMBOLS LEGEND

- + 10A, 240V, WALL SWITCH-1WAY.
2=2WAY N=NEON INDICATOR
- SUB BOARD
- BUILDING MAIN SWITCHBOARD
- DOUBLE GPO AT 300 A.F.L.
- SINGLE GPO AT 300 A.F.L.
- SINGLE GPO ABOVE BENCH HEIGHT
- DOUBLE GPO ABOVE BENCH HEIGHT
- INTERNET DATA POINT (3 = 3 OUTLETS)
- TELEPHONE POINT (3 = 3 OUTLETS)
- INSTANT HOT WATER UNITS IN BENCH SPACE
- VB3470-70w CIRCULAR METAL HALIDE (SMALL) DOWNLIGHT
- METAL SPOT-CONDS0722 SWORD 2000
100w PENDANT LIGHT FITTING
- 2x36w TROFFER FLUORESCENT WITH OPAL
- EXIT SIGN BY OTHERS
- SUPPLY AIR REGISTER
- RETURN AIR REGISTER
- EMERGENCY FLOODLIGHT
- SMOKE DETECTOR
- TV POINT
- LG 26 LK 330 WALL MOUNTED LCD TV 663x423x80
- TRIGENFLEX R96 OPERATING LAMP

TENANCY 3 - ELECTRICAL PLAN
SCALE 1:100



P1 OPERATING LAMP SUPPORT
A4 SCALE 1:50

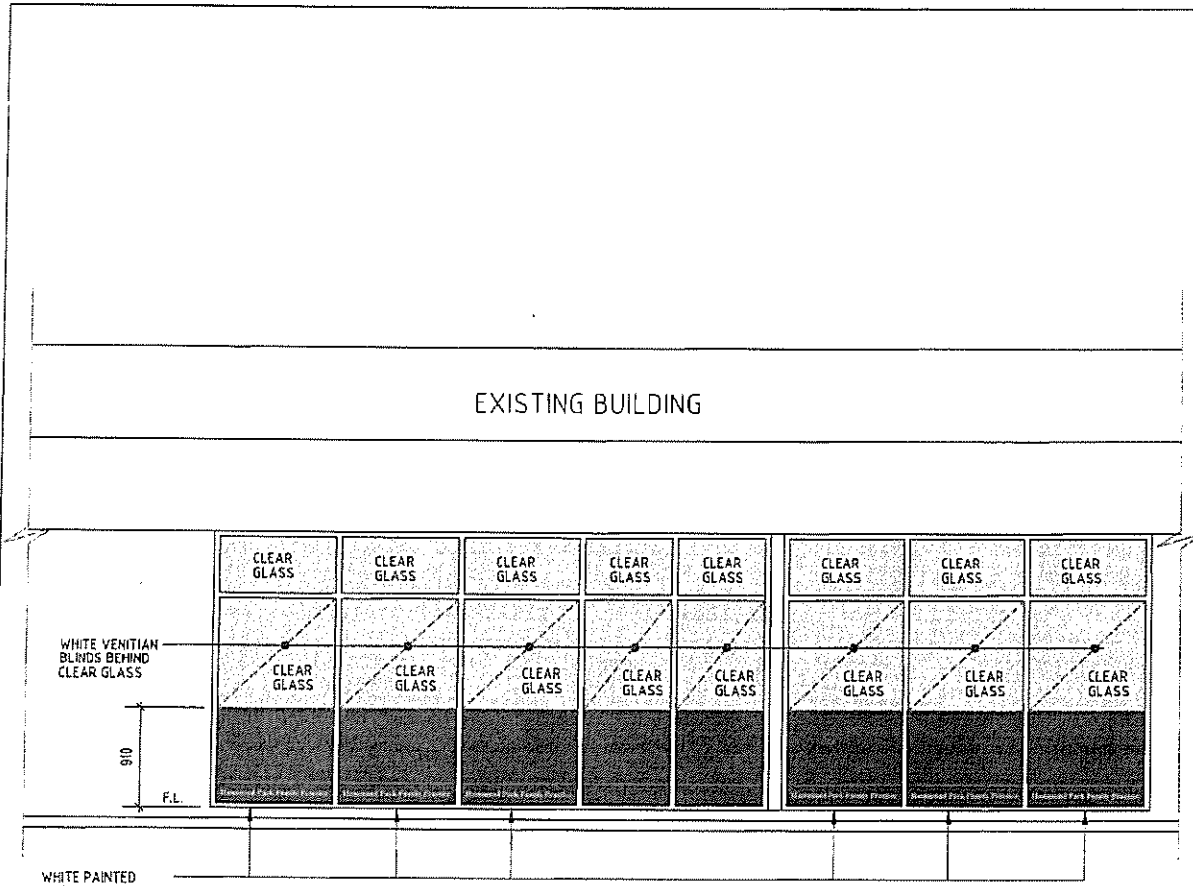


PROPOSED DOCTORS SURGERY
AT LOT 453 MACQUARIE BLD. (TENANCY 3)
HAMMOND PARK

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Scale :	AS SHOWN	Job No.	92283
Date:	20 June 2012		
Drawn:	O.MORALES	Drwg. No.	A4
Checked:	E.BIEMEL		

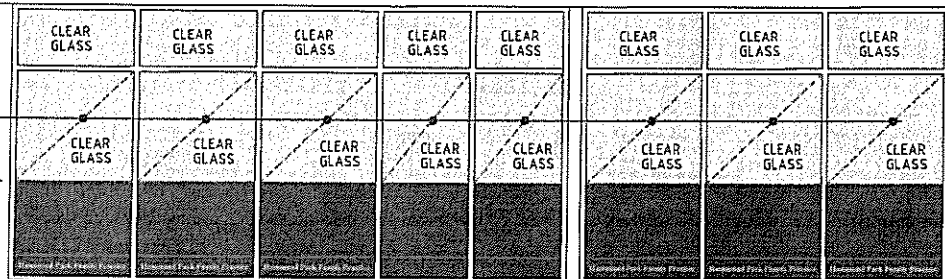
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EXISTING BUILDING

WHITE VENETIAN
BLINDS BEHIND
CLEAR GLASS

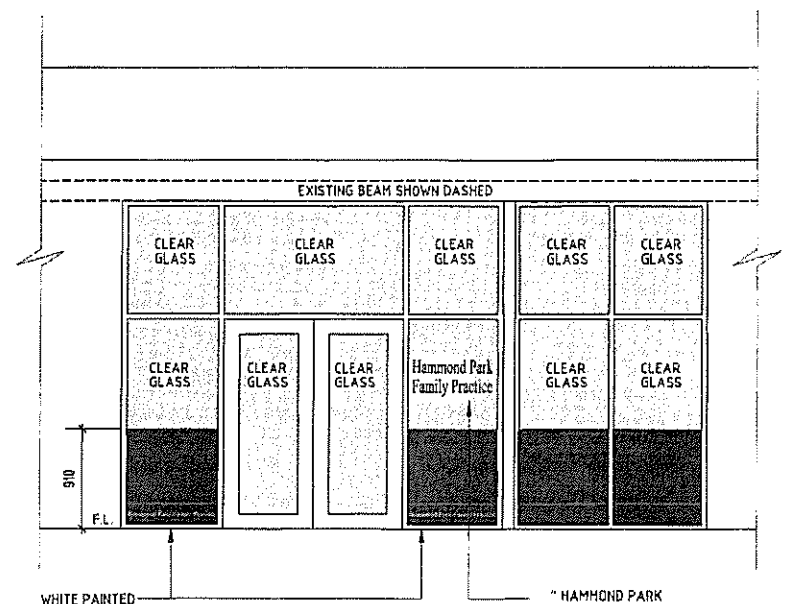
910
F.L.



WHITE PAINTED
WATERMARK ON FILM

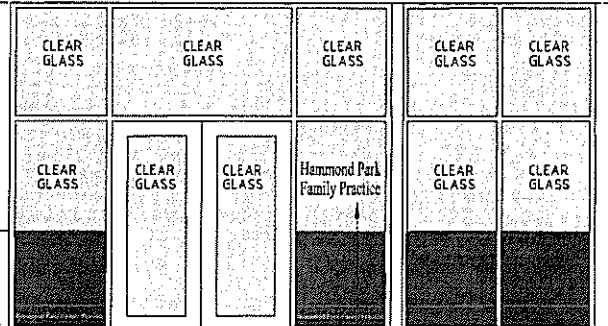
NOTE: SIGNAGE TO WINDOWS
DIGITALLY PRINTED UNDER SELF
ADHESIVE VINYL FILM AND APPLIED
INTERNALLY

1
A2
WINDOW TREATMENTS
PART ELEVATION
SCALE 1:50



EXISTING BEAM SHOWN DASHED

910
F.L.



WHITE PAINTED
WATERMARK ON FILM

"HAMMOND PARK
FAMILY PRACTICE -
IN 50mm LETTERING
TO APPROVED FONTS

2
A2
WINDOW TREATMENTS
PART ELEVATION
SCALE 1:50

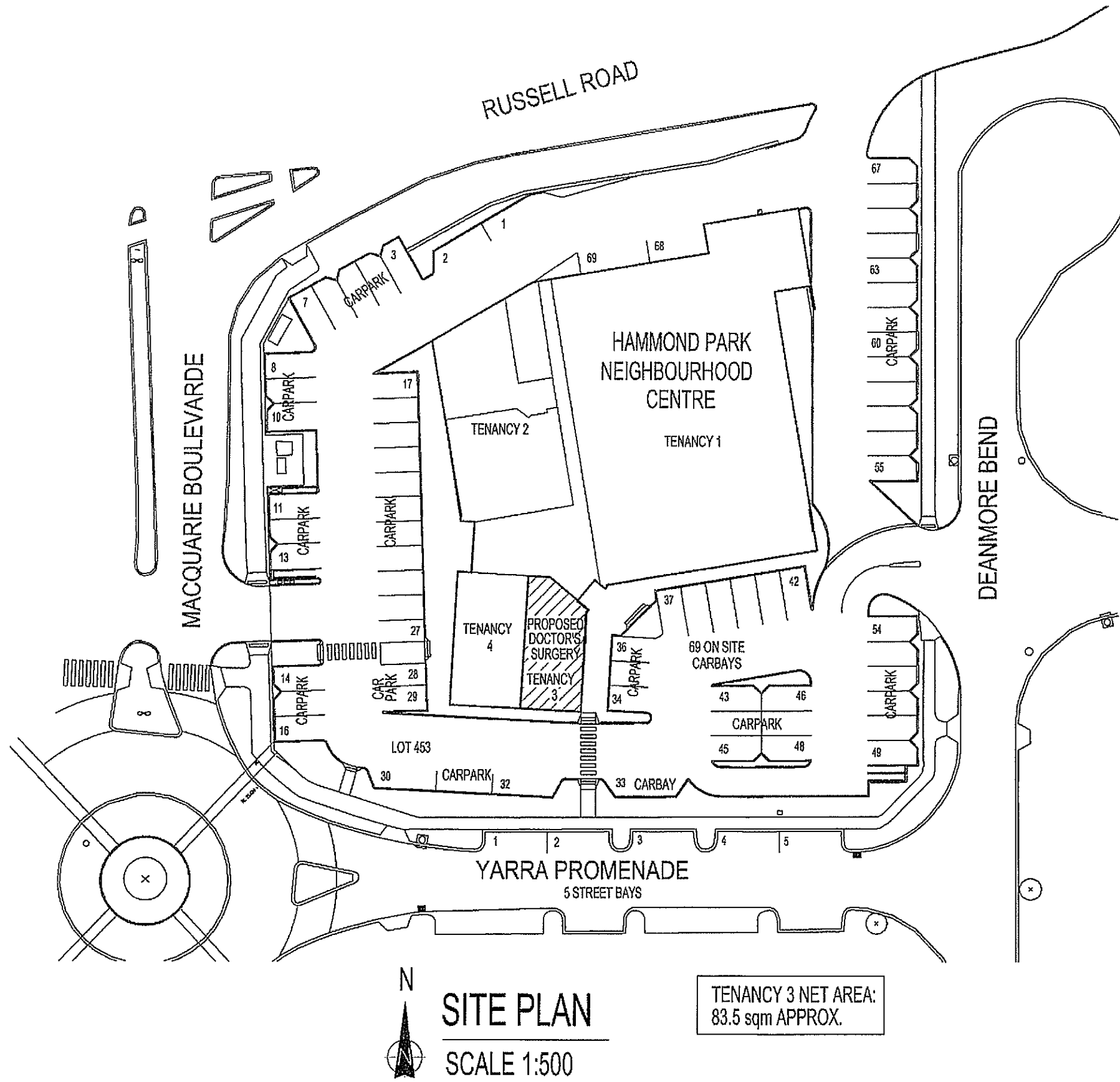
PROPOSED DOCTORS SURGERY
AT LOT 453 MACQUARIE BLD. (TENANCY 3)
HAMMOND PARK



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Scale:	AS SHOWN	Job No.	92283
Date:	20 June 2012		
Drawn:	D.MORALES	Drwg. No.	A5
Checked:	E.BIEMEL		

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SITE PLAN
SCALE 1:500

TENANCY 3 NET AREA:
83.5 sqm APPROX.

PROPOSED DOCTORS SURGERY
AT LOT 453 MACQUARIE BLD. (TENANCY 3)
HAMMOND PARK



Erwin Biemel & Associates

UNIT 5-2 WITTENBERG DRIVE, GANNING VALE, W.A. 6155
TEL: (08) 9455 3099 FAX: (08) 9455 3680

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Scale :	AS SHOWN	Job No.	92283
Date:	9 May 2012	Drwg. No.	A1
Drawn:	D.MORALES	Checked:	E.BIEMEL

13 102

The Mayor

City of Cockburn

Western Australia

Dear Mayor

We the Business owners of the Hive Shopping Centre at Hammond Park, 1 Macquarie Boulevard do strongly support the application of Dr Michael Gendy to operate a two (2) man Medical Practice at Tenancy 3.

We support his request on the following grounds:

- The demand for medical service is increasing in this neighbourhood as the demography in this suburb consist mainly of young children who need medical attention all the time
- There is no other medical service in our neighbourhood to provide for this increase in demand
- Most of the staff who works in this centre either live locally and hence walk or catch a bus to work
- There is never a shortage of parking bays
- It is not uncommon to have 30 – 35 car bays empty most of the day
- Most of the shoppers do not stay too long at the Centre other than to shop at the IGA or the Pharmacy. Hence car bays are never occupied for any length of time
- If the Medical practice is proposing to cater for an afterhour's clinic then it is of paramount importance to approve a two doctor clinic.
- Our customer feedback is always positive about having vital medical services in our area especially a 7 day practice that accommodates emergencies. It is important to our business's success to keep the local community happy.

Thank you for your support.

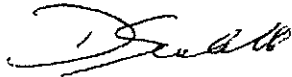
I.Singh



Owner of IGA

Hammond Park Guardian Pharmacy

Denny S



Owner

Cafe 15

Manager

Kim Browning



DG Beauty Salon

Owner




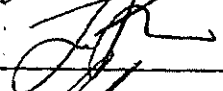
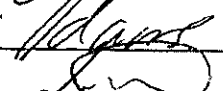

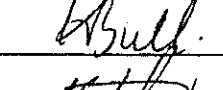
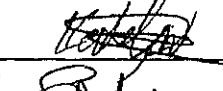
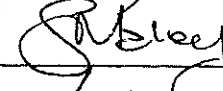
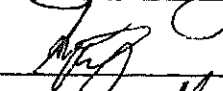
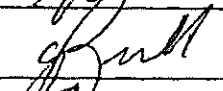
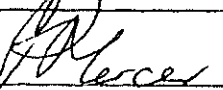
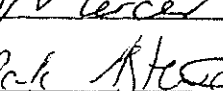
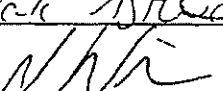
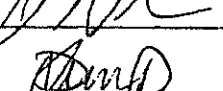


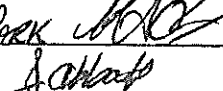
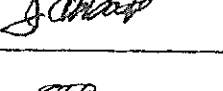

Diana Scata



Dated:

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We have found no problem parking at all times at the shopping centre and some of us who live nearby walk to the Centre.

Name	Address	Signature	Telephone
Luis Ramos	12 Kinglake way		0431 862230
Julian Jones	Aubin grove		0439365401
Louise Rosin	4 Antina way Success 10 Tongaroo Blvd Success	LR	0417 650659
K. Hutchins	4 Sutter Dr Success		0416439708
T. BROWN	UNITY WAY ATWELL		0412911623
J Adams	3 LONDON LINK Aubin Grove		0416628031
J Thrupp	3 Cornell way		0410927503
K BULL	7 MANITOBA PLACE Success		0426 585 289
K Williamson	10 Starbush Gardens, Success		0448 692 559
S. Maloney	8 Caridean way Hammond Park		0401 386 860
A Chaw	58 Sutter Dr Success		0422 497 583
C PARNELL	4 BODELLA ATWELL		0408 555 123
P. MERCER	23 DULCET LINK, ATWELL		0413738897
A. Stewart	5 Warrago St Hammond Park		0407922090
L. Wilkinson	7 DULCET LINK ATWELL		0414 379 428
D. Jump	15 Covington loop, Success		0407784482
S. Jump	15 Covington loop, Success		04133344990
K. OUTRAM	20 MOHAN LOOP, HAMMOND PARK		0417920157
J. Abbott	10 Jubilee Ave Success		0451013899
S Williamson	10 Starbush Gardens Success		0666 482 217
Nick Jinks	32 BAUMEA TURN HAMMOND PARK		96987012

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Name	Address	Signature	Telephone
Amanda Dixon	2 Hunter Way Hammond Park		0107999910
Emma Nash	49 Davies St Turin Success		
WENDY TROTT	7 ALEPPO ST. AUBINGROVE		
Belinda Stokes	19 Ironbark Terr Hammond Park		0419837192
Chelsea Katta	44 Columbus Loop Success		0417941083
Kylie Irons	2 Grenada Lane		0428341976
Kat Armstrong	25 YARRA PROM		0423347216
Scott PAYNE	25 YARRA PROM		0423347216.
Amr Ajjawi	9 IRONBARK TCE		0406966112
MARIA FITZPATRICK	14 ROSEE BLVD,		0411161128
CHRIS GRAY	91 Towner Retreat, Atwell		-
MARGARET CASS	7 IRONBARK TCE		9414 5967
Darryl Munro	31 Unity Way Atwell		08 94993315
Carol Dennell	9 Richmond Est. Success		0417 592945
JOE JACKSON	65 FOLLARD PDE ATWELL		0423331695
BAILA HAZLO	3/2 PREEG YANKEBOP		0410811618
Daniel Hayward	25 Leichhardt Crg		0438997314
Jennette Ward	3 Karakin place bertam		0433126737
Jennifer Caradonna	27 barcoo bend <small>hammond park</small>		0416918173
Jaudia Caradonna	27 Barcoo bend <small>hammond park</small>		0412 823 356
Greg Adams	3 London WNK AUBIN GROVE		0419030081

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Name	Address	Signature	Telephone
Sharon Wham	4 Moorah Way, Hammond Park		9498 5019
Heather Disney	489 Lyon Rd, Wandli		94100143
Daniela Rebelo	65 Plumwood Ave Hammond Park.		94986332
M Bokljat	Emperes App		94991799
Ivan Ricci	100 Warriner Rd Beelarra		94372180
Todd Eloger	17 Cordonia Ave		94142152
Alicia Monkey	37 Barcoo Bend		94993167
M. MROOH LONT	37 JACKAROOD AVE		
S. Scharman RN	4 Lerex Bend 15 Sandieford Way HAMMOND PARK WA 6167		0417984015
Janet ZAGARI	HAMMOND PARK WA 6167		048904324
T. MATHIOLI	8 Innes Vista Hammond Park		
KHEARNDEN	16 Woodrow Ave, Hammond Park		0417098452
Dianne Dayton	235 Barse Road Beelarra		0420274315
Tina Vogler	8 Ashenden Blvd		0417452810
A DeGennaro	18 Warrigo St Ham PK		0417982129
Nathan Cheung	12 Featherflower Rd, Hammond Park		0416114013
KATIE SMICK	30 Samouille Way Hammond Park		0419097972
R de Arcaj's	312 Lyon Rd Grove Aubin Grove		0457765658
MARK LAMER	22 CAMDEN BUDS AUBIN GROVE		0414714281
AIMEE HARLOWAY	9-1 JARDINE ST		0415653743
LIARIE BERNI	9 NAMOI ST Hammond Park.		

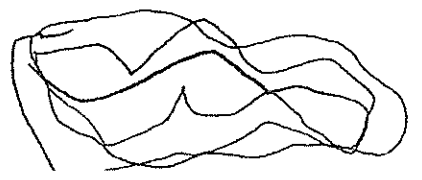
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Name Address Signature Telephone

1. Marie Doherty	24 Starbush Gardens Success	Melba	0407772822
1. Kristelle Bailey	3 Camden Blvd Aubin Grove	K Bailey	0411074425.
2. MARK WILKINSON	7 DULCE LINK TRWELL	[Signature]	0418939505
1. ROMA GOODCHILD	29 ROPER BLVD HAMMOND PARK	[Signature]	94985616
1. Katie Brennan	40 Macquarie Blvd	[Signature]	0405943665
1. KAREN BROOK	35 TRIANONRA CRT, BANJOA	[Signature]	0417976577
0. BRYAN WEBB	7 WARRAGO ST. HAMMOND PARK	[Signature]	94982488
1. Aulis ZAMAN	3 CAMPASPE DRIVE	[Signature]	0420143018
2. Patricia Boyle	8 Auld Vista Success	P Boyle	0416055930
3. VIVIANE GRANT	17 PLUMWOOD AVE HAMMOND PK.	V Grant	08 94985179
+ Katrina Fisher	9 Jackadder Ave, Hammond PK	[Signature]	0404 052 500
5. Julie Elliot	18 Windchime Tce Anner	J Elliot	0410804673.
6. Jodie Connors	8 Carnegie Parade, Success	J Connors	0438 778144.
7. Peta Costius	11 Barcoo Bend Hammond Park	P Costius	0405 496631.
6. Sandra Squitres	6 Jardine Str Success	[Signature]	04 03661 665
1. Steve Oliver	6 Jardine St Success	[Signature]	0427696917
1. Dande Johnston	19 Lorne Ave Hammond Park	[Signature]	0439476356
1. BLYTHE JONES	10 FEATHERFLOWER RD HAMMOND PARK	B Jones	0417 988 038
1. KYLIE JEMMA	10 Bremerway Success	[Signature]	0401333533
1. Sandra Herrera	15 Barcoo Bend	[Signature]	0420278350
1. M. Sieling	Hammond PK	[Signature]	0449827310
5. H. Edgell	5 Dominion, success	H Edgell	042859521



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Name Address Signature Telephone

Name	Address	Signature	Telephone
1. A. B. Stewart	31 Columbus Loop ^{Success}	A. B. Stewart	94997966
2. G. Alexander	10/42 Baler Court ^{Hammond Park}	G. Alexander	94994874
A. Roberts	3 Harvest Lakes Blvd Atwell	A. Roberts	9440675
1. J. Sharp	17 La Grange St, Success	J. Sharp	0410306505
1. T. Newhall	1 Terrat Mens Atwell	T. Newhall	0433366488
1. Pikia Tarahiti	1 Terrat Mens Atwell	Pikia Tarahiti	0403495309
1. Teagan Newhall	1 Terrat mens Atwell	Teagan Newhall	0430112680
2. Gavin Newhall	1 Terrat mens Atwell	G. Newhall	0418242630
3. Tayla Newhall	33 Berigan Drive ^{Stn Lake}	Tayla Newhall	0433064849
4. Lisa Webb	18 Sweetwater Bend ^{Success}	Lisa Webb	
5. Lauren Kallucke	49 Baler Ct ^{Hammond Park}	Lauren Kallucke	043949825
6. Jo Gardner	17 Terra Prom H/PK	Jo Gardner	0407779345
1. A. Mares	17 Terra Prom H/PK	A. Mares	94986305
5. A. DAVIS	12 Canidean Way, H/PK	A. Davis	0421637713
1. I. JOREK	15 Entente Tee, Atwell	I. Jorek	0402217411
1. K. Stackhouse	Hammond Park	K. Stackhouse	-
1. S. O'Halloran	6 Corimbla Lane ^{Auburn Grove}	S. O'Halloran	0426737360
2. A. Tyler.	17 Jouisance Bend ^{Atwell}	A. Tyler	0418298754
3. L. WATTS	7 IRONBARK TEE H/PARK	L. Watts	94145967
4. G. WATTS	7 IRONBARK TEE H/PARK	G. Watts	94145967
5. Neil Bartholomew	6 Flametree Bend H/Park	Neil Bartholomew	0449505095

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Name Address Signature Telephone

Name	Address	Signature	Telephone
06 H. O'HARA	Lorimer Rd Watteup	H. O'Hara	0420293384
07 Shiva Bazzagani	10 Flinders Cres. Hammond Park	Shiva	0423227208
08 Alicia Pearce	Leichhardt Cross	A Pearce	04299805
09 Jessica Cartwright	12 Roper Blvd. Hammond Park	J Cartwright	0421850131
10 Debra Williams	Starbush Gdns, Success	Debra	0427454808
11 Mick Murphy	192 Gill-Banyup	M Murphy	0418928465
12 Don Campbell	59 Plumwood Ave Hammond Park	Don Campbell	0432384002
13 Bev Math	40 Jackabee Ave	Bev Math	0428108408
14 Yvonne Bradford	835 Wentworth Parade Success	Y Bradford	93990463
15 L Marinovich	215 Wentworth Parade Success	L Marinovich	94178800
16 Adrian McLeath	5 Madison Link Success	A McLeath	0423948747
17 LINDA JMERILLI	20 ROPER BLVD HAMMOND PARK	L Jmerilli	0400205121
18 VALERIE OSBOERNE	64 Collis Rd Watteup	Valerie Osborne	0400783366
119 BEV SMITH	1 Flinders Gs HM PK	Bev Smith	941456871
120 Melinda Lute	2135 Barfield Rd	M Lute	0406507799
121 CRAIG DAVIOS	46 Macquarie BLVD	Craig Davios	0420920182
122 Jasmin Gagliardi	75A Collier St Milton	J Gagliardi	041530131
123 Jason Corson	2 NAMOI ST HAMMOND PARK	J Corson	-
124 Karen Ohman	314 Wentworth Parade Success	K Ohman	0403574153
125 Elsie Snook	6 ULYSSES ST, SUCCESS	E Snook	0432895836
126 Bev Sheridan	11/18 Deunmore Road Hammond PARK	B Sheridan	-

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Name	Address	Signature	Telephone
Sara Reilly	15 Mariner place Colongup	Sara Reilly	0433531769
John Bell	45 Plemons Ave		0466024432
Alex Molina	3 Yarra Promenade Hammond Park		0402 692195
Carol Curtis	6 Galveston Bend, Success		0414 438 358
Charlotte Parnell	4 Bodela Gdns Atwell	epall	0414 693 202.
Philippa Parry	11 Wkela Loop Atwell		0449887849.
Michelle Young	9 Vetcher Meadows		0417120045
Edna Pascoe	22 Featherflower R HP		0407 860658
Christie Bms	31 Frankena Turn Success		0421 858 644
Robert Law	8 McCaus Meadows Yangebup		0408 667 768
Amandales	7 Millstream P/		0415041584
Neisha Pes	7 Millstream PLACE		0450237984
LISA DOIG	3 Susetta App Success		0412863133
MARVIN WILBY	11/31 BARFIELD RD		0457300824
John Pes.	7 MILLSTREAM PLACE		0438747994
KATE FIELDING	9 NELSON ST. HAMMOND PARK		0404110041
Jo Bumbak	11 Junier Way H/Park		0423 926690
J RICHARDSON	58 HEBBLE LOOP BANYUP		

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Name	Address	Signature	Telephone
B. SELBY	10. PANTON DRIVE. SUCCESS.	B Selby.	0894991918.
Jhauna Tromp	26 Flametree bend Hammond Park	J. J Tromp	0894994650
Sandra Erceg	292 Tapper Road Atwell	SE	0409348380
KANE HURST	128 BRENCHLEY DRV ATWELL	K Hurst	0451 117625.
Sarah Twomey	5 LAIGO WAY, HAMMOND PARK	S Twomey	0422231218.
Mariann Jeffrey	226 GAEBLER RD WA 6164 AUBIN GROVE	M Jeffrey	0439479237
Lanni Limmanta	18 Brushfoot Boulevard Success	L Limmanta	0451 968 895
Ling Huang	6 Majorelle Pkwy Aubin Grove	L Huang	0430 685 332.
'ACQUE SCOTT	36 LONGDON AVE BEELIAR	A Scott	0417 189374
GERALDO FORMENTUS	32 Johnsonia Bend, Hammond Park WA 6164	G Formentus	0417.537-508
Emma Zoppia	14 Longwood Bend Aubin Grove	E Zoppia	0403329488
Wayne Biddle.	5 Inverson Blvd Hammond Park	W Biddle	0417977622.
Sheena Kachuwakku	1 Dianella RD, Hammond Park	Sheena	04244.73196

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Name	Address	Signature	Telephone
Natasha Pederson	71 Brushfoot Blvd Success		9499 3113
Tracey Sutherland	8 MEERUP Drive Success		9499 3279
JANDRA DILLON	9 ISAAC ST HAMMOND PARK		0406397357
Sarah Harris	77 Barfield Road HAMMOND PARK		0405230565
Gleada McGregor	215 Gaebler Road Aubin Grove		0433477686
Nicolene Styn	6 Ponderosa Road Aubin grove		0426249921
Sarah Oates	21 Barcos Bend Hammond Park		0425 019 118
Matthew Oates	"		0425 284 280
ASHLEY MURDOCH	7 MONARCH GATE		0452079871
Jim O'Hara	137 Lorimer st		94100295
Kathrine Robinson	8 Frankena Turn Success		0422515863
Estelle Powell	35 Sciano Ave, Success		0429792861
Richard Betts	14 Campys loop Hammond park		0427 674 589
JUDY CASAS	508 Russell Rd WATTLEUP		0433412255
KIRSTI ARMSTRONG	15 FLAMETREE BEND		9414 5151
Kim Hansen	47 Camden Blvd		042 5356306
PETER ALEXANDER	9, RANDWICK BEND, HARRISDALE		0409683136
Heidi Engels	712 Judline St, Success		
JANE FORBES	264 YANKEEBURD YANKEEBURD		0400965850
MATHEW NELSON	17 TWILIGHT MEWS AUBIN GROVE		
Ros Burges	12 Possum Crt Beelion		0437785311

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Name	Address	Signature	Telephone
Steve Cotton	3 Beinde Drive		0402185280
MEL CROOK	5 BLACKFORD TURN		0439311029
MERCY ABEY JOSE	123 Para Drive, WA-6112		0430717250
Emily Smith	28 Mohan loop		0411084601
MARK LIDOLETON	2 Weebwa Way		0409 293 786
NATALIE CARROTTEEDS	8 Gaunt Rd		0404206726
Rebecca SII	22 Princeton cct Auburn Grove		0410314399
Claire Barham	8 Bondi Way		0430922477
GAMT WALLACE	25 CAMERON WAY AUBURN GROVE SUITE 201		0418994542 0404092733
Narelene Day	25 Carnegie Parade SUCCESS		
SIM BROWNING	5 HAZNER WY COORANGUP		0401727320
Rob Twomey	5 Laigo Way Hammond Park		0467806890
Marie Kjellgren	9 Sandieford Way Hammond Park.		0422976647
Tom Lloyd	46 SILBERLINE ROAD BERTRUM.		0438554740
Kuan Tan	62 Cameton Blvd, Auburn Grove.		0431745400
Ydia Sii	" "		0433761086
Kyrie Elwin	6 Pitch way Auburn Grove		0607445247
MELDA ROSS	4 Neo DRU. Red Hill GROVE		0418956493
Shane Ross	4 Neo Drive senille Grove		0439430161
ANITA MICALE	33 CINCOTTA LOOP BEEVER		0448044658

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Name	Address	Signature	Telephone
Marie Enright	73 Harmony Ave, Atwell 6164		0416144197.
Kay Aiken	17 Bologna Gd.		0400206853.
Valene Osborne	Aubin Grove 64 Collis Road Wattleup		0400783366
BOB DENNIBERLEY	32 SCORALL CRIS, ANIKATU		0419927356
Travis Darcy	8 Plymouth Corner Success		0424701880
Darlene Darcy	8 Plymouth Corner Success		0406242324
AKASH SAHNI	56 KESINA TURN AUBIN GROVE		0433919112
ADAR MATHUR	20 SANDLEFORD HAY SUCCESS		0410839173.
HARSHIKA PATEL	31 BRUSHFOOT BVD, SUCCESS		0401934382
Paul Diller	9 1500C ST H. Park		0400010380.
CHERYL CAMERON	85 LES VEUILLIERS PASS PLAR		0421008720
Angelica McGregor	9 Southaven Green, Success		0405517622
Courtney Caygill	21 Dunlap Avenue Success		0420704145.
Lesley Lavery	33 Columbus Loop Success		0410356368
Deborah McPherson	19 Camden Blvd Aubin Grove		0452215704
T. MCPHERSON	19 CAMDEN BLVD AUBIN GROVE		0408909937
A. Moloney	40 Barkfield Rd Hammond Park		0430490905
Caradi Miller	3 Blondell Drive, Munster		0438658698
KELCY DE FLORENCA	9 NORVALUPCC AUBIN GROVE		0404974598
Lianne Clark	14 Leichhardt Crisg Hammond Park		0400249415

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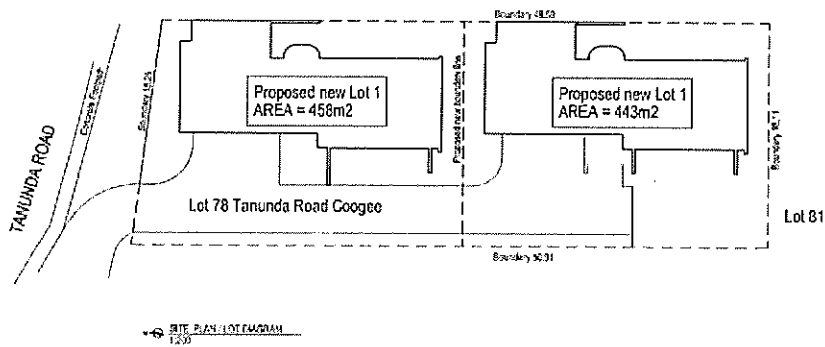
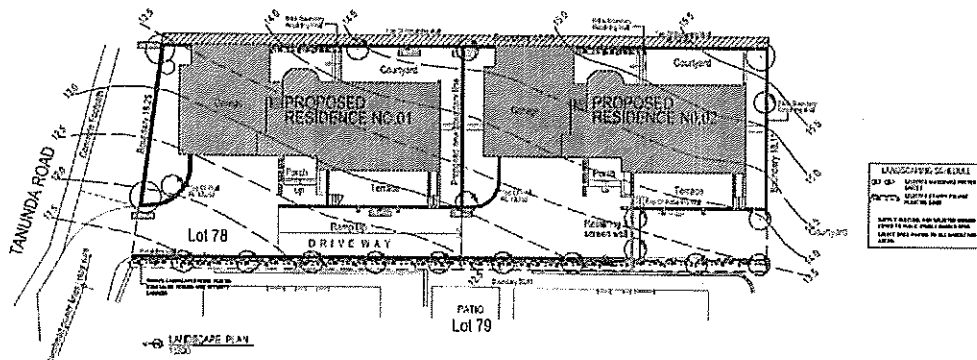
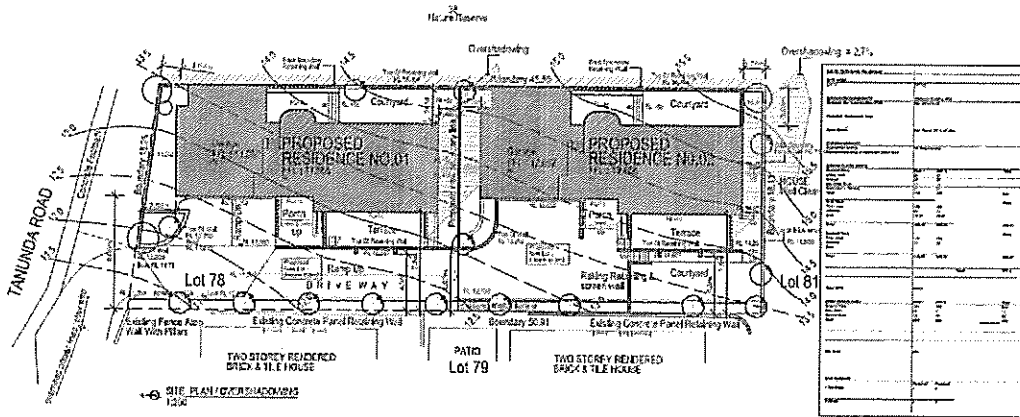
We have found no problem parking at all times at the shopping centre and some of us who live nearby walk to the Centre.

Name	Address	Signature	Telephone
Sarina Das-Bartholomew	54 Princeton Cct, v Aubin Grove blbf		0433753449.
Michelle Donald	4 Barwon Turn Hammond PK 6164		0407465728.
NEIL ENSOR	25 SOLACE CINDS ATWELL		0405444023
Kieran O'Neill	43 Darwin Tce Dudly Park		0424171411
Jo Reid	36 Roper Blvd Hammond Park		0437845653
ALIESA WILSON	67 Peregrine circle Beelihar		0408901143
CHRIS WAKE	114, THE HORSESHOE WANDI		0417957662.
Diane Muffit	192 Gibbs Rd, Bazup 6164		9417 2747
Tajla Newhill	39 Leontes Way Coolbellup 6165		0433064849
Sacha Marriott	33 Berrigan Dr South Lake 6164		0468738507
Guy Alderson	5 Witterin Loop Success		0416201924
Melissa Robertson	13 Bree close Attuel		0429186827.
Bonny Samuel	1 Monarch Gate Success		0410364333
Jaine Tamapua	16/31 Mulata Cres, Success		0405711665
Tony Nagy	16, Leichhardt Crossing Hammond PK		0414511651
Eden C Nagy	As above		0450449262

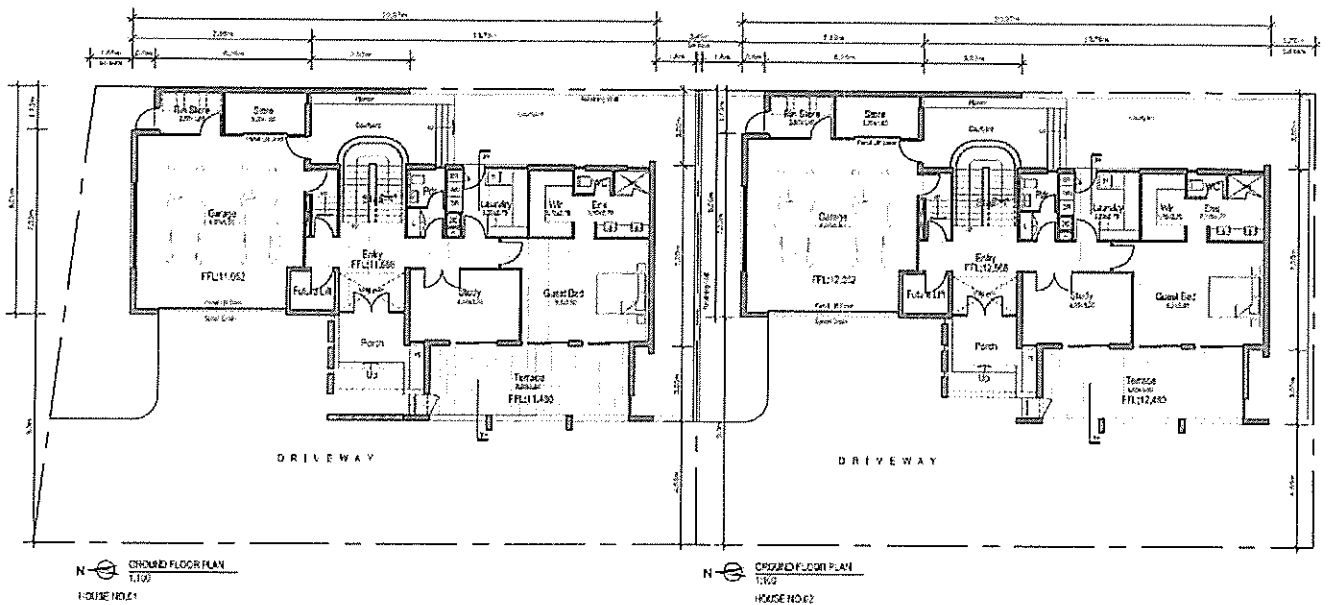
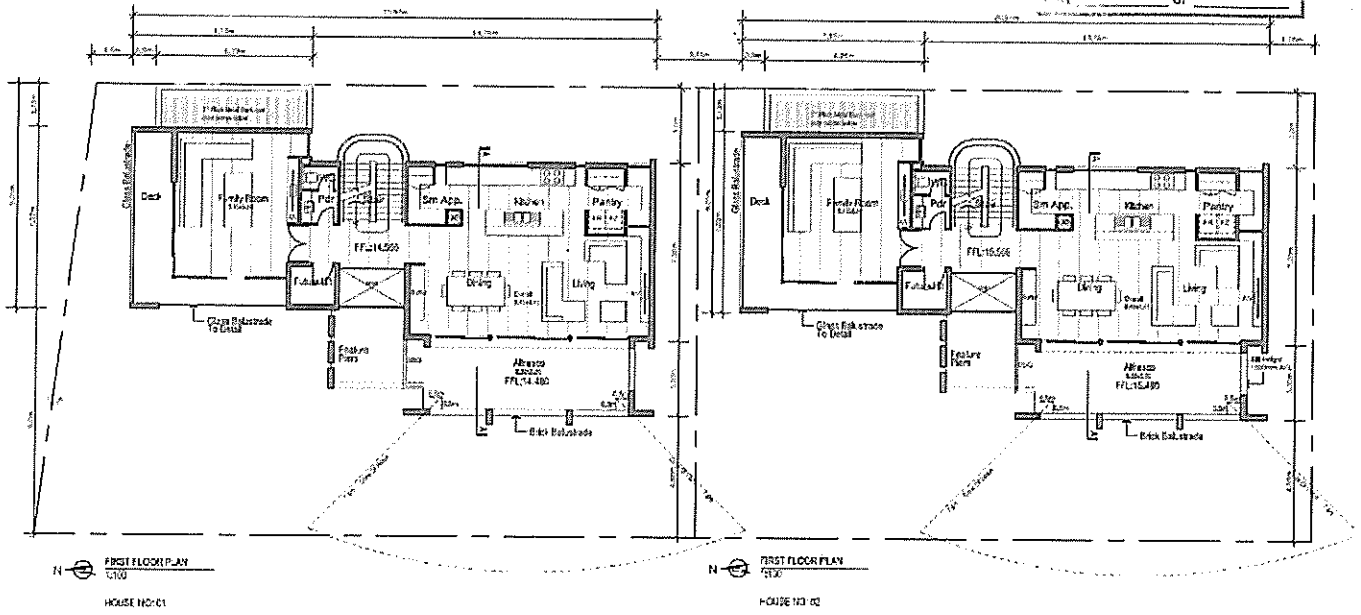
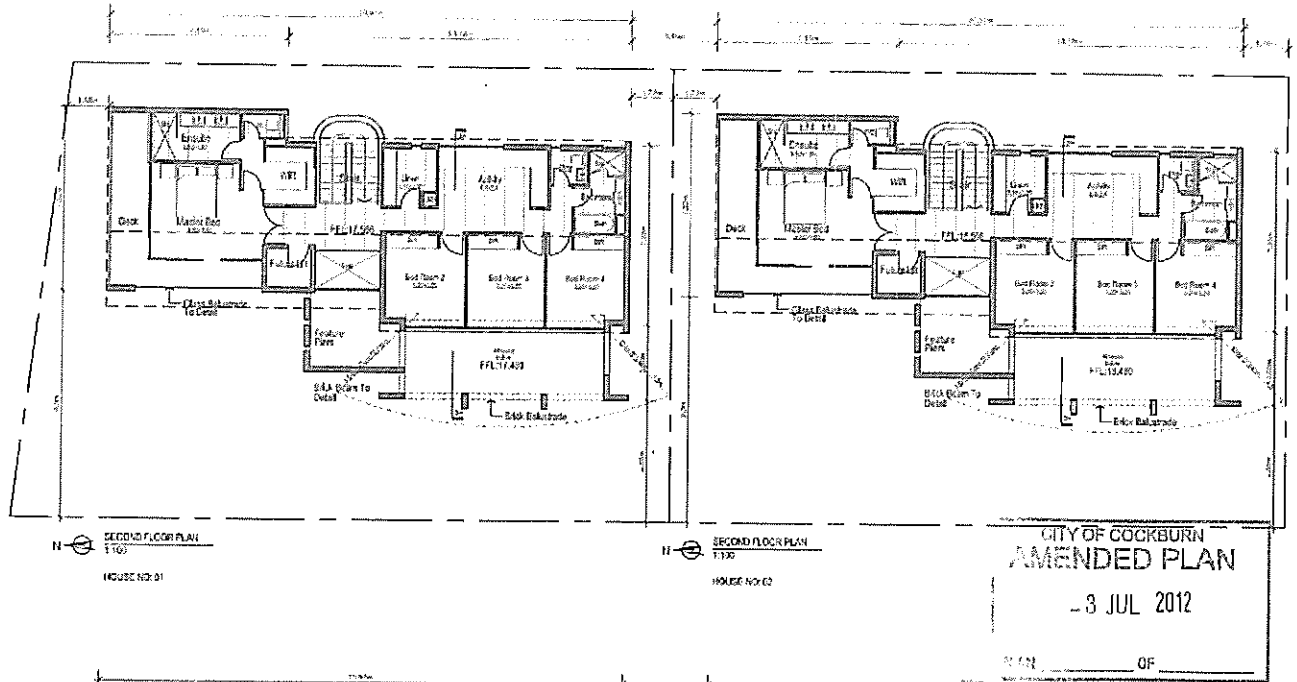
We the undersigned support additional doctors for the Hammond Park Medical Service. We do urgently require an afterhour's clinic on a seven day basis so that we could avail ourselves with medical service throughout the day. It is imperative that we have sufficient doctors servicing our needs

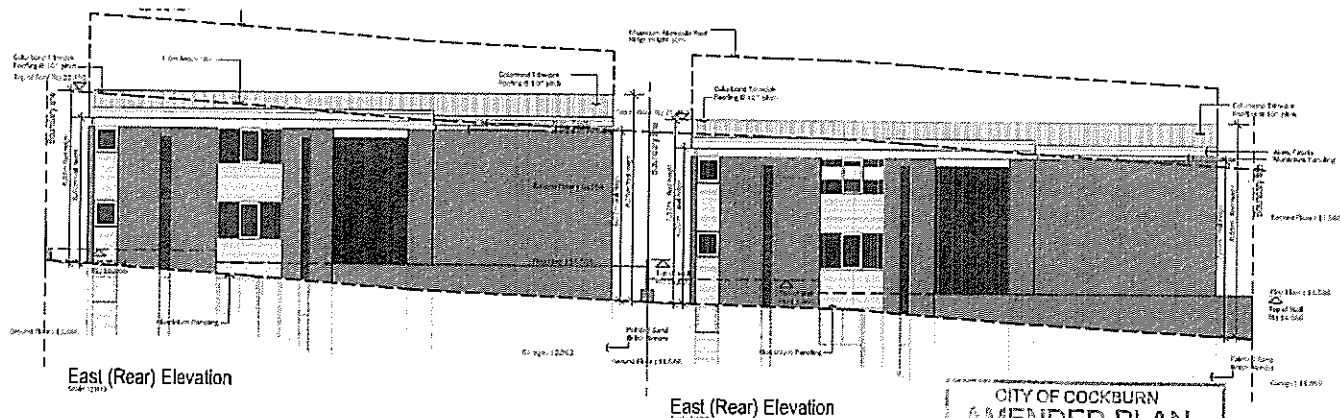
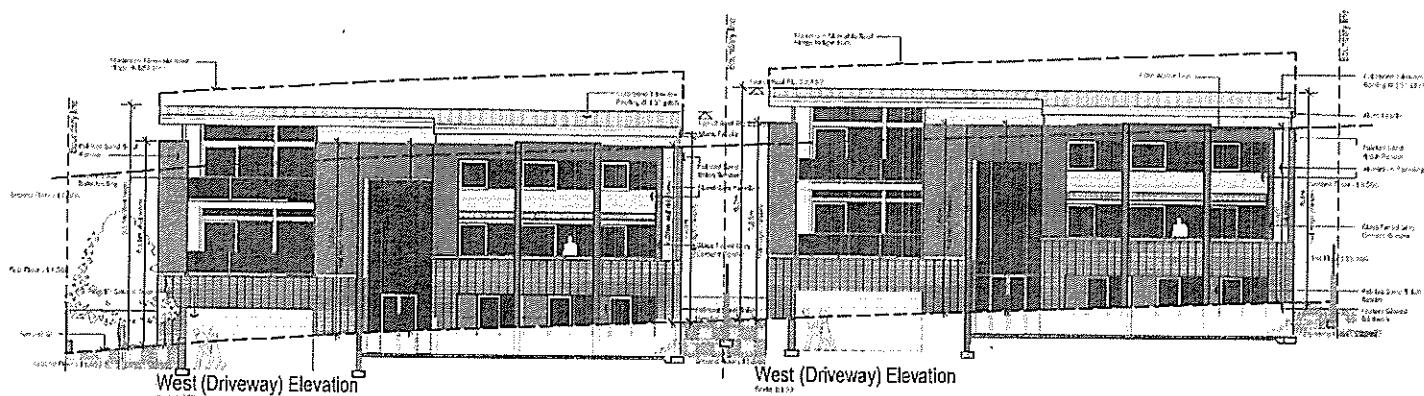
We have found no problem parking at all times at the shopping centre and some of us who live nearby walk to the Centre.

Name	Address	Signature	Telephone
Amanda Carter-Selwood	19 moonah Way Hammond park		94985282
NICHOLAS MATTIOLI	8 INNESS VISTA HAMMOND PARK		94987866
R Whitby	61 unity Way Atterd		9498775
Poonam Patel	16 Butchart Tera,		94994239,
Suzanne Smith	Aubin grove 16 swinburner way		0430044586
Jennifer Caradonna	27 barcoo bend gosaells hammond park.		041691843
JOHN OUTRAM	20 MOHAN LOOP HAMMOND PARK.		0428643384
Cathryn Franco	9 Oakridge Mads Success.		0414839566
Yvonne Carmichael Smith	8 Eastcott St Naroonna		044784366
PETER QUIGLEY	84 KANGAROO LOOP BIRCHMONT.		0488624169
Julieanne	Po Box 99 Inglewood		0411408530
Sarah Beeching	81 Barfield road Hammond park.		0406909063
Michelle Adelson	51 willerin Loop Success		0425545400
GAIL TOMICH	6 ARISTATA PDE AUBIN GROVE		94991171
Natalie Santich	21 Plumwood AVE Ham. PK		04986884
Charlotte Adams	135 Beechup Rd.		0408117243
Paula Bean	13 Alexandria Follow Success		0422125113
Tony MARTINIS	5 FAIRWAY CRES Meadow Str		0412.926.511

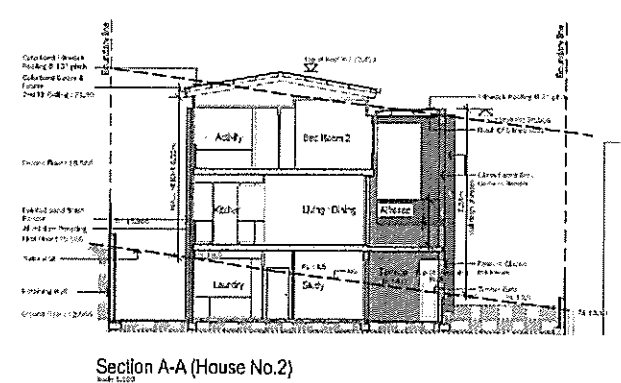
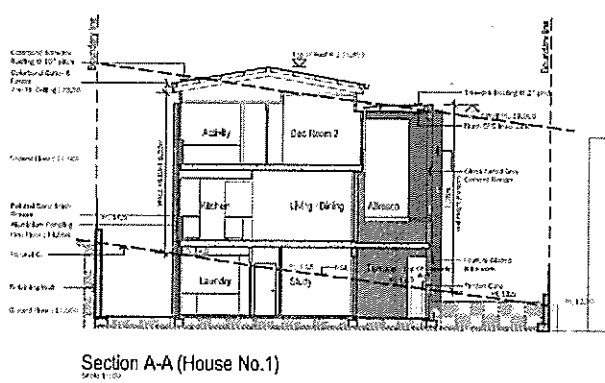
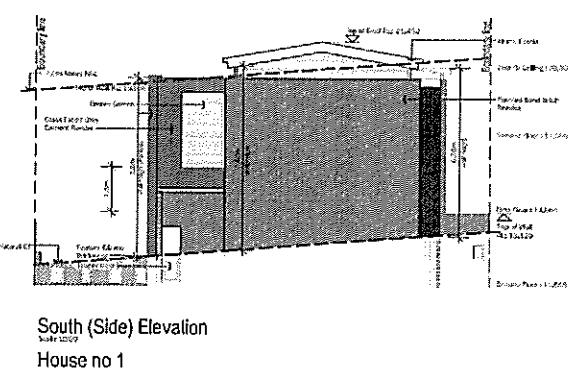
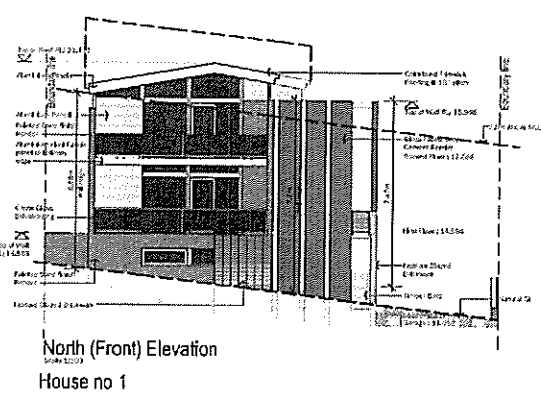


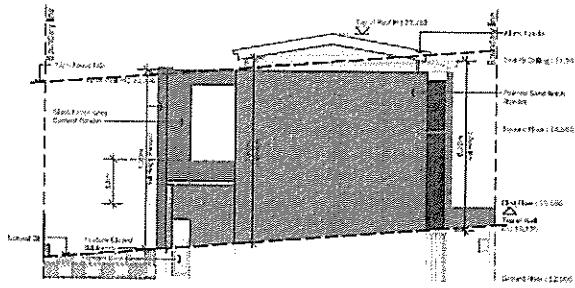
CITY OF COCKBURN
AMENDED PLAN
- 3 JUL 2012
PLAN _____ OF _____



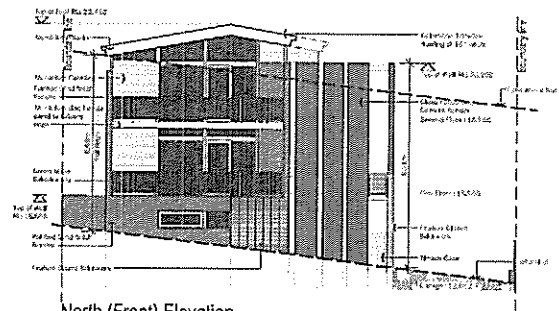


CITY OF COCKBURN
AMENDED PLAN
 - 3 JUL 2012
 PLAN _____ OF _____

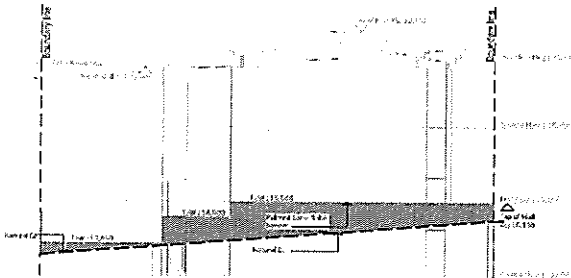




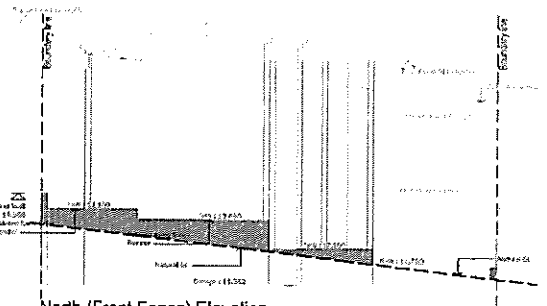
South (Side) Elevation
House no 2



North (Front) Elevation
House no 2



South (Rear Fence) Elevation
House no 2



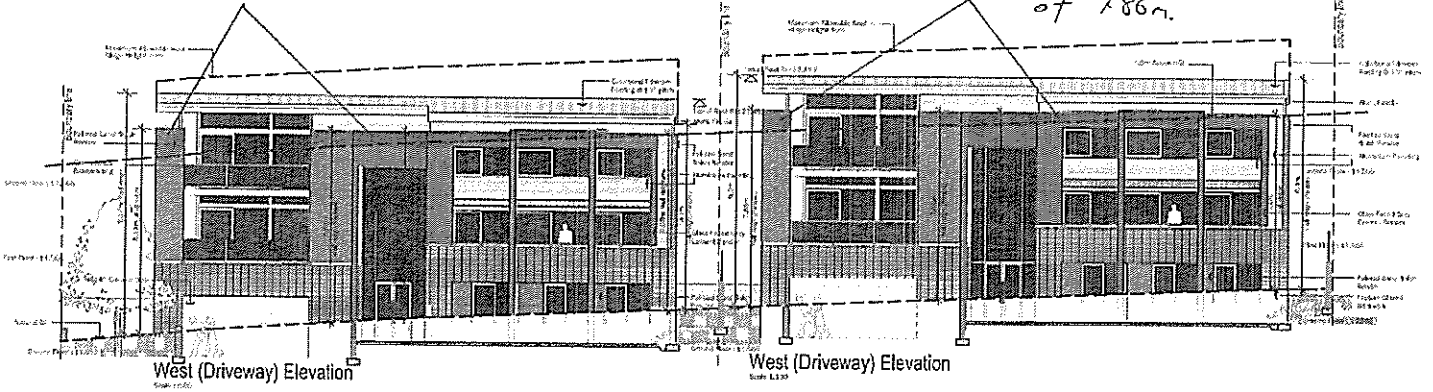
North (Front Fence) Elevation
House no 1

CITY OF COCKBURN
AMENDED PLAN
3 JUL 2012
PLAN _____ OF _____

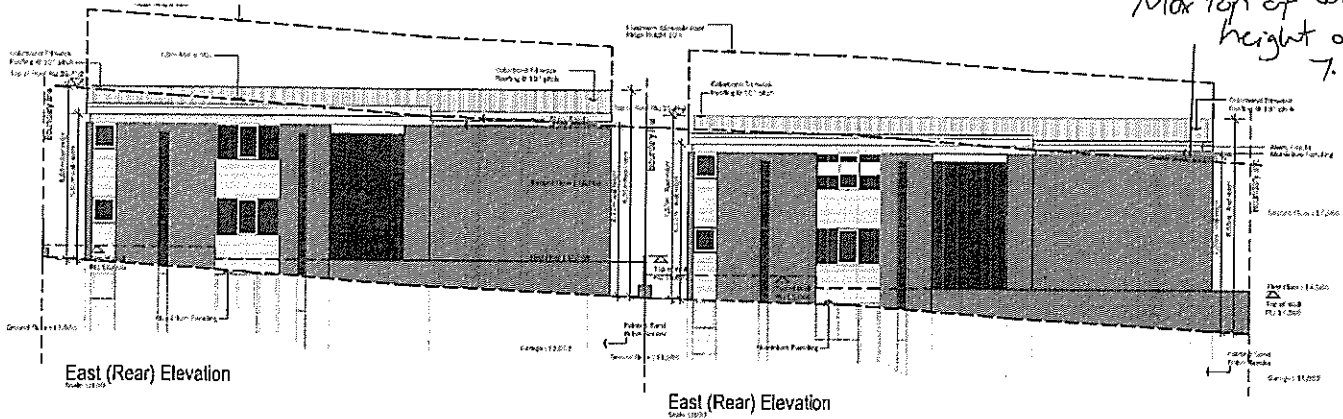
Variations to Height Limits

Max top of wall height of 8.14m.

Max top of wall height of 7.86m.

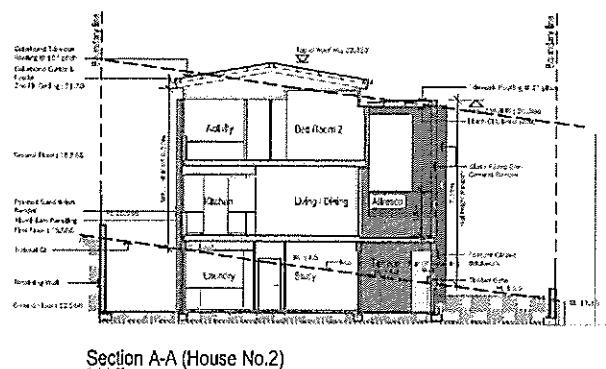
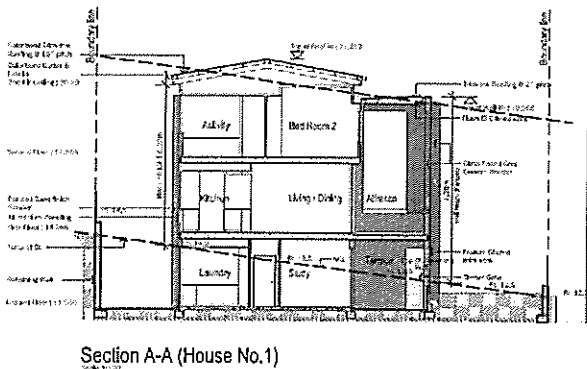
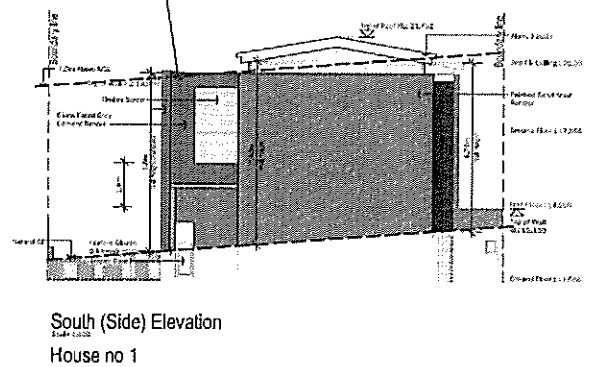
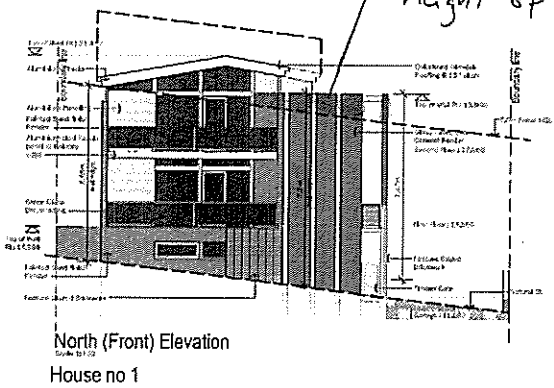


Max top of wall height of 7.1m.

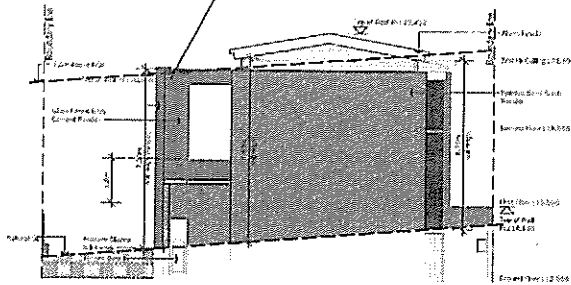


Max top of wall height of 8.14m.

Max top of wall height of 7.1m.

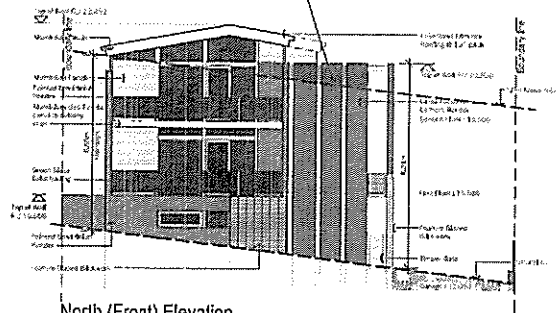


Max top of wall height
of 7.1m

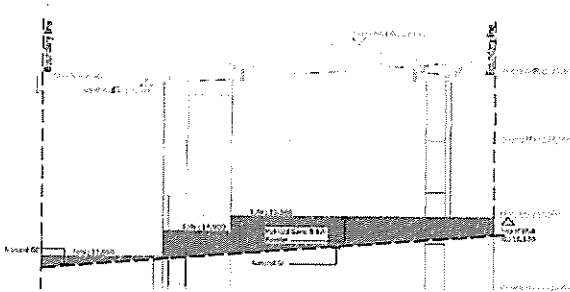


South (Side) Elevation
House no 2

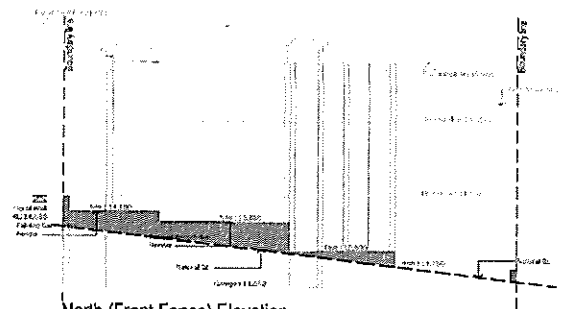
Max top of wall height
of 8.21 metres.



North (Front) Elevation
House no 2



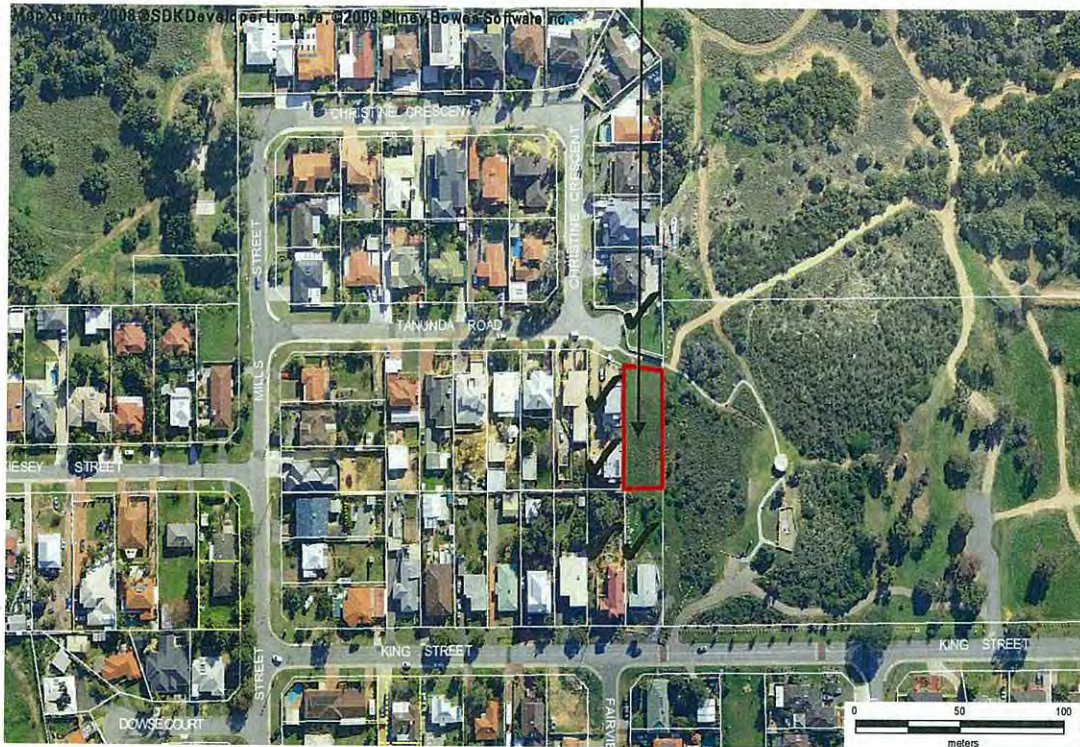
South (Rear Fence) Elevation
House no 2



North (Front Fence) Elevation
House no 1

Location Plan:

No. 19 (Lot 78) Tanunda Road, Coogee



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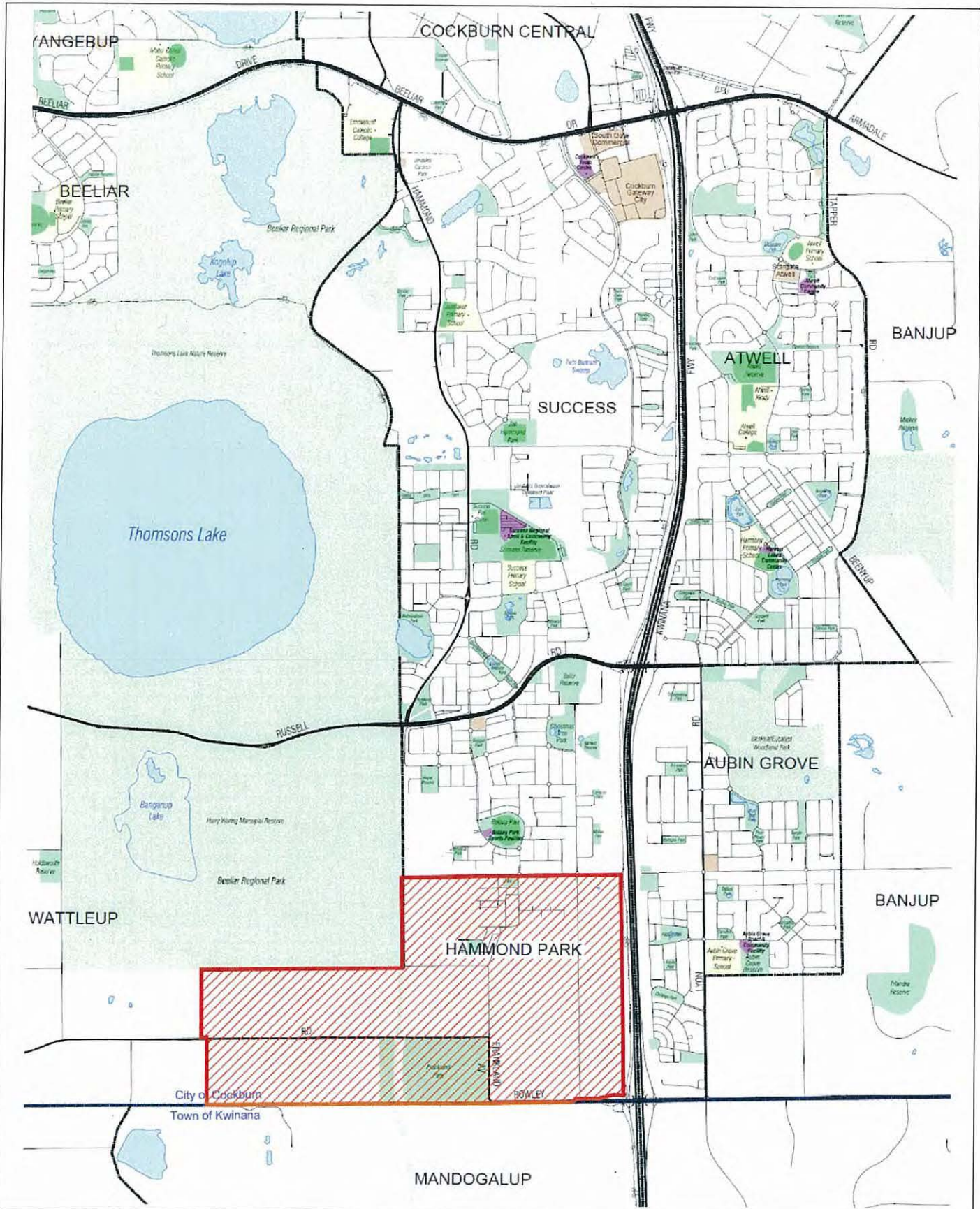
Properties Consulted



SCHEDULE OF SUBMISSIONS

PROPOSED TWO GROUPED DWELLINGS (THREE STOREY) – NO. 19 TANUNDA ROAD, COOGEE

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
1	Z Inman & J Winter 16 King Street Coogee WA 6164	<p>OBJECTION</p> <p>While it is understandable this proposal is within the absolute height limitations for planning, we are concerned this proposal is stretching the boundaries of the rules to the limit, setting a precedent for future development on battle axed blocks, in the area engulfing homes, and gardens of persons who wish to remain in the area compromising the current relaxed ambience.</p> <p>The impact of this proposal on immediate neighbours will be significant, however we presume the height could be higher, consequently in principal do not object. However the impact on the wider locality should also be considered, with the loss of views from the Rotary Lookout in King Street.</p> <p>Old Coogee has not been designed for this type of intense development. While we appreciate the planning difficulties with infill development, and blending newer style housing with older dwellings, there is a need for climate sensitive design and planning, fitting not only to the needs of new occupants, but protecting the amenity of the locality.</p> <p>Unfortunately there is real potential for this proposed type of housing to be built on every block in this neighbourhood, with each development scrambling for their piece of the view, resulting in permanent, changes to backyard microclimates (for those that desire to maintain their gardens), with only a momentary window of sun in winter. Currently there is</p>	<p>Noted. As identified in the submission, the proposed two (2) three-storey grouped dwellings are within the maximum top of roof pitch height allowed under the Coogee Residential Height Requirements Policy APD 53 of 10 metres, however exceeds the maximum top of wall (roof over) height allowed of 7 metres.</p> <p>The development is not setting a precedent for future similar developments as every new development is assessed on a case by case basis. With the proposed application, it demonstrates a development in keeping with the overall building height of the Tanunda Road streetscape from a performance point of view given the sloping and inclining gradient of the subject lot.</p> <p>In addition, the architectural style incorporated into the front dwelling facing Tanunda Road minimises the building visual bulk from the street and the proposed ground floor levels, via excavation not fill, reduce the overall height of the dwellings.</p> <p>The impact on the adjoining properties is considered minimal as the proposed development is compliant with overshadowing and visual privacy requirements of the R-Codes and during the</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>imminent development progress to the NW (17 Tanunda) and to the east (18 King), which if similar development to this proposal were to occur on these sites, our native water wise garden will become almost permanently in shade and gradually perish.</p> <p>I would argue Council to consider not this proposed development as a development individually, but more as what benchmark it is setting for the entire neighbourhood, after all Port Coogee residential areas (to date) has virtually no three storey residences.</p> <p>Should this proposal proceed then it is essential that council ensures no damage to the flora and fauna of the adjacent Rotary lookout occurs (as it is noted that parapet walls are to abut the reserve), and that the pedestrian pathway from Tanunda to the lookout is not obstructed and pedestrian access is maintained safely, especially considering builders access to this site will be limited.</p>	<p>consultation period only one (1) objection was received from the south-western property which does not directly abut the subject site.</p> <p>The proposed low roof pitched slate roofs have been designed in order to assist in reducing the building height and remain harmonious with the existing streetscape. Therefore given this and the above, the proposed development in regards to its height variation to the Coogee Residential Height Requirements Policy APD53 is deemed acceptable.</p> <p>In terms of the concerns regarding no damage to flora and fauna of the adjacent lookout and pedestrian pathway, any damage that may occur, if it does, will be the responsibility of the developers of the subject site to address. However, given the location of the dwellings, it seems unlikely that any damage to the flora and fauna as well as pedestrian pathway to the lookout will occur.</p>



 <p>City of Cockburn GIS Services Department</p>	<p>Location Plan</p>	<p>Southern Suburbs District Structure Plan (Stage 3) and Scheme Amendment No.28</p>	 <p>NORTH</p>
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PLAN NOTES

1. FUTURE RESIDENTIAL DEVELOPMENT SHALL NOT DIRECTLY ABUT ROWLEY ROAD. FUTURE LOCAL STRUCTURE PLANNING IS TO DEMONSTRATE A SUITABLE INTERFACE TREATMENT (E.G. LINEAR PUBLIC OPEN SPACE, SERVICE ROAD DESIGN WITH FRONTING RESIDENTIAL DEVELOPMENT) BEING PROVIDED TO THE FUTURE ROWLEY ROAD FREIGHT ACCESS ROUTE.

2. FUTURE ACCESS ROAD TO BE PROVIDED AS A FULL INTERSECTION UNTIL ROWLEY ROAD IS UPGRADED AND CONSTRUCTED TO A REGIONAL ROAD AT WHICH TIME THE INTERSECTION WILL BE CONVERTED AND MAINTAINED AS LEFT INLETT OUT ACCESS ONLY.

3. AS PART OF THE UPGRADING OF ROWLEY ROAD, GRADE SEPARATED PEDESTRIAN AND VEHICULAR ACCESS IS TO BE PROVIDED AS A CONTINUATION OF BARFIELD ROAD, IN ORDER TO MAINTAIN CONNECTIVITY BETWEEN FUTURE DEVELOPMENT TO THE SOUTH OF ROWLEY ROAD.

4. FUTURE STRUCTURE PLANNING OF THE CELL SOUTH OF WATTLEUP ROAD MUST PROVIDE AN APPROPRIATE INTERFACE WITH RESIDENTIAL DEVELOPMENT NORTH OF WATTLEUP ROAD. THIS IS TO HAVE PARTICULAR REGARD TO THE POSITION OF THE WESTERN AUSTRALIAN PLANNING COMMISSION IN RESPECT OF ITS POSITION ON THE ACCEPTABILITY (OR OTHERWISE) OF RESIDENTIAL DEVELOPMENT SOUTH OF WATTLEUP ROAD, AND ALTERNATIVE (NON-RESIDENTIAL) LAND USES THAT MAY BE REQUIRED ANY IMPACTS ASSOCIATED WITH THE FUTURE DEVELOPMENT OF NON-RESIDENTIAL LAND USES IN THE CELL SOUTH OF WATTLEUP ROAD MUST BE FULLY CONTAINED WITHIN THE CELL BOUNDARIES.

5. NEIGHBOURHOOD CENTRE - THE DESIGN AND FUNCTION OF THE PROPOSED NEIGHBOURHOOD CENTRE SHALL BE BASED ON 'MAIN STREET' PRINCIPLES AND RELEVANT PROVISIONS OF LIVEABLE NEIGHBOURHOODS. ANY ASSOCIATED LOCAL STRUCTURE PLAN MUST ADEQUATELY DEMONSTRATE THROUGH CONCEPT PLANS AND/OR DETAILED AREA PLANS THE MANNER IN WHICH THE CENTRE ADDRESSES THE REQUIREMENTS OF THE RELEVANT SECTIONS OF THE SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN - STAGE 3 REPORT, AND PARTICULARLY THE NEIGHBOURHOOD CENTRE CONCEPT PLAN PROVIDED WITHIN THE REPORT DOCUMENT.

6. NEIGHBOURHOOD NODES - THE DESIGN AND FUNCTIONALITY OF THE NEIGHBOURHOOD NODES SHALL BE DISTINCTLY DIFFERENT TO THE PRIMACY OF THE NEIGHBOURHOOD CENTRE. THESE LOCATIONS ARE TO PROVIDE FOR A RANGE OF MORE LOCALLY FOCUSED ACTIVITIES AND FUNCTIONS. WHERE RETAIL IS PROPOSED, THESE ARE TO NOT EXCEED A MAXIMUM RETAIL FUNCTION OF 500SQM, WITH SUCH FUNCTION BASED UPON A 'CONVENIENCE STORE' TYPE USE.

7. CENTRAL PRECINCT - A COMPREHENSIVE LOCAL STRUCTURE PLAN WILL BE REQUIRED FOR THE CENTRAL NEIGHBOURHOOD CENTRE AND ADJACENT DEVELOPMENT. THE CITY WILL NOT CONSIDER INDIVIDUAL STRUCTURE PLANS IN THIS LOCATION DUE TO THE NEED TO ENSURE THE SUITABLE INTEGRATION OF DEVELOPMENT.

C. PUBLIC OPEN SPACE AND DRAINAGE
PUBLIC OPEN SPACE AREAS ARE INDICATIVE ONLY AND SUBJECT TO DETAILED DESIGN AND MODELLING AT THE LOCAL STRUCTURE PLAN STAGE. ALL LOCAL STRUCTURE PLANS MUST ALSO INCORPORATE A LOCAL WATER MANAGEMENT STRATEGY TO DEMONSTRATE THE MANNER IN WHICH DRAINAGE CAN BE SELF CONTAINED WHILST TAKING INTO CONSIDERATION THE DISTRICT/REGIONAL DRAINAGE OBJECTIVES, AND ENSURING BEST PRACTICE WATER SENSITIVE URBAN DESIGN. THE FUNCTIONALITY OF OPEN SPACE FOR INFORMAL ACTIVE RECREATION MUST ALSO BE CONSIDERED AS PART OF FUTURE OPEN SPACE AND DRAINAGE CO-PLANNING.

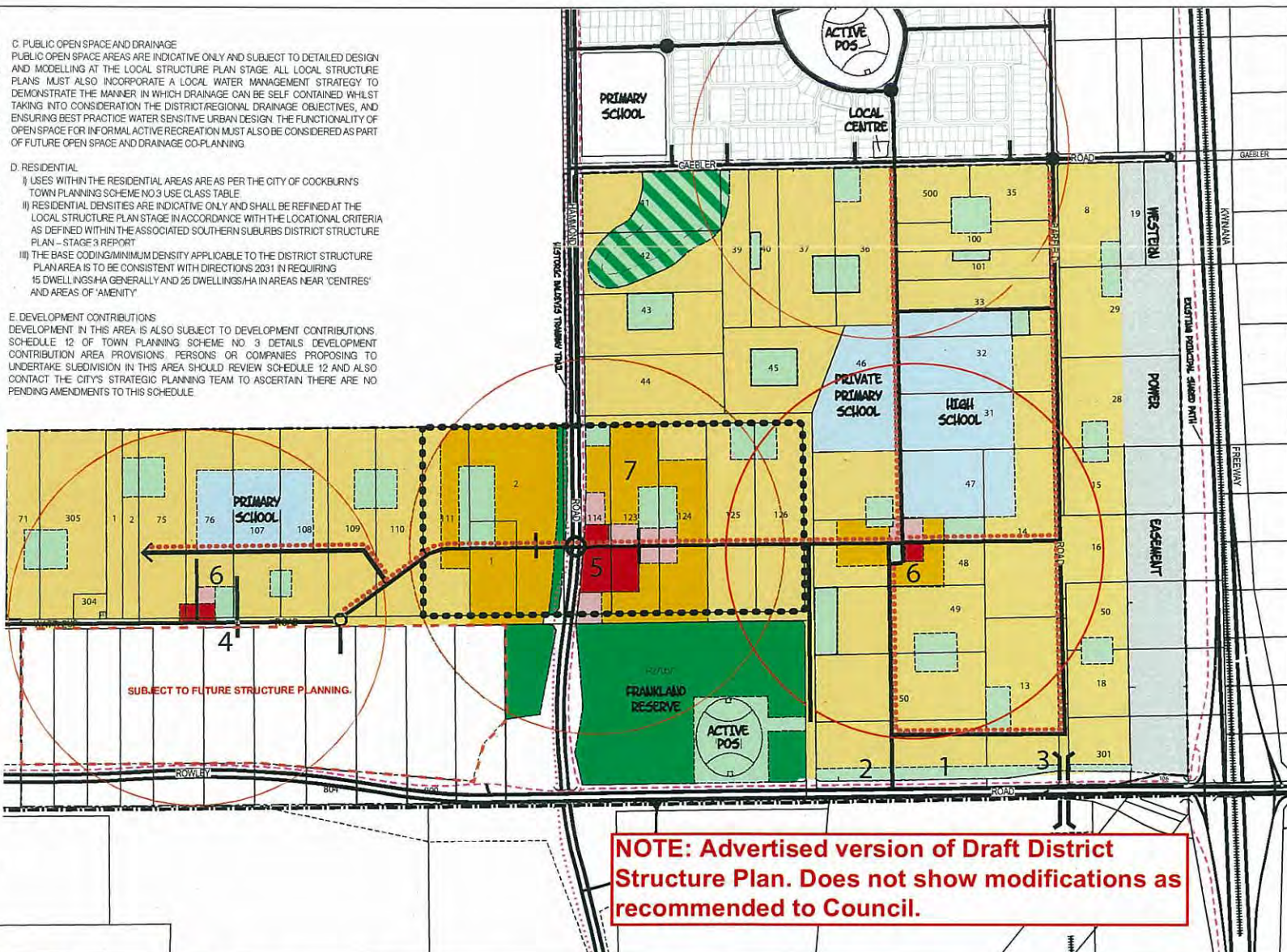
D. RESIDENTIAL
I) USES WITHIN THE RESIDENTIAL AREAS ARE AS PER THE CITY OF COCKBURN'S TOWN PLANNING SCHEME NO 3 USE CLASS TABLE.
II) RESIDENTIAL DENSITIES ARE INDICATIVE ONLY AND SHALL BE REFINED AT THE LOCAL STRUCTURE PLAN STAGE IN ACCORDANCE WITH THE LOGICAL CRITERIA AS DEFINED WITHIN THE ASSOCIATED SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN - STAGE 3 REPORT.
III) THE BASE CODING/MINIMUM DENSITY APPLICABLE TO THE DISTRICT STRUCTURE PLAN AREA IS TO BE CONSISTENT WITH DIRECTIONS 2031 IN REQUIRING 15 DWELLINGS/HA GENERALLY AND 25 DWELLINGS/HA IN AREAS NEAR 'CENTRES' AND AREAS OF 'AMENITY'.

E. DEVELOPMENT CONTRIBUTIONS
DEVELOPMENT IN THIS AREA IS ALSO SUBJECT TO DEVELOPMENT CONTRIBUTIONS SCHEDULE 12 OF TOWN PLANNING SCHEME NO 3 DETAILS. DEVELOPMENT CONTRIBUTION AREA PROVISIONS, PERSONS OR COMPANIES PROPOSING TO UNDERTAKE SUBDIVISION IN THIS AREA SHOULD REVIEW SCHEDULE 12 AND ALSO CONTACT THE CITY'S STRATEGIC PLANNING TEAM TO ASCERTAIN THERE ARE NO PENDING AMENDMENTS TO THIS SCHEDULE.

GENERAL NOTES

- A. ALL LOCAL STRUCTURE PLANS MUST INCLUDE AND BE INFORMED BY:
- i) DETAILED LWMs BASED UPON REGIONAL DRAINAGE STUDY,
 - ii) DETAILED NOISE MANAGEMENT STRATEGY WHERE LSP ADJAINS ROWLEY ROAD,
 - iii) FIRE MANAGEMENT PLAN WHERE LSP IS LOCATED NEAR ROS OR SIGNIFICANT POS,
 - iv) FLORA AND FAUNA MANAGEMENT PLAN,
 - v) TRAFFIC MANAGEMENT PLAN,
 - vi) CONTAMINATED SITES & ACID SULPHATE SOILS MANAGEMENT PLAN WHERE REQUIRED,
 - vii) HERITAGE STUDY WHERE LSP INCLUDES FORMER HISTORIC TRAMWAY,
 - viii) TRANSITION AND/OR INTERFACE STRATEGY IN RESPECT OF EXISTING RURAL USES,
 - ix) NEIGHBOURHOOD CENTRE CONCEPT PLAN AND DETAILED AREA PLAN WHERE INCLUDED WITHIN LSP AREA,
 - x) NEIGHBOURHOOD NODE CONCEPT PLAN AND DETAILED AREA PLAN.

B. LOCAL STRUCTURE PLANS
SUBDIVISION AND DEVELOPMENT OF THE SUBJECT AREA IS TO BE IN ACCORDANCE WITH AN ENDORSED APPLICABLE LOCAL STRUCTURE PLAN.



NOTE: Advertised version of Draft District Structure Plan. Does not show modifications as recommended to Council.

FIGURE 7



SCALE 1:12,500

Residential Density
 High
 Medium

Local Centre (Pedestrian Based Retail)
 Mixed Business / Commercial / Home Based Business
 Community Facilities / Schools etc
 Western Power Easement
 Conservation Category Wetland (In accordance with DEC Geomorphic Wetlands Swan Coastal Plain dataset)

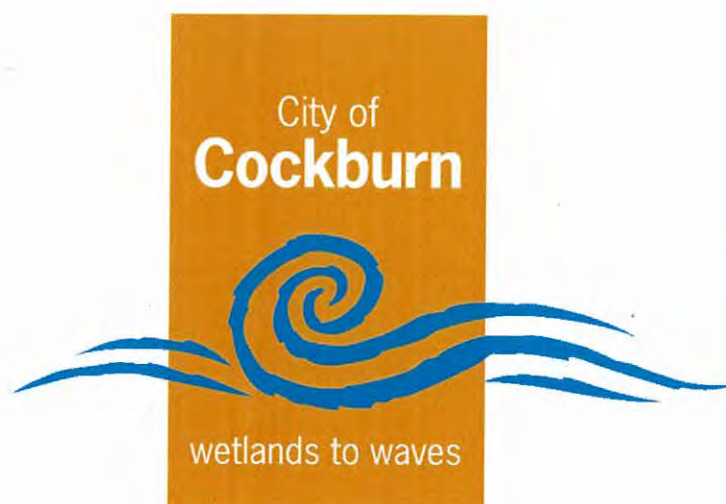
Key Open Space Areas:
 Public Open Space
 Regional Open Space

Key Road Structure (Existing and Proposed)
 Major Shared Path
 Historic Baldvis Tramway Trail
 400m Walkable Catchment
 Central Precinct

DRAFT

SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN - STAGE 3, HAMMOND PARK / WATTLEUP

Figure 7



DISTRICT STRUCTURE PLAN

Southern Suburbs Stage 3

Prepared by: City of Cockburn Strategic Planning

August 2012

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SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN STAGE 3

PART ONE - BACKGROUND

1.0 INTRODUCTION

1.1 Background

The area within the Southern Suburbs District Structure Plan Stage 3 ("SSDSP3") relates to the area of land on the western side of the Kwinana Freeway, south of Gaebler Road to Rowley Road, Hammond Park and from the Freeway west along Wattleup Road and is the last remaining large greenfield area to be developed in the City of the Cockburn.

The SSDSP3 was originally adopted by the Council in 2005, however since that time, a number of factors have led to the need for the City to undertake a review of the SSDSP3. The review aims to ensure that it is consistent with all current government strategic planning initiatives.

Specifically, the Public Transport Authority ("PTA") has confirmed the location of Mandogalup Train Station south of Rowley Road, which differs from its location on the original plan, and the Department of Planning & Infrastructure ("DoP") has confirmed that Rowley Road is identified as a prospective strategic freight route, which has implications for proposed future land uses in this area. The Western Australian Planning Commission ("WAPC") also resolved to extend the Kwinana Industrial (including Air Quality) Buffer by an additional 0.5 kilometres which means it now extends into the SSDSP3 area. This has also been the subject of a recent State Administrative Tribunal appeal, which has resulted in advice being given that the buffer issue needs further examination.

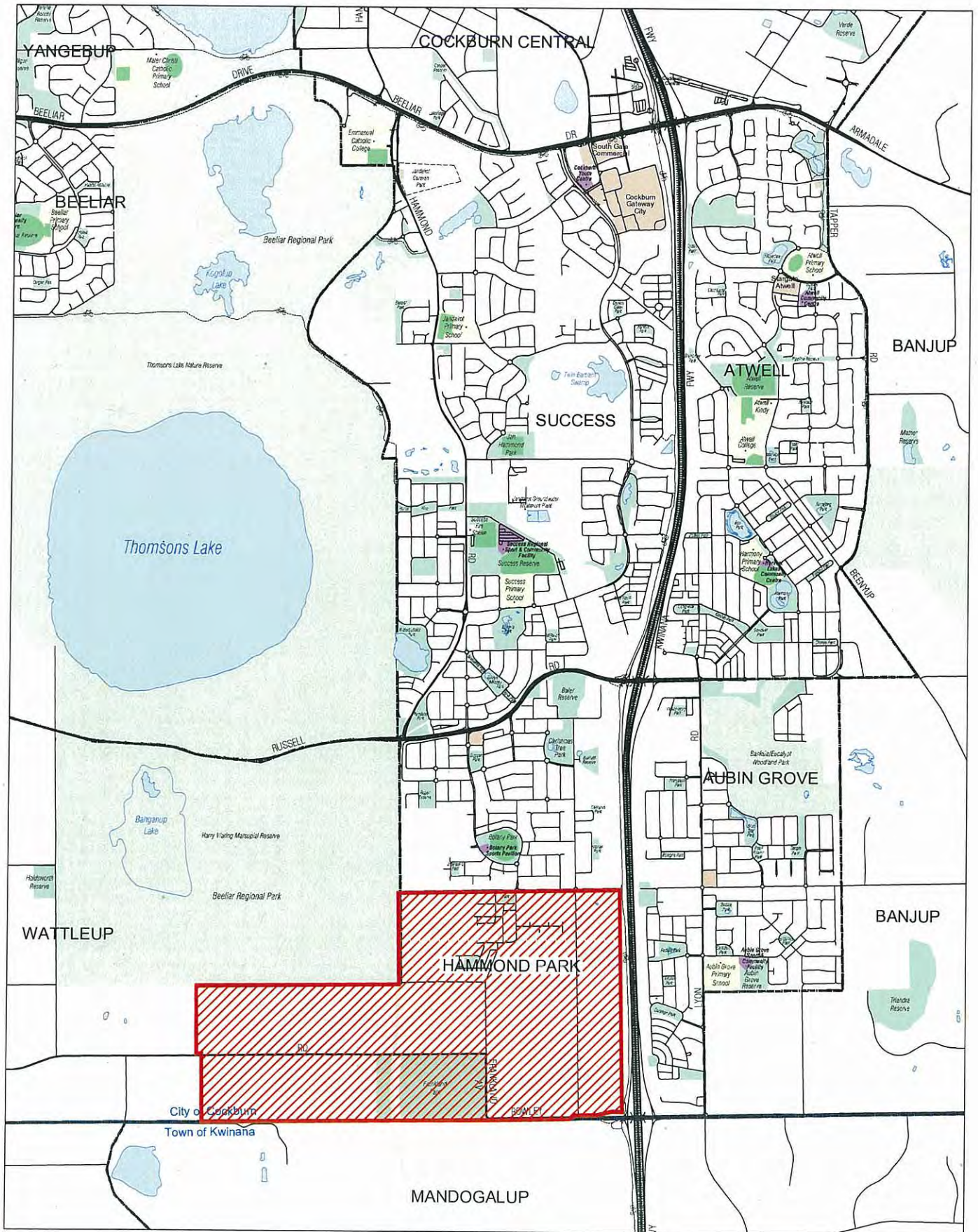
These and other similarly important factors have all been comprehensively reviewed and integrated into a revised SSDSP3.

1.2 Purpose

The SSDSP3 shows the broad land use framework including the major road network, neighbourhood structure, commercial, education, community and significant open space areas. It will form the basis of co-ordinating and considering Local Structure Plans ("LSP'S") and plans of subdivision to be prepared by landowners in the area.

1.3 Planning Area

The SSDSP3 covers the urban area of approximately 234 hectares bounded by Gaebler Road to the north, Kwinana Freeway to the east, Rowley Road to the south and extending westwards along Wattleup Road. The extent of the area is shown on **Figure 1 – Site Location**. The surrounding areas to the subject land are a mix of residential, extractive industry and rural uses (proposed for future urban) and a conservation reserve (Harry Waring Marsupial Reserve).



LOCATION
SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN
- STAGE 3, HAMMOND PARK

City of Cockburn
 wetlands to waves

FIGURE 1

1.4 Land Ownership

Since the SSDSP3 was originally adopted in 2005, there have been some lots which have changed ownership and a small degree of land assembly by developer groups. The majority of lots are however still in private individual ownership.

1.5 Adoption of the Southern Suburbs District Structure Plan – Stage 3

It is proposed to adopt the SSDSP3 by resolution of Council as a “guiding document”. It is not adopted pursuant to Part 6 of TPS No. 3 and therefore does not implement zoning or apply land uses.

The SSDSP3 gives guidance to the assessment of LSP’s, which will be advertised, considered and determined pursuant to Part 6 of TPS No. 3.

2.0 STATUTORY AND STRATEGIC PLANNING CONSIDERATIONS

2.1 Metropolitan Region Scheme

The SSDSP3 area is generally zoned ‘Urban’ under the Metropolitan Region Scheme (“MRS”) as shown on **Figure 2 – Metropolitan Region Scheme Zoning**. Pursuant to the 2005 endorsed Southern Suburbs District Structure Plan, the WAPC lifted the deferment of the ‘Urban’ zone over the Structure Plan area in October 2008.

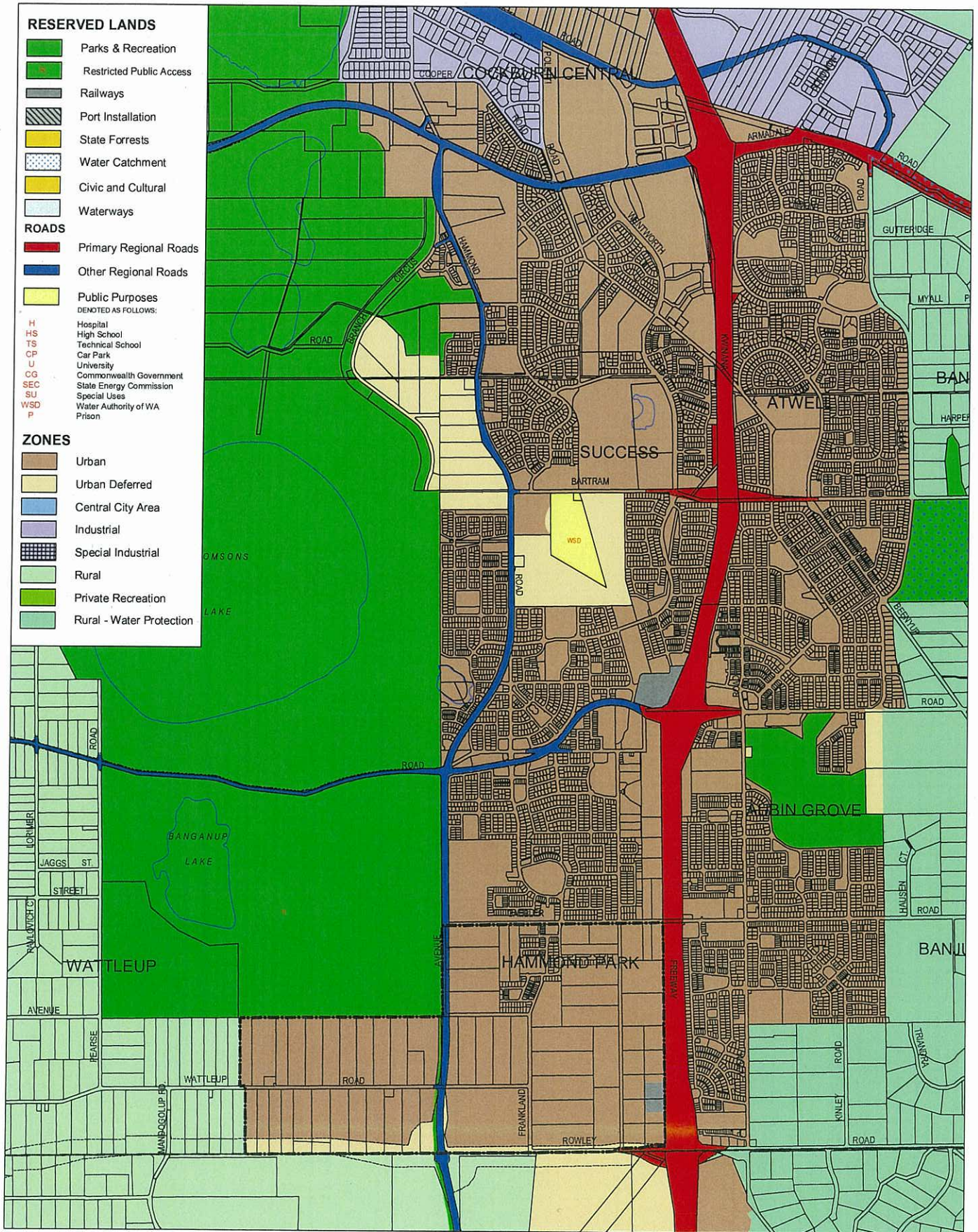
The Frankland Avenue/Hammond Road extension is reserved as an ‘Other Regional Road’ under the MRS. Rowley Road abuts the southern boundary of the SSDSP area, adjoining the western boundary of the Structure Plan area, and its alignment and future widenings sit within the ‘Urban Deferred’ zone. A portion in the southeast of the SSDSP area is reserved for ‘Railways’, as the site was originally earmarked for parking for the train station, which is now to be located further south.

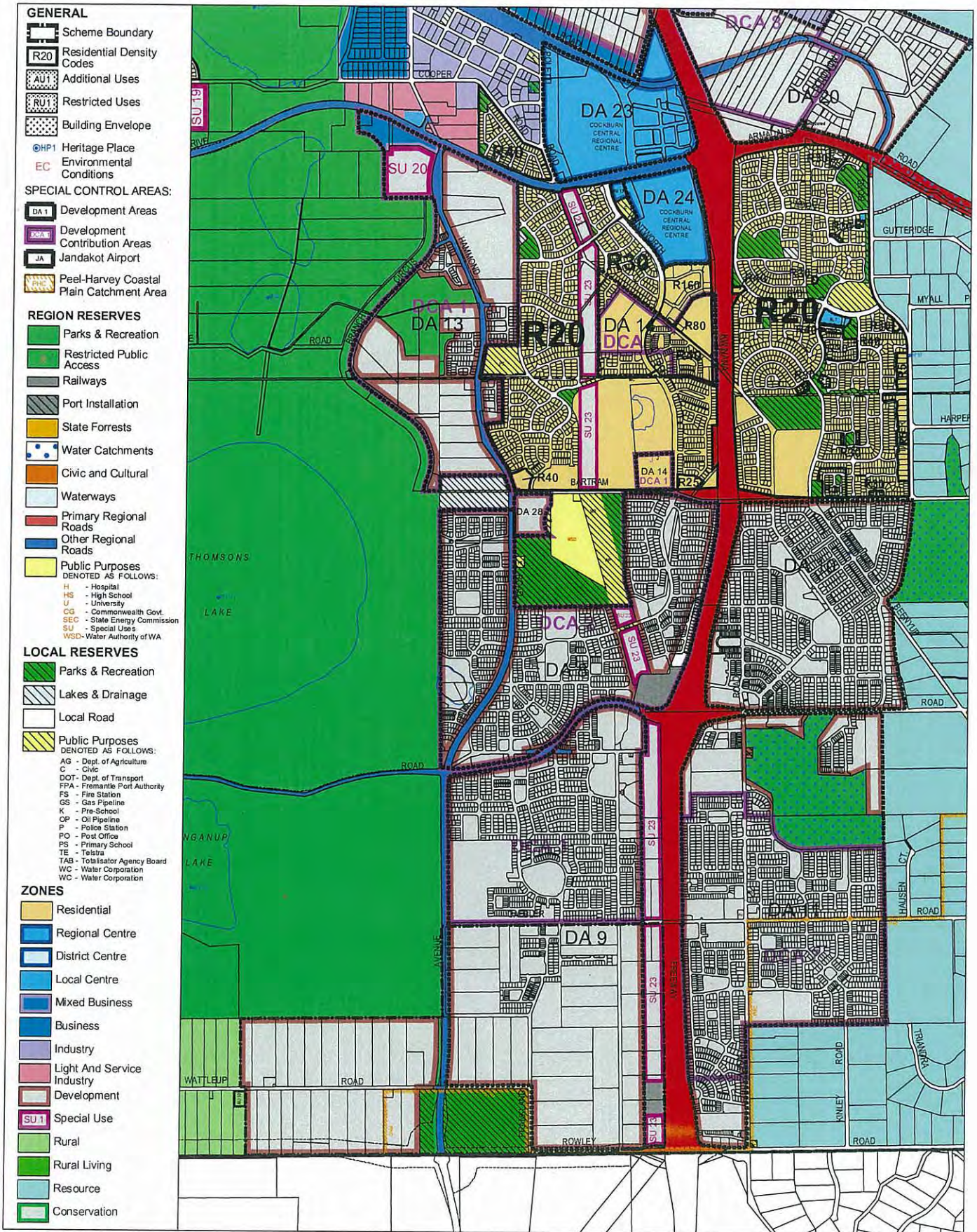
There is a strip of ‘Parks and Recreation’ west of Hammond Road between the southern boundary of Harry Waring Marsupial Reserve and Rowley Road, which corresponds to the Historic Baldivis Tramway Trail. Approximately 24.3 hectares adjoining the southern boundary is Reserved for ‘Parks and Recreation’, which generally corresponds to the location of Frankland Reserve.

2.2 City of Cockburn Town Planning Scheme No. 3

Under the City of Cockburn TPS No. 3 the land is zoned ‘Development’ with subdivision and development to be in accordance with LSP’s adopted pursuant to Section 6.2 of the Scheme. The eastern portion of the area is currently referred to as Development Area No.9 in Town Planning Scheme No.3 No. 3 as shown within **Figure 3 – Town Planning Scheme No. 3 Zoning**.

Amendment No 28 to TPS No 3 initiated by Council at its meeting held on 15th February 2005 proposes to nominate the area as Development Areas 26 and 27 with appropriate conditions in Schedule 11 and Development Contribution Areas 9 and 10 to formalise development contributions to common infrastructure through the requirements of Schedule 12. Endorsement of this amendment by the WAPC is still outstanding.





TOWN PLANNING SCHEME No.3
 SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN
 - STAGE 3, HAMMOND PARK

City of
Cockburn
 wetlands to waves

FIGURE 3

2.3 Directions 2031 and Draft Outer Metropolitan Perth and Peel Sub-Regional Strategy

Directions 2031 and Beyond: Metropolitan Planning Beyond the Horizon ("Directions 2031") was adopted by the WAPC in August 2010 and is the latest spatial planning framework for Perth and Peel and outlines the planning vision and direction which will guide the planning of the City to 2031 and beyond.

Directions 2031 recognises the benefits of a more consolidated city, which include;

- o A reduced overall need for travel;
- o Supports the use of public transport, cycling and walking for access to services, facilities and employment; and
- o A more energy efficient urban form.

Directions 2031 aims to provide for different lifestyle choices, vibrant nodes for economic and social activity and a more sustainable urban transport network. It states the role and function of centres will vary depending on their catchment, centres should generally:

- o provide services, employment and activities that are appropriate for and accessible to the communities they support;
- o be integrated with and encourage the efficient operation of the transport network, with particular emphasis on promoting public transport, walking and cycling and reducing the number and length of trips;
- o be designed based on transit oriented development principles;
- o provide opportunities as places to live through higher density housing and the development of social and cultural networks;
- o encourage the agglomeration of economic activity and cultivation of business synergies; and
- o support the development of local identity and sense of place.

Directions 2031 seeks a 50 per cent improvement on current infill residential development trends of 30 and 35 per cent; and, has set a target of 47 per cent or 154,000 of the required 328,000 dwellings as infill development. This translates to 11,100 new dwellings as part of infill/redevelopment opportunities within the City of Cockburn.

Directions 2031 seeks a 50 per cent increase in the current average residential density of 10 dwellings per gross urban zoned hectare; and, has set a target of 15 dwellings per gross urban zoned hectare of land in new development areas. This translates to 18,280 new dwellings as part of greenfield development opportunities within the City of Cockburn.

The Draft Outer Metropolitan Perth and Peel Sub-Regional Strategy ("Draft Strategy") forms an integral part of Directions 2031. The Draft Strategy along with its counterpart for Central Metropolitan Perth provides the strategic spatial plan which will achieve the objectives of Direction 2031. The Draft Strategy identifies the subject area as an 'Urban Zone Undeveloped' area and classifies it as 'SOU1', with the potential to deliver 3000+ dwellings in the future.

2.4 State Planning Policy 4.2 – Activity Centres for Perth and Peel

State Planning Policy 4.2 – Activity Centres for Perth and Peel ("SPP 4.2") replaced the WAPC's Metropolitan Centres Policy. The main purpose of SPP 4.2 is to specify broad requirements for the planning and development of new activity centres and the redevelopment and renewal of existing centres in Perth and Peel.

It is mainly concerned with the distribution, function, broad land use and urban design criteria of activity centres, and with coordinating their land use and infrastructure planning.

Other purposes of the policy include the integration of activity centres with public transport; ensuring they contain a range of activities to promote community benefits through infrastructure efficiency and economic benefits of business clusters; and lower transport energy use and associated carbon emissions. The policy also reflects the WAPC intention to encourage and consolidate residential and commercial development in activity centres so that they contribute to a balanced network.

Within the City of Cockburn, SPP 4.2 identifies Cockburn Central as a 'Secondary Centre' and Phoenix Park as a 'District Centre'. SPP 4.2 does not identify Neighborhood or Local Centres, rather these centres are designated through the structure planning process.

2.5 City of Cockburn Draft Local Commercial Activity Centre Strategy

In 2011, Pracsys prepared a Draft Local Commercial Activity Centre Strategy ("LCACS") for the City of Cockburn consistent with the requirements of SPP4.2. The Draft LCACS was adopted for the purposes of advertising by Council at its December 2011 Ordinary Meeting. Reflecting the revised policy context in which it was developed, the Draft LCACS is not a traditional local commercial strategy. The previous focus of local commercial strategies upon commercial development and the application of floorspace caps have been completely reinvented through the new State planning framework.

The Draft LCACS's assessment framework responds to both the scale of the proposal and the importance of the particular centre. The Draft LCACS outlines the minimum reporting requirements of a proposal and seeks to provide both the City and proponents for development within the City's activity centres with a clear shared understanding of the expectations flowing from the Draft LCACS.

In respect to the subject area, the Draft LCACS identifies scope for a 'Hammond Park Local Centre'. The role of the future local centre is identified as being for *"daily and some weekly household chopping needs, and a very small range of other convenience stores"*.

2.6 City of Cockburn Local Planning Strategy

The SSDSP3 is consistent with the following actions that are identified within the City of Cockburn Local Planning Strategy:

- o promote higher density and mixed use developments to reduce car use and promote cycling, walking and public transport;
- o ensure there is an appropriate housing and density mix to fulfill existing and potential demand from various groups;
- o promote medium and high density housing in and near regional and district centres and near public transport facilities;
- o provide a range of housing opportunities; and
- o promote mixed land uses in communities, especially through the location of housing in commercial centres.

2.7 Southern Suburbs District Structure Plan (October 1999)

In October 1999, the City and the WAPC adopted the Southern Suburbs District Structure Plan for that part of the newly developing urban area north of Gaebler Road west of the Freeway and north of Gibbs Road east of the Freeway. This plan has been the basis of coordinating local structure plans and subdivision proposals for Frankland Springs Estate, Hammond Park (Australand), Success Lakes, Success (Gold Estates), Harvest Lakes Estate, Atwell (LandCorp) and Atwell Waters, Atwell (Peet & Co). The plan showed a notional neighbourhood structure over the Stage 3 area and noted that this was to be the subject of further detailed investigations and planning.

Through the subsequent process of preparing local structure plans and detailed plans of subdivision for land north of Gaebler Road there have been minor departures from the District Structure Plan. However, these do not adversely impact or constrain the Stage 3 area.

2.8 Environmental Protection

There are no lakes within the study area which are listed as Environmental Protection Swan Coastal Plains Policy Lakes ("EPP Lakes").

There is one wetland within the SSDSP3 which is classified as a 'Conservation Category Wetland' in accordance with the Department of Environment and Conservation ("DEC") Geomorphic Wetlands Swan Coastal Plain dataset. The wetland is located to the south east of the intersection of Gaebler Road and Frankland Avenue (Hammond Road).

Proposed LSP's will need to ensure these issues are investigated and managed in accordance with relevant government guidance documents, including

- o Position Statement No. 4 – Environmental Protection of Wetlands (EPA 2004)
- o Environmental Protection (Kwinana)(Atmospheric Wastes) Policy 1999

2.9 Cockburn Sound Catchment Area

The SSDSP3 area is located within the Cockburn Sound Catchment Area which extends to the Kwinana Freeway and as far north as Beeliar Drive. The Cockburn Sound Management Council ("CSMC") was established in August 2000 to facilitate coordination of environmental management and planning of Cockburn Sound and its catchment.

In 2005 the State Environmental (Cockburn Sound) Policy 2005 was released by the Minister for the Environment after extensive scientific and public consultation. The Policy authorises the Cockburn Sound Management Council to report annually on the 'State of the Sound' and have this report tabled in Parliament.

An Environmental Management Plan, prepared by the CSMC, outlines on-ground actions for implementing the Policy, and establishes the particular roles and responsibilities of managers and user groups.

In August 2003, the local authorities including the City of Cockburn signed a Memorandum of Understanding to ensure the mutual and coordinated effort in the management and protection of the Cockburn Sound catchment area. This resulted in the preparation of a Local Planning Policy (Cockburn Sound Catchment

Policy) which aims to ensure the protection of the marine waters of Cockburn Sound from nutrient contamination (particularly nitrogen) from diffuse land sources.

The policy links the objectives of the Environmental Management Plan with State and Local Government to provide a consistent and unified approach to ensure planning and management decisions by Local Government within the catchment do not result in unsustainable additional nutrient loading or contamination of surface or groundwater resources.

2.10 Bush Forever

In December 2000, the WAPC published Bush Forever as a 10 year strategic plan to protect regionally significant bushland of at least 10% of each of the original 26 vegetation complexes of the Swan Coastal Plain portion of the Perth Metropolitan Region. Bush Forever replaces the System 6 recommendations for the Swan Coastal Plain portion of the Perth Metropolitan Region. In June 2010 the WAPC released a State of Planning Policy 2.8 - Bushland Policy for the Perth Metropolitan Region ("SPP 2.8").

Whilst there are no sites identified within the SSDSP3 area for Bush Forever, Harry Warring Reserve which is located to the northwest of the subject area is included as a Bush Forever site. Given existing market garden, vacant land and Frankland Avenue separates the subject area from Harry Warring Reserve, future development created by the SSDSP3 is not expected to impact on the Bush Forever site. However careful consideration will need to be given as part of future structure plans to ensure an appropriate interface with the reserve.

In addition to the sites outlined in the Bush Forever report of December 2000, the Bush Forever office is examining additional sites that were identified for inclusion. These additional areas include Frankland Reserve. At its meeting held in September 2003 Council resolved to advise the WAPC that it supported the inclusion of Frankland Reserve as a Bush Forever site, subject to a degraded area in the eastern part of the reserve being available for the development of active sporting facilities and consideration being given to realigning Hammond Road and Rowley Road to minimise the impact on Frankland Reserve.

The realignment of Hammond Road was not supported by the WAPC and the Other Regional Road Reserve remains in its current alignment. No further amendments to SSP2.8 have occurred at this stage and as such Frankland Reserve is not currently identified as a Bush Forever site.

The City is not aware of any other land within the structure plan area that is currently under consideration for inclusion as a Bush Forever site.

2.11 Strategic Plan for Perth's Greenways

In December 1998 the Ministry for Planning released the final report on the Strategic Plan for Perth's Greenways. The report provides background information on the value of Greenways. At a strategic level the Greenway plan complements Perth's Bush Forever and is intended to act as a catalyst for the development of plans identifying strategic linkages between large conservation areas by providing corridors of vegetation to connect these sites and promote the importance of protecting and managing these greenways.

The historic Baldvis tramway trail which is down the western boundary of Hammond Road reserve is Greenway link 78. The link is formalised as a strip of Parks and Recreation which links Frankland Reserve to Harry Warring and Thomsons Lake.

2.12 City of Cockburn Greening Plan

The City's Greening Plan adopted in June 2001 is a long-term strategic plan for the maintenance and enhancement of remnant vegetation, the revegetation of previously cleared areas, road reserves, public land and the enhancement of ecological landscape, streetscape values and community amenity within the City of Cockburn.

The Greening Plan proposes the Baldvis tramway trail as a green link whilst the Kwinana Freeway is identified as a link that requires rehabilitation by the State Government to create a bushland corridor with a distinct Cockburn character. The landscaping theme proposed for major roads including Hammond, Gaebler and Barfield Roads is wetland whilst fruit trees are proposed for Wattleup Road. Rowley Road was not proposed at that time. Any construction of Rowley Road as a freight route should maximise retention of remnant vegetation where possible.

3.0 SITE CONTEXT AND ANALYSIS

3.1 Physical Description

The structure plan area is located in the central part of the Swan Coastal Plain within the Bassendean Dune System which consists of low hills of leached siliceous sands interspersed with sand flats and seasonal wetlands. Regional soil mapping shows the upland areas comprise deep Bassendean sands, generally light grey at the surface and yellow at depth which are well drained and of aeolian origin. The low lying areas at the northern boundary consist of shallow Bassendean sands over silts and clays of the Guildford formation of variable thickness. The water table is generally higher in these areas and often prone to inundation.

The area in the vicinity of Gaebler Road and the northern portion of Barfield Road is generally flatter and includes a conservation category wetland (dampland) in the vicinity of Gaebler Road. To the south and extending westwards along Wattleup Road the land is more elevated and undulating with levels ranging between RL30m AHD and RL49m AHD falling to flatter land at the western boundary at RL20m AHD to 23m AHD. Slopes within the elevated central portion range between 10 and 12%.

Vegetation within the structure plan area is of the Bassendean Vegetation Complex - central and south which is described as ranging from Woodlands of Jarrah-Sheoak- Banksia on the sand dunes to low woodland of melaleuca species and sedge land on the low lying depressions and swamps (**refer to Figure 4 – Aerial Photography**).

The broad information contained in the David Wills Russell Road Arterial Drainage Scheme report shows the 5 year average minimum ground water levels being 21m AHD at Barfield Road, 18m AHD at Hammond Road and 12m AHD at the western extremity along Wattleup Road. Accordingly the lower land to the immediate south of Gaebler Road, west of Barfield and west of Hammond Road



FIGURE 4



AERIAL PHOTO

SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN - STAGE 3, HAMMOND PARK

FIGURE 4

City of
Cockburn



wedlands to waves

just south of Harry Warring Reserve will require filling to achieve minimum separation to the proposed controlled ground water levels.

Based on the City's general experience, it is possible that some of the lower lying areas or depressions throughout the structure plan area could contain peaty or clayey materials. In these areas detailed geotechnical investigations will need to be undertaken prior to subdivision and or development and appropriate remediation works completed as part of the development works.

3.2 Existing Land Uses

In the past the subject land was zoned rural and some 35% of the land was cleared and used for a variety of rural pursuits.

With the exception of some intensive market garden operations along Gaebler Road, most of the eastern portion of the structure plan area is either vacant land or has been developed for rural living purposes. Along the southern side of Wattleup Road the majority of the land is used for intensive agricultural and horticultural purposes including a poultry farm whilst there are a number of market gardens and turf farms to the north of Wattleup Road (**refer to Figure 5 – Existing Land Uses**).

Most of the lots have been developed with residences and outbuildings associated with the rural activities. Many of the houses are relatively new, in good condition and are to be retained as part of the subdivision of the land. Some buildings are beyond their economic life and will be demolished as part of the subdivision. A decision on the future of any improvements on the land can be made at the time of preparing local structure plans and detailed plans of subdivision.

3.3 Adjoining Land Uses

Land to the north of Gaebler Road is zoned Urban in the MRS and Development in City of Cockburn TPS No.3. LSP's and detailed plans of subdivision have been approved and the vast majority of the area has now been developed for housing. Planning for SSDSP3 acknowledges these approvals.

Land south of the structure plan area within the Town of Kwinana is a mixture of natural bushland, sand quarries and packing/storage facility as shown on the existing Land Use plan. There is also a communications tower near the corner of Rowley Road and Frankland Avenue. These activities pose no constraints to development given their nature and separation from the urban area by the wide reserve for Rowley Road which traverses the southern boundary of the structure plan area.

3.4 Site Contamination and Uncontrolled Fill

Whilst there are no known contaminated sites in the area, there is the potential that land could be contaminated by harmful substances including pesticides as a result of past agricultural and horticultural activities. Accordingly, development and subdivision of those areas will be the subject of a Soil Contamination Assessment and if areas are identified it will be necessary for these to be remediated to recognised health standards determined by the DEC.

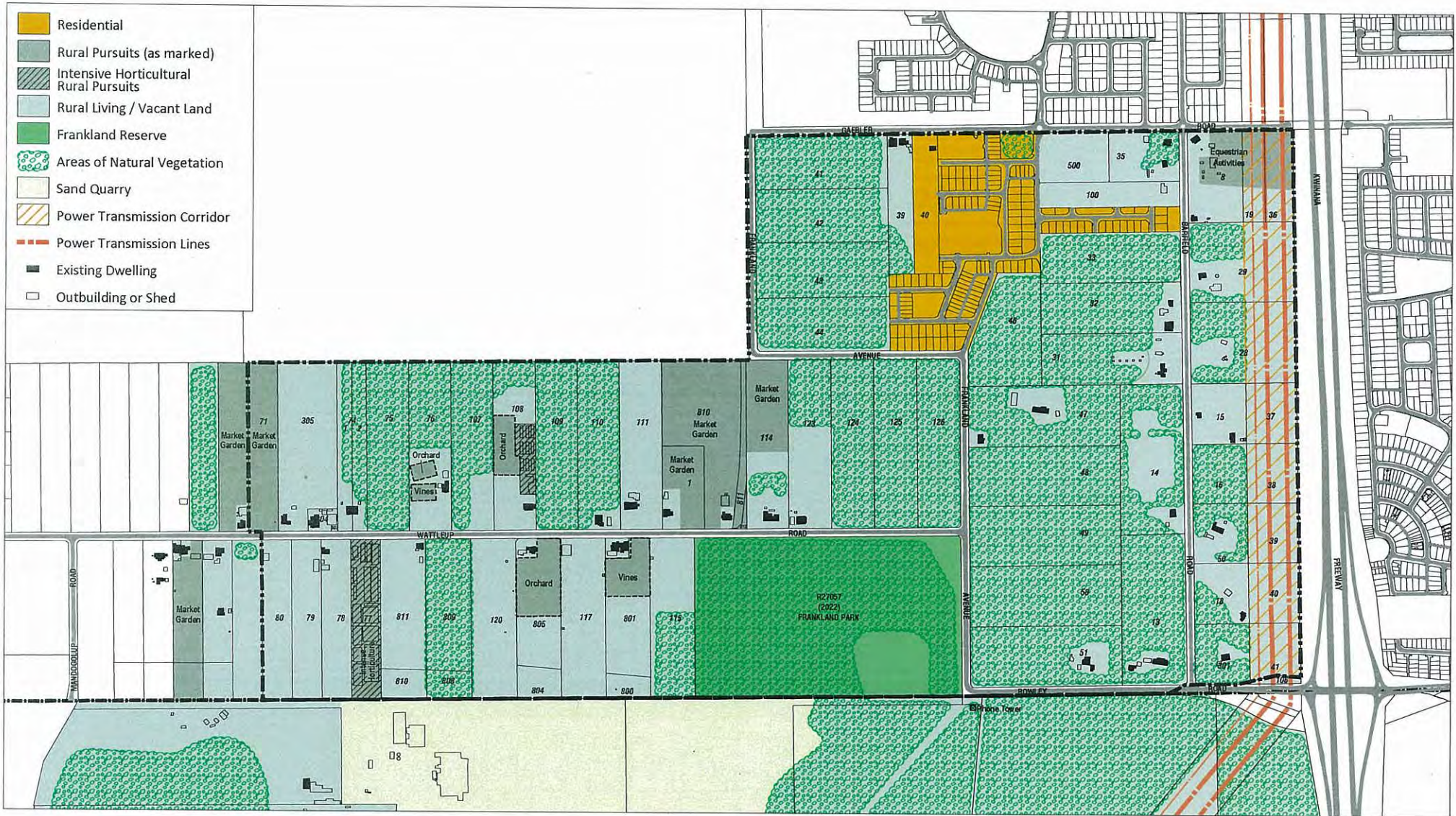


FIGURE 5

EXISTING LAND USE

SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN - STAGE 3, HAMMOND PARK

FIGURE 5

City of
Cockburn



wetlands to waves

Likewise, it was not uncommon for low lying areas within the rural zone to be filled with both appropriate and inappropriate material and often over peat or clay areas. Accordingly, a geotechnical report will be required at the time of subdivision to confirm the suitability of these areas for residential purposes.

3.5 Sites of Indigenous Heritage Significance

A desktop review through the Department of Indigenous and Affairs' ("DIA") Aboriginal Heritage Inquiry System, found no archaeological or ethnographical Aboriginal Sites listed in the study area.

It is possible that sites that have not yet been listed on the Register System may exist. The Aboriginal Heritage Act 1972 protects all Aboriginal sites in Western Australia whether they are known to the DIA or not.

As part of an environmental assessment of land in Banjup (1997), McDonald Hales & Associates conducted a desktop survey of Aboriginal sites within a 3 km radius just west of Lyon Road. Nine recorded Aboriginal sites occur within this radius. Seven of these are archaeological sites which are mostly artefact scatters but one is believed to be a kangaroo pit or trap. The remaining two are ethnographic sites which pertain to wetlands and are reported to have mythological association. There are known sites of significance on the west side of Barfield Road to the north of the study area and at Wattleup Swamp which is on the north side of Wattleup Road on rural land to the west.

The high number of reported heritage sites within the surrounding area suggests a potential for sites being located within the study area, particularly as it is located on the Bassendean sands. An archaeological and ethnographic survey of the study area prior to any development may identify sites which have not yet been recorded. This issue should be addressed by individual proponents in consultation with DIA.

3.6 Sites of European Heritage Significance

The historic Baldivis tramway trail runs along the western boundary of Frankland Avenue/Hammond Road reserve. The tramway was provided to service the grouped settlement at Baldivis following the Second World War, acknowledging the need to ensure cheap access to employment and other services.

The link is formalised as a strip of MRS Parks and Recreation reserve which links Frankland Reserve to Harry Warring and Thomsons Lake. The reserve lies over privately owned land.

The infrastructure associated with the tramway has long been removed, or overgrown. In some instances, evidence of tramway sleepers have been found. In recent times, the approach taken to the conservation and interpretation of this trail has been to respect its integrity and to not reduce or modify the existing reserve.

3.7 Wetlands

A dampland lies within the north west portion of the SSDSP3 area, located on Lots 39 and 41 Gaebler Road and Lot 42 Frankland Avenue and is identified as a CCW under the DEC's Geomorphic Wetlands dataset

The CCW was subject to a wetland classification review in 2010 and the DEC determined that the CCW is a fully functioning wetland and resolved to retain its CCW classification. Careful consideration needs to be given to the wetland at the LSP stage to ensure that subdivision and drainage impacts are minimised and appropriate ongoing management measures are implemented.

3.8 Implications for Urban Development

Implications for the subdivision and development of the area for residential and associated uses as a result of previous or existing land use activities are as follows:

- Some land will need to be filled to achieve adequate vertical clearances to the high groundwater levels.
- Geotechnical investigations will need to be undertaken on areas that have been subject to uncontrolled fill and the lower areas to substantiate suitable ground conditions.
- Completion of soil contamination surveys will need to be undertaken on land previously used for agricultural and horticultural purposes to ensure its suitability for residential purposes.
- An archaeological and ethnographic survey of the study area prior to any development may identify sites of Indigenous Heritage which have not yet been recorded. This issue should be addressed by individual proponents in consultation with DIA.

These requirements are normal within the City of Cockburn and do not affect the suitability of the area for urban development but rather represent matters that need to be addressed at the detailed planning and development phase.

4.0 DEVELOPMENT OPPORTUNITIES AND CONSTRAINTS

The previous sections have identified a number of opportunities and constraints that have an influence on the planning of the structure plan area. These together with other factors are shown within **Figure 6 - Opportunities & Constraints** map and described in the following section.

4.1 Buffers

The following buffers affect the area:

4.1.1 Market Gardens and Turf Farms

The DEC's general recommendation is that residential development should not be within 500 metres of a market gardens or turf farms unless the developer can demonstrate that specific circumstances justify a lesser buffer. This position is based on Environmental Protection Authority's ("EPA") Guidance Statement No. 3 on the Separation Distances between Industrial and Sensitive Land Uses ("GS No.3").

GS No.3 is intended to provide advice on generic separation distances between specific industry and sensitive land uses to avoid or minimise the potential for land use conflict. The recommended 500 metre buffer distance is not intended to be an absolute separation distance, rather a default distance for the purposes of:

- identifying the need for specific separation distance or buffer definition studies; and

- o providing general guidance on separation distances in the absence of site specific technical studies.

The DEC will also consider management options such as retention of a vegetated buffer, construction of a separation wall and Memorials on Titles to mitigate against negative impacts from turf farms and market gardens.

The WAPC's State Planning Policy 4.1 - State Industrial Buffer ("SPP 4.1") states that proposals satisfying the recommended buffer distances set out in the EPA's GS No. 3 are deemed to comply with the objectives of the policy.

However, the WAPC's Planning Bulletin 63 - Policy for Dealing with Potential Conflicts Between Residential Subdivision and Market Gardens in East Wanneroo ("PB63"), requires only specially designed fencing, together with a protected and maintained vegetation buffer of one line of evergreen trees or bushes (minimum 1.5 metres high) for residential lots abutting a market garden, for land zoned Urban or Urban Deferred in the MRS.

There appears to be a large discrepancy between the EPA's GS No. 3 and the WAPC's SPP 4.1 and the approach taken by the WAPC in PB63 regarding appropriate setbacks between residential development and market gardens or turf farms. As the EPA's GS No.3 and WAPC's SPP 4.1 are higher order policy documents, they must be used to guide buffer distances to any existing market gardens and turf farms within the SSDSP3 area.

Existing market gardens are shown on the Opportunities and Constraints plan and associated buffers will be determined on a case by case basis at the local structure plan and subdivision phases.

4.1.2 Kwinana Environmental Protection (Atmospheric Wastes) Policy

The Kwinana Environmental Protection (Atmospheric Wastes) Policy – Area B marginally affects the extreme south western corner of the SSDSP3. The affected area is within the portion of the SSDSP3 which is subject to future structure planning due to the implications of the Alcoa residue disposal area buffer detailed in the following section. As the area affected is very minor, it is envisaged that this would be accommodated within a road reserve as part of future structure planning and subdivision of that land should it occur.

4.1.3 Alcoa Residue Storage Area

In 2010 as part of the broader Kwinana Industrial Buffer review process, and based on advice from the Department of Health ("DoH") and Department of Environment and Conservation, the WAPC resolved to extend the buffer surrounding Alcoa's Mandogalup Residue Disposal Area.

The expanded buffer includes a 1 kilometre area where future residential expansion is restricted (within the Town of Kwinana), with an additional 0.5 kilometres beyond this (into Wattleup and Hammond Park) also restricting residential expansion and development of some other sensitive land uses (i.e. kindergartens, hospitals and aged persons housing). The purpose of the additional buffer area was to recognise uncertainties related to the level and frequency of impacts from the Alcoa Residue Disposal Area and Kwinana Industrial Area more broadly. The WAPC have stated that the extended portion of the buffer will be reviewed in five years time to ensure that its extent and location reflects the current conditions in the area.

4.2 Acid Sulfate Soils

The WAPC's Planning Bulletin No. 64 – Acid Sulfate Soils ("PB No. 64") sets out the issues relating to Acid Sulfate Soils ("ASS") and provides guidance on the matter. Maps forming part of PB No. 64 show the eastern portion of the SSDSP3 area is of moderate to low risk and the western portion being of low to no risk. Three isolated high risk areas are shown being along the southern side of Gaebler Road and north western corner of the area along Wattleup Road.

Proposals for subdivision and development in the areas of high and moderate to low risk will need to address this issue in detail as part of the local structure planning and subdivision processes. Investigations will be required where subdivision works involve removing organic soils, dewatering and or soil disturbance immediately above or below the groundwater table.

4.3 Peel-Harvey Drainage Coastal Plain Catchment Area

The northern limit of the Peel-Harvey Catchment Area abuts the southern boundary of the SSDSP3 area. As the catchment area does not encroach within the SSDSP3 area, there are no implications for the subject land.

4.4 Fire Risk

Harry Waring Reserve and Frankland Reserve are considered important conservation reserves. Like all conservation reserves, they pose a fire risk and accordingly residential development should be separated from the reserve boundaries by a subdivision road or similar separating interface. The use of a perimeter road (public street) and public open space areas are the preferred interface treatments. Well designed, these can significantly reduce the risk of bush fire hazard.

The use of a perimeter road and/or public open space areas can also provide opportunities for better land management of the reserves and to minimise potential issues and problems associated with illegal dumping and anti-social behaviour by maximising passive surveillance.

Fire management plans will need to be prepared at the LSP stage where proposed development is located adjacent to regional open space, significant public open space or other densely vegetated areas.

4.5 Elevated Land

The elevated land particularly in the western portion of the structure plan area provides the opportunity for innovative designs which capture internal and external views. It is important to build upon these opportunities through the LSP process when designing road layouts, neighbourhood cells and public open space areas.

4.6 Open Space Opportunities

Public parkland used by people living or working in urban areas can significantly promote quality of life and amenity. Regional, district and local open space

(neighbourhood parks) provided for in structure planning can contribute towards legibility, identity and sense of place that helps build community.

Almost two thirds of the SSDSP3 area is vacant or remnant undeveloped land which provides opportunity to strategically locate public open space areas within the design. Given that some of this land is vacant and cleared, this provides the ability to develop open space areas without the need for extensive clearing of existing bushland and vegetation.

There are well located areas of good quality bushland within the structure plan area that are ideally suited for incorporation in neighbourhood parks. This will minimise the need for landscaping, provide a high level of visual amenity, protect additional areas of high quality vegetation and reduce the use of ground water for irrigation.

4.7 Power Transmission Corridor

A Western Power transmission corridor containing three steel pylon high voltage lines is located down the eastern side of the SSDSP3 area adjacent to the Kwinana Freeway. Some of the land is owned by Western Power whilst some remains in private and State Government agency ownership.

It is understood that the transmission corridor accommodates all easement requirements associated with the power lines and there are no restrictions on development outside of the corridor area.

Development of the corridor area is restricted in accordance with Western Power requirements. It should be noted that the City will not accept any of the corridor land as part of the normal 10% public open space contribution as its use is severely restricted.

4.8 Urban Water Management

The guiding document for drainage within the SSDSP3 area is the Russell Road Arterial Drainage Scheme report prepared by David Wills and Associates in December 2002 (**refer to Appendix 1 for full report**). This report was based on data from existing boreholes near the SSDSP3 area.

Consistent with the requirements of the DoP's Better Urban Water Management ("BUWM") and the objectives of State Planning Policy 2.9 - Water Resources ("SPP 2.9"), landowners/developers will need to prepare Local Water Management Strategy's ("LWMS") and Urban Water Management Plan's ("UWMP") as part of structure planning and subdivision proposals for their land. The Russell Road Arterial Drainage Scheme report will form the basis for future LSP's and LWMS's.

The Russell Road Arterial Drainage Scheme report demonstrates that the SSDSP3 area is capable of supporting urban development and able to achieve appropriate urban water management outcomes.

4.8.1 Water Management Objectives

Existing LWMS's approved within the SSDSP3 area demonstrate best practice integrated urban water management ("IUWM") and water sensitive urban design ("WSUD"). Future LWMS's will be required to achieve these same objectives and

criteria. The principles and objectives of best practice IUWM and WSUD that guides LWMS's are drawn from the following documents:

- o Stormwater Management Manual for Western Australia (DoW, 2007);
- o Liveable Neighbourhoods Edition 4 (WAPC, 2007); and
- o Better Urban Water Management (WAPC, 2008).

4.9 Freight Corridor

In August 2011, the WAPC established a Planning Control Area for the future extension of Rowley Road west from the Kwinana Freeway. This was to protect the road corridor pending its inclusion as a Primary Regional Roads Reserve under the MRS as it is currently zoned 'Urban Deferred'. Given that the upgrading of Rowley Road to a freight corridor standard will occur sometime in the future, it is necessary to plan for an appropriate interface to urban development within the SSDSP3.

The WAPC's State Planning Policy 5.4 - Road and Rail Transport Noise and Freight Considerations in Land Use Planning ("SPP5.4"), addresses transport noise from within major transport corridors, including primary freight routes, and its impact on nearby noise-sensitive land uses. SPP5.4 is therefore relevant to the SSDSP3 as its provisions apply where "a proposed new noise-sensitive development in the vicinity of an existing or future major road, rail or freight handling facility". Future development therefore needs to appropriately accommodate any impacts associated with the freight road corridor in accordance with SPP5.4.

The City's preference at this stage is for no future residential development to directly abut the Rowley Road reserve. Future LSP's will need to demonstrate a suitable interface treatment (e.g. linear public open space, service road design with fronting residential development) being provided.

PART TWO – DISTRICT STRUCTURE PLAN

5.0 DISTRICT STRUCTURE PLAN DESIGN

5.1 Development Principles

The key development principles of the Southern Suburbs District Structure Plan – Stage 3, **as shown in Figure 7** are to:

1. Provide a framework for urban land uses within the District Structure Plan area that integrates with the Sub Regional context;
2. Respond to the social and economic needs of the community in a timely way;
3. Provide a framework for future Local Structure Planning and subdivision, allowing for refinement of detail and recognition of previous uses;
4. Provide for a variety of housing choice through a range of densities, with higher codings being located near public open space, centres and along high frequency public transport routes.
5. Define a robust road network reflecting and accommodating public and private transport priorities, responding to the Sub Regional transport network;
6. Main-street based centres;
7. An integrated open space, conservation and drainage network, balancing environmental, recreational and drainage objectives;

PLAN NOTES

1. FUTURE RESIDENTIAL DEVELOPMENT SHALL NOT DIRECTLY ABUT ROWLEY ROAD. FUTURE LOCAL STRUCTURE PLANNING IS TO DEMONSTRATE A SUITABLE INTERFACE TREATMENT (E.G. LINEAR PUBLIC OPEN SPACE, SERVICE ROAD DESIGN WITH FRONTING RESIDENTIAL DEVELOPMENT) BEING PROVIDED TO THE FUTURE ROWLEY ROAD FREIGHT ACCESS ROUTE.
2. FUTURE ACCESS ROAD TO BE PROVIDED AS A FULL INTERSECTION UNTIL ROWLEY ROAD IS UPGRADED AND CONSTRUCTED TO A REGIONAL ROAD AT WHICH TIME THE INTERSECTION WILL BE CONVERTED AND MAINTAINED AS LEFT IN/LEFT OUT ACCESS ONLY.
3. AS PART OF THE UPGRADE OF ROWLEY ROAD, GRADE SEPARATED PEDESTRIAN AND VEHICULAR ACCESS IS TO BE PROVIDED AS A CONTINUATION OF BARFIELD ROAD, IN ORDER TO MAINTAIN CONNECTIVITY BETWEEN FUTURE DEVELOPMENT TO THE SOUTH OF ROWLEY ROAD.
4. FUTURE STRUCTURE PLANNING OF THE CELL SOUTH OF WATTLEUP ROAD MUST PROVIDE AN APPROPRIATE INTERFACE WITH RESIDENTIAL DEVELOPMENT NORTH OF WATTLEUP ROAD. THIS IS TO HAVE PARTICULAR REGARD TO THE POSITION OF THE WESTERN AUSTRALIAN PLANNING COMMISSION IN RESPECT OF ITS POSITION ON THE ACCEPTABILITY (OR OTHERWISE) OF RESIDENTIAL DEVELOPMENT SOUTH OF WATTLEUP ROAD, AND ALTERNATIVE (NON-RESIDENTIAL) LAND USES THAT MAY BE REQUIRED. ANY IMPACTS ASSOCIATED WITH THE FUTURE DEVELOPMENT OF NON-RESIDENTIAL LAND USES IN THE CELL SOUTH OF WATTLEUP ROAD MUST BE FULLY CONTAINED WITHIN THE CELL BOUNDARIES.
5. NEIGHBOURHOOD CENTRE - THE DESIGN AND FUNCTION OF THE PROPOSED NEIGHBOURHOOD CENTRE SHALL BE BASED ON 'MAIN STREET' PRINCIPLES AND RELEVANT PROVISIONS OF LIVEABLE NEIGHBOURHOODS. ANY ASSOCIATED LOCAL STRUCTURE PLAN MUST ADEQUATELY DEMONSTRATE THROUGH CONCEPT PLANS AND/OR DETAILED AREA PLANS THE MANNER IN WHICH THE CENTRE ADDRESSES THE REQUIREMENTS OF THE RELEVANT SECTIONS OF THE SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN - STAGE 3 REPORT, AND PARTICULARLY THE NEIGHBOURHOOD CENTRE CONCEPT PLAN PROVIDED WITHIN THE REPORT DOCUMENT.
6. NEIGHBOURHOOD NODES - THE DESIGN AND FUNCTIONALITY OF THE NEIGHBOURHOOD NODES SHALL BE DISTINCTLY DIFFERENT TO THE PRIMACY OF THE NEIGHBOURHOOD CENTRE. THESE LOCATIONS ARE TO PROVIDE FOR A RANGE OF MORE LOCALLY FOCUSED ACTIVITIES AND FUNCTIONS. WHERE RETAIL IS PROPOSED, THESE ARE TO NOT EXCEED A MAXIMUM RETAIL FLOOR AREA OF 300SQM, WITH SUCH FUNCTION BASED UPON A CONVENIENCE STORE TYPE USE.
7. CENTRAL PRECINCT - A COMPREHENSIVE LOCAL STRUCTURE PLAN WILL BE REQUIRED FOR THE CENTRAL NEIGHBOURHOOD CENTRE AND ADJACENT DEVELOPMENT. THE CITY WILL NOT CONSIDER INDIVIDUAL STRUCTURE PLANS IN THIS LOCATION DUE TO THE NEED TO ENSURE THE SUITABLE INTEGRATION OF DEVELOPMENT.

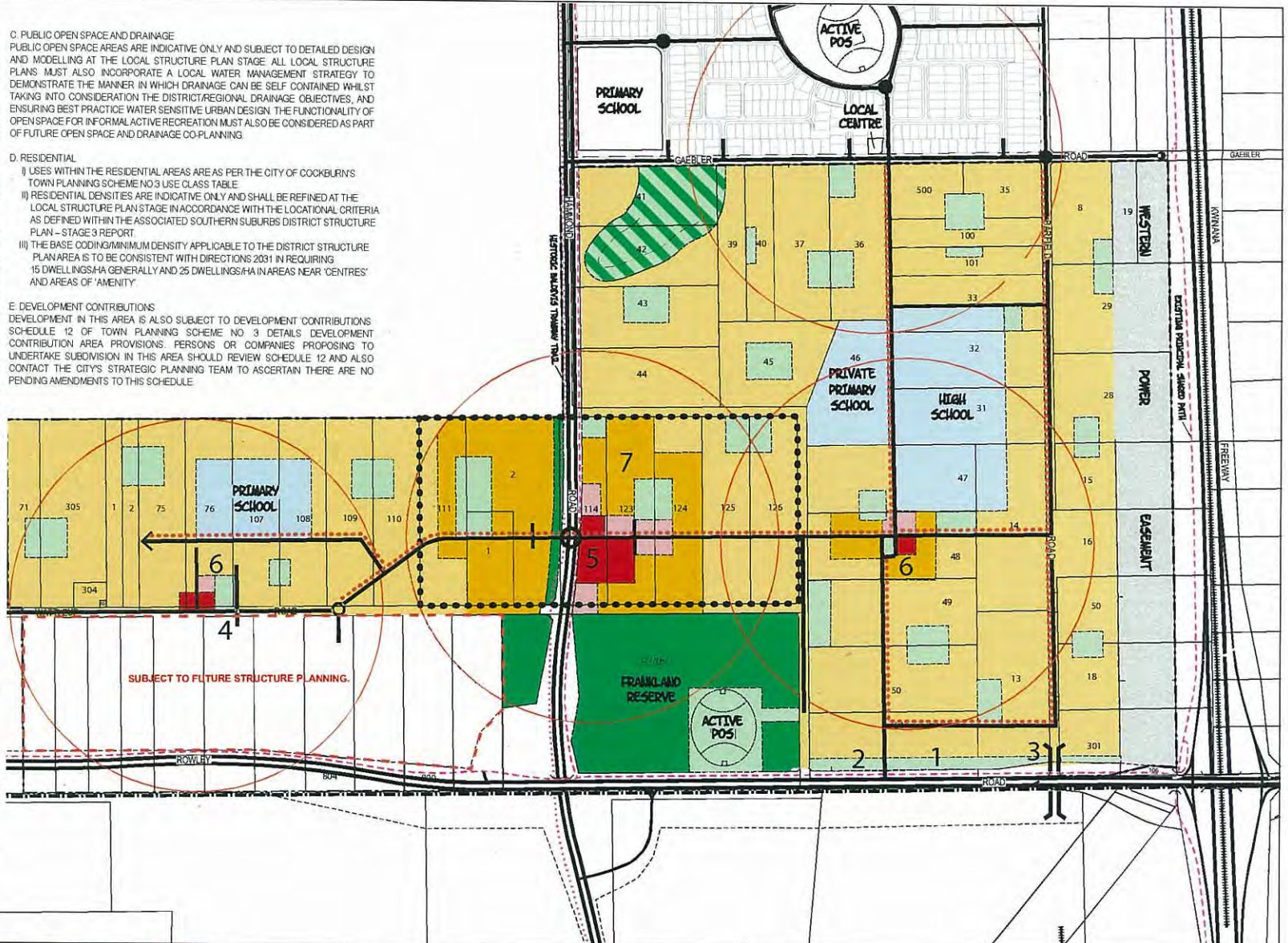
GENERAL NOTES

- A. ALL LOCAL STRUCTURE PLANS MUST INCLUDE AND BE INFORMED BY:
- i) DETAILED LWMS BASED UPON REGIONAL DRAINAGE STUDY,
 - ii) DETAILED NOISE MANAGEMENT STRATEGY WHERE LSP ADJOINS ROWLEY ROAD,
 - iii) FIRE MANAGEMENT PLAN WHERE LSP IS LOCATED NEAR ROS OR SIGNIFICANT POS,
 - iv) FLORA AND FAUNA MANAGEMENT PLAN,
 - v) TRAFFIC MANAGEMENT PLAN,
 - vi) CONTAMINATED SITES & ACID SULPHATE SOILS MANAGEMENT PLAN WHERE REQUIRED,
 - vii) HERITAGE STUDY WHERE LSP INCLUDES FORMER HISTORIC TRAMWAY,
 - viii) TRANSITION AND/OR INTERFACE STRATEGY IN RESPECT OF EXISTING RURAL USES,
 - ix) NEIGHBOURHOOD CENTRE CONCEPT PLAN AND DETAILED AREA PLAN WHERE INCLUDED WITHIN LSP AREA,
 - x) NEIGHBOURHOOD NODE CONCEPT PLAN AND DETAILED AREA PLAN.
- B. LOCAL STRUCTURE PLANS SUBDIVISION AND DEVELOPMENT OF THE SUBJECT AREA IS TO BE IN ACCORDANCE WITH AN ENDORSED APPLICABLE LOCAL STRUCTURE PLAN.

C. PUBLIC OPEN SPACE AND DRAINAGE
PUBLIC OPEN SPACE AREAS ARE INDICATIVE ONLY AND SUBJECT TO DETAILED DESIGN AND MODELLING AT THE LOCAL STRUCTURE PLAN STAGE. ALL LOCAL STRUCTURE PLANS MUST ALSO INCORPORATE A LOCAL WATER MANAGEMENT STRATEGY TO DEMONSTRATE THE MANNER IN WHICH DRAINAGE CAN BE SELF CONTAINED WHILST TAKING INTO CONSIDERATION THE DISTRICT/REGIONAL DRAINAGE OBJECTIVES, AND ENSURING BEST PRACTICE WATER SENSITIVE URBAN DESIGN. THE FUNCTIONALITY OF OPEN SPACE FOR INFORMAL ACTIVE RECREATION MUST ALSO BE CONSIDERED AS PART OF FUTURE OPEN SPACE AND DRAINAGE CO-PLANNING.

D. RESIDENTIAL
i) USES WITHIN THE RESIDENTIAL AREAS ARE AS PER THE CITY OF COCKBURN'S TOWN PLANNING SCHEME NO 3 USE CLASS TABLE.
ii) RESIDENTIAL DENSITIES ARE INDICATIVE ONLY AND SHALL BE REFINED AT THE LOCAL STRUCTURE PLAN STAGE IN ACCORDANCE WITH THE LOCAL CRITERIA AS DEFINED WITHIN THE ASSOCIATED SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN - STAGE 3 REPORT.
iii) THE BASE CODING/MINIMUM DENSITY APPLICABLE TO THE DISTRICT STRUCTURE PLAN AREA IS TO BE CONSISTENT WITH DIRECTIONS 2031 IN REQUIRING 15 DWELLINGS/HA GENERALLY AND 25 DWELLINGS/HA IN AREAS NEAR 'CENTRES' AND AREAS OF 'AMENITY'.

E. DEVELOPMENT CONTRIBUTIONS
DEVELOPMENT IN THIS AREA IS ALSO SUBJECT TO DEVELOPMENT CONTRIBUTIONS SCHEDULE 12 OF TOWN PLANNING SCHEME NO 3. DETAILS DEVELOPMENT CONTRIBUTION AREA PROVISIONS. PERSONS OR COMPANIES PROPOSING TO UNDERTAKE SUBDIVISION IN THIS AREA SHOULD REVIEW SCHEDULE 12 AND ALSO CONTACT THE CITY'S STRATEGIC PLANNING TEAM TO ASCERTAIN THERE ARE NO PENDING AMENDMENTS TO THIS SCHEDULE.



0 250m
SCALE 1:12,500

<p>Residential Density</p> <ul style="list-style-type: none"> High Medium 	<p>Local Centre (Pedestrian Based Retail)</p> <ul style="list-style-type: none"> Local Centre (Pedestrian Based Retail) Mixed Business / Commercial / Home Based Business Community Facilities / Schools etc Western Power Easement Conservation Category Wetland (In accordance with DEC Geomorphic Wetlands Swan Coastal Plain dataset) 	<p>Key Open Space Areas:</p> <ul style="list-style-type: none"> Public Open Space Regional Open Space 	<p>Key Road Structure (Existing and Proposed)</p> <ul style="list-style-type: none"> Major Shared Path Historic Baldvis Tramway Trail 400m Walkable Catchment Central Precinct
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**SOUTHERN SUBURBS
DISTRICT STRUCTURE PLAN
- STAGE 3, HAMMOND PARK / WATTLEUP**

Figure 7

8. Provide for sustainable land use and lot design that responds to solar orientation principles as well as Crime Prevention Through Environmental Design ("CPTED");
9. Encourage local employment within centres, as well as through home based businesses; and
10. Reflect and integrate the development area with surrounding land uses.

5.2 Neighbourhood Structure

Planning of the area is based on walkable catchments of:

- o 400 metres (5 minute walk) which focus on centres, significant areas of public open space and public transport.

Given the shape and extent of the urban area there are significant gaps between each of the 4 neighbourhoods shown. As a result of the limited extent of the urban area and the range of facilities to be provided, there is no justification for the delineation of additional neighbourhoods to reduce the extent of land outside the walkable catchments, particularly as this would most likely result in considerable overlap and no functions to act as the core or focus.

The internal road system within the neighbourhood structure should be permeable and maximise the walkability, particularly to public transport, commercial, recreation, education and community facilities.

5.3 Transport and Access Network

5.3.1 Major Road Network

The DoP has identified the need to upgrade and extend Rowley Road as a dual carriageway which could potentially provide the main link from the Kwinana Freeway to the Latitude 32 industrial area and the future outer harbour. Whilst Rowley Road has not been designated as a 'Primary Regional Road' or 'Other Regional Road' under the MRS, Planning Control Area No. 95 has been designated over the alignment and the WAPC has purchased some of the land required for the road. Directions 2031 identifies the upgrade of the Outer Harbour access roads, including Rowley Road as a strategic road planning project.

Rowley Road provides direct access to the Kwinana Freeway with access onto Rowley Road being restricted to three points as per the SSDSP3 being Hammond Road, Barfield Road and a new central north-south road located between Hammond and Barfield Roads.

Hammond Road will be extended from Gaebler Road to Rowley Road incorporating in part Frankland Road and ultimately go under Rowley Road and continue south as shown on the MRS and the Jandakot Structure Plan.

The main east-west link is Wattleup Road, which is proposed to be realigned to the north to avoid steep contours at the eastern end of existing Wattleup Road.

The road layout shown on the SSDSP3 encourages the majority of traffic generated from the development of the area to utilise Hammond Road and the new central north-south road which will link Russell Road with Rowley Road through Frankland Springs, Lot 412 Gaebler Road and the Southern Suburbs Stage 3 development. This road is focused on directing traffic towards the future Mandogalup railway station and will be the main bus route for the area. Volumes

of traffic along the Barfield, Wattleup, Hammond and the proposed main north-south road are unknown at this stage. Access onto Barfield and Wattleup roads will need to be determined at the time of preparing LSP's having regard to projected traffic volumes.

5.3.2 Traffic Management

Traffic management devices will be required at various locations within the SSDSP3 area. In the long term it is proposed that the intersection of Hammond and Rowley Roads be grade separated. In the first instance the intersection would be at grade and in the medium term would require traffic lights or a roundabout. As part of the upgrading of Rowley Road, a grade separated pedestrian and vehicular access is required to be provided as a continuation of Barfield Road in order to maintain connectivity between future development to the south of Rowley Road. As detailed in sections 5.3.3 and 6.7, the City considers these future modifications to be the responsibility of the State Government.

The intersection of Rowley Road and the north-south central road is proposed to be provided as a full intersection until Rowley Road is upgraded and constructed to a regional road at which time the intersection is to be converted and maintained as left in/left out access only.

Traffic lights will be required at the intersection of Hammond and realigned Wattleup Road to facilitate traffic and pedestrian/cyclist movement. Notional roundabouts have been shown at various intersections however these and any others will need to be assessed in detail at the LSP stage.

5.3.3 Shared Paths/Connections

An interconnecting network of shared paths will be provided within the Structure Plan area with the main connections being identified as Hammond Road, Rowley Road, Wattleup Road (existing and realigned) and the Kwinana Freeway. The historic Baldivis Tramway Trail is parallel to the western side of Hammond Road and opportunities to link the trail to the proposed centre near the intersection of Hammond Road and Wattleup Road. This will provide trail users with the opportunity to access and use the retail facilities within the neighbourhood centre identified for this intersection. LSP's will need to identify key links into the main shared path network.

Pedestrian connections south to the proposed Mandogalup railway station are imperative. Safe, grade separated pedestrian crossings to Rowley Rd should be planned to maximise use of the future station. It is considered the funding of these crossings should be sourced by the State Government as the need for the grade separation arises due to two State Government decisions:

1. Designation of Rowley Road as a major freight route, and;
2. Relocation of the Mandogalup Station approximately 500m south of Rowley Road than originally planned.

5.4 Residential Development

The SSDSP3 area will ultimately comprise a range of residential densities, housing types and tenures. The detail, including specification of density codes as per the provisions of the Residential Design Codes of Western Australia (R-Codes), will be clarified through both the LSP and subdivision processes.

5.4.1 Housing Principles

The housing principles for the SSDSP3 area to guide future structure planning and subdivision are:

1. Provide diversity in housing choice, lot sizes and tenure;
2. Achieve minimum targets of 15 dwellings per gross urban zoned hectare of land (inclusive of land required for infrastructure and POS) and 22 dwellings per site hectare of residential land (exclusive of land required for infrastructure and POS);
3. Higher density dwellings (25 dwellings per gross hectare) and aged persons/independent living accommodation within 400 metres of centres, public open space and along high frequency bus routes;
4. Promoting mixed use development around and within commercial centres;
5. Housing design, lot layout and access arrangements that create attractive streetscapes; and
6. Environmentally sustainable design approaches in terms of solar orientation of lots.

5.4.2 Residential Densities and Housing Types

The location and layout of R-Code boundaries will be determined at the LSP stage followed by detailed subdivision design. LSP's will be required to demonstrate the achievement of a minimum 15 dwellings per gross urban zoned hectare of land and a minimum of 22 dwellings per site hectare of residential land. Areas near 'centres' will generally need to demonstrate the achievement of a minimum 25 dwellings per gross urban zoned hectare of land.

The following provides a summary of locational criteria for residential land uses within the SSDSP3 area, providing a framework for the designation of R-Codings at the LSP stage but does not preclude other density responses within the structure plan area.

Variations to the densities prescribed under the SSDSP3 will be assessed against the housing principles set out in section 5.4.1 - Housing Principles, and the locational criteria set out below.

5.4.2.1 Locational Criteria – Medium Density - Residential R30 (base)

1. Residential R30 will be the minimum base coding over the SSDSP3 area. Allowing for the provision of traditional single dwelling lots from 300m².

5.4.2.2 Locational Criteria - Medium Density - Residential R35-R60

1. Generally surrounding areas of high amenity, such as open space;
2. Located so as to maximise access to and use of services and facilities such as public open space, activity nodes and public transport routes; and
3. Located to enhance passive surveillance of public spaces.

5.4.2.3 Locational Criteria – High Density - Residential R60+

1. Located within or adjacent to the Neighbourhood Centre or Neighbourhood Nodes;
2. Located to enhance passive surveillance of public spaces.

5.4.2.4 Locational Criteria – Aged and Dependant Persons’ Dwellings

1. Generally surrounding areas of high amenity, such as open space;
2. Located so as to maximise access to and use of services and facilities such as public open space, commercial centres and public transport routes: and
3. Located to enhance passive surveillance of public spaces.

5.4.3 Lot Yield Estimates

Based on the locational criteria set out in section 5.4.2.4, the SSDSP3 is forecast to yield approximately 3411 dwellings. **Table 1** below outlines how the 3411 dwelling target is expected to be delivered within the R30, R35-R60 and R60+ coded residential areas. As shown in the table below, the SSDSP3’s total gross urban land area has been calculated at 128 hectares and total site area at 83 hectares.

Density	Estimated Average Lot Size	Total Area (site hectare)	Estimated Yield
R30	300 m ²	61 ha	2033
R35 – R60	220 m ²	12 ha	545
R60+	120 m ²	10 ha	833
Total		83 ha	3411
Total Site area (83ha)		Dwelling Units per site ha	41
Total Gross areas (128ha)		Dwelling Units per gross ha	26

Note: The calculations above do not include land which is subject to future structure planning (i.e. land affected by Alcoa Reside Storage Area buffer).

Based on the above, the SSDSP3 notionally achieves an average residential density of 26 dwellings per gross urban zoned hectare which is in accordance with the minimum density target of 15 dwellings for greenfield developments set out in *Directions 2031*. The forecast of approximately 3411 dwelling units for SSDSP3 is also consistent with the Draft Outer Metropolitan Perth and Peel Sub-Regional Strategy which identifies the subject area as having the potential to deliver 3000+ dwellings in the future.

It should be noted however that minimum and average site areas not applicable to multiple dwellings in areas codes R30 or greater under the R-Codes as amended in November 2010. As the majority lots will be coded above R30 under the SSDSP3, which have the potential to be developed as multiple dwellings, the site hectare dwelling calculations can be considered relatively conservative.

These densities and yields are indicative only and will be refined and developed as part of LSP and subdivision design.

5.4.4 Aged Persons Accommodation/Independent Living

Specific sites for such a facility have not been identified on the SSDSP3. However in order to ensure the design of any proposed aged person’s accommodation integrates with the surrounding urban fabric the following principles apply in the preparation of LSP’s:

1. Located so as to maximise access to and use of services and facilities such as public open space, centres and public transport routes;
2. Development to address and survey surrounding public streets, with visually permeable fencing and major openings addressing the street;
3. Development integrated with surrounding land uses, including pedestrian access and permeability; and
4. Any community/shared facilities to address the primary street.

Detailed Area Plans will be required to be prepared for aged persons/ independent living sites in accordance with these principles at subdivision stage. The City may waive this requirement only where matters relating to development design have been satisfactorily addressed at the LSP stage.

All proposed aged persons/ independent living sites will also need to demonstrate compliance with the requirements of the City's Local Planning Policy No.12 - Aged or Dependent Persons' Accommodation - Development Guidelines ("LPP12"). LPP 12 includes specific locational and site planning criteria which needs to be met as part of any aged persons/ independent living development.

5.5 Education Facilities

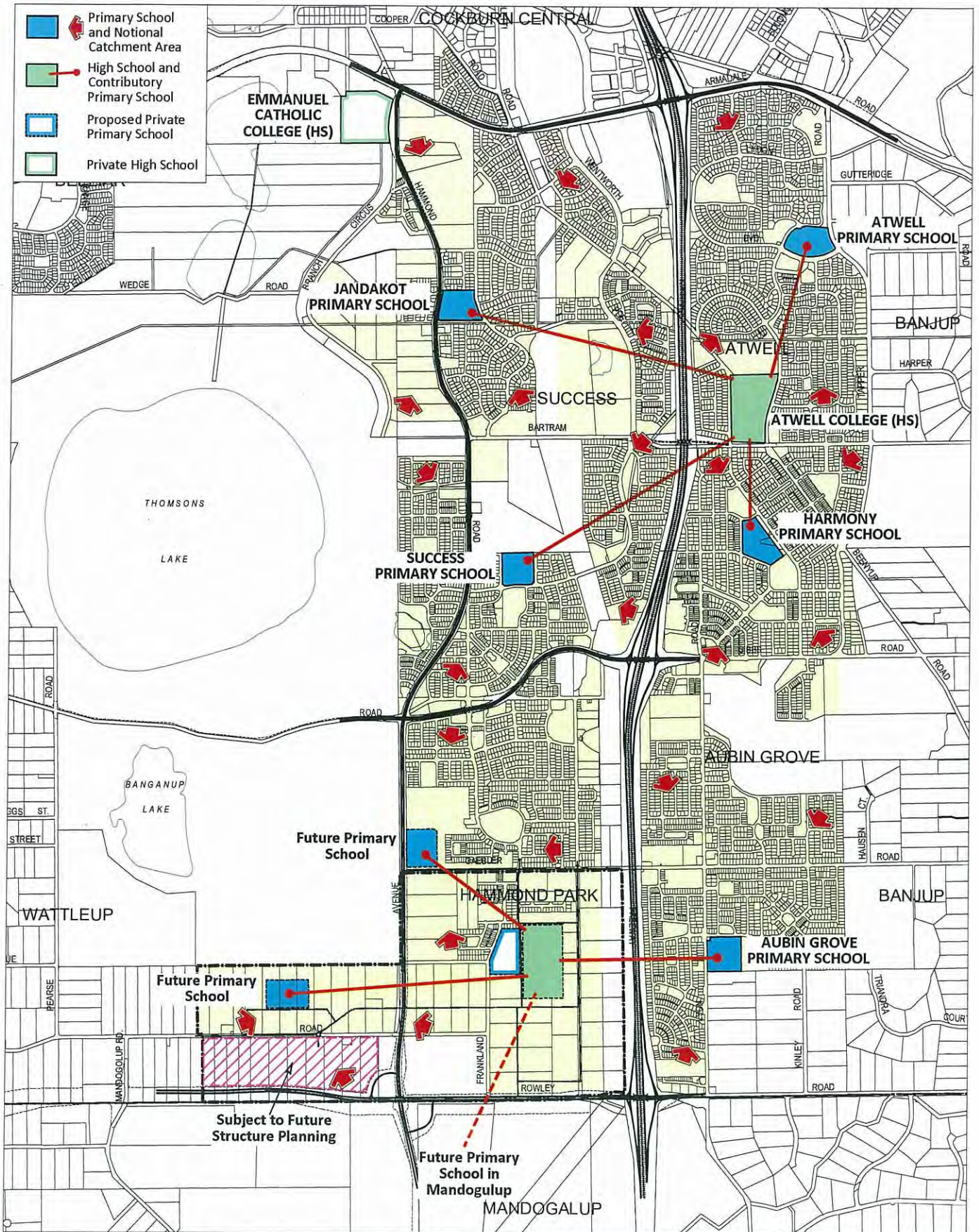
An assessment of the need for schools in the area has been undertaken as part of the preparation of the structure plan in 2005. Based on the projected number of lots within the structure plan area and the development up to Russell Road there is a need for a high school and two primary schools in the area west of the Freeway between Russell and Rowley Roads (**refer to Figure 8 – Education Facilities**).

Planning for the area north of Gaebler Road has provided a primary school site at the intersection of Gaebler and Hammond Roads. A private Catholic primary school has also been identified for the area. This site (Lot 46 Frankland Avenue) is already owned by the Roman Catholic Archbishop of Perth and was purchased for the purpose of developing it as a primary school.

The proposed high school will provide for the future primary schools identified in Hammond Park (Lot 412 Gaebler Road), Aubin Grove (Lot 448 Lyon Road) and Wattleup (shown on the District Structure Plan). There is likely to be a future primary school over the local government boundary to the south in the Town of Kwinana which will also contribute to the high school.

The location and notional catchment areas for the proposed primary and high schools in the locality are shown on the Education Facilities plan. In accordance with Liveable Neighbourhoods, the school sites have been located on the edge of the 400 metre walkable catchments or 800 metre walkable catchment of the railway station (original location) in the case of the high school, on neighbourhood connector roads and on largely flat sites. The high school and private school are also located on a bus route which connects to Cockburn Central Interchange and the future Mandogalup train station.

High school and private school students often travel considerable distances to access the schools and rely on public transport. As Transperth will not provide special bus services to schools it is important that they be located on or very close to the regular bus service routes.



EDUCATION FACILITIES
 SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN
 - STAGE 3, HAMMOND PARK

City of
Cockburn
 wetlands to waves

FIGURE 8

No alternative suitable sites were identified in the structure plan area for the high school. Regardless, it is not considered that the relocation of the future train further south warrants a relocation of the high school as part of this review of the SSDSP3 as expectations have been set for some years and surrounding development has assumed its location remaining where it is.

In respect to the primary school, the site could be moved northwards to the boundary of the Harry Waring Reserve. In considering the 2005 plan, this location was not favoured as it was less central to the catchment and would result in longer walking/cycling distances for students south of Wattleup Road. Given the recent advice from the WAPC relating to Alcoa Residue Storage Area buffer, the Department of Education and Training ("DET") will need to advise whether the proposed location is still suitable given potential changes to the catchment.

All subdivision within the area will be required to make a pro-rata contribution to the Department for Education and Training to cover the cost of acquisition of the proposed primary school site in accordance with normal practice. The high school site will be acquired by the DET.

5.6 Parks and Recreation

5.6.1 Regional/District Parks and Recreation

Frankland Reserve has been nominated as a Bush Forever site however it has a degraded area in the south-east corner which has been identified on the SSDSP3 for active open space including a playing field in accordance with Council's decision to support the Bush Forever nomination at its meeting on the 16 September 2003. The degraded area is approximately 3ha in size and may need to be enlarged to 4ha to ensure that a viable playing field can be constructed on the site.

Given the potential population in the area it would be desirable to provide three active playing fields between Russell and Rowley Roads. Active playing areas are proposed north of Gaebler Road and on portion of Frankland Reserve as stated above. In the event that land holdings in the area are consolidated into a single ownership of sufficient size, consideration will be given to consolidating the POS areas shown to provide an additional active sports area. Ideally this would be in the area west of Hammond Road. Alternatively further consideration will need to be given to the facilities and area of Frankland Reserve to be developed.

The current proposal for Frankland Reserve as identified in the City's 'A Plan for the District 2010-2020' comprises one active playing field and construction of an access road, car park, and clubrooms/community facilities (600m²). The facility will serve the population of Hammond Park complementing the limited junior sport (AFL) provision provided at the nearby active reserve Hammond Park (north of Gaebler Road). Frankland Reserve will provide scope for junior and senior AFL, athletics and soccer. No additional community facilities have been deemed necessary to serve the future population's needs in Hammond Park.

The MRS Parks and Recreation link on the west side of Hammond Road between Harry Waring Reserve and Frankland Reserve has been retained as part of the Historic Baldivis Tramway Trail. The 2005 plan indicated that an amendment should be pursued to the Metropolitan Region Scheme to delete a portion of the Parks and Recreation Reserve. This was to enable the location of the proposed centre at the intersection of the realigned Wattleup and Hammond Roads.

In recent times, the approach taken to the conservation and interpretation of this trail has been to respect its integrity and to not reduce or modify the existing reserve.

5.6.2 Neighbourhood/Local POS – location and designation

A series of neighbourhood and local parks have been shown throughout the residential areas on the SSDSP3. The selection of the POS sites was based on the following:

1. Securing bushland not only in recognition of the quality of the bushland in the area but also the potential difficulty in securing an adequate water allocation for irrigation purposes;
2. Along ownership boundaries to enable owners to be able to satisfy their 10% public open space (POS) requirements within their own land holding;
3. In reasonably close proximity to existing wetlands to provide a high level of amenity for the surrounding proposed high/medium density; and
4. Where possible in lower lying areas of the lots to allow for the integration of drainage facilities.

The SSDSP3 allocates POS areas based on the above principles. Where land holdings are to be consolidated and developed comprehensively, the POS areas can be amalgamated into larger parks which will reduce the City's maintenance costs. In areas where consolidation is not contemplated, public open space arrangements have been designed so that each land owner is able and required to satisfy their open space requirements on their own landholding.

The City will only accept cash-in-lieu of POS for lots which are fragmented, isolated and relatively small in nature and where the provision of land would result in a very small and impractical area being provided. The only lots where cash in lieu of open space is currently contemplated is for lots 31 and 32 Barfield Road where the provision of land would result in a very small area being provided given that these lots are adversely affected by the high school site. The cash in lieu can be spent on facilities and works to be undertaken in Frankland Reserve.

The SSDSP3 requires that where ever possible, established trees should be incorporated into future development either in parks or road reserves. Established trees will assist in giving additional character as well as providing shade opportunities which are often lacking in new estates.

POS, credits and expenditure of cash-in-lieu for POS will be determined in accordance with the relevant WAPC policies, practices and legislation at the LSP and subdivision stages.

5.6.3 Neighbourhood/Local POS – design and embellishment

These parklands are intended to serve the regular small scale needs of the immediate residential population within a five to ten minute walking distance. The predominant use should be for informal recreation for individuals and households, especially low level children's play, dog walking and relaxation. They are not intended for any formal/organized active team sports and should not therefore warrant parking on-site.

Embellishment with infrastructure pitched above this role will not be accepted. Similarly, design of these parklands which have higher than average (for the City of Cockburn) maintenance or whole of life cycle costs will not be accepted.

The types of supporting amenities could include paths, shade, seating, children's play spaces etc

5.6.4 Neighbourhood/Local POS – ongoing planning

The following additional POS information will be required at the LSP stage:

- o Preliminary landscape design concepts setting out key design features and principles, where development is proposed;
- o Notional landscape management regime and asset cost schedule inclusive of an approximate unit rate per hectare for maintenance (indicative only).

5.7 Public Transport

A future railway station known as Mandogalup is proposed within the Kwinana Freeway Reserve, south of Rowley Road. The exact location and timing of this station has not been confirmed but it is estimated to be approximately 10 -15 years away. Since the 2005 plan, this preferred location of this station has shifted from its original location, which was north of Rowley Road.

A "Park 'n' Ride" facility was originally provided on the western side of the Freeway for the former station location. The land required for the car park was purchased by the WAPC. As this location was aligned to the original location, it is likely the WAPC will seek to acquire land south of Rowley Road also. This review has assumed the existing car park proposal north of Rowley Road will also remain. The land is heavily constrained by the existing powerline easement which may make disposal of the land difficult. Additional 'overflow' car parking in proximity to stations may be advantageous.

Two bus routes originating from the transit interchange at Cockburn Central are proposed to be progressively extended through the structure plan area. The bus routes are proposed to follow the proposed new central north-south road to the future Mandogalup railway station and along Hammond Road. The proposed local road network has been designed to accommodate the bus routes. It is likely that a future bus route will be required along Wattleup Road to connect into the main north-south network as there is a considerable area in the western area along Wattleup Road that is outside of the 400 metre walkable catchment for the Hammond Road service.

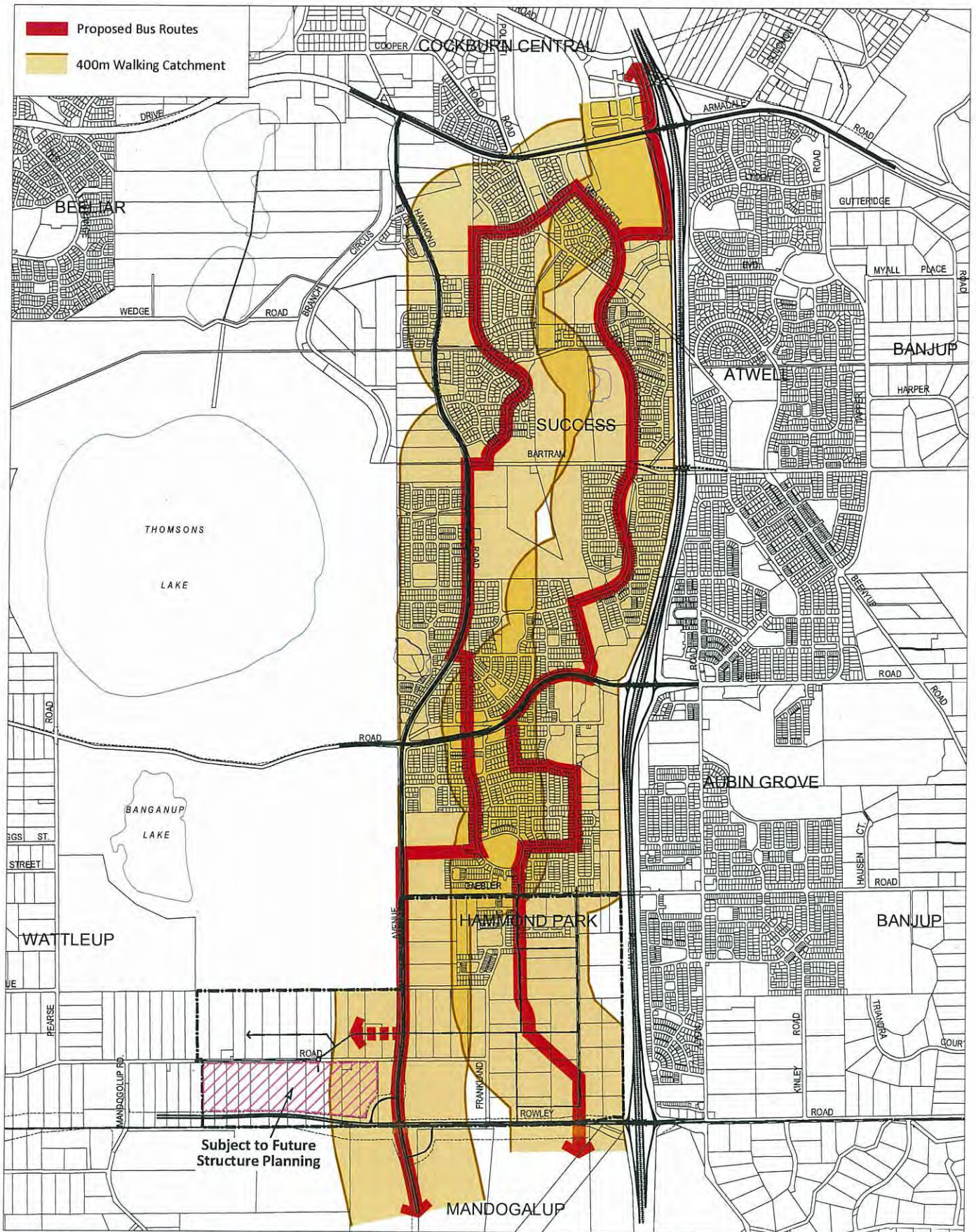
The provision of a future bus service along Wattleup Road could also link to the Latitude 32 Industrial area and terminate at the Fremantle Rockingham transit system which is to be routed down the west side of the freight line within the industrial area. The proposed bus and train services will provide a comprehensive public transport system for the area linking major land use activities. The proposed location of the future bus routes and associated walkable catchments are shown on **Figure 9 - Public Transport**.

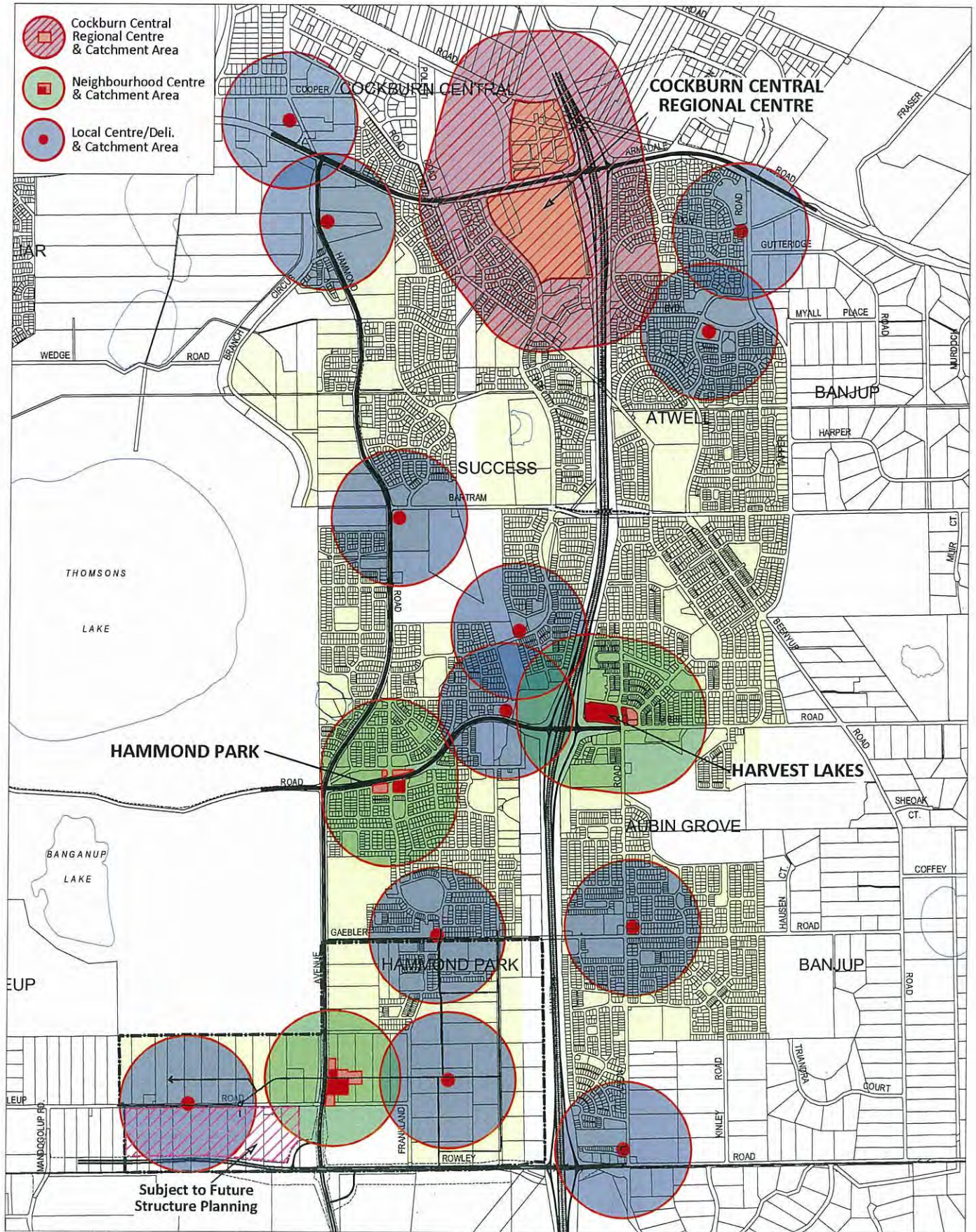
5.8 Commercial Facilities

5.8.1 Location and Hierarchy

The location and hierarchy of commercial/retail centres are shown on **Figure 10 - Neighbourhood Structure and Retail Hierarchy Plan**.

A Neighbourhood Centre has been designated at the intersection of Hammond Road and the realigned Wattleup Road. This centre will provide the main activities and shopping facilities for the structure plan area. It is envisaged the





NEIGHBOURHOOD STRUCTURE & RETAIL HEIRARCHY
 SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN - STAGE 3, HAMMOND PARK **FIGURE 10**

City of Cockburn
 wetlands to waves

Neighbourhood Centre will include a supermarket, café's and small specialty shops. The total retail/commercial floorspace is likely to be in the order of 5000 square metres. The centre will also provide opportunities for other (non-retail) small businesses and local employment, consistent with the aims of SPP 4.2.

The design and function of the proposed Neighbourhood Centre will be based on 'main street' principles and relevant provisions of Liveable Neighbourhoods. Any associated LSP must adequately demonstrate through concept plans and/or detailed area plans the manner in which the centre addresses these requirements and the concept plan for the centre as shown within **Figure 11 – Neighbourhood Centre Concept Plan**.

Two small Neighbourhood Nodes have been provided within the western and eastern precincts of the SSDSP3 area. The design and functionality of the Neighbourhood Nodes will be distinctly different to the primacy of the Neighbourhood Centre. These locations are to provide for a range of more locally focused activities and functions. Where retail is proposed, these are to not exceed a maximum retail function of 300sqm, with such function based upon a 'convenience store' type use.

All centres will also provide opportunities for other (non-retail) small businesses and local employment, consistent with the aims of SPP 4.2.

5.8.2 Mixed Business/Commercial/Home Based Business Precincts

The SSDSP3 identifies areas for mixed business, commercial and home based business adjacent to the Neighbourhood Centre and Neighbourhood Nodes. This allows for office/residential mixed use development to encourage local employment opportunities. The provision of mixed business and complementary uses surrounding all the centres will provide 'main street' design opportunities which has the potential to provide a high quality built environment that will be the central focus of the area.

5.9 Area subject to Future Structure Planning

As outlined in section 4.1.3 of this report, a portion of the SSDSP3 is impacted by the revised buffer surrounding Alcoa's Mandogalup Residue Disposal Area. In view of the expanded buffer, the affected area within the SSDSP3 area has been designated as 'subject to future structure planning'.

The decision not to exclude the area entirely from the SSDSP3 reflects the future desire for residential or compatible development to occur within this area subject to the appropriate modelling and environmental studies being undertaken. This is in line with the advice of the State Administrative Tribunal ("SAT"), providing that current modelling which was used to prepare the buffer should be given no weight, and needs to be undertaken again. Once this has been completed and the actual implications known, appropriate structure planning can then take place for this land.

Future structure planning of this cell must provide an appropriate interface with residential development north of Wattleup road. This is to have particular regard to the position of the WAPC in respect of its position on the acceptability (or otherwise) of residential development south of Wattleup road, and alternative (non-residential) land uses that may be required. Any impacts associated with the future development of non-residential land uses in the cell south of Wattleup road must be fully contained within the cell boundaries.



6.0 SERVICING INFRASTRUCTURE

Sinclair Knight Merz were commissioned by the City of Cockburn to conduct an investigation into the engineering servicing and infrastructure requirements relevant to the future development of the SSDSP3 area.

Information was obtained from the Water Corporation, Western Power, Alinta Gas and Telstra. Responses were generally qualified with statements regarding the dynamic nature of planning data and no assurances were given to the future accuracy of the information provided. This information was collected for the 2005 plan and it is assumed the conclusions it reached still apply. The urban front has now reached the SSDSP3 area and subdivision within the area (Hammond Rise) is well underway.

The 2005 servicing report confirmed that the land can be serviced for residential purposes and that developers will need to make appropriate arrangements with the servicing authorities for the extension of infrastructure into the area. **Refer to Appendix 2 for full report.**

6.1 Sewerage

The information available for the 2005 plan revealed there was no Water Corporation sewerage infrastructure in the study area at that time. The development front has now proceeded south into Hammond Park and a sewer connection for the area exists in Gaebler Road. This is in line with the conceptual sewerage catchment plan previously provided by the Corporation which showed the SSDSP3 area being serviced by the sewerage infrastructure in Hammond Park as development advanced southwards.

Most of the development area is planned to gravitate northwards to the Deanmore Bend connection. Some of the land requires fill to enable the sewerage system to gravitate northwards. These areas also need to be filled to obtain the necessary vertical separation to ground water.

The western and very southern parts of the area will require the construction of a permanent pump station with a pressure main connecting back into the Deanmore Bend catchment. The pump station is shown notionally on the north side of Wattleup Road in the low point located almost at the western extremity of the urban area.

The proposed pump station is not on the Water Corporation's construction schedule for the near future. Accordingly if development in this catchment proceeds ahead of the development front then prefunding of works may be required.

6.2 Water

The information available during preparation of the 2005 plan indicated there was no potable water reticulation infrastructure operated by the Water Corporation within the SSDSP 3 area. The area is within the existing Thomsons Lake Water Supply Scheme area.

As at 2005, an ON600 main existed in Hammond Road some 1.5 Km north of Gaebler Road. Preliminary planning indicated that water supply for the area could be from the following:

1. From the north from the existing main in Hammond Road;
2. From the south through several feeding mains gravitating into mains along Frankland Avenue, or;
3. From the west gravitating to a main along Wattleup Road.

At that stage, the above mains were not planned for construction before 2010. The urban front has now advanced and with residential development now occurring within the northern section of the SSDSP 3 area, it is obvious at least one of these options has progressed.

Regardless, developers should undertake consultation with the Water Corporation at an early stage for the initial water supply, timing and any prefunding requirements for the future distribution main.

6.3 Power

There is a network of high voltage transmission cables and towers through the area. Western Power advises that the available power supply in the surrounding high voltage cables in Rowley Road, Gaebler Road and Wattleup Roads is not adequate to serve the development. Western Power have advised that a future substation in Jandakot (approximately 10 kilometres away) will be constructed to service the area however a scheduled start date has not been confirmed. The project does not appear in Western Power's current project list.

6.4 Natural Gas

Correspondence from Alinta Gas Networks Pty Ltd states that there is no Alinta Gas infrastructure within the study area or its surrounds and, there are currently no plans to extend natural gas service into this area. Future applications for gas services will be considered on a case by case basis. It is anticipated that natural gas would be extended from the north as the development front progresses southwards. The current serviced network indicates gas is available in the development area north of Gaebler Road.

6.5 Telecommunications

Telstra has confirmed the location of existing infrastructure in the area including a fibre optic cable running along Rowley Road and terminating near the corner of Gaebler and Barfield Roads.

Telstra plans expansion of their infrastructure to suit ongoing development and thus does not have any planning information for this area. Advice received indicates extension of the fibre optic network to the north east of the study area is expected to occur as the development front moves south. Servicing of the area is likely to be fed from existing fibre optic cables in Wattleup Road and Rowley Road.

6.6 Stormwater Drainage

To facilitate the urban development east of the Beelihar Regional Park, the Water Corporation agreed to construct the Southern Lakes Main Drainage Scheme. The majority (70%) of the structure plan area falls under this scheme.

In accordance with current practice, individual developers will be required to provide for stormwater retention and drainage within their individual lot boundaries. Disposal of stormwater from individual house lots will be achieved by the installation of soakwells by the lot owners at the building stage. The option adopted for various areas will be determined by the geotechnical investigations. The use of soakwells will facilitate the recharge of the aquifer.

The Russell Road Arterial Drain Scheme Report prepared by David Wills and Associates for the City of Cockburn shows a large portion of the SSS3 area between Gaebler Road and Rowley Road requiring connection into a regional drainage network. This regional drainage network will control the groundwater levels in the area and therefore developers by requiring little fill to ensure a 3m separation between development and the groundwater.

Development Contribution Areas No.9 and 10, which are proposed to cover the SSDSP3 area, include provisions for the collection of contributions towards the regional drainage infrastructure. These contribution catchments have been based on the Water Corporations Southern Lakes Drainage Boundary as this catchment was used for Development Contribution Area No. 7 - Aubin Grove as approved via Amendment No. 17 to Town Planning Scheme No.3.

In preparation for the 2005 plan, the Water Corporation and (then) Water and Rivers Commission advised developers should endeavour to retain as much surface runoff on-site as possible in accordance with water sensitive design guidelines, and to make provision for nutrient stripping strategies for water leaving the site. This strategy remains unchanged and is now embedded in the document 'Better Urban Water Management' which guides the implementation of WAPC's State Planning Policy 2.9 - Water Resources ("SPP2.9").

Maximum nutrient levels for water entering the Russell Road buffer lake system will be the subject of negotiation between the City of Cockburn, Water Corporation, Department of Water and Department of Environment and will be determined prior to the design and construction of the buffer lake system.

The Russell Road buffer lake is the upstream limit of the South Jandakot Main Drain. Availability of funds for the 1,200mm diameter extension of the Branch Drain from the Bartram Road buffer lake system to the yet to be constructed Russell Road buffer lake are subject to approval of the Water Corporation's overall Capital Investment Program and the rate of catchment development. Developer Contribution Areas will apply to the area to cover the works downstream of the Russell Road Buffer Lake (Lake Copulup on Hammond Road).

Drainage infrastructure upstream of the Russell Road buffer lake is to be funded by developments within the catchment. The City of Cockburn and developers will be expected to liaise with the Water Corporation and the Water and Rivers Commission to determine the drainage needs of each development within the study area.

Future developments within the SSDSP 3 area should allow for detailed drainage and water quality assessments to ensure local authority, Water Corporation, Department of Water and the Department of Environmental requirements are satisfied.

6.7 Construction of Proposed Regional Roads

6.7.1 Rowley Road

Planning Control Area No. 95 has been designated over the alignment of Rowley Road and the WAPC has purchased some of the land required for the road. Given it is likely this road will become a freight route servicing the Latitude 32 industrial development and the proposed outer harbour, it is not considered appropriate for developers within the SSDSP3 area to contribute to the construction of Rowley Road.

Construction of Rowley Road is not likely to be significantly progressed till either the proposed outer harbour or the Latitude 32 area becomes operational. Notwithstanding this, currently there is a high volume of trucks using Wattleup Road which will not be acceptable when the area is developed for residential purposes. Accordingly construction of Rowley Road as a single carriageway between Frankland Avenue and Mandogalup Road at an early time is seen as a high priority.

Acquisition, funding, design and construction of Rowley Road are the responsibility of the State Government given the road will be a 'Primary Regional Road' (red road). Any any interim construction or upgrades to Rowley Road not directly related to subdivision/development within the SSDSP3 area should also be the responsibility of the State. The 2005 plan considered Rowley Road would attract contributions, this was on the assumption it would be a lower order 'Other Regional Road' (blue road) under the MRS. Since that time, a number of documents, most recently Directions 2031 and beyond and the Draft Outer Metropolitan Perth and Peel Sub-regional Strategy have indicated the portion of Rowley Road west of Kwinana Freeway to be a major freight route being planned by Main Roads WA. From this it is clear that the road will be designated as a 'Primary Regional Road' under the MRS and not an 'Other Regional Road'.

Infrastructure necessitated from the designation of Rowley Road should also be considered a responsibility of the State, rather than developers within the SSDSP3 area. These items of infrastructure are:

1. Any interim/medium term traffic management (such as a roundabout or traffic lights) to the intersection of Hammond Road and Rowley Road prior to grade separation;
2. Penultimate grade separation of Hammond Road and Barfield Road from Rowley Road, including bridges and associated ramps and earthworks;
3. Any interim/medium term at grade vehicle and pedestrian crossings to access the proposed Mandogalup railway station south of Rowley Road, and;
4. Penultimate grade separation of vehicle and pedestrian crossings to access the proposed Mandogalup railway station south of Rowley Road.

Building of any sensitive land uses adjacent to Rowley Road will need to be cognisant of its purpose as a freight route. SPP5.4 suggests a suite of mitigation measures which parties (including Main Roads WA and developers) can implement to address noise issues. It is considered reasonable that Main Roads WA consider the use of alternative aggregate or 'quiet grade' asphalt in this location. This is being planned for another growth corridor in Perth's northeast and should equally be employed for Rowley Road.

6.7.2 Hammond Road

Hammond Road is designated as an 'Other Regional Road' under the MRS. As with development cells further north, it is considered appropriate that proportional contributions be made towards the construction of Hammond Road including the cost of land, full earthworks, construction of one carriageway and dual use path (one side only). The details of the cost sharing arrangements to be introduced are set out as part of Amendment No 28 to TPS No 3

The extension of Hammond Road is identified within the City's 'A Plan for the District 2010 -2020' as requiring land acquisition to occur by 2013/14 in order for design and construction to occur in 2014/15. These targets are also dependent on sufficient funds being accumulated through the developer contribution process.

Any proportion of works to extend Hammond Road which are not covered by developer contributions will need to be provided by the City. The exception to this are works required to grade separate the intersection with Rowley Road which should be sourced by the State Government as the need for the grade separation arises due to two State Government decisions:

1. Designation of Rowley Road as a major freight route servicing the proposed outer harbour, and;
2. Designation of Rowley Road as a major freight route servicing the proposed Latitude 32 industrial area

6.8 Upgrading of Existing Local Roads

Existing roads being Barfield, Wattleup, Gaebler Roads and Frankland Avenue are currently constructed to rural standards. In accordance with normal practice, developers will be required to pay the cost of upgrading these roads to residential standards. The normal formula applied is to pay half the cost of upgrading that section which abuts an owner's property including any required traffic management devices.

7.0 IMPLEMENTATION

7.1 Metropolitan Region Scheme Zoning

The majority of the SSDSP3 area is zoned 'Urban' under the MRS. The future Hammond Road alignment is reserved appropriately under the MRS as 'Other Regional Roads'. The proposed Rowley Road freight corridor is zoned MRS and its reclassification to 'Primary Regional Roads' will be necessary prior to its construction.

The future land uses identified in the SSDSP3 are consistent with the predominant 'Urban' zoning of the locality.

7.2 City of Cockburn Town Planning Scheme Zonings

The subject land is zoned 'Development' in TPS No.3 with all subdivision and development being in accordance with an adopted structure plan as provided by

the Development Area provisions contained in Schedule 11 of Town Planning Scheme No.3. This District Structure Plan sets the general framework, however it will still be necessary for individual or groups of owners to prepare LSP's for their land. Accordingly no further action is required in respect to the zoning of the land.

However, to facilitate and effectively control development it is proposed to rationalise Development Area 9 (DA9) and introduce two new development areas - Development Area 26 (DA26) and Development Area 27 (DA27) covering the structure plan area with appropriate conditions in Schedule 11 (Development Areas) and two new development contribution areas (DCA9-Hammond Park and DCA10-Wattleup) covering contributions to regional roads and drainage infrastructure with appropriate conditions in Schedule 12 (Development Contribution Plan).

The above proposals are the subject of Amendment No 28 to TPS No. 3 which Council resolved to adopt at its meeting held on 11 August 2005. Consideration of the amendment is still pending by the WAPC and the Minister for Planning.

7.3 Development Area and Development Contribution Area requirements

Development areas within the City are the subject of specific requirements set out in Schedule 11 (Development Areas) and Schedule 12 (Development Contribution Plan).

At the time of preparing TPS No 3, planning of the SSDSP 3 area was not sufficiently advanced to enable appropriate requirements to be determined. With the preparation of the SSDSP3 in 2005, this represented the most appropriate time to introduce appropriate requirements through Amendment No. 28 to TPS No 3.

7.3.1 Designation of Development Area requirements

The portion of Development Area 9 (DA 9) – Gaebler Road extending south from Gaebler Road to Rowley Road lies within the SSDSP3 area. North of Gaebler Road was part of the SSDSP Stage 2 area. For this reason it was deemed more appropriate to separate these development areas with their own provisions.

TPS 3 shows the western portion of the SSDSP 3 area being zoned 'Development' however it does not contain a development area number. It is unclear why this has occurred and it was recommended that DA27 be designated to the area.

Both DA26 and DA27 will require a detailed schedule to be included in Schedule 11 - Development Areas of the Scheme outlining the objective and requirements of these areas. The proposed schedules are in keeping with others included in this section of the Scheme and are for administrative purposes.

7.3.2 Designation of Development Contribution Area requirements

The SSDSP3 is affected by draft DCA9 and DCA10 and all landowners will be required to make a contribution to drainage, upgrading of regional roads, traffic management devices and professional services in accordance with Schedule 12 and Section 6.3 of TPS No 3. These contributions will be payable at the subdivision approval and clearance stage.

Public open space has been allocated on the basis that the areas to be provided represent 10% of the subject properties and where no open space is provided

that cash-in-lieu will be accepted as detailed in Section 5.6 of this report. No further arrangements will be required.

7.4 Planning Control

The SSDSP3 shows the broad allocation and location of land use activities and will form the basis of assessing detailed proposals for individual properties. The District Structure Plan is a "guiding document" adopted by the Council and is not a structure plan adopted pursuant to section 6.2.9 of the City's TPS No. 3. Therefore land uses and zones are not given the full effect as though they are part TPS No.3.

To progress the subdivision and development of a land holding it will be necessary for landowners or groups of small landowners to prepare and submit a detailed LSP and supporting report for their land. Each structure plan should be generally in accordance with the SSDSP3, and should show detail including the proposed road and lot layout, detail areas of POS, R-Codes and other information set in the Development Area provisions of TPS No. 3.

Any significant departures from the SSDSP3 would need to be identified and justified. All LSP's are to be adopted by Council and endorsed by the WAPC in accordance with the Development Area provisions of the Scheme.

The above approach has worked successfully in Atwell (LandCorp and Peet & Co), Success (Gold Estates), Hammond Park (Australand and Gold Estates) and Aubin Grove (Stockland) where detailed local structure plans for each estate have been coordinated by the adopted Southern Suburbs District Structure Plan (Stage 2) prepared for that area. The same approach is considered appropriate for the SSDSP 3 area given the similarities between the two areas.

8.0 LAND AVAILABILITY

There are considerable lead times to have planning/development approvals in place. Planning formalities required to be completed include the following:

- o Adoption of LSP's for individual projects;
- o Approval of detailed plans of subdivision; and
- o Approval of Detailed Area Plans.



TOWN PLANNING SCHEME NO. 3

AMENDMENT NO. 28

August 2012

TOWN PLANNING AND DEVELOPMENT ACT, 1928 (AS AMENDED)
RESOLUTION TO AMEND TOWN PLANNING SCHEME
CITY OF COCKBURN
TOWN PLANNING SCHEME NO. 3
AMENDMENT NO. 28

RESOLVED that the Council, in pursuance of Section 7 of the Town Planning and Development Act, 1928 (as amended), amend the above Town Planning Scheme by:

1. Including the following in Schedule 12 – Development Contribution Plan of the Scheme:

Ref No:	DCA 9
Area:	Hammond Park
Provisions:	<p>All landowners within DCA 9 shall make a proportional contribution to the cost of:</p> <ul style="list-style-type: none"> ▪ 61.6% of the cost of widening and upgrading of Hammond Road between Gaebler Road and Rowley Road; and ▪ Make a proportional contribution of 50% of the cost of constructing Rowley Road between the Kwinana Freeway and Hammond Road reservation. <p>All landowners except Lot 51 Rowley Road and Lot 301 Barfield Road within DCA 9 shall make a proportional contribution to the cost of regional drainage infrastructure.</p> <p>In relation to those portions of properties that have been identified on the Southern Suburbs District Structure Plan – Stage 3 for the purpose of a high school, if these sites are not developed for school purposes in the future a proportional contribution to the regional infrastructure will be required.</p> <p>The proportional contribution is to be determined in accordance with the provisions of clause 6.3 and contained on the Development Contribution Plan.</p> <p>Contributions shall be made towards the following items:</p> <ul style="list-style-type: none"> • The purchase of land reserved for Hammond Road under the Metropolitan Region Scheme; • A 50% proportional contribution towards the purchase of a nominal 40 metre wide road reserve for Rowley Road and where necessary to accommodate channelisation at intersections and drainage to the extent that Rowley Road abuts DCA 9 & DCA 10; • Full earthworks; • Construction of a two-lane road and where the reserve width is less than 40 metres wide, kerbing to the verge side of the carriageway shall be provided; • Dual use path (one side only); • Pedestrian crossings (where appropriate at the discretion of the local government); • Drainage infrastructure associated with Hammond Road and Rowley Road;

	<ul style="list-style-type: none"> • Costs associated with the relocation of servicing infrastructure resulting from the implementation of this scheme, where appropriate; • Costs associated with the provision of regional drainage infrastructure; • Costs to administer cost sharing arrangements – preliminary engineering design and costings, valuations, annual reviews and audits and administration costs; • Traffic management devices. <p>All landowners within DCA 9 and DCA 10 shall make a 1% Public Open Space cash-in-lieu contribution to the cost of the purchase of land for the conservation of a wetland in excess of the normal 10% public open space requirement located on Lots 41 Gaebler Road and 42 Frankland Avenue, Hammond Park.</p>
Participants and Contributions:	In accordance with the Cost Contribution Schedule adopted by the local government for DCA 9.

Ref No:	DCA 10
Area:	Wattleup
Provisions:	<p>All landowners within DCA 10 shall make a proportional contribution to the cost of:</p> <ul style="list-style-type: none"> • 38.4% of the cost of widening and upgrading of Hammond Road between Gaebler Road and Rowley Road; and • Make a proportional contribution of 50% of the cost of constructing Rowley Road between the Hammond Road reservation and Lot 81 Wattleup Road. <p>The landowners of Lots 1, 2, 110, and 111 Wattleup Road shall make a proportional contribution towards regional drainage infrastructure.</p> <p>In relation to those portions of properties that have been identified on the Southern Suburbs District Structure Plan – Stage 3 for the purpose of a primary school, if these sites are not developed for school purposes in the future a proportional contribution to the regional infrastructure will be required.</p> <p>The proportional contribution is to be determined in accordance with the provisions of clause 6.3 and contained on the Development Contribution Plan.</p> <p>Contributions shall be made towards the following items:</p> <ul style="list-style-type: none"> • The purchase of land reserved for Hammond Road under the Metropolitan Region Scheme; • A 50% proportional contribution towards the purchase of a nominal 40 metre wide road reserve for Rowley Road and where necessary to accommodate channelisation at intersections and drainage to the extent that Rowley Road abuts DCA 9 & DCA 10; • Full earthworks; • Construction of a two-lane road and where the reserve width is less than 40 metres wide, kerbing to the verge side of the carriageway shall be provided; • Dual use path (one side only);

	<ul style="list-style-type: none"> • Pedestrian crossings (where appropriate at the discretion of the local government); • Drainage infrastructure associated with Hammond Road and Rowley Road; • Costs associated with the relocation of servicing infrastructure resulting from the implementation of this scheme, where appropriate; • Costs associated with the provision of regional drainage infrastructure; • Costs to administer cost sharing arrangements – preliminary engineering design and costings, valuations, annual reviews and audits and administration costs; • Traffic management devices; • Where required, the relocation of servicing infrastructure. <p>All landowners within DCA 9 and DCA 10 shall make a 1% Public Open Space cash-in-lieu contribution to the cost of the purchase of land for the conservation of a wetland in excess of the normal 10% public open space requirement located on Lots 41 Gaebler Road and 42 Frankland Avenue, Hammond Park.</p>
Participants and Contributions:	In accordance with the Cost Contribution Schedule adopted by the local government for DCA 10.

2. Amending the Scheme Map to include Development Contribution Area No. 9 and Development Contribution Area No. 10 accordingly.
3. Modify the Scheme Map to replace Development Area No. 9 – Gaebler Road, south of Gaebler Road with Development Area No. 26 – DA26.
4. Including the following in Schedule 11 – Development Areas of the Scheme:

Ref. No.	Area	Provisions
DA 26	Rowley Road (Development Zone)	<ol style="list-style-type: none"> 1. Local Structure Plan adopted to guide subdivision, land use and development. 2. To provide for Residential development and associated uses including schools, open space, shops and community infrastructure. 3. The provisions of the Scheme shall apply to the land uses classified under the Structure Plan, in accordance with clauses 4.3 and 6.2.6.3. 4. Those uses which may be permitted within the Mixed Business and Local Centre Zones as set out in Table 1 - Zoning Table, and the adopted Southern Suburbs District Structure Plan Stage 3 shall be developed in accordance with the following Design Requirements: <ol style="list-style-type: none"> (a) Building Location <ol style="list-style-type: none"> (i) Development fronting the southern end of Barfield Road (off Rowley Road) and the new road into the future Mandogalup railway station is required to have a nil setback to the street front to provide a 'main street' character to the centre; (ii) Development fronting Hammond Road and the relocated Wattleup Road shown on the

Southern Suburbs District Structure Plan Stage 3 is required to have a nil setback to the street front to provide a 'main street' character to the centre.

(b) Building Form

- (i) Buildings shall be generally contiguous, other than for pedestrian access points or alfresco dining areas. All buildings within the area identified in the mixed business and pedestrian based retail areas shall exhibit a high degree of architectural integrity and avoid the traditional 'big box' supermarket style.
- (ii) The floor plan of all buildings within the area (other than the residential development) shall be sufficiently robust to allow land use change to occur over time.
- (iii) Development on street corners should contain strong architectural landmark elements to reinforce the corner. In particular, development on the corners of Barfield Road and the new road into the future Mandogalup railway station, and Hammond Road and Wattleup Road should provide an 'entry statement' to the centre.
- (iv) Residential development is required to address the public streets to provide streetscape amenity and casual surveillance to the street.
- (v) Where possible, development should be two storeys in height, or where single storey, the façade should be constructed to an equivalent second storey height.
- (vi) All buildings should have pitched roofs of at least 25 degrees.

(c) Materials:

- (i) Materials may comprise a combination of masonry, render and tiles or custom orb sheeting. Facades should be designed to provide for varying textures and articulation to clearly define separate tenancies and reinforce a fine grained character for the area.

(d) Building Frontage:

- (i) The facades of development along Barfield Road, the new road into the future Mandogalup railway station, Hammond Road and Wattleup Road extension shall comprise at least 60% of the façade area below the eave line as clear windows to provide a connection between uses inside the building and activity on the street.

- (ii) Windows shall not be obscured by more than 25% to ensure surveillance to streets and carparks for security purposes, and to minimise adverse impacts on streetscape.

(e) Pedestrian Access/ Amenity:

- (i) Primary access to all tenancies shall be provided from the street, with secondary access permissible from the rear of the development, to encourage activity along the main street, and vibrancy within the area.
- (ii) All development shall have awnings or verandahs along public streets to provide shelter and comfort for pedestrians and encourage use of the public realm.

(f) Vehicle Access:

- (i) Vehicle accesses shall be restricted to those indicated on the final adopted Local Structure Plans for the area.

5. The local government may adopt Design Guidelines for any development precincts as defined on the Structure Plan. All development in such precinct is to be in accordance with the adopted guidelines in addition to any other requirements of the Scheme, and where there is any inconsistency between the design guidelines and the Scheme, the Scheme shall prevail.

6. No subdivision or development of incompatible use will be supported within the buffer areas associated with;

- Market gardens on Lots 39 and 40 Gaebler Road, Lots 114 and 123 Wattleup Road;
- Intensive horticulture on Lot 37 Gaebler Road and Lot 101 Barfield Road;

until these uses cease or the buffer areas are scientifically determined and approved by the Department of Environment. Buffer requirements are to be determined in consultation with the local government and Department of Environment. Buffer areas are to be shown on the Structure Plan.

7. All landowners within DA 26 and DA 27 will be required to provide 1% cash-in-lieu and generally 9% land for public open space. The 1% cash-in-lieu contribution will be used to purchase land for the conservation of a wetland in excess of the normal 10% public open space requirement located on Lots 41 Gaebler Road and 42 Frankland Avenue, Hammond Park.

		<p>8. In the event that Rowley Road is designated as a primary freight route, landowners of lots abutting Rowley Road within DA 26 and DA 27 will be required to construct noise walls in accordance with a qualified acoustic engineers report at the time of subdivision and/or development.</p> <p>9. Landowners of lots within DA 26 abutting the Western Power easement adjacent to the Kwinana Freeway will be required to engage a suitably qualified acoustic engineer to prepare an acoustic report to determine appropriate strategies to ameliorate any noise issues generated by the proximity to the Kwinana Freeway and Railway.</p>
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5. Including the following in Schedule 11 – Development Areas of the Scheme:

Ref. No.	Area	Provisions
DA 27	Wattleup Road (Development Zone)	<p>1. Local Structure Plan adopted to guide subdivision, land use and development.</p> <p>2. To provide for Residential development and associated uses including schools, open space, shops and community infrastructure.</p> <p>3. The provisions of the Scheme shall apply to the land uses classified under the Structure Plan, in accordance with clause 4.3 and 6.2.6.3</p> <p>4. Those uses which may be permitted within the Mixed Business and Local Centre Zones as set out in Table 1 - Zoning Table, and the adopted Southern Suburbs District Structure Plan Stage 3 shall be developed in accordance with the following Design Requirements:</p> <p>(a) Building Location</p> <p>(i) Development fronting Hammond Road and Wattleup Road (other than the residential development) is required to have a nil setback to the street front to provide a 'main street' character to the centre.</p> <p>(b) Building Form</p> <p>(i) Buildings shall be generally contiguous, other than for pedestrian access points or alfresco dining areas. All buildings within the area identified in the mixed business and pedestrian based retail areas shall exhibit a high degree of architectural integrity and avoid the traditional 'big box' supermarket style.</p> <p>(ii) The floor plan of all buildings within the area (other than the residential development) shall be sufficiently robust to allow land use change to occur over time.</p> <p>(iii) Development on street corners should contain strong architectural landmark</p>

		<p>elements to reinforce the corner. In particular, development on the corners of Hammond Road and Wattleup Road should provide an 'entry statement' to the centre.</p> <ul style="list-style-type: none"> (iv) Residential development is required to address the public streets to provide streetscape amenity and casual surveillance to the street. (v) Where possible, development should be two storeys in height, or where single storey, the façade should be constructed to an equivalent second storey height. (vi) All buildings should have pitched roofs of at least 25 degrees. <p>(c) Materials:</p> <ul style="list-style-type: none"> (i) Materials may comprise a combination of masonry, render and tiles or custom orb sheeting. Facades should be designed to provide for varying textures and articulation to clearly define separate tenancies and reinforce a fine grained character for the area. <p>(d) Building Frontage:</p> <ul style="list-style-type: none"> (i) The facades of development along Hammond Road and Wattleup Road shall comprise at least 60% of the façade area below the eave line as clear windows to provide a connection between uses inside the building and activity on the street. (ii) Windows shall not be obscured by more than 25% to ensure surveillance to streets and carparks for security purposes, and to minimise adverse impacts on streetscape. <p>(e) Pedestrian Access/ Amenity:</p> <ul style="list-style-type: none"> (i) Primary access to all tenancies shall be provided from the street, with secondary access permissible from the rear of the development, to encourage activity along the main street, and vibrancy within the area. (ii) All development shall have awnings or verandahs along public streets to provide shelter and comfort for pedestrians and encourage use of the public realm. <p>(f) Vehicle Access:</p> <ul style="list-style-type: none"> (i) Vehicle accesses shall be restricted to those indicated on the final adopted Local Structure Plans for the area.
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| | <p>5. The local government may adopt Design Guidelines for any development precincts as defined on the Structure Plan. All development in such precinct is to be in accordance with the adopted guidelines in addition to any other requirements of the Scheme, and where there is any inconsistency between the design guidelines and the Scheme, the Scheme shall prevail.</p> <p>6. No subdivision or development of incompatible use will be supported within the buffer areas associated with;</p> <ul style="list-style-type: none"> ▪ Poultry farm on Lot 120 Wattleup Road; ▪ Market gardens on Lots 1, 2, 70, 71 and 117 Wattleup Road; ▪ Orchard on Lot 805 Wattleup Road; ▪ Turf farms on Lots 78, 79, 80, 122 and 305 Wattleup Road; ▪ Intensive horticulture on Lot 77 Wattleup Road <p>until these uses cease or the buffer areas are scientifically determined and approved by the Department of Environment. Buffer requirements are to be determined in consultation with the local government and Department of Environment. Buffer areas are to be shown on the Structure Plan.</p> <p>7. All landowners within DA 26 and DA 27 will be required to provide 1% cash-in-lieu and generally 9% land for public open space. The 1% cash-in-lieu contribution will be used to purchase land for the conservation of a wetland in excess of the normal 10% public open space requirement located on Lots 41 Gaebler Road and 42 Frankland Avenue, Hammond Park.</p> <p>8. In the event that Rowley Road is designated as a primary freight route, landowners of lots abutting Rowley Road within DA 26 and DA 27 will be required to construct noise walls in accordance with a qualified acoustic engineers report at the time of subdivision and/or development.</p> |
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6. Amending the Scheme Map to include Development Area No. 27 – DA 27 accordingly.

Dated this 15th day of February 2005

CHIEF EXECUTIVE OFFICER
FILE NO. 93028

REPORT

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|----|--------------------------------------|--|
| 1. | LOCAL AUTHORITY | City of Cockburn |
| 2. | DESCRIPTION OF TOWN PLANNING SCHEME: | Town Planning Scheme No 3 |
| 3. | TYPE OF SCHEME: | District Zoning Scheme |
| 4. | SERIAL NO. OF AMENDMENT: | Amendment No. 28 |
| 5. | PROPOSAL: | <ol style="list-style-type: none">1. Introducing two new Development Contribution Areas to the Hammond Park/Wattleup area (DCA9 – Hammond Park and DCA10 – Wattleup).2. Amending the Scheme Map to introduce DCA 9 and DCA10.3. Introducing two new Development Areas to the Hammond Park/Wattleup area (DA26 – Rowley Road and DA27 – Wattleup Road).4. Amending the Scheme Map to replace part of DA9 – Gaebler Road, south of Gaebler Road with DA 26 and introducing DA27). |

Note: This Amendment Report should be read in conjunction with the draft Southern Suburbs District Structure Plan – Stage 3 – Hammond Park/Wattleup.

SCHEME AMENDMENT REPORT

Background

In October 1999 the Council and the Western Australian Planning Commission adopted Southern Suburbs District Structure Plan (Attachment 1 – Southern Suburbs District Structure Plan) for that part of the newly developing urban area north of Gaebler Road, Hammond Park and Gibbs Road, Success. This area has now largely been developed with the remaining pockets in the process of being developed or local structure plans being prepared.

In October 2003 the Council and the Western Australian Planning Commission adopted the Southern Suburbs District Structure Plan – Stage 2 – Banjup (Attachment 2 – Southern Suburbs District Plan – Stage 2 - Banjup) for the new urban development area on the eastern side of the Kwinana Freeway between Gibbs Road, Aubin Grove and Rowley Road, Aubin Grove. This area is also experience rapid development with the Sanctuary (Stocklands) and LWP creating new estates in the area. The City is still receiving a number of local structure plans and subdivision applications for this area.

Both District Structure Plans show the broad land use framework including major road network, neighbourhood structure, commercial, education, community and significant open space areas and have been successfully used to guide the local structure planning process in these areas.

Council's Strategic Planning Services has prepared a draft Southern Suburbs District Structure Plan – Stage 3 – Hammond Park/Wattleup (SSS3) for the 'Urban Deferred' area on the western side of the Kwinana Freeway, south of Gaebler Road, Hammond Park to Rowley Road, Aubin Grove and from the Freeway west along Wattleup Road (Attachment 3 – Draft Southern Suburbs District Structure Plan – Stage 3 – Hammond Park/Wattleup). Again this Plan is to show the broad land use framework including major road network, neighbourhood structure, commercial, education, community and significant open space areas and to guide the local structure planning process for the area.

At its meeting on the 15 February 2005 Council resolved to adopt the draft SSS3 Plan for the purposes of public consultation in accordance with Part 6 of Town Planning Scheme No. 3.

Whilst the SSS3 area is zoned 'Development' under Council's Town Planning Scheme No. 3 a number of modifications and new schedules are proposed to ensure the implementation of the District Structure Plan including the introduction of two new Development Areas and two new Development Contribution Areas.

Report

Amendment No. 28 to Town Planning Scheme No. 3 proposes the following:

1. To introduce 'Development Contribution Area No. 9 – Hammond Park' to Schedule 12 to require contributions towards:
 - the widening and upgrading of Hammond Road between Gaebler Road and Rowley Road;
 - to make a 50% proportional contribution towards the purchase of a nominal 40 metre wide road reserve for Rowley Road and where

- necessary to accommodate channelisation at intersections and drainage to the extent that Rowley Road abuts DCA 9 & DCA 10;
 - to make a 1% Public Open Space cash-in-lieu contribution to the cost of the purchase of land for the conservation of a wetland in excess of the normal 10% public open space requirement located on Lots 41 Gaebler Road and 42 Frankland Avenue, Hammond Park; and
 - to ensure a coordinated approach is taken to regional drainage in the area with all developers, except Lot 51 Rowley Road and Lot 301 Barfield Road, contributing to the provision of the drainage infrastructure.
- 2. To introduce 'Development Contribution Area No. 10 – Wattleup' to Schedule 12 to require contributions towards:
 - the widening and upgrading of Hammond Road between Gaebler Road and Rowley Road;
 - to make a 50% proportional contribution towards the purchase of a nominal 40 metre wide road reserve for Rowley Road and where necessary to accommodate channelisation at intersections and drainage to the extent that Rowley Road abuts DCA 9 & DCA 10;
 - to make a 1% Public Open Space cash-in-lieu contribution to the cost of the purchase of land for the conservation of a wetland in excess of the normal 10% public open space requirement located on Lots 41 Gaebler Road and 42 Frankland Avenue, Hammond Park; and
 - to ensure a coordinated approach is taken to regional drainage in the area with the landowners of Lots 1, 2, 110 and 111 Wattleup Road, contributing to the provision of the drainage infrastructure.
- 3. To modify Development Area No. 9 – Gaebler Road to end at Gaebler Road and to introduce a new 'Development Area No. 26 – Rowley Road', south of Gaebler Road.
- 4. To introduce 'Development Area No. 27 – Wattleup Road' for that land zoned 'Urban Deferred' from the Hammond Road reservation west to Lot 81 Wattleup Road.

Justification for Amendment

Introduction of Development Contribution Area No. 9 and 10

As a result of the future development of the SSS3 area there is a need for development contributions towards regional infrastructure such as the widening and upgrading of Hammond Road and Rowley Road and regional drainage infrastructure. To facilitate the collection of development contributions for the regional infrastructure it is proposed to introduce two new development contributions to the area, these being DCA 9 and DCA 10. The development contributions have been split into two areas as it is likely that development of DCA 9 will occur first given that this area is less affected by buffers from existing uses such as market gardens, poultry farms and turf farms.

Development contributions for the upgrading of Hammond Road and Rowley Road

Contributions being sought for the upgrading of Hammond Road have been split into percentage contributions based on the developable area of the DCA's contributing to the need for the upgrading of Hammond Road. DCA 9 has 130.4ha of developable land and therefore will contribute 61.6% towards the cost for the upgrading and widening of Hammond Road. DCA 10 has 79.9ha of developable land and therefore will contribute 38.4% towards the cost for the upgrading and widening of Hammond Road.

Contributions being sought for the upgrading of Rowley Road are proposed to be split equally between City of Cockburn developers and Town of Kwinana developers given that this road straddles both Council's. The timing for the widening and upgrading of Rowley Road is unknown at this stage and will be required to service the Hope Valley/Wattleup industrial area and possibly the outer harbour.

Development contributions for the provision of regional drainage infrastructure

As with Amendment No. 17 to Town Planning Scheme No. 3 the introduction of DCA 9 and DCA 10 to Schedule 12 of Town Planning Scheme No. 3 will ensure that a coordinated approach is taken to the regional infrastructure requirements for the SSS3 area, with all developers contributing to the provision of the infrastructure.

To assist the City to resolve technical issues associated with the provision of a stormwater drainage system to service the SSS3 area, the City engaged David Wills and Associates to prepare the 'Southern Suburbs District Structure Plan Area – Russell Road Arterial Drain Scheme' (December 2002).

To facilitate the urban development east of Beelihar Regional Park the Water Corporation agreed to construct the Southern Lakes Main Drainage Scheme. After considerable negotiation with various government departments the final agreed main drainage system was to consist of three major "buffer lakes" (used for nutrient stripping and compensating effects) and a gravity pipe system to convey stormwater discharge from the proposed future urban area adjacent to Thomsons Lake to the degraded Yangebup Lake to the north. From Yangebup Lake, stormwater in excess of specified operating and environmental criteria was to be then pumped into Cockburn Sound.

Additional drainage pump stations to control the rise in the water levels in various lake systems adjacent to the urban areas are also required to be constructed to assist in meeting the environmental criteria set out in the Southern Lakes Main Drainage Strategy Environmental Management Plan which was approved by the Department of Environmental Protection in 1988.

Within the SSS3 area, contributions for regional drainage infrastructure will be collected for the construction of a groundwater control point (swale) on Gaebler Road (within Lot 412 Gaebler Road). Some drainage from the development of land to the south of Gaebler Road will flow directly into this groundwater control point and the remainder of the DCA 9 area and part of DCA 10 will benefit from lower ground water levels requiring less fill to develop the land.

The ground water control point will be constructed by Gold Estates as part of the development of Lot 412 Gaebler Road, who will then be paid for a proportion of costs of the construction of this ground water control point from funds collected within DCA 9 and 10.

As with Amendment No. 17 the area defined for the collection of development contributions for drainage infrastructure for DCA 9 and 10 is based on the declared water corporation drainage boundary (Attachment 4 – Arterial Drainage Infrastructure Plan for the area)

Not all of the land within DCA 9 and 10 is within the declared water corporation drainage boundary and accordingly the lots required to contribute to drainage infrastructure have been clearly spelt out in DCA 9 and 10.

Conclusion

The City already has seven (7) other Development Contribution Areas within the Town Planning Scheme (and further proposed) which are used to collect contributions from developers within the designated area for similar purposes as proposed above. All the required provisions required to manage Development Contribution Areas already exist within the Scheme and Amendment No. 28 proposes to introduce 'Development Contribution Area No. 9' and 'Development Contribution Area No. 10' into Schedule 12 and amend the Scheme Map to identify the area. Western Australian Planning Commission Planning Bulletins No. 18 and 37 outline the requirements for the application of development contribution areas and what works can be included in the contributions. The proposed works comply with both Planning Bulletins.

Modifications to Development Area No. 9 and Introduction of Development Area No. 26

TPS3 shows the area south of Gaebler Road to Rowley Road as Development Area No. 9. Given the development to occur in this area as a result of SSS3, it is proposed to replace DA 9 south of Gaebler Road with DA 26. An additional Schedule is proposed to be included in Schedule 11 – Development Areas of the Scheme outlining the objective and requirements of the area including 'mainstreet' design principles for the centres designated on the District Structure Plan. Requirements for 'mainstreet' design principles are currently being introduced into TPS 3 for the Frankland Springs Estate on Russell Road in Hammond Park through Scheme Amendment No. 22. Similar principles are proposed within DA 26 and 27.

The Schedule also recognises incompatible uses in the area, such as a poultry farm, market gardens and intensive horticulture, which prevent parts of the area being developed for residential purposes until these uses have ceased or the buffer areas are scientifically determined and approved by the Department of Environment (Attachment 5 - Opportunities and Constraints Map for Southern Suburbs Stage 3 area).

Introduction of Development Area No. 27

TPS 3 shows the western portion of SSS3 being zoned 'Development' however it does not contain a development area number. It is unclear why this has occurred and it is recommended that 'Development Area No. 27' be designated to the area. This will also require a Schedule to be included into Schedule 11 – Development Areas of the Scheme outlining the objective and requirements of this area including 'mainstreet' design principles for the centres designated on the District Structure Plan. The proposed Schedule is in keeping with others included in this section of the Scheme and is for administrative purposes.

Again the proposed Schedule recognises incompatible uses in the area and prevents a large portion of the area being developed for residential purposes until these uses have ceased or the buffer areas are scientifically determined and approved by the Department of Environment.

Conclusion

Amendment No. 28 proposes a number of modifications to Town Planning Scheme No. 3 to ensure the implementation of the Southern Suburbs District Structure Plan – Stage 3 – Hammond Park/Wattleup. These modifications are essential to ensure the co-ordinated and equitable approach to the provision of regional infrastructure and to guide the preparation of local structure plans. The proposed amendment is in keeping with the current provisions of Town Planning Scheme No. 3 and complies with Planning Bulletins No. 18 and 37.

UPDATES AND MODIFICATIONS TO DCA 9 & DCA 10 AND DA 26 & DA 27 PROVISIONS – APRIL 2012

Since Amendment No. 28 was originally initiated in 2005, assessment by officers of the Department of Planning (“DoP”) and a number of associated contributing factors have led to the need for the City of Cockburn (“City”) to modify the Amendment provisions and readvertise the proposal in accordance with the directive given by the DoP in February 2012. In addition, a comprehensive review of the associated Southern Suburbs District Structure Plan – Stage 3 (“SSDSP3”) has been undertaken to reflect the modified Amendment provisions. In particular, the following factors have influenced the requirement to update Amendment 28 and the SSDSP3:

- relocation of the future Mandogalup Train Station south of Rowley Road,
- Rowley Road designated as a strategic freight route,
- extension of the Kwinana Industrial (including Air Quality) Buffer,
- retention of Hammond Road extension alignment through Frankland Reserve.

In addition to the above, the timing of the update meant that it was necessary to ensure Amendment 28 and SSDSP3 are consistent with all current government strategic planning initiatives including Statement of Planning Policy 3.6 - Development Contributions for Infrastructure (“SPP 3.6”), Directions 2031 and beyond, Liveable Neighbourhoods – Edition 4) etc.

The specific modifications to Development Contribution Plan (“DCP”) and Development Area (“DA”) provisions are detailed below:

DCA 9 – Hammond Park	
Modification	Justification
Specification of method of calculating cost contributions, period of operation, priority and timing for the provision of infrastructure and review process	SPP3.6 was gazetted in November 2009 and prescribes the principles underlying development contributions and the form, content and process for the preparation of a development contribution plan under a local planning scheme. As such, the DCP has been modified to adhere to the requirements of SPP3.6.
Removal of requirement to contribute towards construction of Rowley Road between the Kwinana Freeway and Hammond Road reservation	Planning Control Area No. 95 has been designated over the alignment of Rowley Road and the Western Australian Planning Commission (“WAPC”) has purchased some of the land required for the road. Given it is likely this road will become a freight route servicing the Latitude 32 industrial development and the proposed outer harbour, it is not considered appropriate for developers within the SSDSP3 area to contribute to the construction of Rowley Road. Acquisition, funding, design and construction of Rowley Road are therefore the responsibility of the State Government given the road will be a ‘Primary Regional Road’ (red road).
Addition of requirement to contribute towards ‘full earthworks’ in relation to	This requirement has been added in order to maintain consistency with DCP 10 and is a standard requirement relating to the upgrading

Hammond Road upgrading	and widening of regional roads.
Removal of requirement to make 1% POS cash-in-lieu contribution toward purchase of Lots 41 Gaebler Road and 42 Frankland Avenue, Hammond Park	The Department of Planning ("DoP") advised that the inclusion of POS as a DCP item cannot be supported as conservation assets are not POS and the <i>Planning and Development Act 2005</i> does not provide for cash-in-lieu to be expended on the acquisition of conservation assets.
Addition of provision requiring contributions to be made toward the relocation of servicing infrastructure, where required.	This requirement has been added in order to maintain consistency with DCP 10 and is a standard requirement relating to the development and/or upgrading of infrastructure in greenfield areas.

DCA 10 – Wattleup	
Modification	Justification
Specification of method of calculating cost contributions, period of operation, priority and timing for the provision of infrastructure and review process	SPP3.6 was gazetted in November 2009 and prescribes the principles underlying development contributions and the form, content and process for the preparation of a development contribution plan under a local planning scheme. As such, the DCP has been modified to adhere to the requirements of SPP3.6.
Removal of requirement to contribute towards construction of Rowley Road between the Hammond Road reservation and Lot 81 Wattleup Road	Planning Control Area No. 95 has been designated over the alignment of Rowley Road and the WAPC has purchased some of the land required for the road. Given it is likely this road will become a freight route servicing the Latitude 32 industrial development and the proposed outer harbour, it is not considered appropriate for developers within the SSDSP3 area to contribute to the construction of Rowley Road. Acquisition, funding, design and construction of Rowley Road are therefore the responsibility of the State Government given the road will be a 'Primary Regional Road' (red road).
Removal of requirement to make 1% POS cash-in-lieu contribution toward purchase of Lots 41 Gaebler Road and 42 Frankland Avenue, Hammond Park	The Department of Planning ("DoP") advised that the inclusion of POS as a DCP item cannot be supported as conservation assets are not POS and the <i>Planning and Development Act 2005</i> does not provide for cash-in-lieu to be expended on the acquisition of conservation assets.

DA 26 – Rowley Road	
Modification	Justification
Removal of Provision 4. Mixed Business/Local Centre design requirements	Section 5.8 of the SSDSP3 details the requirements for the development of the neighbourhood centre and neighbourhood nodes within the Development Area. The design requirements will be further refined through the Structure Plan process.

Removal of Provision 5. Design Guidelines for development precincts	Sections 5.1 and 5.4 of the SSDSP3 provide guidance with regards to the City's objectives for the design of future subdivision and development. Future Structure Plans and Detailed Area Plans will provide further opportunities to refine the character of the Development Area.
Removal of Provision 6. Buffer requirements for Market garden and intensive horticulture uses	The majority of the sites listed within the provision have ceased operation and buffer requirements are detailed within Section 4.1.1 of the SSDSP3 and the WAPC's Planning Bulletin 63. The refinement of buffers and/or mitigation measures will be determined through the Structure Plan process.
Removal of Provision 7. 1% POS cash-in-lieu contribution for Lots 41 Gaebler Road and 42 Frankland Avenue, Hammond Park	The DoP advised that the inclusion of POS as a DCP item cannot be supported as conservation assets are not POS and the <i>Planning and Development Act 2005</i> does not provide for cash-in-lieu to be expended on the acquisition of conservation assets. Landowners will be required to provide 10% POS as per section 5.6.2 of the SSDSP3 and Liveable Neighbourhoods.
Removal of Provision 8. Rowley Road noise wall requirements	The SSDSP3 specifies that future residential development shall not directly abut Rowley Road. Future Structure Plans must demonstrate a suitable interface treatment being provided to the future Rowley Road freight access route. The construction of noise walls will be the responsibility of the State Government given Rowley Road's designation as a strategic freight route. SPP5.4 also specifies requirements with regards to future freight routes and sensitive land uses.
Removal of Provision 9. Kwinana Freeway and Railway noise amelioration requirements	Future Structure Plans for land in the vicinity of the Kwinana Freeway will need to demonstrate compliance with SPP5.4. It is noted this provision was developed prior to the gazettal of SPP5.4.

DA 27 – Wattleup Road	
Modification	Justification
Removal of Provision 4. Mixed Business/Local Centre design requirements	Section 5.8 of the SSDSP3 details the requirements for the development of the neighbourhood centre and neighbourhood nodes within the Development Area. The design requirements will be further refined through the Structure Plan process.
Removal of Provision 5. Design Guidelines for development precincts	Sections 5.1 and 5.4 of the SSDSP3 provide guidance with regards to the City's objectives for the design of future subdivision and development. Future Structure Plans and Detailed Area Plans will provide further opportunities to refine the character of the Development Area.

<p>Removal of Provision 6. Buffer requirements for Market garden, poultry farm, orchard, turf farm and intensive horticulture uses</p>	<p>The majority of the sites listed within the provision have ceased operation and buffer requirements are detailed within Section 4.1.1 of the SSDSP3 and the WAPC's Planning Bulletin 63. The refinement of buffers and/or mitigation measures will therefore be determined through the Structure Plan process.</p>
<p>Removal of Provision 7. 1% POS cash-in-lieu contribution for Lots 41 Gaebler Road and 42 Frankland Avenue, Hammond Park</p>	<p>The DoP advised that the inclusion of POS as a DCP item cannot be supported as conservation assets are not POS and the <i>Planning and Development Act 2005</i> does not provide for cash-in-lieu to be expended on the acquisition of conservation assets. Landowners will be required to provide 10% POS as per section 5.6.2 of the SSDSP3 and Liveable Neighbourhoods.</p>
<p>Removal of Provision 8. Rowley Road noise wall requirements</p>	<p>The SSDSP3 specifies that future residential development shall not directly abut Rowley Road. Future Structure Plans must demonstrate a suitable interface treatment being provided to the future Rowley Road freight access route. The construction of noise walls will be the responsibility of the State Government given Rowley Road's designation as a strategic freight route. SPP5.4 also specifies requirements with regards to future freight routes and sensitive land uses.</p>

In addition to the above modifications, the southern boundaries of DCA's 9 and DCA 10 and DA's 26 and 27 have been modified on the proposed Scheme zoning map to reflect Planning Control Area 95 - Rowley Road extension.

SCHEME AMENDMENT

PLANNING AND DEVELOPMENT ACT, 2005
CITY OF COCKBURN
TOWN PLANNING SCHEME NO. 3
AMENDMENT NO. 28

The City of Cockburn under and by virtue of the powers conferred upon in that behalf by the *Planning and Development Act 2005*, hereby amend the above Town Planning Scheme by:

1. Amending Schedule 12 - Development Contribution Plan contained in the scheme text by including DCA 9 – Hammond Park and DCA 10 – Wattleup as follows.

Schedule 12 Development Contribution Plan

Ref No:	DCA 9
Area:	Hammond Park
Infrastructure and administrative items to be funded:	<p>Contributions shall be made towards the following items:</p> <ul style="list-style-type: none"> ▪ Proportional (61.6%) cost of widening and upgrading of Hammond Road between Gaebler Road and Rowley Road including: <ul style="list-style-type: none"> ○ Construction of one carriage way comprised of two lanes for Hammond Road and where the reserve width is less than 40 metres wide, kerbing to the verge side of the carriageway shall be provided; ○ The purchase of land reserved for Hammond Road under the Metropolitan Region Scheme; ○ Full earthworks; ○ Dual use path (one side only); ○ Pedestrian crossings (where appropriate at the discretion of the local government); ○ Land and infrastructure associated with the drainage of Hammond Road; ○ Costs associated with the relocation of servicing infrastructure resulting from the implementation of this scheme, where appropriate; ○ Traffic management devices. • Costs associated with the provision of regional drainage infrastructure; • Costs to administer cost sharing arrangements – preliminary engineering design and costings, valuations, annual reviews and audits and administration costs.
Method for calculating contributions:	<p>All landowners within DCA 9 shall make a proportional contribution to 61.6% of the cost of widening and upgrading of Hammond Road between Gaebler Road and Rowley Road.</p> <p>All landowners except Lot 51 Rowley Road and Lot 301 Barfield Road within DCA 9 shall make a proportional contribution to the cost of regional drainage infrastructure.</p>

	<p>The proportional contribution is to be determined in accordance with the provisions of clause 6.3 and contained on the Development Contribution Plan.</p> <p>Contributions shall be calculated on a per hectare basis.</p>
Period of Operation:	Until 30 June 2027. However the DCP may also be extended for further periods with or without modification by subsequent Scheme Amendments.
Priority and Timing:	In accordance with the City of Cockburn DCA9 and DCA10 Capital Expenditure Plan.
Review Process:	<p>The plan will be reviewed when considered appropriate, though not exceeding a period of five years duration, having regard to the rate of subsequent development in the development contribution area since the last review and the degree of development potential still existing.</p> <p>The estimated infrastructure costs contained in the Hammond Park Cost Contribution Schedule will be reviewed at least annually to reflect changes in funding and revenue sources and indexed based on the Building Cost Index or other appropriate index as approved by an appropriately qualified independent person.</p>
Participants and Contributions:	In accordance with the Cost Contribution Schedule adopted by the local government for DCA 9.

Ref.No:	DCA 10
Area:	Wattleup
Provisions:	<p>Contributions shall be made towards the following items:</p> <ul style="list-style-type: none"> ▪ Proportional (38.4%) cost of widening and upgrading of Hammond Road between Gaebler Road and Rowley Road including: <ul style="list-style-type: none"> ○ Construction of one carriage way comprised of two lanes for Hammond Road and where the reserve width is less than 40 metres wide, kerbing to the verge side of the carriageway shall be provided; ○ The purchase of land reserved for Hammond Road under the Metropolitan Region Scheme; ○ Full earthworks; ○ Dual use path (one side only); ○ Pedestrian crossings (where appropriate at the discretion of the local government); ○ Land and infrastructure associated with the drainage of Hammond Road; ○ Costs associated with the relocation of servicing infrastructure resulting from the implementation of this scheme, where appropriate; ○ Traffic management devices. • Costs associated with the provision of regional drainage infrastructure; • Costs to administer cost sharing arrangements – preliminary engineering design and costings, valuations, annual reviews and audits and administration costs.
Method for	All landowners within DCA 10 shall make a proportional contribution to

calculating contributions:	<p>38.4% of the cost of widening and upgrading of Hammond Road between Gaebler Road and Rowley Road.</p> <p>The landowners of Lots 1, 2, 110, and 111 Wattleup Road shall make a proportional contribution towards regional drainage infrastructure.</p> <p>The proportional contribution is to be determined in accordance with the provisions of clause 6.3 and contained on the Development Contribution Plan.</p> <p>Contributions shall be calculated on a per hectare basis.</p>
Period of Operation:	Until 30 June 2027. However the DCP may also be extended for further periods with or without modification by subsequent Scheme Amendments.
Priority and Timing:	In accordance with the City of Cockburn DCA9 and DCA10 Capital Expenditure Plan.
Review Process:	<p>The plan will be reviewed when considered appropriate, though not exceeding a period of five years duration, having regard to the rate of subsequent development in the development contribution area since the last review and the degree of development potential still existing.</p> <p>The estimated infrastructure costs contained in the Wattleup Cost Contribution Schedule will be reviewed at least annually to reflect changes in funding and revenue sources and indexed based on the Building Cost Index or other appropriate index as approved by an appropriately qualified independent person.</p>
Participants and Contributions:	In accordance with the Cost Contribution Schedule adopted by the local government for DCA 10.

2. Amending the Scheme Map to include Development Contribution Area No. 9 and Development Contribution Area No. 10 accordingly.
3. Including the following in Schedule 11 – Development Areas of the Scheme:

Ref. No.	Area	Provisions
DA 26	Rowley Road (Development Zone)	<ol style="list-style-type: none"> 1. Structure Plan/s adopted and endorsed in accordance with Clause 6.2 of the Scheme to guide subdivision, land use and development. 2. To provide for residential development and compatible land uses. 3. The provisions of the Scheme shall apply to the land uses classified under the Structure Plan in accordance with Clause 6.2.6.3.

Ref. No.	Area	Provisions
DA 27	Wattleup Road (Development Zone)	<ol style="list-style-type: none"> 1. Structure Plan/s adopted and endorsed in accordance with Clause 6.2 of the Scheme to guide subdivision, land use and development. 2. To provide for residential development and compatible land uses. 3. The provisions of the Scheme shall apply to the land uses classified under the Structure Plan in accordance with Clause 6.2.6.3.

4. Modify the Scheme Map to replace the existing Development Area No. 9 – DA9, south of Gaebler Road, with Development Area No. 26 – DA26.
5. Modify the Scheme Map to replace the existing Development Area (unnumbered), west of the Hammond Road extension, with Development Area No. 27 – DA 27.

ADOPTION

Adopted by resolution of the Council of the City of Cockburn at the ordinary meeting of the Council held on 15th day of February 2005

MAYOR

CHIEF EXECUTIVE OFFICER

FINAL APPROVAL

Adopted for final approval by resolution of the City of Cockburn at the ordinary meeting of Council held on the _____ day of _____ 201____, and the Common Seal of the Municipality was pursuant to that resolution hereunto affixed in the presence of:

MAYOR

DATE

(Seal)

CHIEF EXECUTIVE OFFICER

DATE

RECOMMENDED/SUBMITTED FOR FINAL APPROVAL BY THE WESTERN AUSTRALIAN PLANNING COMMISSION

DELEGATED UNDER s16 PLANNING AND DEVELOPMENT ACT 2005

DATE

FINAL APPROVAL GRANTED

MINISTER FOR PLANNING

DATE

Insert Existing Zoning Map

Insert Proposed Zoning Map

ATTACHMENT ONE

Insert Southern Suburbs District Structure Plan Stage 1

ATTACHMENT TWO

Insert Southern Suburbs District Structure Plan Stage 2

ATTACHMENT THREE

Insert Draft Southern Suburbs District Structure Plan Stage 3

ATTACHMENT FOUR

Insert Russell Road Arterial Drainage Scheme Report Plan

ATTACHMENT FIVE

Insert SSDSP3 Opportunities and Constraints Plan

DRAFT COST CONTRIBUTION SCHEDULE – DCA 9 HAMMOND PARK (as at April 2012)

Description

All owners of land within DCA 9 are required to make a proportional contribution to 61.6% of the cost of widening and upgrading Hammond Road between Gaebler Road and Rowley Road and all land owners with the exception of lot 51 Rowley Road and lot 301 Barfield Road shall make a proportional contribution to the cost of regional drainage infrastructure.

Specific details of the contributions are set out in TPS 3 Schedule 12 - DCA 9 Hammond Park.

LAND ACQUISITION FOR HAMMOND ROAD		
Prorata contribution to 61.6% land acquisition (Incl widenings and drainage)	As per Schedule 1	1,708,127.34
CONSTRUCTION OF HAMMOND ROAD		
Prorata contribution to 61.6% of the total construction cost	As per Schedule 2	1,935,602.90
ADMINISTRATION COSTS		
General	As per Schedule 3	2,750.00
Annual	As per Schedule 3	100,710.00
Total Cost		3,747,190.24

	Area	Hammond Rd
Totals	130.4	\$3,747,190.24
Less previous contributions as per Schedule 4	0.000	0.00
Balance	130.400	3,747,190.24

Contribution for the area for which Cost Contributions have yet to be made for Hammond Rd \$28,736.12 per ha

Contribution for the area for which Cost Contributions have yet to be made for regional drainage as per Schedule 5 \$787.93 per ha

SUMMARY OF COST CONTRIBUTIONS

Item		Lots 51 & 301	Balance DCA 9
Hammond Road	As above	28,736.12	28,736.12
Regional Drainage	As per Schedule 5	Nil	787.93
Total cost per ha		\$28,736.12	\$29,524.05

SCHEDULE 1 - HAMMOND ROAD LAND COSTS

Description	Ownership	Area (ha)	Contra Value	Purchase Price	Estimated Value	Total Cost	DCA 9	DCA 10
MRS RESERVE							61.60%	38.40%
Lot 41 Gaebler Road	Broad Vision Projects Pty Ltd	0.2538			291,043	291,043	179,282.49	111,760.51
Lot 42 Frankland Ave	Beauchamps Nom, Fobbing Hall P/L	0.2555			293,023	293,023	180,502.17	112,520.83
Lot 43 Frankland Ave	Beauchamps Nom, Fobbing Hall P/L	0.2616			300,008	300,008	184,804.93	115,203.07
Lot 44 Frankland Ave	Beauchamps Nom, Fobbing Hall P/L	0.2490			285,518	285,518	175,879.09	109,638.91
Lot 114 Wattleup Rd	A & I Gaglia	1.5377			1,522,763	1,522,763	938,022.01	584,740.99
Lot 2 Wattleup Rd	D & G Ellement	0.0814			80,579	80,579	49,636.66	30,942.34
Res 27057 Wattleup Rd	City of Cockburn	1.1730			Nil	0	0.00	0.00
	Totals	3.8120	0	0	2,772,934	2,772,934	1,708,127.34	1,064,806.66

Note 1: Land already acquired, confirmation of costs required.

Note 1

SCHEDULE 2 - HAMMOND ROAD CONSTRUCTION COSTS

Description		Actual Cost	Estimated Cost	Total Cost	DCA 9	DCA 10
Construction - Gaebler Rd to Rowley Rd	Maunsell June 06		2,642,212.50	2,642,212.50	61.60%	38.40%
Traffic Lights (Hammond/Wattleup Roads)			500,000.00	500,000.00	1,627,602.90	1,014,609.60
	Totals		3,142,212.50	3,142,212.50	1,935,602.90	1,206,609.60

Note 1: estimated costs updated from 2006 estimate using ABS Engineering Construction Activity Index

Note 1

SCHEDULE 3 - ADMINISTRATION COSTS

GENERAL

		Total Cost
Land valuations 06/05	McGees	750.00
Hammond Rd cost estimates 06/05	Maunsell	2,000.00
		2,750.00

ANNUAL

		Total Cost
Land revaluation	McGees 10 years @ \$1000	10,000.00
Hammond Rd construction cost	Maunsell 10 years @ \$1500	15,000.00
Regional Drainage works lot 412	GHD consulting eng 10 years @ \$1000	10,000.00
Audit costs	Barrett and Partners 10 years @ \$700	7,000.00
Administration	City of Cockburn 10 years @ \$5871	58,710.00
		100,710.00

SCHEDULE 4 - PREVIOUS CONTRIBUTIONS

Owner	Date	Amount Paid		Hammond Rd		Regional Drainage		
		Area	Contribution	Area	Contribution	Area	Contribution	
Gold Estates	Sep-11	122.7375		122.7375		175.7559	149,429.99	Note 1
Total		0.0000	\$0.00	0.0000	\$0.00	0.0000	\$149,429.99	
Area for which contributions are yet to be made		122.7375		122.7375		175.7559		

Note 1: Gold Estates completed construction of the Groundwater Interception System on 23/09/11. Reimbursement/credit is required from DCA9 and DCA10.

SCHEDULE 5 - REGIONAL DRAINAGE

ORIGINAL CONTRIBUTING AREA

Lot 412 Gaebler Rd (Gold Estates) excl road widening and primary school site	43.6036
DCA 9 except lot 51 Rowley Rd (4.5745ha net) and lot 301 Barfield Rd (1.5034ha net)	116.6596
DCA 10 lots 1, 2, 110 and 111 Wattleup Road only	15.4927
Total	175.7559

Developed area as per Schedule 4 0.0000

Area for which contributions are yet to be made As per Schedule 4 175.7559

COST OF DRAINAGE WORKS

		Estimated Cost	Actual Cost	Total Cost
Ground Water interception works Lot 412 Gaebler Rd	GHD 11/12		149,429.99	149,429.99
Oversize drainage area - Lot 412 Gaebler Rd - 0.24 ha	Mcgees 09/10	264,000.00		264,000.00
Total				\$413,429.99

Contributions as per Schedule 4 0.00

Cost contributions yet to be made \$413,429.99

Contribution for the area for which Cost
Contributions have yet to be made for Regional Drainage \$2,352.30 per ha

DRAFT COST CONTRIBUTION SCHEDULE – DCA 10 WATTLEUP (as at April 2012)

Description

All owners of land within DCA 10 are required to make a proportional contribution to 38.4% of the cost of widening and upgrading Hammond Road between Gaebler Road and Rowley Road. The owners of lots 1, 2, 110, & 111 Wattleup Road shall make a proportional contribution to the cost of regional drainage infrastructure.

Specific details of the contributions are set out in TPS 3 Schedule 12 - DCA 10 Wattleup.

LAND ACQUISITION FOR HAMMOND ROAD		
Prorata contribution to 38.4% land acquisition (Incl widenings and drainage)	As per Schedule 1	1,064,806.66
CONSTRUCTION OF HAMMOND ROAD		
Prorata contribution to 38.4% of the total construction cost	As per Schedule 2	1,206,609.60
ADMINISTRATION COSTS		
General	As per Schedule 3	2,750.00
Annual	As per Schedule 3	100,710.00
Total Cost		2,374,876.26

	Area	Hammond Rd
Totals	79.9	\$2,374,876.26
Less previous contributions as per Schedule 4	0.000	0.00
Balance	79.900	2,374,876.26

Contribution for the area for which Cost Contributions have yet to be made for Hammond Rd

\$29,723.11 per ha

Contribution for the area for which cost contributions have yet to be made for regional drainage as per Schedule 5

\$787.93 per ha

SUMMARY OF COST CONTRIBUTIONS

Item		Lot 1,2,110&111	Balance DCA 10
Hammond Road	As above	29,723.11	29,723.11
Regional Drainage	As per Schedule 5	787.93	Nil
	Total cost per ha	\$30,511.04	\$29,723.11

SCHEDULE 1 - HAMMOND ROAD LAND COSTS

Description	Ownership	Area (ha)	Contra Value	Purchase Price	Estimated Value	Total Cost	DCA 9 61.60%	DCA 10 38.40%
MRS RESERVE								
Lot 41 Gaebler Rd	Broad Vision Projects Pty Ltd	0.2538			291,043	291,043	179,282.49	111,760.51
Lot 42 Frankland Ave	Beauchamps Nom, Fobbing Hall P/L	0.2555			293,023	293,023	180,502.17	112,520.83
Lot 43 Frankland Ave	Beauchamps Nom, Fobbing Hall P/L	0.2616			300,008	300,008	184,804.93	115,203.07
Lot 44 Frankland Ave	Beauchamps Nom, Fobbing Hall P/L	0.2490			285,518	285,518	175,879.09	109,638.91
Lot 114 Wattleup Rd	A & I Gaglia	1.5377			1,522,763	1,522,763	938,022.01	584,740.99
Lot 2 Wattleup Rd	D & G Ellement	0.0814			80,579	80,579	49,636.66	30,942.34
Res 27057 Wattleup Rd	City of Cockburn	1.1730			Nil	0	0.00	0.00
Totals		3.8120	0	0	2,772,934	2,772,934	1,708,127.34	1,064,806.66

Note 1

Note 1: Land already acquired, confirmation of costs required.

SCHEDULE 2 - HAMMOND ROAD CONSTRUCTION COSTS

Description		Actual Cost	Estimated Cost	Total Cost	DCA 9 61.60%	DCA 10 38.40%
Construction - Gaebler Rd to Rowley Rd	Maunself June 06		2,642,212.50	2,642,212.50	1,627,602.90	1,014,609.60
Traffic Lights (Hammond/Wattleup Roads)			500,000.00	500,000.00	308,000.00	192,000.00
Totals			3,142,212.50	3,142,212.50	1,935,602.90	1,206,609.60

Note 1

Note 1: estimated costs updated from 2006 estimate using ABS Engineering Construction Activity Index

SCHEDULE 3 - ADMINISTRATION COSTS

GENERAL

		Total Cost
Land valuations 05/06	McGees	750.00
Hammond Rd cost estimates 05/06	Maunsell	2,000.00
		2,750.00

ANNUAL

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Audit costs	Barrett and Partners 10 years @ \$700	7,000.00
Administration	City of Cockburn 10 years @ \$5871	58,710.00
		100,710.00

SCHEDULE 4 - PREVIOUS CONTRIBUTIONS

Owner	Date	Amount Paid		Hammond Rd		Regional Drainage	
		Area	Contribution	Area	Contribution	Area	Contribution
Gold Estates	Sep-11	77.6254		77.6254		175.7559	149,429.99
Total		0.0000	\$0.00	0.0000	\$0.00	0.0000	\$149,429.99
Area for which contributions are yet to be made		77.6254		77.6254		175.7559	

Note 1: Gold Estates completed construction of the Groundwater Interception System on 23/09/11. Reimbursement/credit is required from DCA9 and DCA10.

SCHEDULE 5 - REGIONAL DRAINAGE

ORIGINAL CONTRIBUTING AREA

Lot 412 Gaebler Rd (Gold Estates) excl road widening and primary school site	43.6036
DCA 9 except lot 51 Rowley Rd (4.5745ha net) and lot 301 Barfield Rd (1.5034ha net)	116.6596
DCA 10 lots 1, 2, 110 and 111 Wattleup Road only	15.4927
Total	175.7559

Developed area as per Schedule 4 0.0000

Area for which contributions are yet to be made As per Schedule 4 175.7559

COST OF DRAINAGE WORKS

		Estimated Cost	Actual Cost	Total Cost
Ground Water interception works Lot 412 Gaebler Rd	GHD 11/12		149,429.99	149,429.99
Oversize drainage area - Lot 412 Gaebler Rd - 0.24 ha	Mcgees 09/10	264,000.00		264,000.00
Total				\$413,429.99

Contributions as per Schedule 4 0.00

Cost contributions yet to be made \$413,429.99

Contribution for the area for which Cost
Contributions have yet to be made for Regional Drainage \$2,352.30 per ha

SCHEDULE OF SUBMISSIONS
DRAFT SOUTHERN SUBURBS DISTRICT STRUCTURE PLAN – STAGE 3 AND
SCHEME AMENDMENT NO. 28 (PROPOSED DEVELOPMENT CONTRIBUTION AREAS 9 & 10 AND DEVELOPMENT AREAS 26 & 27)

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
1	Roberts Day Level 1, 130 Royal Street EAST PERTH WA 6004	<p>SUPPORT (subject to modifications)</p> <p><u>SSDSP3</u></p> <p>1. In lodging the submission we act on behalf of Gold Estates Ltd and the Department of Housing. The subject land held by the respective parties comprises Lots 13, 14 and 18 Barfield Road and Lots 48-51 Frankland Road, which is generally located in the south east corner of the SSDSP3.</p> <p>Generally the SSDSP3 is supported as a broad structure and land use framework for guiding the preparation and approvals of local structure plans and future applications for subdivision. We note and support the intent of the SSDSP3 as outlined in section 1.5 of the report that it proposed that the SSDSP3 is adopted as a "guiding document" that gives "guidance to the assessment of LSP's".</p> <p>Whilst we generally support the SSDSP3, there are matters specific to the subject land in which we provide more detailed comment as outlined below:</p> <p>2. <u>Rowley Road</u></p> <p>We do not support the current wording of plan note 1 nor the associated interpretation on the SSDSP3 which delineates a strip of Public Open Space (POS) along the Rowley Road frontage. This is overly prescriptive and therefore restricts the options that may be considered in preparing the design of the proposed LSP. The SSDSP3 refers to the intent of the document as a "guiding document" and as such should retain the flexibility to allow consideration at the LSP stage of other options, including the potential for a configuration that includes lots backing on to Rowley Road.</p> <p>Based upon previous discussions with Council officers, our understanding is that the present wording is based on a Council perception that:</p> <ul style="list-style-type: none"> (i) lots backing onto a major road leads to "unsightly and generally poor development" and (ii) that a POS strip is desirable along the Rowley Road frontage to facilitate both open space amenity and environmental factors. 	<p>1. Noted.</p> <p>2. Noted.</p>

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		<p>We believe this position is not sustainable in view of the following considerations :</p> <p>A. Rowley Road is a designated 'major freight route' necessitating the construction of a 3m high noise barrier wall to meet noise amelioration requirements. Residential development will therefore be screened from view from passing traffic on Rowley Road irrespective of whether the development is fronting on or backing on to Rowley Road or whether it is 'unsightly' or of high quality.</p> <p>B. Rowley Road is proposed as a 'Primary Regional Road' in the MRS, in recognition of the road's designation as a 'major freight route'. This is the same MRS classification as the Kwinana Freeway where significant developments have been constructed with lots backing onto the freeway reserve, more recent local examples include 'The Walk' at Aubin Grove and 'Honeywood' at Wandii.</p> <p>C. Council has previously supported local structure plans where lots back onto designated major transport routes. Examples include Wattleup Local Structure Plan where lots back onto Rowley Road and Watsons Local Structure Plan where lots back onto the major freight railway (refer attached).</p> <p>D. The provision of a linear form of POS abutting Rowley Road will have limited ecological value and will be compromised further by the combination of its narrow width and impacted by the 'edge effect' where it is generally accepted that the longer the edge of the POS, the larger the area of POS that is likely to be subject to disturbance and subsequent degradation. Confirmation of this advice is provided in the attached correspondence from Env Plan Environmental.</p> <p>E. The placement of a narrow band of POS abutting Rowley Road, principally to provide a green buffer to Rowley Road, will also detract from the POS hierarchy currently being contemplated for the POS in the proposed LSP. POS has been sited specifically in response to the principles identified in the SSDSP3, that in addition to facilitating recreation needs, include the need to preserve natural bushland and incorporate drainage facilities within the open space. Furthermore, the provision of a 20m strip POS as indicated on the SSDSP3 would result in an area of approximately one hectare being isolated, primarily for the purpose of providing a buffer, which represents one-third of the POS requirement for the subject land. In order that matters pertaining to detail design are appropriately considered within the preparation of the local structure plan, we do not support the current wording of Plan Note 1. Therefore, to ensure that flexibility is preserved to allow for all options to be investigated at the LSP stage, it is considered that plan note 1 should be amended to read "Future local structure planning is to demonstrate a suitable interface treatment being provided to the future Rowley Road freight access route" and the SSDSP3 plan amended</p>	<p>2A. Noted. However the original intention of the POS spine abutting Rowley Road was to ensure the visual and acoustic amenity of future residents in the vicinity of this strategic freight route could be preserved.</p> <p>2B. Noted. However the topography of those estates mentioned is different to that in the southern portion of Hammond park and it is the City's view that there is a better design solution than having residential lots directly back onto a strategic freight route.</p> <p>C. Noted. As above.</p> <p>D. Supported. The SSDSP3 spatial plan will be modified by removing this POS link and rewording associated notation. The interface with Rowley Road will be determined at the LSP stage and it is the City's expectation that the proposed treatment will ensure the amenity of future residents is suitably addressed. This will express requirements for an enlarged service road reserve interface as a minimum.</p> <p>E. Supported. As above.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>accordingly, by the deletion of the POS strip abutting Rowley Road.</p> <p>G. We support SSDSP3 Note 2 however seek a minor modification to delete reference to 'Regional Road' and insert 'Primary Regional Road' to avoid confusion and to align the description with the proposed MRS designation.</p> <p>H. We support statements in the SSDSP3 report stipulating that any infrastructure requirements arising from Rowley Road as a designated freight route are to be funded by the State (p 23) and that developers are not required to contribute to the construction of Rowley Road (p38). In addition, this position should be reinforced and clarified further by also inserting additional text stipulating that this position also relates to any interim construction or upgrades not directly related to the subdivision of abutting residential land. Reference should also be made that requires Rowley Road to be constructed with 'quiet grade' asphalt on the basis that the land was zoned Urban in the MRS and Development zone in the TPS, prior to decisions made in respect to the designation of Rowley Road as a freight route.</p> <p>I. As it was always anticipated and identified that the subject land would be developed for residential development, & recognising that the proponents will be funding the requirement for a noise wall barrier, the State should be the responsible party for a contribution which assists in remediating noise through the provision of 'quiet grade' asphalt.</p> <p>3. <u>Barfield Road</u></p> <p>We do not support SSDSP Note 3 regarding the requirement for grade separated pedestrian and vehicular access for the continuation of Barfield Road across Rowley Road. Provision has been made by the State for Hammond Road to facilitate the north-south district level connection via a grade separated intersection located 1km to the west. Regional north-south connection is provided for via the Kwinana Freeway, 300m to the east.</p> <p>It is therefore considered highly unlikely that the State will be in a position to provide an additional grade separated intersection in this location given the proximity of both the existing and proposed major road infrastructure. Given the uncertainty in this regard, it would be a severe impost on the development potential of the subject land for a significant area of land being sterilised whilst initially awaiting a decision on the potential need and then, whilst awaiting acquisition of the affected land.</p> <p>Irrespective of the outcome however, there is no requirement for grade separated pedestrian access in Barfield Road as this exists within the current infrastructure of</p>	<p>G. Supported. The SSDSP3 notation 2 will be modified to reflect Rowley Road's status as a 'Primary Regional Road' under the MRS.</p> <p>H. Supported (in part). The relevant text of the SSDSP3 report will be amended to outline that any interim construction or upgrades to Rowley Road not directly related to the subdivision of abutting residential land is the responsibility of the State Government. The City's preference is for 'quiet grade' asphalt for Rowley Road similar to the Kwinana Freeway and this preference will be stated in the report text however the City cannot mandate its use as part of the SSDSP3 as this falls outside the City's control.</p> <p>I. Not supported. As above.</p> <p>3. Not Supported. The City maintains that a future linkage to residential development and the Mandogalup Train Station to the south of the SSDSP3 area is important and desirable. This is also supported by the need for adequate access to be provided for future residents in the Mandogalup residential cell to access the future High School in the SSDSP3 area. At the District Structure Plan level, there seems merit to protect</p>

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		<p>the Kwinana Freeway and easily accessed through linkages proposed in the LSP.</p> <p>4. <u>Miscellaneous</u></p> <p>A. We seek the inclusion of modified text to p24 '5.4.1 Housing Principles' through the insertion of the word 'generally' in point 3 so that it reads "generally within 400 metres of centres" This will clarify the intent of the report and will align with the SSDSP3 plan that the 25 dwellings/gross ha is applicable 'generally' within 400m and not a blanket requirement for all development to meet this requirement within 400m of centres.</p> <p>B. We do not support the level of detail required for 'landscape management regime and asset cost schedule' at the LSP stage (p30). This level of detail is more appropriate as a condition of subdivision approval when final decisions have been made in respect to all aspects of the proposed development including ultimate yields, location and configuration of POS and the accurate requirements for drainage. All of these aspects influence decisions in respect to both the management and asset cost of the provision of POS and will remain unknown until plans are progressed to a detail not known at the LSP phase.</p> <p>C. We also seek a modification to the 'Public Transport' plan (Fig 9) which assumes grade separation at Barfield Road. In the absence of both support for the grade separation and a commitment from the State to fund the required infrastructure, Figure 9 should be modified and the proposed bus route redirected west, to connect back into Hammond Road which is the designated grade separated intersection.</p> <p>Amendment No. 28</p> <p>5. The intent of Amendment No. 28 is understood and generally supported. The introduction of Development Contribution Areas and Development Areas into TPS 3 provides the basis for a coordinated and equitable approach for the provision of regional infrastructure and for this reason is supported where it is applied equitably. The basis of our submission as outlined below is that some of the contributions proposed within DCA 9 Hammond Park are not equitable.</p> <p>A. In regard to the cost of widening and upgrading of Hammond Road we believe that the clause specifying construction should be reworded to properly reflect the requirements of State Planning Policy 3.6 (SPP 3.6). As such, the wording 'Construction of an additional two lanes' should be deleted and replaced with 'Construction of one carriageway comprised of two lanes' This will ensure that there is no ambiguity in any future interpretation of the requirement as it accords</p>	<p>future grade separated access at this point, however it is noted that this may be further rationalised through subsequent local structure planning to determine how this specific access is created. The associated notation will be modified to reflect this. It should also be noted that the Town of Kwinana has also reiterated the need for this link given future residential development which is planned for the Mandogalup area in line with the Town's 'Eastern Residential Intensification Concept'.</p> <p>4A. Supported.</p> <p>4B. Supported (in part). The City's expectation is for LSP's to be supported by appropriate details to ensure consistency across the SSDSP3 area in terms of POS provision, design and maintenance. POS design and maintenance details at the LSP stage are only anticipated to be conceptual and not overly detailed. As a minimum the City would expect LSP's to outline an approximate unit rate per hectare for maintenance of proposed POS areas. The SSDSP3 report text will be modified to more accurately reflect the City's expectations in this regard.</p> <p>4C. Supported (in part). The figure depicting potential bus routes is only indicative and will be subject to further detail and liaison with the Public</p>

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		<p>directly with the stated requirements contained within SPP 3.6.</p> <p>B. Hammond Road is reserved in the Metropolitan Region Scheme (MRS) as a 'blue road'- an Other Regional Road. The MRS designation recognises the future status and function of Hammond Road as a regional road and its reservation deems it appropriate that the 'purchase of land reserved for Hammond Road' is a State responsibility. As such we seek the deletion of this clause.</p> <p>C. We accept the inclusion of 'traffic management devices' on the basis that this does not include traffic lights. Our assumption is based on other DCA's contained within Schedule 12 of TPS 3 (specifically DCA 5, DCA 8 and DCA 11,) where the requirement for traffic signals is identified separate from, and in addition to 'traffic management devices.' That is, traffic signals do not form part of the 'traffic management devices' required for DCA 9.</p> <p>D. In addition to the above and in accordance with the expressed intent of the SSDSP3 report (p.38) that it is not considered appropriate for developers within the SSDSP3 area to contribute to the construction of Rowley Road', specific reference or clarification is required to be included in the DCA 9 provisions. We believe this needs to be clearly articulated as there are potential areas of 'overlap' at the Hammond Road/Rowley Road intersection where all costs associated with Rowley Road (particularly crossing Rowley Road- i.e. construction costs of this section of Hammond Road, dual use path, pedestrian crossing, etc) where the responsibility for funding clearly resides with the State.</p> <p>E. In formulating the Local Structure Plan (LSP) for the abovementioned lots, a Local Water Management Strategy (LWMS) has been completed for inclusion as a technical appendix. The over-arching drainage concept for the land is that <u>all</u> drainage be contained on site. The LWMS demonstrates that it is not necessary, nor appropriate, to connect to a regional drainage network. In light of this conclusion, an objection to the imposition of the proposed regional drainage charge within the DCP provisions to this land is lodged for Council's consideration please.</p>	<p>Transport authority at the relevant LSP stages.</p> <p>5. Noted.</p> <p>5A. Supported.</p> <p>5B. Not supported. Given that residential and associated development within the SSDSP3 area is the primary reason for Hammond Road requiring to be widened/upgraded, the purchase of land required for the extension remains the responsibility landowners/developers. This is consistent with all other DCA's within the City which have been in operation for the past 10 years or more.</p> <p>5C. Not supported. The original design of the Hammond Road extension is subject to review and the decision whether to signalise the intersection of Hammond Road and Wattleup Road will need to be determined once detailed designs have been completed. At the original design stage some 7 years ago the preferred intersection treatment was a roundabout and this was identified in the associated cost contribution schedules for DCA's 9 and 10. However based on current traffic</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
			<p>forecasts, the City has recommended that the intersection be signalised. The wording of traffic management devices in the Scheme text is appropriately broad to enable the optimum solution to be delivered once final designs and studies have been completed. Maintaining the term 'traffic management device' in the Scheme provisions enables the associated Cost Contribution Schedule (which sits outside of the Scheme) to be reviewed/modified and updated as appropriate without the need for a formal Scheme Amendment. This ensures that required infrastructure can be delivered in a timely manner.</p> <p>5D. Supported. Additional notations will be added to the relevant DCA provisions to reflect this.</p> <p>5E. Not supported. Within the SSDSP3 area, contributions for regional drainage infrastructure are required to be collected for the construction of a groundwater control point (swale) on Gaebler Road (within former Lot 412 Gaebler Road). Some drainage from the development of land to the south of Gaebler Road will flow directly into this groundwater control point and the remainder of the DCA 9 area and part of DCA 10 will benefit from lower ground water levels requiring less fill to develop the land. As such any dispensations on contributions for regional drainage infrastructure will need to be justified by appropriate evidence that the LSP has not received any benefit from the previous regional drainage works.</p>

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2	Bernie O'Shea 50 Ruislip Street LEEDERVILLE WA 6007	<p>OBJECT</p> <p>1. This submission is made on behalf of Roman Catholic Archbishop of Perth, who is a landowner in Hammond Park (Lot 46 Frankland Avenue) and is represented by the Catholic Education Office of WA. As discussed in a recent meeting with the Mayor of Cockburn, the preferred option for development contributions as they pertain to our primary school site under DCA 9 Hammond Park is for a total exemption as applies to the Department of Education site directly across the road for the following reasons:</p> <ul style="list-style-type: none"> - The expectation of a contribution of almost \$29,000 per hectare from one provider in the same industry with the same community service focus and nothing from another is inconsistent and difficult to fathom. In developing schools for Catholic education over the past twenty years, there has never been an expectation from Local Government that Catholic education is to be treated like a developer. Schools are an important part of the social infrastructure in a community and as such should be totally exempt from being seen as a developer who will operate with a different "for profit" perspective. - Catholic education operates as a low fee paying system and indeed, has been likened by the Minister for Education as providing an educational option closest to that of the public system. - We have 158 schools across the state and aim to provide an affordable education, particularly establishing new schools in lower socio-economic areas where we reduce costs to the minimum and rely on our well established schools to cross subsidise the new schools. This is the case with Hammond Park Catholic Primary School. In a climate of limited funding and increased cost of provision, unreasonable demands for development contributions ultimately will preclude this offering, to the detriment of the diversity offered in the community. - The Cockburn Community Development Strategic Plan (2011-2014) emphasises the importance of diversity under the heading 'Desired Effect - Long Term Goals' (pg 1) by stating, "<i>Ensure social diversity - To have made sure that Cockburn is appropriately planned and structured to accommodate social diversity</i>". - Furthermore the Cockburn Community Development Strategic Plan (2011-2014) states that, "<i>The City of Cockburn acknowledges that not for-profit organisations (NFPs) and volunteers make an invaluable contribution in enabling the City to achieve its vision in ensuring Cockburn is the most attractive place to live, work and visit in the Perth metropolitan area</i>". The City's vision and strategic initiatives and desired effects - long term goals underpin the relationship between the City's Corporate Strategic Plan (2011 - 2016) and the new direction identified in the 	<p>1. Not supported. The subject site will be receiving direct benefit from the infrastructure covered by DCA9 and as such the nature of the development is not reason enough to warrant an exemption from contributing to the DCA. There is no sound basis for exemption in this case as development within the area would not be possible without the regional drainage network being in place. Furthermore there is no evidence that confirms that patrons of the school site will not be using the upgraded Hammond Road.</p> <p>DCP's are ultimately intended to provide for the equitable sharing of the costs of infrastructure and administrative costs between owners/developers, ensure that cost contributions are reasonably required as a result of the subdivision and development of land in the development contribution area and coordinate the timely provision of infrastructure.</p> <p>The State Government High School (within DCA9) and Primary School (within DCA10) sites will be subject to DCA contribution requirements as they too will be receive benefit from the infrastructure associated with the DCA. If an exemption were to be afforded to the school sites then, despite the reality of the demand they will be creating, then the additional costs arising from the exemptions will be borne by all other landowners and their associated DCA contributions will be</p>

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		<p>Cockburn Community Development Strategic Plan (2011-2014).</p> <p>It would be realistic to assume that Structure Plan decisions including development contributions are consistent with the stated objectives and principles within the City's Strategic Corporate and Community development plans.</p> <p>- A Catholic school in a developing area provides parents with a low cost educational choice. If we have the opportunity to work with the City within an understanding of our funding limitations, I have no doubt that the community at large will share in the social benefits we are able to provide.</p> <p>I trust that the considerable concerns of Catholic education will be conveyed to the Council in the light of the City's stated objectives and commitment to the Not for Profit sector. In particular, the injustice of treating two different education providers on either side of the same street with completely different interpretations of their contribution to the community framework will be obvious and hence rectified.</p>	<p>far greater. This scenario would fail the test of 'Need and Nexus' and 'Equity' in terms of SPP 3.6.</p> <p>The Primary School site which is the subject of a DCA exemption outlined by the submissioner relates to the future Hammond Park Primary School on the corner of Gaebler Road and Frankland Avenue (future Hammond Road). This site is the subject of another DCA being DCA3 and it was determined at the time of adoption of DCA3 that the school site be exempt from contributing. DCA3 was adopted prior to the introduction of SPP3.6 and can therefore not be used as an adequate basis for current decision making.</p>
3	<p>Brett Dunn Department of Water PO Box 332 MANDURAH WA 6210</p>	<p>SUPPORT</p> <p>Thank you for your correspondence dated 8 June 2012. The Department of Water (DoW) has reviewed the proposal and offers the following advice:</p> <p>1. Urban Water Management</p> <p>Consistent with Better Urban Water Management (BUWM) (WAPC, 2008) and policy measures outlined in State Planning Policy 2.9, the future Local Structure Plans should be supported by a Local Water Management Strategy (LWMS) prior to final approval of the future Structure Plan. The LWMS should demonstrate how the subject area will address water use and management. It should contain a level of information that demonstrates the site constraints and the level of risk to the water resources. The LWMS should include, but not be limited to:</p> <ul style="list-style-type: none"> • Site characteristics, constraints and opportunities; • Capacity of land to support proposed land use; • Stormwater management strategy; • Fit-for-purpose water use strategy; • Issues to be determined at time of subdivision; and • Recommended monitoring and implementation framework. 	<p>1. Noted. The advice provided relates to subdivision/development works and will be implemented as part of a future subdivision and development application/s for the subject land.</p>

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		<p>2. Groundwater</p> <p>The subject area is located within the Jandakot Groundwater Area and Cockburn groundwater Area as proclaimed under the Rights in Water and Irrigation Act 1914.</p> <p>Any groundwater abstraction in this proclaimed area for purposes other than domestic and/or stock watering taken from the superficial aquifer is subject to licensing by the Department of Water.</p> <p>The issuing of a groundwater licence is not guaranteed but if issued will contain a number of conditions that are binding upon the licensee. Please note, a high proportion of groundwater in the Jandakot Groundwater Area is currently allocated, thus there may be limited resources available to new developments.</p> <p>Proposed land uses requiring groundwater should identify and potentially secure fit-for purpose water early in the planning process by consulting with the Licensing Branch at the DoW's Mandurah office on 9550 4222.</p> <p>3. DEC Issues:</p> <p>It appears that there are Acid Sulphate Soils on the subject land. For this reason, this proposal must be referred to the Land Use Planning section at the Department of Environment and Conservation's Swan Region.</p>	<p>2. Noted. As above</p> <p>3. Noted. As per recommendation (1.) above.</p>
4	Department of Planning Locked Bag 2506 PERTH WA 6001	<p>OBJECT- subject to further information</p> <p>1. The DSP area is bounded by Kwinana Freeway (a Primary Regional Road), Rowley Road (a future Primary Freight Route and current Planning Control Area No. 95) and Hammond Road (an Other Regional Road). The estimated residential lot yield in the DSP area is 3411, and the DSP area will also include two primary schools, one high school and three Local Centres and Mixed Business/Commercial areas.</p> <p>This development will be a very significant traffic generator and this therefore justifies the need for a detailed and thorough 'Transport Impact Assessment' to assist the Department in assessing the transport impacts of the proposed DSP on the surrounding Regional Road network. The information currently supporting this development application is considered insufficient to enable the Department to assess the transport implications of the proposed DSP.</p> <p>The Transport Impact Assessment should be in accordance with the WAPC's 'Transport Assessment Guidelines for Developments', available online at http://www.planning.wa.gov.au/publications/1197.asp</p>	<p>1. Noted. The City considers that the future development proposed as part of the DSP is consistent with the density targets set by the WAPC's Directions 2031 and can be integrated within the existing road network with minimal impact. The subject is bounded by existing and future high level regional roads which provide suitable access/egress opportunities.</p> <p>It should be noted that the SSDSP3 is proposed to be adopted as a guiding document only and not pursuant to the City's Scheme or subject to endorsement by the WAPC.</p>

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		<p>2. The Department expects that proponents will follow the Guidelines in order to develop an accurate and thorough Transport Impact Assessment. The Transport Impact Assessment should also be referred to Main Roads WA, the Public Transport Authority and the Department of Transport for comment. In accordance with 'State Planning Policy 5.4 - Road and Rail Transport Noise & Freight Consideration in Land Use Planning', it is likely that a transport noise assessment will also need to be carried out. This should be referred to the Department and Main Roads WA for comment.</p> <p>3. The Department will provide more detailed transport advice regarding the proposal back to the City of Cockburn once the required Transport Impact Assessment is made available to us for consideration.</p>	<p>At the LSP stage, developers will be required to submit 'Transport Impact Assessment' reports which demonstrate compliance with the City's and Main Roads requirements.</p> <p>This will need to be based on assumptions for the regional traffic network consistent with the District Structure Plan, so that each traffic study does not simply consider its implications in isolation, but consider implications according to the regional traffic information currently held by the City of Cockburn which has been used in development the regional road network design and connections,</p> <p>2. Noted.</p> <p>3. Noted. As per recommendation (1.) above.</p>
5	Gary & Denise Ellement 456 Wattleup Road HAMMOND PARK WA 6164	<p>SUPPORT – subject to modifications</p> <p>1. We support the modification of the Scheme map to designate West of Hammond Road extension with Development Area No.27-DA27.</p> <p>2. <u>Note 7 (Central Precinct) as per Updated Draft SSDSP3 (2012)</u></p> <p>We request consideration be given to excluding properties West of Hammond Road in the proposed hub. We understand it is currently proposed that any development plan would require all eight land owners in the proposed commercial and high residential density hub to work together to provide structure plans that take into consideration of the whole hub area.</p> <p>All the landowners West of Hammond Road by Hub 7, oppose their inclusion for the following reasons:</p> <p>A. The Hammond Road extension will ultimately be a dual carriageway with an approximate width of 40 metres. This provides a man-made barrier between the West and East properties thus severing the connection between the land holdings</p>	<p>1. Noted.</p> <p>2. Supported. The City maintains the requirement for a comprehensive LSP to cover the identified 'Central Precinct' however the extent of the precinct will be reduced from that shown on the Draft SSDSP3 spatial map. The precinct will only now include Lots 114, 123 and 124 Wattleup Road (lots east of Hammond Road extension) given the presence of the Neighbourhood Centre, supporting mixed businesses and higher density residential development proposed for these sites.</p>

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		<p>B. The Tramway running parallel to Hammond Road on the West side creates a further 11 to 20 metre barrier between the East and West properties (the width varies from North to South). The WAPC has advised they intend to revegetate this reserve to provide a connection between Harry Warring Reserve and Franklin Park thus creating a natural barrier between the West & East properties.</p> <p>C. In summary the overall distance (being the dual carriageway and Tramway) between the properties to the West and the East is between 50 and 60 metres. This provides a distinct and significant barrier which we believe makes consultation with the properties to the East onerous and redundant. We request the properties West of Hammond Road be treated as a separate Sub-Hub.</p> <p>3. Attached is an email from property owners Kerry & John Groves in supports of this submission:</p> <p>We would like to be part of your submission. We agree that a submission arguing that Hammond Road and the 20 metre reserve alongside provides a broad barrier and division that mean we need to be treated separately and would be willing to form out own sub-hub. This would save us from onerous and unnecessary consultation with the landowners to Frankland Avenue.</p> <p>The three properties affected i.e. Ours, David's (the owner of Lot 1) and yours being west of Hammond Road are in agreement and we can work together. Kerry and John Grove – Lot 111 Wattleup Road</p>	<p>2A. Noted. As per (2.) above.</p> <p>2B. Noted. As per (2.) above.</p> <p>2C. As per (2.) above.</p> <p>3. As per (2.) above.</p>
6	Margaret & Tony Ravlich 39 Barrington Street MUNSTER WA 6166	<p>SUPPORT – subject to modification/s</p> <p>1. We are in support of the residential development as soon as possible with The inclusion of South of Wattleup Road we strongly advise the City of Cockburn to remove the Alcoa Air Buffer so as the residential Development can go ahead.</p>	<p>1. Noted. In 2010 as part of the broader Kwinana Industrial Buffer review process, and based on advice from the Department of Health and Department of Environment and Conservation, the WAPC resolved to extend the buffer surrounding Alcoa's Mandogalup Residue Disposal Area. It should be noted that matters relating to the buffer are primarily the responsibility of State Government authorities and thus is not within the scope of the City's control.</p> <p>The expanded buffer includes a 1 kilometre area where future residential</p>

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			<p>expansion is restricted (within the Town of Kwinana), with an additional 0.5 kilometres beyond this (into Wattleup and Hammond Park) also restricting residential expansion and development of some other sensitive land uses (i.e. kindergartens, hospitals and aged persons housing). The purpose of the additional buffer area was to recognise uncertainties related to the level and frequency of impacts from the Alcoa Residue Disposal Area and Kwinana Industrial Area more broadly. The WAPC advised that the extended portion of the buffer will be reviewed in five years' time to ensure that its extent and location reflects the current conditions in the area.</p> <p>The implication/s of the buffer was recently considered by the State Administrative Tribunal ("SAT") in the case of Wattleup Road Development Company Pty Ltd and Western Australian Planning Commission [2011] WASAT 160. The case concerned an application for review of the refusal by the WAPC of a residential subdivision application within the southern portion of the SSDSP3 area. The SAT ultimately affirmed the WAPC's decision to refuse subdivision approval.</p> <p>The City's decision not to totally exclude the area entirely from the SSDSP3 reflects the future desire for residential or compatible development to occur within this area subject to the appropriate modelling and environmental studies being undertaken. This is in line with the</p>

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			<p>advice of the SAT, providing that current modelling which was used to prepare the buffer should be given no weight, and needs to be undertaken again. Once this has been completed and the actual implications known, appropriate structure planning can then take place for this land.</p>
7	Affected Landowner (Names & address withheld)	<p>OBJECT – subject to modifications</p> <ol style="list-style-type: none"> 1. In March 2007 the Education Department purchased 2.03 hectares of land (the rear portion of Lot 47) for the sole purpose of building a new High School; refer to site plan attached. As you can see the boundary is dog-legged in shape and the access road to service the said High School must follow this boundary. As per the new District Structure Plan, access now runs parallel to Frankland Avenue, which will result in the loss of approx. 0.6 hectares of our land, reducing our commercial value for subdivision. Therefore we require the road to follow the dog-legged boundary. 2. The Public Open Space as per new amendment is shown to be mostly on our portion of land on Lot 47. Therefore it should be reduced to the correct percentage required of our land holding only. 	<ol style="list-style-type: none"> 1. Not supported. The design and size of the High School site and surrounding development is yet to be determined by the LSP and subdivision process. The previous indicative road design was based on the future Mandogalup Train Station and adjacent neighbourhood centre being a node for the SSDSP3 area and as such major roads were shown to be angled towards this location. Given that the station and neighbourhood centre no longer form part of the SSDSP3 design, major roads within this area are proposed to be more traditionally designed. The exact design of the high school site and abutting western road (Irvine Parade extension) will be determined at the LSP stage. <p>Matters of individual economic circumstances do not form part of the planning processes handled by the City and as such the purchase of the land required for the High School is a separate matter between the landowner and the Department of Education.</p> <ol style="list-style-type: none"> 3. Noted. The POS areas shown on the Draft SSDSP3 spatial map are only notional and do not accurately reflect

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			<p>the requirement for landowners to provide 10% POS within their landholding. All LSP's will be required to demonstrate compliance with the minimum 10% provision of POS.</p>
8	<p>Affected Landowner (Names & address withheld)</p>	<p>OBJECT – subject to modifications</p> <ol style="list-style-type: none"> 1. We are extremely disappointed that our property, Lot 115 Wattleup Road has been excluded from SSDSP3. The proposed plans clearly show that our property will be adversely affected by the increase of the buffer zone. As property owners, we stand to lose the development potential previously proposed and subsequent devaluation of our property. 2. This property has been a part of our family for the past 45 years; we have duly paid council rates, land taxes and maintained it. Viewing the plans, it appears that southern side of Wattleup Road is to be used as the boundary for the buffer zone. An alternate and suitable option is to make use of the proposed Rowley Road freight route as the boundary for the buffer zone instead of Wattleup Road. 	<ol style="list-style-type: none"> 1. Noted. Matters of property values or individual economic circumstances are not factors controlled by or able to be considered as part of the structure planning process. <p>In 2010 as part of the broader Kwinana Industrial Buffer review process, and based on advice from the Department of Health and Department of Environment and Conservation, the WAPC resolved to extend the buffer surrounding Alcoa's Mandogalup Residue Disposal Area. It should be noted that matters relating to the buffer are primarily the responsibility of State Government authorities and thus is not within the scope of the City's control at this point.</p> <p>Despite this the City maintains the desire for residential or compatible development to occur within this area subject to the appropriate modelling and environmental studies being undertaken.</p> 2. Noted. Matters of individual economic circumstances are not factors controlled by or able to be considered as part of the structure planning process. The Kwinana Air Quality Buffer extension was determined in order to recognise uncertainties related to the level and frequency of

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			impacts from the Alcoa Residue Disposal Area and Kwinana Industrial Area more broadly. It was not arbitrarily assigned and as such using Rowley Road as a buffer extent is not considered appropriate in this case.
9	A & IM Gaglia 466 Wattleup Road HAMMOND PARK WA 6164	<p>SUPPORT – subject to modifications</p> <ol style="list-style-type: none"> Given the scale of actual and proposed housing development in DCA 9, DA26; In the interests of public safety, efficient traffic management and protection of native fauna, we strongly recommend that increased funding and prioritisation is accorded to the extension of Hammond Road south of Gaebler Road, concurrent with its extension south of Russell Road. The actual and potential population growth with unchanged major infrastructure necessitates an additional alternative access route to serve residents and ratepayers equitably. 	<ol style="list-style-type: none"> Noted. The City's 'Regional & Major Roadworks 2006 – 2022' plan identifies that the Hammond Road extension south of Gaebler Road is notionally due to commence construction in 2014 however it is impractical to absolutely specify a timing for the delivery of the road, given it will largely be dependent on the rate of development taking place and the degree to which landowner's who are affected by the road are at a stage where they wish to subdivide and develop their land. The City we continue to monitor the situation for the Hammond Road extension and ensure that it can act at the appropriate stage to deliver the road infrastructure. Noted. As above.
10	RPS PO Box 465 SUBIACO WA 6904	<p>OBJECT – subject to modifications</p> <p>RPS acts on behalf of the following landowners with respect to this submission:</p> <ul style="list-style-type: none"> Lot 123 Wattleup Road, Hammond Park - Hammond Park Estates Pty Lot 41 Hammond Road, Hammond Park - Broad Vision Projects Pty <p>The following comments are provided with respect to the advertised documentation.</p> <ol style="list-style-type: none"> Amendment 28: No comment or objection. Draft Southern Suburbs District Structure Plan Stage 3: there are two aspects of the SSDSP3 which the landowners wish to provide comment on. 	<ol style="list-style-type: none"> Noted.

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		<p>A. <u>Central Precinct</u> - Note 7 of the Draft SSDSP3 map provides that a comprehensive Local Structure Plan will be required for the Central Neighbourhood Centre, and that the City will not consider individual structure plans in this location due to the need to ensure the suitable integration of development. The landowners represented by RPS in this matter strongly object to the abovementioned requirement for the following reasons:</p> <ul style="list-style-type: none"> - Development occurring on the two parcels of land comprised within the Central Neighbourhood Precinct which are separated by Hammond Road is incapable of being integrated in any meaningful fashion by virtue of the presence of Hammond Road, which is reserved as Other Regional Roads in the MRS, and forms a significant physical barrier between the land parcels, thereby limiting any prospect of meaningful integration of land uses on opposite sides of the road. - The landowners of Lot 114, 123 - 126 Wattleup Avenue are presently collaborating in order to prepare a Local Structure Plan over the landholdings to the east of Hammond Road which fall within the Central Precinct in the SSDSP3. We understand that the landowners of Lots 111, 1 and 2 Wattleup Avenue have no intention of developing those landholdings in the short to medium term and as such, would be unwilling to participate in any structure planning exercise which occurs within the Central Precinct. To this end, there is no justification in preventing the landowners in the eastern portion of the Central Precinct from advancing structure planning, subdivision and development over this portion of the precinct given the absence of any planning justification to prepare a structure plan over the entire precinct. - The land uses identified in the SSDSP3 within the eastern portion of the Central Precinct are substantially different to the land uses identified in the western portion. The SSDSP3 identifies Local Centre, and Mixed Business/Commercial/Home Based Business land uses within the eastern portion of the Central Precinct whereas the western portion of the Central Precinct as illustrated in the DSP provides for high and medium density residential land use together with public open space. To this end, planning for the eastern portion of the Central Precinct can be readily undertaken independently of the western portion without adversely affecting the orderly and proper planning of the overall Central Precinct. - The SSDSP3, in its adopted form has been used as a guide to inform the preparation of local structure plans within the DSP area. Ostensibly, the City has not historically supported local structure plans inconsistent with the DSP, and would continue to have strong planning grounds upon adoption of the Draft 	<p>2A. Supported (in part). The City maintains the requirement for a comprehensive LSP to cover the identified 'Central Precinct' however the extent of the precinct will be reduced from that shown on the Draft SSDSP3 spatial map. The precinct will now only include Lots 114, 123 and 124 Wattleup Road given the presence of the Neighbourhood Centre, supporting mixed businesses and higher density residential development proposed for these sites.</p>

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		<p>SSDSP3 to only support local structure plans that are capable of demonstrating consistency with the DSP. To this end, there is no justification to require the parcels of land on opposing sides of Hammond Road to be comprised within a single local structure plan as this would contradict the reasoning for the DSP.</p> <ul style="list-style-type: none"> - There is no justification contained within the SSDSP3 report supporting the preparation of a single local structure plan for the entire precinct. In fact, the sketch contained in Figure 11 of the DSP report illustrates an indicative layout for the Local Centre within the eastern portion of the Central Precinct, thereby demonstrating that this area can be planned for independently of the western portion of the precinct. - The proponent of the local structure plan for the eastern portion of the Central Precinct has no objection to providing a context plan illustrating an indicative development layout over the western portion of the Central Precinct, which may go some way to demonstrating how the planning of the two 'sub-precincts' vaguely integrate. <p>B. Conservation Category Wetland It is noted that the adopted SSDSP3 and the draft SSDSP3 illustrate the presence of a Conservation Category Wetland over a portion of Lots 41 and 42 Hammond Road. The City is advised that there is no scientific justification for the retention of this wetland and to this end, local structure plans currently being prepared for these properties assume that the wetland is ultimately removed from the Department of Environment and Conservation's Geomorphic Wetlands Swan Coastal Plain Dataset.</p> <p>Please refer to the enclosed report prepared by Bayley Environmental Services which demonstrates the folly of referring to this area as a wetland.</p>	<p>B. Not supported. Conservation Category Wetland's (CCW) are allocated by the State Government as having the highest degree of conservation value of all wetlands on the Swan Coastal Plain. Protecting the CCW, by appropriate buffers particularly, is required to the highest degree under State Government planning and environmental policy. This forms a fundamental determinant to orderly and proper planning.</p> <p>It should be noted that the Department of Environment and Conservation ("DEC") recently considered a request to modify the classification of the subject wetland and resolved to retain its classification as a CCW. This is reflected in the DEC's letter dated 16 December 2010 which was sent to the proponent representing landowners</p>

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			<p>affected by the CCW.</p> <p>Future LSP's will need to demonstrate how the requirements of the DEC and environmental attributes of the CCW are addressed appropriately.</p>
11	<p>RPS PO Box 465 SUBIACO WA 6904</p>	<p>OBJECT – subject to modifications</p> <p>RPS acts on behalf of the owner of Lot 28 Barfield Road, Hammond Park, Condor Nominees Pty Ltd in relation to this matter. The following comments are provided with respect to the advertised documentation.</p> <p>1. Amendment 28: The quantum of the contribution required to be made by developers for the DA 9 area on a per hectare basis appears to be reasonable based on the items which require funding. Not with standing, the City's attention is drawn to the following items noted for funding for which clarification is sought:</p> <p>A. Reference to the construction of additional two-lanes for Hammond Road. It is unclear from this clause if the widening of Hammond Road is to be undertaken as a dual-carriageway based on the reading of this clause. Assuming the landowners abutting Hammond Road are compensated for the cost of acquiring their land to facilitate the widening of Hammond Road in order to provide for a dual-carriageway, it is assumed that the remaining landowners within the DA9 area are contributing to the cost of widening and upgrade on the basis of dual-carriageway, dual-use paths, earthworks, drainage etc. Further clarification is sought in this regard.</p> <p>B. Reference to traffic management devices There are concerns that reference to traffic management devices in both Amendment 28 and the SSDSP3 is too generic, and provides scope for the City to seek further contributions from individual landowners at the subdivision stage over and above the cost contribution framework, in order to provide contributions towards the provision of traffic control lights (which should be specified within the developer contribution framework), or traffic management devices that should be funded by the individual developer. This could create the potential for developers to be contributing twice for an item otherwise already covered within the scheme contribution framework.</p> <p>2. Draft Southern Suburbs District Structure Plan - Stage 3:</p> <p>A. Residential Density: It is recognised that the deletion of the future Mandogalup Railway Station from the south eastern portion of the SSDSP3 area will remove the demand for high density residential development in the vicinity of the former</p>	<p>1. Noted.</p> <p>1A. Noted. The wording of the provision relating to the Hammond Road extension will be modified to reflect the intention that the DCP's will be responsible for the construction of the second carriageway comprised of two lanes, with the City being responsible for the first carriageway.</p> <p>1B. Not supported. At the original design stage some 7 years ago the preferred intersection treatment was a roundabout and this was identified in the associated cost contribution schedules for DCA's 9 and 10. However based on current traffic forecasts, the City has recommended that the intersection be signalised. The wording of traffic management devices in the Scheme text is appropriately broad to enable the optimum solution to be delivered once final designs and studies have been completed.</p>

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		<p>station. The subject land falls within the 800 metre walkable catchment of the former station. The adopted SSDSP3 illustrates a target dwelling yield of 30 dwelling units per hectare generally within 400 metres of the former station, and a target dwelling yield of 20 dwelling units per hectare within the 800 metre walkable catchment.</p> <p>Conversely, the draft SSDSP3 appears to remove specific dwelling yield targets from the SSDSP3 mapping, and simply refers to a medium density residential coding applying to the subject land and surrounding area. Additionally, it is unclear from Section 5.4.3 of the draft SSDSP3 report as to whether a yield of 20 dwelling units per hectare would be achievable on the subject land. Based on the general treatise provided in the draft SSDSP3 report, it is considered that a yield of 20 dwelling units per hectare on the subject land would be appropriate and justifiable.</p> <p>B. <u>Aboriginal Heritage</u>: Noting the provisions of Section 3.5 of the draft SSDSP3 report, in order to address the issue of Aboriginal heritage, prior to any earthworks taking place, the proponent will make an application to the Minister for Indigenous Affairs pursuant to Section 18 of the Aboriginal Heritage Act 1972 to facilitate the carrying out of earthworks. If deemed as necessary by the Department of Indigenous Affairs, the following measures may also be put in place:</p> <ul style="list-style-type: none"> • An Aboriginal Heritage Management Plan can be prepared and implemented as a condition of subdivision approval. • Aboriginal monitors can be employed during the subdivisional earthworks phase with an appropriate level of monitoring by authorised persons to ensure that any artefacts discovered during the course of earthworks are appropriately identified and protected in accordance with the Act. <p>C. <u>Central Precinct</u>: Note 7 of the Draft SSDSP3 map provides that a comprehensive Local Structure Plan will be required for the Central Neighbourhood Centre, and that the City will not consider individual structure plans in this location due to the need to ensure the suitable integration of development.</p> <p>We have spoken with the main proponent for the Central Precinct and are concerned that the planning for the precinct may become bogged down by requiring an LSP to be provided for the entire precinct, thereby delaying the provision of retail and other facilities within this location which would contribute to overall amenity of the area and provide facilities in convenient proximity to Lot 28. Ultimately, planning for the eastern portion of the Central Precinct on the eastern side of Hammond Road can be readily undertaken independently of the western portion (western side of Hammond Road) without adversely affecting the orderly and proper planning of the overall Central Precinct.</p>	<p>Maintaining the term 'traffic management device' in the Scheme provisions enables the associated Cost Contribution Schedule (which sits outside of the Scheme) to be reviewed/modified and updated as appropriate without the need for a formal Scheme Amendment. This ensures that required infrastructure can be delivered in a timely manner.</p> <p>2A. Noted. The density targets prescribed by the SSDSP3 spatial map are intentionally broad and section 5.4.2 of the SSDSP3 report outlines the applicable locational criteria for residential densities. The subject site is identified as "medium density" which allows for a variety of residential densities ranging from R30 – R60, the details of which will be determined at the LSP stage.</p> <p>2B. Noted.</p> <p>2C. Supported (in part). The City maintains the requirement for a comprehensive LSP to cover the identified 'Central Precinct' however the extent of the precinct will be reduced from that shown on the Draft SSDSP3 spatial map. The precinct will now only include Lots 114, 123 and 124</p>

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		<p>D. <u>Western Power Easement</u>: It is noted that the Western Power easement which falls on the subject land provides a significant setback, and therefore buffer to traffic noise emanating from the Kwinana Freeway. Given that the easement provides a significant constraint to the development potential of the site (2.3226ha out of a total land area of 4.8166ha), it should be open to the landowner to explore earth bunding methods within the Western Power easement to ameliorate traffic noise from the freeway (if an acoustic study commissioned at the Local Structure Planning stage indicates that active noise buffering measures such as bunding or an acoustic wall are required) without further affecting the development potential of the landholding by requiring the construction of an acoustic wall within the portion of the land area unaffected by the easement.</p> <p>We note that Western Power's policy for land use activities within the easement enables the modification of ground levels within the easement area, subject to Western Power approval. To this end, the landowner would seek to investigate this option should active noise buffering measures be determined as required to achieve compliance with SPP 5.4.</p>	<p>Wattleup Road given the presence of the Neighbourhood Centre, supporting mixed businesses and higher density residential development proposed for these sites.</p> <p>2D. Noted. However matters pertaining to the Western Power easement are the responsibility of Western Power and not the City. Matters relating to noise attenuation will be dealt with at the LSP stage in accordance with the City's and WAPC's requirements.</p>
12	Department of Planning Locked Bag 2506 PERTH WA 6001	<p>SUPPORT – subject to conditions</p> <p>Thank you for referring the above proposal to Policy Development for comment. Bush Forever identifies regionally significant bushland for protection. Assessment of any proposal that may affect a Bush Forever area should recognise and show due consideration of the high conservation values of the site. It is understood the proposal is for the following:</p> <ul style="list-style-type: none"> • Stage 3 of the Southern Suburbs District Structure Plan (SSDSP) for the area known as Hammond Park, Wattleup; • Amending the City of Cockburn Town Planning Scheme No. 3 (TPS 3) to include Development Areas 26 & 27 and Development Contribution Plans 9 & 10. <p>1. The subject site includes two Bush Forever Nominated Areas: Nominated Area 11 - Frankland Reserve, Rowley Road, Wattleup and Nominated Area 12 – Rowley/Frankland/Barfield Roads, Banjup. The subject site is also located adjacent to the southern boundary of Bush Forever Area 392 - Harry Waring Marsupial Reserve, Wattleup (BF 392). The vegetation on Nominated Areas 11 & 12 and BF 392 is representative of the Bassendean Complex - Central and South, of which 24% is remaining, and 13% is proposed for protection, as well as some Herdsman Complex, of which 31% is remaining and 24% is proposed for protection.</p> <p>These figures are based on 2000 data included in Bush Forever 2000 (Volume 1) and therefore are indicative only. The current vegetation cover for the area may vary</p>	<p>1. Noted.</p>

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		<p>from the 2000 figures.</p> <p>2. <u>Proposed Stage 3 of SSDSP</u>: The WAPC resolved in December 2008 to retain Nominated Area 11 for possible inclusion in the Bush Forever register, which was supported by the City of Cockburn.</p> <p>The WAPC resolved in December 2008 to remove Nominated Area 12 from possible inclusion in the Bush Forever register due to several landowner objections to inclusion. The process of removal or inclusion has not been completed. Portions of Frankland Avenue and Rowley Road through Wattleup and Hammond Park are within Planning Control Area No. 95- Rowley Road, which is subject of a current Metropolitan Region Scheme (MRS) amendment for Fremantle Ports, Kwinana Quay and Access Corridors.</p> <p>The proposed alignment of Frankland Avenue will result in Nominated Area 11 (Frankland Reserve) being dissected, resulting in the north-west portion being isolated from the rest of the reserve. It is noted that the City of Cockburn did not support the proposed realignment of Franklin Avenue/Hammond Road to go around Nominated Area 11 in February 2010. The MRS amendment affecting road alignments is deferred and subsequently is not finalised. Any proposals including the affected areas, such as Stage 3 of the SSDSP, are therefore premature in terms of orderly and proper planning and not supported by the WAPC. Notwithstanding the above, it is noted on appended Figure 7 of the Stage 3 SSDSP proposal that active public open space use is proposed within a degraded portion of Nominated Area 11. This is generally consistent with the City's February 2010 resolution to exclude a 4 hectare (ha) area of Frankland Reserve for active recreation purposes.</p> <p>Policy Development (previously the Bush Forever Office) has previously commented on potential use of this portion of Nominated Area 11 as public open space and advised that more active recreational use, such as an oval, located in an area that is completely cleared of native vegetation may be suitable, if appropriately designed and managed to avoid impacts on the surrounding regionally significant bushland. It is noted in the report accompanying Stage 3 that the degraded area is approximately 3ha and that 4ha may be required in the final oval design. Policy Development raises no objection to an oval being situated on this portion of the reserve, subject to the following:</p> <p>A. Any development including construction, access, drainage and ongoing maintenance associated with a future oval/active recreation use and associated uses (such as car parking) on Nominated Bush Forever Area 11 (Franklin Reserve) being limited to identified cleared and degraded areas (approximately 3 hectares) and no further disturbance occurring. The City is advised to contact</p>	<p>2. Not supported. The realignment of Hammond Road was not supported by the City and the transport section of the Department of Planning confirmed in 2011 that the amendment proposing the realignment would not proceed and the Other Regional Road Reserve remains in its current alignment. No further amendments to SPP 2.8 – Bushland have occurred at this stage and as such Frankland Reserve is not currently identified as a Bush Forever site. There appears to be a disconnect in terms of advice being received from the DoP and the information contained in this submission does not align with advice provided by DoP's transport branch. This issue has now been resolved from the City's viewpoint, with the DoP confirming that associated actions to remove and instate a new Planning Control Area for the correct endorsed alignment taking place.</p> <p>2A. Noted.</p>

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		<p>Policy Development to discuss details of the proposal.</p> <p>B. Remediation of the degraded north-east portion of Nominated Bush Forever Area 11 (Franklin Reserve) with native vegetation of local provenance, which is to be sourced from an accredited nursery and be free of disease, weeds and die back.</p> <p>C. No building materials, rubbish or other matter is to be deposited into Nominated Bush Forever Area 11 (Franklin Reserve) during or after development.</p> <p>3. The subject site includes a Conservation Category Wetland (CCW), the highest conservation value wetland requiring mandatory conservation, located adjacent to Gaebler Road. Medium density residential lots are proposed on the SSDSP in and around the CCW which is inconsistent with the intent of the CCW. It is noted in the arterial drainage document report accompanying the Stage 3 proposal that it is intended to drain events greater than 1 in 5 years into the CCW adjacent to Gaebler Road and Thomsons Lake and may therefore adversely impact on the regionally significant bushland. In accordance with the definition of adverse impact contained within State Planning Policy 2. 8 - Bushland Policy for the Perth Metropolitan Region, no direct drainage into a CCW or Bush Forever area is supported. It is also noted that Nominated Area 11 abuts land located within the Peel-Harvey Coastal Plain Catchment, requiring careful management of drainage and nutrients with development of adjoining sites.</p> <p>The drainage document accompanying the Stage 3 proposal was written in 2003, prior to the preparation of the Better Urban Water Management guiding document. It is suggested that a Local Water Management Strategy is prepared in consultation with the Department of Water and Department of Environment and Conservation which takes all of the above matters into account and does not involve drainage into the CCW.</p> <p>4. Scheme Amendment proposals Policy Development considers the proposed inclusion of Development Contribution Areas 9 & 10 in TPS 3 would not cause any direct or indirect adverse impacts on Bush Forever Area 392 or Nominated Areas 11 and 12 and, therefore, has no objection to this aspect of proposed Amendment No. 28. Development areas 9 & 10 and associated provisions include compliance with the SSDSP such that Policy Development considers that the above matters in relation to Stage 3 of the SSDSP need to be satisfactorily addressed prior to finalisation of Amendment No. 28.</p> <p>5. Please note that this is a departmental response to Bush Forever issues only and does not reflect comments of other branches of the Department of Planning or a formal position of the Western Australian Planning Commission, which may need to</p>	<p>2B. Noted.</p> <p>2C. Noted.</p> <p>3. Noted. Matters relating to water management will be dealt with at the LSP stage through an appropriate Local Water Management Strategy to the satisfaction of the City and Department of Water.</p> <p>4. Noted.</p>

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		be consulted on this proposal.	5. Noted
13	Department of Indigenous Affairs PO Box 3153 EAST PERTH WA 6892	<p>SUPPORT</p> <p>Thank you for your correspondence dated 8th June 2012 seeking our comment on the two development schemes that are about to commence in your municipality: (i) Draft Southern Suburbs District Structure Plan 3 (SSDSP3); and (ii) Scheme Amendment No. 28 (Proposed Development Contribution Areas 9 and 10, and Development Areas 9 and 10). The details of the proposed district structure plan and scheme amendment have been provided to the Department of Indigenous Affairs (DIA) in the letter dated 8th June 2012, with the specific location of the areas to be affected shown in the attached draft plans.</p> <ol style="list-style-type: none"> The information you provided has been reviewed and based on that information it is advised that there are no registered Aboriginal heritage sites (as currently mapped on the Register of Aboriginal Sites) in the area under consideration. One Aboriginal archaeological site (DIA 18937, Mandogalup Lake) is located immediately to the southeast of the development area's south eastern corner. Heritage surveys have been conducted in the area, identifying some heritage values. The close proximity of the proposed development area to the sites that are noted in the Register, and the lack of comprehensive surveying in this area, suggests that there may still be Aboriginal heritage sites to be discovered in the area. All Aboriginal heritage sites (whether known to the DIA or not) are protected under the Aboriginal Heritage Act, 1972 (AHA). It is recommended that advice on compliance with the AHA be provided to all parties involved with or affected by the district structure plan and scheme amendment that are the subject of this development proposal. Please find below a link to our Due Diligence Guidelines for information that will assist your personnel in identifying the risk that the proposed activities may have on adversely impacting Aboriginal heritage values: http://www.dia.wa.gov.au/Documents/HeritageCulture/Heritage%20management/AHA Due Diligence Guidelines.pdf Should cultural material or a new site be discovered in the course of your development project, there is an obligation upon your organisation under section 15 of the AHA to report the information to the Registrar of Aboriginal Sites. 	<ol style="list-style-type: none"> Noted. The advice provided relates to subdivision/development works and will be implemented as part of a future subdivision and development application/s for the subject land. Noted. As above Noted. As per recommendation (1.) above.
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	Environment & Conservation PO Box 1167 Bentley DC WA 6983	<p>1. General</p> <p>The structure plan area ('subject land'), approximately 234ha in size, is located adjacent to Harry Waring Marsupial Reserve, which is also known as Bush Forever Site No. 392 Harry Waring Marsupial Reserve, Wattleup and forms part of Beeliar Regional Park. Frankland Avenue/Hammond Road separates the reserve from the main portion of the subject land, with a portion of the subject land abutting the reserve. DEC (as the former Department of Conservation and Land Management) provided advice to the City of Cockburn (CoC) in a letter dated 7 April 2005 in relation to the earlier Draft Southern Suburbs District Structure Plan - Stage 3, 2005 (CoC Ref. 9669). DEC has also provided advice for local structure plans (CoC Ref. SM/M/059 - Lot 9001 Gaebler Road and Lot 35 Barfield Road; CoC Ref. SM/M/030 - Lots 43 & 44 Frankland Avenue) and subdivisions (WAPC Ref. 141420- Lots 36, 37 & 45 Gaebler Road and 101 Barfield Road; WAPC Ref. 143435- Lot 40 Gaebler Road; WAPC Ref. 143374- Lots 43 & 44 Frankland Avenue) within this structure plan area. The relevant aspects of DEC's advice and additional provided in the following advice.</p> <p>2. Wetland management and buffers</p> <p>A Conservation category wetland ('Frankland Avenue wetland') as identified in DEC's Geomorphic Wetlands Swan Coastal Plain dataset is located within the subject land (Lots 41 and 42). Conservation category wetlands (CCW) are wetlands that support a high level of ecological attributes and functions, and are the highest priority for protection. The Environmental Protection Authority (EPA) recommends that all Conservation category wetlands and their buffers are fully protected. Proposals that are likely to lead to a significant adverse impact upon these wetlands are likely to require formal assessment by the EPA. comments are:</p> <p>A. A buffer around the CCW is required to protect its ecological functionality. The width of the buffer should be determined based on the values of the wetland to be protected, the proposed adjacent land use(s), the threats posed by the adjacent land use and the management that will be applied to the adjacent land use. The wetland buffer should be measured outwards from the geomorphic wetland boundary, as displayed in DEC's Geomorphic Wetlands Swan Coastal Plain database. As an indication, EPA Guidance Statement No.33- Environmental Guidance for Planning and Development (EPA 2008) states that wetlands that are to be protected require a minimum 50 metre buffer distance.</p> <p>B. DEC notes that the Draft District Structure Plan (Figure 7) does not show the intended purpose of the CCW. DEC recommends that the CCW and an</p>	<p>1. Noted.</p> <p>2. Supported. The City recognises the importance of the wetland and has accordingly reflected its status as a Conservation Category Wetland on the SSDSP3 spatial map.</p> <p>2A. Supported. The SSDPS3 spatial map depicts a generic 50 metre buffer from the wetland boundary. It is noted that the specifics of the buffer will be determined at the LSP stage.</p> <p>2B. Not supported. The subject area is zoned 'Urban' under the Metropolitan Region Scheme and is privately owned. As such the ultimate intention</p>

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		<p>appropriate buffer be retained as Regional Open Space for conservation purposes. DEC also notes that the buffer boundary is not accurately depicted on the Draft District Structure Plan and should extend to the boundary of Lot 43 and further into Lot 39, reflecting the Local Structure Plan with the modified road layout dated 02.08.2010 for Lots 43 and 44 Frankland Avenue.</p> <p>C. Given the proposed adjacent residential land uses DEC recommends the preparation and implementation of a Wetland Management Plan for the Frankland Avenue wetland and buffer. The Wetland Management Plan should include strategies to vegetate the buffer where required and enhance existing native vegetation. The buffer should remain at natural ground level (without the use of fill) and revegetation should comprise local endemic species. The position of fences, gates and management access tracks should also be addressed in the plan. In planning for future subdivision, it should be noted that no development (including stormwater infrastructure) should be located within the CCW or the wetland buffer, nor should any stormwater drainage (including road drainage) discharge directly into the CCW. Drainage design should aim to maintain natural drainage processes (where possible). This could include the use of flush kerbs with drainage infiltration swales adjacent to the road to maximise infiltration at source. DEC supports the preparation and implementation of a Local Water Management Plan to the satisfaction of the Department of Water, as a condition of any future subdivision.</p> <p>3. Native vegetation and flora</p> <p>An extensive area of regionally significant native vegetation is located within the adjacent Beeliar Regional Park and DEC records indicate the presence of priority and rare flora within the local area, including <i>Dodonaea hackettiana</i> (Priority 4) and <i>Caladenia huegelii</i>, which is listed as 'flora considered likely to become extinct or rare therefore in need of special protection' (generally referred to as threatened flora) under the Wildlife Conservation Act 1950. DEC advises that a flora survey in accordance with EPA's Guidance Statement 51- Terrestrial Flora and Vegetation Surveys for Environmental Impact Assessment in Western Australia is required to determine the presence or absence of priority or rare flora, and priority or threatened ecological communities. If conservation significant flora or ecological communities are present on site, appropriate action should be undertaken to protect it, or to mitigate impacts. Such surveys should be carried out prior to finalising local structure plans, as the results may directly influence this process.</p> <p>DEC recommends areas of high conservation value, such as native vegetation in very good or good condition (Keighery 1994), which contain conservation significant flora species, and comprise fauna habitat, be retained and incorporated into future</p>	<p>is for the CCW area to be ceded at the subdivision stage in accordance with the City's and WAPC's standard approach.</p> <p>2C. Supported. Matters relating to wetland management/rehabilitation, buffers, water management etc. will be determined as part of future LSP's and subdivisions</p> <p>3. Supported. All LSP's will be required to be supported and informed by comprehensive flora studies prior to assessment and/or adoption.</p>

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		<p>development as POS reserved for Conservation and Passive Recreation. <i>Caladenia huegelii</i> is also protected by the Commonwealth's Environment Protection and Biodiversity Conservation Act 1999 (EPBC Act). Therefore, regardless of any decision under Western Australian planning or environmental approvals processes, the proponent should contact the Commonwealth Department of Sustainability, Environment, Water, Population and Communities (SEWPaC) to determine what responsibilities they have under the EPBC Act.</p> <p>4. Fauna management</p> <p>The subject land is known to serve as foraging habitat for the threatened Carnaby's black cockatoo (<i>Calyptorhynchus latirostris</i>). It is acknowledged that there are large remnants of native vegetation in the local area; however the cumulative impacts from the reduction of Carnaby's foraging habitat on the Swan Coastal Plain has resulted in vegetation that provides a food source for Carnaby's cockatoos being considered as significant habitat. The continual net loss of critical habitat will result in additional pressure on the current population of Carnaby's black cockatoos. Clearing of native vegetation, in particular banksia, jarrah or tuart trees will result in a loss of foraging habitat for this species.</p> <p>The subject land also comprises fauna habitat trees, as observed during a site visit on 3 July 2012. It must be considered that the development of nesting hollows is a dynamic process and so existing nesting hollows are important, as well as the maintenance of healthy trees to allow for the development of future hollows. In addition, mature hollow-bearing Eucalypts can be suitable for nesting by a range of bird species from small insectivores to the larger parrots.</p> <p>A. The area is also known to contain <i>Lomandra hermaphrodita</i>, which can support populations of the Graceful sun-moth (<i>Synemon gratiosa</i>). Both Carnaby's black cockatoo and Graceful sun moth are listed as 'fauna that is rare or likely to become extinct' under the Wildlife Conservation Act 1950. While the Graceful sun-moth is listed as threatened fauna, recent survey effort has significantly extended the range of the species and located more populations, including within conservation reserves. Therefore it is considered to be less threatened than the records at the time of listing suggested. DEC is currently reviewing the conservation status of Graceful sun moth and it is possible that DEC may subsequently recommend that Graceful sun-moth does not meet criteria for listing as 'endangered' but that it meets criteria for 'vulnerable', or may be found to be not threatened and therefore considered as priority fauna. Despite any possible change in status, it is important for proposals that may impact on Graceful sun-moth habitat to still consider actions for the conservation of Graceful sun-moth. Therefore, DEC requests further investigation and identification of potential</p>	<p>4. Supported. All LSP's will be required to be supported and informed by comprehensive fauna studies prior to assessment and/or adoption. Matters relating to vegetation retention, alternative designs, offsets etc. will be determined at the LSP and subdivision stage.</p> <p>4A. Noted. As above.</p>

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		<p>habitat including the specific distribution of <i>L. hermaphrodita</i>, followed by a targeted survey (in accordance with current DEC guidelines).</p> <p>The Priority 4 Quenda (<i>Isoodon obesulus</i> subsp. <i>fusciventer</i>) and the Priority 3 Lined skink (<i>Lerista lineata</i>) have also been identified within the local area. Given the potential fauna habitat value of the native vegetation on the subject land, DEC recommends that a fauna survey in accordance with the EPA's Guidance Statement 56- Terrestrial Fauna Surveys for Environmental Impact Assessment in Western Australia be undertaken prior to the local structure plan being finalised. If suitable habitat for conservation significant fauna is present on site, appropriate action should be undertaken to protect it, or to mitigate impacts.</p> <p>B. DEC recommends that the areas of native vegetation in good or better condition that comprise suitable habitat for conservation significant fauna and habitat trees also be incorporated into future development as POS reserved for Conservation and Passive Recreation. Both Carnaby's black cockatoo and Graceful sun-moth are protected by the Commonwealth's EPBC Act. Therefore, regardless of any decision under Western Australian planning or environmental approvals processes, the proponent should contact SEWPaC to determine what responsibilities they have under the EPBC Act.</p> <p>C. Possible environmental offsets Provided the development of the area within the subject land is considered to be environmentally acceptable by approval authorities, DEC would be prepared to assist in negotiations with the proponent regarding the possible development of environmental offsets for any residual impacts.</p> <p>5. Public open spaces and ecological corridors</p> <p>DEC considers that the current district structure plan does not adequately protect areas of high conservation value. The proposed POS outlined on the Draft District Structure Plan consists of up to 28 discreet areas for purposes such as drainage and active/passive recreation.</p> <p>A. DEC recommends the consolidation of POS areas as opposed to small fragmented sites, to ensure the long term viability of the protected areas and to reduce impacts from edge effects (e.g. weed incursion). This view is reflected in the Structure Plan report (page 19) "there are well located areas of good quality bushland in the structure plan area that are ideally suited for incorporation into neighbourhood parks. This will minimise the need for landscaping" and (page 29) "Where land holdings are to be consolidated and developed comprehensively, the POS areas can be amalgamated into larger parks which will reduce the City's</p>	<p>4B. Noted. As per recommendation (4.) above.</p> <p>4C. Noted. As per recommendation (4.) above.</p> <p>5. Noted. The SSDSP3 spatial map is notional only and the areas of POS have been in accordance with the requirement for all landowners to contribute 10% POS on their land. Matters of conservation and vegetation retention will be considered and determined at the LSP and subdivision stage.</p>

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		<p>maintenance costs".</p> <p>B. DEC notes that Frankland Reserve is vested in the City of Cockburn for the purpose of public recreation. Given it is an extensive area of bushland DEC concurs with the proposed development of a sports oval within the reserve, subject to the development being limited to the degraded area of approximately 3ha. If there is any requirement of additional areas for active recreation, then consideration should be for existing cleared areas to be utilised.</p> <p>C. DEC notes the proposed ecological linkage between the subject land, Harry Waring Reserve and Frankland Reserve as depicted in the Draft District Structure Plan. This linkage or greenway is an area of the Historic Baldivis Tramway Trail, which has been recently acquired by the Western Australian Planning Commission and is reserved for Parks and Recreation. This land is approximately 10m wide and a site visit on 3 July 2012 observed the area has been sown for vegetation growing. Although the concept of establishing and maintaining ecological corridors is supported, DEC questions the effectiveness of this linkage given its narrow width and the lack of existing/protected vegetation. DEC recommends a vegetated linkage of a minimum width of 20m between Harry Waring Reserve and Frankland Reserve, and between Harry Waring Reserve and Frankland Avenue wetland.</p> <p>6. Clearing permit</p> <p>The subject land contains native vegetation, which the applicant should make every effort to retain and protect. Any clearing of native vegetation requires a clearing permit obtained from DEC, unless of a kind that is exempt in accordance with Schedule 6 of the Environmental Protection Act 1986 or Regulation 5 of the Environmental Protection (Clearing of Native Vegetation) Regulations 2004. DEC notes that an application to clear 2ha of native vegetation on the private primary school site, Lot 46 Frankland Avenue, has been submitted to DEC's Native Vegetation Conservation Branch.</p> <p>7. Boundary interface treatment</p> <p>DEC notes that the western portion of the subject land abuts Harry Waring Reserve. The University of Western Australia currently has a lease from DEC to manage the reserve for conservation and research purposes; this effectively restricts public access to the reserve. DEC supports having a perimeter road between residential development and conservation areas, for reasons of public safety, protection of bushland and fire safety for residents. DEC recommends that the width of the perimeter road is adequate to accommodate all road, dual use path and drainage</p>	<p>5A. Noted.</p> <p>5B. Noted.</p> <p>5C. Not supported. The linkage is for the purposes of cultural heritage rather than ecological in this case and as such no change to the width of the link is recommended given it will fulfil its intended purpose within the current configuration.</p> <p>6. Noted.</p>

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		<p>infrastructure, and still allow for a gently sloping fill batter (perhaps 1 in 6) which meets the natural ground level well inside the road boundary, and which will allow for successful revegetation.</p> <p>In planning for any future subdivision, it should be noted that no vegetation, earth spoil or any other debris is to be stored or disposed of within the boundary of the regional park.</p> <p>8. Pedestrian and cyclist networks</p> <p>DEC suggests that the shared path network within the subject land should be extended to connect to Rowley Road, Kwinana Freeway and the future Mandogalup train station on the south side of Rowley Road. DEC supports the commitment to construct a dual use path along [the eastern side of] Hammond Road, as stated in the Structure Plan report (page 39) "it is considered appropriate that proportional contributions be made towards the construction of Hammond Road including the cost of land, full earthworks, construction of one carriageway and dual use path (one side only)".</p> <p>9. Site contamination</p> <p>Due to previous and current potentially contaminating land uses (e.g. horticultural activities and poultry farms); there is considerable potential for widespread soil and/or groundwater contamination within the subject land. Therefore, investigations for soil and groundwater contamination will need to be carried out in accordance with DEC's Contaminated Sites Management Series guidelines. Where these investigations identify soil and/or groundwater contamination that requires remediation to render the site suitable for the proposed use, such remediation (including validation of remediation) will need to be completed to the specifications and satisfaction of DEC's Contaminated Sites Branch, and in accordance with the requirements of the Contaminated Sites Act 2003. These investigations should be carried out prior to finalising local structure plans, as the results may have a direct bearing upon future allowable land uses.</p> <p>10. Air quality- buffering and appropriate land use planning</p> <p>DEC notes a minor portion of the subject land is within the Kwinana air quality buffer (transition zone) established under the Environmental Protection (Kwinana) (Atmospheric Wastes) Policy 1999 area with the potential of impacts on air quality within the subject land from emissions from the Kwinana Industrial Area, Alcoa Residue Storage Area and the Kwinana Motorplex. A portion of the subject land is also within a poultry farm buffer (potential dust, noise and odorous emissions) and</p>	<p>7. Supported. The SSDSP3 spatial map will be modified to outline the requirement for an interface road/s between residential development and conservation areas.</p> <p>8. Supported. All LSP's will be required to provide suitable pedestrian and cycle paths.</p> <p>9. Supported. LSP's will be required to be supported by appropriate contamination investigation (where required) prior to assessment and/or adoption.</p>

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		<p>there are market gardens or intensive horticulture located within the subject land (potential gaseous, dust, noise and odorous emissions).</p> <p>Consideration should be given to placement of residential areas to avoid any conflicts between different land uses within and adjacent to the subject land; any conflicts should be considered in planning for the area. DEC's standard position is not to support developments which are inconsistent with the generic setbacks outlined within EPA Guidance Statement No. 3 Separation Distances between Industrial and Sensitive Land Uses (June 2005).</p> <p>11. Noise management</p> <p>It is noted that future development is proposed against Frankland Avenue/Hammond Road and Rowley Road, which could result in potential noise impacts from road traffic. DEC notes the information provided in Section 4.9 Freight Corridor of the Draft Southern Suburbs District Structure Plan (February 2012) report, particularly regarding the need to comply with WAPC State Planning Policy 5.4 - Road and Rail Transport Noise and Freight Considerations in Land Use Planning (2009). Draft EPA Guidance Statement No.14- Road and Rail Transportation Noise (1998) may also be of assistance. DEC recommends the preparation of a noise management plan in relation to the road traffic noise prior to future subdivision planning.</p> <p>12. Acid sulfate soil management</p> <p>DEC records show a portion of the subject land as having a known "High to Moderate" risk of acid sulfate soils occurring within three metres of the natural soil surface or deeper. Therefore it is likely an acid sulfate soils management plan will be required.</p> <p>13. Weed control</p> <p>In order to protect the conservation values of the adjacent Harry Waring Marsupial Reserve and other conservation reserves (e.g. Frankland Reserve), the proponent should ensure that flora species known to be invasive or environmentally damaging are not used in any landscaping, where they may spread into the conservation areas.</p> <p>14. Fire management</p> <p>All necessary fire management requirements should be provided for future subdivisions, in accordance with the (Interim) Planning for Bushfire Protection Guidelines (Edition 2- Western Australian Planning Commission and Fire and Emergency Services Authority, May 2010) and any other relevant policies. DEC</p>	<p>10. Supported. Matters relating to buffers and separation distances will be determined at the LSP stage.</p> <p>11. Supported. Matters relating to noise and mitigation measures (if required) will be determined at the LSP stage.</p> <p>12. Noted.</p> <p>13. Supported. Matters relating to weed</p>

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		<p>notes the information provided in Section 4.4 Fire Risk of the Draft Southern Suburbs District Structure Plan (February 2012) report, particularly regarding "the use of a perimeter road and public open space areas are the preferred interface treatments. Well designed, these can significantly reduce the risk of bush fire hazard."</p> <p>DEC requests the provision of a perimeter road to provide adequate separation between conservation areas and urban development. Such a hard edge helps mitigate fire risk and provides improved access for fire suppression.</p>	<p>control will be determined at the subdivision/development stage.</p> <p>14. Supported. As per recommendation (7.) above. LSP's will be required to be supported and informed by Bushfire Management Plans (where appropriate).</p>
15	Burgess Design Group PO Box 8779 Perth BC WA 6849	<p>SUPPORT – subject to modifications</p> <p>1. We write with regard to the "Draft District Structure Plan -Southern Suburbs Stage 3" which is currently being advertised by the City for public comment. Burgess Design Group is pleased to lodge a submission on behalf of our client, E & G Developments Pty Ltd, representing the landowners of Lot 126 Wattleup Road (corner Frankland Avenue), Hammond Park. We also thank the City for the opportunity to attend the community/landowner forum in Success on Wednesday 11th July, which was professional, informative and interactive.</p> <p>Lot 126 Wattleup Road, (the subject land) is located in the 'Central Precinct' of the Draft District Structure Plan (DDSP), west of and abutting Frankland Avenue and north of Wattleup Road. The DDSP depicts the land as 'Medium' density residential with a portion (equating to 10%) of public open space in its north-western corner.</p> <p>North of the subject land is land also depicted as 'Medium' density residential with associated public open space, to the east is similar land, with higher density residential around a small Local Centre and both a Private Primary School and High School. To the south is the Frankland Reserve Regional Open Space (including the proposed active recreational area) and to the west abutting Lot 125 is similarly depicted, with density increasing to 'High' over Lot 124 and the larger Local Centre then further to the west. A key east-west road traverses the southern third of the subject land, together with a 'Major Shared Path'.</p> <p>Lot 126 is ideally located, benefiting from three road frontages and being within the 400 metre catchments of both Local Centres.</p> <p>It is noted that all services are now available for the development of Lot 126 to proceed. It is proposed to commence development once planning approvals have been granted. Importantly, some services are currently not available for the lower lying land to the west of Lot 126, which thus has an impact on the coordination of</p>	<p>1. Noted.</p>

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		<p>Local Structure Plan approvals and development, as discussed further herein.</p> <p>2. Key Elements of Proposed Draft District Structure Plan</p> <p>The DDSP Report outlines a number of elements in relation to the proposed DDSP. These are outlined below with key elements applicable to Lot 126 listed and our response then provided.</p> <p>A. Development Principles <i>"Provide a framework for future Local Structure Planning and subdivision, allowing for refinement of detail and recognition of previous uses." "Provide for a variety of housing choice through a range of densities, with higher coding being located near public open space, centres and along high frequency public transport routes "</i></p> <p>The subject land is located within the "Central Precinct", which is described as follows on the Draft DSP.</p> <p><i>"A comprehensive Local Structure Plan will be required for the central neighbourhood centre and adjacent development. The City will not consider individual structure plans in this location due to the need to ensure the suitable integration of development."</i></p> <p>We understand the City's position on this Central Precinct and the importance of ensuring that development is appropriately coordinated. However, there is a significant challenge in doing so, given the following:</p> <ul style="list-style-type: none"> - The complexity of the differing expectations of landowners; - The complexity of the differing timing aspirations of landowners; - The need to undertake technical analysis (for instance vegetation or heritage survey) over all landholdings when one owner may not wish to be party to the Local Structure Plan (thus restricting access); - The need for assumptions to be drawn on very detailed planning design - particularly in relation to the Local Centre; - The availability of services to parts of the precinct, but not the whole of the precinct; and, - The equitable sharing of costs for what is a complex and expensive exercise. <p>We would be supportive of advising other neighbouring landowners within the Central Precinct that our client is keen to prepare a Local Structure Plan (LSP) and to work with any owners wishing to participate. However, we oppose the possibility of being delayed or obstructed by those owners not willing to participate</p>	<p>2. Noted.</p> <p>2A. Noted.</p>

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		<p>and would simply propose to draw a notional layout over their land in accordance with the principles of the DDSP.</p> <p>In doing so our LSP would be comprehensive over our clients land and any other participating land owners and simply indicative only over other parcels.</p> <p>We further submit that the Central Precinct should be separated into west and east parts either side of Hammond Road, due to the significantly severing nature of this road and the associated tramway infrastructure. For instance, the subject land (Lot 126) has no influence or impact upon any land located west of Hammond Road.</p> <p>Moreover, given existing road frontage on three of its four sides, Lot 126 is an example of a lot that can be designed and implemented independently of surrounding lots, needing only to interface directly with the abutting Lot 125.</p> <p>In relation to the framework for Local Structure Planning, the City's requirements for planning of the Central Precinct and with regard to the provision of a variety of housing choice, we make the following comment.</p> <p><u>Comment</u></p> <p>i) We object to the requirement for a comprehensive Local Structure Plan for the 'Central Precinct'. We respectfully request that the City accept individual Local Structure Plans from owners within the Central Precinct provided that the individual LSP's provide broad brush/indicative planning over abutting landholdings to show a level of coordination and functionality between them. For instance, a road and local open space layout could be shown over the whole of the Central Precinct based on broad level assumptions, with complex areas such as the Local Centre requiring the provision of a future Detailed Area Plan by the relevant site owner.</p> <p>ii) We support the notion of providing a variety of housing choice over the subject land.</p> <p><u>B. Neighbourhood Structure</u> <i>"Planning of the area is based on walkable catchments of 400m (5 minute walk) which focus on centres, significant areas of public open space and public transport."</i></p> <p><u>Comment</u></p> <p>i) We note and support that the subject land is uniquely positioned within overlapping catchments of both proposed Local Centres within the DDSP and</p>	<p>2Ai).Not supported (in part). The City maintains the requirement for a comprehensive LSP to cover the identified 'Central Precinct' however the extent of the precinct will be reduced from that shown on the Draft SSDSP3 spatial map. The precinct will only now include Lots 114, 123 and 124 Wattleup Road given the presence of the Neighbourhood Centre, supporting mixed businesses and higher density residential development proposed for these sites.</p> <p>2Aii).Noted.</p> <p>2B. Noted.</p>

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		<p>is also located within walking distance of both the Private Primary School and High School.</p> <p>C. <u>Major Road Network</u> <i>"The main east-west link is Wattleup Road, which is proposed to be realigned to the north to avoid steep contours at the eastern end of existing Wattleup Road."</i></p> <p>The subject land abuts the eastern portion of Wattleup Road and the DDSP thus shows the proposed realignment within Lot 126.</p> <p><u>Comment</u> i) We support the proposed realignment of Wattleup Road and agree to ensure that planning design within Lot 126 reflects this new alignment to integrate with abutting land holdings to the west and east.</p> <p>D. <u>Traffic Management</u> <i>"Traffic lights will be required at the intersection of Hammond Road and realigned Wattleup Road to facilitate traffic and pedestrian/cyclist movement."</i></p> <p>The requirement for traffic lights at this intersection within the Central Precinct and abutting the Local Centre adds to the complexity of this Local Structure Plan area.</p> <p><u>Comment</u> i. We reiterate our previous comment that individual Local Structure Plans within the Central Precinct should be permitted by the City provided that coordination between landholdings occurs with notional plans drawn based on land use assumptions of the DDSP.</p> <p>E. <u>Shared Paths/Connections</u> <i>"An interconnecting network of shared paths will be provided within the Structure Plan area with the main connections being identified as Wattleup Road"</i></p> <p><u>Comment</u> i. We support the notion of pedestrian connectivity and permeability within the DDSP area and as such the provision of a main connection along the realigned Wattleup Road within the subject land. ii. It should be noted that the text on Page 23 does not specifically reference the 'Major Shared Path' as being on the realigned portion of Wattleup Road, though the plan of the DDSP does.</p> <p>F. <u>Residential Development</u> <i>"The detail, including specification of density codes as per the provisions of the</i></p>	<p>2Bi). Noted.</p> <p>2C. Noted.</p> <p>2Ci). Noted.</p> <p>2D. Noted.</p> <p>2Di. Not supported. As per recommendation (2Ai) above.</p> <p>2E. Noted.</p> <p>2Ei. Noted.</p> <p>2Eii. Noted. Reference in the text relates to both the existing and the realigned portion/s of Wattleup Road.</p>

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		<p><i>Residential Design Codes of Western Australia (R-Codes), will be clarified through both the LSP and subdivision processes."</i></p> <p><u>Comment</u></p> <p>i. We support the notion of applying density codes at the Local Structure Plan stage in accordance with the intentions of the DDSP.</p> <p><u>G. Housing Principles</u></p> <p><i>"Achieve minimum targets of 15 dwellings per gross urban zoned hectare of land (inclusive of land required for infrastructure and POS) and 22 dwellings per site hectare of residential/and (exclusive of land required for infrastructure and POS)."</i></p> <p>The subject land is approximately 4.13ha in area. Based on the above principles the anticipated yield per gross hectare would be 62 lots and per site hectare of residential land (being the overall site area minus land for POS and drainage) would be some 80 lots.</p> <p><u>Comment</u></p> <p>i. We support the objectives stated in 'Development Principles', 'Residential Development' and 'Housing Principles' with regard to diversity of housing and density.</p> <p>ii. We anticipate providing a range of lot sizes on Lot 126 and achieving an overall dwelling yield (both single house lots and group housing dwellings) of approximately 70 to 80 dwellings per the principles of the DDSP.</p> <p><u>H. Residential Densities and Housing Types</u></p> <p><i>"Residential R30 will be the minimum base coding over the SSDSP3 area. Allowing for the provision of traditional single dwelling lots from 300m²"</i></p> <p>"Residential R35-60 generally surrounding areas of high amenity, such as open space"</p> <p>"Located to enhance passive surveillance of public spaces."</p> <p><u>Comment</u></p> <p>i. We support the diversity created by providing a minimum base coding of R30 and encouraging lot sizes from 300m²</p> <p>ii. We support the flexibility of coding (R35-60) adjacent to POS; and,</p> <p>iii. We assume that the City will accept larger lot sizes in order to achieve the necessary diversity, provided development occurs in accordance with density targets.</p>	<p>2F. Noted.</p> <p>2Fi. Noted</p> <p>2G. Noted.</p> <p>2Gi. Noted.</p> <p>2Gii. Noted.</p> <p>2H. Noted.</p> <p>2Hi. Noted.</p> <p>2Hii. Noted.</p> <p>2Hiii. Noted. The acceptance or otherwise of 'larger lots' will be determined at the LSP stage and will only be supported</p>

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		<p>I. <u>Neighbourhood/Local POS- Location and Designation</u> <i>"Securing bushland not only in recognition of the quality of the bushland in the area but also the potential difficulty in securing an adequate water allocation for irrigation purposes."</i></p> <p><i>"Along ownership boundaries to enable owners to be able to satisfy their 10% public open space (POS) requirements within their own land holding."</i></p> <p><u>Comment</u></p> <p>i. We support the principle that a land owner satisfies their own requirement for the provision of 10% POS;</p> <p>ii. We support the protection of high quality bushland where necessary, balanced with the need to also create areas for passive recreation (such as small grassed spaces) for usability and amenity; and,</p> <p>iii. We support the integration of drainage within POS as necessary.</p> <p>J. <u>Neighbourhood/Local POS- Design and Embellishment</u> <i>"The predominant use should be for informal recreation for individuals and households, especially low level children's play, dog walking and relaxation."</i></p> <p><i>"Embellishment with infrastructure above this role will not be accepted."</i></p> <p><i>"Types of supporting amenities could include paths, shade, seating, children's play spaces etc."</i></p> <p><u>Comment</u></p> <p>i. We support the above stated comments in relation to the design and embellishment of POS.</p> <p>K. <u>Planning Control</u> <i>"To progress the subdivision and development of a land holding it will be necessary for landowners or groups of small landowners to prepare and submit a detailed LSP and supporting report for their land"</i></p> <p><u>Comment</u></p> <p>i. We support the notion that landowners can prepare and submit an LSP on an individual basis and request the same for the subject land, Lot 126 Wattleup Road.</p> <p>3. Summary</p> <p>The following are the summary points of our submission on the proposed Draft</p>	<p>where it is demonstrated that the relevant density targets will not be compromised.</p> <p>2I. Noted.</p> <p>2Ii. Noted.</p> <p>2Iii. Noted.</p> <p>2Iiii. Noted.</p> <p>2J. Noted.</p> <p>2Ji. Noted.</p> <p>2K. Noted.</p> <p>2Ki. Noted. As per recommendation (2Ai) above.</p>

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		<p>District Structure Plan Southern Suburbs Stage 3.</p> <p>A. We object to the requirement for a comprehensive Local Structure Plan for the 'Central Precinct'. We respectfully request that the City accept individual Local Structure Plans from owners within the Central Precinct provided that the individual LSP's provide broad brush/indicative planning over abutting landholdings to show a level of coordination and functionality between them. For instance, a road and local open space layout could be shown over the whole of the Central Precinct based on broad level assumptions, with complex areas such as the Local Centre requiring the provision of a future Detailed Area Plan by the relevant site owner;</p> <p>B. We support the notion of providing a variety of housing choice over the subject land;</p> <p>C. We note and support that the subject land is uniquely positioned within overlapping catchments of both proposed Local Centres within the DDSP and is also located within walking distance of both the Private Primary School and High School;</p> <p>D. We support the proposed realignment of Wattleup Road and agree to ensure that planning design within Lot 126 reflects this new alignment to integrate with abutting landholdings to the west and east;</p> <p>E. We reiterate our previous comment that individual Local Structure Plans within the Central Precinct should be permitted by the City provided that coordination between landholdings occurs with notional plans drawn based on land use assumptions of the DDSP;</p> <p>F. We support the notion of pedestrian connectivity and permeability within the DDSP area and as such the provision of a main connection along the realigned Wattleup Road within the subject land;</p> <p>G. It should be noted that the text on Page 23 of the DDSP report does not specifically reference the 'Major Shared Path' as being on the realigned portion of Wattleup Road, though the plan of the DDSP does;</p> <p>H. We support the notion of applying density codes at the Local Structure Plan stage in accordance with the intentions of the DDSP;</p> <p>I. We support the objectives stated in 'Development Principles', 'Residential Development' and 'Housing Principles' with regard to diversity of housing and</p>	<p>3. Noted.</p> <p>3A. As per recommendation (2Ai) above.</p> <p>3B. Noted.</p> <p>3C. Noted.</p> <p>3D. Noted.</p> <p>3E. As per recommendation (2Ai) above.</p> <p>3F. Noted.</p> <p>3G. As per recommendation (2Eii) above.</p> <p>3H. Noted.</p>

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		<p>density;</p> <p>J. We anticipate providing a range of lot sizes on Lot 126 and achieving an overall dwelling yield (both single house lots and group housing dwellings) of approximately 70 to 80 dwellings per the principles of the DDSP;</p> <p>K. We support the diversity created by providing a minimum base coding of R30 and encouraging lot sizes from 300m2 and upwards;</p> <p>L. We support the flexibility of coding (R35-60) adjacent to POS;</p> <p>M. We assume that the City will accept larger lot sizes in order to achieve the necessary diversity, provided development occurs in accordance with density targets;</p> <p>N. We support the principle that a land owner satisfies their own requirement for the provision of 10% POS;</p> <p>O. We support the protection of high quality bushland where necessary, balanced with the need to also create areas for passive recreation (such as small grassed spaces) for usability and amenity;</p> <p>P. We support the integration of drainage within POS as necessary;</p> <p>Q. We support the comments in relation to the design and embellishment of POS; and,</p> <p>R. We support the notion that landowners can prepare and submit an LSP on an individual basis and request the same for the subject land, Lot 126 Wattle up Road.</p> <p>We thank the City for the opportunity to provide comment on the Draft District Structure Plan Southern Suburbs Stage 3 and trust this submission is to its satisfaction.</p>	<p>3I. Noted.</p> <p>3J. Noted.</p> <p>3K. Noted.</p> <p>3L. Noted.</p> <p>3M. As per recommendation (2Hiii) above.</p> <p>3N. Noted.</p> <p>3O. Noted.</p> <p>3P. Noted.</p> <p>3Q. Noted.</p> <p>3R. As per recommendation (2Ai) above.</p>
16	Burgess Design Group PO Box 8779 Perth BC WA 6849	<p>OBJECT</p> <p>We write with regard to the "Draft District Structure Plan -Southern Suburbs Stage 3" (DDSP) which is currently being advertised by the City for public comment. Burgess Design Group is pleased to lodge a submission on behalf of our client, Primewest Management, representing the landowners (Wattleup Syndicate) of lots 117 & 805</p>	

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		<p>Wattleup Road, Wattleup.</p> <p>We also thank the City for the opportunity to attend the community/landowner forum in Success on Wednesday 11th July, which was professional, informative and interactive. The purpose of this correspondence is to make submission on the DDSP, as follows.</p> <p>1. Figure 7 of DDSP depicts the subject land as within an area "Subject to Future Structure Planning". Text on the plan then states: <i>"Future structure planning of the cell south of Wattleup Road must provide an appropriate interface with residential development north of Wattleup Road. This is to have particular regard to the position of the Western Australian Planning Commission in respect of its position on the acceptability of (or otherwise) of residential development south of Wattleup Road, and alternative (non-residential) land uses that may be required. Any impacts associated with the future development of non-residential land uses in the cell south of Wattleup Road must be fully contained within the cell boundaries."</i></p> <p>Section 4.1.3 on page 17 of the DDSP Report comments in relation to the Alcoa Residual Storage Area buffer and states:</p> <p><i>"The expanded buffers include an additional 0.5 kilometres restricting residential expansion and development of some other sensitive land uses (i.e. kindergartens, hospitals and aged persons housing). The purpose of the additional buffer area was to recognise uncertainties related to the level and frequency of impacts from the Alcoa Residue Disposal Area and Kwinana Industrial Area more broadly. The WAPC have stated that the extended portion of the buffer will be reviewed in five years' time to ensure that its extent and location reflects the current conditions in the area."</i></p> <p>Section 5.9 on page 33 of the DDSP Report provides additional comment on the area subject to future structure planning.</p> <p><i>"The decision not to exclude the area entirely from the SSDSP3 reflects the future desire for residential or compatible development to occur within this area subject to the appropriate modelling and environmental studies being undertaken. This is in line with the advice of the State Administrative Tribunal (SAT), providing that current modelling which was used to prepare the buffer should be given no weight, and needs to be undertaken again. Once this has been completed and the actual implications known, appropriate structure planning can then take place for this land."</i></p> <p><u>Comment</u> We acknowledge the complexity surrounding the planning of this portion of the DDSP area and the history of negotiations and assessment in relation to the Alcoa buffer. The following is an overview of a number of the planning elements to date.</p>	<p>1. Not supported. The following findings of the SAT in the Wattleup Road Development Company Pty Ltd vs. Western Australian Planning Commission [2011] WASAT 160 matter directly relate to the SSDSP3 -</p> <p><i>"3. The Tribunal found that there is a threat of serious or irreversible environmental damage for residents of the proposed subdivision in relation to dust from the residue disposal area and the sand quarry. The Tribunal also found that there is scientific uncertainty as to the environmental damage. The conditions precedent to the satisfaction of the precautionary principle were therefore established. Consequently, a precautionary measure may be taken to avert the anticipated threat of environmental damage, provided that it is proportionate to the threat. The Tribunal determined that precluding subdivision until adequate air quality monitoring at the site demonstrates acceptability is proportionate to the threat, appropriate and cost-effective. The Tribunal found that adequate air quality monitoring would require prior consultation with the Department of Environment and Conservation, monitoring for a period of at least 12 months and assessment and reporting in relation to relevant health and amenity standards and chemical</i></p>

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		<p>The Kwinana Industrial Area and the surrounding areas have been the subject of extensive planning, environmental, strategic and statutory studies in regard to land use patterns and the impact of established industrial land uses on air quality for some thirty years.</p> <p>In 1980 the then Metropolitan Region Planning Authority published 'A Planning Strategy for the South West Corridor' which referenced the findings of the Kwinana Air Modelling Study and defined a buffer surrounding the Kwinana Industrial Area.</p> <p><u>Lots 117 and 805 were shown to be located within an area suitable for residential development.</u></p> <p>In 1990 the then Department of Urban Development released the Urban Expansion Policy Statement for the Perth Metropolitan Region, which identified the Wattleup locality as a Category B area defined as being suitable for urban development subject to the resolution of constraints. Specific details of the constraints impacting on the Wattle up locality were not outlined.</p> <p>In 1992 the Environmental Protection (Kwinana) (Atmospheric Waste) Policy was gazetted under Part III of the Environmental Protection Act 1986 to provide a statutory framework for the EPA to maintain acceptable air quality levels within the locality.</p> <p>The EPP defined an air quality buffer that reflected the buffer line published in A Planning Strategy for the South West Corridor. <u>Lots 117 and 805 were shown to be located within Area C where rural and residential/and uses were identified as being suitable.</u> This recommendation was supported by the imposition of more stringent air quality standards, compared to Areas A and B, to protect existing and future residents.</p> <p>In 1999 the Environmental Protection Authority undertook a formal review of the 1992 EPP and re-issued the policy unchanged. The decision to re-issue the policy unchanged was in consideration of a number of factors, including, but not limited to:</p> <ul style="list-style-type: none"> • Dramatic improvements in air quality from the late 1970's to 1997; • The WAPC were preparing the Fremantle Rockingham Industrial Area Regional Strategy which would identify appropriate land uses within the buffer areas as defined by the EPP; and, • The draft State Air EPP, which would outline state wide standards for ambient air quality in accordance with the national standards, was soon to be released and may result in the Kwinana EPP being repealed. 	<p><i>composition of dust.</i></p> <p><i>4. The Tribunal found that no weight should appropriately be placed on the definition by the Western Australian Planning Commission of an off-site buffer from the residue disposal area affecting the site, because of significant methodological concerns in relation to the scientific assessment utilised for the purpose of establishing the buffer. The Tribunal observed that it would seem sensible for the air quality monitoring and assessment in relation to the site referred to in the previous paragraph to inform the confirmation or variation of the buffer by the Western Australian Planning Commission. The Tribunal also suggested that, in light of the significant methodological concerns raised in the evidence and the inadequate duration of the monitoring that underlay the scientific assessment used by the Western Australian Planning Commission to define the buffer, the buffer should not be reflected in the town planning framework at this stage, and that any amendment of the planning framework in this respect should await the results of the air quality monitoring and assessment at the site."</i></p> <p>Given the uncertainties outlined above, the City has an obligation to landowners and developers within the affected area to ensure that relevant planning framework reflects current decisions and issues and does not unnecessarily create false expectations in relation to</p>

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		<p>In 2000 the WAPC published the 'Fremantle- Rockingham Industrial Area Regional Strategy' to provide a strategic land use planning direction in response to growing development pressure and the need to protect the Kwinana Industrial Area as a strategic industrial area for the State.</p> <p>The Strategy reflected the Kwinana EPP buffer (a line that combined Areas A and B from the Kwinana EPP) and illustrated <u>Lots 1.17 and 805 as being zoned Urban Deferred</u> under the Metropolitan Region Scheme (MRS) and located wholly outside of the Kwinana EPP Air Quality Buffer.</p> <p>In 2002, the WAPC together with the then Department of Environmental Protection and the Department of Minerals and Petroleum Resources, released the 'Review of the Kwinana Air Quality Buffer' paper which identified a Residential Exclusion Area, a composite planning and environmental buffer, to protect both nearby residents and the Kwinana Industrial Area.</p> <p>The Residential Exclusion Area was defined following consideration of all of the constraints to residential development, including the Alcoa Residue Storage Areas. The Review proposed that further investigations be undertaken to better define the extent of the noise and dust impact of the Alcoa facility illustrating the potential for offsite impacts south of the future Rowley Road extension (south of lots 117 and 805), but highlighted that "Until this work is completed the current EPP buffer line be retained".</p> <p>In October 2008 the WAPC released the 'Review of the Kwinana Air Quality Review-Position Paper' to provide an update to the 2002 Review and the status of its recommendations. The Paper identified a modified buffer to be implemented from 2008 to 2009 to provide certainty until further information became available in 2009 and the buffer is again reviewed and finalised.</p> <p><u>The plan included within the Paper illustrates Lots 1.17 and 805 as being located wholly outside of the buffer line and wholly outside of land illustrated as being an "area of further investigation" and labelled Area 5 and Area 9.</u></p> <p>In 2009, the WAPC continued to support the future urban development of lots 117, 805 and the surrounding land with the release of <u>Draft Directions 2031 which identified the land as 'Urban'</u>.</p> <p>In August 2010 the WAPC released the final <u>Directions 2031 document again depicting the subject land as 'Urban'</u>.</p>	<p>development potential of sites.</p> <p>The City therefore designated the area affected by the buffer within the SSDSP3 as 'subject to future structure planning'. The decision not to exclude the area entirely from the SSDSP3 reflects the future desire for residential or compatible development to occur within this area subject to the appropriate modelling and environmental studies being undertaken. This is in line with the advice of the SAT, providing that current modelling which was used to prepare the buffer should be given no weight, and needs to be undertaken again. Once this has been completed and the actual implications known, appropriate structure planning can then take place for this area.</p> <p>It should be noted that the current LSP for the site was adopted by Council prior to the progression of the Alcoa Air Quality Buffer expansion. Clause 6.2.12.1(a) of the City's Scheme stipulates that an LSP only comes into effect on the day on which it is endorsed by the WAPC. As the WAPC has not provided its endorsement to the LSP, it can only be referred to as an LSP adopted by Council and not one which has been endorsed by the WAPC.</p>

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		<p>The comments above outline the extensive 30 year history of planning and environmental strategic and statutory planning documents that have been prepared and published to guide decision making authorities on the growth and development of a portion of the South West metropolitan corridor in the context of the Kwinana Industrial Area, <u>all of which identified Lots 117 and 805 for future urban development and wholly located outside of the Kwinana Air Quality Buffer, Residential Exclusion Area and areas identified for further investigation.</u></p> <p>It was on this basis that our client purchased the land and commenced work to obtain the necessary approvals to facilitate the urban development of the land, including the engagement of a comprehensive project team. A brief summary of the key milestones is outlined below.</p> <p>In May 2008, on behalf of Primewest Management (Wattleup Syndicate) and in support of the request to transfer the land to the Urban zone of the MRS, 360 Environmental undertook a dust study on lots 117 and 805.</p> <p>The results of the study found that over the three month sampling period there were no breaches to the recommended guidelines for either PM10 or dust deposition and an analysis of the data collected from wind coming from between 180°-235° (the general direction of the Alcoa Bauxite residue storage area) revealed that there were no significant dust levels of concern.</p> <p>On 31 October 2008, in consideration of the planning and environmental framework, outlined above and the extensive and detailed work undertaken by the various landowners and their consultant teams, lots 117 and 805 (and surrounding landholdings) were transferred from the Urban Deferred zone to the Urban zone under the MRS, by way of a notice being published in the Government Gazette following a resolution of the WAPC.</p> <p>The decision of the WAPC to transfer the land to the Urban zone confirmed its suitability for urban purposes and on this basis Primewest (Wattleup Syndicate) made significant financial investments to undertake further detailed planning and design to facilitate the subdivision and development of the land in accordance with its Urban zoning, with the support of the City of Cockburn.</p> <p>On 11 December 2008 the City of Cockburn Council resolved to adopt the local Structure Plan for lots 117 and 805 and subsequently forwarded the documents and the recommendation to the WAPC for assessment and approval.</p> <p>Since it's gazettal, the Kwinana EPP has provided three essential functions, one being to provide a tool to enable the DEC to manage industrial emissions to an</p>	

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		<p>acceptable standard within each of the three buffer areas, the second to prevent the encroachment of sensitive land uses within the core buffer area and the third to protect the air quality for existing and future residents of areas fringing the Kwinana Industrial Area.</p> <p>Whilst the Kwinana Air Quality Buffer has been the subject of ongoing reviews by the Environmental Protection Authority and the WAPC for in excess of 10 years, commencing with the review of the 1992 EPP in 1999, the delineation of the Buffer remains largely unchanged.</p> <p>Some 44 months have now passed since the land was zoned Urban under the MRS and 43 months since the local Structure Plan for lots 117 and 805 was adopted by the City of Cockburn.</p> <p>In consideration of the comprehensive orderly and proper planning process that has been undertaken over the past 30 years, a process that ultimately supported the Urban zoning of lots 117 and 805 and subsequently significant financial investment by Primewest (Wattleup Syndicate) it is unreasonable for either the City or WAPC to place the land under a 'Subject to Future Structure Planning' classification in the Draft District Structure Plan Southern Suburbs Stage 3.</p> <p>With regard to the recent SAT matter, our client has been party to that decision and is thus undertaking environmental analysis to determine the validity of appropriate buffer distances in accordance with relevant guidelines. We are confident, based on research previously undertaken and as outlined above, that the buffer will be reduced as part of this work and development of the land can then be progressed.</p> <p>Moreover, we again remind the City that the subject land has an endorsed local Structure Plan.</p> <p>2. Submission Summary</p> <p>The following are the summary points of our submission on the Draft District Structure Plan Southern Suburbs Stage 3.</p> <p>A. There has been a considerable history of planning and environmental work, studies and reports in relation to the future development of this area over the past 30 plus years. The subject land has been identified for urban and residential purposes and as being located outside of these buffers over this time;</p> <p>B. We note that the DDSP shows an additional 500m buffer associated with the Alcoa Residual Storage Area and further note the SAT decision that questions the</p>	

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		<p>accuracy of the current modelling associated with the definition of this buffer;</p> <p>C. We strongly oppose the inclusion of Lots 117 & 805 within the precinct shown on the DDSP as being 'Subject to Future Structure Planning', given the historical planning framework outlined herein and the currently approved Local Structure Plan over the site;</p> <p>D. We strongly oppose use of the subject land or other properties situated within the area shown in the DDSP as being 'Subject to Future Structure Planning' for purposes other than residential development. We anticipate that the buffer review will determine that there is no impact on this land and that it will thus be available for full and unconstrained urban development;</p> <p>E. We confirm that the only outstanding planning element requiring resolution before the subject land can be developed for urban purposes is resolution of the buffer in accordance with the SAT decision, with relevant studies currently being undertaken in this regard; and,</p> <p>F. We again confirm that the subject land has an approved Local Structure Plan and that once the buffer is finally determined to not impact the site, development for residential purposes can and will occur.</p> <p>We thank the City for the opportunity to provide comment on the Draft District Structure Plan Southern Suburbs Stage 3 and trust this submission is to its satisfaction.</p>	<p>2. Noted.</p> <p>2A. Noted. However decisions need to reflect orderly and properly planning based on relevant current factors.</p> <p>2B. Noted. The buffer is shown in accordance with the WAPC's 'Revised Kwinana Industrial (including Air Quality) Buffer, as of 21 September 2010' spatial plan.</p> <p>2C. Not supported. As per recommendation (1.) above.</p> <p>2D. Not supported. As per recommendation (1.) above.</p> <p>2E. Noted. The City will only consider future LSP's and applications for development/subdivision once the matters outlined by the SAT have been addressed.</p> <p>2F. Noted. The current LSP for the site was adopted by Council prior to the progression of the Alcoa Air Quality Buffer expansion. Clause 6.2.12.1(a) of the City's Scheme stipulates that an LSP only comes into effect on the day</p>

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			on which it is endorsed by the WAPC. As the WAPC has not provided its endorsement to the LSP, it can only be referred to as an LSP adopted by Council and not one which has been endorsed by the WAPC.
17	Affected Landowner (Names & address withheld)	<p>OBJECT</p> <ol style="list-style-type: none"> 1. Loss of locally and regionally significant vegetation and fauna habitat, Loss of locally and regionally significant vegetation and fauna habitat on lots 46, 33, 32 and 31. Submission as a private citizen. 2. As a local resident I am concerned with the proposed Southern Suburbs District Structure Plan - Stage 3 (the Plan or Draft SSDSP3) and the impact the proposal will have on local areas of remnant native vegetation and the associated flora and fauna that inhabit these areas within DA26. The Plan appears to lack consideration of the high environmental value of large areas of remnant native vegetation within Stage 3 of the Plan, in the area referred to as DA26. Large areas of remnant native vegetation occur within DA26, with some areas of vegetation considered to be in Excellent and Pristine condition when reviewed against the Keighery Vegetation Condition Rating (Keighery 1994, and enclosed). Parts of these remnant vegetation areas are also utilised by local threatened fauna on a regular basis, including the Carnaby's Black Cockatoo (<i>Calyptorhynchus latirostris</i>) (Endangered) (photographs enclosed) and Quenda (<i>Isodon obesulus</i>), and provide important habitat for a variety of local fauna including birds, reptiles and mammals. 3. Details of specific concerns with the Plan are listed below. A CD of photographs to demonstrate the high conservation value of the vegetation within a portion of the DA26 area is provided with this submission for reference. 4. <u>Lack of Environmental Impact Assessment Information, Especially in Relation to Native Vegetation</u> <p>The Draft Southern Suburbs District Structure Plan - Stage 3 provides very little information on the assessment of environmental values within the proposal area, specifically DA26 which comprises of large areas of remnant vegetation. Apart from a very brief paragraph under Section 3.1 (Physical Description) and 4.6 (Open Space Opportunities) of the Plan, sections 2.8 (Environmental Protection), 3 and 4 of the Plan provide no information or background on the remnant native vegetation within DA26 and the quality and diversity of this native vegetation, particularly in regards to how representative the vegetation communities may be when compared to remaining vegetation levels across the Swan Coastal Plain or Perth Metropolitan Region.</p>	<ol style="list-style-type: none"> 1. Noted. 2. Noted. All LSP's will be required to be supported and informed by comprehensive flora and fauna studies prior to assessment and/or adoption. Matters relating to vegetation retention, alternative designs, offsets etc. will be determined at the LSP and subdivision stage on the advice of the DEC and other relevant authorities. <p>The EPA provided its broad assessment and input at the time when the area was rezoned to Urban under the MRS.</p> <ol style="list-style-type: none"> 3. Noted. 4. Noted. The SSDSP3 is a guiding document prepared by the City to prove a suitable planning framework for landowners and developers to undertake structure planning and subdivision for land within the subject area. Matters relating to detailed site investigations and studies are the responsibility of landowners/developers at the LSP

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		<p>Section 4.6 (Open Space Opportunities) of the Plan states that almost two thirds of the SSDSP3 is vacant or remnant undeveloped land. However no separation is made between the two (vacant undeveloped or remnant undeveloped) to provide for a comparison and therefore gauge their significance in regards to land lost to future development. It would appear on aerial maps that the majority of the proposed DA26 area is remnant vegetation of high quality and conservation value.</p> <p>The Plan also does not consider local fauna and the impact the proposal is likely to have on threatened fauna given the potential for remnant vegetation areas within DA26 to be significant habitat. Carnaby's Black Cockatoo (<i>Calytorhynchus latirostris</i>) (Endangered) frequently utilises native vegetation on Lot 33 for foraging and feeding and is one such fauna species likely to be affected by the extent of the proposed residential/commercial developments within DA26.</p> <p>A. <u>Recommendations for the Draft SSDSP3:</u></p> <p>Inclusion of additional information in the Plan on the environmental values within DA26 including extensive information on native vegetation, flora and fauna within DA26 and their significance on a local and regional scale;</p> <ul style="list-style-type: none"> • Inclusion of mapping in the Plan showing the extent, quality and significance of remnant native vegetation within DA26; • Inclusion of an assessment of potential threatened flora/fauna within DA26 and the impact of the proposal on these species, with details of the findings included in the Plan for consideration. <p>5. <u>Lack Of Conservation Of Remnant Native Vegetation</u></p> <p>As mentioned above the Draft SSDSP3 area includes large areas of remnant native vegetation. These areas of vegetation are marked for a range of development including residential/commercial development, public open space and community facilities (e.g. schools).</p> <p>The State's Strategic Plan for Perth's Greenways (1998) and the City of Cockburn's Greening Plan both aim to maintain and enhance remnant vegetation for flora/fauna conservation. Both plans are referred to in the Draft SSDSP3. However it would appear that neither plan has been considered in any great extent in the preservation and conservation of remnant native vegetation within the SSDSP3 area, and specifically DA26.</p>	<p>stage.</p> <p>4A. Not supported. It is not appropriate at the broad SSDSP3 level to undertake detailed studies which will be undertaken at the more detailed LSP stage.</p> <p>5. Noted. All LSP's will be required to be supported and informed by comprehensive flora studies prior to assessment and/or adoption. Matters relating to vegetation retention, alternative designs, offsets etc. will be determined at the LSP and subdivision stage on the advice of the DEC and other relevant authorities.</p>

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		<p>A number of lots within DA26 comprise of remnant native vegetation in excellent to pristine condition when compared to the Keighery Vegetation Condition Rating (Keighery 1994). From my direct observations these include Lots 46 and 33, with lots 32 and 31 comprising vegetation in good to excellent condition. It would appear on aerial mapping (Google maps, 2012) that a number of additional lots within DA26, being lots 48, 49, 50, 124, 125 and 126, also comprise remnant native vegetation of a high quality. These are large areas of remnant vegetation at risk of being cleared and developed, within the already highly cleared Swan Coastal Plain and Perth Metropolitan Region.</p> <p>Detail on the condition of vegetation within Lots 33 and 46 is enclosed within this submission for your consideration. Photographs are also provided of vegetation within lots 32 and 31 for review, although it is noted that generally the vegetation along the firebreaks on these properties that was easy to photograph did not appear to be in as good a condition as the vegetation observed further in from the firebreak.</p> <p>This summary and photographs of these properties demonstrates that the remnant vegetation within DA26 is of high conservation value, and is likely to be significant as a remnant of vegetation within the respective vegetation communities. The summary also demonstrates that the vegetation communities are varied across the proposal area rather than homogenous across the DA26 area. The vegetation ranges from dense Banksia spp. woodland with a dense Xanthorrhoea sp. understorey on the northern portion of Lot 33 to predominantly Eucalyptus/Sheoak open woodland with a dense herb understorey within the southern portion of Lot 46. Therefore, the vegetation within DA26 is considered likely to host a number of local flora and fauna species, including those of conservation value (e.g. threatened flora/fauna). Detail is provided on these lots only as they occur immediately adjacent to my residential property and were accessible by a firebreak.</p> <p>Given the amount of high quality remnant native vegetation, the Draft SSDSP3 should contain provision to reserve and conserve these areas of vegetation for flora/fauna protection and locate development as much as practicable to those areas that are already cleared, have been cleared, or areas of vegetation with low conservation value. However, this is missing from the Plan and significant areas of high quality vegetation are proposed for development.</p> <p>In addition, Section 4.6 of the Plan refers to Public Open Space (P.O.S) opportunities. Whilst this section does refer to the utilisation of areas of 'good quality bushland' for P.O.S. to retain some of the remnant vegetation, the section also refers to utilising already cleared land for P.O.S opportunities. Given the large amount of high quality remnant vegetation and known utilisation of the bushland within DA26 by threatened fauna such as Carnaby's Black Cockatoo, the Plan should aim to</p>	<p>The City will support the retention of significant vegetation in line with the appropriate recommendations of detailed flora studies.</p>

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		<p>conserve and protect remnant native vegetation as much as possible through the strategic placement of P.O.S in these areas and reservation for conservation purposes. This would allow development to occur on the areas of land that have already been cleared or have low environmental value, reducing the impact of the proposed developments on local flora and fauna. Strategic placing P.O.S to maximise remnant vegetation protection would also align with the values of the State's Strategic Plan for Perth's Greenways (1998) and the City of Cockburn's Greening Plan.</p> <p>The strategic placement of P.O.S and conservation of remnant native vegetation on lots within DA26 could also assist in maintaining vital ecological corridors and linkages to the Harry Waring Marsupial Reserve to bushland areas to the south and east.</p> <p>Recommendations for inclusion into the Draft SSDSP3</p> <p>A. Provision for comprehensive environmental impact assessments of lots containing remnant native vegetation to assess the impacts of the proposed zoning changes on remnant native vegetation representation and local flora / fauna populations;</p> <p>B. Relocation of clearing activities required for residential/commercial development and school zones to those areas that are cleared, have already been cleared or comprise of remnant native vegetation in poor condition that does not hold significant conservation value;</p> <p>C. Conservation of remnant native vegetation in very good to pristine condition within lots 33, 32, 31 and 46;</p> <p>D. Conservation of remnant native vegetation in very good, excellent and pristine condition through the placement of these areas into areas of P.O.S; and</p> <p>E. Conservation of native vegetation in P.O.S through reservation/protection to prevent or minimise clearing.</p> <p>6. <u>Lack of Fauna Assessment & Threatened Fauna Conservation</u></p> <p>The Draft SSDSP3 does not provide any information on the assessment of native fauna within the Plan area, and the potential for DA26 to comprise of locally or regionally significant native fauna habitat. Whilst it is acknowledged that the Harry Waring Marsupial Reserve is located adjacent to DA26, the adjoining remnant vegetation is a vital corridor linking the reserve to other remnant bushland areas to</p>	<p>5A. Supported (in part). The subject land is already zoned Urban under the MRS and Development under TPS No. 3 however as outlined above all LSP's will need to be supported and informed by appropriate vegetation studies.</p> <p>5B. Supported (in part). The City will support the siting of development within cleared or areas of low conservation value in accordance with appropriate vegetation studies at the LSP and subdivision stages. This will need to be balanced against orderly and proper planning for development sites.</p> <p>5C. Supported (in part). The City will support the retention of high quality vegetation on these sites in</p>

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		<p>the east and west through DA26.</p> <p>Specifically, the vegetation within Lot 33 appears to be significant on a local and regional level for the Endangered Carnaby's Black Cockatoo (<i>Calyptorhynchus latirostris</i>). Carnaby's Black Cockatoo gangs of 50+ birds regularly visit the Banksia woodland on Lot 33 to feed and forage on Banksia. In April/May of 2012 a large gang of cockatoos utilised this area on a weekly basis for up to 5 weeks. Photographs from late April 2012 are provided on a disc to provide evidence that the vegetation is utilised by this species. Whilst I have only been able to collect photographic evidence from Lot 33 it is likely that this species also utilises the surrounding lots comprising of Banksia woodland, and they are often heard throughout the bushland areas of Lot 33, 32 and 46.</p> <p>In addition, the vegetation provides suitable habitat for a variety of local birds, reptile and mammal species given its excellent to pristine condition, diverse flora and dense understorey and proximity to nearby regional parks (Harry Waring Marsupial Reserve and Beeliar Regional Park). Numerous birds have been seen and heard within the bushland area of Lot 33/46 as well as snakes, kangaroos and bandicoots (Quenda).</p> <p>On a recent review of vegetation within Lot 46 from the firebreak a number of large Eucalypts were also observed, comprising of hollows that may be suitable for larger bird species such as Carnaby's Black Cockatoo. A large nest resembling that of a bird of prey was also observed within the vegetation of lot 46. Photographs of these nesting areas are provided on the disc of photographs.</p> <p>Given the quality of the vegetation, diversity and range of habitats within DA26, large scale clearing within this area is likely to have a significant impact on local fauna populations, including that of the Endangered Carnaby's Black Cockatoo.</p> <p><u>Recommendations for inclusion into the Draft SSDSP3</u></p> <p>A) Provision for comprehensive fauna assessments to occur on all lots within DA26 containing remnant native vegetation prior to the finalisation of zoning and land use planning to allow for areas of high conservation value to be retained and protected;</p> <p>B) Provision for lots 46 and 33 to be set aside as Regional Open Space (Conservation) given the high quality of vegetation on these sites, their known utilisation by the Endangered Carnaby's Black Cockatoo and other local wildlife species, and presence of mature trees with suitable nesting sites/hollows;</p> <p>C) Provision for a wildlife corridor of remnant native vegetation to be retained</p>	<p>accordance with appropriate vegetation studies at the LSP and subdivision stages. This will need to be balanced against orderly and proper planning for development sites.</p> <p>5D. As above.</p> <p>5E. Supported.</p> <p>6. Noted. All LSP's will be required to be supported and informed by comprehensive fauna studies prior to assessment and/or adoption. Matters relating to vegetation retention, alternative designs, offsets etc. will be determined at the LSP and subdivision stage on the advice of the DEC and other relevant authorities.</p> <p>6A. Supported (in part). The subject land is</p>

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		<p>linking Harry Waring Marsupial Reserve to Frankland Reserve/ remnant vegetation on the southern side of Rowley Road, Mandogalup to the south, and the Western Power Easement to the east.</p> <p>7. <u>Vegetation within Lots 33 Barfield Road and 46 Frankland Avenue</u></p> <p>The following provides a brief summary of the vegetation within the abovementioned lots that demonstrates that the areas proposed for development within DA26 contain significant remnant native vegetation populations suitable of providing habitat for a number of local fauna species. Please note that vegetation has only been assessed from a firebreak, and therefore the vegetation may have higher conservation significance than noted.</p> <p>A. Lot 33 Barfield Road</p> <p>Lot 33 backs on to my own residential property, and comprises of dense <i>Banksia menziesii</i>/<i>Banksia attenuata</i> woodland within the northern portion, morphing into <i>Banksia</i> spp./<i>Allocasuarina</i> sp. woodland towards the southern portion.</p> <p>The vegetation would be considered to be in Excellent to Pristine condition when compared to the Keighery Condition Rating Scale (from Bushforever, Keighery 1994). There are signs of edge effects along the northern and western fire break where the adjoining residential development has occurred, namely some weed invasion and rubbish. However these effects do not extend far into the vegetation. The vegetation on Lot 33 also comprises of a dense and species rich understorey that includes but is not limited to a number of native herbs and grasses, <i>Banksia ilicifolia</i>, <i>Kunzea</i> sp., <i>Stirlingia latifolia</i>, <i>Hibbertia hypericoides</i>, <i>Xanthorrhoea</i> sp., (poss) <i>Patersonia occidentalis</i>, <i>Jacksonia</i> sp., <i>Adenanthos</i> sp. and <i>Calectasia</i> sp. (poss. <i>grandiflora</i>).</p> <p>Given the condition of the vegetation and its floral diversity, it is likely that the vegetation within this lot is of high local faunal significance. A number of birds inhabit the bushland within this lot, including local nectivores (honey-eaters) and insectivores (such as the Willy Wagtail). The vegetation is full of birdlife, as evidenced in the chorus of bird song you can hear during the day. In addition a gang of 50+ Carnaby's Black Cockatoos (<i>Calyptorhynchus latirostris</i>) (Endangered) frequent the vegetation within this property to feed on the <i>Banksia</i> flowers and nuts. They appear to utilise this particular property on a regular basis, coming and going on a regular basis where they may utilise the vegetation for feeding at least once a week for a period of a month or more. Photographs and dates of sightings are currently being compiled for the WA Museum for recording purposes.</p>	<p>already zoned Urban under the MRS and Development under TPS No. 3 however as outlined previously all LSP's will need to be supported and informed by appropriate fauna studies.</p> <p>6B. Not supported. As above.</p> <p>6C. Supported (in part). The City will support the provision of suitable wildlife corridors in accordance with appropriate flora and fauna studies at the LSP and subdivision stages. This will need to be balanced against orderly and proper planning for development sites.</p> <p>7. Noted.</p> <p>7A. Noted. A LSP will be required for the site prior to subdivision/development and will need to be supported and informed by comprehensive flora and fauna studies prior to assessment and/or adoption. Matters relating to vegetation retention, alternative designs, offsets etc. will be determined at the LSP and subdivision stage on the advice of the DEC and other relevant authorities.</p>

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		<p>In addition to its avian inhabitants, the vegetation is also known to provide habitat for local kangaroo populations, reptiles (e.g. snakes) and mammals (bandicoots). These animals are regularly seen in and around the vegetation on Lot 33.</p> <p>The loss of this vegetation for residential development and a school site is likely to have a significant impact on local fauna species that utilise the vegetation for habitat. Whilst there is a nearby conservation area (Harry Waring Marsupial Reserve), the vegetation on Lot 33 is considered to provide significant habitat for fauna in its own right, given its frequent utilisation by Carnaby's Black Cockatoo (Carnaby's). It is likely that Carnaby's have commenced using Lot 33 and its surrounds for foraging following the loss of the nearby Banksia woodland in Wandí for Honeywood residential estate. In addition, the vegetation on this lot forms part of an ecological linkage/habitat corridor to the nearby Harry Waring Marsupial Reserve.</p> <p>The vegetation on this lot is also in Excellent/Pristine condition, and should be considered for conservation by the Council given the already significant loss of remnant native vegetation on the Swan Coastal Plain, and within the Perth Metropolitan Region.</p> <p>B. Lot 46 Frankland Avenue</p> <p>Lot 46 is proposed to comprise of a private primary school. Lot 46 comprises of a predominantly Banksia spp., Eucalyptus sp. and Allocasuarina woodland. The vegetation again comprises of a dense and diverse herb and shrub understorey and is in predominantly Pristine to Excellent condition according to the Keighery Vegetation Condition Rating Scale, with minimal disturbances seen. The vegetation also comprises of a middle storey of Kunzea sp, and Banksia grandis, and is often dense in parts.</p> <p>This lot contains a number of large Eucalyptus trees with hollows that may be suitable for nesting by local bird species, including the Endangered Carnaby's Black Cockatoo (see photo on disc of one such hollow). A large nest, resembling that of a bird of prey, is also located within Lot 46 (see photo on disc). The understorey has a dense leaf litter layer, making it suitable habitat for bandicoots and reptiles. Whilst I have not observed Carnaby's Black Cockatoos within this area specifically (as I do not have direct access), it is likely that they would utilise this vegetation as an extension to the vegetation utilised on Lot 33.</p> <p>The loss of vegetation on Lot 46 for a school site is likely to have a significant impact on local fauna species that utilise the vegetation for habitat or as an</p>	<p>7B. Noted. A LSP will be required for the site prior to subdivision/development and will need to be supported and informed by comprehensive flora and fauna studies prior to assessment and/or adoption. Matters relating to vegetation retention, alternative</p>

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		<p>ecological linkage/habitat corridor to the nearby Harry Waring Marsupial Reserve. The vegetation is also in Excellent/Pristine condition, and should be considered for conservation by the Council for the intrinsic floral value that it holds.</p>	<p>designs, offsets etc. will be determined at the LSP and subdivision stage on the advice of the DEC and other relevant authorities.</p>
18	<p>Dykstra Planning PO Box 316 KELMSCOTT WA 6991</p>	<p>OBJECT – subject to modifications</p> <p>1. This submission has been prepared by Dykstra Planning on behalf of Beauchamp Nominees Pty Ltd and Fobbing Hall Pty Ltd, landowners of Lot 42 Frankland Avenue, Hammond Park. Lot 42 occurs within the Southern Suburbs District Structure Plan (SSDSP) area, which depicts a Conservation Category Wetland (CCW) over a large area of Lot 42 and the eastern part of the adjoining Lot 41. The corresponding entry in the Legend identifies this area as: "Conservation Category Wetland (In accordance with DEC Geomorphic Wetlands Swan Coastal Plain Dataset)".</p> <p>The accompanying District Structure Plan Report, under Section 3.7 – Wetlands states the CCW was subject to a wetland classification review in 2010, as a result of which the DEC determined to retain the CCW classification.</p> <p>In fact, the decision by the DEC to retain the CCW classification is subject to an Appeal submitted on 1 May 2012 and hence the 2010 reclassification request has not yet been finally determined. A copy of correspondence from the DEC confirming receipt of the request to review its decision and the likely timeframe for this to be considered is attached.</p> <p>2. In view of the above, this submission requests both the SSDSP Report and Plan be modified to reflect the actual status of the damp land, to the effect that:</p> <p><i>"The CCW status is subject to further investigation and potential reclassification. A review of land use opportunities over Lots 41 and 42 Frankland Avenue may be considered at Local Structure Planning stage."</i></p>	<p>1. Not supported. Conservation Category Wetland's (CCW) are allocated by the State Government as having the highest degree of conservation value of all wetlands on the Swan Coastal Plain. Protecting the CCW, by appropriate buffers particularly, is required to the highest degree under State Government planning and environmental policy. This forms a fundamental determinant to orderly and proper planning.</p> <p>It should be noted that the Department of Environment and Conservation ("DEC") recently considered a request to modify the classification of the subject wetland and resolved to retain its classification as a CCW. This is reflected in the DEC's letter dated 16 December 2010 which was sent to the proponent representing landowners affected by the CCW.</p> <p>Until such time that the appeal has been determined, the status of the CCW remains unchanged and as such future LSP's will need to demonstrate how the requirements of the DEC and environmental attributes of the CCW are addressed appropriately.</p> <p>2. Not supported. The lodgement of an appeal against the DEC's decision does not warrant a modification to the</p>

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			SSDSP3 as the CCW classification has not changed. Consistent advice to the City from the DEC is that the wetland is a CCW.
19	Roberts Day Level 1, 130 Royal Street EAST PERTH WA 6004	<p>OBJECT (subject to modifications)</p> <p>1. SSDSP3</p> <p>The City of Cockburn has invited submissions and comment on the above document. We represent Wattleup Road Development Company Pty Ltd, the owners of Lots 121, 122 and 801 Wattle up Road, Hammond Park, which are located within the area covered by the draft Southern Suburbs District Structure Plan Stage 3 (SSDSP). We have reviewed the above document and make the following observations and comments:</p> <p>A. <u>Exclusion of Land subject to the Alcoa Residue Storage Area Buffer</u></p> <p>- <i>Background</i> The SSDSP recognises the 1.5km residential and sensitive uses exclusion area required from residue storage areas operated by Alcoa. This was extended from 1km in 2010 by the Western Australian Planning Commission (WAPC), based on advice from the Department of Health and Department of Environment and Conservation. As the SSDSP states, the purpose of extending the buffer was to "recognise uncertainties related to the level and frequency of impacts" from the residue disposal areas. WAPC resolved to review the buffer in five years (i.e. by 2015).</p> <p>Council endorsed a Local Structure Plan over Lots 121, 122 and 801 Wattleup Road Hammond Park in December 2008 (see attached). Prior to the decision by the WAPC, a subdivision application for the subject land was lodged, and refused by the WAPC on the grounds that a portion of the land was within the 1.5km exclusion area. This decision was reviewed at the State Administrative Tribunal (SAT; Wattleup Road Development Company Pty Ltd vs. Western Australian Planning Commission [2011] WASAT 160, decision attached), which resolved that monitoring should be undertaken for a minimum period of 12 months to confirm the subject land is suitable for residential development. In reaching its decision, SAT explicitly stated that the precautionary principle should prevail based on the information available at the time of the decision. All other matters have been addressed to the satisfaction of SAT.</p> <p>The 1.5km buffer is the primary outstanding issue impeding residential development of the subject land, as is substantially reflected in the Points of</p>	<p>1. Noted.</p> <p>1A. Not supported. The following findings of the SAT in the Wattleup Road Development Company Pty Ltd vs. Western Australian Planning Commission [2011] WASAT 160 matter directly relate to the SSDSP3 -</p> <p><i>"3. The Tribunal found that there is a threat of serious or irreversible environmental damage for residents of the proposed subdivision in relation to dust from the residue disposal area and the sand quarry. The Tribunal also found that there is scientific uncertainty as to the environmental damage. The conditions precedent to the satisfaction of the precautionary principle were therefore established. Consequently, a precautionary measure may be taken to avert the anticipated threat of environmental damage, provided that it is proportionate to the threat. The Tribunal determined that precluding subdivision until adequate air quality monitoring at the site demonstrates acceptability is proportionate to the</i></p>

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		<p>Agreement included in the Joint Statement of Planning Experts for the SAT review (attached, refer paragraph 3 of the statement). Furthermore, SAT, in its published decision, stated that due to methodological issues "the buffer should not be reflected in the town planning framework at this time" (refer paragraph 69 of the attached SAT decision), or until monitoring has been completed.</p> <p>Furthermore, through information provided in the SAT Review, it is understood that the residue storage areas are to be closed and rehabilitated in the near future (refer paragraph 60 of the attached SAT decision).</p> <p>- <i>Modification to SSDSP</i></p> <p>Monitoring in accordance with the SAT decision has now commenced. In light of this and the proceedings of the SAT hearing (specifically the acknowledgement that all other outstanding matters between WAPC and the landowner were resolved), any future subdivision application lodged over the subject land should include monitoring as a condition of subdivision approval. The condition would relate to the location of dwellings on lots and (if necessary) reconfiguring lots to allow the appropriate location of dwellings. This would not preclude subdivision but allow for redesign as necessary.</p> <p>Having regard for the resolution achieved in the SAT process, there should be no additional encumbrance to achieving subdivision approvals.</p> <p>In light of this, and given the land is zoned appropriately with an LSP in place, we object to the removal of the subject land from the SSDSP. The structure previously proposed over the land under the SSDSP should be reinstated, including a notation regarding timing for the completion of monitoring. This would provide adequate comfort to the WAPC, Department of Environment and Conservation, and the City.</p> <p>This will allow for the efficient and timely achievement of approvals over the land, and for residential land to be released to the market sooner. The inclusion in the SSDSP at this stage will also enable coordinated development with the balance of the SSDSP area which is a key objective of any structure plan.</p> <p>Furthermore, the likely closure and rehabilitation of residue storage areas in the near future should be noted within section 4.1.3 of the SSDSP with the buffer automatically removed as a consideration in relevant planning decisions accordingly when this occurs.</p> <p>B. <u>State Government Infrastructure</u></p>	<p><i>threat, appropriate and cost-effective. The Tribunal found that adequate air quality monitoring would require prior consultation with the Department of Environment and Conservation, monitoring for a period of at least 12 months and assessment and reporting in relation to relevant health and amenity standards and chemical composition of dust.</i></p> <p><i>4. The Tribunal found that no weight should appropriately be placed on the definition by the Western Australian Planning Commission of an off-site buffer from the residue disposal area affecting the site, because of significant methodological concerns in relation to the scientific assessment utilised for the purpose of establishing the buffer. The Tribunal observed that it would seem sensible for the air quality monitoring and assessment in relation to the site referred to in the previous paragraph to inform the confirmation or variation of the buffer by the Western Australian Planning Commission. The Tribunal also suggested that, in light of the significant methodological concerns raised in the evidence and the inadequate duration of the monitoring that underlay the scientific assessment used by the Western Australian Planning Commission to define the buffer, the buffer should not be reflected in the town planning framework at this stage, and that any amendment of the planning framework in this respect should await the results of the air quality monitoring and assessment at the site."</i></p>

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		<p>The subject land is affected by infrastructure requirements related to the future Latitude 32 Industrial Precinct and Outer Harbour. This is reflected in the establishment of a Planning Control Area (PCA) over the proposed Rowley Road alignment running through the site.</p> <p>These items should be funded by the State Government, in accordance with established planning principles and legal precedent. These items, and subsequent impacts on future development of the SSDSP area, relate to the development of infrastructure of regional and State importance, as recognised by the establishment of the PCA.</p> <p>Accordingly, the SSDSP should reflect this in identifying the State Government as the responsible authority for funding for the construction of Rowley Road and associated infrastructure. Whether the State Government has budget allocation for the works is not a relevant planning consideration in infrastructure funding. Clarification of this matter will remove risk for landowners within the SSDSP area and promote urban development in the area.</p> <p>2. AMENDMENT NO.28</p> <p>We refer to the above proposal, which was initiated by Council on 8 March 2012 and for which submissions have been invited. We represent QUBE Wattleup Development Pty Ltd, the owners of Lots 121, 122 and 801 Wattleup Road, Hammond Park, which are located within the area covered by DCA10 as referred to in the amendment documentation.</p> <p>We have reviewed the above document and make the following observations and comments.</p> <p>A. Within DCA10, one of the contribution items proposed is the "proportional (38.4%) cost of widening and upgrading of Hammond Road between Gaebler Road and Rowley Road, including construction of an additional two lanes for Hammond Road and where the reserve width is less than 40 metres wide, kerbing to the verge side of the carriageway shall be provided".</p> <p>Hammond Road is identified as an Other Regional Road under the Metropolitan Region Scheme (MRS). The Western Australian Planning Commission State Planning Policy 3.6 Developer Contributions for Infrastructure (SPP3.6) stipulates that road infrastructure works required may include:</p> <p><i>"new district distributor roads including earthworks for the whole road reserve, the construction of one carriageway comprised of two lanes and associated drainage</i></p>	<p>Given the uncertainties outlined above, the City has an obligation to landowners and developers within the affected area to ensure that relevant planning framework reflects current decisions and issues and does not unnecessarily create false expectations in relation to development potential of sites.</p> <p>The City therefore designated the area affected by the buffer within the SSDSP3 as 'subject to future structure planning'. The decision not to exclude the area entirely from the SSDSP3 reflects the future desire for residential or compatible development to occur within this area subject to the appropriate modelling and environmental studies being undertaken. This is in line with the advice of the SAT, providing that current modelling which was used to prepare the buffer should be given no weight, and needs to be undertaken again. Once this has been completed and the actual implications known, appropriate structure planning can then take place for this land.</p> <p>1B. Noted. Section 6.7.1 of the SSDSP3 report outlines the responsibility for the construction of Rowley Road as a State Government responsibility given its status as a Primary Regional Road under the MRS and a strategic freight route.</p> <p>2. Noted.</p>

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		<p><i>works"</i></p> <p>The "additional two lanes" proposed for DCA10 are therefore a greater contribution than envisaged in SPP3.6, and should be reworded to confirm that contributions are required for earthworks and for one carriageway with two lanes for this section of Hammond Road.</p> <p>B. In addition, it is noted that the contributions include the cost of acquisition of land reserved for Hammond Road under the MRS. As a road reserved under the MRS, Hammond Road is by definition a road of regional significance. Consequently it is considered inequitable and therefore inconsistent with SPP3.6 to require the cost of acquisition to be met by local landowners. We therefore object to this item being included as a contribution item for DCA10.</p> <p>C. Finally, as stated by the City of Cockburn in its draft district structure plan for the locality (Southern Suburbs District Structure Plan-Stage 3), costs associated with Rowley Road should not be borne by local landowners but by the State Government due to its connection with development of port facilities. This should be confirmed in the Developer Contribution Plans, including any intersection works required for Rowley Road and any other roads.</p>	<p>2A. Supported. The wording of the provision relating to the Hammond Road extension will be modified to reflect the intention that the DCP's will be responsible for the construction of the second carriageway comprised of two lanes, with the City being responsible for the first carriageway.</p> <p>2B. Not supported. Given that residential and associated development within the SSDSP3 area is the primary reason for Hammond Road requiring to be widened/upgraded, the purchase of land required for the extension remains the responsibility landowners/developers. This is consistent with all other DCA's within the City which have been in operation for the past 10 years or more.</p> <p>2C. Supported.</p>
20	Main Roads Western Australia PO Box 6202 EAST PERTH WA 6892	<p>OBJECT</p> <p>1. Thank you for your letter dated 8 June 2012 requesting Main Roads comments on the above proposed Structure Plan and Scheme Amendment No. 28. Main Roads do not support the proposed Structure Plan and provides the following comments:</p>	<p>1. Noted.</p>

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		<p>A. The structure plan proposed 3400 houses. It appears that the transport impact of the proposed structure plan has not been analysed. Transport Impact Assessment needs to be undertaken as per the Western Australian Planning Commission's Transport Assessment Guidelines.</p> <p>B. Rowley Road is expected to carry a significant traffic volume in the future. The subject site is also located next to Kwinana Freeway. Hence, traffic noise is likely to be a significant concern. Noise study needs to be undertaken as per the State Planning Policy (SPP) 5.4. The associated cost for noise mitigation/attenuation shall be contributed by the proponent.</p> <p>C. Access onto Rowley Road needs to be rationalised. Hence, Main Roads recommend that connection "2" be closed when Rowley Road is upgraded. Main Roads supports the proposed underpass (3) to south of the Rowley Road (proposed train station). However, the exact location needs to consider the redevelopment of the land south of Rowley Road.</p>	<p>1A. Noted. The City considers that the future development proposed as part of the DSP is consistent with the density targets set by the WAPC's Directions 2031 and can be integrated within the existing road network with minimal impact. The subject is bounded by existing and future high level regional roads which provide suitable access/egress opportunities.</p> <p>At the LSP stage, developers will be required to submit Transport Impact Assessment' reports which demonstrate compliance with the City's and Main Roads requirements.</p> <p>1B. Supported.</p> <p>1C. Noted. Maintaining connection or otherwise to Rowley Road will be determined at the LSP and/or detailed design stage.</p>
21	Water Corporation PO Box 100 LEEDERVILLE WA 6902	<p>SUPPORT</p> <p>1. Thank you for your letter of 8 June 2012 inviting comments from the Water Corporation on the above DSP and Amendment No.28. The Corporation has no in principle objections to the changes to the DSP or the proposed introduction of developer contribution areas.</p> <p>2. The Corporation's main interest in this area, and at this scale of planning, is to ensure that the structure plan provides sufficient opportunities for infrastructure corridors for major water and wastewater pipes, particularly within existing and proposed roads through the area.</p> <p>3. The urban development of this area will be provided with water from the Thompson's Lake Reservoir via existing and planned major distribution mains. The Corporation's long-term water distribution main planning for this area proposes to place large diameter distribution mains within major roads such as Rowley Road, Hammond Road and Wattleup Road. The Corporation's water planning for this area is currently under review and would benefit from any more detailed information the City could provide regarding the progression of road planning for this section of Rowley Road,</p>	<p>1. Noted.</p> <p>2. Noted.</p> <p>3. Noted. The City can provide the Water Corporation with more detailed information regarding the extension of Hammond Road and realignment of Wattleup Road as developments progress.</p>

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		<p>the timing/staging of the southward extension of Hammond Road, and the timing, width and realignment of Wattleup Road as indicated on the DSP.</p> <p>4. The Corporation is prepared to work closely with the Council's engineers and planners to ensure that major road cross-sections make adequate provision for the alignments of future water and wastewater trunk mains and to adapt water and wastewater trunk main planning with the road layout as it becomes refined through more detailed local structure plans.</p>	<p>4. Noted. As above.</p>
22	<p>Department of Health PO Box 8172 PERTH BC WA 6849</p>	<p>SUPPORT (subject to conditions)</p> <p>Thank you for your letter dated 8 June 2012 requesting comment from the Department of Health (DOH) on the above proposal. The DOH provides the following comment:</p> <p>1. <u>Water and Sewerage</u></p> <p>Indicative developments to the density of R30 to R60 are required to connect to reticulated sewerage by the Government Sewerage Policy - Perth Metropolitan Region.</p> <p>2. <u>Toxicological Evaluation</u></p> <p>A. There should be no residential development or subdivisions within the existing Alcoa air quality and Kwinana EPP buffer areas. Both buffers are actively in place for the life of the respective industries and unless there is an official notification based on scientific work to suggest reduction of the existing buffer distances, all buffer requirements should be implemented.</p> <p>B. The DOH further reiterates that any development adjacent to agricultural or horticultural land use should be outside relevant buffer distances and these buffer areas should be maintained.</p> <p>3. <u>SSDSP3 and Amendment 28 Content</u></p> <p>A. 'Public Health' is recommended to be integrated to the structure plan under the headings and within the various strategies outlined. Public Health should include disability access, public transport access, disaster preparedness, health, social and mental wellbeing of the residents, workforce and visitors. Incorporating 'public health' into the design and implementation of the structure plan will ensure more sustainable developments.</p> <p>B. The new Public Health Bill will require integration of Public Health Planning into existing plans and strategies. This is an opportune time to start incorporating such</p>	<p>1. Noted. The advice provided relates to subdivision/development works and will be implemented as part of a future subdivision and development application/s for the subject land.</p> <p>2A. Noted. Areas affected by the Alcoa Air Quality and Kwinana EPP buffers have been designated as "subject to future structure planning".</p> <p>2B. Noted. As above.</p> <p>3A. Not supported. The SSDSP3 is only a guiding document and matters relating to design and implementation can only be considered at the local structure planning stage. The SSDSP3 is based on Liveable Neighbourhoods (LN) which considers matters of public health and is reflected in planning for future communities within the SSDSP3</p>

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		<p>public health principles and objectives into existing planning schemes and strategies.</p> <p>It is suggested that a clear statement is made to incorporate public health issues under the structure plan and associated strategies.</p> <p>C. This proposal provides the City of Cockburn the opportunity to minimise the land use conflicts and incompatible activities which is the most common issue in land use and development. Health impacts draw many attentions in those issues and hence, should be appropriately and adequately addressed at this level.</p> <p>Enclosed is a scoping tool that should be incorporated into the comprehensive review of the Draft SSDSP3. The scoping tool is available for download at: http://www.public.health.wa.gov.au/cproot/3965/2/Residential%20estates%20and%20precincts%20JUNE%202011.pdf</p> <p>D. Health concerns about buffers are not limited to the industries and infrastructures. Consideration must be given to the need for adequate buffers to protect residents from lifestyle and public health impacts such as mosquitoes, spray drift from agricultural activities or odour from poultry farms for example.</p> <p>You should consider incorporating Health Impact Assessment (HIA) and/or Public Health Assessment (PHA) principles in your decision making process. For your information and guidance, you may access the relevant information at the following sites: HIA- http://www.public.health.wa.gov.au/2/1400/2/health_risk_assessment.pm PHA- http://www.public.health.wa.gov.au/2/1399/2/public_health_assessment.pm</p>	<p>area. Therefore the need for a specific section relating to public health is not required given the application of LN.</p> <p>3B. Not supported. As above.</p> <p>3C. Noted. The SSDSP3 outlines the requirement for appropriate studies and mitigation measures relating to potential land uses conflicts within the subject area.</p> <p>3D. Noted. As above.</p>
23	Li Chi Chan & Chung Mo Chan 408 Wattleup Road HAMMOND PARK WA 6164	<p>SUPPORT (subject to modifications)</p> <ol style="list-style-type: none"> 1. We are writing to you with regards to the above mentioned plan. We support the development within the SSDSP3 precinct. 2. However, we oppose the area identified for the future public primary school on the site of Lot 108, 107 and lot 76. <p>A. Firstly, since the area to the south of Wattleup Rd, Hammond Park have now been removed from development, the public primary school at the corner of Gaebler Rd and future Hammond Rd is sufficient to service the students in the precinct. There is no need for another primary school within such close proximity.</p> <p>B. Secondly, the proposed primary school site was located previously because the previously proposed future train station was located north of Rowley Rd. With the</p>	<p>1. Noted.</p> <p>2. Noted.</p> <p>2A. Not supported. The area to the south of Wattleup Road has been designated as "subject to future structure planning" in recognition of the impacts associated with the Alcoa Air Quality Buffer extension and residential development may occur</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>Mandogalup train station confirmed to a site to the south of Rowley Rd, such site is no longer appropriate.</p> <p>C. Thirdly, the proposed site sites on unlevelled land, with big drops from east to west and north to south. Site work for this school site would cost significantly more than a levelled flat site.</p> <p>D. We suggest removing this school site all together from the SSDSPS3. If the area to the south of Wattleup Rd within the previous SSDSPS3 becomes urban, it may then warrant the need for another public school, which may then be located in the area to the south of Wattleup Rd.</p>	<p>within this area in the future.</p> <p>2B. Not supported. The primary school was located in this area based on potential residential catchment/s and not on the basis of a future train station location.</p> <p>2C. Not supported. The Department of Education ("DoE") have supported the primary school location and matters relating to design will be dealt with at the subdivision/development stage.</p> <p>2D. Not supported. The primary school site has been selected based on its accessibility and ability to integrate with surrounding development. Relocating the school closer to the future Rowley Road strategic freight route and further from residences is not considered to be practical in this case.</p>
24	George Dropulich 32 Sciano Ave SUCCESS WA 6164	<p>SUPPORT (subject to modifications)</p> <p>1. I am writing to you with regards to the above mentioned plan. I support the development within the SSDSPS3 precinct.</p> <p>2. However, I as owner of Lot 107 Wattleup Rd oppose the area identified for the future public primary school on the site of Lot 108, 107 and lot 76.</p> <p>A. Firstly, since the area to the south of Wattleup Rd, Hammond Park has now been removed from the proposed development, the public primary school at the corner of Gaebler Rd and future Hammond Rd is sufficient to service the students in the precinct. There is no need for another primary school within such close proximity. Furthermore, there is doubt about any development proceeding to the south of Wattleup road given Alcoa's long term operations and interests being backed by a 1962 Act of Parliament. A school site in close proximity to a buffer zone has a potential political and monetary risk for the Cockburn ratepayers and the State Government, and nil risk to Alcoa due the 1962 act.</p> <p>B. Secondly, the proposed primary school site was located previously because the</p>	<p>1. Noted.</p> <p>2. Noted.</p> <p>2A. Not supported. The area to the south of Wattleup Road has been designated as "subject to future structure planning" in recognition of the impacts associated with the Alcoa Air Quality Buffer extension and residential development may occur within this area in the future.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>previously proposed future train station was located north of Rowley Rd. Now with the Mandogalup train station confirmed to a site to the south of Rowley Rd, such site is no longer appropriate. As a consequence of this, the population densities have altered with more concentration now focused along a 500 metres band along Kwinana Freeway.</p> <p>C. Thirdly, the proposed site is on unlevelled land, with big drops from east to west and north to south. Site work for this school site would cost significantly more than a levelled flat site.</p> <p>D. I suggest removing this school site all together from the SSDSPS3. If area to the south of Wattleup Rd within the previous SSDSPS3 becomes urban, it may then warrant the need for another public school, which may then be located in the area to the south of Wattleup Rd.</p>	<p>2B. Not supported. The primary school was located in this area based on potential residential catchment/s and not on the basis of a future train station location.</p> <p>2C. Not supported. The Department of Education ("DoE") have supported the primary school location and matters relating to design will be dealt with at the subdivision/development stage.</p> <p>2D. Not supported. The primary school site has been selected based on its accessibility and ability to integrate with surrounding development. Relocating the school closer to the future Rowley Road strategic freight route and further from residences is not considered to be practical in this case.</p>
25	The Lin Family Sail Holdings Pty Ltd Lot 109 Wattleup Rd, HAMMOND PARK WA 6164	<p>SUPPORT (subject to modifications)</p> <p>1. We support the development within the SSDSPS3 precinct.</p> <p>2. However, we oppose the area identified for the future public primary school on the site of Lot 108, 107 and lot 76.</p> <p>A. Firstly, since the area to the south of Wattleup Rd, Hammond Park have now been removed from development, the public primary school at the corner of Gaebler Rd and future Hammond Rd is sufficient to service the students in the precinct. There is no need for another primary school within such close proximity.</p> <p>B. Secondly, the proposed primary school site was located previously because the previously proposed future train station was located north of Rowley Rd. With the Mandogalup train station confirmed to a site to the south of Rowley Rd, such site is no longer appropriate.</p> <p>C. Thirdly, the proposed site sits on unlevelled land, with big drops from east to west and north to south. Site work for this school site would cost significantly more than</p>	<p>1. Noted.</p> <p>2. Noted.</p> <p>2A. Not supported. The area to the south of Wattleup Road has been designated as "subject to future structure planning" in recognition of the impacts associated with the Alcoa Air Quality Buffer extension and residential development may occur within this area in the future.</p> <p>2B. Not supported. The primary school was located in this area based on potential residential catchment/s and</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>a levelled flat site.</p> <p>D. We suggest removing this school site all together from the SSDSPS3. If area to the south of Wattleup Rd within the previous SSDSPS3 becomes urban, it may then warrant the need for another public school, which may then be located in the area to the south of Wattleup Rd.</p>	<p>not on the basis of a future train station location.</p> <p>2C. Not supported. The Department of Education ("DoE") have supported the primary school location and matters relating to design will be dealt with at the subdivision/development stage.</p> <p>2D. Not supported. The primary school site has been selected based on its accessibility and ability to integrate with surrounding development. Relocating the school closer to the future Rowley Road strategic freight route and further from residences is not considered to be practical in this case.</p>
26	Department of Education 151 Royal Street EAST PERTH WA 6004	<p>SUPPORT</p> <p>1. The Department of Education has reviewed the document and advises that there is a requirement for the identified primary and high schools. The Department is satisfied with the location of the sites in relation to their local catchments.</p> <p>2. The Department would require further information regarding the future Rowley Road freight access route via a noise impact assessment so that it may address any effect on the proposed schools.</p> <p>3. Other than the above the Department has no objection to the Structure Plan or the Proposed Scheme Amendment.</p>	<p>1. Noted.</p> <p>2. Noted. The Rowley Road strategic freight route is the responsibility of Main Roads WA and matters relating to noise and design will be assessed and implemented as the design and construction of the road progresses.</p> <p>3. Noted.</p>

CITY OF COCKBURN

MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF065152	23351	COCKBURN GP SUPER CLINIC PTY LTD OPERATING FUNDS	4/07/2012	50,000.00
EF065153	11865	VALMA LUCY OLIVER MONTHLY COUNCILLOR ALLOWANCE	6/07/2012	583.33
EF065154	11867	KEVIN JOHN ALLEN MONTHLY COUNCILLOR ALLOWANCE	6/07/2012	1,833.33
EF065155	12740	MAYOR LOGAN HOWLETT MONTHLY COUNCILLOR ALLOWANCE	6/07/2012	6,166.67
EF065156	15883	TONY ROMANO - COUNCILLOR MONTHLY COUNCILLOR ALLOWANCE	6/07/2012	583.33
EF065157	19059	CAROL REEVE-FOWKES MONTHLY COUNCILLOR ALLOWANCE	6/07/2012	583.33
EF065158	20634	LEE-ANNE SMITH MONTHLY COUNCILLOR ALLOWANCE	6/07/2012	583.33
EF065159	21185	BART HOUWEN MONTHLY COUNCILLOR ALLOWANCE	6/07/2012	583.33
EF065160	23338	STEVE PORTELLI MONTHLY COUNCILLOR ALLOWANCE	6/07/2012	583.33
EF065161	23339	STEPHEN PRATT MONTHLY COUNCILLOR ALLOWANCE	6/07/2012	583.33
EF065162	23340	SHAHYAZ MUBARAKAI MONTHLY COUNCILLOR ALLOWANCE	6/07/2012	583.33
EF065163	10154	AUST TAXATION DEPT PAYROLL DEDUCTIONS	9/07/2012	186,216.00
EF065164	18553	SELECTUS PTY LTD PAYROLL DEDUCTIONS	9/07/2012	6,132.07
EF065165	10176	BELIAR PRIMARY SCHOOL COMMUNITY/SUSTAINABILITY GRANT	10/07/2012	905.00
EF065166	10249	BURDIYA ABORIGINAL CORPORATION CO-HEALTH INNOVATION / COMMUNITY GRANT	10/07/2012	19,407.20
EF065167	10409	COOLBELLUP SPORTING ASSOC INC SECURITY GROUND FEES REIMBURSEMENTS	10/07/2012	1,872.19
EF065168	10953	MELVILLE-COCKBURN CHAMBER OF COMMERCE SPONSORSHIP	10/07/2012	2,000.00
EF065169	11030	NEWTON PRIMARY SCHOOL SUSTAINABILITY GRANT	10/07/2012	3,130.00
EF065170	11399	SOUTH COOGEE VOLUNTEER BUSHFIRE BRIGADE EXPENSE REIMBURSEMENTS	10/07/2012	350.00
EF065171	11406	SOUTH LAKE OTTEY FAMILY & NEIGHBOURHOOD CENTRE CO-HEALTH INNOVATION / SUSTAINABILITY	10/07/2012	3,261.50
EF065172	11408	SOUTH LAKE PRIMARY SCHOOL COMMUNITY/SUSTAINABILITY GRANT	10/07/2012	1,650.00
EF065173	11447	SPEARWOOD DALMATINAC CLUB INC COMMUNITY GRANT	10/07/2012	9,484.57
EF065174	11456	SPEARWOOD PRIMARY SCHOOL SCHOOL GRADUATION AWARDS 2011	10/07/2012	715.00
EF065175	11458	SPEARWOOD UNITING CHURCH CULTURAL GRANT	10/07/2012	3,850.00
EF065176	11847	YANGEBUP PRIMARY SCHOOL SCHOOL GRADUATION AWARDS 2011	10/07/2012	870.00
EF065177	11867	KEVIN JOHN ALLEN MONTHLY COUNCILLOR ALLOWANCE	10/07/2012	235.61
EF065178	12326	TELETHON TRUST DONATION	10/07/2012	3,335.00
EF065179	12637	CAITLIN BRIDGLAND SPONSORSHIP GRANT	10/07/2012	2,000.00
EF065180	12656	COOGEE BEACH SURF LIFESAVING CLUB INC POOR GROVE SLSC DEVELOPMENT COSTS	10/07/2012	16,850.83

EF065181	12890	SUCCESS RESIDENTS ASSOCIATION COMMUNITY GRANT	10/07/2012	2,379.00
EF065182	15239	KAFATAHA ASSOCIATION OF WA CO-HEALTH INNOVATION FUND	10/07/2012	2,500.00
EF065183	15653	COOGEE BEACH PROGRESS ASSOCIATION NEWSLETTER PRINTING CONTRIBUTION	10/07/2012	100.00
EF065184	15883	TONY ROMANO - COUNCILLOR MONTHLY COUNCILLOR ALLOWANCE	10/07/2012	750.00
EF065185	16608	HARVEST LAKES RESIDENTS ASSOCIATION SUSTAINABLE EVENTS GRANT	10/07/2012	2,000.00
EF065186	18684	ROAN BARRETT CPA STUDY FEES CONTRIBUTION	10/07/2012	407.50
EF065187	19059	CAROL REEVE-FOWKES MONTHLY COUNCILLOR ALLOWANCE	10/07/2012	513.25
EF065188	19323	DEPARTMENT FOR CHILD PROTECTION REFUND OF OVERPAYMENT OF GRANT FUNDS	10/07/2012	26,796.14
EF065189	19588	TIRA LA HOGUE SALARY PACKAGED LAPTOP REIMBURSEMENT	10/07/2012	1,690.16
EF065190	19661	SOLID WOMEN INC CO-HEALTH INNOVATION FUND	10/07/2012	2,750.00
EF065191	19691	SIAN HALSALL EXPENSES REIMBURSEMENT	10/07/2012	340.00
EF065192	21722	CORNERSTONE APOSTOLIC BIBLE MINISTRIES INC. COMMUNITY GRANTS	10/07/2012	5,000.00
EF065193	21723	COOGEE PRIMARY SCHOOL P&C ASSOCIATION INC. CULTURAL GRANT	10/07/2012	3,000.00
EF065194	22029	AUBIN GROVE RESIDENTS ASSOCIATION INC SUSTAINABLE EVENTS GRANT	10/07/2012	1,500.00
EF065195	22625	KEVIN SIM SALARY PACKAGE REIMBURSEMENT	10/07/2012	650.00
EF065196	22689	SPEARWOOD ALTERNATIVE SCHOOL SCHOOL GRADUATION AWARDS 2011	10/07/2012	680.00
EF065197	23337	FREMANTLE GP NETWORK LIMITED CO-HEALTH INNOVATION FUND	10/07/2012	5,500.00
EF065198	23963	MELODY WAEREA UNIFORM EXPENSES REIMBURSEMENT	10/07/2012	738.65
EF065199	24042	BRINGING THEM HOME COMMITTEE (WA) INC ENTERTAINMENT SERVICES	10/07/2012	1,100.00
EF065200	24043	WILLIAM MARCHBANK EXPENSES REIMBURSEMENT	10/07/2012	431.46
EF065201	24044	SARAH HYDE SALARY PACKAGED LAPTOP REIMBURSEMENT	10/07/2012	473.52
EF065202	24045	PETER TRAINE STUDY EXPENSES REIMBURSEMENT	10/07/2012	110.30
EF065203	24046	STEVE ELLIOTT SALARY PACKAGED LAPTOP REIMBURSEMENT	10/07/2012	996.00
EF065204	24047	FLORA YING JIA SALARY PACKAGED LAPTOP REIMBURSEMENT	10/07/2012	1,199.00
EF065205	24048	FREMANTLE WORKERS SOCIAL & LEISURE CLUB CO-HEALTH INNOVATION FUND	10/07/2012	2,640.00
EF065206	24049	BOORARKA INDIGENOUS CORPORATION CO-HEALTH INNOVATION FUND	10/07/2012	5,000.00
EF065207	24050	LADO CROATIAN FOLLORIC ENSEMBLE OF WA INC CULTURAL GRANT	10/07/2012	2,500.00
EF065208	24052	NIKKI TARLING YOUTH ART SCHOLARSHIP	10/07/2012	500.00
EF065209	24053	SOUTH SIDE SYMPHONY ORCHESTRA INC CULTURAL GRANT	10/07/2012	2,000.00
EF065210	24054	CHARLES JONES SUSTAINABILITY GRANT	10/07/2012	3,632.80
EF065211	10118	AUSTRALIA POST POSTAGE CHARGES	13/07/2012	20,735.71
EF065212	10154	AUST TAXATION DEPT PAYROLL DEDUCTIONS	13/07/2012	52,334.00

EF065213	10365	COC VOLUNTARY SES EXPENSE REIMBURSEMENTS	13/07/2012	12,371.24
EF065214	10590	FIRE & EMERGENCY SERVICES AUTH OF WA COST SHARING - COMMUNITY FIRE MANAGER	13/07/2012	4,214.38
EF065215	10788	JANDAKOT VOLUNTEER BUSH FIRE BRIGADE EXPENSE REIMBURSEMENTS	13/07/2012	1,996.11
EF065216	10944	MCLEODS LEGAL SERVICES	13/07/2012	14,345.79
EF065217	10997	WILSON PARKING AUSTRALIA SECURITY SERVICES	13/07/2012	23,647.31
EF065218	11030	NEWTON PRIMARY SCHOOL SUSTAINABILITY GRANT	13/07/2012	980.00
EF065219	11399	SOUTH COOGEE VOLUNTEER BUSHFIRE BRIGADE EXPENSE REIMBURSEMENTS	13/07/2012	450.00
EF065220	12025	TELSTRA CORPORATION COMMUNICATIONS SERVICES	13/07/2012	8,520.40
EF065221	14130	BLUE GUM MONTESSORI SCHOOL SAND & TREES REIMBURSEMENT	13/07/2012	478.16
EF065222	15363	JONES LANG LASALLE (WA) PTY LTD SHOP RENT - GATEWAY SHOPPING CENTRE	13/07/2012	21,714.83
EF065223	20701	SARAH GARRETT SALARY PACKAGED LAPTOP REIMBURSEMENT	13/07/2012	1,510.99
EF065224	21362	DANNY SANTOSO CPA STUDY FEES CONTRIBUTION	13/07/2012	407.50
EF065225	21403	ROBERTA BUNCE COMMUNITY CARE VOLUNTEER REIMBURSEMENTS	13/07/2012	47.40
EF065226	22349	FREMANTLE TRAILERS TRAILERS - BOAT AND BOX	13/07/2012	5,745.07
EF065227	23250	DEPARTMENT OF PLANNING DAP APPLICATIONS & DAP FEES	13/07/2012	150.00
EF065228	24060	NOELENE BARRETT COUNCIL APPROVED EX-GRATIA PAYMENT	13/07/2012	2,206.00
EF065229	24061	MICHAEL PARKINSON SALARY PACKAGED LAPTOP REIMBURSEMENT	13/07/2012	1,211.40
EF065230	10152	AUST SERVICES UNION PAYROLL DEDUCTIONS	13/07/2012	3,020.40
EF065231	10305	CHILD SUPPORT AGENCY PAYROLL DEDUCTIONS	13/07/2012	4,300.36
EF065232	10733	HOSPITAL BENEFIT FUND PAYROLL DEDUCTIONS	13/07/2012	2,396.20
EF065233	11001	MUNICIPAL EMPLOYEES UNION PAYROLL DEDUCTIONS	13/07/2012	853.60
EF065234	11856	WA LOCAL GOVERNMENT SUPER PLAN PAYROLL DEDUCTIONS	13/07/2012	302,310.60
EF065235	11857	CHAMPAGNE SOCIAL CLUB PAYROLL DEDUCTIONS	13/07/2012	1,164.00
EF065236	11859	STAFF SOCIAL CLUB PAYROLL DEDUCTIONS	13/07/2012	52.80
EF065237	11860	45S CLUB PAYROLL DEDUCTIONS	13/07/2012	54.00
EF065238	18005	COLONIAL FIRST STATE PAYROLL DEDUCTIONS	13/07/2012	343.74
EF065239	18247	ELLIOTT SUPERANNUATION FUND PAYROLL DEDUCTIONS	13/07/2012	261.01
EF065240	18432	HESTA SUPER FUND PAYROLL DEDUCTIONS	13/07/2012	2,224.62
EF065241	18718	HEALTH SUPER FUND PAYROLL DEDUCTIONS	13/07/2012	923.88
EF065242	18719	COLONIAL FIRST STATE - DAVID GIBSON PAYROLL DEDUCTIONS	13/07/2012	183.89
EF065243	18795	SUPERWRAP PAYROLL DEDUCTIONS	13/07/2012	273.49
EF065244	19010	SUMMIT PERSONAL SUPER PLAN PAYROLL DEDUCTIONS	13/07/2012	298.17

EF065245	19193	REST SUPERANNUATION PAYROLL DEDUCTIONS	13/07/2012	35.28
EF065246	19343	WATSON SUPERANNUATION FUND PAYROLL DEDUCTIONS	13/07/2012	410.30
EF065247	19726	HEALTH INSURANCE FUND OF WA PAYROLL DEDUCTIONS	13/07/2012	3,115.44
EF065248	19727	MTAA SUPER FUND PAYROLL DEDUCTIONS	13/07/2012	167.74
EF065249	19997	AUSTRALIANSUPER PAYROLL DEDUCTIONS	13/07/2012	9,607.19
EF065250	20019	FIDUCIAN SUPERANNUATION SERVICE PAYROLL DEDUCTIONS	13/07/2012	726.21
EF065251	20056	CBUS PAYROLL DEDUCTIONS	13/07/2012	451.92
EF065252	20217	DOWNING SUPERANNUATION FUND PAYROLL DEDUCTIONS	13/07/2012	2,835.64
EF065253	20300	CATHOLIC SUPER & RETIREMENT FUND PAYROLL DEDUCTIONS	13/07/2012	584.79
EF065254	20337	THE LLOYDS SUPERANNUATION FUND PAYROLL DEDUCTIONS	13/07/2012	2,067.49
EF065255	20755	COLONIAL FIRST STATE - ROBERT GRAEME WATSON PAYROLL DEDUCTIONS	13/07/2012	407.02
EF065256	20978	MARITIME SUPER - SEAFARERS DIVISION PAYROLL DEDUCTIONS	13/07/2012	428.71
EF065257	21365	ING LIFE - ONEANSWER PERSONAL SUPER PAYROLL DEDUCTIONS	13/07/2012	92.68
EF065258	21526	TASPLAN SUPER PAYROLL DEDUCTIONS	13/07/2012	54.52
EF065259	21921	MAURICIO FAMILY SELF MANAGED SUPER FUND PAYROLL DEDUCTIONS	13/07/2012	1,664.16
EF065260	21996	ANZ ONEANSWER PERSONAL SUPER PAYROLL DEDUCTIONS	13/07/2012	346.07
EF065261	22067	STEPHENS SUPERANNUATION FUND PAYROLL DEDUCTIONS	13/07/2012	656.53
EF065262	22857	ANZ ONEANSWER PERSONAL SUPER PAYROLL DEDUCTIONS	13/07/2012	455.09
EF065263	22901	FONTANA SUPER PLAN PAYROLL DEDUCTIONS	13/07/2012	1,127.73
EF065264	23552	AGEST SUPER PAYROLL DEDUCTIONS	13/07/2012	185.89
EF065265	23695	NETWEALTH INVESTMENT & SUPERANNUATION PAYROLL DEDUCTIONS	13/07/2012	979.80
EF065266	23856	W & E MARCHBANK SUPERANNUATION FUND PAYROLL DEDUCTIONS	13/07/2012	629.08
EF065267	14182	MICHAELA JONES JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065268	21052	EMILY HADWIGER JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065269	22761	JAKE DOUBIKIN JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065270	22765	JARROD PICKETT JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065271	22770	MADELINE WILKINS JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065272	22771	MATTHEW GREAVES JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065273	24066	MADISON BARNES JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065274	24067	JARRAD BLACK JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065275	24068	ALYCE CORREIA JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065276	24069	ABBEY COWAN JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00

EF065277	24070	ASHLEIGH GRANT JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065278	24071	JOHN HOOKHAM JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065279	24072	JACOB JOHNSON JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065280	24073	KEVIN KALEMBER JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065281	24074	EMILY KELLETT-LEWIS JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065282	24075	MATTHEW LUFF JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065283	24076	RYAN MILBY JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065284	24077	TANISHA MISTIADIS JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065285	24078	KRISTIAN SANTICH JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065286	24079	JOHN GRAHAM TAMARIKI JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065287	24080	MICHAEL TUI JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065288	24081	PHOEBE WILCOX JUNIOR TRAVEL ASSISTANCE	20/07/2012	400.00
EF065289	10154	AUST TAXATION DEPT PAYROLL DEDUCTIONS	23/07/2012	198,484.00
EF065290	10177	BEELIAR RESIDENTS ADVANCEMENT GROUP COMMUNITY GRANT	23/07/2012	600.00
EF065291	10239	BUDGET RENT A CAR - PERTH MOTOR VEHICLE HIRE	23/07/2012	856.36
EF065292	10365	COC VOLUNTARY SES EXPENSE REIMBURSEMENTS	23/07/2012	6,359.46
EF065293	11294	SAFEMAN (WA) PTY LTD PROTECTIVE CLOTHING/EQUIPMENT	23/07/2012	793.22
EF065294	11789	WALGA ADVERTISING/TRAINING SERVICES	23/07/2012	15,347.78
EF065295	11867	KEVIN JOHN ALLEN MONTHLY COUNCILLOR ALLOWANCE	23/07/2012	53.28
EF065296	12656	COOGEE BEACH SURF LIFESAVING CLUB INC POOR GROVE SLSC DEVELOPMENT COSTS	23/07/2012	9,047.66
EF065297	12740	MAYOR LOGAN HOWLETT MONTHLY COUNCILLOR ALLOWANCE	23/07/2012	2,100.83
EF065298	13820	PA CAMERON EXPENSES REIMBURSEMENT	23/07/2012	815.00
EF065299	14562	KLAUS FAHRNER LANDOWNER BIODIVERSITY GRANT	23/07/2012	1,351.90
EF065300	15286	GUNTHER & HELKE FAHRNER LANDOWNER BIODIVERSITY GRANT	23/07/2012	1,210.00
EF065301	16017	KATHLEEN LEVETT LANDOWNER BIODIVERSITY GRANT	23/07/2012	358.00
EF065302	16018	GREG & CORRINE HALL LANDOWNER BIODIVERSITY GRANT	23/07/2012	1,507.00
EF065303	17040	DEPARTMENT OF PREMIER AND CABINET PUBLICATIONS	23/07/2012	322.40
EF065304	17323	GRAEME & JEAN DOIG LANDOWNER BIODIVERSITY GRANT	23/07/2012	807.00
EF065305	17325	KATINKA RUTHROF LANDOWNER BIODIVERSITY GRANT	23/07/2012	1,210.00
EF065306	18038	COCKBURN NETBALL CLUB SPORTS GRANT	23/07/2012	1,000.00
EF065307	18536	ALFRED & MERILYN ROSENAUER LANDOWNER BIODIVERSITY GRANT	23/07/2012	2,050.00
EF065308	18553	SELECTUS PTY LTD PAYROLL DEDUCTIONS	23/07/2012	6,418.67

EF065309	20634	LEE-ANNE SMITH MONTHLY COUNCILLOR ALLOWANCE	23/07/2012	1,969.19
EF065310	20695	JENNIFER & STEPHEN WALTON LANDOWNER BIODIVERSITY GRANT	23/07/2012	1,500.00
EF065311	21301	SIMON MORGAN REIMBURSEMENT - TAFE FEES	23/07/2012	289.80
EF065312	21389	YVONNE NICHOLLS LANDOWNER BIODIVERSITY GRANT	23/07/2012	1,760.00
EF065313	21390	HAZEL PARSONS LANDOWNER BIODIVERSITY GRANT	23/07/2012	2,000.00
EF065314	21591	DAVID & FRANCIS MARTIN LANDOWNER BIODIVERSITY GRANT	23/07/2012	2,000.00
EF065315	21592	VIV & LEANNE SLOSS LANDOWNER BIODIVERSITY GRANT	23/07/2012	1,391.50
EF065316	23302	BUILDING SERVICES BOARD BUILDING SERVICES LEVIES	23/07/2012	759.00
EF065317	23338	STEVE PORTELLI MONTHLY COUNCILLOR ALLOWANCE	23/07/2012	448.91
EF065318	23527	SAM SEYMOUR-EYLES STUDY FEES REIMBURSEMENT	23/07/2012	2,000.00
EF065319	24051	SOO KEE & LINDA CHAN LANDOWNER BIODIVERSITY GRANT	23/07/2012	1,849.00
EF065320	24082	PS BEZUIDENHOUT LANDOWNER BIODIVERSITY GRANT	23/07/2012	2,673.00
EF065321	24083	WENG KONG SUM & ANNIE GAY MUSIC LESSONS REFUND	23/07/2012	30.00
EF065322	24085	STIRLING GLASS & ALUMINIUM INSURANCE CLAIM REIMBURSEMENT	23/07/2012	200.00
EF065323	24086	KATHLEEN KOELEN SALARY PACKAGED LAPTOP REIMBURSEMENT	23/07/2012	1,124.00
EF065324	24087	ADAM HARRIS SALARY PACKAGED LAPTOP REIMBURSEMENT	23/07/2012	600.00
EF065325	11753	WASTE MANAGEMENT & RECYCLING FUND QUARTERLY LANDFILL LEVY PAYMENT	26/07/2012	1,224,822.96
EF065326	10118	AUSTRALIA POST POSTAGE CHARGES	27/07/2012	15,839.98
EF065327	10154	AUST TAXATION DEPT PAYROLL DEDUCTIONS	27/07/2012	52,580.00
EF065328	10335	CLASSIC HIRE EQUIPMENT HIRING SERVICES	27/07/2012	11,576.40
EF065329	10888	LJ CATERERS CATERING SERVICES	27/07/2012	4,757.50
EF065330	16409	GUNTHER SCHLOMER SALARY PACKAGED LAPTOP REIMBURSEMENT	27/07/2012	2,089.90
EF065331	19396	ACP EVENTS PARTY EQUIPMENT HIRE	27/07/2012	510.00
EF065332	24084	COSIMA FEIST OUTRAGE HOLIDAY PROGRAM REFUND	27/07/2012	44.00
EF065333	24125	BASSAM SAHAR UNI FEES CONTRIBUTION	27/07/2012	895.75
EF065334	10015	ABSOLUTE ASPHALT PTY LTD ASPHALTING SERVICES/SUPPLIES	31/07/2012	2,431.00
EF065335	10032	ADVANCED TRAFFIC MANAGEMENT (WA) PTY LTD CONTROLLERS AND SIGNS	31/07/2012	3,715.53
EF065336	10043	AKN LOCK SERVICE SECURITY SERVICES	31/07/2012	138.60
EF065337	10051	ALL LINES LINE MARKING SERVICES	31/07/2012	770.00
EF065338	10058	ALSCO PTY LTD HYGIENE SERVICES/SUPPLIES	31/07/2012	369.64
EF065339	10071	AUSTRALASIAN PERFORMING RIGHT ASSOC. LTD LICENCE - PERFORMING RIGHTS	31/07/2012	1,611.90
EF065340	10086	ARTEIL WA PTY LTD ERGONOMIC CHAIRS	31/07/2012	1,169.30

EF065341	10091	ASLAB PTY LTD ASPHALTING SERVICES/SUPPLIES	31/07/2012	2,029.69
EF065342	10190	BETTA TURF TURFING SERVICES	31/07/2012	95.00
EF065343	10201	BIG W DISCOUNT STORES VARIOUS SUPPLIES	31/07/2012	198.42
EF065344	10207	BOC GASES GAS SUPPLIES	31/07/2012	1,490.25
EF065345	10212	BOSS BOLLARDS SECURITY PRODUCTS	31/07/2012	1,045.00
EF065346	10226	BRIDGESTONE AUSTRALIA LTD TYRE SERVICES	31/07/2012	11,553.68
EF065347	10246	BUNNINGS BUILDING SUPPLIES PTY LTD HARDWARE SUPPLIES	31/07/2012	876.89
EF065348	10247	BUNZL AUSTRALIA LTD PAPER/PLASTIC/CLEANING SUPPLIES	31/07/2012	95.54
EF065349	10283	CENTRAL INSTITUTE OF TECHNOLOGY TRAINING SERVICES	31/07/2012	22,000.00
EF065350	10296	CHALLENGER TAFE - FREMANTLE TRAINING SERVICES - BUSINESS	31/07/2012	606.00
EF065351	10328	CITY OF MELVILLE SECURITY SERVICES	31/07/2012	15.40
EF065352	10333	CJD EQUIPMENT PTY LTD HARDWARE SUPPLIES	31/07/2012	246.91
EF065353	10338	TRANSPACIFIC CLEANAWAY WASTE DISPOSAL SERVICES	31/07/2012	86,395.41
EF065354	10346	COATES HIRE OPERATIONS PTY LTD EQUIPMENT HIRING SERVICES	31/07/2012	3,265.15
EF065355	10348	COCA COLA AMATIL SOFT DRINK SUPPLIES	31/07/2012	1,377.00
EF065356	10359	COCKBURN PAINTING SERVICE PAINTING SUPPLIES/SERVICES	31/07/2012	1,886.50
EF065357	10360	COCKBURN PARTY HIRE HIRE OF PARTY EQUIPMENT	31/07/2012	313.65
EF065358	10375	VEOLIA ENVIRONMENTAL SERVICES WASTE SERVICES	31/07/2012	4,339.17
EF065359	10380	COLQUHOUNS FREMANTLE BAG COMPANY WOVEN BAGS	31/07/2012	412.50
EF065360	10384	COMMUNICATIONS AUSTRALIA PTY LTD COMMUNICATION SERVICES	31/07/2012	40,568.87
EF065361	10386	COMMUNITY NEWSPAPER GROUP ADVERTISING SERVICES	31/07/2012	28,157.92
EF065362	10394	CD'S CONFECTIONERY WHOLESALERS CONFECTIONERY	31/07/2012	687.35
EF065363	10431	CRYSTAL CLEAN COMPUTER SERVICES COMPUTER CLEANING SERVICES	31/07/2012	4,342.91
EF065364	10443	CUSTOM PICTURE FRAMERS FRAMING SERVICES	31/07/2012	280.00
EF065365	10483	LANDGATE MAPPING/LAND TITLE SEARCHES	31/07/2012	8,995.05
EF065366	10522	DYMOCKS HAY ST BOOKS	31/07/2012	8,484.21
EF065367	10526	E & MJ ROSHER PTY LTD MOWER PARTS	31/07/2012	6,701.25
EF065368	10535	ECOSYSTEM MANAGEMENT SERVICES PLANTS	31/07/2012	1,128.60
EF065369	10537	EDUCATIONAL ART SUPPLIES CO ART/CRAFT SUPPLIES	31/07/2012	499.97
EF065370	10557	ENVAR SERVICE PTY LTD PREVENTATIVE MAINTENANCE SERVICES	31/07/2012	7,899.70
EF065371	10573	FAIRBRIDGE WESTERN AUSTRALIA INC OUTDOOR RECREATION SERVICES	31/07/2012	1,320.00
EF065372	10574	FOODLINK FOOD SERVICE GROCERIES/CONSUMABLES	31/07/2012	494.63

EF065373	10580	FC COURIERS COURIER SERVICES	31/07/2012	937.35
EF065374	10590	FIRE & EMERGENCY SERVICES AUTH OF WA COST SHARING - COMMUNITY FIRE MANAGER	31/07/2012	18,945.76
EF065375	10597	FLEXI STAFF PTY LTD EMPLOYMENT SERVICES	31/07/2012	73,432.87
EF065376	10601	ISS HYGIENE SERVICES HYGIENE SERVICES	31/07/2012	15,680.50
EF065377	10609	FORESTVALE TREES P/L PLANTS - TREES/SHRUBS	31/07/2012	32,928.50
EF065378	10636	FUJI XEROX AUSTRALIA PTY LTD PHOTOCOPY CHARGES	31/07/2012	45,122.39
EF065379	10641	GALVINS PLUMBING PLUS PLUMBING SERVICES	31/07/2012	2,160.59
EF065380	10655	GHD PTY LTD CONSULTANCY SERVICES	31/07/2012	3,300.00
EF065381	10679	GRASSTREES AUSTRALIA PLANTS & PLANTING SERVICES	31/07/2012	18,018.00
EF065382	10683	GRONBEK SECURITY LOCKSMITH SERVICES	31/07/2012	5,427.53
EF065383	10688	GYMCARE SPORTING EQUIPMENT MAINTENANCE	31/07/2012	342.65
EF065384	10697	HARDWARE DISTRIBUTORS WA HARDWARE SUPPLIES	31/07/2012	2,035.22
EF065385	10708	HEAVY AUTOMATICS PTY LTD EQUIPMENT MAINTENANCE SERVICES	31/07/2012	1,513.60
EF065386	10709	HECS FIRE FIRE SYSTEM MAINTENANCE	31/07/2012	12,523.50
EF065387	10726	HOLTON CONNOR ARCHITECTS & PLANNERS ARCHITECTURAL SERVICES	31/07/2012	3,300.00
EF065388	10737	RAIN SCAPE WATERWISE SOLUTIONS RETICULATION/IRRIGATION SUPPLIES	31/07/2012	6,189.42
EF065389	10743	ICON-SEPTECH PTY LTD DRAINAGE PRODUCTS	31/07/2012	9,719.77
EF065390	10757	INDOOR KART HIRE O'CONNOR ENTERTAINMENT SERVICES	31/07/2012	1,250.00
EF065391	10768	INST OF PUBLIC WORKS ENG AUST - WA MEMBERSHIP FEES	31/07/2012	370.00
EF065392	10779	J F COVICH & CO PTY LTD ELECTRICAL SERVICES	31/07/2012	16,179.41
EF065393	10780	JACKSONS DRAWING SUPPLIES PTY LTD DRAWING SUPPLIES	31/07/2012	218.97
EF065394	10781	JANDAKOT EARTHMOVING & RURAL CONTRACTORS FIREBREAK CONSTRUCTION	31/07/2012	15,620.00
EF065395	10783	JANDAKOT METAL INDUSTRIES METAL SUPPLIES	31/07/2012	169.40
EF065396	10787	JANDAKOT ACCIDENT REPAIR CENTRE PANEL BEATING SERVICES	31/07/2012	1,352.48
EF065397	10794	JASON SIGNMAKERS SIGNS	31/07/2012	1,573.00
EF065398	10796	JCA DEALER SERVICES REPAIRS/MAINTENANCE SERVICES	31/07/2012	255.00
EF065399	10803	GECKO CONTRACTING TURF & LANDSCAPE MTNCE MOWING/LANDSCAPING SERVICES	31/07/2012	4,598.00
EF065400	10814	JR & A HERSEY PTY LTD SAFETY CLOTHING SUPPLIES	31/07/2012	1,260.60
EF065401	10836	KERB DOCTOR SUPPLY & LAY KERBING	31/07/2012	7,712.10
EF065402	10879	LES MILLS AEROBICS INSTRUCTION/TRAINING SERVICES	31/07/2012	1,034.26
EF065403	10901	LOST LADDER WINDOW CLEANING WINDOW CLEANING SERVICES	31/07/2012	14,756.50
EF065404	10903	LOVEGROVE TURF SERVICES PTY LTD TURF MAINTENANCE SERVICES	31/07/2012	2,970.00

EF065405	10912	M2 TECHNOLOGY PTY LTD MESSAGING SERVICES	31/07/2012	396.00
EF065406	10913	MACDONALD JOHNSTON ENGINEERING CORP REPAIR SERVICES	31/07/2012	1,614.56
EF065407	10918	MAIN ROADS WA REPAIRS/MAINTENANCE SERVICES	31/07/2012	4,206.05
EF065408	10923	MAJOR MOTORS PTY LTD REPAIRS/MAINTENANCE SERVICES	31/07/2012	736.04
EF065409	10938	MAXWELL ROBINSON & PHELPS PEST & WEED MANAGEMENT	31/07/2012	25,733.45
EF065410	10939	LINFOX ARMAGUARD BANKING SECURITY SERVICES	31/07/2012	1,871.06
EF065411	10942	MCGEES PROPERTY PROPERTY CONSULTANCY SERVICES	31/07/2012	6,600.00
EF065412	10944	MCLEODS LEGAL SERVICES	31/07/2012	4,145.95
EF065413	10946	MEDIA ON MARS GRAPHIC DESIGN SERVICES	31/07/2012	2,557.20
EF065414	10950	MELVILLE MITSUBISHI MOTOR VEHICLES & PARTS	31/07/2012	1,026.85
EF065415	10960	METRO FILTERS FILTER SUPPLIES	31/07/2012	22.50
EF065416	10972	MIRACLE RECREATION EQUIPMENT PLAYGROUND/PARK EQUIPMENT	31/07/2012	1,540.00
EF065417	10973	MIRCO BROS PTY LTD FERTILISER SUPPLIES	31/07/2012	46.15
EF065418	10981	MOBILE MASTERS COMMUNICATIONS EQUIPMENT/SERVICES	31/07/2012	257.40
EF065419	10990	MOWER CITY SALES & SERVICES PTY LTD LAWN MOWING EQUIPMENT	31/07/2012	1,419.95
EF065420	10991	BEACON EQUIPMENT MOWING EQUIPMENT	31/07/2012	394.90
EF065421	10993	MP ELECTROLOCATION ROAD DESIGN SERVICES	31/07/2012	1,100.00
EF065422	10997	WILSON PARKING AUSTRALIA SECURITY SERVICES	31/07/2012	728.53
EF065423	11022	NATIVE ARC DONATION	31/07/2012	200.00
EF065424	11028	NEVERFAIL SPRINGWATER LIMITED BOTTLED WATER SUPPLIES	31/07/2012	1,008.00
EF065425	11055	OFFICE LINE OFFICE FURNITURE	31/07/2012	799.70
EF065426	11068	VODAFONE HUTCHISON AUSTRALIA PTY LTD PAGING SERVICES	31/07/2012	593.66
EF065427	11076	P & D AMARANTI ENGINEERING ENGINEERING SERVICES	31/07/2012	100.00
EF065428	11125	PERTH SCIENTIFIC EQUIPMENT EQUIPMENT	31/07/2012	1,116.50
EF065429	11136	DONEGAN ENTERPRISES FENCING REPAIRS/MAINTENANCE	31/07/2012	1,694.00
EF065430	11152	FULTON HOGAN INDUSTRIES PTY LTD ROAD MAINTENANCE	31/07/2012	6,122.12
EF065431	11164	PMP PRINT PTY LTD PRINTING SERVICES	31/07/2012	275.00
EF065432	11182	PREMIUM BRAKE & CLUTCH SERVICE BRAKE SERVICES	31/07/2012	5,726.05
EF065433	11204	QUALITY COMICS COMIC BOOKS	31/07/2012	261.25
EF065434	11208	QUICK CORPORATE AUSTRALIA PTY LTD STATIONERY/CONSUMABLES	31/07/2012	1,589.91
EF065435	11214	RAECO INTERNATIONAL PTY LTD STATIONERY SUPPLIES	31/07/2012	1,797.71
EF065436	11235	REINFORCED CONCRETE PIPES PTY LTD CONCRETE PIPE SUPPLIES	31/07/2012	2,109.27

EF065437	11240	PINK HYGIENE SOLUTIONS SANITARY SERVICES	31/07/2012	2,158.28
EF065438	11243	REPCO AUTO PARTS AUTO SUPPLIES	31/07/2012	396.00
EF065439	11245	RETECH RUBBER PTY LTD RUBBER SUPPLIES	31/07/2012	10,998.90
EF065440	11248	RICOH AUSTRALIA OFFICE EQUIPMENT	31/07/2012	181.50
EF065441	11257	RNR CONTRACTING PTY LTD SUPPLY & DELIVER EMULSION	31/07/2012	6,732.00
EF065442	11261	ROCKFACE INDOOR ROCK CLIMBING GYM ENTERTAINMENT ENTRY FEES	31/07/2012	420.00
EF065443	11264	ROCLA PIPELINE PRODUCTS CONCRETE LINER SUPPLIES	31/07/2012	1,838.76
EF065444	11284	ROYAL LIFE SAVING SOCIETY AUSTRALIA TRAINING SERVICES	31/07/2012	120.00
EF065445	11294	SAFEMAN (WA) PTY LTD PROTECTIVE CLOTHING/EQUIPMENT	31/07/2012	891.12
EF065446	11304	SANAX MEDICAL & FIRST AID SUPPLIES MEDICAL SUPPLIES	31/07/2012	317.13
EF065447	11307	SATELLITE SECURITY SERVICES PTY LTD SECURITY SERVICES	31/07/2012	15,259.14
EF065448	11308	SBA SUPPLIES HARDWARE SUPPLIES	31/07/2012	370.98
EF065449	11318	SELECT SECURITY WA PTY LTD SECURITY SERVICES	31/07/2012	293.70
EF065450	11331	SHAWMAC PTY LTD CONSULTANCY SERVICES - CIVIL	31/07/2012	2,068.00
EF065451	11333	SHELFORD CONSTRUCTIONS PTY LTD CONSTRUCTION SERVICES	31/07/2012	23,806.47
EF065452	11361	SIGMA CHEMICALS PTY LTD CHEMICAL SUPPLIES	31/07/2012	544.50
EF065453	11380	SNAP PRINTING FREMANTLE PRINTING SERVICES	31/07/2012	950.15
EF065454	11406	SOUTH LAKE OTTEY FAMILY & NEIGHBOURHOOD CENTRE CO-HEALTH INNOVATION / SUSTAINABILITY	31/07/2012	3,626.40
EF065455	11425	SOUTHERN METROPOLITAN REGIONAL COUNCIL WASTE DISPOSAL GATE FEES	31/07/2012	259,024.20
EF065456	11441	SPARE PARTS PUPPET THEATRE ENTERTAINMENT SERVICES	31/07/2012	737.00
EF065457	11453	SPEARWOOD NEWSROUND NEWSPAPER SUPPLIES	31/07/2012	815.94
EF065458	11459	SPEARWOOD VETERINARY HOSPITAL VETERINARY SERVICES	31/07/2012	603.00
EF065459	11469	SPORTS TURF TECHNOLOGY TURF CONSULTANCY SERVICES	31/07/2012	9,768.00
EF065460	11482	ST JOHN AMBULANCE AUSTRALIA FIRST AID TRAINING & SUPPLIES	31/07/2012	149.00
EF065461	11483	ST JOHN AMBULANCE AUST WA OPERATIONS FIRST AID COURSES	31/07/2012	657.00
EF065462	11493	SAI GLOBAL LTD PUBLICATIONS - STANDARDS	31/07/2012	379.32
EF065463	11505	STATE LIBRARY OF WESTERN AUSTRALIA BOOK SUPPLIES	31/07/2012	2,905.10
EF065464	11511	STATEWIDE BEARINGS BEARING SUPPLIES	31/07/2012	366.49
EF065465	11525	STRACHAN RA & TD PLUMBING SERVICES	31/07/2012	3,432.00
EF065466	11531	SUNNY INDUSTRIAL BRUSHWARE PTY LTD BRUSH/ROAD BROOM SUPPLIES	31/07/2012	1,944.25
EF065467	11546	T FAULKNER & CO INSTALLATIONS/SUPPLY OF HAND RAILS	31/07/2012	2,020.00
EF065468	11557	TECHNOLOGY ONE LTD IT CONSULTANCY SERVICES	31/07/2012	14,091.00

EF065469	11613	TIMEZONE FREMANTLE ENTERTAINMENT SERVICES	31/07/2012	300.00
EF065470	11619	TITAN FORD AUTOMOTIVE SERVICES	31/07/2012	778.00
EF065471	11625	TOTAL EDEN PTY LTD RETICULATION SUPPLIES	31/07/2012	73,293.39
EF065472	11651	TREE WATERING SERVICES TREE WATERING SERVICES	31/07/2012	1,378.00
EF065473	11652	TRENCHBUSTERS HIRING SERVICES	31/07/2012	1,261.70
EF065474	11655	TRISLEYS HYDRAULIC SERVICES PTY LTD POOL EQUIPMENT/REPAIRS	31/07/2012	643.50
EF065475	11657	TRUCKLINE PARTS CENTRES AUTOMOTIVE SPARE PARTS	31/07/2012	2,109.62
EF065476	11659	TRUGRADE MEDICAL SUPPLIES MEDICAL SUPPLIES	31/07/2012	794.00
EF065477	11665	TUNNEL VISION PLUMBING SERVICES	31/07/2012	990.00
EF065478	11669	TYCO SERVICES FIRE ALARM SYSTEM REPAIRS	31/07/2012	618.12
EF065479	11697	VAT MAN-FAT FILTERING SYSTEMS FILTER CLEANING SERVICES	31/07/2012	193.00
EF065480	11701	VIBRA INDUSTRIAL FILTRATION A/ASIA FILTER SUPPLIES	31/07/2012	555.50
EF065481	11715	WA BLUEMETAL ROADBASE SUPPLIES	31/07/2012	2,603.42
EF065482	11722	WA HINO SALES & SERVICE REPAIRS/MAINTENANCE SERVICES	31/07/2012	1,254.59
EF065483	11725	WA LIBRARY SUPPLIES PTY LTD LIBRARY SUPPLIES	31/07/2012	794.00
EF065484	11726	WA LIMESTONE LIMESTONE SUPPLIES	31/07/2012	4,147.34
EF065485	11739	WA SPIT ROAST COMPANY CATERING SERVICES	31/07/2012	1,491.20
EF065486	11742	WACKER NEUSON PTY LTD HARDWARE/EQUIPMENT SUPPLIES	31/07/2012	682.44
EF065487	11773	WESFARMERS LANDMARK LIMITED CHEMICAL SUPPLIES	31/07/2012	439.52
EF065488	11786	WESTCARE INDUSTRIES STATIONERY/SAFETY VESTS	31/07/2012	702.90
EF065489	11787	DEPT OF TRANSPORT (WA GOVT) WA GOVT DEPARTMENT	31/07/2012	861.00
EF065490	11793	WESTERN IRRIGATION PTY LTD IRRIGATION SERVICES/SUPPLIES	31/07/2012	13,743.63
EF065491	11806	WESTRAC PTY LTD REPAIRS/MTNCE - EARTHMOVING EQUIPMENT	31/07/2012	122,018.75
EF065492	11810	ABAXA PREVIOUSLY WH LOCATIONS LOCATING SERVICES	31/07/2012	955.90
EF065493	11824	WORK CLOBBER SAFETY CLOTHING	31/07/2012	92.50
EF065494	11854	ZIPFORM PRINTING SERVICES	31/07/2012	6,346.32
EF065495	11972	COBEY MAINTENANCE SERVICES TURF MANAGEMENT	31/07/2012	7,288.60
EF065496	11974	GREENWASTE SERVICES MULCHING/SHREDDING SERVICES	31/07/2012	12,384.90
EF065497	11984	NEWTON CABINETS CABINET MAKING SERVICES	31/07/2012	3,608.00
EF065498	11987	SAFETY ZONE AUSTRALIA PTY LTD SAFETY EQUIPMENT	31/07/2012	255.71
EF065499	11990	EARTHCARE (AUSTRALIA) P/L LANDSCAPING SERVICES	31/07/2012	943.80
EF065500	12007	SHANE MCMASTER SURVEYS SURVEYING SERVICES	31/07/2012	13,310.00

EF065501	12014	TUTT BRYANT EQUIPMENT BT EQUIPMENT PTY LTD T/AS EXCAVATING/EARTHMOVING EQUIPMENT	31/07/2012	928,460.03
EF065502	12123	TRANEN PTY LTD PLANT SUPPLIES	31/07/2012	1,183.88
EF065503	12127	ABLE WESTCHEM CHEMICAL/CLEANING SUPPLIES	31/07/2012	1,173.36
EF065504	12153	HAYS PERSONNEL SERVICES PTY LTD EMPLOYMENT SERVICES	31/07/2012	21,193.32
EF065505	12320	MUNDARING GARDEN CENTRE PLANT SUPPLIES	31/07/2012	20,116.75
EF065506	12415	FACE PAINTING FUN AND GAMES ENTERTAINMENT SERVICES	31/07/2012	1,880.00
EF065507	12542	SEALIN GARLETT CEREMONIAL SERVICES	31/07/2012	400.00
EF065508	12589	AUSTRALIAN INSTITUTE OF MANAGEMENT TRAINING SERVICES	31/07/2012	3,340.00
EF065509	12672	NORMAN DISNEY & YOUNG CONSULTANCY SERVICES	31/07/2012	50,380.00
EF065510	12694	SPECIALISED LIFTING SERVICE LIFTING EQUIPMENT & SERVICES	31/07/2012	166.10
EF065511	12712	MISS MAUD CATERING SERVICES	31/07/2012	401.63
EF065512	12779	WESTERN RESOURCE RECOVERY PTY LTD WASTE DISPOSAL SERVICES	31/07/2012	1,030.48
EF065513	12796	MEDIA MONITORS AUSTRALIA PTY LTD MEDIA MONITORING SERVICES	31/07/2012	346.89
EF065514	12883	CONSERVATION VOLUNTEERS AUSTRALIA ENVIRONMENTAL SERVICES	31/07/2012	3,960.00
EF065515	12998	PLAYRIGHT AUSTRALIA PTY LTD INSPECTION SERVICES - PLAYGROUNDS	31/07/2012	37,620.00
EF065516	12999	SV GLASS GLAZING SERVICES	31/07/2012	13,570.70
EF065517	13102	MICHAEL PAGE INTERNATIONAL EMPLOYMENT SERVICES	31/07/2012	4,912.29
EF065518	13111	OCE-AUSTRALIA LIMITED COPIERS/PRINTERS	31/07/2012	529.77
EF065519	13187	CHEFMASTER AUSTRALIA BIN LINERS	31/07/2012	169.25
EF065520	13373	THE HIRE GUYS HIRING SERVICES	31/07/2012	875.00
EF065521	13409	KLEENIT CLEANING SERVICES	31/07/2012	1,496.00
EF065522	13462	ATI-MIRAGE PTY LTD TRAINING SERVICES	31/07/2012	579.72
EF065523	13510	BEAUREPAIRES FOR TYRES TYRES	31/07/2012	645.00
EF065524	13563	ECOJOBS ENVIRONMENTAL PERSONNEL EMPLOYMENT SERVICES	31/07/2012	14,885.75
EF065525	13582	DBS FENCING FENCING SERVICES	31/07/2012	4,290.00
EF065526	13671	CORPORATE EXPRESS AUSTRALIA LTD OFFICE/STATIONERY SUPPLIES	31/07/2012	6,075.49
EF065527	13764	DIMENSION DATA LEARNING SOLUTIONS COMPUTER SOFTWARE	31/07/2012	8,470.00
EF065528	13767	ELLIOTTS IRRIGATION PTY LTD IRRIGATION SERVICES	31/07/2012	1,234.20
EF065529	13832	INSIGHT CALL CENTRE SERVICES COMMUNICATION SERVICES	31/07/2012	7,680.81
EF065530	13937	HIND'S TRANSPORT SERVICES TRANSPORT SERVICES	31/07/2012	16,568.95
EF065531	14035	EXPO DOCUMENT COPY CENTRE (WA) PTY LTD PORTABLE DISPLAY SYSTEMS	31/07/2012	5,148.00
EF065532	14049	TURF GRASS ASSOCIATION OF AUSTRALIA TRAINING PROVIDER	31/07/2012	594.00

EF065533	14188	CHALLENGER INSTITUTE OF TECHNOLOGY - MURDOCH TRAINING PROVIDER	31/07/2012	220.60
EF065534	14447	ANDOVER DETAILERS DETAILING SERVICES	31/07/2012	301.00
EF065535	14593	AUSTREND INTERNATIONAL PTY LTD ALUMINIUM SUPPLIES	31/07/2012	6,930.00
EF065536	14640	LANDMARK ENGINEERING & DESIGN OUTDOOR FURNITURE - PARKS/RESERVES	31/07/2012	19,536.00
EF065537	14667	APPEALING SIGNS SIGNS	31/07/2012	2,997.32
EF065538	14744	MUCHEA TREE FARM PLANTS/GREENSTOCK	31/07/2012	30,628.00
EF065539	14777	LGIS INSURANCE BROKING INSURANCE PREMIUMS	31/07/2012	186,373.24
EF065540	14791	ALBERTS ALL SUBURBS CATERING CATERING SERVICES	31/07/2012	1,815.00
EF065541	14834	GILDEN TREE FARM PLANT/TREE SUPPLIES	31/07/2012	8,250.00
EF065542	14908	OAKVALE CAPITAL LIMITED CONSULTANCY SERVICES	31/07/2012	4,252.88
EF065543	15072	DRUM PRINT & PUBLICATIONS PRINTING SERVICES	31/07/2012	1,564.00
EF065544	15162	PERTH MANAGEMENT SERVICES PROPERTY MANAGERS	31/07/2012	97.47
EF065545	15327	LKL CONTRACTING BOBCAT HIRE / LANDSCAPING SERVICES	31/07/2012	21,441.71
EF065546	15337	CHUBB SECURITY SERVICES LTD SECURITY SERVICES	31/07/2012	283.33
EF065547	15393	GREENWAY ENTERPRISES HARDWARE SUPPLIES	31/07/2012	845.25
EF065548	15462	GREENSLADES & CO P/L PET FOOD SUPPLIES	31/07/2012	72.00
EF065549	15515	ANYBODY'S FITNESS WATER AEROBICS	31/07/2012	680.00
EF065550	15541	JANDAKOT NEWS NEWSPAPER SUPPLIERS	31/07/2012	171.96
EF065551	15544	NNT CLOTHING - UNIFORMS	31/07/2012	894.51
EF065552	15550	APACE AID PLANTS & LANDSCAPING SERVICES	31/07/2012	7,151.04
EF065553	15588	NATURAL AREA MANAGEMENT & SERVICES WEED SPRAYING	31/07/2012	15,114.90
EF065554	15611	CHALLENGER TAFE - ROCKINGHAM TRAINING SERVICES	31/07/2012	398.00
EF065555	15678	A2Z PEST CONTROL PEST CONTROL	31/07/2012	473.00
EF065556	15862	FREMANTLE MILK DISTRIBUTORS MILK DELIVERY	31/07/2012	701.65
EF065557	15914	T-QUIP MOWING EQUIPMENT	31/07/2012	749.95
EF065558	16050	SOUTH METROPOLITAN PERSONNEL EMPLOYMENT SERVICES	31/07/2012	51.15
EF065559	16064	CMS ENGINEERING PTY LTD AIRCONDITIONING SERVICES	31/07/2012	10,205.25
EF065560	16107	WREN OIL WASTE DISPOSAL	31/07/2012	36.30
EF065561	16291	WA PROFILING ROAD PROFILING SERVICES	31/07/2012	5,313.00
EF065562	16396	MAYDAY EARTHMOVING GRADER HIRE	31/07/2012	8,123.50
EF065563	16403	ROBINSON BUILDTECH BUILDING SERVICES - ALTERATIONS	31/07/2012	9,328.11
EF065564	16535	WIDDESON'S HIRE SERVICE HIRE OF CATERING SUPPLIES	31/07/2012	134.50

EF065565	16568	BIOWISE SOIL SUPPLIES	31/07/2012	4,312.00
EF065566	16894	TREBLEX INDUSTRIAL PTY LTD CHEMICALS - AUTOMOTIVE	31/07/2012	633.60
EF065567	16985	WA PREMIX CONCRETE SUPPLIES	31/07/2012	19,153.64
EF065568	17033	MULTI SWEEP SWEEPING SERVICES	31/07/2012	7,016.36
EF065569	17040	DEPARTMENT OF PREMIER AND CABINET PUBLICATIONS	31/07/2012	1,086.35
EF065570	17092	CENTRAL SCREENS SECURITY SYSTEMS/PRODUCTS	31/07/2012	200.00
EF065571	17097	VALUE TISSUE PAPER PRODUCTS	31/07/2012	649.00
EF065572	17178	THE CLEAN UP COMPANY WASTE DISPOSAL SERVICES	31/07/2012	1,611.50
EF065573	17362	JOHN EARLEY TRAINING	31/07/2012	240.00
EF065574	17471	PIRTEK (FREMANTLE) PTY LTD HOSES & FITTINGS	31/07/2012	1,498.52
EF065575	17481	ADS AUTOMATION PTY LTD DOOR/GATE REPAIRS	31/07/2012	747.65
EF065576	17587	WEST COAST SHADE SHADE STRUCTURES	31/07/2012	7,293.00
EF065577	17600	ERLECTIONS (WA) GUARD RAILS	31/07/2012	22,605.88
EF065578	17942	MRS MAC'S FOOD SUPPLIES	31/07/2012	292.20
EF065579	18073	PARAMOUNT SECURITY SERVICES SECURITY SERVICES	31/07/2012	360.80
EF065580	18086	DONALD CANT WATTS CORKE (WA) P/L PROJECT MANAGEMENT SERVICES	31/07/2012	7,700.00
EF065581	18100	DAVIS LANGDON AUSTRALIA COST MANAGEMENT SERVICES	31/07/2012	9,108.00
EF065582	18114	BOLLIG DESIGN GROUP P/L ARCHITECTURAL SERVICES	31/07/2012	14,960.00
EF065583	18122	SIGNMAN SIGNAGE	31/07/2012	451.44
EF065584	18147	AURECON AUSTRALIA PTY LTD CONSULTANCY - CIVIL ENGINEERING	31/07/2012	23,061.50
EF065585	18217	METROPOLITAN OMNIBUS COMPANY BUS HIRE	31/07/2012	693.00
EF065586	18389	GAVIN CONSTRUCTION BUILDING CONSTRUCTION SERVICES - VARIOUS	31/07/2012	1,235,947.40
EF065587	18490	MTD MAKING THE DIFFERENCE COUNSELLING SERVICES	31/07/2012	2,750.00
EF065588	18506	ABCO PRODUCTS CLEANING PRODUCTS	31/07/2012	255.64
EF065589	18508	JOHN TURNER BRICK LAY	31/07/2012	4,312.00
EF065590	18613	ECO-HIRE EQUIPMENT HIRE	31/07/2012	17,099.50
EF065591	18614	BOWMAN & ASSOCIATES PTY LTD CONSULTANCY SERVICES - PROJECT MGMT	31/07/2012	46,750.00
EF065592	18639	HAMILTON HILL DELIVERY ROUND NEWSPAPER DELIVERY SERVICE	31/07/2012	39.90
EF065593	18678	DOWNER EDI ENGINEERING ELECTRICAL PTY LTD ELECTRICAL SERVICES	31/07/2012	110,000.00
EF065594	18695	MYAREE CRANE HIRE CRANE HIRE	31/07/2012	253.00
EF065595	18760	MELVILLE SUBARU VEHICLE PURCHASE	31/07/2012	360.90
EF065596	18764	AFFIRMATIVE PAVING BRICK PAVING SERVICES	31/07/2012	4,917.00

EF065597	18884	SILICH ENTERPRISES PTY LTD BOLLARDS	31/07/2012	4,229.50
EF065598	18904	WESTERN PLANT HIRE PLANT HIRE	31/07/2012	3,960.00
EF065599	18941	ALLSTAMPS STATIONERY	31/07/2012	191.30
EF065600	18953	BOSTON MAINTENANCE SERVICES MAINTENANCE SERVICES	31/07/2012	194.59
EF065601	18962	SEALANES (1985) P/L CATERING SUPPLIES	31/07/2012	2,072.39
EF065602	18988	SYMONDS SEEDS PLANTS	31/07/2012	2,716.78
EF065603	18997	GISSA INTERNATIONAL PTY LTD TRAINING SERVICES	31/07/2012	7,865.00
EF065604	19017	AIRBRUSH TATTOOS WA TATTOOING SERVICES	31/07/2012	330.00
EF065605	19038	DOWSING CONCRETE CONCRETING SERVICES	31/07/2012	74,542.77
EF065606	19058	VALENTINE'S CAMERA HOUSE FREMANTLE PHOTOGRAPHIC EQUIPMENT	31/07/2012	797.41
EF065607	19066	DVA FABRICATIONS LIBRARY SUPPLIES	31/07/2012	1,360.00
EF065608	19155	WE LIKE TO PARTY BALLOONS & PARTY SUPPLIES	31/07/2012	139.00
EF065609	19306	ZIP HEATERS (AUST) PTY LTD HEATERS	31/07/2012	276.32
EF065610	19366	JOSEPHINE'S EDUTAINMENT ENTERTAINMENT SERVICES	31/07/2012	150.00
EF065611	19533	WOOLWORTHS LTD GROCERIES	31/07/2012	636.02
EF065612	19541	TURF CARE WA P/L TURF SERVICES	31/07/2012	14,949.00
EF065613	19545	GRASSWEST BUILDING & GARDEN MAINTENANCE	31/07/2012	1,960.00
EF065614	19619	SKIPPER TRUCKS PURCHASE OF NEW VEHICLE	31/07/2012	108,917.59
EF065615	19628	PAPERBARK TECHNOLOGIES ARBORICULTURAL CONSULTANCY SERVICES	31/07/2012	1,284.00
EF065616	19830	AUTO MASTERS - SPEARWOOD MECHANICAL SERVICES	31/07/2012	2,828.91
EF065617	19847	PFD FOOD SERVICES PTY LTD CATERING SERVICES	31/07/2012	1,975.05
EF065618	19856	WESTERN TREE RECYCLERS SHREDDING SERVICES	31/07/2012	120,933.92
EF065619	19862	CITY & METRO PEST MANAGEMENT WA PEST CONTROL SERVICES	31/07/2012	755.00
EF065620	20000	AUST WEST AUTO ELECTRICAL P/L AUTO ELECTRICAL SERVICES	31/07/2012	5,420.58
EF065621	20112	PENNANT HOUSE FLAGS AND BANNERS	31/07/2012	152.90
EF065622	20114	LOUNGE BACKLINE MUSICAL INSTRUMENTS	31/07/2012	3,289.30
EF065623	20124	SHIRLEY BICKLEY SCHOOL OF DANCING TRAINING SERVICES - DANCING	31/07/2012	416.00
EF065624	20135	SCP CONSERVATION MAINTENANCE SERVICES - BUSHLAND	31/07/2012	2,950.00
EF065625	20145	IAN ATKINS PLUMBER PLUMBING SERVICES	31/07/2012	24,200.00
EF065626	20251	GLADSTONE HEALTH & LEISURE COMPUTER SOFTWARE	31/07/2012	13,025.33
EF065627	20299	REHAB REPAIRS REPAIRS - HEALTHCARE EQUIPMENT	31/07/2012	148.50
EF065628	20307	OCTAGON-BKG LIFTS MAINTENANCE SERVICES	31/07/2012	1,292.50

EF065629	20321	RIVERJET P/L EDUCTING-CLEANING SERVICES	31/07/2012	11,302.50
EF065630	20322	PLANTRITE PLANT SUPPLIES	31/07/2012	23,491.82
EF065631	20341	WILHELMINA MARIA HOUWEN GARDENING SERVICES	31/07/2012	960.00
EF065632	20464	THE SALVATION ARMY COMMUNITY GRANT	31/07/2012	763.40
EF065633	20535	HOME-GROWN THEATRE DRAMA CLASSES	31/07/2012	132.26
EF065634	20583	ANIMAL PEST MANAGEMENT SERVICES FERAL ANIMAL CONTROL	31/07/2012	1,991.00
EF065635	20623	RGS CONSULTING CONSULTANCY - GENERAL	31/07/2012	1,925.00
EF065636	20748	CRANWORKS AUSTRALASIA HYDRAULIC REPAIR SERVICES	31/07/2012	3,389.22
EF065637	20763	JECODA CONCRETE CONCRETE SUPPLY	31/07/2012	1,408.00
EF065638	20833	BOOMERS PLUMBING & GAS PLUMBING SERVICES	31/07/2012	828.30
EF065639	20848	SURVEYING AND SPATIAL SCIENCES INSTITUTE MEMBERSHIP	31/07/2012	409.00
EF065640	20882	BELL-VISTA FRUIT & VEGETABLE FRUIT & VEGETABLE	31/07/2012	971.07
EF065641	20924	INDIGO PERSONAL TRAINING PERSONAL TRAINING	31/07/2012	3,200.00
EF065642	20940	ROBERT HALF AUSTRALIA PTY LTD EMPLOYMENT SERVICES	31/07/2012	771.54
EF065643	21126	ULVERSCROFT LARGE PRINT (AUST.) P/L BOOKS	31/07/2012	72.60
EF065644	21127	JOANNA AYCKBOURN INSTRUCTION - SINGING	31/07/2012	600.00
EF065645	21193	SPM CONSULTANTS PTY LTD CONSULTANCY SERVICES	31/07/2012	11,693.00
EF065646	21198	STUDIO KRAZE VIDEO PRODUCTIONS	31/07/2012	120.00
EF065647	21290	ONSITE RENTALS EQUIPMENT HIRE /TOILETS ETCE	31/07/2012	948.86
EF065648	21291	CHITTERING VALLEY WORM FARM ENVIRONMENTAL EDUCATION	31/07/2012	2,432.00
EF065649	21300	LEASECHOICE LEASE AGREEMENT	31/07/2012	1,862.46
EF065650	21363	TENDERLINK.COM PTY LTD COMPUTER SOFTWARE	31/07/2012	550.00
EF065651	21371	SANPOINT PTY LTD KERBING SERVICES	31/07/2012	101,103.75
EF065652	21386	FREMANTLE LAWYERS PTY LTD LEGAL SERVICES	31/07/2012	15,563.85
EF065653	21397	THE PLAYROOM O'CONNOR TOYS AND GAMES	31/07/2012	392.68
EF065654	21529	BRAND SUCCESS PROMOTIONAL PRODUCTS	31/07/2012	2,942.50
EF065655	21580	ROLLERDROME MORLEY ROLLER-SKATING	31/07/2012	264.00
EF065656	21581	COASTAL ZONE MANAGEMENT PTY LTD CONSULTANCY SERVICES	31/07/2012	16,830.00
EF065657	21627	MANHEIM PTY LTD IMPOUNDED VEHICLES	31/07/2012	5,759.60
EF065658	21674	MCLERNS SUPPLY AND DEMAND OFFICE FURNITURE	31/07/2012	373.90
EF065659	21678	IANNELLO DESIGNS GRAPHIC DESIGN	31/07/2012	2,178.00
EF065660	21694	UNITED EQUIPMENT PTY LTD USED EQUIPMENT	31/07/2012	461.12

EF065661	21696	TRANSPACIFIC SUPERIOR PAK PTY LTD SIGNAGE SERVICES	31/07/2012	392.05
EF065662	21697	ICT EXPRESS PTY LTD CONSULTANCY SERVICES - IT	31/07/2012	22,253.00
EF065663	21790	BELGRAVIA LEISURE PTY LTD LEISURE SERVICES - POOL COMPLEX	31/07/2012	140.00
EF065664	21791	THE LEISURE INSTITUTE OF WA (AQUATICS) INC. PROFESSIONAL ORGANISATION	31/07/2012	105.00
EF065665	21796	GREEN LEAF GARDENS LANDSCAPING SERVICES	31/07/2012	1,800.00
EF065666	21909	DIEBACK WORKING GROUP TRAINING SERVICES	31/07/2012	300.00
EF065667	21916	DAVIES FIRST NATIONAL REAL ESTATE CONSULTANCY SERVICES - REAL ESTATE	31/07/2012	25,312.49
EF065668	21922	DIGGA-WEST EARTHMOVING EQUIPMENT	31/07/2012	1,150.60
EF065669	21946	RYAN'S QUALITY MEATS MEAT SUPPLIES	31/07/2012	2,059.94
EF065670	21990	MEDIBANK HEALTH SOLUTIONS PTY LTD MEDICAL SERVICES	31/07/2012	2,191.20
EF065671	22122	PLATTERS 'R' US CATERING SERVICES	31/07/2012	255.00
EF065672	22126	HAPPY VALLEY RESOURCES P/L MOLTONI WASTE DISPOSAL	31/07/2012	22,088.00
EF065673	22133	AIR-BORN AMUSEMENTS AMUSEMENT SERVICES	31/07/2012	2,617.50
EF065674	22169	GREENSTAR GROUP WA PTY LTD GREENSTAR GROUP WA AIR CONDITIONING SERVICES	31/07/2012	2,239.93
EF065675	22194	INDEPENDENT VALUERS OF WA VALUATIONS	31/07/2012	1,100.00
EF065676	22242	ASPHALT SURFACES PTY LTD ASPHALTING SERVICES	31/07/2012	123,669.42
EF065677	22260	SOUTHERN STAR TRAMPOLINE ACADEMY SPORT & RECREATION	31/07/2012	1,100.00
EF065678	22261	OLD MACDONALD'S TRAVELLING FARMS - WA SOUTH/EAST ENTERTAINMENT SERVICES	31/07/2012	600.00
EF065679	22332	MACQUARIE EQUIPMENT RENTALS PTY LTD LEASE RENTAL	31/07/2012	14,514.57
EF065680	22339	EDITH COWAN UNIVERSITY EDUCATIONAL SERVICES - TERTIARY	31/07/2012	1,755.60
EF065681	22349	FREMANTLE TRAILERS TRAILERS - BOAT AND BOX	31/07/2012	1,111.85
EF065682	22388	CARRINGTON'S TRAFFIC SERVICES TRAFFIC MANAGEMENT SERVICES	31/07/2012	19,497.50
EF065683	22448	CAKES WEST PTY LTD CATERING	31/07/2012	38.50
EF065684	22553	BROWNES FOOD OPERATIONS CATERING SUPPLIES	31/07/2012	925.90
EF065685	22569	KINETIC HEALTH GROUP PTY LTD MEDICAL SERVICES	31/07/2012	720.50
EF065686	22602	DENSFORD CIVIL PTY LTD VARIOUS PROJECT PAYMENTS	31/07/2012	470,433.24
EF065687	22639	SHATISH CHAUHAN TRAINING SERVICES - YOGA	31/07/2012	260.00
EF065688	22653	PCYC FREMANTLE SPONSORSHIP	31/07/2012	400.00
EF065689	22682	BEAVER TREE SERVICES PTY LTD TREE PRUNING SERVICES	31/07/2012	73,942.00
EF065690	22736	JAMES FOLEY ILLUSTRATIONS ARTISTIC	31/07/2012	400.00
EF065691	22737	CJS LIMESTONE CONTRACTORS PTY LTD LIMESTONE WORKS	31/07/2012	7,689.00
EF065692	22749	PEOPLE SOLUTIONS AUSTRALASIA PTY LTD CONSULTANCY - HR	31/07/2012	1,430.00

EF065693	22805	COVS PARTS PTY LTD MOTOR PARTS	31/07/2012	160.06
EF065694	22806	AUSTRALIAN FUEL DISTRIBUTORS PTY LTD FUEL SUPPLIES	31/07/2012	124,028.09
EF065695	22854	LGISWA INSURANCE POLICIES	31/07/2012	898,130.13
EF065696	22856	COMMUNITY VOICE CONSULTANCY CONSULTANCY SERVICES	31/07/2012	3,000.00
EF065697	22913	AUSTRALIAN OFFICE LEADING BRANDS.COM.AU ENVELOPES	31/07/2012	3,576.38
EF065698	22963	WEST BIOFUEL TECHNOLOGIES PTY LTD EARTHMOVING /EXCAVATION SERVICES	31/07/2012	30,013.50
EF065699	23309	FUN IN TRAINING AUSTRALIA PTY LTD FITNESS CLASSES-INSTRUCTIONS	31/07/2012	627.00
EF065700	23450	CLEVER DESIGNS UNIFORMS	31/07/2012	369.60
EF065701	23473	MAX FAIRCLOUGH PHOTOGRAPHY PHOTOGRAPHY SERVICES	31/07/2012	640.00
EF065702	23570	A PROUD LANDMARK PTY LTD LANDSCAPE CONTRUCTION SERVICES	31/07/2012	54,676.05
EF065703	23625	VDM CONSULTING (WA) PTY LTD CONSULTANCY SERVICES	31/07/2012	3,877.50
EF065704	23683	ASTRON SOIL & WATER PTY LTD CONSULTANCY-ENVIRONMENTAL	31/07/2012	19,544.97
EF065705	23685	ASTRO SYNTHETIC TURF PTY LTD SITE INSPECTIONS	31/07/2012	220.00
EF065706	23689	SCOPE GRAPHICS GRAPHIC DESIGN	31/07/2012	825.00
EF065707	23694	TIGER FITNESS (WA) PTY LTD GYM EQUIPMENT/SERVICE	31/07/2012	3,215.30
EF065708	23728	LAUREN ANDERSON CONTRACTING PHYSIOLOGY-EXERCISE	31/07/2012	350.00
EF065709	23779	COMO HEALTH AND FITNESS CENTRE FITNESS INSTRUCTION SERVICES	31/07/2012	3,168.00
EF065710	23817	ARUP PTY LTD CONSULTANCY-ENG,PLANNING,DESIGN	31/07/2012	9,513.35
EF065711	23819	GLENN MILLER PROPERTY CONSULTANTS LICENSED LAND VALUATION SERVICES	31/07/2012	6,100.00
EF065712	23826	FIA TECHNOLOGY PTY LTD FLOATING REEDBEDS	31/07/2012	5,500.00
EF065713	23854	FRATELLE GROUP PTY LTD ARCHITECTUAL SERVICES	31/07/2012	8,662.50
EF065714	23858	SPECIALISED SECURITY SHREDDING DOCUMENT DESTRUCTION SERVICES	31/07/2012	21.68
EF065715	23860	CHELSEA EASTWOOD NETBALL COACHING	31/07/2012	1,708.50
EF065716	23864	NATIONAL CENTRE FOR SUICIDE PREVENTION TRAINING (WA) TRAINING	31/07/2012	690.00
EF065717	23872	ASB MARKETING PTY LTD PROMOTIONAL PRODUCTS	31/07/2012	55.00
EF065718	23967	RDA OAKFORD HORSERIDING SERVICES	31/07/2012	800.00
EF065719	23973	OUTDOOR CAMERAS AUSTRALIA CAMERAS	31/07/2012	509.00
EF065720	23976	MR SCANMAN PTY LTD SCANNERS	31/07/2012	355.00
EF065721	23982	WELLINGTON SURPLUS STORES OUTDOOR EQUIPMENT	31/07/2012	316.80
EF065722	23985	WINNACOTT KATS JUNIOR FOOTBALL CLUB INC REGISTRATION FEES-KIDSPORT	31/07/2012	400.00
EF065723	23986	SHAMROCK RETAINING RETAINING WALLS	31/07/2012	18,115.00
EF065724	23990	NOONGAR RADIO ENTERPRISES RADIO BROADCASTING	31/07/2012	2,244.00

EF065725	23991	PAINT INDUSTRIES PTY LTD PAINT SUPPLIES	31/07/2012	383.63
EF065726	24034	ALESSANDRA & CO TRANSLATING SERVICES	31/07/2012	530.00
EF065727	24035	NEXT POWER RENEWABLE ENERGY	31/07/2012	31,190.17
EF065728	24040	SKYTUNE INVESTMENTS PTY LTD ROCK BREAKING	31/07/2012	900.00
EF065729	24041	ALLEN AIR & REFRIDGERATION AIR CONDITIONING SERVICES	31/07/2012	283.80
EF065730	24058	ACADEMY SERVICES (WA) PTY LTD CLEANING SERVICES	31/07/2012	414.90
EF065731	12656	COOGEE BEACH SURF LIFESAVING CLUB INC POOR GROVE SLSC DEVELOPMENT COSTS	31/07/2012	562,608.90
EF065732	13105	CITY OF PERTH LOCAL GOVT SERVICES	31/07/2012	18.00
EF065733	19005	REBECCA BERRY CAT STERILISATION	31/07/2012	100.00
EF065734	22313	FOCAS CAT STERILISATION CONTRIBUTION	31/07/2012	250.00
EF065735	23253	KOTT GUNNING LEGAL SERVICES	31/07/2012	199.10
EF065736	24091	DAVID MARTIN CROSSOVER CONTRIBUTION	31/07/2012	300.00
EF065737	24092	PRABODH WIJESINGHE CROSSOVER CONTRIBUTION	31/07/2012	300.00
EF065738	24093	LOUISA REBELO CROSSOVER CONTRIBUTION	31/07/2012	300.00
EF065739	24096	WAYNE NYBRO CROSSOVER CONTRIBUTION	31/07/2012	300.00
EF065740	24097	STEPHEN GOLDSWORTHY CROSSOVER CONTRIBUTION	31/07/2012	300.00
EF065741	24098	BRIAN CAMPBELL CAT STERILISATION	31/07/2012	50.00
EF065742	24099	RACHEL D'ARRIGO CAT STERILISATION	31/07/2012	50.00
EF065743	24100	MELISSA DELAHUNTY CAT STERILISATION	31/07/2012	50.00
EF065744	24101	LUKE EVANS & NATASHA RIDLEY CAT STERILISATION	31/07/2012	50.00
EF065745	24103	MICHELE KENT CAT STERILISATION	31/07/2012	50.00
EF065746	24104	FAYE KIMM CAT STERILISATION	31/07/2012	50.00
EF065747	24105	SHARON MURRAY CAT STERILISATION	31/07/2012	100.00
EF065748	24106	SUSAN ROWE CAT STERILISATION	31/07/2012	50.00
EF065749	24107	DIANA STOCK CAT STERILISATION	31/07/2012	50.00
EF065750	24108	WALLACE BRYAN SHAW CAT STERILISATION	31/07/2012	100.00
EF065751	24109	TJANDRA SETIAWAN OVERPAYMENT	31/07/2012	1,189.16
EF065752	24110	AR & KL MACKAY INTERIM ADJUSTMENT	31/07/2012	206.27
EF065753	24124	DAMIEN KELLY CROSSOVER CONTRIBUTION	31/07/2012	300.00
024942	13932	ARMAGUARD BANKING SERVICES	4/07/2012	4,511.70
024943	13932	ARMAGUARD BANKING SERVICES	11/07/2012	5,652.60
024944	13932	ARMAGUARD BANKING SERVICES	11/07/2012	5,652.60

024945	11760	WATER CORPORATION SEWER EASEMENT	13/07/2012	8,069.00
024946	19595	BEELIAR PARTNERSHIP UNIT TRUST DEVELOPER CONTRIBUTION REFUND	13/07/2012	100,000.00
024947	13932	ARMAGUARD BANKING SERVICES	18/07/2012	1,918.20
024948	13981	SOUTH LAKE LEISURE CENTRE PETTY CASH REIMBURSEMENT	18/07/2012	888.00
024949	10244	BUILDING & CONST INDUSTRY TRAINING FUND LEVY PAYMENT	23/07/2012	230,774.80
024950	10589	FINES ENFORCEMENT REGISTRY FINES ENFORCEMENT FEES	23/07/2012	13,932.00
024951	13932	ARMAGUARD BANKING SERVICES	25/07/2012	2,347.75
024952	10816	JUNGLE GYM ENTRY FEES	30/07/2012	322.00
024953	11597	THE PERTH MINT ENTRY FEES	30/07/2012	185.00
024954	12257	SENSIS PTY LTD ADVERTISING SERVICES	30/07/2012	480.92
024955	12351	CITY OF NEDLANDS LIBRARY SUPPLIES	30/07/2012	11.00
024956	12372	CITY OF WANNEROO LIBRARY BOOK REIMBURSEMENT	30/07/2012	14.39
024957	12509	OFFICE OF THE PUBLIC ADVOCATE REGISTRATION FEES	30/07/2012	60.00
024958	13465	KOSMIC ELECTRONIC INDUSTRIES SOUND EQUIPMENT	30/07/2012	1,250.00
024959	17343	RAC BUSINESSWISE MEMBERSHIP SUBSCRIPTION	30/07/2012	52.00
024960	17798	WESTERN DIAGNOSTIC PATHOLOGY ANALYTICAL SERVICES	30/07/2012	140.14
024961	18579	DEPARTMENT OF INDIGENOUS AFFAIRS GRANT REFUND	30/07/2012	330.00
024962	21471	WA MACHINERY GLASS GLAZING	30/07/2012	429.00
024963	21546	SUNPALM AUSTRALIA GARDENING SUPPLIES	30/07/2012	8,028.24
024964	21672	MEGA MUSIC AUSTRALIA MUSICAL INSTRUMENTS/SOUND EQUIPMENT	30/07/2012	2,382.49
024965	22903	UNIQUE INTERNATIONAL RECOVERIES LLC DEBT COLLECTORS	30/07/2012	729.60
024966	23696	JULIE REIDY DRAFTING SERVICES	30/07/2012	7,150.00
024967	23698	BIKE FORCE SUCCESS BIKES - BIKE ACCESSORIES	30/07/2012	2,354.00
024968	24055	ALBANY REGIONAL BOOKING SERVICE BOOKING - ACCOMADATION	30/07/2012	1,095.00
024969	24111	CEDRIC & SHIRLEY MITCHELL RATES REFUND	30/07/2012	436.56
024970	24112	JENNIFER & ZDRAVKO PUPICH RATES REFUND	30/07/2012	587.10
024971	24113	ALISON PATRICIA FOSTER RATES REFUND	30/07/2012	504.31
024972	24114	JEFFREY & MICHELLE HULL RATES REFUND	30/07/2012	248.40
024973	24115	DOREEN & PAUL GIBSON RATES REFUND	30/07/2012	218.29
024974	24116	VALERIE MAY HOWETT RATES REFUND	30/07/2012	376.35
024975	24117	HELEN NEHAL CHIESA RATES REFUND	30/07/2012	360.76
024976	24118	JAMES COOK RATES REFUND	30/07/2012	436.56

024977	24119	KEVIN LESLIE WALLIS RATES REFUND	30/07/2012	421.51
024978	24120	ROMA GLADYS COTTERELL RATES REFUND	30/07/2012	398.93
024979	24121	LILIA SOMERVILLE & SHARNA GRANT RATES REFUND	30/07/2012	181.86
024980	24122	EDWARD GEORGE DICKER LEE RATES REFUND	30/07/2012	1,646.14
024981	24123	LYNDA SELKIRK RATES REFUND	30/07/2012	479.79
024982	13932	ARMAGUARD BANKING SERVICES	1/08/2012	4,335.45
024983	20751	DEPARTMENT OF TRANSPORT VEHICLE REGISTRATIONS ADD RETENTION HELD NIL LESS PRIOR PERIOD CANCELLED CHEQUES/EFTS	2/08/2012	44,882.70
024943	13932	ARMAGUARD	11/07/2012 -	5,652.60

TOTAL

10,539,894.10

TOTAL AS PER AP SOURCE 13GLACT9991000

10,539,894.10

TOTAL AS PER TR SOURCE 13GLACT9991000

10,539,894.10

ADDITIONAL DIRECT PAYMENTS

BANK FEES

MERCHANT FEES COC	1,403.96
MERCHANT FEES SLLC	1,171.01
MERCHANT FEES VARIOUS OUT CENTRES	375.62
FLEXIPHONE FEE NATIONAL BPAY CHARGE	650.11
B POINT FEES	-
RTGS/ACLR FEE	2.50
NAB TRANSACT FEE	564.96
	4,168.16

FAMILY DAY CARE AND IN HOME CARE PAYMENTS

FDC PAYMENTS	46,128.86
IHC PAYMENTS	95,180.10
	141,308.96

PAYROLL TRANSACTIONS

COC 03/07/12	CITY OF COCKBURN 042958	483.22
COC 03/07/12	CITY OF COCKBURN 042958	5343.42
COC 03/07/12	CITY OF COCKBURN 042958	682095.02
COC 09/07/12	CITY OF COCKBURN 042958	385.22
COC 10/07/12	CITY OF COCKBURN 042958	226631.28
COC 11/07/12	CITY OF COCKBURN 042958	12831.09
COC 17/07/12	CITY OF COCKBURN 042958	694953.84
COC 19/07/12	CITY OF COCKBURN 042958	3172.76
COC 24/07/12	CITY OF COCKBURN 042958	221601.82
COC 29/06/12	CITY OF COCKBURN 042958	392.22
		1,847,889.89

CREDIT CARD PAYMENTS

CBA CREDIT CARD PAYMENT	61,336.58
	61,336.58

TOTAL PAYMENTS FOR JULY

12,594,597.69

PAYMENT SUMMARY

CHEQUE PAYMENTS

CHQ024942 – CHQ024983

CANCELLED PAYMENTS

024943

ELECTRONIC FUNDS TRANSFER PAYMENT

EF065152 – EF065753

STATEMENT OF FINANCIAL ACTIVITY

for the period ended 31 July 2012

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
Operating Revenue						
Governance	54,365,914	54,477,523	0%	(111,610) X	64,039,340	64,033,597
Financial Services	7,847	41,091	-81%	(33,244)	581,100	581,100
Information Services	60	303	-80%	(243)	3,627	3,627
Human Resource Management	34,567	37,728	-8%	(3,161)	159,768	130,340
Library Services	7,474	7,915	-6%	(440)	47,601	44,791
Community Services	2,579,184	2,630,851	-2%	(51,668)	6,415,069	6,303,706
Human Services	1,286,458	798,140	61%	488,318 ✓	6,380,841	6,221,506
Development Services	738,718	552,105	34%	186,613 ✓	3,253,904	3,126,770
Planning Services	441,795	415,182	6%	26,614	1,114,070	1,114,070
Waste Services	17,102,732	16,296,144	5%	806,589 ✓	32,012,872	31,994,738
Parks & Environmental Services	228,705	114,139	100%	114,566 ✓	179,187	6,760
Engineering Services	4,359	18,335	-76%	(13,976)	180,183	176,560
Infrastructure Services	73,978	218,277	-66%	(144,299) X	225,750	8,150
	76,871,792	75,607,733	2%	1,264,059	114,593,311	113,745,715
Less: Restricted Grants & Contributions b/fwd	(1,075,687)	(1,077,414)	0%	1,727	(1,077,414)	-
Add: Developer Contributions	14,606	12,475	17%	2,131	122,645	122,645
Total Operating Revenue	75,810,711	74,542,793	2%	1,267,918	113,638,542	113,868,360

Operating Expenditure

Governance	(230,882)	(297,908)	-22%	67,027	(3,682,502)	(3,456,151)
Financial Services	(1,155,090)	(1,227,808)	-6%	72,717	(4,471,879)	(4,471,879)
Information Services	(208,917)	(369,343)	-43%	160,426 ✓	(3,881,598)	(3,881,598)
Human Resource Management	(192,507)	(206,437)	-7%	13,930	(2,219,167)	(2,189,739)
Library Services	(199,203)	(267,478)	-26%	68,274	(2,831,632)	(2,783,692)
Community Services	(588,807)	(858,413)	-31%	269,606 ✓	(10,831,400)	(10,746,769)
Human Services	(500,856)	(745,923)	-33%	245,067 ✓	(7,518,330)	(7,350,808)
Development Services	(303,245)	(453,741)	-33%	150,496 ✓	(4,359,659)	(4,232,525)
Planning Services	(88,151)	(233,686)	-62%	145,535 ✓	(1,837,816)	(1,774,180)
Waste Services	(1,362,928)	(1,334,462)	2%	(28,466)	(17,902,061)	(17,902,061)
Parks & Environmental Services	(458,373)	(974,289)	-53%	515,917 ✓	(10,654,196)	(10,406,522)
Engineering Services	(405,325)	(633,643)	-36%	228,317 ✓	(7,557,494)	(7,553,872)
Infrastructure Services	(493,360)	(705,716)	-30%	212,356 ✓	(7,721,211)	(7,448,857)
	(6,187,644)	(8,308,846)	-26%	2,121,202	(85,468,945)	(84,198,652)

STATEMENT OF FINANCIAL ACTIVITY

for the period ended 31 July 2012

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
Less: Net Internal Recharging	299,230	259,665	15%	39,565	3,115,859	3,115,859
Add: Reverse Impairment Charge - Investments	-	-	0%	-	-	-
Add: Depreciation on Non-Current Assets						
Computer & Electronic Equip	(27,916)	(17,232)	62%	(10,684)	(206,784)	(206,784)
Furniture & Equipment	(14,785)	(15,098)	-2%	313	(181,143)	(181,143)
Plant & Machinery	(214,105)	(271,341)	-21%	57,236	(3,256,091)	(3,256,091)
Buildings	(238,186)	(282,169)	-16%	43,983	(3,386,022)	(3,386,022)
Roads	(699,304)	(875,000)	-20%	175,696	(10,500,000)	(10,500,000)
Drainage	(203,347)	(190,000)	7%	(13,347)	(2,280,000)	(2,280,000)
Footpaths	(69,911)	(73,106)	-4%	3,195	(877,274)	(877,274)
Parks Equipment	(121,722)	(123,334)	-1%	1,612	(1,480,000)	(1,480,000)
	(1,589,275)	(1,847,280)	-14%	258,005	(22,167,314)	(22,167,314)
Total Operating Expenditure	(7,477,690)	(9,896,461)	-24%	2,418,771	(104,520,400)	(103,250,107)
Change in Net Assets Resulting from Operations	68,333,021	64,646,332	6%	3,686,689	9,118,142	10,618,253
Non-Operating Activities						
Profit/(Loss) on Assets Disposal						
Plant & Machinery	100,920	22,000	359%	78,920	(100,364)	(315,364)
Freehold Land	1,245,000	170,645	630%	1,074,355	15,072,727	1,175,000
Furniture & Office Equipment	-	-	0%	-	-	-
Buildings	-	-	0%	-	15,000	15,000
	1,345,920	192,645	599%	1,153,275	14,987,363	874,636
Less: Underground Power Infrastructure Contribution	-	-	0%	-	(5,025,000)	(5,025,000)
Asset Acquisitions						
Land and Buildings	(60,297)	(3,550,414)	-98%	3,490,117	(43,924,604)	(35,818,923)
Infrastructure Assets	(512,929)	(1,904,688)	-73%	1,391,759	(25,633,675)	(17,259,411)
Plant and Machinery	-	(495,000)	-100%	495,000	(5,605,491)	(3,627,000)
Furniture and Equipment	-	-	0%	-	(40,000)	(40,000)
Computer Equipment	(100,000)	(366,504)	-73%	266,504	(3,014,150)	(1,167,500)
	(673,226)	(6,316,606)	-89%	5,643,380	(78,217,920)	(57,912,834)
Note 1.						
Add: Transfer to Reserves	(623,879)	(447,099)	40%	(176,780)	(45,276,959)	(31,392,984)
	68,381,836	58,075,272	18%	10,306,564	(104,414,373)	(82,837,928)

STATEMENT OF FINANCIAL ACTIVITY

for the period ended 31 July 2012

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
Add Funding from						
Grants & Contributions - Asset Development	197,014	1,479,840	-87%	(1,282,826) X	12,780,409	10,936,929
Less: held in restricted funds from prior years	(143,792)	(95,975)	50%	(47,817)	(296,660)	(219,500)
Proceeds on Sale of Assets	1,345,920	192,645	599%	1,153,275 ✓	21,218,727	7,106,000
Reserves	-	4,507,569	-100%	(4,507,569) X	55,062,608	38,638,204
Loan Funds Raised	-	250,000	-100%	(250,000) X	4,865,000	4,865,000
Contributed Developer Assets	-	-	0%	-	-	-
	69,780,978	64,409,352	8%	5,371,626	(10,784,289)	(21,511,296)
Less: Transfer from Reserves - Impaired Investments	-	-	0%	-	-	-
Non-Cash/Non-Current Item Adjustments						
Depreciation on Assets	1,589,275	1,847,280	-14%	(258,005) X	22,167,314	22,167,314
Profit/(Loss) on Assets Disposal	(1,345,920)	(192,645)	599%	(1,153,275) X	(14,987,363)	(874,636)
Non-Current Accrued Debtors	-	-	0%	-	-	-
Non-Current Leave Provisions	25,591	-	0%	25,591	-	-
Net Change in Restricted/Committed Cash	1,219,479	1,173,389	4%	46,090	1,374,074	219,500
Deferred Pensioners Adjustment	-	-	0%	-	-	-
	71,269,403	67,237,376	6%	4,032,027	(2,230,264.34)	882
Opening Funds	6,632,766	2,259,380	194%	4,373,387	2,259,380	-
Closing Funds	77,902,170	69,496,756	12%	8,405,414	29,115	882
	-	-	-	-	-	-

Note 2, 3.

Notes to Statement of Financial Activity

Note 1.

Additional information on the capital works program including committed orders at end of month:

Assets Classification	Actuals	Commitments at Month End	Commitments & Actuals YTD	YTD Revised Budget	Full Year Revised Budget	Uncommitted at Month End
	\$	\$			\$	\$
Land and Buildings	(60,297)	(35,700)	(95,997)	(3,550,414)	(43,924,604)	43,828,607
Infrastructure Assets	(512,929)	(467,295)	(980,224)	(1,904,688)	(25,633,675)	24,653,451
Plant and Machinery	-	(143,572)	(143,572)	(495,000)	(5,605,491)	5,461,919
Furniture and Equipment	-	-	-	-	(40,000)	40,000
Computer Equipment	(100,000)	(356,464)	(456,464)	(366,504)	(3,014,150)	2,557,686
	(673,226)	(1,003,031)	(1,676,257)	(6,316,606)	(78,217,920)	76,541,663

Note 2.

Closing Funds in the Financial Activity Statement are represented by:

	Actuals	YTD Revised Budget	Full Year Revised Budget	Adopted Budget
	\$	\$	\$	\$
Current Assets				
Cash & Investments	68,066,379	129,322,472	53,973,827	56,957,676
Rates Outstanding	58,000,706	-	-	-
Rubbish Charges Outstanding	15,303,528	-	-	-
Sundry Debtors	17,366,133	-	-	-
GST Receivable	242,836	-	-	-
Prepayments	310,033	-	-	-
Accrued Debtors	323,711	-	-	-
Stock on Hand	14,190	-	-	-
	159,627,516	129,322,472	53,973,827	56,957,676
Current Liabilities				
Creditors	(14,948,958)	-	-	-
Income Received in Advance	(736,999)	-	-	-
GST Payable	(367,428)	-	-	-
Withholding Tax Payable	-	-	-	-
Provision for Annual Leave	(2,487,889)	-	-	-
Provision for Long Service Leave	(1,750,150)	-	-	-
	(20,291,424)	-	-	-
Net Current Assets	139,336,092	129,322,472	53,973,827	56,957,676
Add: Non Current Investments	7,956,474	-	-	-
	147,292,566	129,322,472	53,973,827	56,957,676
Less: Restricted/Committed Assets				
Cash Backed Reserves #	(62,474,382)	(55,499,105)	(49,818,786)	(51,676,294)
Deposits & Bonds Liability *	(2,848,740)	-	-	-
Grants & Contributions Unspent *	(4,067,275)	(4,326,611)	(4,125,926)	(5,280,500)
	77,902,170	69,496,756	29,115	882
Closing Funds (as per Financial Activity Statement)	77,902,170	69,496,756	29,115	882

See attached Reserve Fund Statement

* See attached Restricted Funds Analysis

Note 3.

Amendments to original budget since budget adoption. Surplus/(Deficit)

Ledger	Project/ Activity	Description	Council Resolution	Classification	Non Change - (Non Cash Items) Adjust.	Increase in Available Cash	Decrease in Available Cash	Amended budget Running Balance
					\$	\$	\$	\$
		Budget Adoption		Closing Funds Surplus(Deficit)				882
OP	9144	Lease recovery from 13 Kent St property		Operating Income		3,000		3,882
OP	9165	Community Youth Bus is no longer required, majority of fund was sent to various Disability & Inclusion OP's		Operating Expenditure		1,481		5,363
OP	8138	Environmental OP project was loaded incorrectly		Operating Expenditure		10,000		15,363
GL	725	Reduction in Reserve transfer for Naval Base lease revenue		Operating Income		192,765		208,128
OP	9101	Transferring Coogee Caravan Park lease revenue to Reserve		Operating Income			179,013	29,115
				Closing Funds Surplus (Deficit)	<u>0</u>	<u>207,247</u>	<u>179,014</u>	<u>29,115</u>

Statement of Comprehensive Income by Nature and Type

for the period ended 31 July 2012

	Actual	Amended YTD Budget	\$ Variance to YTD Budget	Forecast	Amended Budget	Adopted Budget
	\$	\$	\$	\$	\$	\$
OPERATING REVENUE						
01 Rates	52,775,636	53,032,648	(257,011)	53,971,989	54,229,000	54,229,000
05 Fees and Charges	21,787,910	20,931,192	856,719	46,033,353	45,176,634	45,176,634
10 Grants and Subsidies	776,833	203,880	572,953	8,690,136	8,117,182	8,456,698
15 Contributions, Donations and Reimbursements	59,934	46,499	13,435	517,999	504,564	431,710
20 Interest Earnings	376,027	293,402	82,626	5,643,968	5,561,342	5,561,342
25 Other revenue and Income	34,370	35,174	(803)	49,016	49,819	12,976
Total Operating Revenue	75,810,711	74,542,793	1,267,918	114,906,460	113,638,542	113,868,360
OPERATING EXPENDITURE						
50 Employee Costs - Salaries & Direct Oncosts	(2,854,515)	(3,129,281)	274,766	(37,545,153)	(37,819,919)	(37,798,025)
51 Employee Costs - Indirect Oncosts	(10,770)	(66,016)	55,246	(867,945)	(923,192)	(898,818)
55 Materials and Contracts	(1,289,658)	(3,159,150)	1,869,492	(30,489,263)	(32,358,755)	(31,355,338)
65 Utilities	(245,643)	(366,879)	121,235	(4,363,715)	(4,484,950)	(4,484,950)
70 Interest Expenses	-	-	-	-	-	-
75 Insurances	(985,995)	(1,023,207)	37,212	(1,829,488)	(1,866,700)	(1,866,700)
80 Other Expenses	(801,064)	(564,314)	(236,750)	(8,252,179)	(8,015,429)	(7,794,821)
85 Depreciation on Non Current Assets	(1,589,275)	(1,847,280)	258,005	(21,909,309)	(22,167,314)	(22,167,314)
96 Internal Recharging (Unbalanced)	(578)	607	(1,185)	5,958	7,143	7,143
Add Back: Indirect Costs Allocated to Capital Works	299,808	259,058	40,749	3,149,466	3,108,716	3,108,716
Total Operating Expenditure	(7,477,690)	(9,896,461)	2,418,771	(102,101,629)	(104,520,400)	(103,250,107)
CHANGE IN NET ASSETS RESULTING FROM OPERATING ACTIVITIES						
	68,333,021	64,646,332	3,686,689	12,804,831	9,118,142	10,618,253
NON-OPERATING ACTIVITIES						
11 Capital Grants & Subsidies	(248,440)	1,265,594	(1,514,035)	7,169,438	8,683,473	6,939,454
16 Contributions - Asset Development	445,454	214,246	231,208	4,328,145	4,096,936	3,997,475
95 Profit/(Loss) on Sale of Assets	1,345,920	192,645	1,153,275	16,140,638	14,987,363	874,636
57 Acquisition of Crown Land for Roads	-	-	-	-	-	-
58 Underground Power Scheme	-	-	-	-	(5,025,000)	(5,025,000)
Total Non-Operating Activities	1,542,934	1,672,485	(129,551)	27,638,221	22,742,773	6,786,565
NET RESULT	69,875,955	66,318,817	3,557,138	40,443,053	31,860,915	17,404,818

Notes to Statement of Comprehensive Income

Note 1.

Additional information on main sources of revenue in fees & charges.

	Actual	Amended YTD Budget	Amended Budget	Adopted Budget
	\$	\$	\$	\$
<u>Community Services:</u>				
Recreational Services	22,530	34,717	532,245	532,245
South Lake Leisure Centre	148,437	179,987	2,765,549	2,765,549
Law and Public Safety	2,251,711	2,241,740	2,570,500	2,570,500
	2,422,678	2,456,444	5,868,294	5,868,294
<u>Waste Services:</u>				
Waste Collection Services	15,524,122	15,288,000	15,288,000	15,288,000
Waste Disposal Services	1,578,174	1,004,991	16,668,909	16,668,909
	17,102,296	16,292,991	31,956,909	31,956,909
	19,524,974	18,749,435	37,825,203	37,825,203

Note 2.

Additional information on Salaries and Direct On-Costs by each Division.

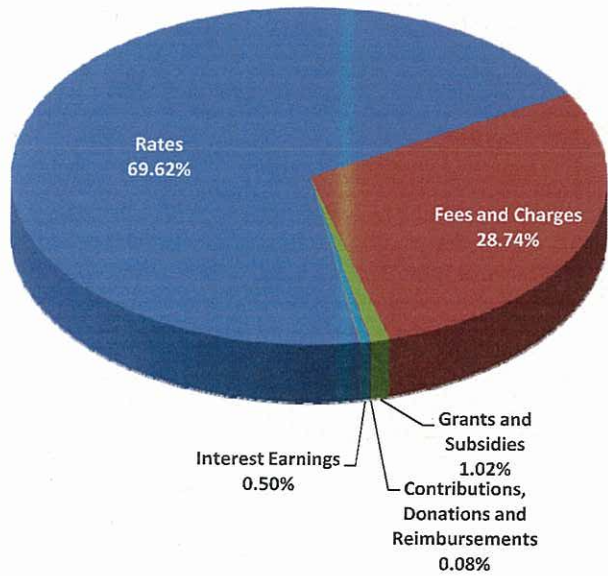
	Actual	Amended YTD Budget	Amended Budget	Adopted Budget
	\$	\$	\$	\$
Executive Division	(145,117)	(139,391)	(1,715,193)	(1,715,193)
Finance & Corporate Services Division	(490,783)	(461,233)	(5,734,260)	(5,729,205)
Community Services Division	(904,750)	(972,981)	(11,570,336)	(11,553,496)
Planning & Development Division	(350,685)	(351,693)	(4,315,600)	(4,315,600)
Engineering & Works Division	(963,181)	(1,203,984)	(14,484,530)	(14,484,530)
	(2,854,515)	(3,129,281)	(37,819,919)	(37,798,025)

Note 3

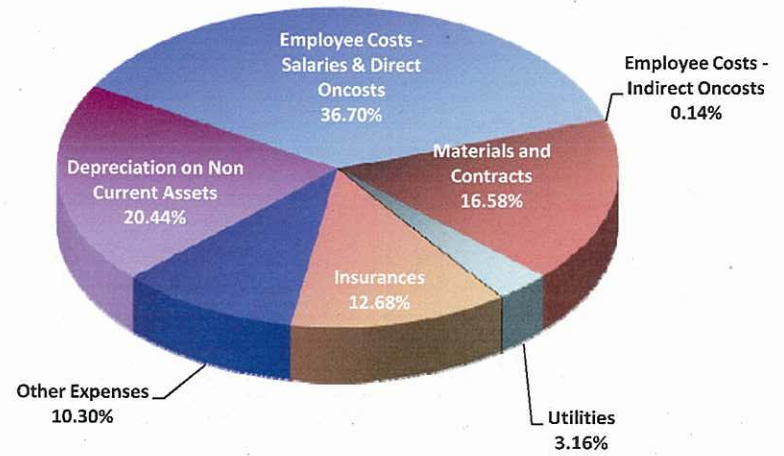
Additional information on Materials and Contracts by each Division.

	Actual	Amended YTD Budget	Amended Budget	Adopted Budget
	\$	\$	\$	\$
Executive Division	(81,218)	(120,841)	(1,321,935)	(1,316,192)
Finance & Corporate Services Division	(32,660)	(269,254)	(2,588,541)	(2,588,541)
Community Services Division	(313,754)	(794,886)	(7,386,927)	(7,103,674)
Planning & Development Division	(40,711)	(332,221)	(1,769,026)	(1,578,255)
Engineering & Works Division	(821,315)	(1,641,948)	(19,292,327)	(18,768,676)
Not Applicable	0	0	0	0
	(1,289,658)	(3,159,150)	(32,358,755)	(31,355,338)

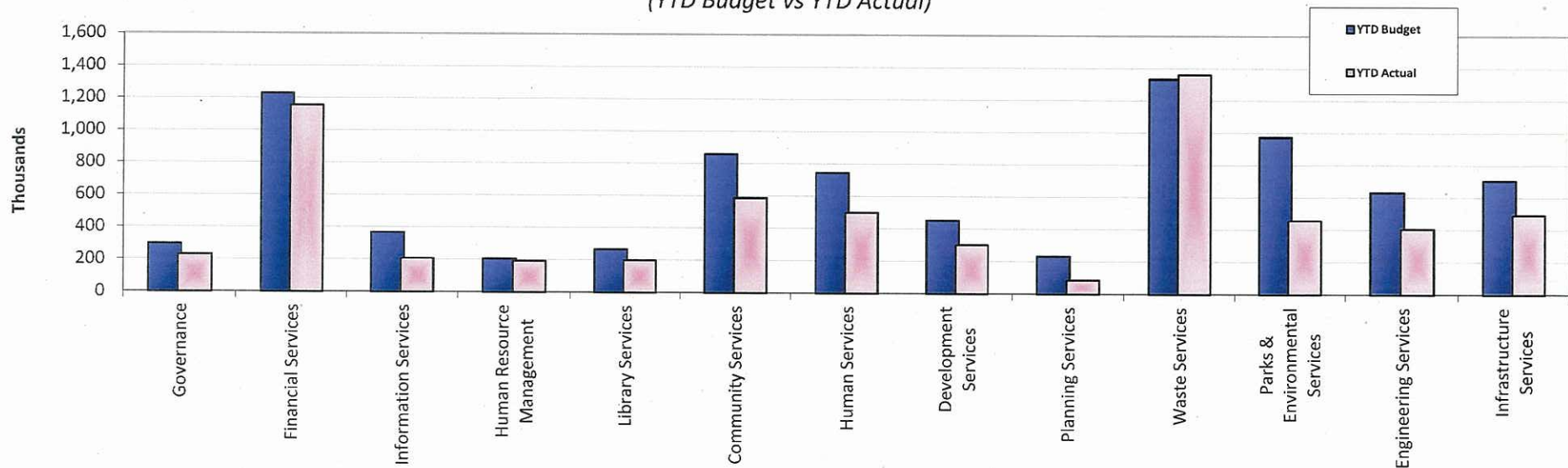
Operating Income by Nature and Type
(YTD Actual)



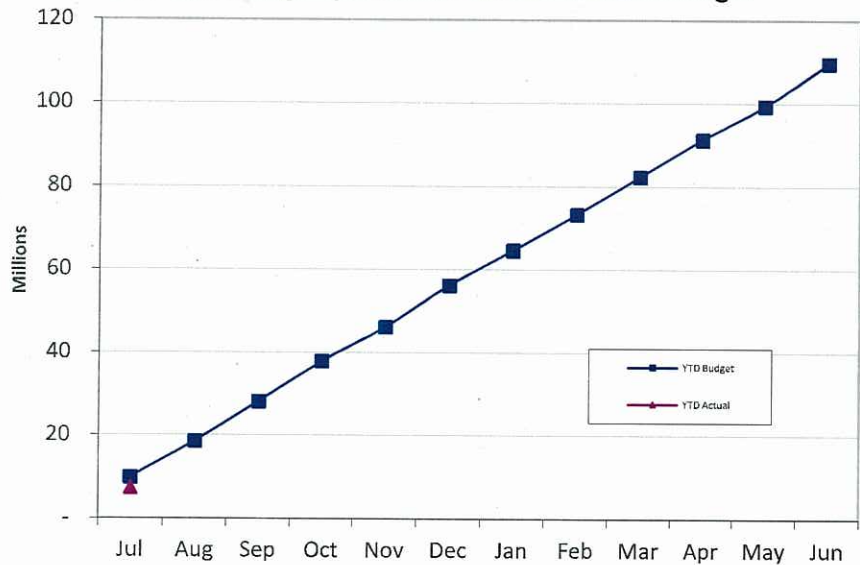
Operating Expenditure by Nature and Type
(YTD Actual)



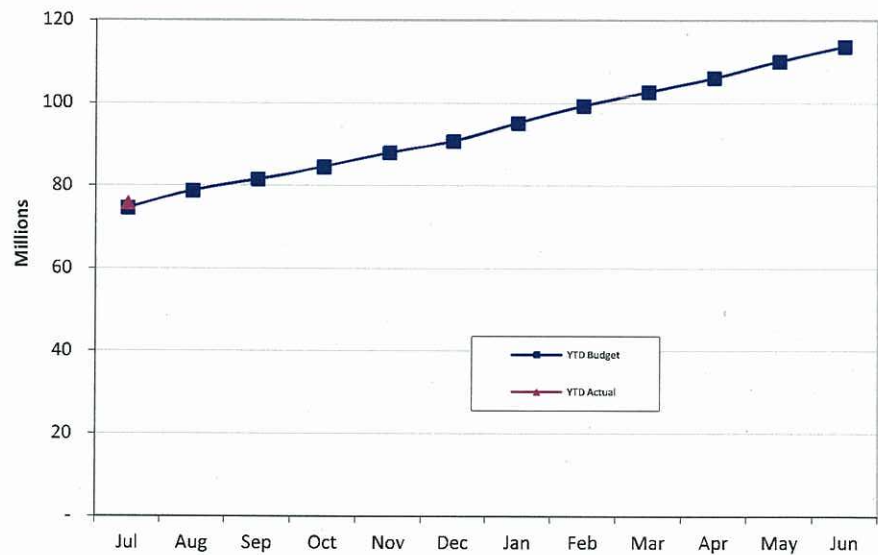
Operating Expenditure by Business Unit
(YTD Budget vs YTD Actual)



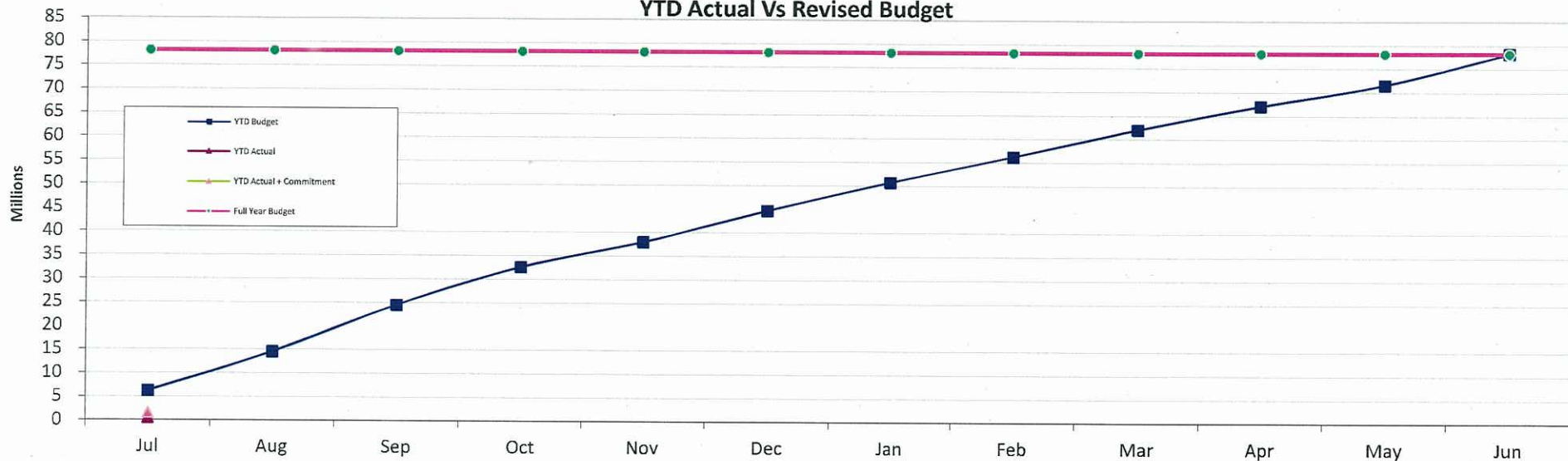
YTD Operating Expenditure Vs YTD Revised Budget



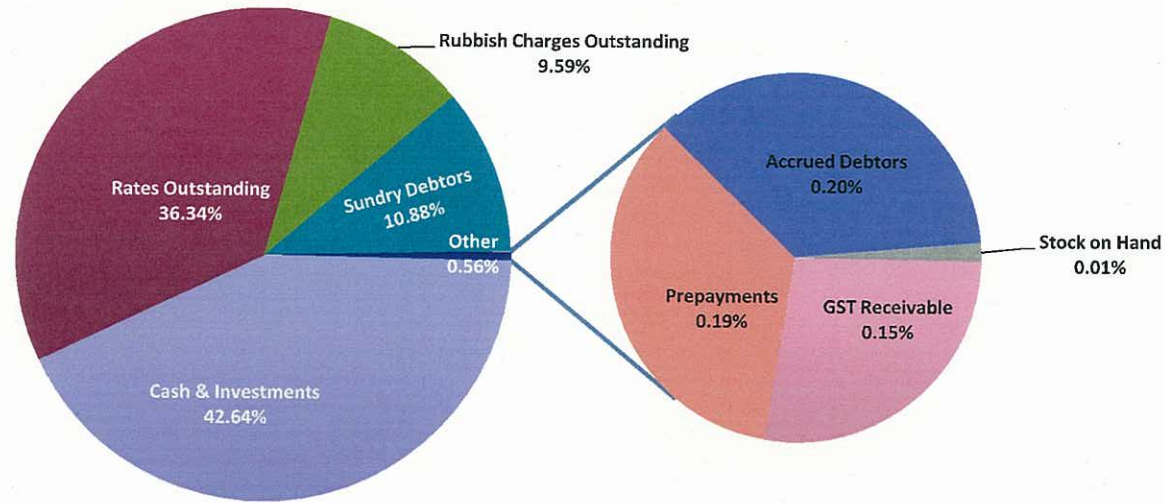
YTD Operating Income Vs YTD Revised Budget



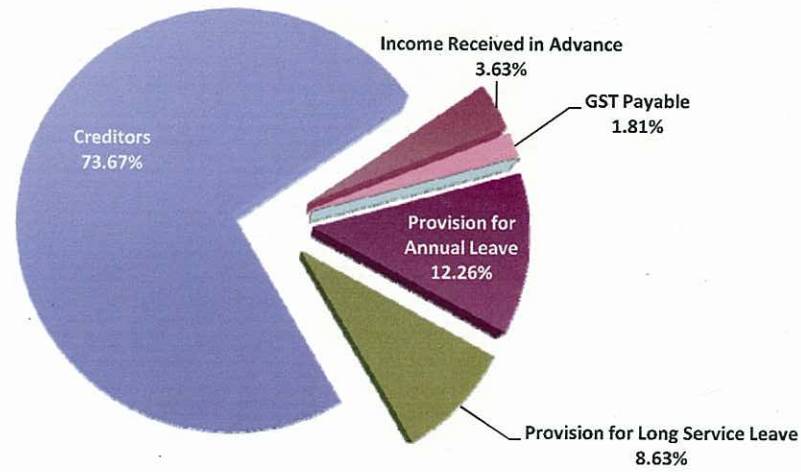
**Capital Expenditure
YTD Actual Vs Revised Budget**



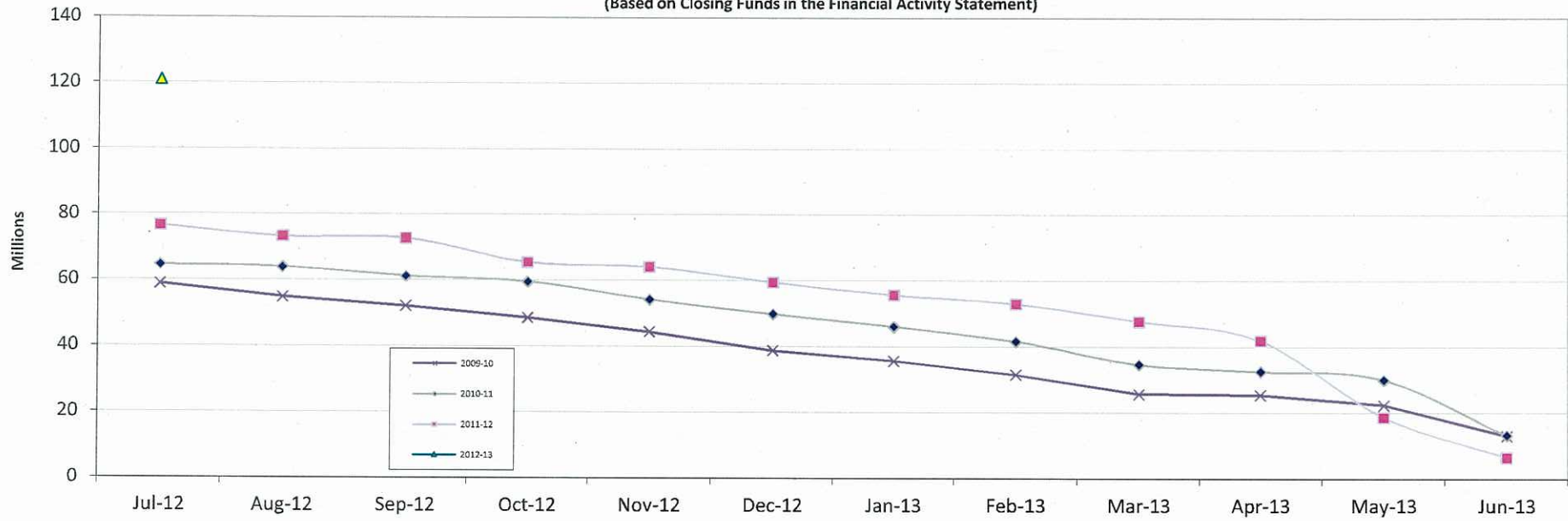
Current Assets (YTD Actual)



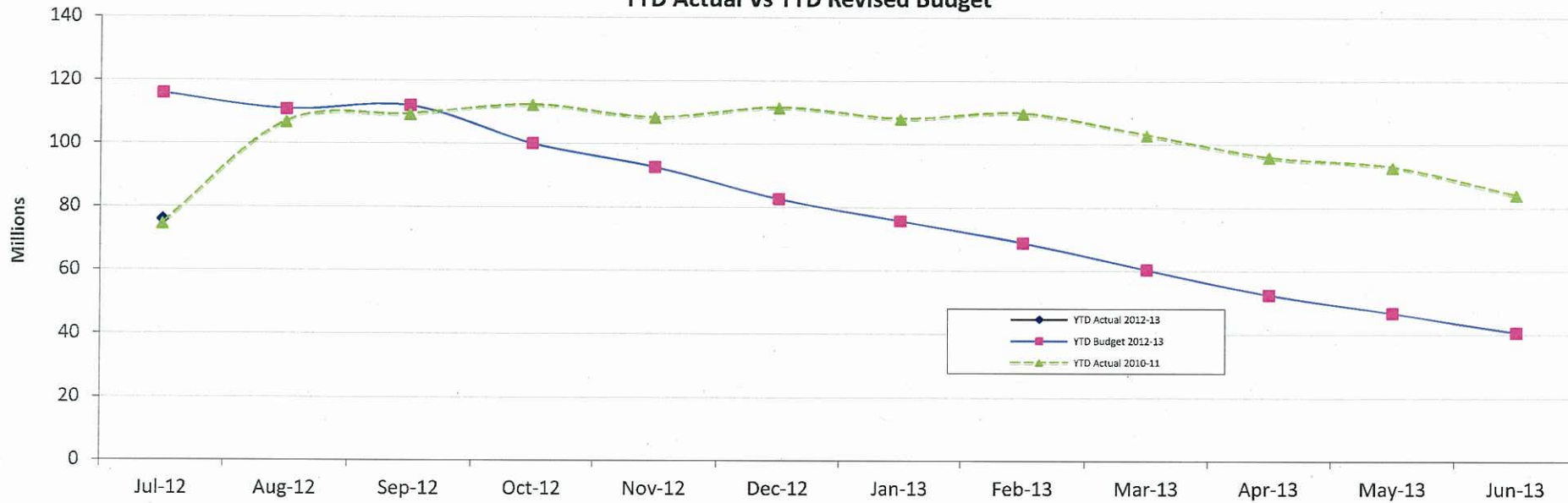
Current Liabilities (YTD Actual)



Municipal Liquidity Over the Year
 (Based on Closing Funds in the Financial Activity Statement)



Cash & Investments Positions
 YTD Actual Vs YTD Revised Budget



Mr. Stephen Cain
CEO, City of Cockburn
P.O. Box 1215
Bibra Lake DC
Western Australia 6965

Dear Sir,

I am writing to you regarding the rates for my properties at 316 Rockingham Road, Spearwood and 17 Winterfold road Hamilton Hill. I have spoken with the rates department regarding these properties being rated as commercial and not residential, they advise me I must write to you to have the classification changed.

Both of the above properties are residential lodging houses. They provide long term (average stay of resident is in the years) residential accommodation at low cost. I provide accommodation to people whom might otherwise find it very difficult to secure affordable housing.

I am a private operator and not subsidised in any way by government. Most private operators are leaving this area of accommodation due to rising costs and government compliance issues. I wish to keep providing affordable housing however, costs such as electricity, gas, water rates and council rates are rising well above the inflation rate and more importantly the incomes of my residents.

To make matters worse the rates department have determined that both of the above lodging houses are commercial operations and are subsequently rated higher than other residential properties. Being a private concern and unsubsidised I must pass costs associated with running the lodging houses on to the residents. I am uncertain why residents of a lodging house pay higher rates than other residents of the City of Cockburn?

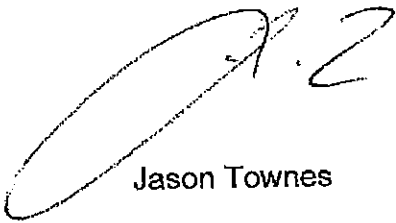
Points you might like to consider are

1. A residential lodging house is no more a commercial concern than other private rental accommodation
2. We do not charge GST for our accommodation and are not considered "commercial" by the ATO.
3. Western Power initially was charging me commercial rates for power but accepted my argument that we are charged residential rates not commercial.
4. Why should the residents of a lodging house pay commercial council rates whilst other residents of Cockburn pay residential rates?

Questions

1. How does the City of Cockburn determine a properties commercial or residential status?
2. What specific law and or policy determine the property status?
3. Who makes the determination of a property status?
4. What are my avenues of appeal if you determine I must pay commercial rates?

Regards

A handwritten signature in black ink, appearing to read 'J. Townes', with a large, sweeping loop on the left side.

Jason Townes

161 Solomon Street

Beaconsfield

WA 6162

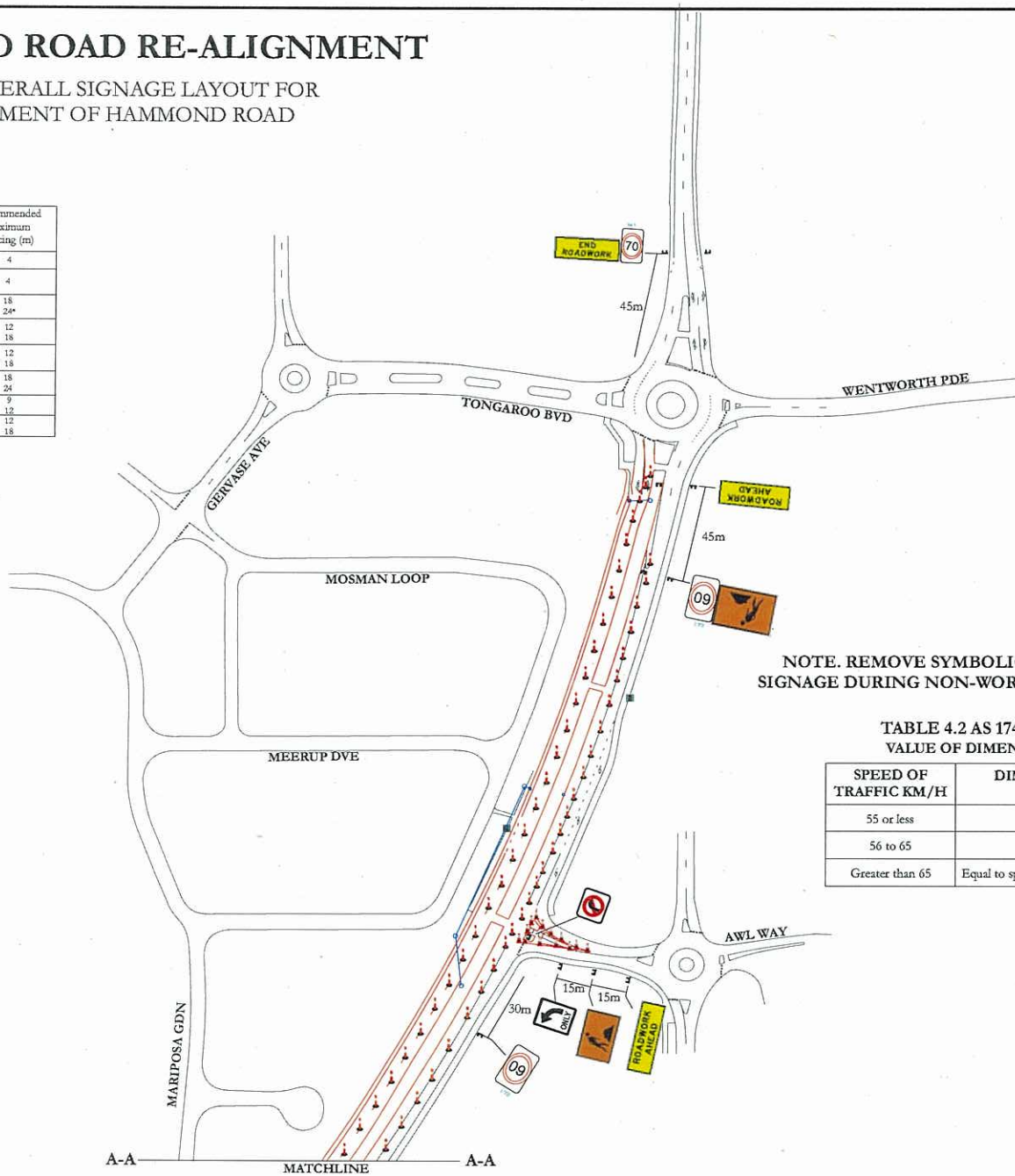
HAMMOND ROAD RE-ALIGNMENT

STAGE 3: OVERALL SIGNAGE LAYOUT FOR RE-ALIGNMENT OF HAMMOND ROAD

Table 3.7 AS 1742.3-2009
CONE SPACINGS

Purpose and Usage	Traffic Speed, km/h	Recommended Maximum Spacing (m)
All purposes	<50	4
Centre-line on approach to a traffic controller position	All cases	4
Outer edge of traffic lanes - e.g. Work on shoulder or parking lane	51 to 70 >70	18 24*
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* Spacing may be extended to 60m where the length of delineation exceeds 1 km.



NOTE. REMOVE SYMBOLIC WORKER SIGNAGE DURING NON-WORKING HOURS

TABLE 4.2 AS 1742.3-2009
VALUE OF DIMENSION D

SPEED OF TRAFFIC KM/H	DIMENSION D m
55 or less	15
56 to 65	45
Greater than 65	Equal to speed of traffic, in km/h

NOTE. DRAWING CONTINUES ON TCD 5949-03-01



NOTES

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- Spacing of delineation devices.
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LEGEND

- WORK AREA
- SIGN LOCATION
- BUS STOP

POSTED SPEED LIMIT	WORK SITE SPEED LIMIT
70 km/h	60 km/h

LOCATION HAMMOND ROAD

CLIENT CITY OF COCKBURN

CONTACT DAVE HALL 0417 939 952

DATE 06/09/2012 SCALE NOT TO SCALE

DRAWN BY PAUL ASHPOLD AS/PTM K34430 SIGNED

DESIGNED BY PAUL ASHPOLD AS/PTM K34430 SIGNED

APPROVED BY PAUL ASHPOLD AS/PTM K34430 SIGNED

TITLE

HAMMOND ROAD RE-ALIGNMENT

STAGE 3: SIGNAGE LAYOUT FOR RE-ALIGNMENT OF HAMMOND ROAD

DRAWING NUMBER	REVISION	SIZE
5949-03-01a	0	A1

Carringtons Traffic Services
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OCM 13/9/2012 Item 16.2 Attach 1

HAMMOND ROAD RE-ALIGNMENT

STAGE 2: OVERALL SIGNAGE LAYOUT FOR FERTITO ENTRANCE ROAD CLOSURE

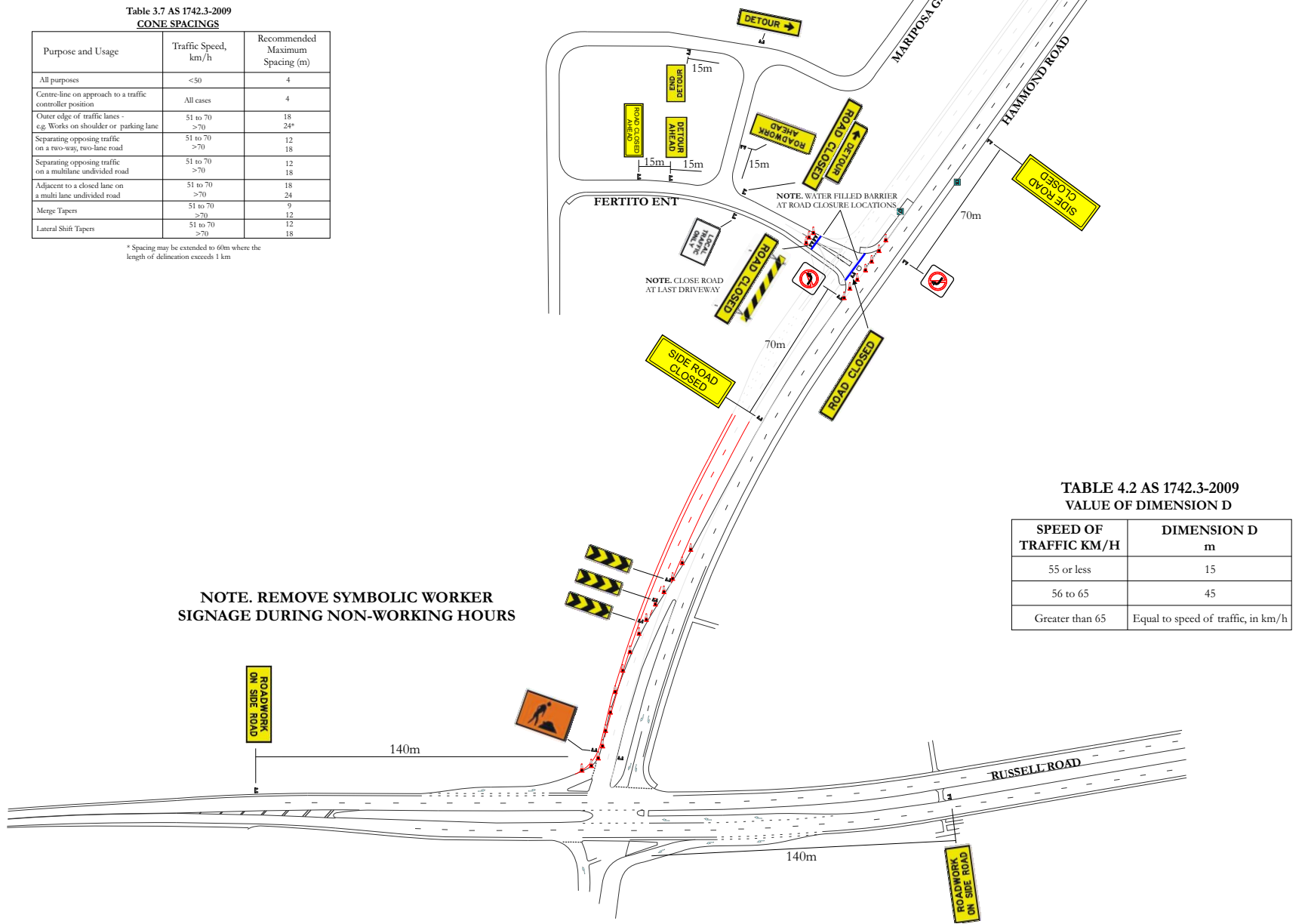
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NOTE. DRAWING CONTINUES ON TCD 5949-02-01a

A-A MATCHLINE A-A



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LEGEND

- WORK AREA
- SIGN LOCATION
- BUS STOP

POSTED SPEED LIMIT	WORK SITE SPEED LIMIT
70 km/h	70 km/h

LOCATION HAMMOND ROAD

CLIENT CITY OF COCKBURN

CONTACT DAVE HALL 0417 939 952

DATE 06/09/2012

SCALE NOT TO SCALE

DRAWN BY PAUL ASHFOLD AWTM E34450

DESIGNED BY PAUL ASHFOLD AWTM E34450

APPROVED BY PAUL ASHFOLD AWTM E34450

TITLE

HAMMOND ROAD RE-ALIGNMENT
STAGE 2: SIGNAGE LAYOUT FOR FERTITO ENTRANCE ROAD CLOSURE

DRAWING NUMBER	REVISION	SIZE
5949-02-01	0	A3

Carringtons Traffic Services
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www.carringtonswa.com

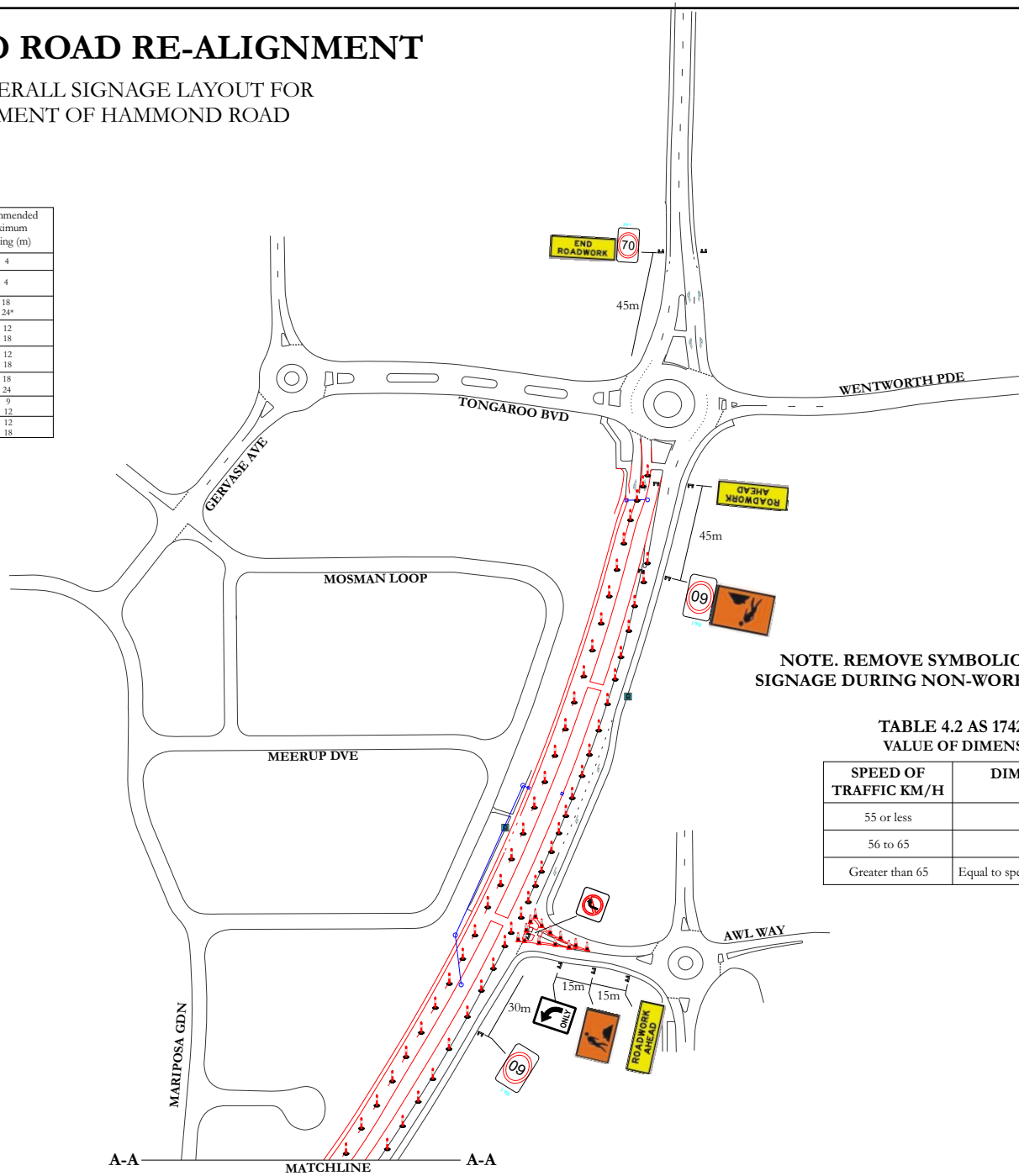
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LEGEND

- WORK AREA
- SIGN LOCATION
- BUS STOP

POSTED SPEED LIMIT	WORK SITE SPEED LIMIT
70 km/h	60 km/h

LOCATION HAMMOND ROAD

CLIENT CITY OF COCKBURN

CONTACT DAVE HALL 0417 939 952

DATE 06/09/2012 SCALE NOT TO SCALE

DRAWN BY PAUL ASHFOLD AWTM E34450 SIGNED

DESIGNED BY PAUL ASHFOLD AWTM E34450 SIGNED

APPROVED BY PAUL ASHFOLD AWTM E34450 SIGNED

TITLE
HAMMOND ROAD RE-ALIGNMENT
STAGE 3: SIGNAGE LAYOUT FOR RE-ALIGNMENT OF HAMMOND ROAD

DRAWING NUMBER	REVISION	SIZE
5949-03-01a	0	A3

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Fax: (08) 9356 7751
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NOTE. DRAWING CONTINUES ON TCD 5949-03-01

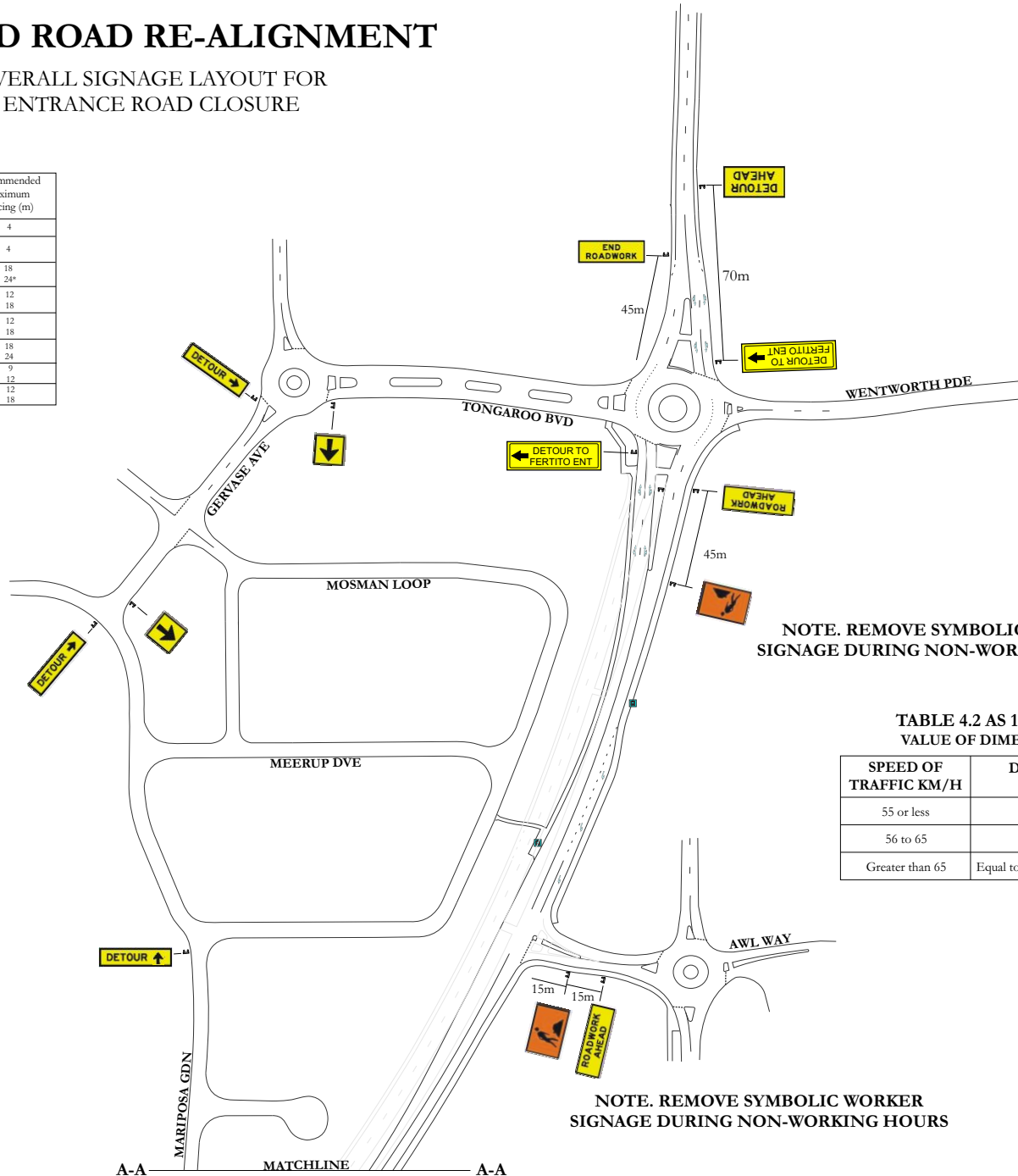
HAMMOND ROAD RE-ALIGNMENT

STAGE 2: OVERALL SIGNAGE LAYOUT FOR FERTITO ENTRANCE ROAD CLOSURE

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LEGEND

- WORK AREA
- SIGN LOCATION
- BUS STOP

POSTED SPEED LIMIT	WORK SITE SPEED LIMIT
70 km/h	70 km/h

LOCATION HAMMOND ROAD

CLIENT CITY OF COCKBURN

CONTACT DAVE HALL 0417 939 952

DATE 06/09/2012 SCALE NOT TO SCALE

DRAWN BY PAUL ASHFOLD AWTM E34450 SIGNED

DESIGNED BY PAUL ASHFOLD AWTM E34450 SIGNED

APPROVED BY PAUL ASHFOLD AWTM E34450 SIGNED

TITLE
HAMMOND ROAD RE-ALIGNMENT
STAGE 2: SIGNAGE LAYOUT FOR FERTITO ENTRANCE ROAD CLOSURE

DRAWING NUMBER 5949-02-01a REVISION 0 SIZE A3



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35 Kimmer Place, Queens Park
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Communication's Strategy and Action Plan 2012 – 2017

DRAFT

Corporate Communications



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Executive Summary

In 2006 the City developed a five year Communication Strategy. This has run its course with the outcomes detailed in Appendix 1.

This updated strategy will:

- address the best and most sustainable ways for the City to communicate the breadth of facilities, events and services the organization provides to their diverse Community;
- provide a framework for communicating the City's current and future strategic plan objectives and inform stakeholders on areas which are of concern to them.
- consider ways to increase two-way dialogue, and improve consultation and engagement between the City and its key stakeholders (see Appendix 2) to improve the perceptions of transparency, honesty and trust.
- build on the strengths of existing and emerging communication channels.

This strategy has been developed using results of:

1. The annual Catalyse Community and Business Perceptions Survey 2012;
2. Results from the annual Customer Satisfaction KPI Survey 2011; and,
3. Results from two community focus groups on City Communications undertaken by Catalyse, August 2011 (see Appendix 3).

This plan will cover the period July 2012 to June 2017 and will be reviewed each year.

In the City's 2012 Community Perceptions Survey undertaken by Catalyse, the City received an overall satisfaction rate of 86% with satisfaction climbing in the past five years to reach and pass 2005-6 levels.

- ✓ 68% of those surveyed said that they were satisfied with how the community is informed about local issues (34% were delighted). 92% were familiar with information provided by the City and it was considered a matter of priority for 4% of those surveyed.
- ✓ 53% of respondents agreed that the City has developed and communicated a clear vision for the area (what the area will look like in 10 years), ahead of the industry average of 37%. 27% disagreed. Older and newer residents are more likely to agree with this statement.
- ✓ 88% of those surveyed were familiar with Cockburn Soundings. 81% were satisfied and 46% delighted with it.
- ✓ 55% were aware of the City's website. While only 1% considered it a priority, 81% were satisfied and 49% delighted. This 49% is the industry high.
- ✓ 78% were satisfied with the efficiency and effectiveness of customer service and 47% delighted.

In the annual KPI Customer Satisfaction Research undertaken by Research Solutions in 2011 overall customer satisfaction with Customer Services was 96% (rating 6-10) with 68% of customers truly satisfied (rating 9-10).

Communication Focus Groups (Community) September 2011 (Catalyse) (See Appendix 3)

Catalyse concluded that most stakeholders believe that the City is doing an 'okay' job at communicating with residents. To improve, the City's communications strategy should:

1. Incorporate a more human touch.
2. Tackle 'hard' issues.
3. Add a personal and targeted approach (by suburb or local neighbourhood, but not by Ward).
4. Engage residents in genuine, two-way communications.

Specific discussion focused on key corporate communications tools and as a result actions have been undertaken (See Appendix 3). The broader advice has been taken into consideration when developing this Communications Strategy.

DRAFT

Evaluation

Evaluation of this plan will be through the:

1. Annual Community and Business Perceptions Survey;
2. Annual KPI Customer Satisfaction Surveys;
3. Internal Communications Survey (bi-ennial);and,
4. Analysis of media messages.
5. Feedback from Regional Seniors Group, Disability Reference Group, Childrens' Reference Group, Aboriginal Reference Group

Table 1

Measure	Satisfaction 2011	Satisfaction 2012	KPI 2013	KPI 2014	KPI2015-17
Community satisfaction with how the City informs the community;	64% satisfied 17% dissatisfied 24% delighted	68% satisfied 16% dissatisfied 34% delighted	70% satisfied 16% dissatisfied 35% delighted	72% satisfied 15% dissatisfied 36% delighted	To be reviewed
Whether the City has communicated a clear vision for the future	46% satisfied 30% disagreed	53% satisfied 27% disagreed	55% satisfied 25% disagreed	57% satisfied 23% disagreed	
Community satisfaction with community consultation	53% satisfied 24% dissatisfied 17% Truly satisfied	59% satisfied 24% dissatisfied 23% Truly satisfied	65% satisfied 20% dissatisfied 25% Truly satisfied	70% satisfied 12% dissatisfied 28% truly satisfied Community Engagement Framework implemented	
Community satisfaction with customer service	Perceptions: 72% satisfied KPI Target 96% satisfied 68% truly satisfied	Perceptions: 78% satisfied KPI Target 96% satisfied 70% truly satisfied	Perceptions: 80% satisfied KPI Target 96% satisfied 70% truly satisfied	Perceptions: 81% satisfied KPI Target 96% satisfied 70% truly satisfied	
Community satisfaction with	Perceptions: 80% satisfied	Perceptions: 81% satisfied	Perceptions: 83% satisfied	Perceptions: 84% satisfied	

Cockburn Soundings	36% delighted	46% delighted	47% delighted	47% delighted	To be reviewed
Community satisfaction with the local government authority	79%	81%	82%	83%	
Council's Leadership within the community	59% satisfied 17% dissatisfied	67% satisfied 15% dissatisfied	68% satisfied 14% dissatisfied		
Council's Website	77% satisfied 36% delighted	81% satisfied 49% delighted	82% satisfied 50% delighted	83% satisfied 50% delighted	
Annual Calendar Did residents keep or display it	N/A	79%	80%	82%	
Analysis of media messages	In development				
Awareness of City's events	87%	91%	92%	92%	
Internal Communications Survey	In development				

Other specific measures as detailed in the strategy can be included in this table at the appropriate time.

The City's Vision

It is council's continuing intention to ensure that the Cockburn of the future will be the most attractive place to live, work, visit and invest in, within the Perth metropolitan area.

The City's Mission

To make the City of Cockburn the most attractive place to live, work, visit and invest in, within the Perth metropolitan area.

Strategic Alignment

1. The City's Corporate Strategic Plan 2006-2016 identifies seven vision statements and objectives for achieving the mission. Under the initiative outcome Governance Excellence the City commits to a well informed community.
2. The Plan for the District is the City's needs based infrastructure plan, designed to deliver major capital works in a timely manner, while maintaining the financial sustainability of the City.
3. The City's Annual Business Plan provides the operational link between the Strategic Plan and its Plan for the District
4. The City's Access and Equity Position Statement PSCS1 whereby the City aims to maintain and improve the quality of life of its residents by creating an accessible and inclusive community in which information 'is open and available to all residents'.
5. The City's Disability Action and Inclusion Plan 2012-2017 makes the following commitment: 'All information provided by the City of Cockburn shall be available in alternative formats upon request.'
6. The City's Reconciliation Action Plan 2011-2013 commits to 'promote good news stories about Aboriginal and Torres Strait Islander people via a range of avenues.'
7. The City's Sustainability Strategy – 2012-2016 states: 'Society: promote Cockburn as an attractive place to live, work and visit.'
8. The City's Community Development Strategic Plan – 2011-2014 has a vision, 'to support and contribute to a community in which people can feel a sense of belonging, connection and contribution, a community that celebrates diversity, embraces difference and supports the most vulnerable, a community in which people feel able to facilitate positive change in their lives and the lives of others.'
9. The City's Customer Service Charter(s) (updated 2012) gives clear guidelines for staff on how to deliver the best possible customer service internally and externally.
10. The City's Corporate Governance Charter commits to the City having 'effective communication policies and practices, internally and externally, and will be open to, and encourage, feedback from all Stakeholders.'
11. The Information Services Strategic Plan 2010-2015 mission is, 'To provide business value through reliable, innovative and customer-focused technology solutions that manage the City's corporate knowledge, encourage effective communication and assist technology users to achieve their goals.'

The following policies are aligned with this strategy:

1. Policy SC6 – Communication and Information Dissemination.
2. Policy SC4 – Establishment of Issue Based Committees
3. Policy SC2 – Strategic Consultation with Community Stakeholders.
4. Policy SCS1 – Media Activity

Communication Issues

1. Targeted marketing at specific sectors is a costly use of ratepayers money, therefore general communication needs to be written to be read by all target markets while acknowledging that specific target groups require different methods and strategies to ensure accessibility.
2. The human resource required to communicate ongoing investment and progress on the significant number of new projects, complex issues and existing services, facilities and events.
3. Brand embodies the vision and values of a product, organization or service is the perception that customers have of the organization. The City's corporate branding across some its most popular services such as libraries is inconsistent.
4. Multicultural – the 2006 census reported that 22% of Cockburn residents did not speak English as a first language at home.
5. Results from exit interviews and staff surveys consistently say that staff do not know enough about the City's vision and mission.
6. 24% of the community does not feel that they are being consulted with adequately.
7. The increasing population.
8. Ensuring a consistent message across the City business units.
9. Information Technology - the pace at which new technology develops.
10. The reputation of government leaders.
11. Existing communication channels receive high levels of satisfaction with the community.
12. Financial resources to adequately undertake the requirements of the proposed Communications Strategy.
13. The use of social media offers the opportunity to be more responsive and transparent. Research shows that appropriate use of social media can assist government to show a more “human” side and can increase trust. Social media can improve accessibility of information, provide the opportunity for two way dialogue, offers the opportunity to broaden messages and is a complementary tool, which while cost effective to use in most cases, does require human resource.
14. Human Resources will need to be increased to sufficient standards to adequately undertake the marketing actions within this strategy.
15. Destination Marketing. As this strategy is developed, provision will need to be considered for a destination marketing and product development resource to proactively promote Cockburn as a tourist destination in the future. This role would hypothetically work across the organization and link in to Economic Development, Corporate Communications (including Events and Culture), Strategic Planning, Community Services, Human Services and Environmental Services and other business areas.
16. Economic Development - If the need is established for communication to businesses on information they wish to hear about, this would fall under the role of an Economic Development resource and link to Corporate Communications. This is currently the role of the Melville Cockburn Chamber of Commerce. The Business Perceptions Survey can be utilized to determine the need for the City to take a role in regular communications to businesses.

Guiding Principles – Communications will:

- I. Be accessible;
- II. Be honest;
- III. Be transparent;
- IV. Tackle the hard issues;
- V. Show a human side to Council;
- VI. Be customer-focussed
- VII. Be accountable

Primary Communication Objectives

1. To develop staff knowledge of City activity, the City's vision and future direction, through internal Communications.
2. To improve the City's community consultation and engagement processes.
3. To improve the community's knowledge of the City's Vision for the area.
4. To improve stakeholder awareness of the City's services and community facilities.
5. To keep stakeholders up-to-date with the status of major projects and issues
6. To promote City-run events.
7. To improve the perception of local government through the continual improvement of customer service.
8. To ensure that the City of Cockburn primary brand is used consistently across all marketing communication and service points.
9. To achieve continual improvement in the perception of the City as a local government and the elected members as its leaders.
10. To ensure that communications material is accessible.

Key Communication Messages

The successful communication of the following messages will be evaluated from 2013-14 following the implementation of a more refined media analysis process which monitors positive, negative and neutral coverage of editorial.

1. The City is a great place to live, work, visit and invest.
2. The City makes decisions based on an assessment of environmental, economic and social considerations.
3. Decisions are community focussed to “consult, listen and action.”
4. The City is strategically planning to make Cockburn the outcome of its vision.
5. The City provides a range of services and events to fulfil the needs of the community.
6. The City is responsive and transparent.
7. The City is working to resolve the issues that are of most concern to stakeholders.

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Key Communication Channels

The following communication channels will be used, as appropriate, to achieve the objectives. These communication channels are all reviewed on an ongoing basis.

Table 2

Communication Channel	Frequency	Reach
Corporate Communications		
Strategic Community Plan	Annual	Website, libraries
Annual Report	For December Council meeting annually	Website, libraries
Rates Brochure	Annual (31 July)	All ratepayers
Resource Recovery Calendar	Annual (June)	All residents and businesses
Annual Business Plan	Annual (June)	Website
Community Calendar	Annual (November)	All residents and businesses
Website	Updates Ongoing	Publicly available
Annual community feedback and research <ul style="list-style-type: none"> • Community Perceptions Survey • Business Perceptions Survey • KPI Customer Satisfaction Research including internal staff satisfaction survey 	Annual (To receive reports by June)	Website

Communication Channel	Frequency	Reach
External Offline		
Statutory advertising	As required.	Print media, Website, Notice Boards.
Updates in the local paper and non statutory advertising	Weekly	Print media to City of Cockburn residential and businesses (excluding rural areas), plus key distribution points
Billboards	As related to key infrastructure projects and events;	As required. High traffic or relevant locations
Cockburn Soundings newsletter	Bi-monthly	All residents (including rural) and businesses plus key distribution points.
Media releases	Ongoing	Predominantly local print media, radio plus others, as appropriate.
Signage and banners at City sponsored events	Ongoing	Event attendees
Posters at City building, external locations (eg schools, shopping centres)	Ongoing	Residents
Meet Elected members at events	As required	Event attendees
Resident welcome pack (letter, vouchers, calendar)	Monthly	New home owners
Service brochures and flyers	Ongoing	Individual nominated distribution points
Promotional material at City events.	Ongoing	Attendees at events
Electronic Communications		
Customer Contact Centre	Monday – Friday, 8.30am-4.30pm	Publicly available
Customer request system	Monday – Friday, 8.30am – 4.30pm	Available for use by staff
Email customer@cockburn	Ongoing	Publicly available
24 hours after hours answering service	After 4.30pm weekdays and weekends	Publicly available
Facebook, Twitter	Ongoing	Viral
E-newsletters (Corporate and other service units, including Environmental Services, South Lake Leisure Centre, CoHealth and various Human Services)	Bi-monthly	On subscription (Currently Corporate e-newsletter averages 1200)

Communication Channel	Frequency	Reach

Action Plan

Objective 1: To develop staff knowledge of City activity and the City's vision and future direction, through internal Communications.

Purpose: For employees to understand how their actions relate to the City's vision and strategic plans to improve productivity and cohesion of service delivery and customer satisfaction.

Research: Results from exit interviews and staff surveys consistently identify that staff do not know enough about the City's vision and mission for the future.

Stakeholders: City of Cockburn employees and volunteers; elected members

Action	Comment	Budget	Responsibility	When
1.1. Conduct bi-ennial staff communication surveys	To identify issues and develop strategies to improve internal communications	Existing	Manager Communications Marketing Officer Media and Communications Officer Manager Human Resources	2012 2014 2016
1.2. Establish a social networking community accessible to staff for both work and social purposes	To be considered as a component of social media strategy	TBC	Manager Information Services Web Administrator Manager Communications	Subject to new CMS
1.3. Develop processes for wider distribution of existing communication materials to staff	Pending results of internal Communications Survey.	Existing	Marketing Officer	2012-13
1.4. Ensure that staff and elected members are aware of public consultations	Develop a workflow process.	Existing	Manager Communications Strategic Business Managers Group (SBMG)	2012-13
1.5. Ensure that staff are aware of updated	Highlighting reviewed policies at SBMG	Existing	Manager Corporate	2013-14

Action	Comment	Budget	Responsibility	When
policies	for dissemination to staff		Communications; SBMG	
1.6. Inform elected members of matters of importance and interest through the Elected Member's Newsletter	<ul style="list-style-type: none"> a. Define matters of importance /interest. b. Identify areas of concern/gaps for Elected Members. 	Existing	Manager Corporate Communications; Elected Members; Executive Services/Directors	2012-13
1.7. Conduct survey /focus group of elected members	Identify areas of improvement they would like to see in communications	\$5000	Manager Corporate Communications; Director Governance and Community Services; CEO	2013-14

Objective 2. To improve the City's community consultation and engagement processes.

Purpose: To develop a community consultation and engagement model resulting in improved trust and respect between the community stakeholders and the City.

Research: 59% of respondents were satisfied with community consultation, 23% were delighted and 24% dissatisfied. **Stakeholders:** All

Action	Comment	Budget	Responsibility	When
2.1. New website (see objective 10) to include option for community feedback at every opportunity.	Subject to new website.	IS Services (TBC)	Manager Information Services; Manager Corporate Communications	2013
2.2. Develop strategy for the use of social media	To ensure alignment with communication strategy objectives	Existing	Manager Corporate Communications	2012-13
2.3. Develop a Community Engagement framework (internal and external), to include a review of relevant Council Policies.	Listed on the Community Development Strategy and Plan and on the Reconciliation Action Plan	TBC	Director Community Services; Community Development Coordinator; SBMG	2014
2.4. Develop a social media policy	To protect employees and Elected Members from the risks of using social media and to protect the reputation and brand of the organisation.	Existing	Manager Corporate Communications (Manager Human Resources; Manager Information Services)	2012-13
2.5. Communicate what action the City has	<ul style="list-style-type: none"> a. Full page advertising in the 	Existing	Manager Corporate	Annually

Action	Comment	Budget	Responsibility	When
undertaken and/or will take as a result of the annual research undertaken.	<ul style="list-style-type: none"> b. Gazette for both surveys b. Double page spread in Cockburn Soundings for both surveys c. Surveys on website (promoted on front page initially) d. Surveys and actions promoted through e-newsletter e. Surveys and actions promoted through community groups 	(Staff Training Budget)	Communications; Community Development Coordinator; SBMG.	

Objective 3. To improve stakeholder knowledge of the City's vision for the area.

Purpose: To demonstrate that the City is planning to make the City the best place to live, work, visit and invest by involving the community in the development of this vision.

Research: Community Perceptions Survey. 53% of residents surveyed in 2012 agreed that the City had developed and communicated a clear vision for the City and 27% disagreed. **Stakeholders:** All

Action	Comment	Budget	Responsibility	When
3.1. Conduct interviews of those surveyed for the 2012 Annual Community Perceptions Survey who said they did not think the City had communicated a clear vision for the City	To determine how to improve these results	\$10,000	Manager Corporate Communications	2013-14
3.2. Consider CEO and or Mayor Blog as part of the social media strategy	For internal and external use	Existing	Corporate Communications	2013
3.3. Produce a summary of the Community Strategic Plan and promote widely in the Community.	To facilitate the document being more widely read	Existing	Executive Services; Corporate Communications	2012-13
3.4. Produce a summary of the budget each	Promote via Soundings, media	Existing	Corporate	Annually

Action	Comment	Budget	Responsibility	When
year	releases, advertise in "Update" and website		Communications; Director Finance and Corporate Services	
3.5. Produce a static display of the City's vision	To be moved around Council buildings and to shopping centres	\$10,000	Strategic Planning; Corporate Communications	2013-14

Objective 4: To improve stakeholder awareness of the City's services, facilities and strategies.

Purpose: To ensure that stakeholders are aware of the range of public programs, services and facilities the City provides.

Research: Catalyse Community Perceptions Surveys – (awareness of services.) **Stakeholders:** All

Action	Comment	Budget	Responsibility	When
4.1. Promote new facilities and services 4.2. e.g. 2013-14 4.3. Emergency Services Headquarters 4.4. Integrated Health Super Clinic, Library and Community Centre 4.5. Coogee Beach Surf Life Saving Club	Develop list of forthcoming openings each year for budgeting purposes.	Project Funds	Corporate Communications and/or facility/service providers	Annually
4.6. Produce a general "live, work, invest and play" video for the website	Targeted at those considering locating to Cockburn	\$10,000	Media and Communications Officers	2013-14
4.7. Refine and update marketing templates on staff portal and promote the use of these.	Assist to develop and implement Communication Plans for the City's key services (Appendix 4), facilities and strategies using the relevant communications tools detailed above.	New (TBC)	Manager Corporate Communications; Marketing Officer; Media and Communications Officers	Ongoing (from 2013/14)
4.8. Produce service e-newsletters where relevant for targeted groups	This will reduce the time spent by non communications officers creating time-consuming printed newsletters. E-newsletters also provide useful statistics	\$3,000 (to be drawn from existing relevant	Corporate Communications to work with: South Lake Leisure Centre;	2012-13

Action	Comment	Budget	Responsibility	When
		Unit Budgets)	Children's Services; Family Day Care; Environmental Services	
4.9. Maximise promotional opportunities from award successes	To maximise benefits of winning an award	Existing (within existing relevant Unit budgets)	Corporate Communications; Web Administrator; CEO	2012-13

Objective 5: To keep stakeholders up-to-date with the status of major projects and issues

Purpose: To maintain a well informed, up to date community.

Research: Community Focus Groups undertaken in 2011 determined that residents want to be kept informed about what is happening in regard to major projects and issues in Cockburn – even those the City is not directly responsible for. **Stakeholders:** All

Action	Comment	Budget	Responsibility	When
5.1. Manage the communications for ad hoc communication issues in conjunction with relevant business units	Each year a number of communications issues arise across business areas, which require careful communication. These are not planned and take resource away from day-to-day tasks	Existing	Manager Corporate Communications; Media and Communications Officers; Executive; Mayor	Ongoing
5.2. Identify key projects and issues annually and develop a related Communication Plan for each	For 2012-13 these are likely to include the management of traffic, amalgamations, progress at Cockburn Central, Cockburn Central West; Coogee Surf Life Saving Club; Cockburn Emergency Services Building; Community Strategic Plan; GP Super Clinic; Cockburn Gateway Shopping City;		Relevant (internal) Project Manager; Corporate Communications; Web Administrator	Ongoing

5.3. Develop communication plans for Cockburn Central Town Centre and Cockburn Central West	To ensure a holistic approach to communicating the development of these Activity Centres	Existing	Manager Corporate Communications; Media and Communications Officers	Ongoing
5.4. Create and promote one 'Major Projects' page for the website	Major projects are currently spread across a range of areas on the website	Existing	Media and Communication Officer; Web Administrator	2012-13
5.5. Develop processes to ensure that officers keep website pages up-to-date	There will be the ability to set automatic reminders on new CMS	Existing	Corporate Communications; Web Administrator; Manager Information Services	2013

Objective 6: To promote City events

Purpose: To ensure the community is aware of City-run events and achieve optimum attendance.

Research; 91% of residents surveyed (Catalyse Community Perceptions Survey, 2012) were familiar with the City's events.

Stakeholders: Residents (including people with disabilities and those of Aboriginal and Torres Strait Islander descent), City Employees, Elected Members, Media, Partnership agencies, Community Groups.

Action	Comment	Budget	Responsibility	When
6.1. Develop a communication plan for each event	To ensure attendance	Existing	Event organiser and Corporate Communications	Ongoing
6.2. Form events working groups to develop consistently sustainable, accessible events with healthy food options to support other City strategies	To ensure a consistent message	Existing	Environmental Services; Events Officers; Arts and Cultural Coordinator	Ongoing
6.3. Develop Event Strategy	To provide guidance when developing events	Existing	Corporate Communications	2013-14
6.4. Convene Council Reference Group for Summer of Fun Events program	To receive feedback from past season and assist with	Existing	Corporate Communications	Ongoing

	planning for next season			
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Objective 7: To improve the perception of local government by the continuous improvement of customer service.

Purpose: To maintain a workforce that is responsive to the community's needs.

Research: Annual Community Perceptions Survey. Annual Customer Satisfaction Research. **Stakeholders:** All

Action	Comment	Budget	Responsibility	When
7.1. Develop an online strategy for delivering online services to our customers	To free up staff to deal with more complex matters and to reduce the need to recruit additional staff in the Contact Centre to deal with increased population	IS Services (TBC)	Manager Information Services; Business Systems Manager; SBMG	2013-14
7.2. To enable elected members to track the status of customer requests online	Underway	Existing	IS Services; Customer Service	2012-13
7.3. To enable customers to track the status of customer requests online	To improve transparency and to reduce the number of calls following up on customer requests	IS Services (TBC)	IS Services; Customer Service	2013-14
7.4. Continually review incoming call subjects and ensure the information is easily found on the website	To improve customer service and reduce calls to the Contact Centre	Existing	Customer Service Coordinator	Ongoing
7.5. Trial short informational videos for the website, (produced in house) explaining the most asked questions received in the Contact Centre	To improve accessibility of information, customer service and to reduce calls to the Contact Centre	Existing	Customer Service Coordinator; Media and Communications Officer	2013-14
Manager IS Services has requested that this be deleted.				

Objective 8: To ensure the City of Cockburn primary brand is used consistently in all marketing, communication and service points.

Purpose: To market the City's corporate logo as the publicly recognised brand of the organisation and to create a stakeholder connection to the City and, in particular, its infrastructure.

Research: Annual Community Perceptions Survey; Annual Customer Satisfaction Survey:

Action	Comment	Budget	Responsibility	When
8.1. To develop an implementation plan to bring all council websites and portals in to one interface	To ensure a consistent approach to information and de-duplication of information	Existing	Manager Information Service and Manager Corporate Communications	2013-17
8.2. Promote the writing style guide 8.3. (See Appendix 5)	To ensure a consistency across Corporate documents	Existing	Corporate Communications	Ongoing
8.4. Update Cockburn Style Guide 8.5. (See Appendix 6)	The style guide is out-of-date and incomplete.	\$15,000	Corporate Communications	2013-14
8.6. Develop Signage Style guide to standardise signage at all City Buildings and Parks	1: develop a corporate design 2: audit signage; develop signage tender to include footing and lighting specifications	Existing New (TBA)	Manager Corporate Communications; Infrastructure Services/Parks and Environment	2012 2013-14
8.7. Develop brand compliant word templates and promote use to staff	These will form part of the Cockburn style guide	Existing	Marketing Officer	2013-14
8.8. Develop program to standardise forms across the organisation	In the interim all Business Units will be asked to ensure that their documentation uses the current logo.	\$10,000	Corporate Communications; SBMG	2013-14
8.9. Develop and maintain corporate image library	Identify and prioritise required images	Existing	Corporate Communications; Business Units	Ongoing

Action	Comment	Budget	Responsibility	When
8.10. Update departmental marketing material to corporate standards	As it requires updating	Existing	Graphic Designer	Ongoing
8.11. Ensure that City-run events are branded with City of Cockburn logo	To ensure that the community are aware which events the City organises	Existing	Corporate Communications; Relevant Business Units	Ongoing
8.12. Phase out use of all 'alternative' logos	To ensure strong brand recognition	Existing	Corporate Communications; SBMG	Ongoing

Objective 9: To achieve continual improvement in the perception of the City as a local government and its leaders.

Purpose: To ensure that stakeholders have confidence in the stewardship of their local government and in the governance of Council.

Research: Annual Community Perceptions Survey. Annual Customer Satisfaction Research. **Stakeholders:** All

Action	Comment	Budget	Responsibility	When
9.1. Undertake reputational analysis study of Executive and Elected Members	To determine issues and develop strategies accordingly	\$10,000	Manager Corporate Communications	2013-14

Objective 10: To ensure that communications materials are easily accessible.

Purpose: To comply with the City's Access and Equity Position Statement (PSCS1). **Research:** Annual Customer Satisfaction Survey.

Stakeholders: Disability Reference Group; Disability Services Stakeholders; Community representatives.

Action	Comment	Budget	Responsibility	When
10.1. Review and standardise how the City promotes the availability of accessible information	To ensure clarity of information	Existing	Marketing Officer; Disability Access and Inclusion Officer	2012-13

Action	Comment	Budget	Responsibility	When
10.2. To develop a plan to integrate the use of plain English throughout the organisation	Government use of language can be unintelligible	Existing	Media and Communication Officer (with Disability Access and Inclusion Officer)	2012-13
10.3. Secure photo library software to enable officers to source photos	This must integrate with current systems	Information Services	Manager Information Services; Corporate Communications	2012-13
10.4. Use images representative of the community	Identify criteria and priorities e.g. 20% of the population has a disability; multi-cultural considerations	Existing	Marketing Officer; Disability Access and Inclusion Officer; Graphic Designer; Disability Reference Group	Ongoing
10.5. Develop a new website as follows: Front end to integrate with a new content management system Website which can take advantage of the opportunities provided by social media Website at least adhere to the latest accessibility standards ...	Ensure the search function on the website differentiates between minutes and agendas and website pages and that information is easy to find. Easy to navigate. Adheres to best practice accessibility guidelines.	Existing (IS Services)	Manager Information Services; Manager Communications	2013

Reporting

Reporting will be via the following channels:

- Annual Perceptions Survey presentations to Directors, Elected Members and Staff
- KPI Customer Satisfaction Survey presentations to Directors, Elected Members and Staff
- Quarterly reports to directors and Elected Members
- Annual review of this document
- Annual Review of relevant Policies

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Appendices

Appendix 1: Review: Communication Strategy September 2006-2011

In 2006 the City of Cockburn developed a Communication Strategy and implementation plan, which is to be reviewed in 2011/12. The Strategy had 23 recommendations and outcomes are detailed below.

Recommendation 1: That Council continues to produce Cockburn Soundings on a bi-monthly basis.

Measurement: That it was produced and distributed within set timeframes and budget. That the satisfaction rating of 80% is maintained or surpassed in future surveys.

Outcome: Soundings has been produced as per timeframes and budget. Satisfaction 2006-2008 was 79%, 74%, 76% and has increased in 2009-2011 to 80%, 79% and 81%.

Recommendation 2: That Council consider increasing the number of editions of the Cockburn Soundings (July 2007).

Measurement: Financial and staffing implications to be considered.

Outcome: Cockburn Soundings was retained as a bi monthly publication with ad hoc publications as required.

Recommendation 3: That from 2007/8 Cockburn Soundings discontinue the use of paid advertising in its issue and the current contributor be offered a similarly priced sponsorship opportunity

Measurement: Paid advertising removed; alternative sponsorship arrangements negotiated with Cockburn Gateway's management.

Outcome: Paid advertising was removed. Since then a 'Sponsorship' Position Statement was adopted by Council in 2011.

Recommendation 4: A survey of recipients of the welcome kit be undertaken throughout 2006/7 to verify the effectiveness of the: Customer Handbook, Community Directory, Information Request form.

Measurement: 50% survey return rate and welcome kits modified, if necessary to reflect the results of the survey.

Outcome: Since 2012, new house purchasers receive a letter with vouchers for Phoenix Shopping Centre, Cockburn Gateway Shopping City and the new annual calendar. This includes details of waste collections, CoSafe, Community events, Council services and contact details.

Recommendation 5: A review of the Co-Info Database to be undertaken to determine its effectiveness as an ongoing community information source.

Measurement: Information obtained to enable a considered opinion to be made on the future of the database.

Database either improved in effectiveness or discontinued and substituted with alternative facility.

Outcome: The database is being continued and has replaced the printed version which was superseded.

Recommendation 6: That Council investigates the expansion of email newsletter, with a view to expanding the number of discrete newsletters distributed, and specializing them by topic and community interest.

Measurement: Overall subscription to e newsletter increases by 10% per annum.

Outcome: The recommendation is a proposal for 2012-13 in the updated Strategy.

Recommendation 7: Introduce a Corporate Publications Procedure to address issues of uniformity in style guidelines, formatting and establishing a formal process for such publications.

Measurement:

- (a) In house procedure agree to and implemented and adhered to by relevant staff
- (b) Uniformity in council produced publications;
- (c) Inclusiveness principles given necessary

Outcome:

- (a) In house procedure is established.
- (b) The style guide was established and is adhered to but requires updating. A writing style guide has also been developed.

(c) DAIP guidelines are strictly adhered to.

Recommendation 8: Review the Corporate Style Guide to ensure it is current and relevant, including investigation of a corporate template for in-house designing.

Measurement: Style Guide updated and utilized in Council Design process.

Outcome: This was done but the style Guide requires further updating. Further templates are a recommendation of the updated Strategy and funds are recommended to be allocated in 2013-14 to update the existing out-of-date style guide.

Recommendation 9: Council tender its printing services using a panel of preferred suppliers.

Measurement: Panel of providers selected to undertake printing services. Tender panel appointed.

Outcome: Cockburn Soundings printing was tendered out. An organizational print tender is on the procurement list for 2012-13 financial year.

Recommendation 10: That an abridged version of the Plan for the District 2006-16 be produced for public information and availability, following its adoption by Council every two years.

Measurement; Document printed and delivered by February 2007.

Outcome: This has been superseded. An abridged version of the new Strategic Community Plan (2012 -2022) will be made available electronically.

Recommendation 11: That summary versions of identified non-statutory documents presented to council for adoption or endorsement be produced for public information and made available as appropriate, commencing with the City of Cockburn Strategic Plan 2006-16.

Action: Summarised versions of identified documents to be available on-line.

Measurement: Copies of approved documents available on-line.

Outcome: All such documents are now placed on the website and hard copy documents are made available at the libraries, administration building and identified relevant outlets as appropriate.

Recommendation 12: That the potential for all Council information related publications to be made available on Council's website be investigated with ease of identification and access to the documents being the primary priority of the exercise.

Action:

- (1) List of Council Information publications available for general access to be compiled.
- (2) Web administrator input on proposal provided.
- (3) Report presented to SBMG for endorsement.
- (4) Decision made through SBMG on best option to progress the proposal.

Timeframe: June 2007

Measurement: Increase in Council related information provided on Council's website.

Outcome: It is now standard practice for documents of interest to the public to be uploaded to the website.

Recommendation 13: The coordination of information made available on the Council's website to be investigated, to include a review of the current web administrator's function, workflow and reporting hierarchy.

Action:

- (1) Discussion paper prepared for consideration by affected staff.
- (2) Meeting conducting between affected staff.
- (3) Recommendation passed to SBMG for consideration.

Timeframe: June 2007

Measurement: Review of website administration completed and any subsequent recommendations implemented.

Outcome: Review undertaken. The Web Administrator position remains in Information Services (IS) and IS and Corporate Communications work very closely on content management.

Recommendation 14: That a review of current practices and procedures for dealing with the media, together with media relations in general be undertaken following the appointment of the Media Liaison Officer in 2007.

Action: Review of Council Policy SCS1 Media Activity to be undertaken.

Officer: Media Liaison Officer (to be appointed early 2007).

Timeframe: June 30, 2007.

Measurement: Any recommended amendments to current practices and media relations to be reflected in updated policy.

Outcome: The policy has been reviewed and is still considered relevant, with minor amendments to content to be recommended to Council.

Recommendation 15: That the potential to amalgamate the roles of Cashier/Customer Service Officer be collaboratively investigated by the supervisory staff of the current positions.

Action:

- (1) Position description forms for the proposed amalgamated roles to be prepared.
- (2) Discussions with affected employees be undertaken.
- (3) Agreed outcomes recommended to management for endorsement and subsequent implementation.

Officer: Customer Services Coordinator and Financial Accountant.

Timeframe: December 31, 2006

Measure: Positions finalized and ready for implementation when new administration building commences operation.

Outcome: Amalgamation of roles undertaken November 2011.

Recommendation 16: An analysis of direct in-dial calls received to be undertaken to determine the effectiveness of this method of customer contact.

Action:

- (1) Data identifying direct in-dial contacts received for a predetermined sample period.
- (2) Analysis of data undertaken and a report prepared on the results.

Officer: Customer Services Coordinator.

Timeframe: December 31, 2007.

Measurement: Report, together with any recommendations, prepared for consideration by management.

Outcome: The white pages listing has been tailored to allow direct contact with external services to the administration and the Contact Centre system has been tailored to filter 80% of calls and direct to the most appropriate area. The City has removed “press” (1, 2, 3, etc) options.

Recommendation 17: A thorough analysis of Call Centre operations in local government be undertaken to ascertain the most effective method of integrating this service into the City’s public communication mechanisms.

Action:

- (1) On-site investigation of call centre operations in metropolitan Councils undertaken.
- (2) Prepare report for consideration by management.

Officer: Communications Manager

Timeframe: October 31, 2006.

Measure: Planned timeframe for implementation of call centre operation completed.

Outcome: On-site investigations of call centre operations in metropolitan Councils was undertaken and a report was prepared for consideration by management, resulting in the current operation being introduced in 2008.

Recommendation 18: The position descriptions of all staff affected by the integration of duties resulting in the re-working of job functions and responsibilities required to introduce new customer service initiatives into the refurbished Administration Building during 2007, be reviewed to ensure adequate skills and resources are allocated to the functions necessary to effectively implement these identified initiatives.

Officer: Communications Manager

Timeframe: October 31, 2006

Measure: Planned timeframe for implementation of call centre operation completed.

Outcome: Implementation of call centre operation completed.

Recommendation 19: That Council consolidates its mechanisms for gathering and analyzing community satisfaction survey data into one process, and revisits KPI mechanisms which rely on this data to ensure that the data gathered and KPI mechanisms are useful on measuring performance and offering assistance in improving customer service standards.

Action: Continue to undertake the triennial Community Needs Survey (CNS) and Community Satisfaction Monitor (CSM) utilizing current mechanisms.

Officer: Manager Community Services

Timeframe: May, 2007

Measure: CNS and CSM undertaken and presented to elected members. Investigation and tender completed for survey needs.

Outcome: CNS and CSM continue to be utilized as an integral measure of community and customer satisfaction.

Recommendation 20: That Council investigates the potential for a web based community opinion and feedback facility to be made available for public research and polling purposes, with the possibility of utilizing the e-newsletter version of Cockburn City Update as a mechanism to achieve this.

Action:

- (1) Prepare discussion paper and arrange meeting with required personnel.
- (2) Prepare report and any subsequent recommendations for consideration through SBMG.

Officer: Manager Community Services; Manager Information Systems; Web Administrator.

Timeframe: June 30, 2007.

Measure: Any recommended initiatives implemented within specified timeframe.

Outcome: The principle of community engagement is a primary focus of the new Community Strategic and Business Plans and will be trialed during this process during the remainder of 2012. Furthermore, a Social Media Strategy will be developed in 2012-13.

Recommendation 21: That the outcomes of the needs and satisfaction research undertaken by Council, be presented to the Regional Community Development Group, following their provision to elected members.

Action:

- (a) Review Council's Policies SC2 Strategic Consultation with Community Stakeholders and SC4 Establishment of Issue Based Committees.
- (b) Arrange for presentation of CNS and CSM data to be presented to the Regional Community Development Group, following their presentation to elected members.

Officer: Director Administration and Community Services

Timeframe: June 30, 2007

Measure:

- (1) New policies adopted by Council.
- (2) Regional Community Development Group involved in processes relative to Council's ongoing community needs and satisfaction research.

Outcome: Policy SC2 was reviewed by Council in December 2007 and current processes endorsed. The proposed new Strategy will incorporate the development of a more broad based Engagement Framework. Policy SC4 was reviewed in 2012 and considered to be adequate in its intent. The establishment of Reference Groups has superseded the previous system of community based "advisory committees" following the 2007 Council elections.

Recommendation 22: That the Regional Community Development Group to be included in any consultation process which addresses an issue or issues of district wide concern and that the Group be notified of the outcomes of any appropriate specific local research or consultation exercises for information purposes.

Action:

- (1) Review Council's Policies SC2 Strategic Consultation with Community Stakeholders and SC4 Establishment of Issue Based Committees.
- (2) Arrange for presentation of CNS and CSM data to be presented to Regional Community Development Group following their presentation to elected members.

Officer: Director Administration and Community Services

Timeframe: June 30, 2007.

Measure:

- (1) New policies adopted by Council
- (2) Regional Community Development Group involved in processes relative to Council's ongoing community needs and satisfaction research.

Outcome: Policy SC2 was reviewed in 2007. Policy SC4 was reviewed in 2012. (See “Outcomes” for recommendation 21 above)
The establishment of Reference Groups has superseded the previous system of advisory committees following the 2007 Council elections.

Recommendation 23: A review of Council Policies SC2 Strategic Consultation with Community Stakeholders and SC4 Establishment of Issue Based Committees to be undertaken to reflect the intent of Recommendation 20 and 21 above.

Action:

- (1) Review Council’s Policies SC2 Strategic Consultation with Community Stakeholders and SC4 Establishment of Issue Based Committees.
- (2) Arrange for presentation of CNS and CSM data to be presented to Regional Community Development Group, following their presentation to elected members.

Officer: Director Administration and Community Services

Timeframe: June 30, 2007

Measure:

- (1) New policies adopted by Council.
- (2) Regional Community Development Group involved in processes relative to Council’s ongoing community needs and satisfaction research.

Outcome: Policy SC2 was reviewed in 2007. Policy SC4 was reviewed in 2012. (See ”Outcomes” for recommendation 21 above)
The establishment of Reference Groups has superseded the previous system of advisory committees following the 2007 Council elections.

Appendix 2 Stakeholders

<p><u>External:</u></p> <p>Residents Ratepayers Businesses Media Itinerant workers. State and Federal Government Departments</p> <hr style="border: 1px solid purple;"/> <ul style="list-style-type: none"> • Australian Marine Complex • Child and Adolescent Health Services • Cockburn Police • Cockburn Sound Management Council • Department for Child Protection • Department for Child Protection - Fremantle • Department of Commerce • Department of Communities • Department of Corrective Services • Department of Education, Employment and Workplace Relations • Department of Health, Aged and Continuing Care <p>Directorate</p> <ul style="list-style-type: none"> • Department of Housing & Works • Department of Planning • Department of Transport • Department of Water • Department of Environment & Conservation 	<p><u>Internal:</u></p> <p>Staff Elected Members</p> <p><u>Business Units:</u></p> <p>Finance Services Human Resources Human Services Information Services Waste Services Parks Services Environmental Services Infrastructure Services Engineering Services Building Services Statutory Planning Services Strategic Planning Services Health Services Land Administration Services Recreation Services Children Services Family Services Senior Services Disability Services South Lake Leisure Centre Youth Services Aboriginal Development Services Aged and Disabled Services</p>
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<ul style="list-style-type: none"> • Department of Planning • Department of Transport • Fire and Emergency Services Authority of WA • Landcorp • Main Roads Western Australia • Public Transport Authority. • State Heritage Office <p>Reference and Advisory Groups Suppliers Community Groups Customers of the City's services and facilities Partners Federal and State Government politicians Melville Cockburn Chamber of Commerce Developers</p>	<p>Corporate Communications Community Development Services Ranger & Community Safety Services Library and Information Services Executive Services</p>
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Appendix 3 Results from two community focus groups on City Communications in August 2011(Catalyse).

Provided as separate attachment.

Response to City of Cockburn Communications Review Focus Groups Key Findings – September 2011

Catalyse undertook the review in September 2011. The conclusions, supported by the Community Perception Surveys, are that most stakeholders believe the City is doing an 'okay' job at communicating with residents. To improve, the City's communications strategy should:

- 1) incorporate a more human touch,
- 2) tackle 'hard' issues,
- 3) add a personal and targeted approach (by suburb or local neighbourhood, but not by Ward, and;

4) engage residents in genuine, two-way communications.

Specific discussion focused on key corporate communications tools and as a result the following actions have been agreed to be implemented. The broader advice will be taken into consideration when developing the Communications Strategy.

Annual Report

- Printed with summary financial statement.
- The full set of financial statements will be available on the website.
- The print run is reduced to 50 copies per year.

Rates Brochure

- A 16 page version has been produced and agreed with Financial Services - a reduction from 28 pages. (This has been implemented for 2012).

Customer Handbook

- Discontinued and the budget information, key service information and contact numbers has been relocated to the annual calendar. The remaining information is available on the website.

Community Directory

- ColInfo – the online database is now the central resource for the public and organizations. No further printed versions will be created. The Contact Centre will still be available to provide and dispense information in alternative formats.

Resource Recovery Calendar

- To be maintained in current format.

Summer of Fun Calendar

- Will be integrated with the Annual Community calendar.

Newspaper Adverts

- No change.

Cockburn Soundings

The following changes will be implemented:

- Facebook and Twitter will be promoted through Soundings and in the future invite comment on some stories (Two way conversation)
- More human interest articles and photos.
- A City of Cockburn Service will be proposed for each edition.
- The writing style will approach Soundings in a more conversational way. A “Listen, Learn and Action” message where relevant.
- A regular article will uncover the meaning of suburb names perhaps moving on to park or road names (by suburb).
- A regular article called ‘Cockburn by numbers’ will highlight interesting statistical facts about Cockburn – e.g. number of bins emptied per day; length of paths in the city, etc.
- Continue to feature the Community Development focus and increase to each edition if warranted.
- Report honestly on the difficult issues the community are reporting and the City’s response..
- Event calendar to include community events where possible.

City of Cockburn Annual Calendar

- Development of the calendar to include one page per month with sufficient space for notations;
- That the calendar be good quality production and contain comprehensive information (eg school term dates, public holidays and colour coding)
- A4 size

Website

- Will seek to improve through community feedback, when upgraded.

DRAFT



CATALYSE®

City of Cockburn Communications Review

Key findings from focus group research

September 2011

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Executive Summary

- In May 2011, CATALYSE® conducted a Community Perceptions Survey to measure overall satisfaction with the City of Cockburn. The Survey evaluated perceptions of local services, infrastructure and facilities; identified performance gaps; and benchmarked the City against other WA local governments.
- A rigorous and reliable method was used to survey a random sample of households. The result was a valid dataset ($n = 401$), with a sampling precision +/- 5% at the 95% confidence interval.
- The survey data revealed that overall satisfaction among residents with the City of Cockburn remains high. However, satisfaction with how well the City informs residents about local issues is moderate and below industry standard.
- The strategic purpose of this focus group research was to provide the City with community-led recommendations for improving communications. This feedback will enable the City to update its Communication Strategy, with the ultimate aim of improving levels of satisfaction with how the City informs residents.
- Four key themes emerged from the focus group data. In essence, the recommendations to improve the City's communications included 1) incorporating a more human touch, 2) tackling 'hard' issues, 3) adding a personal and targeted approach to the City's publications, and 4) incorporating genuine, two-way communications.

Research Method

- Two focus groups were conducted with residents on 25 August 2011 at the City of Cockburn's Seniors Centre in Spearwood.
- Participants were recruited by Edith Cowan University's Survey Research Centre, with quotas set by Ward, gender and life stage, and also by current level of satisfaction with communications.
- 20 residents were recruited for the focus groups, and 15 participated on the evening.
- The focus groups were 90 minutes each in duration.
- A structured discussion guide was developed in consultation with the City of Cockburn. Participants were engaged in a series of open ended questions and whole group exercises.
- The focus groups explored:
 - ❖ The community's information needs from the City of Cockburn
 - ❖ Preferred ways of sourcing information
 - ❖ Awareness, and readership or usage, of existing communication materials
 - ❖ Perceived strengths and weaknesses of selected communication materials
 - ❖ General perceptions of the City of Cockburn's website
 - ❖ Suggested improvements to communication/communications materials



Key Findings

Key Findings

Four key themes emerged from the focus group data. In essence, the recommendations to improve the City's communications included incorporating a human touch, tackling 'hard' issues, adding a personal and targeted approach to the City's publications and incorporating genuine, two-way communications.

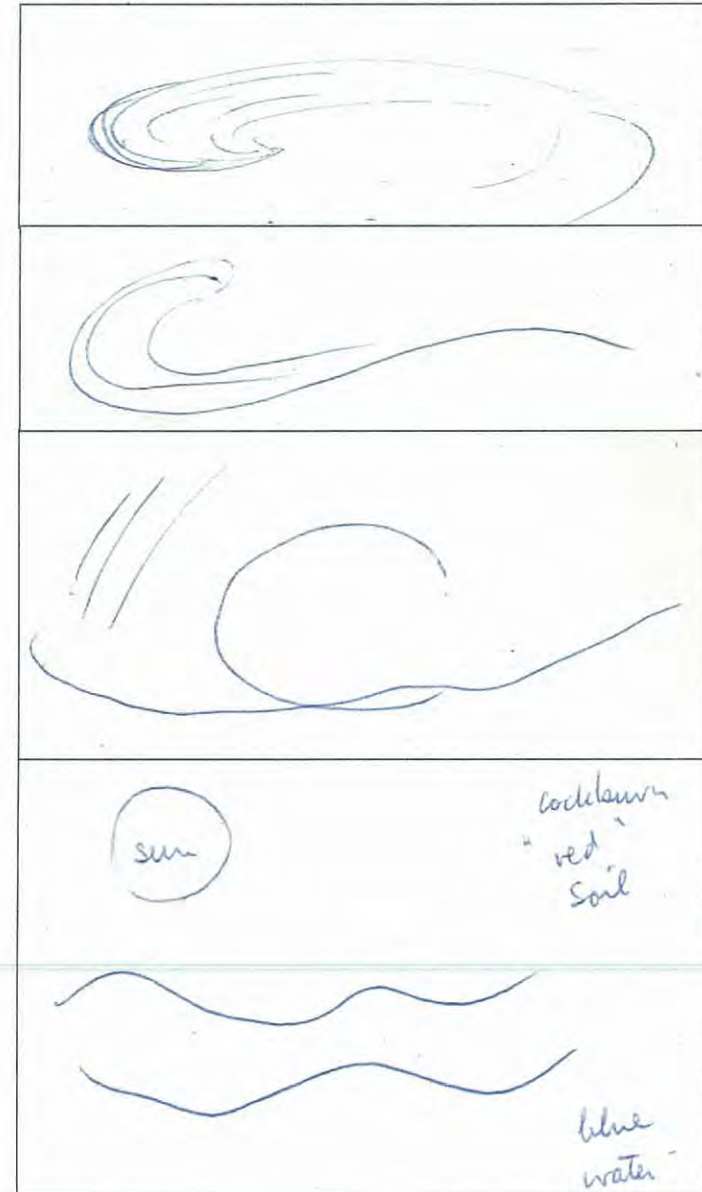
1. Focus group participants desired a more human feel to the City's communications. They look to the City's publications to feature local personalities, community champions and familiar faces. News items recalled with the most fondness were profiles of community champions.
2. Focus group participants desired a more personalised and targeted communications approach, with area/suburb (not ward) specific information. Many focus group participants were unconvinced that the City's generic communication materials were targeted at them – "is this talking to me?" The generic communications were often too wide-meshed and residents were required to sift through a lot of information to find items relevant to their local community.
3. Focus group participants desired genuine two-way communication with the City, rather than being relegated as recipients of one-way information. They wanted the City to facilitate dialogue. One low risk strategy to achieve this is promoting an askcockburn@cockburn.wa.gov.au campaign
4. Participants want both soft and hard news. Residents are wary of PR spin. They want the City to tackle the hard issues head-on and to stop avoiding issues that are difficult and/or controversial. This perception of avoidance undermines the City's relationship with its residents. The message for the City of Cockburn is – "talk with me, not at me!"



Understanding Residents' Information Needs

Understanding residents' information needs: what do residents want?

- The good news is that most participants recognise the City of Cockburn's logo and branding.
- As a warm-up activity, participants were asked to draw the City's logo. During this activity, many identified the colour scheme and discussed the City's tag-line without prompting. The inference is that residents are associating the City's communications materials with the City of Cockburn.
- The bad news is that focus group participants appeared relatively fixed in their awareness, readership and usage of the City's communications materials. It's likely that these established readership and usage patterns will be very hard to shift.



“It does have the word ‘waves’ in it. It mentions waves in the tagline but I can’t remember... It says something like hills to waves...”

Testing specific communication products

Annual Report

- Not surprisingly, few focus group participants had ever seen or read a copy of the City of Cockburn's Annual Report.
- During the focus groups, participants had an opportunity to flick through the report, read the contents page and browse key sections. While acknowledging that the report appeared detailed and well presented, few participants indicated that they would ever be inclined to read such a document. The exceptions to this were: if reading the document was a requirement for work purposes; and/or it contained information pertinent to a particular issue individual residents were agitated about.
- The information in the Annual Report of most interest to focus group participants were: the financial summary; and the future vision (the 'Year Ahead' section),
- When pushed further, focus group residents stated that a 5-6 page glossy brochure summarising the Annual Report would be a preferable means of communication, however it would not necessarily be desirable. Even in an abbreviated and more easily digestible format, focus group participants expressed low interest in this information.
- In summary, focus group participants articulated their preferences for:
 - ❖ A very limited run of Annual Reports distributed in key access areas (such as the library);
 - ❖ Less expenditure on printing and design costs on a document that few read and access;
 - ❖ Notification on the website about where and when the Annual Report is available;
 - ❖ Inclusion of key points within the Cockburn Soundings with reference to an online link;
 - ❖ An independent summary of the report from a community spokesperson/champion or residents association.

Direct feedback about the Annual Report

"I've never actually gone and hunted for one. Sections and bits and pieces of it when I was heavily involved with some community stuff years ago but not recently, no".

"It's very attractive I think. It's a lovely book and it smells good but I'm not interested in it".

"If there was a summary up the front that might look good, would I necessarily read it? I suspect I'd only start reading stuff like this if, like x, I was suffering a little bit of discomfort in ... where I was living and I was getting a bit more agitable about it".

"Can you really make this more interesting? I don't think you can"

"Annual Reports are not sexy; they're not designed for bedtime reading".

"It's a headache. It looks like homework to me".

"I think, what would be appropriate for us would probably be something that summarised in maybe 5 or 6 pages, with lots of bullet points and maybe referenced to other areas where you could find out more information. But for someone that works in community programs, having access to something like this is actually beneficial to me".

"I'd be happy just seeing a local website saying "this is it" and for further information just press this link and blink this just pops up as a PDF file".

"All I'd be interested in is someone independently giving me a summary of saying you know that they've kept a budget or that they're over or under, that they've employed all the employees they should have or that they're running at so many vacancies that they haven't filled and ... that the money was distributed as planned... that sort of thing".

Budget and Rates Brochure

- Feedback about the Budget and Rates brochure was mixed.
- While some focus group participants recalled receiving the Budget and Rates brochure with their rates notice, others did not.
- Those who recalled seeing this brochure were more likely to be ratepayers with responsibility for paying bills within their household. However, it seems that the brochure is rarely passed onto others within the household, with low awareness among non-bill payers. Awareness is also low among those who are renting.
- As the number of organisations using A5 brochures along with their invoices increases (such as utilities and banks), readership appears to be declining. It's as if all brochures of this type are now tarnished with the same superfluous brush.
- Focus group participants were concerned about the shelf life, longevity and relevance of the brochure, with many preferring the information to be in electronic format only – that is, an updatable web brochure.
- Focus group participants liked the ability to see at a glance the City's main revenue and expenditure components. In terms of the Budget and Rates brochure, it is this visual and colourful pie chart that is of most interest. That said, aspects of the Budget and Rates brochure that were raised spontaneously related to pensioner rebates and underground power. People specifically looked at the brochure for information relating to these issues.

“Would that be on the website? To me, that would make a lot more sense. For me as a ratepayer, you would save a lot of money from the printing. And I think people look at that once maybe a couple of times, but you don't keep that on your shelf and think “oh, I'll refer back to that every so often”. So that needs to be on the website”.

“I didn't realise it was in with my rates. If I'm a bit more observant next time, I'll actually check the darn thing. There's some valuable stuff there in that I didn't realise the Council was like a hundred million dollar business give or take and that's pretty significant amount of money to be floating about. That helps me put it into a perspective compared to other things I'm aware of...”

“I actually think it's good. It's easy to read. It's easy to hold. It's not cumbersome”.

“I don't see it. If a rates notice comes into our house, I just close my eyes and give it to someone else and hope it goes away”.

Customer Handbook

- Very few people recalled having ever seen the City's Customer Handbook.
- Focus group participants indicated that when looking for information about local government services they would normally either ring the Council directly or look online.
- Some focus group participants indicated that they might refer to the directory for service information if the directory was made more widely available. Hence, access and distribution were key issues. Suggested distribution points included shops, schools, delis, libraries, shopping centres and notification in the Cockburn Soundings.
- To be relevant, the handbook needs to contain telephone and email contact information.

"I wouldn't have thought to look in here for a lot of stuff. But now I'm looking in here, there's a lot of good stuff on offer. Not all of it's relevant of course but it looks like they offer a lot of stuff".

"I think there is good interconnectivity though. I found out about some art grants because they were talked about in the Soundings and that then led me to see what other grants were on offer by going to the website".

"Where's that actually available from? It begs the question what the Council identifies as general public outlets and where they are? Local shops? Delis? That sort of thing..."

"You wouldn't go looking for this unless you knew it was out there"

"Yeah, I think I remember something like this. If it's got numbers to do with the community, I tend to keep this with the Yellow Pages and White pages in my office there because I can always refer to it if I have to so I do keep things like that".

Community Directory

- Again, very few people recalled having ever seen the City's Community Directory.
- Some people keep directory information with their White or Yellow Pages, whereas for others the Internet has surpassed telephone directories. Some are moving (or have moved) to a paperless household, while others still find comfort and security in having something to hold and keep.
- The key function of the directory is its search function.

"Yes, I would. I would. Because there's probably things in there. Because when you search, you only have one thing in mind but if you flick through that there may be other things in there that may jolt or make you more aware of other things out there that you might access".

"Sometimes when you go online there's just so much information that you just go "Oh! Where do you start?" Whereas, with something like that it's local, it's easy..."

"Isn't this all in the phonebook anyway?".

"I like keeping local things where my telephone is".

"Nope. There's a big Internet out there. The Internet has surpassed all this type of communications as far as I'm concerned. I've got Yellow Pages stacked up for the past four years that haven't been opened yet. I don't use them anymore".

- The detail of the City of Melville's A-Z directory was appealing to many, however, many found it confusing to use.

"This one certainly has got some good information but I'm not sure about how ... you know how it's got A to Z, but I'm not sure if it's the name of the place or the name of the company or just in general. I'm just not too sure how they've set it out. If I was looking for a particular thing... because it could be called something else. Whereas this one (the City of Cockburn's directory) just has dentist and then it has all of the dentist's underneath. It's the layout. It's got schools. I mean, how easy is that".

Resource Recovery Calendar

- There was widespread readership and usage of the Resource Recovery Calendar. Everyone in the focus groups had seen the calendar and were actively using it.
- Most participants were very complimentary of the calendar's design, format and layout, saying that it was easy to use. Participants agreed that the paper and magnet quality was sufficient so that the calendar lasted for 12 months (although it wasn't necessarily "kid-proof" and some joked about having to pick it up off the floor several times a year). The colour-coding was easily understood by residents.
- The delivery and timing of the calendar had been an issue for some people in the past.
- This is a document that residents are highly familiar with and attached to. Make changes to it sparingly.

"This is good. This is better than the last format. They had it in another format previously. But this is much, much better. But when they came out with this it was a really good idea. It's easy to use".

"I think this is super! It clearly explains everything that goes in each bin, when everything is coming..."

"No. I think it's good. I did once take green waste to the regional recovery centre and contrary to published information I was actually charged... but according to this it's supposed to be free".

"No, this is OK. It's difficult for me to get excited about things that I'm chucking out".

"We find that we actually almost don't need even this (Resource Recovery Calendar) because at the beginning of Progress Drive, whenever there's going to be a collection, Council puts a big sign up and... that to me makes so much sense. Again, using those sort of billboards around Bibra Lake saying what's happening around there and I think that could be promoted more."

"This is very handy. It stays on the fridge and gets referred to monthly"

Cockburn Soundings

- There was universal recognition and awareness of the Cockburn Soundings newsletter, although actual readership was mixed.
- For committed readers, the newsletter keeps residents in touch with the City of Cockburn. Readers specifically seek out local identities and people they know in the newsletter. Most like the short articles – “a quick read” (around 5-10 minute mark) with online links to further and more detailed information on the Internet.
- Those that read the newsletter concede that the content is often repetitive and full of propaganda. There was a desire among focus group participants to make the content of the newsletter less glossy and pithy and more issues-based. Readers were looking for less one-sided information and more hard news articles that serve as a catalyst for community conversation. Participants were open to reading summaries of Council minutes, information about road closures, articles about local development issues and a community diary of events (open for community based organisations to advertise local events).
- The key features that residents looked for in the newsletter were:
 - ❖ Photographs of local identities
 - ❖ Profile information
 - ❖ What’s happening locally
 - ❖ Interconnectivity
 - ❖ A personal and human touch to news.
- For many focus group participants, the newsletter gets lost in junk mail. Arriving in the letterbox with all the other junk mail made it harder for the newsletter to stand out.
- The tri-fold presentation often meant that the newsletter looked creased and dog-eared before it was opened. While it leaves the administration office looking glossy and polished, it arrives crumpled and disheveled. Having been folded for a while makes the paper harder to handle and read.

Cockburn Soundings

“Not so much. No, it's probably more that like there's probably not much that would interest me in these sort of things... No, I just wouldn't”.

“Yeah, I do read it. And it's one of the things I do think connects me to feeling like I'm in Cockburn Council. It keeps me aware. I think my demographic doesn't relate to a lot of the groups in it. But I like looking at it”.

“It's the one thing I read from cover to cover. I have a look to see if I know anyone in the pictures. It's got that level of contact about it. It's a easy to read; a friendly feel”.

“Remember this is all in the junk mail as well on the table and it's just another one of those things and then you might see the Bunnings one ... and you might be interested in a tool from Bunnings so you pick that one up instead...”

“By the time you straighten it out, it doesn't look too flash by the time you read it”.

“It's a bit repetitive about different things they do, seniors this, youth stuff... but I kind of like the more human profile things. It kind of connects a bit between local rag and Council...”

“Again, when it comes folded up like that it often goes straight from the letterbox to the recycling”.

“It's largely propaganda though, isn't it? It's about 80% propaganda telling me how wonderful the Council is”.

“There are too many glossy pictures...I'm not saying that that's not a nice thing, but they could be providing more information and less pictures...Do they really need all the pictures?”

Summer of Fun

- Recall and recognition of the Summer of Fun calendar was mixed. Those most likely to use the calendar were seniors and families – those with a track record of attendance at community events.
- Most people were interested in the content of the Summer of Fun calendar, but the delivery and format didn't suit everyone. For most people, the way they used the calendar was to look through it, circle the events they were interested in, transfer that information onto the household's main diary (or web calendar) and then throw it out. This is not a publication that is kept on the fridge like the Resource Recovery Calendar. Residents simply access the information, transfer it and then throw it.
- For those residents that have moved (or are moving) to a paperless household, they would prefer to subscribe to an online events calendar or receive an events guide that automatically updated their web calendar.

"I would much prefer to subscribe to a City of Cockburn events calendar that I could see in something like Google calendar and then choose to go to anything I want..."

"I've noticed that one or two other Councils use signs along the roadside I've seen every now and then...signs going up saying this event is one...Yeah, so I get one of those 'a-ha' moments".

"I think my wife keeps this for the children..."

"One of the things that is good is the Rotary Spring Fair... and the thing that they do... is stick up banners up all over the place and I think that gets more people there than anything else".

"I live in an electronic world. If this was sent out as a thing that would update your calendar automatically then that would be much easier for me than trying to keep track of a piece of paper".

"Yes, I've seen it before. I've noticed it and thought that the Council was trying to engage with young people especially with the rock concerts. And I thought that was pretty valuable even though I've never been to one before. The local signs do work... and you notice them... because it's a visual reminder".

Newspaper Inserts

- Existing readers of local community newspapers normally read the City of Cockburn's insert however this only amounted to about half of the focus group participants.
- Some residents experience regular distribution issues with the delivery of the Gazette and Herald, with many critical comments about the state of the newspapers residents actually receive.
- Local newspaper inserts tend to 'preach to the converted', communicating mainly with those residents who seek out the insert and are therefore receptive and open to reading information from the City.

"I just find it a bit clunky and a bit, you know... Yeah, if I saw that (Cockburn's insert) I would definitely look at it, but the newspaper comes with all this other stuff and... Sometimes, unfortunately, it goes straight in the recycling bin..."

"Well, it's pretty immediate because you get the newspaper every week so I read it a fair bit".

"Yes, I remember seeing this. But by the time you get to the letterbox... the hole is too small so it gets quite scrunched up. By the time you rescue it (the newspaper from the letterbox) it's fairly hard to read because it's ripped and all that".

"I flick through the community newspaper every now and then but I've never felt the need to demand any services from Cockburn. So I look at it and think that it's for someone else to read".

"I scan it on the way to the recycle bin".

"But that's probably one of the few things that are coloured in the local paper... so if someone reads the paper, I'm sure they should be attracted to it. But for someone who doesn't like me, they'll never see it".

Website

- Many focus group participants had visited the City of Cockburn's website, mainly for specific information such as ranger details and library renewals. For some, the City's website is their 'first port of call' for information and has replaced the White/Yellow Pages.
- For most participants, the website's search function is the most important feature.
- There were many complimentary comments about the online library services.
- There were no Facebook friends among the focus group participants and unlikely to be any in the near future. Focus group participants were nervous about befriending the City out of a fear of being bombarded with advertisements. Those in attendance were not enthusiastic about online blogs or local government forums, yet.

"I usually go on there for some specific information, so ranger contact details if I have an issue or library renewals. And I do follow the City of Cockburn's twitter and the library twitter account".

"Some of the planning stuff is a bit longwinded... but the library site is very, very good".

"I'm not a browser, but a single issue user. And the data I've required, I've found, so I'm happy".

"The last time I went on, I was looking at the fun run... Also, sustainability and stuff like that. And we're also renovating our house, so we were looking at who we might need to contact".

"It works for me. The information is there and it's not in a hard to access format. We did notice that there were some inconsistencies (in information/data) which we told the Council to track down..."

"I did some study on webpage stuff recently... It seemed very compact and easy to find your way around".

Proposed Annual Calendar

- Focus group participants were interested in talking about the prospect of an annual calendar put out by the City of Cockburn and expressed some positive comments about the initiative. However, to be successful the calendar would be required to be:
 - ❖ Comprehensive and useful - a full and total replacement of the household's current calendar
 - ❖ One page per month, with sufficient space for writing
 - ❖ Timely, up-to-date and accurate
 - ❖ Useful for parents (containing information such as Vacswim, school dates, public holidays, etc.)
 - ❖ A4 size
 - ❖ Contain local and purposeful photographs (perhaps photos from last year's events)
- Residents that currently use a hardcopy calendar were more interested in this initiative. Perhaps colour-coded entries for events targeting different demographic groups might help.
- While focus group participants were generally positive about the calendar initiative, they may not be willing to replace the Resource Recovery Calendar – a publication that they are very attached to.

**“If it's useful... If it's going to compete and be a second runner
some of the other calendars we get, then it wouldn't get used..
With five or six of us in the house, everyone writes in it...”**

“It's got to be attractive”.

“Yeah, that would be good so you could plan”.

**“We've already got a calendar on our fridge...Everything gets
written on that so it would just replace that with the local events
already marked on there”.**

“There's no room left on the fridge”.

Reflections

Reflections

- The purpose of this study was to provide the City with community-led recommendations for improving communications, with the ultimate aim of improving levels of satisfaction with how the City informs residents.
- Focus group participants are generally feeling overwhelmed by the amount of information they receive. Hence, the discussion focused on improving the quality not quantity of publications.
- Most stakeholders believe that the City is doing an OK job at communicating with residents. To improve, the City's communications strategy should 1) incorporate a more human touch, 2) tackle 'hard' issues, 3) add a personal and targeted approach (by suburb or local neighbourhood, but not by Ward) and 4) engage residents in genuine, two-way communications.

"Something I would like to see is something focused more locally – targeting something in my area... not Soundings that is Council-wide".

"There's not enough. There's not enough personal contact and feeling. I don't feel like I get what Council's passion is. It's a PR exercise. It's becoming more faceless".

"Being more open about it... We always seem to hear ... the pro side of things. The Council is distinctively quiet on a number of big issues".

"I do want to know what's happening in my local area... And so sometimes feel like I have to wade through a bit of information to find out what's happening in my local area".

"I don't like too much stuff coming through the mail. But if I do get something that looks nice and is easy to read, I would prefer to read something like this (Cockburn Soundings) rather than the newspaper that looks boring... I actually think the City of Cockburn does do a fairly good job in terms of their communication..."

"I feel a distinct lack of communication on the hard questions. Part of it being the planning. But the hard stuff doesn't get communicated. At all".

"I think so much information dilutes the pertinence of the stuff you're looking for. It just comes in so many different contexts, ways, size,... and some of this can be improved by the calendar".

"I feel like Cockburn presents a typical corporation, a business thing and I would rather see a more human side. There is some aspects of that in their media but because of my thinking about community, I think the Council is in a real good place... to be more human".

We'd love to discuss this report with you!

If you have any questions, please contact:

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LOCAL GOVERNMENT ACT 1995

City of Cockburn

(LOCAL GOVERNMENT ACT) AMENDMENT LOCAL LAW 2012

Under the powers conferred by the *Local Government Act 1995* and all other powers enabling it, the City of Cockburn resolved on 13 September 2012 to adopt the following local law.

1. Citation

This local law may be cited as the *City of Cockburn (Local Government Act) Amendment Local Law 2012*.

2. Commencement

This local law comes into operation 14 days after the day of its publication in the *Government Gazette*.

3. Principal Local Laws

The *City of Cockburn (Local Government Act) Local Laws 2000* as published in the *Government Gazette* on 9 October 2000 and as published and amended in the *Government Gazette* on 13 November 2001, 15 November 2002, 26 September 2003, 25 November 2003, 27 July 2004, 17 May 2005, 11 January 2008, 10 December 2010, and 16 December 2011, are referred to as the Principal Local Law. The Principal Local Law is amended.

4. Clause 2A.1 amended

In clause 2A.1 delete the words after “between” and insert “1 November in any year until 31 May in the year following”.

5. Clause 2A.2 amended

Clause 2A.2 is amended as follows –

- (a) in subclause (1) delete “1 October” and insert “1 November”; and
- (b) in subclause (3) –
 - (i) delete “30 November” and insert “1 November”; and
 - (ii) delete “31 March” and insert “31 May”.

Dated:

The Common Seal of the City of Cockburn was affixed under the authority of a resolution of Council in the presence of –

LOGAN K. HOWLETT, Mayor

STEPHEN CAIN, Chief Executive Officer

PROPOSAL TO AMEND CONSOLIDATED LOCAL LAW FIRE BREAK PERIOD SUBMISSION BY BANJUP RESIDENTS GROUP (Inc)

12 June 2012

1. Summary

The Banjup Residents Group strongly opposes the City of Cockburn's proposal to change its Local Laws that would extend the Fire Break Period to apply between 1 November and 31 May.

The City appears to have consulted with no stakeholders in the ownership, construction, or use of fire breaks. The City has spoken with neither of the contractors who construct two-thirds of all fire breaks in Banjup; it has not spoken with the Jandakot Volunteer Bush Fire Brigade; and most importantly it has not consulted with the people who live in Banjup.

The City's proposal is badly researched, ill considered, impractical, dangerous, unnecessary, and unacceptable.

The City has published no information in support of its proposals. The City has not shown that the current Fire Break Period policy has resulted in ANY fires that were not contained or that fire fighters were hindered in their attempts to manage fires. Any reasonable person would expect to see reliable and verifiable data, not anecdotes or people's vague recollections, showing the numbers of fires in November, in April, and in May, and the percentage that were inadequately contained because of obstructions in fire breaks.

The change in policy seems to have been instigated on a whim of council officers in the expectation that no-one would respond to the obscure notice in the West Australian and so the new policy would be nodded through by elected members with little scrutiny – such is the disdain exhibited by council officers for the residents of Banjup.

In contrast to the City's approach to this matter, the Banjup Residents Group has spoken at length with the fire break contractors and with the Jandakot Volunteer Bush Fire Brigade. We have also consulted with all of our members who between them own 167 of the properties in Banjup – 40% of the total. We have also analysed weather data from the Bureau of Meteorology to assess trends in rainfall and temperature over the months of November to May and their impacts on vegetation and ground conditions in Banjup. We have also researched the Fire Break Periods of neighbouring local governments and found them all to be inconsistent with the City of Cockburn's proposals.

Banjup residents also note the double standards of the City of Cockburn. The City is seeking to apply a strict fire break regime on ratepayers while it allows tea trees and spearwoods along council verges to proliferate. For example, despite repeated pleas from residents, the City has done nothing to remedy the fire death trap along Scofield Place.

The following pages provide the factual basis for our opposition to the proposed changes to the Fire Break Period. If the City has any reliable information that is contrary to that which we provide, then it should make that information available to Banjup residents so that we can make informed assessments and respond accordingly.

2. No Consultation by the City of Cockburn

Cynically, the City has merely complied with the minimum requirements of the Local Government Act and placed a small notice in the West Australian of Thursday 19 April, which no resident saw – which was likely the intention of the City's officers. The notice called for submissions about the local law amendments to be made by 31 May 2012.

Remarkably, an officer of the City of Cockburn must have had a pang of conscience because an article about the proposed changes to firebreaks was placed in the June edition of Cockburn Soundings. However, that newsletter was not delivered to Banjup addresses until 30 May, giving just one day's notice for a submission. The Banjup Residents Group acknowledges the discretion of the City in allowing a few days' grace for the Group to make this submission.

The notice in the West Australian stated that "*a copy of the proposed local law amendments may be inspected and obtained at the City of Cockburn Administration Office and at the Spearwood, Coolbellup, and Success Libraries*". Enquiry at the Success library elicited the response that they never get copies of such documents. At Spearwood, there is a box of similar documents but the proposed local law amendment was not in it.

A search of the Cockburn web site found nothing about the local law amendments – particularly nothing under "Public Consultation". No documents published in 2012 contained the amendments. A search of the whole web site was, yet again, worse than useless.

A telephone enquiry to the Acting Manager of Community Services also drew a blank. Eventually, a copy of the proposed amendment was provided from the office of the Director of Community Services.

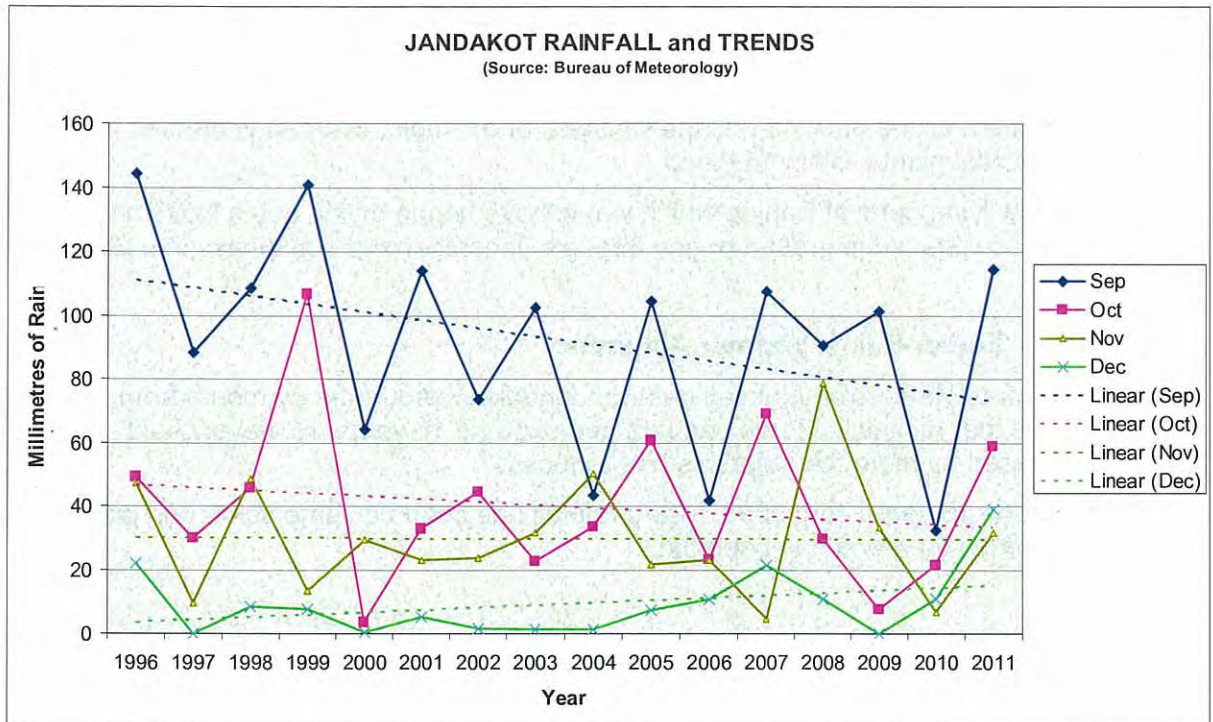
This incompetence and obfuscation further erodes our confidence that the City of Cockburn is a well run organisation.

3. Changing Weather Patterns

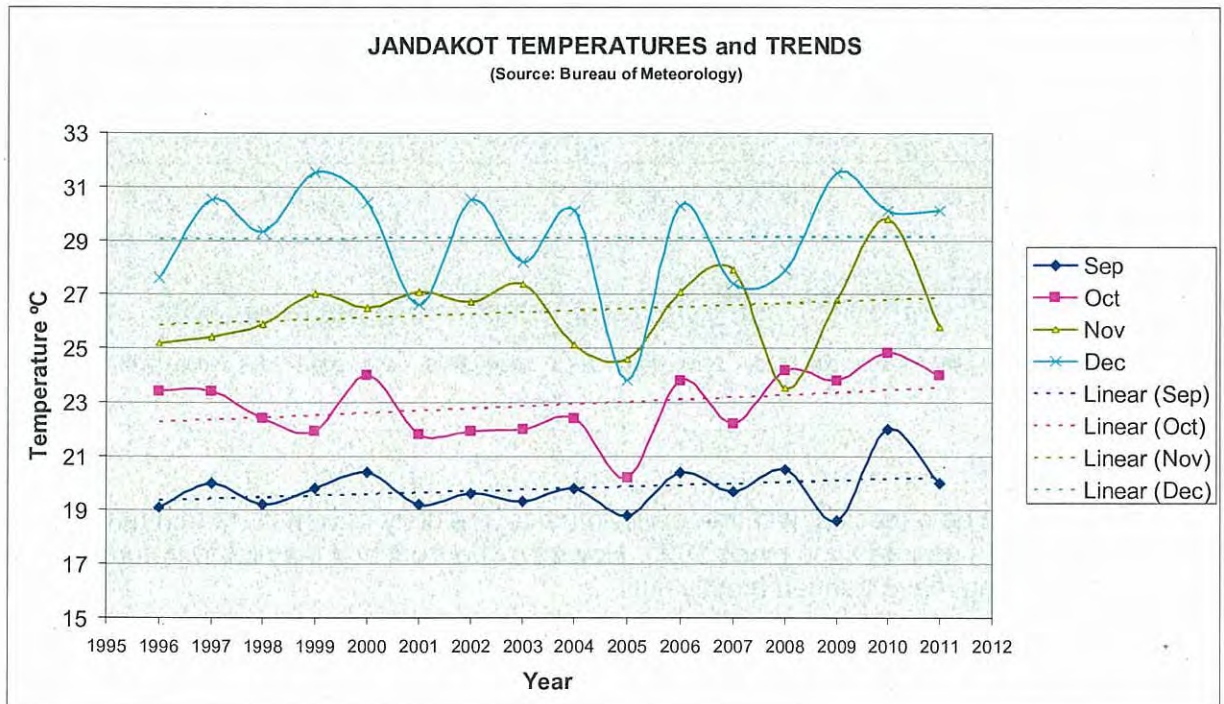
3.1. Drier Springs, Wetter Summers

Bureau of Meteorology rainfall data for Jandakot Aerodrome by month from 1996 to 2011 shows that rainfall in September has reduced markedly over the 16 years. October's rainfall has also reduced but at only one third of September's rate.

Over the 16 years from 1996, November's rainfall has been up some years and down in others but overall the trend has been flat. In contrast, December's rainfall has been increasing.



As might be expected, with the drier springs, the daily maximum spring temperatures have risen by a degree or so since 1996. However, the December temperatures, while fluctuating, have trended flat.



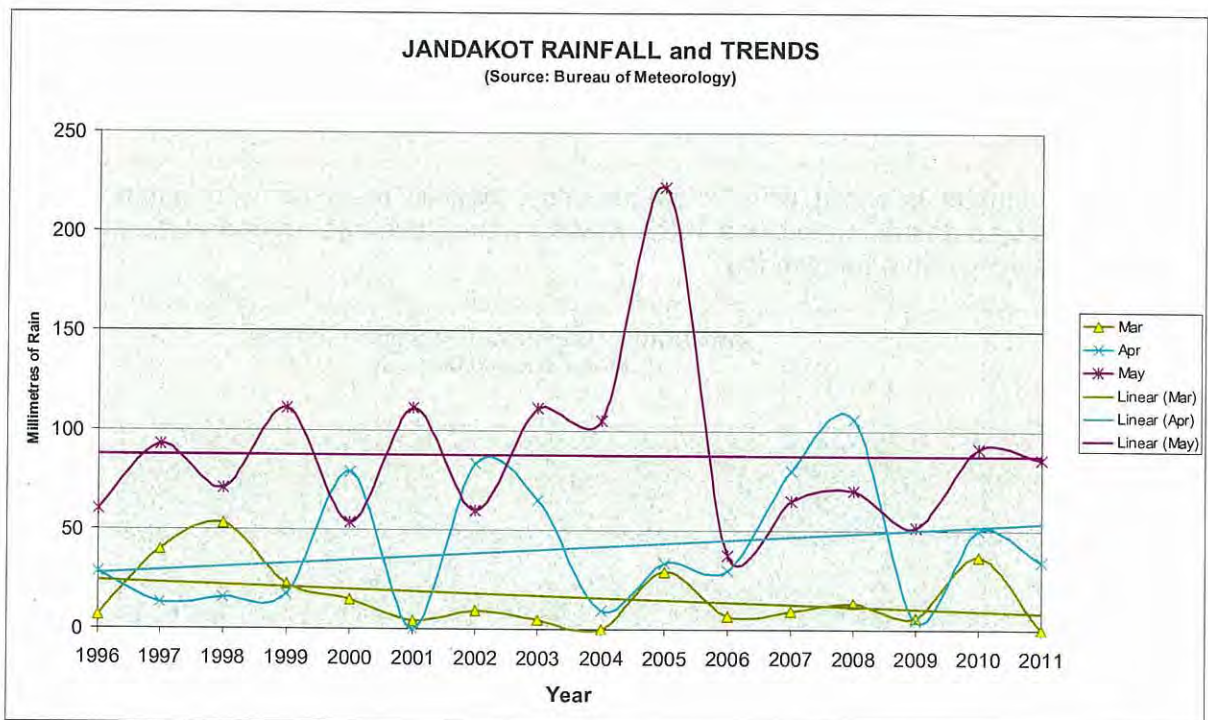
For Banjup, the effect of the changing spring weather patterns is that winter and spring vegetation growth has been drying out earlier but then in November and December there is a secondary growth spurt as late rains bring on seeds that would otherwise have lain dormant until the next year. Cape Daisies, for example, can germinate and grow into substantial plants within 14 days.

For low lying parts of Banjup which would have begun drying out in November, the later rains now leave the ground boggy through December and sometimes into January.

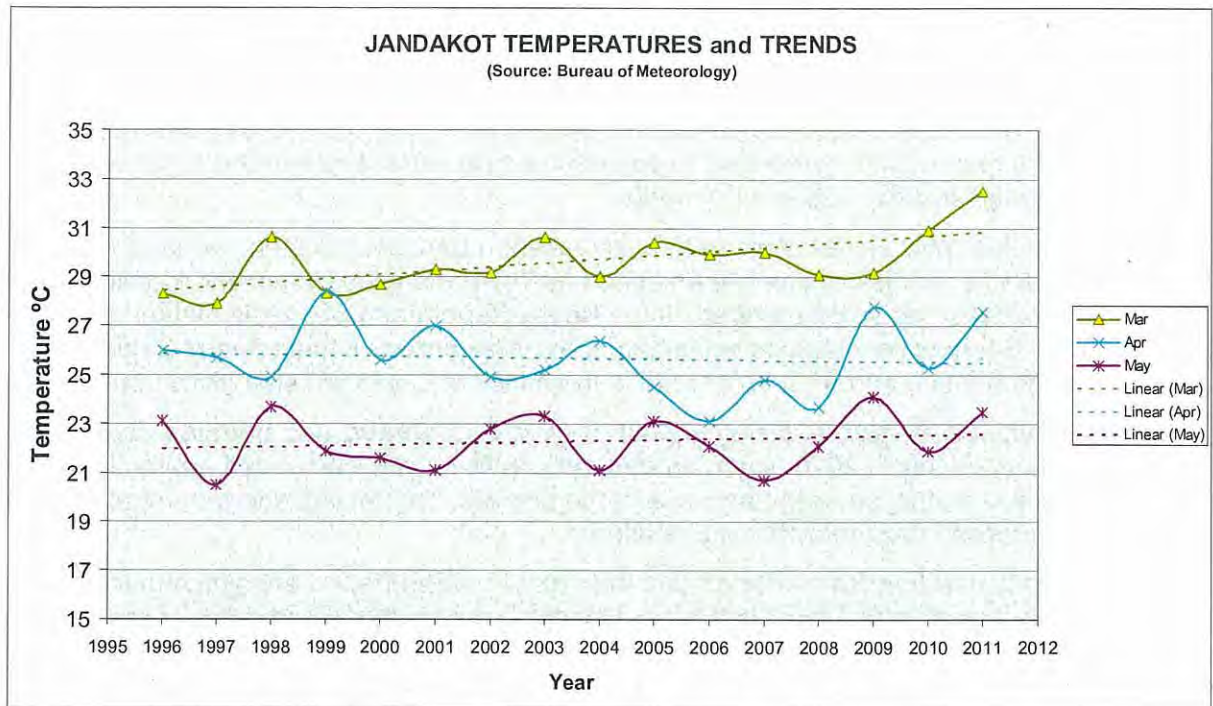
3.2. Earlier Rains, Warmer Autumns

Bureau of Meteorology rainfall data for Jandakot Aerodrome by month from 1996 to 2011 shows that rainfall in March has reduced over the 16 years. However, April's rainfall has increased by more than March's has reduced.

Over the 16 years from 1996, May's rainfall has been up some years and down in others but overall the trend has been flat.



As might be expected, with the drier autumns, the daily maximum March temperatures have risen by a degree or so since 1996. However, the April and May temperatures, while fluctuating, have trended mostly flat.



For Banjup, the effect of the changing autumn weather patterns is that the early rains promote vegetation growth in April and May. This is most noticeable as weed growth on fire breaks. As in late spring, Cape Daisies germinate and grow into substantial plants within a couple of weeks.

4. Fire Break Contractors

The City of Cockburn’s web site lists 2 contractors (nu-track and Jandakot Earth Moving) who construct firebreaks in the City’s area. These contractors maintain about two-thirds of all fire breaks in Banjup. Apparently, the City of Cockburn has not consulted with either of these contractors about changing the Fire Break Period.

Discussions with each of them reveal that they typically start clearing Banjup fire breaks in mid-October and complete by December. Starting earlier is impractical because much of Banjup is still wet and their machines could get bogged. Further, late rains after a fire break has been cleared could promote regrowth, making a further clearance necessary just a month or so after the first clearance.

The contractors also indicate that some parts of Banjup remain a “quagmire” until January or even February and so constructing firebreaks before then is impractical.

Apparently, some years ago the City of Cockburn mandated a start to the Fire Break Period of 15 November but had to revert to 30 November because the contractors found it impossible to start the clearance work early enough to ensure a completion by the required date.

5. Standardising Dates

The City of Cockburn in its notice to amend its local law states that:

"The effect of the amendment will be to standardise the dates by which these measures will be required to conform and be consistent with similar provisions of neighbouring local governments."

It is unclear why there is a need to align vacant urban block clearing periods with those of rural blocks. Cockburn has just amended its Town Planning Scheme to identify "Bush Fire Prone Areas", as recommended by the Keelty Report, and Banjup is marked as one of those areas. So, how can there be uniformity with urban areas? If there is any confusion about the arrangements for urban and for rural in the minds of Cockburn staff, then refer to the map.

The rationale for standardising appears only to be **bureaucratic tidiness**. Banjup residents experienced this in 2010 when "*procedural practices*" were amended arbitrarily by council officers to restrict severely the issue of fire permits. The restrictions were rescinded in 2011 after intense lobbying by Banjup residents.

Currently, the Fire Break Period and the Total Fire Ban Period are synchronised, or "standardised" to use the City's term. Changing the start and end dates of the Fire Break Period would certainly cause confusion for many in Banjup. If the City were to use any change to the dates of the Fire Break Period as a precedent for changing the dates of the Total Fire Ban Period and the concomitant denial of fire permits, then Banjup residents would be even more alarmed at the City's behaviour.

Enquiries of neighbouring local governments reveal no plans for bringing forward the opening of the Fire Break Period in their areas. The matter has not been raised with the District Operational Advisory Committee of Fire Chiefs for neighbouring local governments. The current Fire Break Periods for neighbouring local governments are these:

	Start	End
Armadale	30 November	14 March
Gosnells	30 November	30 April
Kwinana	30 November	31 May
Murray	30 November	31 March
Rockingham	30 November	31 May
Serpentine Jarrahdale	30 November	31 May

Cockburn's intention to change its Fire Break Period to 1 November to 31 May would be consistent with none of these.

If Cockburn is seeking standardisation with neighbouring local governments, then it should look no further than its fire permit policy, which is completely at variance with its neighbours, as the Banjup Residents Group has repeatedly pointed out.

6. Impractical, Ineffective, and Dangerous to Start on 1 November

Over 250 fire breaks in Banjup are constructed by the two main contractors in the area. Neither of these nor any other contractors can start work until mid-October because Banjup ground conditions are too wet for heavy machinery. Allowing only 2 weeks for the construction of fire breaks is clearly impractical. Those property owners who had booked a contractor to build a fire break for them but for whom the contractor could not attend until later in November could be served an infringement notice.

Even if a fire break were cleared for 1 November, the late rains now common in November and December could promote regrowth, making the initial clearance ineffective, and so require further clearance during December or risk infringing local laws.

Further, the City now has a policy of issuing infringement notices without sending a warning letter first. Thus, a property owner who met the fire break criteria on an inspection in November could unexpectedly be issued an infringement notice in December. Such penalties would anger ratepayers who would wonder if the change of start date were just a revenue raising exercise.

Fire fighters use the fire breaks for vehicular access to properties under threat of fire. If a fire break is repeatedly ploughed, then the sand becomes very soft and fire trucks can get bogged, putting men and equipment in danger.

7. Extra Costs for City of Cockburn and Its Ratepayers

The City of Cockburn will not intend to extend the Fire Break Period without requiring compliance throughout the whole period. This will require more Ranger inspections, possibly two or three per property over the extended period as vegetation will grow during the extra months. Cockburn currently assigns 2 Rangers to inspecting fire breaks for the current December start. To enforce compliance during the extended period, the City would need to engage 2 or 3 more Rangers. These would be paid for either by increasing rates or by issuing more infringement penalties, or both. Again, this would ire Banjup ratepayers.

8. Unnecessary Extension into Autumn

While some bush fires do occur in April and May, there is no evidence that vegetation growth on fire breaks during those months hinders access by fire fighters. Mostly, any growth will be weeds that will be green and not a fire hazard: fire fighting vehicles can easily traverse them. It is not necessary for fire breaks to be reduced to '*mineral earth*' during April and May.

If the City of Cockburn were to argue that they would not issue infringements during April and May for weeds on a fire break, then why do they propose to extend the Fire Break Period into those months? If the City were to argue that fire breaks might be obstructed by something other than weeds during April and May, then it should provide reliable statistical evidence of this occurring during those months, together with an analysis of how the obstructions hindered fire fighting, if at all.

The 424 property owners in Banjup need assurance that policy is based on facts, not on hearsay or imagination. Laws must be necessary and sensible to be accepted by the broad community – otherwise we would have laws about which end of a boiled egg to open.

9. Conclusion

The City of Cockburn's proposal to extend the Fire Break Period does not stand scrutiny. No stakeholders were aware of the proposal, let alone consulted about it. If a few hours' research had been put into the proposal, as the Banjup Residents Group has done in preparing this submission, then it would have been clear that bringing the opening of the Period forward to 1 November is fraught with difficulties and pushing the closure of the Period back to 31 May is unnecessary and expensive.

ROYAL AUSTRALIAN NAVY

AUSTRALIAN NAVY CADET HEADQUARTERS



CAMPBELL PARK OFFICES
CP4-7-127, CANBERRA ACT 2600

DANC/OUT/2012/R12116867

Mr Rob Avard
Manager Community Services
City of Cockburn
PO Box 1215
Bilba Lake DC WA 6965

Dear Rob,

TS COCKBURN – RELOCATION PROPOSAL

Please find enclosed Statement of Requirement for Training Ship (*TS*) *Cockburn* supporting the cadet unit's proposed relocation to the vacated Cockburn State Emergency Service building, Kent Street, Spearwood, for your consideration.

The enclosed document has been signed by Commodore Geoff Geraghty, who is the Director General Australian Navy Cadets.

Yours sincerely

A handwritten signature in black ink, appearing to read 'R.A. Hefsey', is written over a circular stamp or seal.

R.A. HEFFEY
CMDR, RAN
Director Australian Navy Cadets

Tel: 0467 734 423

Email: bah.h@defence.gov.au

22 Aug 12

Enclosure:

1. Proposed Unit Relocation - Requirement for ANC Unit Training Ship Cockburn

PROPOSED UNIT RELOCATION - REQUIREMENT FOR AUSTRALIAN NAVY CADET UNIT TRAINING SHIP (TS) COCKBURN

Title

Proposed relocation of Australian Navy Cadets *Training Ship (TS) Cockburn* – Unit Training Facilities from the South Coogee Agricultural Hall, Munster to the Cockburn State Emergency Service building, Kent Street, Spearwood.

Requirement Description & Aim

1. The requirement is to provide suitable permanent training facilities for the Australian Navy Cadet (ANC) Unit *Training Ship (TS) Cockburn*.

Justification

Background

2. The Australian Navy Cadets (ANC) is a voluntary youth organisation open to males and females aged between 12½ and 20 years of age. The ANC is administered by the Royal Australian Navy (RAN) and is part of the Australian Defence Force Cadets (ADFC), the aim of which is:

By shared contribution and commitment from the Australian Defence Force and the community, the Australian Defence Force Cadets will operate as a community based youth training organisation, which stimulates interest in the Australian Defence Force, while providing opportunities to develop personal and team qualities of benefit to the individual and the community.

Cadets attend weekly parades at their unit premises at programmed times and undertake weekend training at their unit, other units or in HMA Ships and Establishments. *TS Cockburn* parades every Friday evening from 1900 to 2200. Weekend training occurs as required, enabling cadets to engage in maritime activity based on a sail training program. The ANC is a Recognised Training Organisation (RTO) and as such offers its members nationally recognised qualifications in courses that they undertake within the ANC structure. Cadets also carry out annual continuous training periods of up to seven days at Commonwealth expense utilising accommodation within Defence Establishments where available.

3. Defence Force Cadets, comprising Navy, Army and Air Force Cadets are an important part of the youth activity structure in many communities around Australia and are often considered the best youth development organisation in the country.

4. *TS Cockburn* is currently located at the South Coogee Agricultural Hall next to the Challenger TAFE and Maritime precinct in Munster. The unit currently comprises 2 staff and approximately 15 cadets. The unit recently enrolled its 104th member but has been unable to retain a high membership to date due to the current isolated location. The majority of cadets who transfer out of the unit state that the unit's location is the major deterrent in continuing their association with cadets. With a move to the proposed location in Spearwood with much more functional facilities, numbers will almost certainly increase steadily as *TS Cockburn* is able to provide a more hands on and varied training programme which is more accessible to the communities youth and their families.

5. From a recruiting perspective, the ADF has benefited from *TS Cockburn* with a number of former cadets having taken up careers in the ADF, in particular Navy. Many of the Defence Force's senior leadership, including the former Chief of Navy, Vice Admiral Russ Crane, AM, CSM, RAN are former cadets. Many cadets also participate in other community, sporting, service clubs and committees during and following their tenure with *TS Cockburn*. The unit participates in a wide range of community activities and is held in high esteem within the general community.

Current Situation

6. A well established Navy Cadet Unit in the Cockburn area will continue to provide much needed, well directed activity and learning opportunities for young people in this popular and growing region. Involvement with cadets will not only enhance their life skills but will foster interest in the Australian Defence Force with a good number of cadets moving on to a career in the Services. Cadets, from the community's perspective, will be offered a structured, safe learning environment to enable them to obtain recognised certificates, skills and knowledge in but not limited to: Senior first Aid, Recreational Skipper Ticket and an appreciation of what is socially acceptable within Australia as a whole.

7. The majority of Navy Cadet Units are 'community' based - much more so than Army and Air Force Cadet Units that are more often school based or accommodated in Defence establishments, particularly Army Reserve Depots, around the country.

8. Being a maritime based youth organisation, it is necessary that *TS Cockburn* maintains liaison with other ANC units and like minded organisations with close access to safe waters for small boat sailing and other watercraft activity. As this is the principle activity for Navy Cadets *TS Cockburn* and unit supporters work hard to provide opportunities for cadets to participate in water based activity. Navy and the ANC organisation desire to work closely with Defence Support Group and other Services and Cadet Corps to see that *TS Cockburn's* needs are included in relocation plans.

Concerns

9. Failure to obtain suitable accommodation for *TS Cockburn* will severely impact on the unit's ability to deliver a viable youth-training program and thereby meet the aim of the ADF Cadets.

10. The community and the ADF, in particular Navy, would lose an organisation in Cockburn that has the potential to become one of the community's most active and successful youth organisations, enhances the image and reputation of the RAN and the broader ADF, and is also a strong recruiting base for the ADF.

Requirements

11. Typical facility requirements for a cadet unit are listed in the Defence Cadet Policy Manual, namely:

Typical Requirements for Cadet Unit Accommodation

The typical requirements shown are representative of a new sole-occupancy Cadet unit. The individual needs of different Cadet units need to be taken into account on a case-by-case basis.

Serial	Facility	Comments
1	Commander's Office	Normally an office dedicated for sole use
2	Orderly Room	Normally a dedicated space but may be shared. Typically 2-3 work-stations
3	Training Office	Normally a dedicated space but may be shared. Typically 2-3 workstations. May be combined with the Orderly Room.
4	Store	Must be a dedicated and secure space. May be a dedicated, secure area within a larger stores complex controlled by others. Separate cells for general stores, uniforms, and hazardous materials.
5	Classrooms	Sized to fit required instructional groups. An ability to seat the full complement in a single room may be required, normally best met by opening interconnecting classrooms. Special needs such as Air Force computer flight training may be provided.
6	Sleeping accommodation	Provided only where overnight activities are conducted routinely (eg Navy activities, Air Force courses) and alternative sleeping arrangements are not available. Separate rooms for males and females for both cadets and staff. Bunk/dormitory for Cadets. Staff may share (up to four per room). In many cases, Cadets are able to sleep on the floor of the facility providing the accommodation meets BCA compliance for over-nighting.
7	Kitchenette / brew area	A kitchen/ galley may be provided when sleeping accommodation is provided or when required for cook training.
8	Toilets/ showers/change room/washing machine/dryer	Separate for male and female cadets. Staff (unisex acceptable) should be separated from cadets if possible, particularly where sleeping accommodation is provided.
9	Outdoor training shelters	As required.
10	Boat and equipment storage	Navy. Ability to access and use the launching ramp as required.
11	Radio room	Navy. As required. May be part of Serials 2/3.
12	Parade ground, flag pole, signage, car park	As required. Part of the car park may be used as the parade ground.
13	Armoury	Not normally provided for Cadet facilities. Access to an ADF armoury is required where Cadet units are authorised to access innocuous Service firearms. The Cadet Bolt Action Training Rifle (BATR-C) will be stored in accordance with Cadet Firearms Training Policy (FTP).
14	Telephone	Two telephone lines provided. One for CadetNet and one for telephone access.
15	Security	Units are to be provided with fences and security systems consistent with their requirements.
16	CadetNet system	One to each Cadet unit
17	First Aid Kit	One to each Cadet unit

12. In order of priority, the requirements for a unit facility for *TS Cockburn* are as follows:

- Classrooms - 4 x 10 to 12 cadets, or.
- Drill hall, multi-use as classrooms/dining hall. Classrooms/Drill hall could be used for

- CO's office with workstation
- Training office with workstations (unit communications as required to be located here)
- Stores -- clothing, watercraft equipment, cleaning and flam.
- Boat storage
- Kitchenette
- Sleeping accommodation
- Parade Ground
- Sick Bay/First Aid Room

Stakeholder Consultation

13. This submission has been prepared in consultation with *TS Cockburn* staff, parents and members of the Unit's Support Committee and Director General Australian Navy Cadet (DGANC) Staff.

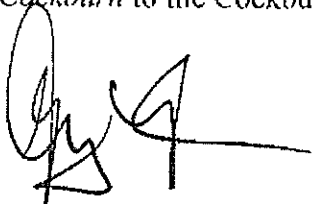
Impact Statement

14. Failure to obtain suitable accommodation for *TS Cockburn* would severely impact on the unit's ability to meet its youth training obligations to its existing cadets and potential new cadet recruits. *TS Cockburn* would not be able to meet the aims of the ADFC stated at paragraph 2.

15. *TS Cockburn* provides a safe learning environment for families seeking a financially viable option for their children. All uniforms are supplied free of charge to each cadet on enrolment and are replaced when required. The majority of camps and activities are funded by Defence or paid for by the unit's funds removing the financial burden on families.

Authorisation

16. I confirm the requirement outlined in this submission and that this initial document provides sufficient and accurate information to allow further consideration in the relocation of *TS Cockburn* to the Cockburn SES building situated in Kent Street, Spearwood.



G.J. GERAGHTY,
Commodore, RANR
Director General Australian Navy Cadets

CP3-7-094

Tel: (02) 6266 3873

22 August 2012

Contact Officer Details:

User Representative:

LCDR Allan Vidler, RANR
Deputy Director Facilities
Navy Cadet Support- Cerberus
Gunroom
HMAS CERBERUS
WESTERN PORT VIC 3920

Tel: (03) 5931 5804

Fax: (03) 5931 6280

Email: allan.vidler@defence.gov.au

7 August 2012

Cockburn Council

To whom it may concern,

Re: The relocation of TS Cockburn from its current location at
Rockingham Road to Kent Street, Spearwood.

My name is Debbie Inkley and my son has been with TS Cockburn as a Navy Cadet for the past 10 months. I am writing to support the relocation of the Unit to Kent Street, Spearwood as I believe it would be a much better location for a multitude of reasons.

The current location on Rockingham Road is very isolated with minimal public transport available. Cadets attend weekly parades at their Unit and parents are required to drive their child/children to and from the Unit due to this factor. If the Unit were to move the Kent Street, public transport is far more accessible at that location, meaning that cadets would be able utilise that public transport.

The current location of the Unit is also unsecured, i.e. no fencing is around the premises at all. The lack of security means that the cadets are unable to undertake weekend training. Where as the Kent Street premises is fully fenced and secure and would therefore be suitable to conduct these weekend training camps.

The Unit also has a number of boats and watercraft equipment but due to the unsecured current location these boats and the equipment are unable to be housed there and are housed off site. Relocating to Kent Street would enable the boats and watercraft equipment to be housed at the Unit premises, thereby enabling more frequent usage and enhancing cadets' knowledge and training in this area.

The current building is also quite small and is inadequate for the requirements of the efficient running of the Unit. There is no room to have separate class rooms for the cadets. There is no separate office for the Commanding officer, nor is there any separate area for the Admin team to carry out their duties. With one big open space it becomes quite difficult to have several separate activities being conducted at one time without losing focus or being distracted.

The Kent Street premises would be ideal as there is a separate dwelling which could be used as the Admin area which would enable those duties to be carried out without interfering with cadet training and activities. The main building would also be ideal as I understand the setup with its separate training rooms and offices would also be ideal for the Units requirements.

Australian Navy Cadets has proven to be a very successful youth training program within the community and by relocating to Kent Street the Unit would be closer to the community and thereby attract more youth to the organisation.

Please consider the relocation as imperative to the ongoing importance of the Navy Cadet organisation.

Thank you for your consideration in this matter.

Yours sincerely,

A handwritten signature in black ink, appearing to read "Inkley", with a stylized flourish at the end.

Debbie Inkley

August 8th 2012

Cockburn Council

To whom it may concern,

Re: The relocation of TS Cockburn from its current location at Rockingham Road to Kent Street, Spearwood.

My name is Michelle Moody and since September 2010 my son has been a Navy Cadet with T.S Cockburn. I am writing as I wish to support the relocation of the Unit to Kent Street, Spearwood.

I believe the Australian Navy Cadet program is a program that can enhance our children's lives by way of education, training and life skills and I believe that relocating to the Kent St Building would give them further opportunity to enhance themselves and grow better as a unit. The below reasons are why:

1. Public transport

Currently the location on Rockingham road is isolated with minimal public transport; Due to this some cadets find it difficult to attend parade nights. The Kent Street location would provide more adequate Public transport which would therefore allow the cadets more opportunity to attend.

2. Fencing / security

The cadet unit has a number of boats and watercraft equipment and with the lack of lockable fencing around the Rockingham road location; these have to be kept offsite where they are not easily accessible. Relocating to Kent Street would enable the boats and watercraft equipment to be housed at the Unit premises, thereby allowing more frequent use and enhancing the cadet's knowledge, training, skills and confidence in this area.

As there is no lockable fencing around the unit currently we are unable to conduct weekend training camps, Kent Street would allow this to happen.

3. Building size

Currently the building size is inadequate for the requirements of efficient running of the Unit.

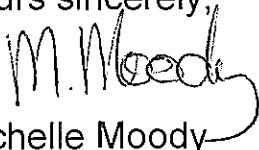
There is no space to have separate class rooms for the cadets; there is also no separate office for the Commanding officer or for the Admin team to carry out their duties. With one large open spaced area it becomes quite difficult to have several separate activities being conducted at one time without losing

focus or being distracted. The premises on Kent Street would be ideal as there is a separate Building which could be used for administration; this would enable those duties to be performed without interfering or distracting the cadets from training and activities. The main building itself already has separate classrooms and offices spaces that would be ideal for the unit's curriculum.

T.S Cockburn and The Australian Navy Cadets have proven to be a successful training program within the community and I believe that relocation to Kent Street would benefit the cadets and families of our unit, it would also be closer to the community and therefore attract more youth to the organisation. Please consider this relocation.

Thank you for your consideration in this matter.

Yours sincerely,

A handwritten signature in black ink that reads "M. Moody". The signature is written in a cursive style with a large, looping flourish at the end of the name.

Michelle Moody

RELOCATION RATIONALE

TRAINING SHIP COCKBURN

TITLE: Rationale for re-locating Australian Navy Cadet unit to Kent Street, Spearwood

Number of participants:

- At present we have 17 cadets, male and female – aged 12½ to 18 years of age. We have had over 100 cadets join since our unit was established; the majority of cadets, who have left/transferred, sited the isolated location of the unit being the reason for leaving our unit
- Envisage increase in numbers, at new location, to be upwards of 50 cadets within first 12-18 months of unit relocation

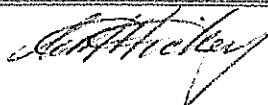
Value your organisation will gain from the new location:

- Retention of existing cadet numbers
- Increase in cadet membership – location more amenable to the communities families
- Location not complicated for members to access entry/exit points
- Street lighting more evident, providing a sense of security for cadet members
- The unit itself would be located in a populated residential area
- Positive exposure to the communities youth – located near sporting fields, busy shopping complex and nearby schools allows community to view cadets in training
- Close to a recognised bus route – alleviates member's family transport issues
- Increase in secured area – facilitates our ability to host other ANC units or hold/host Bi/Tri-Service events/activities and/or training
- Classroom arrangement enables cadets to be trained separately in their rank appropriate course
- Oval/playing fields behind can be utilized for activities and/or sedate exercise programme when available
- Already used as a training facility so has enhanced services/features to what we have available at present
- SES building is already a lock down area to the public – this would not change with our unit; consequently would not take a venue away from the community
- Cadets would be able to train for and host Mess Dinner's/events for council and community groups, especially the Cockburn Returned Services League, on an annual basis
- Larger illuminated bitumen area more effective for Parade and Ceremonial Drill
- ANC is a RTO with staff members qualified to conduct courses in Sail (GISBS 1 & 2), Power Boating (Recreational Skipper' Ticket), Senior First Aid, Food Handling and cooking the building will allow us to conduct these courses on a larger and more regular basis

How you see increasing participation:

- While our present location is a good community hall type facility and has been our home for a number of years, it is not really at all functional for a cadet unit and has limited our operations
- Relocating to the S.E.S. building in Kent Street, Spearwood is more central to the cadet "catchment area", it has a good layout with training rooms, office space, galley, ablutions, locked storage, large caged area for stores/boating equipment and a good sized car park with lighting for a parade ground
- The relocation will enable building and delivering a more structured training programme by offering more in terms of "hands on" for our cadets and through word of mouth will effectively increase our numbers
- Storing all of our equipment in the one location will assist staff and senior cadets with lesson planning and instruction, enhancing the training standards of our members which would be reflected when cadets are assisting with community events such as Anzac Day

Lieutenant (ANC) C. Hickey 9332 3929



Page 1

TRAINING SHIP COCKBURN

- As the S.E.S. building is located more centrally to regular community activities we would have a greater exposure to the community when conducting weekly parade and weekend training programmes for our cadets
- The majority of our training for water activities is theory based; cadets have to undergo a large amount of land based training before being allowed to venture out on the water. We have many areas where we can launch our boats – other water based cadet units, Jervoise Bay Sailing Club and Challenger Beach – so this does not present a problem for our unit. What makes it possible for us to go sailing is that at the S.E.S. location we will have our water craft on hand thereby reducing the time taken to collect from and return to another location.

Proposed use of space and for what purpose:

- Caged Stores Area:

Uniforms – diverse sizes, kept on hand for new members and replacements for cadets

All cadets are supplied with approx cost \$3,000.00 uniform kit and bag.

Standard issue uniform per cadet:

- ✓ Shoes/boots – sailing shoes, general purpose cadet boot, parade shoes, safety boots
- ✓ Socks – combat and black dress socks
- ✓ Pants – long black, long white and DPNU trousers
- ✓ Belt – white with silver buckle, black with silver buckle
- ✓ Shirts – short sleeved white, long sleeved white, DPNU shirt, grey T-shirt, white front
- ✓ Wool knit jumper
- ✓ Dress Battle Jacket
- ✓ Tie
- ✓ Rain Jacket
- ✓ Summer Ceremonial Uniform complete
- ✓ Winter Ceremonial Uniform complete
- ✓ Junior Sailor Cap, Unit baseball cap, ANC baseball cap, DPNU hat, Legionnaires cap
- ✓ ANC issue sunglasses
- ✓ Blue kit bag
- ✓ ANC lever arch file with dividers

Watercraft equipment –

- ✓ Personal Flotation Devices (P.F.D.) – diverse sizes
- ✓ Wet Suit long sleeved sailing tops – diverse sizes
- ✓ Emergency boating equipment – bailer, flares, EPERB, fire extinguisher, etc
- ✓ Anchor, chains, ropes, oars, rowlocks, shackles, etc
- ✓ Sunscreen
- ✓ Communication equipment
- ✓ First aid equipment
- ✓ Sailing helmets – diverse sizes

Camping equipment –

- ✓ Camp Stretchers
- ✓ Bedding
- ✓ Stoves
- ✓ Camp cookware, utensils
- ✓ Tents
- ✓ Tables, chairs
- ✓ Compasses, torches, hydration packs
- ✓ First aid equipment

TRAINING SHIP COCKBURN

- Vehicle Bays 1, 2 and 3: Onsite storage of our watercraft – we have 10 watercraft at present
 - ✓ 1 x Rescue Boat on single trailer
 - ✓ 2 x Corsair Sail craft on double trailer
 - ✓ 1 x Mirror Sail craft on single trailer
 - ✓ 1 x dinghy on single trailer
 - ✓ 1 x Optimist Sail craft on beach trailer
 - ✓ 2 x Aqua Gliders
 - ✓ 2 x two man canoe's
 - ✓ 2 x additional outboard motors for training purposes
 - ✓ Main and Jib sails
 - ✓ Main and jib sheets
 - ✓ Diverse range of sailing and boating attachments

- Training Room: will become Classroom 1
 - ✓ Same rank only for appropriate course at one time
 - ✓ Theory based training utilizing whiteboards, power point presentations, overhead projector, and audio visual equipment.
 - ✓ Storage of training aids, reference books
 - ✓ Cadets to keep their task books, personal property in this area

- Operations Room: will become Classroom 2
 - ✓ Same rank only for appropriate course at one time
 - ✓ Practical training in Seamanship - rope work, navigation

- Communications room: will become Classroom 3
 - ✓ Same rank only for appropriate course at one time
 - ✓ Band training and
Storage of Band equipment –
 - ✓ Base drum with case
 - ✓ Tenor drums with case
 - ✓ Snare drums with case
 - ✓ Band Master's Mace
 - ✓ Drum harness's

- Administration room and Foyer: will become Mess Deck (dining area)
 - ✓ Cadets to use for Stand Easy and meals

- Rooms off Foyer and opposite Communications room: will become administration offices
 - ✓ Commanding Officer and Armoury storage
 - ✓ Executive Officer
 - ✓ Administration Officer
 - ✓ Training Officer
 - ✓ Regulator/Welfare Officer
 - ✓ Gangway

- Kitchen (Galley) and Ablutions (Heads): will remain as stated on current floor plan
 - ✓ Galley will be utilized, at a later date, for cooks and food handling courses

- Stand alone single brick building currently used by S.E.S. members as their "break" room: will become the Sick Bay in conjunction with the Unit Support Committee's offices, storage and meeting room

TRAINING SHIP COCKBURN

- Shed alongside single building: will become the workshop for maintenance and repairs to watercraft and equipment, storage of both ancillary outboard motors, fuel and miscellaneous equipment
- Grassed area: will be utilized for
 - ✓ land survival training
 - ✓ camping
 - ✓ rigging/de-rigging sail craft
 - ✓ cleaning of water craft and equipment
- Carpark: will become the parade ground and used for
 - ✓ Parade and Ceremonial training
 - ✓ Lessons based on physical activity
 - ✓ Colours and Sunset Ceremony

Audited financial statements:

- At present we are waiting on our Accountant to audit our books, this will be carried out in the coming weeks
 - ✓ As per our latest bank statements we have:
 - Unit Committee Account - \$11,500.00
 - Unit Welfare Account - \$ 2,900.00
- Over the past 5 years we have maintained a combined credit balance of over \$10,000.00

How will the cadets pay outgoings and any other costs that will arise:

- Income:
 - ✓ Cadet Funding W.A. – receive \$66.00 per cadet annually
 - ✓ Cadet Fee's – receive \$30.00 per cadet per school term
 - ✓ Donation – receive \$500.00 per year for assisting Jetty to Jetty
 - ✓ Fundraising – receive minimum \$1,000.00 per year for chocolate sales
 - ✓ Committee conduct fund raising events each term
 - ✓ Defence Services – pay unit utilities, supply equipment and materials for training
 - ✓ Offer Senior first aid and other certified courses to the wider community utilizing funds raised for cadet unit
- Expenditure:

Unit Welfare and Unit Committee fund the cadets for –

 - ✓ 8 day Annual Continuous Training camp inclusive of their courses
 - ✓ Senior First Aid
 - ✓ Recreational Skipper's Ticket
 - ✓ Getting into Small Boats Sailing
 - ✓ Unit camps
 - ✓ Insurance – subsidized by Defence Services
 - ✓ Defence Services pay unit utilities – water, electricity, phone, internet
 - ✓ Servicing Fire Extinguishers – Defence Services pay all costs
 - ✓ Boat and Trailer Licences – paid by committee
 - ✓ Sail craft and double trailer licence – paid by Defence Services
 - ✓ Lease – Peppercorn through Council

TRAINING SHIP COCKBURN

Benefits of your service to the community:

- Compliments Youth Services already available in the City of Cockburn
- Ability to offer a more fulfilling programme to our members – we offer discipline, leadership, self confidence and provide certified courses to assist the youth in gaining employment, e.g. Senior First Aid
- ANC is a RTO with staff members qualified to conduct Senior First Aid and other certified courses which would be offered to the wider community at a reduced fee
- Our term fees are very low at \$30.00 per 10 weeks
- We supply all uniforms and equipment at no cost to the cadet or their family
- We offer families within the community a safe, affordable learning environment where their child can become a productive, respectable member of society
- We provide safe learning experiences for our members on Defence vessels and establishments, opportunities that are not available to the general public
- We support and encourage our Codes of values:
 - ✓ Honour: This is the fundamental value on which the ANC, and each person's reputation, depends. This demands honesty, courage, integrity, loyalty and behaviour which is becoming and worthwhile.
 - ✓ Honesty: This means always being truthful and always doing what is right for the ANC and the individual.
 - ✓ Courage: This means having the strength of character to do what is right in the face of adversity, danger or threat.
 - ✓ Integrity: This means the display of truth, honesty and fairness that gains respect and trust from others.
 - ✓ Loyalty: This means being committed to each other, the ANC, the community and to Australia.
- We support and encourage our Guiding Principles:
 - ✓ Justice: Encompasses the fair treatment of individuals, the application of due process, impartial decision-making, non-discriminatory actions and equitable outcomes. Power and authority are applied for the common good
 - ✓ Respect: Recognises and defends the rights of others. That is, treat others with respect and do not abuse, exploit or discriminate against them; and
 - ✓ Responsible care: Based on the primary moral obligation of doing good, rather than harm, and the concept of responsible stewardship. People recognise their responsibilities towards others and contribute to the well being of others and to the common social good.
- Cadet organisations exist for the benefit of the youth that enrol, where every consideration is given to advancing their development. The contribution of the adult staff, the community and the ADF serve to facilitate the youth focus
- The military orientation manifested by the wearing of uniform embellishments, carriage of firearms, drill, ceremonial, the emphasis on military characteristics and training in military subjects is essential and must be retained to allow distinguishing from other youth organizations within the community
- The ANC is supported by shared contribution from the ADF and the community. Equity suggests that the ADF should provide the resources needed for all cadet units to undertake prescribed, mandatory activities; while the community provides resources needed for all approved, discretionary activities. Equity also requires that once established all cadet units should be treated equally.
- We have a Divisional System – basic personnel management organization – within the unit responsible for the training, welfare, discipline and morale of its members designed as a mentor support, development and social structure.

TRAINING SHIP COCKBURN

Letter of support from the navy to the proposal:

- Defence Services – Being processed at present ✓ *included with*
- Unit's Parents – two parents provided letter of support

Composition of committee of management:

- Incorporated Unit Support Committee is designed to access resources, provide support and ensure strong links are maintained and sustained with the local community through supportive activities and positive communication
- The Unit Support Committee (U.S.C.) is formed from parents/guardians and interested members of the community, who can constructively support the objectives of the Committee, on the invitation of the unit's Commanding Officer
- ANC staff and/or cadets are not members of the Unit Support Committee
- The U.S.C. facilitates raising of resources and funding to enable cadet activities that enhance youth development
- Fundraising to support U.S.C. operational expenses
- To provide and maintain equipment not issued by the Commonwealth Government
- Provide insurance for property owned and controlled by the U.S.C., inclusive of property that is on loan or not covered by Defence Insurance
- The Unit Support Committee comprises of: President, Vic President, Secretary, Treasurer and committee members
- The U.S.C. conducts meetings once every school term to ensure forward planning and an Annual General Meeting to elect the executive committee
- The President meets with the unit Commanding Officer on a weekly basis to ensure smooth running of the unit

Insurance coverage:

- Department of Defence provides insurance cover for all units under COMCOVER Insurance, as per attached documentation:
 - ✓ Certificate of Currency – Directors' and Officers' Liability
 - ✓ Certificate of Currency – General Liability and Professional Indemnity
 - ✓ Certificate of Currency – Personal Accident
 - ✓ Certificate of Currency – Property
- Unit Support Committee provides Public Liability insurance cover for Training Ship Cockburn under QBE Insurance (Aust) Limited through OAMPS Insurance Brokers, as per attached documents:
 - ✓ OAMPS Insurance Brokers letter dated 8th March 2012
 - ✓ OAMPS Insurance Brokers Tax Invoice Number I167260 dated 08/03/12
QBE Policy Number: 73A199834PLB – Public Liability Policy
 - ✓ OAMPS Insurance Brokers Schedule of Cover Reference 023838N
 - ✓ OAMPS Insurance Brokers Statement of Account dated 23 April 2012

TRAINING SHIP COCKBURN

SUMMARY:

Number of participants:

- Unit complement of 17 parading cadet members, 2 staff members, 1 Unit Support Volunteer and fully functional unit committee

Value your organisation will gain from the new location:

- Location offers a greater safety environment for cadet members
- Location offers improved transport access for cadet members
- Retention of cadet numbers and staff
- Increase in cadet numbers and staff
- Augments positive exposure to community
- Enhanced structured training programme
- Cadet members able to undertake a more diverse range of courses/training onsite
- Storage of all assets in the one secured location
- Ability to conduct weekend and/or continuous 8 day training for cadet members onsite
- Host inter unit and/or tri-service training/camps onsite
- Ability to offer the community certified courses at a reduced rate
- Resources available onsite to conduct water based activities locally
- Location is already a locked down, restricted area and this would be continued

How you see increasing participation:

- More centrally located to cadet catchment area
- Enhanced training programme will ignite interest and enthusiasm
- Secured, safer environment will alleviate some apprehension from cadet members family
- Cadet members will be more confident and professional in their public duties due to enhanced training programme and weekend training that would be conducted onsite
- Ability to conduct 2 to 8 day continuous training programmes will encourage teamwork, leadership skills, life skills and socially acceptable behaviour
- Cadets will gain skills, confidently, over a shorter time span, that will assist them when applying for employment within the community

Proposed use of space and for what purpose:

- All areas will be utilized to the upmost training advantage for all cadet members
- Unit Committee will have an onsite meeting and storage area that will enhance their capabilities to assist the cadets and the unit to develop into a highly functional youth development programme for the community
- Our watercraft will be able to be stored at the one location, be secured and available for cadet training

Audited financial statements:

- The unit has been able to pay training and other costs for its cadet members while maintaining a credit balance, for over 5 years, in excess of \$10,000.00

How will the cadets pay outgoings and any other costs that will arise:

- Support from Unit Committee, Defence Services, Cadet Fee's and community donations

Benefits of your service to the community:

- Safe learning environment
- Affordable membership
- All uniforms and equipment supplied
- Support, training and a sense of self worth to individuals and the communities youth

Composition of committee of management:

- Full executive committee and committee members

Insurance coverage:

- Insurance is paid for by Defence Services and Unit Support Committee that cover Public Liability, Property, Directors' & Officers' Liability, General Liability & Professional Indemnity and Personal Accident Cover

CURRENT INSURANCE DOCUMENTATION

AUSTRALIAN DEFENCE FORCE CADETS

COMCOVER INSURANCE POLICY INFORMATION SHEET

INTRODUCTION

1. The Defence Insurance Office provides management of the Department of Defence's insurance policy arrangements with Comcover and responds to meet customer requirements for the following insurance services :

- Centrally manage all insurable claims
- Provide advice on insurance queries related to Comcover policy coverage
- Issue Comcover Certificate's of Currency
- Manage Defence's insurance policy including the renewal process

AIM

2. The aim of this document is to outline the provision of insurance to the ADF Cadets. This document will provide guidance on how to make a claim and how to seek additional advice on insurance matters.

DEFINITIONS

3. The following definitions apply in this document :

- a. **DIO** Defence Insurance Office
- b. **Normal Defence Business** is any activity or event undertaken by a Defence representative (including ADF Cadets) that is deemed to be an operational requirement and approved through the chain of command by Regional HQ.
- c. **Notification of Activity** is a form used to notify the Defence Insurance Office of activities that are not automatically covered under the Comcover policy.
- d. **Voluntary Worker** is a person who consents to undertake duties under the direction or at the request, of the ADF Cadets, receives nil or nominal reward, and does so on a temporary or casual basis. Also refer to Annex A part d for further details.
- e. **Certificate of Currency** is a document that is proof of insurance to an interested party. An interested party is a person or entity who has an insurable interest in ADF Cadet activity.
- f. **Comcover** is the Australian Government's general insurance fund. Comcover provides insurance and risk management services to Australian Government Departments and agencies including the Department of Defence.

INSURANCE COVER FOR AUSTRALIAN DEFENCE FORCE CADETS

4. Comcover provides a schedule of cover annually that outlines the classes of insurance provided to the Department of Defence. Some classes of insurance have been extended to cover ADF Cadets. The ADF Cadets are insured for the following classes, subject to Comcover policy terms and conditions:

5. **Defence insurance policies that apply to ADF Cadets:**

- a. *Property Loss, Destruction or Damage, including property in the course of transit;*
- b. *General Liability and Professional Indemnity;*
- c. *Directors and Officers' Liability; and*
- d. *Personal Accident for approved voluntary workers*

6. **Defence Insurance Policies that do not apply to ADF Cadets:** The Department's insurance program with Comcover will NOT cover the ADF Cadets for the following (these must be covered by ADF Cadet units at their own cost):

- a. *Motor Vehicle – registered Motor Vehicles and or Trailers;*
- b. *Aviation Liability;*
- c. *Marine Liability (vessels over 15 meters); and*
- d. *Workers Compensation.*

7. Full descriptions of the Insurance classes found at ANNEX A can also be found on the Defence Insurance Office website; <http://intranet.defence.gov.au/dsg/sites/insurance/>

8. **Unincorporated Parent Support Groups (PSG) / Unit Support Committees (USC)** that are established for the purpose of directly supporting an ADF Cadet unit and which are not incorporated, are covered under Comcover's Property loss/damage, General and Professional Liability, Directors and Officers' Liability and Personal Accident policies as described in paragraph 5.

9. ADF Cadet Units should not have insurance policies that conflict with those provided by Comcover. Defence will not refund any costs that may arise due to the cancellations of these redundant insurance policies.

10. **Incorporated PSG/USC's** associated with the ADF Cadets, will **not** be covered by Comcover. Incorporated PSG/USC's, clubs (& messes) must obtain and retain their own insurance covers.

Certificates of Currency

11. The Defence Insurance Office provides Certificates of Currency as evidence of the Department's insurance coverage. Certificates of Currency should only be issued to third parties by Cadet Units when requested by an interested third party. Certificates are renewed annually and should be updated accordingly. Please refer to the DIO website.

12. If an activity or event is classed as 'Normal Defence Business' and does not involve motor vehicle, aviation, rocket/missile firing or marine vessels (over 15 metres in length) a generic Certificate of Currency can be provided as proof of insurance.

13. Certificates of currency do not provide cover for parties other than the Department of Defence, which includes ADF Cadets. The certificate itself only provides evidence that the Department of Defence holds a current policy. Certificates of Currency will not be issued noting the names of interested parties.

Making Claims

14. Any unit wishing to make a claim (or is having a claim made against them) relating to the policies described above is to refer the issue through their chain of command to Regional HQ.

15. Where a claim is made against ADF Cadets, you must not:

- a. admit liability
- b. settle the claim; or
- c. incur costs in connection with the claim

without prior written approval from Comcover.

16. ADF Cadets must provide the Defence Insurance Office all assistance and information reasonably required for settling claims by and/or against ADF Cadets. Relevant information for *Claims Reporting and Procedures* is available on the DIO website.

Requests for Information

17. Any requests for information regarding these policies are to be referred through the chain of command to Regional HQ who will liaise with DIO.

ADDITIONAL INFORMATION

Further information can be obtained from the following websites

- Defence Insurance Office
<http://intranet.defence.gov.au/dsg/sites/insurance/>
- Comcover's Insurance Policy
<http://www.finance.gov.au/comcover/manuals.html>

RELATED POLICY / REFERENCES

- Comcover Insurance Policy Defence's Schedule of Cover located at
<http://intranet.defence.gov.au/dsg/sites/insurance/>

Note - Copies of the Department's schedule of cover must not be viewed or distributed to entities or individuals outside of Defence.

- Safety, Rehabilitation and Compensation Act 1988
- Military rehabilitation and Compensation Act 2004

ANNEX A

Definitions

a. Property Loss, Destruction or Damage –

- Comcover's Property Policy provides the ADFC with cover for accidental loss, destruction or damage caused to buildings, contents, *watercraft up to and including 15 metres in length*, unregistered trailers and unregistered motor vehicles owned by the cadet unit. A \$500 excess applies for each property insurance claim, which is payable by the unit making the claim. In order to make a claim against this policy, the cadet unit must be able to prove its ownership of the damaged buildings or items.
- For example, if property to the value of \$8000 was stolen from a cadet unit, the unit could claim against the Property policy. The unit would be required to pay a \$500 excess in this case. All theft claims are required to be reported to the police in the first instance.
- Note that Comcover does not provide insurance for ADF Cadet registered trailers or registered motor vehicles. The cadet unit to which the vehicle or trailer is registered will need to arrange and pay for any insurance required.
- DIO advises that ADF Cadet Units should self insure for damage or loss of property for goods valued less than \$500
- It is important to note Defence has exclusion for Specialised Military Equipment (SME) in its Schedule of Cover. SME is equipment built for specific military purposes, including any rotatable spares. Refer to Schedule of Cover for full definition of SME. This exclusion does not apply to SME while in the course of transit.
-

b. Property in Transport –

- Current insurance arrangements, is world-wide cover up to \$50,000,000 any one transit, with a \$500 excess each and every loss. Cover provides for loss/damage, including losses arising from loading or unloading from conveying vessel, vehicle or aircraft subject to Comcover policy terms and conditions.
- Property is defined in the Comcover policy as the following:
"all real or personal property including money which is yours, or is in your care, custody or control or is your responsibility but excluding watercraft more than 15 meters in length and all aircraft, rockets and satellites, livestock, animals, birds or fish, standing timber, growing crops or pastures (unless noted on the schedule of cover)"
- Property in Transit extends to all modes of transport including air, rail, sea and road. The cover responds even when a third party (a carrier) is transporting Defence property or property in Defence's custody, care or control. Always check carriage documentation, as most carriers will have terms and conditions of the agreement for transport to exclude liability for loss, destruction or damage of property.
- Loss/damage to SME while in the course of transit is covered, subject to all other policy terms and conditions.

c. General (Public) Liability-

- Comcover's General Liability policy covers the Department's legal liabilities to pay compensation to other parties by reason of:
- bodily injury (including death);
- loss of or damage to property;
- libel/slander/defamation;
- professional indemnity
- For example, a cadet unit is conducting an abseiling activity from a city building and drops an object onto a privately-owned vehicle causing damage to the vehicle and injury to the owner. The policies above will (subject to legal liability) cover the Department (and therefore the Cadet Unit) in the event the vehicle owner decided to sue.
- A \$2,000 legal cost excess applies to liability claims. DIO will fund the excess

d. Personal Accident (Cert of Currency for this is managed on case by case basis)

- This policy covers injury or death to Volunteer Workers. The Comcover policy definition of a Voluntary Worker is *'a person who consents to undertake duties under the direction or at the request, of the Australian Defence Force Cadets, receives nil or nominal reward, and does so on a temporary or casual basis'*.
- This would include parents, friends and others that are registered with the relevant regional ADFC unit to undertake duties under the direction or at the request of ADF Cadets and who are listed in the activity application submitted for a specific activity (eg, weekend bivouac or annual camp). Their attendance as a voluntary worker should be recorded in order to provide proof of their attendance should an accident occur.
- This policy covers injury or death to approved volunteer workers who are not covered by the Safety, Rehabilitation and Compensation Act 1988 or Military Rehabilitation and Compensation Act 2004. Cadets and ADFC Staff remain covered under these acts for compensation and other benefits if they suffer an injury while participating in ADFC approved activities.