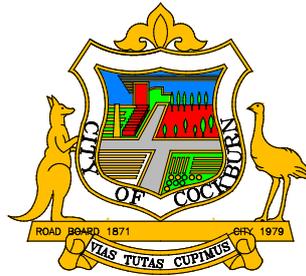


CITY OF COCKBURN



ORDINARY COUNCIL

AGENDA PAPER

FOR

THURSDAY, 8 JUNE 2017

CITY OF COCKBURN

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CITY OF COCKBURN**AGENDA TO BE PRESENTED TO THE ORDINARY
COUNCIL MEETING TO BE HELD ON
THURSDAY, 8 JUNE 2017 AT 7:00 PM**

1. **DECLARATION OF MEETING**
2. **APPOINTMENT OF PRESIDING MEMBER (If required)**
3. **DISCLAIMER (To be read aloud by Presiding Member)**

Members of the public, who attend Council Meetings, should not act immediately on anything they hear at the Meetings, without first seeking clarification of Council's position. Persons are advised to wait for written advice from the Council prior to taking action on any matter that they may have before Council.

4. **ACKNOWLEDGEMENT OF RECEIPT OF WRITTEN DECLARATIONS OF FINANCIAL INTERESTS AND CONFLICT OF INTEREST (by Presiding Member)**
5. **APOLOGIES AND LEAVE OF ABSENCE**
6. **WRITTEN REQUESTS FOR LEAVE OF ABSENCE**
 - 6.1 **(OCM 08/06/2017) - REQUEST FOR LEAVE OF ABSENCE - CLR CHAMONIX TERBLANCHE (083/005) (D GREEN)**

RECOMMENDATION

That Council grant leave of absence to Clr Chamonix Terblanche for the period 28 June 2017 to 18 October 2017, inclusive.

COUNCIL DECISION

Background

By email received 1 June 2017 Clr Terblanche has requested leave of absence from Council duties for the period 28 June 2017 to 18 October 2017.

Submission

N/A

Report

Council may, by resolution, grant leave of absence to a member. Approval is recommended on this occasion.

Strategic Plan/Policy Implications

Leading & Listening

- Deliver sustainable governance through transparent and robust policy and processes.

Budget/Financial Implications

N/A

Legal Implications

Sec.2.25(1) of the Local Government Act, 1995 refers.

Community Consultation

N/A

Risk Management Implications

There are no risk implications associated with this request.

Attachment(s)

Email notification from Clr Terblanche.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

7. RESPONSE TO PREVIOUS PUBLIC QUESTIONS TAKEN ON NOTICE

Nil

8. PUBLIC QUESTION TIME

9. CONFIRMATION OF MEETING

9.1 (OCM 08/06/2017) - MINUTES OF THE ORDINARY COUNCIL MEETING - 11 MAY 2017

RECOMMENDATION

That Council confirms the Minutes of the Ordinary Council Meeting held on Thursday, 11 May 2017, as a true and accurate record.

COUNCIL DECISION

10. DEPUTATIONS

11. PETITIONS

12. BUSINESS LEFT OVER FROM THE PREVIOUS MEETING (If adjourned)

Nil

13. DECLARATION BY MEMBERS WHO HAVE NOT GIVEN DUE CONSIDERATION TO MATTERS CONTAINED IN THE BUSINESS PAPER PRESENTED BEFORE THE MEETING

14. COUNCIL MATTERS

14.1 (OCM 08/06/2017) - MINUTES OF THE DELEGATED AUTHORITIES, POLICIES & POSITION STATEMENTS COMMITTEE MEETING - 18 MAY 2017 (182/001; 182/002; 086/003) (D GREEN) (ATTACH)

RECOMMENDATION

That Council receive the Minutes of the Delegated Authorities, Policies and Position Statements Committee Meeting held on Thursday, 18 May 2017 and adopt the recommendations contained therein.

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COUNCIL DECISION

Background

The Delegated Authorities, Policies and Position Statements Committee conducted a meeting on 18 May 2017. The Minutes of the meeting are required to be presented.

Submission

N/A

Report

The Committee recommendations are now presented for consideration by Council and if accepted, are endorsed as the decisions of Council. Any Elected Member may withdraw any item from the Committee meeting for discussion and propose an alternative recommendation for Council's consideration. Any such items will be dealt with separately, as provided for in Council's Standing Orders. The primary focus of this meeting was to review the Delegated Authorities pursuant to the Local Government Act and Extraneous to the Local Government Act.

In addition, those Delegated Authorities, Policies and Position Statements which were required to be reviewed on an as needs basis have also been included.

Strategic Plan/Policy Implications

Leading & Listening

- Deliver sustainable governance through transparent and robust policy and processes.

- Ensure sound long term financial management and deliver value for money.
- Listen to and engage with our residents, business community and ratepayers with greater use of social media.

Budget/Financial Implications

As contained in the Minutes.

Legal Implications

As contained in the Minutes.

Community Consultation

As contained in the Minutes.

Risk Management Implications

Failure to adopt the Minutes may result in inconsistent processes and lead to non-conformance with the principles of good governance, and non-compliance with the Local Government Act 1995 for delegations made under the Act.

Attachment(s)

Minutes of the Delegated Authorities, Policies and Position Statements Committee Meeting – 18 May 2017.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

14.2 (OCM 08/06/2017) - MINUTES OF THE COCKBURN COMMUNITY EVENTS COMMITTEE MEETING - 16 MAY 2017 (152/010) (M LA FRENAIS) (ATTACH)

RECOMMENDATION

That Council receives the Minutes of the Cockburn Community Events Committee Meeting held on Tuesday, 16 May 2017 and adopts the recommendations contained therein.

COUNCIL DECISION

Background

Council is required to determine the Calendar for the 2017/18 events season, as per Budget Policy SC34, which states a “Provisional allocation for Community Events is to be a maximum of 1.0% of Rates Revenue. Council is to approve the calendar of events.”

The Community Events and related expenses below are funded from this budget. Any other City run events are funded from separate budgets.

The Events team has developed the following proposal for the 2017/18 program of events, based on:

- A review of the 2016/17 season
- Feedback from surveys
- Staff de-brief of the events
- Feedback from people at events and on social media

It is necessary to consider the calendar early in the financial year (July), because:

- It is preferable that marketing for the season commences in September (Fur Run). Therefore adequate time is required for marketing material to be produced in advance.
- October-November Events are included in Cockburn Soundings October edition, which is prepared in August.

Corporate Communications will apply to Health Way and Lottery West for funding for the 2017/18 season. Council needs to have determined the season of events before applications are submitted. These applications require around four months lead-in time and then adequate time to feature these organizations on promotional material should a sponsorship agreement dictate.

Submission

N/A

Report

Proposed 2017 – 2018 Events

Below is the proposed calendar of events. This includes events for the coming financial year and their related budget.

Event Name Location	Date	Budget ex. GST	Comments
<i>Fur Run</i>	24 September 2017	OP 8992 \$9,000	Encourage healthy dogs and provide information for dog owners. Manning Park
<i>Seniors Social Evening 3 (2017)</i>	September 2017	OP 9492 \$12,000	Different theme; entertainment, buffet meal, raffles & prizes. 5.30pm – 11pm. Dalmatinac Club Tickets \$10.00.
<i>Side Splitter</i>	28-29 October and 4-5 November 2017	OP 8854 \$25,000	Comedy festival to be held at Memorial Hall. Includes a 16+ free comedy workshop.
<i>Teddy Bears Picnic</i>	25 October 2017	OP 9307 \$27,000	10am - 1pm Entertainment and rides free for pre-school kids, activities, amusements, arts, parenting information. Manning Park
<i>Christmas on the Green</i>	9 December 2017	OP 9460 \$33,000	Christmas at MacFaul Park.
<i>Australia Day Coogee Beach Festival</i>	26 January 2018	OP 9107 \$83,000	8am – 12am. Free sausage sizzle, free rides, entertainment, family activities. Coogee Beach Reserve.
<i>Community Concert</i>	February 2018	OP 9476 \$160,000	Cockburn ARC Legacy Park, 7pm – 10pm.
<i>Coogee Live</i>	March 2018	\$160,000	Coogee Coast, 3pm – 10pm daily (Friday – Sunday).
<i>Cultural Fair</i>	7 April 2018	OP 9108 \$46,000	Harmony Oval Harvest Lakes. Flavours of Cockburn theme. Have stalls selling a mixture of cuisines. Gourmet food and cooking demonstrations

Event Name Location	Date	Budget ex. GST	Comments
			as well as arts and crafts. Final of "Cockburn's Got Talent".
Seniors Social Evening 1 (2018)	May 2018	OP 8855 \$12,000	Different theme; entertainment, buffet meal, raffles & prizes. 5.30pm – 11pm. Dalmatinac Club Tickets \$10.00.
Seniors Social Evening 2 (2018)	July 2018	OP 8856 \$12,000	Different theme; entertainment, buffet meal, raffles & prizes. 5.30pm – 11pm. Dalmatinac Club. Tickets cost \$10.00 to purchase.
Marketing, research, detailed concept and Insurance for the major events plus miscellaneous		OP9021 \$125,000	Marketing for all events, insurance and miscellaneous expenses which may include research.
Pop up events x 3	TBA to fit in with reserve availability and other events	OP 8857 \$10,500	Simple pop up art and music with coffee and stall food offering. Various locations.
Total		\$714,500	

In 2017/18, it is proposed that the events calendar program follows a new format in terms of introducing a new event and reducing the number of concerts to one.

Dates have been considered in light of key events around Perth that are currently known, such as sporting events and community events, as well as other City of Cockburn events, which the City supports.

The recommendation is that the City continues with one bigger concert to be held at Cockburn Central, Legacy Park and introduces one major arts and cultural light festival, domiciled as "Coogee Live", along the Cockburn Coast.

The choice of artists to be sourced for the concert can be found in the Events Committee minutes attached to this item.

The support act choice would be determined by the cost of the main act. It would be a local Perth band, tribute or cover band.

The City retains three seniors' evening events. The tickets currently cost the City \$45 per person and each person pays a subsidised cost per ticket, which goes towards prizes and giveaways on the night. Last year the cost was \$10.00 per ticket. It is proposed to keep the price to \$10.00 per person for 2017-18. This is based on the capacity of the Dalmatinac Club and the sale of 270 tickets. This means each ticket will cost the City \$35 per person. As the tickets are highly sought after, the process is refined each year to ensure, as far as possible, that only Cockburn residents attend and that there is a waiting list for those who miss out on the previous event.

The following events are retained in current format due to their popularity and good attendance:

- Teddy Bear's Picnic;
- Seniors Events;
- Pop Up arts and music events x 3;
- Side Splitter Comedy Festival;
- Christmas on the Green;
- Cultural Fair;
- Fur Run;
- Australia Day Coogee Beach Festival

Proposed new event 2017/2018

"Coogee Live"

- "Coogee Live" is proposed to be a three-day festival that will showcase the Cockburn Coast through creative activities such as an innovative lighting and laser display, theatre and art exhibitions, and a hawkers market.
- Letters of support from Coogee Beach Progress Association, Friends of the Community and Coogee Beach Surf Life Saving Club have been received by the City.
- Comment on Cockburn results of a survey open from 10 April-5 May;
 - 97.1% of respondents support the idea of "Coogee Live"
 - 80.8% of respondents support the City hosting one concert, instead of two, to enable "Coogee Live" to precede, 12.5% said no and 6.7% had no opinion.
 - 28.8% of respondents were from Coogee, 25.4% from Spearwood, 18.6% from North Coogee, 16.9% from Yangebup and 10.2% from Success
- Preliminary sponsorship funding yet to be finalized, however, possible contributions are:
 - \$170K (COC, \$160K events budget plus \$10K from cultural budget/grant for artists in residence program)
 - \$45K Lotterywest
 - \$45K Healthway

- \$35K Fremantle Ports, Land Corp, Murdoch University.

If sponsorship is not gained from either Lotterywest or Healthway the event would not be able to proceed and the City will revert back to a second concert sourcing an artist from the approved list as decided by the Committee. If sponsorship was approved from Health Way and Lottery West, but not from some of the smaller organizations, the event could still proceed on a smaller scale.

Marketing /Insurance/ Research/Concept Development

The marketing plan for all events approved will include traditional advertising, use of Facebook, annual calendar, mini billboards in parks, posters and promotion at other events. New event detailed concept design and event surveying as well as insurance is covered in this component of the budget (\$125K).

Healthway Funding

The City was successful in securing \$8,000 in sponsorship funding for the Cultural Fair and will continue to seek this partnership in 2018. Healthway have indicated that “Coogee Live” would not impact Cultural Fair funding, but it is not confirmed at this time to be the case.

Strategic Plan/Policy Implications

Community, Lifestyle & Security

- Provide residents with a range of high quality, accessible programs and services.
- Provide safe places and activities for residents and visitors to relax and socialise.

Leading & Listening

- Deliver sustainable governance through transparent and robust policy and processes.
- Ensure sound long term financial management and deliver value for money.

Budget/Financial Implications

\$714,500 including marketing, event concept development and insurance is included in the draft 2017/18 budget.

Legal Implications

N/A

Community Consultation

In 2016 the Community Perceptions Survey (Catalyse) showed 89% of those surveyed were familiar with festivals, events and cultural opportunities in the City of Cockburn.

22% responded excellent, 42% responded 'good' and a further 25% responded ok.

Survey research was undertaken specifically for the Australia Day Coogee Beach Festival.

45% responded 'very satisfied' and 47% 'satisfied'.

Survey research was undertaken specifically for the 'Success Community (Dami Im) concert in February.

47% responded 'very satisfied' and 42% 'satisfied.'

Risk Management Implications

Enabling the program to be adopted at June 2017 Council meeting is required in order to prevent a delay in booking acts, which in turn would result in information being left out of the annual City of Cockburn calendar, and preventing the events team from starting to plan the event. This represents a "Low" level of Operational / Service Disruption Risk.

The risk of not considering new events is that Council is not seen to be listening to community or market trends. This represents a "Moderate" level of Reputation Risk.

Attachment(s)

Minutes of the Cockburn Community Events Committee Meeting – 16 May 2017.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

It is appropriate for Council to provide entertainment activities for its community on a free or subsidised cost basis.

15. PLANNING AND DEVELOPMENT DIVISION ISSUES

15.1 (OCM 08/06/2017) - CHILD CARE PREMISES - LOCATION: 67 (LOT 64) STRATTON STREET, HAMILTON HILL; OWNER: ROTTO INVESTMENTS PTY LTD; APPLICANT: PETER BETZ (052/002) (DA17/0106) (G ALLIEX) (ATTACH)

RECOMMENDATION

That Council :

- (1) grant Planning Approval for a Child Care Premises at No. 67 (Lot 64) Stratton Street Hamilton Hill, in accordance with the attached plans and subject to the following conditions and footnotes.

Conditions

1. No more than 90 children and 12 staff members are permitted at the Child Care Premises at any one time.
2. The hours of operation are restricted to between 6.30am to 6.30pm, Monday to Friday, 8.00am-4.00pm on Saturdays and not at all on Sundays and Public Holidays.
3. Prior to the issue of a Building Permit, a detailed colour and materials schedule shall be submitted to and approved by the City and this includes all fencing. The approved schedule shall then be implemented to the satisfaction of the City.
4. Prior to occupation of the development, landscaping (including verge landscaping and street trees) shall be installed in accordance with the approved Landscape Plan. Landscaping shall be reticulated/irrigated and maintained by the owner/occupier to the satisfaction of the City.
5. Prior to occupation of the development:
 - i. vehicle parking bays, vehicle maneuvering areas, driveways and points of ingress and egress shall be sealed, kerbed, drained, line marked and made available for use to the satisfaction of the City;
 - ii. staff parking bays (bays 1-12) shown on the site plan must be clearly signed and/or marked to the satisfaction of the City;
 - iii. the entry and exits to the carpark shall be clearly signed and marked so that visitors can clearly follow the flow of traffic through the carpark; and
 - iv. the existing crossovers to Stratton Street and Forrest road shall be removed and the area reinstated and landscaped, to the City's specification and satisfaction.

6. New vehicle crossovers shall be designed and constructed in accordance with the City's requirements.
7. Prior to the issue of a Building Permit, plans shall be amended showing the access and egress into the site shall being swapped over so that access to the site is obtained from the western-most crossover onto Stratton Street.
8. A further Acoustic Report shall be submitted to and approved by the City, prior to the issue of a Building Permit, and implemented thereafter, to the satisfaction of the City.
9. Written confirmation from a recognised acoustic consultant that all recommendations made in the Acoustic Report prepared by Norman Disney & Young (dated 11 May 2017) and the further Acoustic Report required under condition 8 have been incorporated into the proposed development, shall be submitted to the City at the time of lodgement of the Building Permit Application.
10. The builder shall provide written confirmation that the requirements of the Acoustic Report referred to in condition 8 have been incorporated into the completed development with the Form BA7 Completion Form, prior to occupation of the development.
11. Prior to the submission of a Building Permit Application for the development, a Noise Management Plan shall be prepared to the City's satisfaction demonstrating that noise emissions will comply with the requirements of the Environmental Protection (Noise) Regulations 1997 (as amended). All noise attenuation measures, identified by the plan or as additionally required by the City, are to be implemented prior to occupancy of the development (or as otherwise required by the City) and the requirements of the Noise Management Plan are to be observed at all times.
12. Walls, fences and landscape areas are to be truncated within 1.5 metres of where they adjoin vehicle access points where a driveway and/or parking bay meets a public street or limited in height to 0.75 metres.
13. All stormwater shall be contained and disposed of on-site.
14. All plant and equipment (such as air conditioning condenser units and communications hardware etc.) shall be screened so as not to be visible from the street or adjoining properties.

15. The premises shall be kept in a neat and tidy condition at all times by the owner/occupier to the satisfaction of the City.
16. Prior to occupation of the development, the street number shall be clearly displayed on the façade of the building and displayed in perpetuity to the satisfaction of the City.
17. No person shall install or cause or permit the installation of outdoor lighting otherwise than in accordance with the requirements of Australian Standard AS 4282 - 1997 "Control of the Obtrusive Effects of Outdoor Lighting".
18. The site must be connected to reticulated sewerage prior to the commencement of the use hereby approved.
19. All waste and recycling materials must be contained within bins and the bin store shall comply with the following standards:
 - i. walls constructed of smooth, impervious, solid material at a height of not less than 1.8m; and
 - ii. a floor of not less than 76mm thickness, constructed of impervious concrete graded to a 100mm industrial floor waste connected to sewer and charged with a hose cock.
20. Prior to the issue of a Building Permit, a Construction Management Plan (CMP) shall be submitted to and approved by the City.
21. No building or construction activities shall be carried out before 7.00am or after 7.00pm, Monday to Saturday, and not at all on Sunday or Public Holidays.
22. A separate application for all signage shall be submitted to, and approved by the City prior to the erection of any signage on site.

FOOTNOTES

1. This is a Planning Approval only and does not remove the responsibility of the applicant/owner to comply with all relevant building, health and engineering requirements of the City, with any requirements of the City of Cockburn Local Planning Scheme No. 3, or the requirements of any other external agency.
2. The development is to comply with the noise pollution provisions of the *Environmental Protection Act 1986*, and more particularly with the requirements of the *Environmental*

Protection (Noise).

3. Access and facilities for persons with disabilities is to be provided in accordance with the requirements of the National Construction Code.
4. The applicant is advised approval is to be obtained from the Commonwealth Department of Family and Community Services.
5. A copy of the licence from the Child Care Services Board to be provided to the City's Manager, Environmental Health.
6. With reference to Condition 4, the street trees installed shall be to the satisfaction and specification of the City.
7. With reference to Condition 6, you are advised to contact the City's Engineering Services on 9411 3554 for further information regarding the City's crossover requirements.
8. With reference to Condition 13, all stormwater drainage shall be designed in accordance with the document entitled "Australian Rainfall and Runoff" 1987 (where amended) produced by the Institute of Engineers, Australia, and the design is to be certified by a suitably qualified practicing Engineer or the like, to the satisfaction of the City, and to be designed on the basis of a 1:100 year storm event.
9. If an odour detected at an adjacent premises is deemed to be offensive by the City, then any process, equipment and/or activities that are causing the odour shall be stopped until the process, equipment and or activity has been altered to prevent odours to the satisfaction of the City.
10. If dust is detected at an adjacent premises and is deemed to be a nuisance by an Environmental Health Officer, then any process, equipment and/or activities that are causing the dust nuisance shall be stopped until the process, equipment and or activity has been altered to prevent the dust to the satisfaction of the City's Manager of Health Services.
11. This development has been defined as a public building and shall comply with the provisions of the Health Act 1911 relating to a public building, and the Public Building Regulations 1992. An application to construct, extend or alter a public building is to be submitted with the Building Licence application. Refer to attached application form.

12. All food businesses must comply with the Food Act 2008 and Chapter 3 of the Australia New Zealand Food Standard Code (Australia Only). Under the Food Act 2008 the applicant must obtain prior approval for the construction or amendment of the food business premises. An Application to Construct or Alter a Food Premises must be accompanied by detailed plans and specifications of the kitchen, dry storerooms, cool rooms, bar and liquor facilities, staff change rooms, patron and staff sanitary conveniences and garbage room, demonstrating compliance with Chapter 3 of the Australia New Zealand Food Standard Code (Australia Only).

The plans to are to include details of:

- (i) the structural finishes of all floors, walls and ceilings;
 - (ii) the position, type and construction of all fixtures, fittings and equipment (including cross-sectional drawings of benches, shelving, cupboards, stoves, tables, cabinets, counters, display refrigeration, freezers etc); and
 - (iii) all kitchen exhaust hoods and mechanical ventilating systems over cooking ranges, sanitary conveniences, exhaust ventilation systems, mechanical services, hydraulic services, drains, grease traps and provisions for waste disposal.
 - (iv) these plans are to be separate to those submitted to obtain a Building Licence
13. All food handling operations must comply with the Food Act 2008 and Chapter 3 of the Australia New Zealand Food Standard Code (Australia Only). Under the Food Act 2008 the applicant must complete and return the enclosed Food Business Notification/Registration Form to the City of Cockburn's Health Services. Operation of this food business may be subject to the requirement to pay an Annual Assessment Fee under the Act.
14. All toilets, ensuites and kitchen facilities in the development are to be provided with mechanical ventilation flued to the outside air, in accordance with the requirements of the National Construction Code (Building Code of Australia), the Sewerage (Lighting, Ventilation and Construction) Regulations 1971, Australian Standard S1668.2-1991 "The use of mechanical ventilation for acceptable indoor air quality" and the City of Cockburn Health Local Laws 2000.
- (2) notifies the applicant and those who made a submission of Council's decision.

COUNCIL DECISION**Background**Site Description

The subject site is 1978m² in area and has frontage to three roads being Forrest Road to the south, Carrington Street the east and Stratton Street to the north. The site abuts two existing residential dwellings to the west. The site currently contains two dwellings, detached garages, small outbuildings, minimal vegetation and crossovers to Stratton Street and Forrest Road. The site has a fall of approximately 2.4m-2.9m from east to west and a fall of approximately 1.74m from north to south.

As part of Council's Hamilton Hill Revitalisation Strategy, the lot (along with surrounding lots) was up-coded from R20 to R40 given Carrington Street is a District Distributor 'A' road and is a high frequency public transport route.

The proposed development is being referred to Council for determination as staff do not have delegation to determine the application due to objections received during the public consultation period.

Submission

N/A

ReportProposal

The proposal includes demolition of all existing buildings and structures on site and the construction of a purpose-built childcare premises specifically:

- Child Care, Early Learning, Vacation Care and Out of School Hours Care;
- Single storey acoustically sound proofed building which is 539.2m² in area and consists of 5 play/learning rooms for different age groups, cot rooms, a nappy room, store rooms, laundry and toilet facilities, a staff room, interactive kitchen and offices;
- External play area located on the southern side of the lot with a total area of 713m²;

- Open-style fencing fronting Carrington Street and Forrest Road;
- Catering for up to 90 children aged from 6 weeks to school age;
- 12 full-time staff;
- Hours of operation between 6.30am and 6.30pm, Monday-Friday and 8.00am-4.00pm on Saturdays;
- 22 at grade on-site parking spaces provided; and
- Access and egress proposed from two crossovers both to Stratton Street.

It should be noted that based on discussion with planning staff, the proposal has evolved from what was originally submitted and the key changes include:

- Deletion of a crossover originally proposed to Carrington Street (based on Main Roads advice);
- Reduction of children from 98 to 90 (to comply with parking requirements);
- Reduction of staff from 13 to 12 (to comply with parking requirements).

Planning Framework

Zoning and Use

The subject site is zoned 'Residential' under the Metropolitan Region Scheme (MRS) and 'Residential R40' under Local Planning Scheme No.3 (LPS 3).

The objective of the 'Residential' zone is:

'To provide for residential development at a range of densities with a variety of housing to meet the needs of different household types through the application of the Residential Design Codes'

LPS 3 defines a 'Child Care Premises' as:

'Has the same meaning as in the Community Services (Child Care) Regulations 1988.'

Under the *Community Services (Child Care) Regulations 1988*, the definition is:

'Premises specified in a licence or permit as premises in which a child care service may be provided.'

A 'Child Care Premises' is listed in Table 1 of LPS 3 under 'Residential Uses' and is an 'A' use (discretionary subject to advertising) within the 'Residential' zone. This means that the use is not permitted unless the local government has exercised its discretion and has granted planning

approval after giving special notice in accordance with clause 64(3) of the deemed provisions within the *Planning and Development (Local Planning Schemes) Regulations 2015*. As the proposal has been advertised to surrounding landowners, the use is capable of approval by Council.

Residential Design Codes (R-Codes)

Whilst the proposal is not required to be assessed against the provisions of the Residential Design Codes, it should be noted that it generally conforms to the appropriate setbacks, heights, open space etc. as required under the R-Codes.

Local Planning Policy 3.1 – Child Care Premises (LPP 3.1)

The proposal is generally consistent with the provisions of LPP 3.1 with the exception of:

- The proposed outdoor play area is located adjacent to a residential dwelling to the west of the site which does not accord with this policy provision and has the potential to negatively impact on the amenity of neighbours. Further discussion about noise is contained in the noise section of the report below.
- The proposal includes a 1.5m landscaping strip across the front boundary in lieu of 2m outlined in the policy. The proposal does however include four mature street trees in the Stratton Street verge therefore the impact of the building is reduced as viewed from Stratton Street.

Agency Referrals

The proposal was referred to Main Roads for comment given the lot is within close proximity to (but not abutting) the Primary Regional Road Reserve (PRR) to the south of the site. Main Roads confirmed in writing that they are satisfied with the proposal.

Neighbour Consultation

The proposal was advertised to 10 nearby land owners potentially affected by the proposal in accordance with the requirements of LPS 3. A total of 6 submissions were received, one indicating no objection and five objecting to the proposal. The main issues and concerns raised during consultation include:

- Increased noise generated by the proposal;
- Increased traffic and traffic congestion generated by the proposal and the potential safety risks associated, health impacts from the vehicle emissions and the general inconvenience;

- Unauthorised parking occurring in and around the site;
- Visual amenity being compromised by the unscreened parking area adjacent to Stratton Street;
- Offensive odours from the bin storage area; and
- The commercial nature of the proposal and therefore being an unsuitable and inappropriate use for a residential area.

Planning Considerations

Location

The appropriateness of the location of the proposal has been raised as a concern from neighbours during the consultation period and in particular that it is not a compatible use for the area. Firstly, it should be noted by Council that the 'Childcare Premises' use in the Zoning Table (Table 1) of LPS 3 is listed as a 'Residential Use' and similar to a school is considered entirely appropriate to be located in a residential zone.

LPP 3.1 stipulates various provisions in relation to the siting of Child Care Premises, particularly in relation to residential amenity. The policy outlines that ideally a site within close proximity to a public transport route would be suitable and that a site within a cul-de-sac or battle-axe lot would not be appropriate. Having frontages to Forrest Road and Carrington Street, the subject site has good road access and is located adjacent to public transport routes. The site is less than 800m (10 minute walk) from the Simms Road Local Centre to the north and less than 200m from a number of commercial premises to the south. The site is well located to accommodate a childcare premises and abuts residential dwellings on only the western side which limits any impact on residential amenity.

Built Form

The proposal is single storey and consists of a mix of rendered brickwork, face brickwork, and feature cladding consistent with the character, bulk and scale of the surrounding residential area. A neutral colour palette of creams, soft tawny browns and grey and proposed to create a modern finish to and to complement surrounding residences. The proposal will not result in overshadowing, overlooking or the creation of an unreasonable visual bulk on any nearby residential property and will not adversely affect the existing residential character of the area. In addition, the proposal includes four street trees in the Stratton Street verge which, in addition to the proposed landscaping within the lot, will soften the façade of the building fronting Stratton Street and contribute to the streetscape.

Traffic & Safety

Concerns about potential traffic congestion and safety issues were raised during consultation. The transport impact statement that was submitted as part of the application states that the maximum peak in traffic is likely to be 29 vehicles per hour which will be between 8.00am to 9.00am, which is outside of the existing morning peak traffic which is between 6.30am to 7.30am (according to the applicants traffic consultant). It should also be noted that a Child Care Centre does not have a start time (unlike a school for example) and therefore parents will intermittently drop children off within an expected 2.5 hours window during the morning. This spread of vehicle movements significantly reduces the risk of traffic queuing and congestion in and around the site. Whilst the drop of time cannot be controlled, the impact of the drop offs during the morning is not anticipated to create a traffic issue or safety concern at the subject site, especially given both Stratton and Carrington Street currently operate under capacity in relation to vehicle movements per day. This position has been confirmed by the City's Transport Engineer and the existing road network is considered to be sufficient to accommodate the proposal with no further traffic concerns.

Access and Egress

The proposal contains two crossovers to Stratton Street, one solely for access and one solely for egress, thereby catering for one directional traffic flow through the parking area for safer movements. This design was based on concerns raised by Main Roads who objected to the initial design which showed the second crossover being on to Carrington Street instead of Stratton Street.

The Stratton Street crossover closest to Carrington Street is proposed as the access crossover which is recommended by the applicant's Traffic Consultant for safety reasons. However, the City's Transport Engineer has raised concerns with the access crossover being too close to the intersection of Stratton Street and Carrington Street. The City's Transport Engineer therefore recommends swapping the access and egress crossovers to allow for safer entry into the site. The applicant is aware of the City's position in relation to the access crossover and is agreeable to swapping the two if necessary. Should Council approve the proposed Child Care Centre, a condition should be imposed to require modification of the access and egress crossovers to the City's satisfaction.

Car parking

Under LPS 3, one car parking bay is required for each employee and one bay for every 10 children accommodated. The proposal includes 12 employees and 90 children which generates the need for 21 parking

bays. As the proposal includes 22 on-site car parking bays, there is a surplus of one bay and the number of parking bays is compliant with LPS 3.

The City's Traffic Engineer had some concern about the width of the parking aisles in relation to the requirements of Australian Standard 2890 (AS2890), because of vertical obstructions adjacent to some of the bays. AS2890 requires different widths for parking bays depending on how frequently the bays are used in any given day. Based on this, the bays noted on the plans for staff are 5.4m long and 2.4m wide whereas the bays for drop-offs and pick-ups are 5.4m long and 2.5m wide, as they allow extra space for the full opening of all doors. Should Council approve the proposed Child Care Centre, a condition should be imposed in relation to the staff parking bays being appropriately signposted/line marked so that visitors park in the appropriately sized bays as per the Australian Standards.

Noise

Potential noise from the proposal was the main objection raised by neighbours during consultation. The applicant submitted an acoustic report which indicates that subject to a number of recommendations being observed, the proposed development will not have any unreasonable noise impacts on nearby residents. The report recommended the following measures:

- Outside play time is limited to a maximum of 40 children for 1 hour of every 4 hours, and 120 for a full day if you consider 3 x 1 hour outdoor sessions spaced by 3 hours each (e.g. 1x hour at 08.00; 1x hour at 12.00; 1x hour at 16.00);
- 400mm high retaining wall and solid continuous barrier to be installed around the play area adjacent to the dwellings (colour bond acceptable);
- Minimum 10.38mm laminated glazing for the windows of sleeping rooms (W08 and W09);
- Minimum 6mm float glazing for the all other windows;
- Minimum $R_w + C_{tr}$ 35 (e.g. 90mm acoustibrick -50mm air gap – 90mm acoustibrick) for all non-vision facade areas facing the road;
- Limit use of air conditioning to day time. If used outside daytime hours (07.00 to 19.00 hours) ensure the silent option is enabled;
- Air conditioning Units to be selected so that their combined noise emission is no more than 65dBA @ 1 metre (all units in operation).

If units are selected to be louder further acoustic review is required to demonstrate compliance with WA environmental noise regulations.

The limitations to the outdoor play time, as noted above, were questioned by officers as it initially appeared unrealistic and difficult to monitor. However the applicant has since informed the City that the outdoor playtime limits are realistic from an operation point of view due to the outdoor area being unsuitable at certain times of day (either too cold, too hot, high UV etc.). From a staffing perspective, activities may require multiple staff members in a smaller controlled area therefore activities tend to be indoors as opposed to outdoors. So whilst the outdoor play areas are certainly going to be used by children, those areas won't be used every hour of the day, so the level of noise will not be unreasonable.

The acoustic consultant is aware of the objections that have been raised in relation to noise and is confident that the day time noise limit of 51dB (L10) can be met, especially given the road traffic noise was measured to in the order of 61-62dB. This is a positive indication that any noise generated by the children will be masked by the already existing road traffic noise.

Whilst the sound of children playing outside (in the limited time that they can play outside) will be audible in the adjacent premises, it is still deemed to be acceptable as it does not result in practical increase of daily background noise levels. Should Council approve the proposed Child Care Centre, a condition should be imposed in relation to the acoustic report recommendations being implemented with and any building measures being incorporated as part of the Building Permit.

Odour

An objection was received in relation to the potential offensive smell of human effluent and decaying food from the bin store that may travel to surrounding residential properties with the prevailing breeze and therefore negatively impact the amenity of residential properties. The proposed bin store is strategically located away from the western residential zoned land (over 9m from the western boundary), on advice from the City during preliminary consultation. LPP 3.1 stipulates the minimum requirements for bin storage areas within Child Care Centres and the proposal complies with these requirements. Given the sealed nature of the bin store, it is not expected that offensive odours from the bins will negatively impact adjacent residential properties.

The applicant has also confirmed that disposal of nappies will be in accordance with the Australian Children's Education & Care Quality Authority and any other relevant authorities. Waste collection from

these bins will be collected by a kerb-side pick-up in conjunction with the normal residential street waste collection. Should Council approve the proposal, a condition should be imposed in relation to the bin store area being built in accordance with the requirements of LPP 3.1.

Signage

The proposal does not include sufficient signage detail. LPP 3.1 states that signage should be compatible and sympathetic to the surrounding amenity and not be placed in a location that detrimentally interferes with the visibility of traffic entering and existing the site. Given the nature of the objections in relation to amenity and given the volume of traffic on Carrington Street and Forrest Road, installation of appropriate signage is an important consideration. Therefore should Council approve the proposal, a condition requiring a separate application for signage should be imposed.

Construction Management

To ensure minimal disruption to surrounding residents during construction, it is common practice for applicants to submit a comprehensive Construction Management Plan (CMP) to the City for approval. This will address issues such as staging of construction works, noise, material delivery and storage, temporary fencing, contractor parking, protection of street furniture and infrastructure, traffic generation and access. Should Council support the proposal, a condition should be imposed requiring the lodgement of a CMP prior to work commencing.

Conclusion

The proposed purpose-built Child Care Premises is supported for the following reasons:

- The proposal is generally compliant with the provisions of LPS 3 and LPP 3.1;
- The proposal is appropriately located in relation to proximity to major roads and access to public transport routes;
- The proposal will positively contribute to the streetscape and is an appropriate bulk and scale in relation to the residential character of the area;
- The proposal is not considered to negatively impact on the amenity of neighbours;
- The purpose built nature of the proposal allows for the building to be acoustically soundproofed to deal with potential noise impacts;
- The proposal includes a well-designed carpark which will not have an impact to the general traffic flow or traffic safety in the area;
- The existing road network is capable of supporting the proposal.

It is therefore recommended that Council approve the proposal subject to the conditions.

Strategic Plan/Policy Implications

City Growth

- Ensure planning facilitates a desirable living environment and meets growth targets.

Economic, Social & Environmental Responsibility

- Improve the appearance of streetscapes, especially with trees suitable for shade.

Budget/Financial Implications

N/A

Legal Implications

N/A

Community Consultation

As discussed in the Consultation section of the report above, the proposal was advertised to 10 nearby land owners potentially affected by the proposal in accordance with the requirements of Local Planning Scheme No.3 (LPS 3). A total of 6 submissions were received, one indicating no objection and five objecting to the proposal.

Risk Management Implications

Should the applicant lodge a review of the decision with the State Administrative Tribunal, there may be costs involved in defending the decision, particularly if legal Counsel is engaged.

Attachment(s)

1. Existing/Demolition Site Plan;
2. Proposed Site Plan;
3. Proposed Landscape Plan;
4. Proposed Floor Plan;
5. Proposed Front & Side Elevation;
6. Proposed Rear & Side Elevation;
7. Stratton Street Elevation;
8. Fence Elevations part 1;
9. Fence Elevations part 2;
10. Perspective 1;
11. Perspective 2

12. Amended Acoustic Report;
13. Amended Transport Impact Assessment;
14. Location Plan

Advice to Proponent(s)/Submissioners

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 8 June 2017 Ordinary Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

15.2 (OCM 08/06/2017) - MODIFICATION TO PLANNING APPROVAL FOR HEALTH STUDIO (DA16/0100) - LOCATION: 9/153 ROCKINGHAM ROAD, HAMILTON HILL; OWNER: WORLDCLASS HOLDINGS PTY LTD; APPLICANT: KELLY BUCKLE (052/002) (DA17/0219) (G ALLIEX) (ATTACH)

RECOMMENDATION

That Council

- (1) grant planning approval for the modification to DA16/0100 for additional operating hours for the Health Studio at 9/153 Rockingham Road Hamilton Hill, in accordance with the attached plans and subject to the following conditions and footnotes:

CONDITIONS

1. Development may be carried out only in accordance with the details of the application as approved herein and any approved plan. This schedules the use of the land and/or a tenancy. The approved development has approval to be used for Health Studio only. In the event it is proposed to change the use of the tenancy, a further application needs to be made to the City for determination.
2. Hours of operation are restricted between 9.00am to 9.00pm from Monday to Saturday and 10.00am to 5.00pm on Sundays and Public Holidays.
3. Classes after 7.00pm between Monday to Saturday shall adhere to the Noise Management Plan as detailed in the Environmental Noise Assessment prepared by Lloyd George Acoustics (Ref 16023492-01c; dated 10 March 2016)

4. The Health Studio for children's use is restricted to a maximum of 15 students, 15 parents/carers, and 3 staff at any one given time (total 33 persons).
5. The Health Studio for adult use is restricted to a maximum of 15 students and 3 staff at any one given time (total 18 persons).
6. The premises shall be kept in a neat and tidy condition at all times by the owner/occupier to the satisfaction of the City.
7. All outdoor lighting must be installed and maintained in accordance with Australian Standard AS 4282 – 1997 "Control of the Obtrusive Effects of Outdoor Lighting".

FOOTNOTES

1. This is a Planning Approval only and does not remove the responsibility of the applicant/owner to comply with all relevant building, health and engineering requirements of the Council, or with any requirements of the City of Cockburn Town Planning Scheme No. 3 or with the requirements of any external agency. Prior to the commencement of any works associated with the development, a Building Permit is required.
 2. The development shall comply with the requirements of the Building Code of Australia.
 4. The development shall comply with the Environmental Protection Act 1986 which contains penalties where noise limits exceed those prescribed by the Environmental Protection (Noise) Regulations 1997.
 5. You are advised that all waste and recycling must be contained within bins. These must be stored within the buildings or within an external enclosure.
- (2) notifies the applicant and those who made a submission of Council's decision.

COUNCIL DECISION

Background

Site Description

The subject site is located at 153 Rockingham Road Hamilton Hill and contains an existing building comprising of 13 commercial units constructed in the mid-1990's. The site, which backs on to Paulik Way, is contained within a small precinct zoned 'Mixed Business' under the City of Cockburn Local Planning Scheme No.3 (LPS 3). The site abuts commercial development adjacent to the eastern and a portion of the western boundary however also abuts two residential-zoned dwellings which front Paulik Way. The tenancy the subject of this application is Unit 9 (Strata Lot 7) which has an area of 197m² and is in the rear half of the complex.

History

Council granted planning approval for the existing Health Studio (Kelete Dance School) at its meeting held on 14 April 2016. The unit was retrofitted to comply with the requirements of the approved Environmental Noise Assessment prepared by Lloyd George Acoustics (Red 16023492-01c; dated 10 March 2016). Condition 2 of the original approval for the Health Studio limited the hours of operation from Monday to Saturday between 9.00am-9.00pm with no approval for Sunday operations.

Subsequent to the approval, the City received complaints in relation to the following matters:

- Breakout noise onsite which consists of people talking/laughing as they enter and leave the premises, car doors closing and car engines running;
- Reversing alarm noise from vans that drop disabled children off to the premises during the day;
- Number of people within the unit being in excess of the approved number of 15 people; and
- The unit being used on a Sunday.

The City met with the unit owner and the operator of the Health Studio to discuss the complaints that had been made and the outcomes are noted below. The operator was advised that if they seek to operate on Sundays, then a modification to the planning approval is required (subject of this report). They were also reminded of their obligation to comply with the approved Noise Management Plan and the restriction in relation to the number of students permitted at the premises at any one time.

Submission

N/A

Report

Proposal

The applicant is seeking to modify Condition 2 of the original Development Approval for the Health Studio (DA16/0100) in relation to operating hours. The applicant is applying to operate on Sundays between 9.00am-5.00pm in addition to operating between 9.00am to 9.00pm from Monday to Saturday (which is already approved). The applicant is not seeking to alter the maximum number of students within a class at any one time however given the original approval refers to 'people' in lieu of 'students' the applicant is seeking to have this wording altered as most students cannot drive and a parent or carer will sometimes be present for people with disabilities.

Planning Framework

Zoning and Use

The subject site is zoned 'Urban' under the Metropolitan Region Scheme (MRS) and 'Mixed Business' under Local Planning Scheme NO.3 (LPS 3).

The objective of the 'Mixed Business' zone is:

"to provide for a wide range of light and service industrial, wholesaling, showrooms, trade and professional services, which, by reason of their scale, character, operation or land requirements, are not generally appropriate to, or cannot conveniently or economically be accommodated within the Centre or industry zones."

A 'Health Studio' is defined in LPS 3 as:

"Land and buildings designed and equipped for physical exercise, recreation and sporting activities including outdoor recreation".

A Health Studio is a 'P' use in the zoning table as per LPS 3. A 'P' use means that the use is permitted by the Scheme providing the use complies with the relevant development standards and requirements of the Scheme.

Local Planning Policies

Local Planning Policy 3.3 – Health Studios

The purpose of this policy is to provide clarity and direction on the types of health studios within the City as well as general siting and design criteria for such land uses and information required by the City to assess such applications which LPS 3 does not provide for. The policy encourages Health Studios and in particular dance schools to be located in commercial and industrial areas with a readily available supply of parking spaces or a capacity to create additional parking spaces.

Other Relevant legislation

Environmental Protection (Noise) Regulations 1997

The Environmental Protection (Noise) Regulations 1997 (Noise Regulations) adopted under the Environmental Protection Act 1986 allows for maximum levels of noise at certain times of the day and night. The regulations have been designed to ensure acceptable levels are met while being flexible to allow normal activities to occur. Factors such as the amount of traffic, nearby commercial or industrial premises and the time of day all impact on acceptable levels of noise.

Consultation

As part of the City's consultation process, the application was advertised to the adjoining neighbour (who owns both residential properties that abut the subject site fronting Paulik Way) for comment. The neighbour objected to the proposal citing the following reasons:

1. Breakout noise from vehicles and people talking and laughing in the carpark is negatively impacting the amenity of the adjoining residents.
2. Existing Noise Management Plan (NMP) not being adhere to in relation to the cones being incorrectly placed therefore negatively impacting the adjoining residents.
3. Reversing alarms from the vans and vehicles negatively impacts the amenity of the adjoining residents

The adjoining resident did not provide any specific objection related to Sunday operations, however articulated to the City that they are severely impacted by the existing use in general. They have complained that operation of the Health Studio inconveniences them as it impacts their sleeping patterns and general enjoyment of their

dwelling and that the noise associated with the Health Studio is unbearable.

Planning Considerations

Noise

In accordance with the Noise Regulations, the day time levels permitted on a Sunday are less than the levels permitted from Monday-Saturday and furthermore unlike Monday-Saturday where the day time levels are from 7.00am-7.00pm, the Sunday day time level starts at 9.00am. In addition, the operator has mentioned in the application that a maximum of 15 students will be adhered to, as had been approved for the other days of the week.

It should also be noted that Lloyd George Acoustics had previously provided a statement in relation to breakout noise from the site noting that:

'noise levels from persons leaving the premises and talking were assessed to comply with the Noise Regulations and that noise impacts from human voices associated with the Health Studio are likely to be similar in level and nature i.e. short conversations, than human voices from the adjacent residential premises or the users of the other adjacent units to that occupied by Kelete Dance School. Similarly, noise from car doors closing were assessed and found to be fully compliant with the Noise Regulations'

Whilst the adjoining neighbour has complained about the breakout noise omitted from the site and the subsequent impact on their amenity, it has been previously confirmed by Lloyd George Acoustics that the breakout noise complies with the Noise Regulations. As such it would be unreasonable to limit the hours and days of operation of a Health Studio, which is permitted use in the Mixed Business zone, based on noise.

With regards to the complaints about the reversing alarms, given the low frequency, limited duration and time of day (being during the day) and the use of warning alarms on vehicles servicing any of the commercial units, this complaint is not considered reasonable. Therefore the City's Environmental Health Service will not undertake noise monitoring.

Student numbers

The applicant is seeking to alter the wording of Condition 4 on the original Development Approval as it currently refers to 'maximum number of people'. The City limits the number of people that can attend a class at any one time for parking purposes so as to not create a

parking shortfall onsite. The intent of the original condition was to limit the number of students to 15 at any one time. However, the wording in the condition refers to 'people' so it may be misconstrued that parents/guardians/carers cannot be present. Given a parent/guardian/carer would typically drive a child or person with a disability, the modification of Condition 4 to provide further clarity is not deemed to create a parking shortfall. The 15 parents/guardians/carers are however limited to before 7.00pm in accordance with the approved Noise Management Plan in the Acoustic Report. Only adult classes are permitted after 7.00pm.

Acoustic requirements

Should Council support the proposal to permit Sunday classes, the following two conditions from the previous planning approval, approved by Council on 14 April 2016, and should be deleted:

- 1. The development shall be retrofitted within 30 days of the date of this approval to comply with the requirements of the Environmental Noise Assessment prepared by Lloyd George Acoustics (Ref 16023492-01c; dated 10 March 2016). The Building Permit Application is to demonstrate that all recommendations made in the Environmental Noise Assessment prepared by Lloyd George Acoustics (Ref 16023492-01c; dated 10 March 2016) have been incorporated into the proposed development.*
- 2. Within 60 days from the date of this approval (or another timeframe agreed to by the City), an acoustic wall shall be constructed along the boundary of the subject site and the adjoining residential zoned property to the west as marked in red on the approved plan to the satisfaction of the City. In this regard detailed plans for the wall shall be submitted to and approved by the City prior to construction.*

The first condition has already been achieved and has been assessed by the City's Environmental Health Officers. The second condition was contested by the adjoining residential land owner as they did not want an acoustic wall constructed and as such the wall was never built. During the assessment process of the current application, there was mention of possibly increasing the height of the boundary fence. Should both parties choose to amend the height of the fence, this can be dealt with under the *Dividing Fences Act 1961*, which is not administered by the City.

Conclusion

The City acknowledges that the proximity of commercial development to the adjoining two western residential dwellings (owned by the same family) has caused ongoing noise concerns and complaints from those

residents. However, the operator of the Health Studio obtained Council approval to use the unit for that purpose and has retrofitted the premises in order to comply with the approved acoustic report. Furthermore there is a Noise Management Plan in place to further mitigate breakout noise during the sensitive times of the night. Approval of the existing Health Studio to extend operating hours to include Sunday day time hours is considered reasonable and advice from the acoustic consultant indicates that breakout noise from the unit will comply with the Noise Regulations.

It is therefore recommended that Council approve the request to modify the previous planning approval to incorporate the additional operating hours subject to conditions.

Strategic Plan/Policy Implications

Community, Lifestyle & Security

- Provide residents with a range of high quality, accessible programs and services.
- Provide safe places and activities for residents and visitors to relax and socialise.

Economic, Social & Environmental Responsibility

- Create opportunities for community, business and industry to establish and thrive through planning, policy and community development.

Budget/Financial Implications

Should the applicant lodge an application for review of the decision in the State Administrative Tribunal, there may be costs involved in defending the matter, particularly if legal counsel is engaged.

Legal Implications

N/A

Community Consultation

See consultation section of the report above.

Risk Management Implications

Should Council approve the application, there is a risk that noise may not be effectively managed which would impact negatively on the amenity of adjoining residents and therefore continual complaints and continual investigations which involves council resources. Should

Council refuse the application, the applicant may choose to lodge a review of the decision with the State Administrative Tribunal and as such there may be costs involved in defending the decision.

Attachment(s)

1. Site plan;
2. Copy of original Development Approval for the Health Studio (DA16/0100);
3. Approved Environmental Noise Assessment prepared by Lloyd George Acoustics (Red 16023492-01c; dated 10 March 2016);
4. Location Plan.

Advice to Proponent(s)/Submissioners

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 8 June 2017 Ordinary Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

15.3 (OCM 08/06/2017) - SUBDIVISION RETAINING WALLS; LOCATION: LOT 9010 FAWCETT ROAD, MUNSTER; OWNER: ANTHONY BEAMISH; APPLICANT: DENISE TYLER-HARE (052/002) (DA17/0212) (G ALLIEX) (ATTACH)

RECOMMENDATION

That Council

- (1) grant Planning Approval for subdivision retaining walls at Lot 9010 Fawcett Road Munster, in accordance with the attached plans and subject to the following conditions and advice notes:

CONDITIONS

1. All stormwater being contained and disposed of on-site to the satisfaction of the City.
2. No construction activities causing noise and/or inconvenience to neighbours being carried out after 7.00pm or before 7.00am, Monday to Saturday, and not at all on Sunday or Public Holidays.
3. Prior to lodging a Building Permit Application, the plans shall be amended so that retaining walls on the southern side of the lot abut the boundary to the satisfaction of the City.

4. Prior to lodging a Building Permit Application, the plans shall be amended to reflect the reduced retaining wall heights as per Plan No.16307-C1-EW-03 (Retaining Wall Plan and Profile – Wall D) dated November 2016 marked in red to the satisfaction of the City.
5. Prior to commencement of the any development works hereby approved, a detailed Dust Management Plan shall be submitted to and approved by the City of Cockburn (Health Services) and implemented thereafter.
6. A construction management plan (CMP) shall be submitted to and approved by the City prior to the commencement of works. The CMP shall be implemented to the satisfaction of the City. The Construction Management Plan shall address the following items:
 - (a) Access to and from the site;
 - (b) Delivery of materials and equipment to the site;
 - (c) Storage of materials and equipment on the site;
 - (d) Parking arrangements for contractors and subcontractors;
 - (e) Management of construction waste; and
 - (f) Other matters likely to impact on the surrounding properties.

FOOTNOTES

1. This is a Planning Approval only and does not remove the responsibility of the applicant/owner to comply with all relevant building, health and engineering requirements of the City, with any requirements of the City of Cockburn Local Planning Scheme No. 3, or the requirements of any other external agency.
2. With respect to condition 3, the City cannot support the 1m setback as it creates an area of land that cannot be accessed.
3. With respect to condition 5, the detailed Dust Management Plan shall comply with the City's Guidelines for the Preparation of a Dust Management Plan for Development Sites within the City of Cockburn".
4. The development shall comply with the noise pollution provisions of the Environmental Protection Act 1986, and more particularly with the requirements of the *Environmental Protection (Noise) Regulations 1997* (as amended).

5. Retaining wall(s) being constructed in accordance with a qualified Structural Engineer's design and a building permit obtained prior to construction.
- (2) notifies the applicant and those who made a submission of Council's decision.

COUNCIL DECISION

Background

The subject site is 1.1033 hectares of vacant land which consists of challenging contours as the site slopes down significantly from east to west by approximately 7.5m. The site forms part of the Munster- Phase 3 Local Structure Plan, which was endorsed by the Western Australian Planning Commission (WAPC) on 5 February 2010. The structure plan indicates that approximately 2210m² of Bush Forever vegetation exists to the west, a future road to the north of the site and the rest of the land is zoned for residential purposes with a mix of Residential R20 and Residential R40.

On 4 November 2015, the WAPC resolved to conditionally approve a subdivision application to subdivide the site into 11 lots. The applicant has since undertaken engineering analysis of the site and realised that direct vehicular access to lot 2 on the approved subdivision plan would be impractical to achieve. As a result, the WAPC has agreed that the lot configuration can be altered and a new Deposited Plan can be submitted to the WAPC at the end of the subdivision process. The amended lot configuration now results in the creation of 10 lots (Attachment 1).

The conditions of the approved subdivision require the land to be filled, stabilised, drained and graded so that lots can accommodate the intended development, coordinate with existing finished ground levels at the boundaries of the development and contain stormwater onsite. Another condition of the subdivision is to connect to sewer, which is a condition that Water Corporation imposes. After deliberating a few options in relation to connecting the lots to sewer, the applicants only feasible option involves extending the sewer through private property along the side boundary of lot 4 (which requires an easement) and then out along the street frontage to service the lots. As a result, the proposed lot levels and the road level is at the minimum standard that Water Corporation requires in relation to meeting minimum cover levels

whilst also being capable of being serviced by gravity sewer at the correct grade.

The retaining walls proposed along this southern boundary range from 0.5m-2.7m above the Natural Ground Level (NGL). As part of the Building Permit Application, the applicant contacted each affected lot owner for detailed comment on the proposed retaining walls and due to the number of objections received by the neighbours, the applicant has chosen to setback some of the retaining walls on the southern boundary by 1m in an attempt to reduce the impact of the walls. This amended plan with a 1m retaining wall setback was submitted to the City's Statutory Planning team for assessment and approval. The application has been referred to Council for determination as objections were received during this process, removing delegation from the City's administration staff.

Submission

N/A

Report

Proposal

This proposal is for subdivision retaining walls, specifically comprising:

- Retaining walls on the southern boundary of the subject site to facilitate the levels of the proposed new lots, with wall heights ranging from 0.5m-2.7m above the Natural Ground Level (NGL);
- Retaining walls setback 1m from the southern boundary;
- Retaining wall on the northern edge of the lot, with a maximum height of 2.7m; and
- Retaining walls adjacent to the southern boundary of Lot 227 Fawcett Road, Munster at a maximum height of 0.783m above the NGL.

Planning Framework

Zoning and Use

The site is zoned 'Development' and is affected by the DA5 provisions of LPS3 which requires the following:

1. *An approved Structure Plan together with all approved amendments shall apply to the land in order to guide subdivision and development;*

2. To provide for residential development except within the buffers to the Woodman Point WWTP, Munster Pump Station and Cockburn Cement; and
3. The local government will not recommend subdivision approval or approve land use and development for residential property contrary to Western Australian Planning Commission and Environmental Protection Authority Policy on land within the Cockburn Cement buffer zone.

The Munster- Phase 3 Local Structure Plan indicates that the land is zoned R20, R40 and has a component of Bush Forever to the west of the lot.

Local Planning Policy 5.12 Subdivision Retaining Walls (LPP 5.12)

The proposed retaining wall application has been assessed against LPP 5.12. Clause 4 of LPP 5.12 states that retaining walls above 0.5m in height above the NGL where the site abuts existing residential development outside the subdivision area also requires planning approval.

Residential Design Codes (R-Codes)

For clarity purposes, it should be noted that the proposed retaining walls will not be assessed against the R-Codes as the R-Codes only apply to 'Residential Development'. Residential Development is defined in the R-Codes as follows:

'Development of permanent accommodation for people, and may include all dwellings, the residential component of mixed-use development, and residential buildings proposing permanent accommodation'.

Neighbour Consultation

The application has been the subject of public consultation and was advertised in the following ways:

- Letters & plans were sent to all adjoining affected landowners (see table below indicating the maximum retaining height proposed against each neighbouring lot); and
- Onsite consultation with landowners at Lot 237 and Lot 238 Ingrilli Court, Munster on Thursday 11 May 2017 to further discuss and understand impacts.

Address	Maximum height of retaining wall proposed	Objection
No. 20 (Lot 227) Fawcett Road, Munster	0.783m above NGL	No

No. 16 (Lot 237) Ingrilli Court, Munster	1.68m at a maximum in the south western corner of the site.	Yes
No. 14 (Lot 238) Ingrilli Court, Munster	1.55m-1.92m above the NGL	Yes
No. 12 (Lot 239) Ingrilli Court, Munster	1.49-1.92m above the NGL	No
No. 10 (Lot 240) Ingrilli Court, Munster	0.89-1.49m above the NGL	Yes
No. 8 (Lot 241) Ingrilli Court, Munster	0.89m above the NGL (very small section affected)	No
No. 4 (Lot 243) Ingrilli Court, Munster	2.7m above the the NGL (very small section affected)	No

A total of three objections were received during the advertising period which are summarised as follows:

- Objections to the 1m setbacks as it creates a dead zone which cannot be accessed and can turn into a hygiene issue, fire hazard and security issue;
- Height of retaining walls and sand pads will negatively impact amenity of the adjacent landowners and in particular loss of views to trees, northern sunlight and privacy;
- Proposal not in-keeping with the existing lot boundary levels thereby limiting enjoyment of the outdoor living area to adjoining properties;
- Bulk and scale of the retaining walls negatively impacting lifestyle of the adjacent residents ;
- Concerns that future two storey dwellings on the new lots will further negatively impact the overshadowing and privacy to residents; and
- Concerns during construction and the issue of temporary fencing.

The City's comments in relation to the submissions received are discussed throughout the report (predominantly in the amenity section).

Planning Considerations

Sewer Connection & Required Road Levels

The conditions of the WAPC approved subdivision require the land to be filled, stabilised, drained and graded so that lots can accommodate the intended development, coordinate with existing finished ground levels at the boundaries of the development and contain stormwater onsite. Another condition of the subdivision is to connect to sewer, which is a condition that Water Corporation imposes. Typically the sewer would be extended around via the road however given Water Corporation has a minimum grade for gravity sewers of 1:200, this option would require a 90m extension which at minimum grades provides an invert level of $(2.12 + 90/200) = 2.57\text{m AHD}$. The existing

road level at this location is 2.49 m AHD, therefore the sewer would essentially project out of the ground and not meet the Water Corporation's minimum cover requirements.

The second option to extend the sewer along the back of the proposed lots was also considered. The Water Corporation has a minimum lot area requirement for placing sewer in private property, and none of these lots comply with this requirement therefore this option could not be executed.

The final option involves extending the sewer through private property along the side boundary of lot 4 (which requires an easement) and then out along the street frontage to service the lots. It is run at the minimum grade from the existing invert level of 2.51 m AHD, which makes the invert level at the front of lots 1 and 2 at 2.87 m AHD. This is the lowest level that can be achieved within the Water Corporation standards.

Each lot then has to be capable of being serviced by gravity sewer at a grade of 1 in 60 for the combined length of the side and rear boundary, which means a distance of 51m. The applicant also has to account for the level of the services laid below the lot which is 600mm. Therefore lot 2 requires a minimum level difference between the lot level and the gravity sewer connection level of $(0.6 + 51/60) = 1.45\text{m}$.

This combined with the invert level of 2.87 m AHD means a minimum lot level of $(2.87+1.45) = 4.32$ m AHD (rounded up to 4.4 m AHD for simplicity in construction). The road level is then set above the sewer invert level to meet minimum cover levels according to the Water Corporation. The road is also controlled by the levels in lot 9, which are then in turn, governed by the levels in the existing lot 227 on Fawcett Road. The lot levels proposed are (according to the applicant) the lowest that can be achieved and as a result have informed the retaining wall heights.

Drainage Concerns

Having considered the reasoning behind the retaining wall heights and pad levels and also the objections from the adjacent land owners, the possibility of reducing the lot levels was explored by the City's officers. The City's Senior Development Engineer conveyed concern about lots being below the road level as this is not good design practice and can have significant implications for the City in stormwater events (i.e. risk of flooding to dwellings). Whilst its bad design practice to have lots lower than the road, if it were proposed, the City would require the lots to retain a 1 in 100 year storm event (as opposed to 1 in 20 year). Furthermore the City's Senior Development Engineer provided advice in relation to maximum crossover gradients, should the lots be below road

level. This advice was then sent to the applicant and subsequently some minor amendments were noted as being possible (see below).

Amendments to proposal

The applicant was made aware of the strong neighbour objections and was provided with further information from the City's Senior Development Engineer in relation to lots below the road level. The following amendments have been suggested by the applicant:

Lot 4 (rear of 14 and 16 Ingrilli)

- Current lot level is 5.15;
- The verge boundary levels are 4.52 and 4.97, so the lot is set just above the road. The road cannot be lowered as it needs to grade up to ensure there is not too significant a level difference between lot 9 and the end of the cul-de-sac;
- Cannot lower lot 9 because it is already creating a significant level difference with Lot 227 Fawcett Road; and
- Can lower the lot level to 5, so that it would still be higher than the road, however this is the lowest level possible.

Lot 5 (rear of 14 Ingrilli)

- Current lot level is 5.55;
- The verge boundary levels will be 5.33 and 4.97, therefore the lot is set just above the road. The road cannot be lowered as it needs to grade up to ensure there is not too significant a level difference between lot 9 and the end of the cul-de-sac;
- Cannot lower lot 9 because it is already creating a significant level difference with Lot 227 Fawcett Road;
- Can lower the lot level to 5.4 so that it is still higher than the road, however this is the lowest level possible.

Lot 7 (rear of 10 Ingrilli)

- Current level 6.25;
- Road grades as described above for lots 4 and 5, mean that the verge boundary levels are 5.71 and 6.09; and
- Can lower the lot level to 6.1 so as to be just above the road, however this is the lowest level possible.

Given the suggested lot level amendments above, the subsequent retaining walls changes that can be made are as follows (see image below):

- Retaining wall to lot 4 is reduced to 0.98m high wall;
- Retaining wall to lot 5 is reduced to 1.35m high wall; and
- Retaining wall to lot 7 is reduced to 1.92m high wall.

All of the retaining wall changes that can be made as noted above are directly adjacent to the three residential properties that objected to the

proposal. Given the new lot boundaries do not align with the existing residential lots, there are still sections of retaining wall associated with lots that remain unchanged which impact the residents on Ingrilli Court. Should Council approve the retaining walls, it is recommended that the plans be amended prior to lodgement of a Building Permit to reflect the above mentioned reductions.

Setback of Retaining Walls

The proposed 1m setback to some of the retaining walls on the southern side of the subject lot came as result of previous consultation between the applicant and the adjacent landowners. The applicant was seeking to reduce the impact of the walls to the adjacent residential properties whilst also seeking to avoid further consultation with the affect properties. The proposed 1m setback is more of a concern to the adjacent landowners as this dead zone lends itself to be a hygiene issue, possible fire hazard (especially given the lot is within a bushfire prone area) and a possible security concern. Officers don't support the 1m setback favourably for similar reasons to those raised by the adjacent residents. Therefore should Council approve the proposal a condition should be imposed in relation to amended plans being required prior to lodgement of the Building Permit which show the retaining walls on the southern boundary of the subject site.

Amenity Impacts

Objections in relation to loss of amenity in terms of losing views of trees, birds, lake etc. are considered unreasonable given the objecting owners purchased a lot adjacent to a vacant site with challenging topography which at the time was always intended to be developed with housing. Even without any retaining, a future dwelling of one or two stories which is as of right under the R-Codes would obstruct the current open vista above the fence line.

In relation to the objections received relating to overshadowing, whilst the proposed retaining walls and associated lot levels will form the new natural ground level, any dwelling proposed on these future lots will be assessed against the R-Codes. The overshadowing calculations as stated in Part 5.4.2 of the R-Codes take into account the natural ground level of the adjacent lot, as this is specifically noted in the R-Codes. Should overshadowing be an issue in the future, dwellings may be further stepped back from the rear boundary in order to prevent the shadow cast from exceeding maximum requirements.

Similarly, overlooking from any future dwelling would be assessed when proposed on the future lots. It should be noted that a 1.8m high dividing fence will ensure overlooking from the proposed ground levels at the subject site does not occur. Should future dwellings be double

storey, the R-Codes setback provisions will be applied to mitigate overlooking. Whilst the City appreciates the neighbours' concerns in relation to bulk and scale, it is important to note that the applicant is making a concerted effort to follow the existing and challenging topography, whilst meeting Water Corporation requirements in relation to sewer and the City's drainage requirements.

Temporary Fencing

The issue of temporary fencing was raised as a serious concern for one of the adjoining land owners due to security concerns. The applicant indicated that they have no control over the contractor who does the site works. Given the sensitivity of the matter, a Construction Management Plan (CMP) could be imposed as condition to ensure minimal disruption to residents. The CMP will address issues such as temporary fencing, staging of works, noise, material delivery and storage, contractor parking, traffic generation of construction vehicles and access. It should be noted however that temporary fencing is standard across most developments that propose to install retaining on the boundary or replace fencing and is most often negotiated between landowners without involvement by the City.

Conclusion

It is acknowledged that the proposed retaining will have a greater impact on adjoining residents than the current situation which provides an open vista. However, due to a number of engineering reasons (supported by the City's Engineering Services), it has been demonstrated to the satisfaction of the City that retaining is required along the boundaries of the subject site. The proposed retaining walls (with suggested agreed modifications) are therefore acceptable to the City as this will allow for development on the subject lot to progress whilst minimising the impacts to the adjacent existing residents as much as possible. It is therefore recommended that Council approve the proposal subject to conditions.

Strategic Plan/Policy Implications

City Growth

- Ensure planning facilitates a desirable living environment and meets growth targets.

Leading & Listening

- Deliver sustainable governance through transparent and robust policy and processes.

Budget/Financial Implications

Should the applicant lodge a review of the decision with the State Administrative Tribunal, there may be costs involved in defending the decision, particularly if legal counsel is engaged.

Legal Implications

N/A

Community Consultation

Refer to consultation section of the report.

Risk Management Implications

Should the applicant lodge a review of the decision with the State Administration Tribunal, there may be costs involved in defending the decision, particularly if legal Counsel is engaged.

Attachment(s)

1. Retaining Wall Site Plan;
2. Retaining Wall Plan & Profile D;
3. Retaining Wall Plan & Profile;
4. Draft Amended Elevation; and
5. Location Plan

Advice to Proponent(s)/Submissioners

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 8 June 2017 Ordinary Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

15.4 (OCM 08/06/2017) - TELCOMMUNICATIONS INFRASTRUCTURE - LOCATION: 25 (LOT 3) HARPER ROAD, BANJUP; OWNER: MO ZHAI & TIE NING CHANG; APPLICANT: AURECON AUSTRALASIA (DA17/0072) (6192574) (R TRINH) (ATTACH)

RECOMMENDATION

That Council :

- (1) refuse to grant planning approval for Telecommunications Infrastructure at No. 25 (Lot 3) Harper Road, Banjup, based on

the following reasons:

Reasons

1. The benefit of improved telecommunications services is not balanced with the visual impact on the surrounding area.
 2. The proposal is incompatible with the surrounding area and is therefore inconsistent with the aims of Local Planning Scheme No. 3.
 3. The proposed use is likely to detract from the visual amenity of surrounding and nearby residents which is undesirable.
 4. The proposed use is likely to detract from the rural character of the area which is undesirable.
 5. The proposed use is likely to detract from the streetscape of the area which is undesirable.
- (2) notifies the applicant and those who made a submission of Council's decision.

COUNCIL DECISION

Background

Site Description

The subject site is 20,748m² in area, is relatively flat and consists of a single dwelling with several outbuildings and contains a substantial amount of mature vegetation. The lot is surrounded by lots of similar size used for predominantly residential purposes. The site also abuts a Water Corporation pipeline at the rear of the property that connects through between Emma Treeby Reserve and Bosworth Reserve (approximately 340m east of the lot). These reserves extend north and south to Regional Reserves via a walking trail. The lot is located approximately 250m from the urban residences on the western side of Tapper Road in Atwell.

The proposed development is being referred to Council for determination as staff does not have delegation to determine the

application as objections were received during the public consultation period.

Submission

N/A

Report

Proposal

The telecommunications infrastructure is proposed to be located 15m from the northern boundary and setback further than 10m from the eastern and western boundaries. The proposed development is located behind the existing dwelling and a large outbuilding.

The proposed telecommunications infrastructure consists of a 104.16m² hardstand compound that includes:

- Ground based equipment shelter painted in 'pale eucalypt';
- 35m monopole with spoke headframe (36.8m total height) (unpainted);
- 3x panel antennas;
- 15x remote radio units;
- 6x combiners; and
- 3x break out boxes.

An Electro Magnetic Emissions (EME) report dated 21/11/2016 was supplied with the application which demonstrated that the maximum EME level calculated for the proposed systems at this site is 3.19V/m; equivalent to 27.045mW/m² or 0.5% of the public exposure limit (Attachment 8).

Planning Framework

Metropolitan Region Scheme (MRS)

The subject site is zoned 'Rural – Water Protection' under the Metropolitan Region Scheme (MRS) and the proposal is consistent with this zone.

Local Planning Scheme No. 3 (LPS 3)

The subject site is located within the 'Resource' zone, Development Contribution Area 13 and the Bushfire Vulnerability Area under LPS 3.

The objective of the 'Resource' zone under LPS 3 is to provide for the protection of the Perth Metropolitan underground water resource. The

lot is located within the Jandakot Groundwater Protection Area and subject to SPP 2.3.

'Telecommunications Infrastructure' is defined by LPS 3 but not listed in the zoning table. Therefore the use is considered a 'use not listed' and is considered an 'A' use (discretionary subject to advertising) and is generally not permitted unless the local government has exercised its discretion and has granted planning approval giving special notice in accordance with clause 64(3) of the deemed provisions within the *Planning and Development (Local Planning Schemes) Regulations 2015*.

State Planning Policy 2.3 – Jandakot Groundwater Protection (SPP 2.3)

The lot is located within the Jandakot Groundwater Protection Area contained within SPP 2.3 and therefore subject to assessment under this policy. The objectives of SPP 2.3 are to ensure that any development does not compromise the groundwater.

The use is considered 'Telecommunications Infrastructure' under SPP 2.3, which has the same definition as LPS 3 and means "*land used to accommodate any part of the infrastructure network and includes any line, equipment, apparatus, tower, antenna, tunnel, duct, hole, pit or other structure used, or for use in or in connection with, a telecommunications network*". This is considered a use that is 'compatible with conditions' under SPP 2.3 and means that the Local Government may use its discretion to determine an application after having due regard for the advice from the Department of Water.

State Planning Policy 5.2 – Telecommunications Infrastructure (SPP 5.2)

The intention of SPP 5.2 is to balance the need for telecommunications infrastructure with the visual character of local areas. The proposed development is not considered a 'low-impact facility' and therefore not exempt from requiring planning approval under the *Commonwealth Telecommunications Act 1997*.

SPP 5.2 notes that telecommunications infrastructure is generally located at high points to be effective. This means that these structures are likely to be visible to the public. SPP 5.2 requires assessment of the benefit of improved telecommunications services balanced with the visual impact on the surrounding area.

The policy measures of SPP 5.2 consider the following criteria:

- Context;
- Visual impact;

- Social/Cultural Heritage impact;
- Design;
- Height;
- Materials/Colours;
- Environment
- Network Coverage; and
- Co-location of infrastructure.

Consultation

Neighbour Consultation

The proposal was advertised via mail-out to 249 nearby landowners to a radius of 500m that were seen to potentially be affected by the proposal in accordance with the requirements of Local Planning Scheme No. 3 (LPS 3) and also advertised on the City of Cockburn website that attracted comments from landowners beyond the 500m radius. A total of 92 submissions were received, 13 indicating no objection and 79 objecting to the proposal. A total of 63 of the 79 objections (79.7%) and 8 of the 13 non-objections (61.5%) were from landowners within the 500m radius. The remaining 16 objections and 5 non-objections were received from landowners beyond the 500m radius. Multiple objections were received from some properties that equated to 85 households providing a response.

The main issues raised during consultation include:

- Impact on visual amenity;
- Concerns about the height of the pole;
- Inappropriateness and inconsistency within a rural area;
- Should be located in alternative location;
- Negative impact on property values;
- Impact on future development potential;
- Health concerns and risks;
- Diminished aesthetic value;
- Mobile coverage;
- Approval would set precedence;
- Impact on natural environment;
- Aircraft safety
- Beneficial for areas other than where it is located;
- Noise; and
- Proximity to houses.

External Agencies

The application was referred to the Department of Water (DoW) as required by State Planning Policy 2.3 – Jandakot Groundwater Protection (SPP 2.3). The DoW had no objection to the development subject to conditions 13 and 22 of Water Quality Protection Note No.

25 (WQPN 25). Conditions 13 and 22 refer to the storage control of hazardous substances to minimise and eliminate risk of groundwater contamination.

Planning Considerations

LPS 3, SPP 2.3 and SPP 5.2 allow for telecommunications infrastructure to be developed on this lot if the benefits of improved telecommunications services are balanced with the visual impact on the surrounding area.

Context

The area consists mostly of lots greater than 2 hectares and used predominantly for residential purposes. Lots within this area generally consist of dwellings that are single or double storey with associated outbuildings. LPS 3 and SPP 2.3 restrict the clearing of land for any purpose other than approved development. Most lots within this area are heavily vegetated with native trees and shrubs which add to the rural character enjoyed by residents.

The subject site consists of a single storey dwelling with multiple outbuildings that do not exceed a height of 6m. The trees located on this property are scattered mainly on the northern and eastern sides. The tallest trees on this property are approximately 30m in height with most of the trees not exceeding 10m in height. The scattered vegetation on the lot would not screen the proposed development from the northern or western boundaries at the ground level. The proposed development is likely to be seen from all directions as the line of sight is not screened by any vegetation of similar heights.

The aesthetics of the area would likely be disrupted by the proposed telecommunications infrastructure and could appear out of place in the rural setting. Telecommunications infrastructure is a use that can be considered within the Resource zone but the impact of such development can only be measured by those directly impacted within close proximity of the development.

Visual Impact

The proposal would be visible from most properties throughout the local area. The scale of the development would result in the views of the natural environment from Bosworth Reserve and walking trail being obstructed by the telecommunications tower that protrudes well above trees and other structures in the area. The lack of vegetation on the site would not screen the proposed development from most directions and will be clearly visible from most residences to the urban area west

of Tapper Road. The lots within 100m of the subject site would also have unobstructed views of the structure.

The structure would be visible from the street and far greater in height than the tallest structure on the property. Trees on the property would slightly alleviate the visual impact of the tower but would not completely screen the tower from view in any direction.

Social/Cultural Heritage Impact

The proposal, if approved is not likely to cause a detrimental impact on any social or cultural heritage matter and therefore in this instance, this consideration is not applicable.

Height

The proposed proposal, if approved will protrude well above most trees in the area and the height is required to provide maximum coverage. Many objections were received regarding the height of the structure and residents generally felt that the height of the pole is unreasonable. The height is necessary for telecommunications infrastructure because they should be above any obstructions to operate effectively. In relation to the purpose of the infrastructure proposed, the height is consistent with most other monopoles and considered reasonable given the optimal requirements for telecommunications infrastructure to operate as mentioned in SPP 5.2. Requesting that the applicant reduce the height of the pole is not reasonable given its intended function.

Materials/Colours

The proposed materials and colours of the telecommunications infrastructure are pale eucalypt for the ground based structures with the pole remaining unpainted (metal colour). The applicant advised that these colours will be sympathetic to the surrounding landscape and sky (in the case of the pole). If Council did consider approval of the proposal, the colours would be considered to be appropriate in order to best reduce the impact of the proposal on the landscape.

Environment

The lot and surrounding area is located within Jandakot Groundwater Protection Area and the objectives of SPP 2.3 are to protect the groundwater and to maintain or increase natural vegetation cover over the policy area. The proposed development is consistent with this objective as the proposal is located in a position that results in no significant vegetation to be removed and the proposal does not have an impact on the groundwater mound. Therefore, the proposal if approved is unlikely to cause any significant environmental impacts.

Network Coverage

Comments raised during the advertising period generally indicated that Vodafone had good coverage throughout the surrounding area. 9 comments indicated that that poor mobile reception existed, while 43 comments indicated an acceptable level of service was currently available in the area. Based on the resident comments, there does not appear to be a clear network issue in the immediate vicinity of the subject site which is the area that would be most impacted by the proposal.

Concerns were raised by surrounding residents that the structure would not provide an immediate benefit to the surrounding lots in the area and would be placed in that location for the benefit of future urban areas in nearby Treeby and Piara Waters. Further development is limited within the Banjup area but those likely to benefit most from the proposed infrastructure would be those living within a close proximity to the subject site. The areas beyond this may however also obtain some benefit from the proposal as a consequence of the height and location of the structure.

Co-location of Infrastructure

The assessment criteria for all planning applications are conducted on a case by case basis. However, SPP 5.2 requires that telecommunications infrastructure be co-located with other carriers where possible. The applicant has not indicated why it is not possible to collocate with other carrier's infrastructure and it is not known where the closest Telstra or Optus infrastructure is located in proximity to the site. Given the recent application for an Optus Tower, also in Harper Road Banjup which was refused by Council at the 9 February 2017 ordinary Council Meeting, it may suggest that there may not be Telstra or Optus infrastructure in close proximity. Some objections received expressed concern that approval of this proposal may set precedence for other telecommunications infrastructure to be located in the area. These comments are not considered to have planning merit because it would be inconsistent with SPP 5.2 and the planning framework. However, if Council approves this proposal, it may be likely that other carriers may wish to collocate on this site (although the City has no information indicating that this will necessarily occur).

Non-Planning Matters Raised

Health Concerns

Health concerns and risks were raised as a major concern for most residents who lodged objections. The applicant provided an EME

report dated 21/11/2016 found on the Radio Frequency National Site Archive website (<http://www.rfnsa.com.au>) demonstrated a maximum EME level calculated for the proposed systems at this site as 3.19V/m; equivalent to 27.045 mW/m² or 0.5% of the public exposure limit (Attachment 8).

The acceptable EME levels are required to comply with the Australian Communications and Media Authority (ACMA) *Radiocommunications Licence Conditions (Apparatus Licence) Determination 2003*. The Australian Radiation Protection and Nuclear Safety Agency (ARPANSA) is the Commonwealth agency that measures and limits the EME levels for human exposure to radiofrequency and therefore local planning controls should not address health or safety standards for telecommunications infrastructure. Therefore the health concerns and risks mentioned are not valid planning considerations that can be considered as part of this assessment.

Future Development Potential

The subject site and surrounding area is located within the Jandakot Groundwater Protection Area and the objectives of SPP 2.3 are to protect the groundwater and to maintain or increase natural vegetation cover over the policy area. SPP 2.3 also considers land use intensification as a potential threat to the Groundwater Protection Area. The majority of objections received commented on the impact of the structure on any future development potential. The lot and surrounding area is zoned 'Rural – Water Protection' under the MRS and there is no seriously entertained planning documents to suggest that this will change. Therefore the impact on future development potential is not a valid planning consideration as the application can only be assessed under the current statutory framework or seriously entertained documents and not speculation.

Property Values

The vast majority of objections raised concern that the proposal will result in a negative impact on property values. The statutory planning framework does not have criteria to measure or consider property values. Therefore the impact on property values is not a valid planning consideration.

Aircraft Safety

Several surrounding landowners expressed concern about aircraft safety. The height of the proposal is well below the height that would impact aircraft safety. Therefore the impact to aircraft safety in the vicinity is insignificant.

Noise

Noise was raised as an issue of concern but it does not have planning merit as telecommunications infrastructure has not previously been known to cause an unreasonable noise level in residential areas. The proposed development would also need to comply with the *Environmental Protection (Noise) Regulations 1997* (as amended).

Distance from Dwellings

The distance from houses was also raised as a concern from objectors and was not considered to have planning merit as there is no prescribed distance of telecommunications infrastructure within SPP 5.2. SPP 5.2 specifically states that buffer zones or setback distances should not be included as a planning control contained in Local Planning Schemes or Local Planning Policies.

Conclusion

It is acknowledged that improvements in information technology infrastructure such as telecommunications are necessary across all areas. It is then important to balance the negative impact on visual amenity with the need for the infrastructure and this is of particular importance in areas where there is an established rural character such as Banjup. It is evident that the installation of telecommunications infrastructure on the subject site will detract from the rural and visual amenity of surrounding residents and the area. It is also evident from the submissions received by local residents that there does not appear to be an overwhelming need for better network coverage in the local area.

Therefore, the benefits of improved telecommunications services to the local area do not appear to be balanced with the visual impact on the area which will be significant. The proposal if approved would detract from the rural character and amenity of the area which would be inconsistent with provisions of LPS 3. For these reasons, it is therefore recommended that Council refuse the application.

Strategic Plan/Policy Implications

City Growth

- Ensure planning facilitates a desirable living environment and meets growth targets.

Community, Lifestyle & Security

- Provide for community facilities and infrastructure in a planned and sustainable manner.

- Advocate for improvements to information technology infrastructure such as the NBN rollout.

Economic, Social & Environmental Responsibility

- Create opportunities for community, business and industry to establish and thrive through planning, policy and community development.
- Sustainably manage our environment by protecting, managing and enhancing our unique natural resources and minimising risks to human health.

Budget/Financial Implications

Should the applicant lodge a review of the decision with the State Administrative Tribunal, there may be costs involved in defending the decision, particularly if legal Counsel is engaged.

Legal Implications

N/A

Community Consultation

The application was advertised to 249 nearby landowners within a 500m radius in accordance with clause 64(3) of the deemed provisions within the *Planning and Development (Local Planning Schemes) Regulations 2015*. A total of 92 submissions were received during the advertising period. See Consultation section of the report above.

Risk Management Implications

Should the applicant lodge a review of the decision with the State Administrative Tribunal, there may be costs involved in defending the decision, particularly if legal Counsel is engaged.

Attachment(s)

1. Site Map
2. Site Location Access Plan
3. Site Setout Plan
4. Elevation Plan
5. Photo Montage Location Map
6. Photo Montage 1
7. Photo Montage 2
8. Electro Magnetic Emissions Report

Advice to Proponent(s)/Submissioners

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 8 June 2017 Ordinary Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

15.5 (OCM 08/06/2017) - PROPOSED SCHEME AMENDMENT NO.124 (LOT 432 RODD PLACE, HAMILTON HILL) (109/124) (D DI RENZO) (ATTACH)

RECOMMENDATION

That Council

- (1) in pursuance of Section 75 of the Planning and Development Act 2005 amend the City of Cockburn Town Planning Scheme No. 3 ("Scheme") by:

Recoding portion of Lot 432 Rodd Place, Hamilton Hill from 'Residential R30' to 'Residential R30/40'.

- (2) note the amendment referred to in resolution (1) above is a 'standard amendment' as it satisfies the following criteria of Regulation 34 of the Planning and Development (Local Planning Schemes) Regulations 2015:

an amendment relating to a zone or reserve that is consistent with the objectives identified in the scheme for that zone or reserve;

an amendment to the scheme so that it is consistent with a region planning scheme that applies to the scheme area, other than an amendment that is a basic amendment;

an amendment that would have minimal impact on land in the scheme area that is not the subject of the amendment;

an amendment that does not result in any significant environmental, social, economic or governance impacts on land in the scheme area;

any other amendment that is not a complex or basic amendment.

- (3) upon preparation of amending documents in support of

resolution (1) above, determine that the amendment is consistent with Regulation 35 of the Planning and Development (Local Planning Schemes) Regulations 2015 and the amendment be referred to the Environmental Protection Authority (“EPA”) as required by Section 81 of the Act, and on receipt of a response from the EPA indicating that the amendment is not subject to formal environmental assessment, be advertised for a period of 42 days in accordance with the Regulations.

COUNCIL DECISION

Background

The subject site is Lot 432 Rodd Place, Hamilton Hill (see Location Plan - Attachment 1). A portion of the site (4504sqm) is zoned ‘Residential R30’, with 3131sqm of the northern portion reserved for ‘Parks and Recreation’ pursuant to City of Cockburn Town Planning Scheme No. 2 (“the Scheme”).

Vehicular access to the residential zoned portion of the subject site is from Rodd Place to the east.

The subject land was formally owned by the City of Cockburn in freehold, and the current reserve and zoning configuration was identified through the Phoenix Rise Master Plan (adopted in 2006), to enable residential development to occur overlooking redeveloped public open space (“POS”). Amendment No. 38 to the Scheme implemented these Phoenix Rise zoning changes, with most of this area, including the residential portion of the subject land, being zoned ‘Residential R25’.

The subject land was subsequently included in the Phoenix Revitalisation Strategy (2009) study area, and the residential zoned portion of the land was recoded from R25 to R30 in 2010 as part of Amendment No. 76.

The subject land was then identified in the City’s Land Management Strategy as being land available for sale. In line with this, the City subsequently sold the land in 2014 to Rodd Place Development Pty Ltd. The purchaser was required to purchase the entire Lot 432 and subsequently cede the portion of the land reserved for recreation back to the City free of cost.

It was also a requirement that the purchaser upgrades the reserved land and redesign and develop the stormwater sump to the satisfaction of the City. This was intended to ensure that coordinated redevelopment occurs, with a positive relationship between the residential component and future POS component.

On 2 December 2014 a development application for 47 multiple dwellings on the residential zoned portion of the land was approved by the City, with the northern portion of the site to be redeveloped for POS which would be ceded to the City.

At the time that this approval was issued, *State Planning Policy 3.1 - Residential Design Codes* ("R-Codes") required the 'density' of development in R30 coded areas to be assessed under the 'plot ratio' controls specified in Part 6 of the R-Codes, allowing for a plot ratio of 0.5:1 on the subject site. As the development proposed a plot ratio of 0.44:1, the proposal was considered compliant in this respect.

Subsequent to the approval being issued the Western Australian Planning Commission ("WAPC") amended the R-Codes, to require development within areas coded less than R40 (i.e. including the subject site) be assessed under Part 5 of the R-Codes rather than Part 6. Part 5 contains 'minimum site area per dwelling' requirements which are not able to be varied, and would limit the number of dwellings that could be approved on the subject site to around 25.

The development approval was valid for a period of two years (consistent with the Planning and Development (*Local Planning Schemes*) *Regulations 2015*) during which time the development needed to be 'substantially commenced' in order for the approval to remain valid.

The development was not substantially commenced within the required two year time frame, and therefore the 2014 approval is no longer valid. This means that any new development applications for the subject site must be assessed against the current R-Codes requirements, and therefore the previously approved 47 multiple dwellings cannot be approved.

It is on this basis that the landowner has applied to rezone the 'Residential' zoned portion of the site from R30 to R40, and they assert that this will enable them to seek approval for the same development (47 multiple dwellings) previously approved by the City prior to the changes to the R-Codes.

Submission

The request to recode the 'Residential' zoned portion of the subject land from R30 to R40 has been received from TPG Place Match on behalf of the landowner.

Report

Proposed Amendment No. 124 seeks to recode the 'Residential' zoned portion of the subject land from 'Residential R30' to 'Residential R40'. The current zoning of the subject land and surrounding area is shown in Attachment 2.

Proponent Justification

The proponent's amendment request is included at Attachment 3, and includes justification for the proposal (see page 23). The proponent has stated that the primary purpose of the recoding is to allow them to seek approval for the 47 multiple dwellings that were approved by the City in 2014 prior to the change to the R-Codes for multiple dwellings in R30 coded areas.

It is their assertion that the proposed recoding will allow the site to be developed for the previously approved, high quality 'multiple dwelling' outcome, which will enhance the POS on the northern portion of the site, deliver a bona fide 'public park', provide passive surveillance over that park, and provide an appropriate transition between the existing split coded R35/80 site to the east of the site and the existing R30 properties to the west.

While this may be the proponents' intention, they are not bound to the proposal that was previously approved, and recoding of the subject land will mean that they, or a future landowner, can apply for any development at a coding of R40.

Attachment 3 (Figure 8) compares a grouped and multiple dwelling development scenario. This is intended to demonstrate that the proposed R40 multiple dwelling scenarios provides better surveillance of the POS, and achieves a greater setback to existing residential development to the south.

However, if the site is recoded the landowner is not bound to this dwelling configuration, and 34 single storey grouped dwellings across the site (offering minimal passive surveillance, and minimal setback to adjacent residential development) could be the possible development outcome.

It is therefore critical that the proposed recoding be assessed on its own merits.

Assessment of Proposal

The subject land is located within the Phoenix Revitalisation Strategy area; therefore consideration must be given to the Strategy in considering the appropriateness of this proposal.

To summarise, the residential codings in the Phoenix Revitalisation Strategy were designated generally as follows:

- * R40 and greater within the 400m walkable catchment of the Phoenix Activity Centre
- * R30 within the 400m – 800m catchment of the Phoenix Activity Centre
- * R30/40 adjacent to POS with design guidelines/criteria contained within a Local Planning Policy. This sought better design outcomes opposite, abutting or adjacent to POS; and the provisions provide an opportunity to achieve a density bonus subject to specific dwelling design requirements. The specific requirements aim to provide a variety in the design, height and roofline of dwellings and maximise passive surveillance of POS areas
- * Bethanie Illawong Aged Care site (1 Rodd Place adjacent to the subject land) was recoded from R30 to a split coding of 'R35/R80 in response to the identified specific need for more aged care dwellings. To ensure appropriate development of this site there were extensive and detailed provisions included in the Scheme, and a 'Restricted Use' was included to ensure the site is only developed to facilitate more aged and dependent persons accommodation.

The R30 coding of the subject land was therefore left unchanged, given its distance from the Phoenix Activity Centre.

As outlined above, codings of R40 were only designated within the 400m walkable catchment of the Phoenix Activity Centre. Therefore recoding of the subject land to R40 is not considered to be justified, and would effectively represent an ad hoc 'spot rezoning' within the current planning framework.

The argument itself that an R40 coding will result in increased housing diversity is not supported, as the Phoenix Revitalisation Strategy provided an appropriate range of densities in appropriate locations to

facilitate housing diversity and to help achieve infill targets set out in Directions 2031 and Beyond.

It is however noted that the Phoenix Revitalisation Strategy did designate split codings of R30/40 adjacent to POS. The subject land includes a large portion of POS, and it is therefore considered appropriate to give consideration to a split coding of R30/R40 for the subject land.

With the split R30/40 coding the lower R30 coding applies as of right, and should the higher coding of R40 be sought the criteria set out in Local Planning Policy 1.2 'Residential Design Guidelines' must be met.

A split coding of R30/40 is consistent with the residential coding designations of the Phoenix Revitalisation Strategy, and would also provide a transition from the Aged Care site which is coded 'R35/R80'.

A coding of R30/40 as an alternative to the proposed R40 coding has been discussed with the proponent who has provided preliminary support for this approach.

Applying R30/40 Split Coding

The criteria for the split codings are set out in Local Planning Policy 1.2 'Residential Design Guidelines', under clause 15, as follows:

Split Coded R30/40 Lots

Split coded residential lots which are located opposite or adjacent to Public Open Space (POS) may be developed up to the stated maximum R40 density, where development is consistent with the requirements of this policy and the following criteria:

- 1. At least one of the dwellings is two storey or incorporates a habitable mezzanine/loft (excluding bedrooms) in order to create variety in design and height and provide opportunity for surveillance of the POS;*
- 2. New dwellings located on the front portion of a lot should have major windows fronting the street, and must not be orientated to solely face internal driveways;*
- 3. Wherever possible rear dwellings should be designed so that significant sections of the front elevations can be seen from the street (i.e. major openings to internal living areas);*
- 4. Provision of an outdoor living area within the front setback of an existing or proposed front dwelling which complies with the*

requirements of Section 8 of this Policy in order to promote surveillance of the POS;

5. *Development on lots larger than 1500m² shall also demonstrate a suitable level of variety in design and height and promote surveillance of the POS.*

It is considered that these criteria, in conjunction with the other provisions of the policy, would ensure that development at a coding of R40 would need to provide good surveillance of the POS, and be designed with visual interest.

From a design perspective, the previously approved multiple dwellings are considered to meet this criteria.

This approach would also require a development application for any development at an R40 coding; therefore preventing the land from being subdivided into R40 sized lots which could result in development that lacks the cohesion that is achievable through comprehensive development of the site.

To compare the two codings, under a coding of R30 the subject land could be developed for an estimated 25 grouped or multiple dwellings (average site area of 300sqm). A coding of R40 could yield 34 grouped dwellings or 47 multiple dwellings (average site area of 220sqm for grouped dwellings; 180sqm for multiple dwellings).

Pursuant to the R-Codes the maximum building heights, minimum open space, and street setbacks requirements are the same for R30 and R40.

Traffic and Parking

Vehicular access to the subject land is from Rodd Place, which is an access road.

Given that development at a coding of R40 would potentially yield more dwellings, consideration must be given to the impact of additional vehicle movements on Rodd Place.

Under a coding of R30 the possible yield of 25 dwellings may generate an estimated 150 vehicle trips per day (based on 5-6.5 daily vehicle trips per large unit or townhouse).

A coding of R40 could yield 47 multiple dwellings which are estimated to generate a maximum of 235 vehicle trips per day (based on 4-5 daily vehicle trips per smaller grouped/multiple dwelling).

Development at a coding of R40 therefore has the potential to increase daily vehicle movements on Rodd Place by 85.

Currently Rodd Place has seven dwellings on the northern side, and 22 aged care units on the southern side (Bethanie Illawong Aged Care, also accessed directly from Southwell Crescent). Current development is therefore likely to generate a maximum of 156 daily vehicle trips. In conjunction with the anticipated 235 vehicle trips under an R40 coding development scenario for the subject land this is a total of 391 vehicle trips per day.

Rodd Place is classified as an 'Access Road', and the maximum desirable volume for such roads is 3000 vehicle trips per day (Main Roads WA - Road Hierarchy for Western Australia Road Types and Criteria).

It is noted that future redevelopment in Rodd Place in accordance with the current residential codings may result in twice as many residential dwellings to the north (if each were to be subdivided), and an additional 108 aged care dwellings to the south (noting access would also be possible from Southwell Crescent).

Should maximum re-development/additional development opportunities be utilised by those landowners (excluding the subject land) there is a potential total of 742 vehicle movements on Rodd Place. In conjunction with development of the subject land at an R40 coding (estimated 235 vehicle trips per day); this is a estimated maximum total of 977 vehicle trips per day on Rodd Place. This is still substantially less than the 3000 vehicle trips per day that the road has capacity for.

It is therefore considered that the total anticipated daily vehicle trips on Rodd Place under an R40 coding scenario for the subject land, and factoring in possible additional development of other properties in Rodd Place, would be acceptable and within the design capacity of the existing road.

Vehicle parking is required to be addressed in accordance with the requirements of the R-Codes, which includes requirements for on-site visitor parking. The previously approved development application for 47 multiple dwellings (contained within Attachment 2 – page 18) demonstrated that the resident and visitor parking could be accommodated on the subject land without impacting of the amenity of the adjacent area.

Conclusion

It is considered that there is insufficient justification to support the proposed recoding of the subject land from R30 to R40, given the

residential coding principles that underpinned the Phoenix Revitalisation Strategy.

However, the Phoenix Revitalisation Strategy did designate R30/40 split codings adjacent to POS to improve passive surveillance and built form interest. Given that the subject land will be adjacent to POS it is considered justified to recode it from R30/40. There is preliminary support for this approach from the proponent.

It is therefore recommended that the Council in pursuance of Section 75 of the *Planning and Development Act 2005* amend the Scheme by recoding the residential zoned portion of Lot 432 Rodd Place, Hamilton Hill from 'Residential R30' to 'Residential R30/R40' to be advertised for public comment.

Strategic Plan/Policy Implications

City Growth

- Continue revitalisation of older urban areas to cater for population growth and take account of social changes such as changing household types.
- Ensure growing high density living is balanced with the provision of open space and social spaces.
- Ensure a variation in housing density and housing type is available to residents.

Community, Lifestyle & Security

- Create and maintain recreational, social and sports facilities and regional open space.

Budget/Financial Implications

The fee for processing this proposed Scheme Amendment has been calculated in accordance with Schedule 3 of the *Planning and Development Regulations 2009*, and has been paid by the proponent.

Legal Implications

N/A

Community Consultation

If initiated by Council, Amendment No. 124 will be advertised in accordance with the requirements of Section 47 of *Planning and Development (Local Planning Schemes) Regulations 2015*.

This requires the proposal to be advertised for a minimum of 42 days in the following manner:

- * Publish the notice in a newspaper circulating in the scheme area;
- * Display a copy of the notice in the offices of the local government for the period for making submissions set out in the notice;
- * Give a copy of the notice to each public authority that the local government considers is likely to be affected by the amendment;
- * Publish a copy of the notice and the amendment on the website of the local government.

Letters will be sent to adjacent and nearby landowners and government agencies seeking comments on the proposal. All submissions received will subsequently be collated and addressed, and presented to Council for consideration.

Risk Management Implications

The officer's recommendation takes in to consideration all the relevant planning factors associated with this proposal. It is considered that the officer recommendation is appropriate in recognition of making the most appropriate planning decision.

If the Amendment is not initiated there is no right of appeal for the applicant.

As stated in the report, it is noted that the applicant is not bound by the previously approved multiple dwelling application, and recoding of the subject site could result in a different proposal being put forward to be considered on its merits in accordance with the R30/40 coding framework.

Attachment(s)

1. Location Plan
2. Current zonings
3. Proponent Request for Amendment Initiation

Advice to Proponent(s)/Submissioners

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 8 June 2017 Ordinary Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

15.6 (OCM 08/06/2017) - PROPOSED AMENDMENT TO GADD STREET STRUCTURE PLAN - LOT 1 GADD STREET, PORTION OF LOT 80 AND LOT 761 BRANCH CIRCUS, SUCCESS; OWNER: PROPERTY NOMINEES PTY LTD, ASH ROW PTY LTD AND WATER CORPORATION; APPLICANT: RPS GROUP (110/162) (T VAN DER LINDE) (ATTACH)

RECOMMENDATION		
That Council		
<p>(1) adopts the Schedule of Submissions prepared in respect to the proposed Structure Plan amendment.</p> <p>(2) pursuant to Deemed Provision 20 of City of Cockburn Town Planning Scheme No. 3, recommend to the Western Australian Planning Commission that the proposed Structure Plan amendment be approved, subject to the following modifications:</p> <p>1. Amend “<i>Attachment 3 – Amendment Summary</i>” to show the correct proposed residential density coding over the south-west portion of the site.</p> <p>2. Modify the table titled “<i>Specific Amendments to Barfield Road Structure Plan</i>” to include an additional item as follows:</p>		
<i>Part One</i>	<i>Section 4.1</i>	<p><i>Include additional matters to be addressed as a condition of subdivision as follows:</i></p> <p><i>v. Appropriate acid sulphate soil and contaminated site investigations.</i></p> <p><i>vi. Preparation of a landscaping plan which includes the requirement to embellish and maintain POS as low bushfire threat vegetation, incorporate a 3m wide dual use path adjacent to dwellings fronting the POS to allow for emergency vehicle access, provide access gates to subdivisional roads from this dual use path, and any other</i></p>

			<p><i>requirements to ensure the bushfire risk to future dwellings is acceptable.</i></p> <p><i>vii. Upgrade and modification of the intersection of Darlot Avenue and Hammond Road to the satisfaction of the City of Cockburn.</i></p>
<p>(3) advise the landowners within the Structure Plan amendment area and those who made a submission of Council's recommendation accordingly.</p>			

COUNCIL DECISION

Background

The proposed Structure Plan amendment applies to Lot 1 Gadd Street, portion of Lot 80 and Lot 761 Branch Circus, Success ("subject land") (see Attachment 1 – Location Plan).

A Structure Plan prepared over these lots was adopted by Council on 10 September 2015 and approved by the Western Australian Planning Commission ("Commission") on 13 March 2017 (see Attachment 2 – Existing Structure Plan).

The Structure Plan Amendment proposes to increase the density code over a number of portions of the subject land from R25 to R30 and R30 to R40, remove a portion of road reserve adjacent to the proposed Public Open Space ("POS") and include additional laneways servicing the rear of proposed R40 codes lots. The proposed Structure Plan amendment is depicted at Attachment 3.

A subdivision application was approved by the Commission over the subject land on 23 December 2015 in accordance with the Council adopted Structure Plan (later to be endorsed by the Commission in March 2017). On 15 June 2016, the applicant lodged an amended subdivision application with the Commission which was then referred to the City for comment. Due to the nature of the proposed amendments, the City advised that a Structure Plan amendment would need to be lodged and approved prior to approval of the subdivision application. Thus, the applicant has now lodged the proposed Structure Plan amendment which reflects the changes proposed by the amended subdivision application.

This report now seeks to provide a recommendation on the proposal in light of the advertising process and assessment by the City's officers.

Submission

The amended Structure Plan was lodged by RPS Group on behalf of Ash Row Pty Ltd and Property Nominees Pty Ltd (the landowners).

Report

The subject land incorporates Lot 1 Gadd Street, portion of Lot 80 and Lot 761 Branch Circus, Success. The Armadale to Thomson Lake Water Pipeline, located within Lot 80 owned by the Water Corporation, traverses the subject land. The subject land is 8.65 hectares in size and is bound by Parks and Recreation Reserve to the north, west and south west, existing residential development to the east and undeveloped, 'Urban Deferred' zoned land to the south.

The subject land is zoned 'Urban' under the Metropolitan Region Scheme ("MRS") and 'Development' under City of Cockburn Town Planning Scheme No. 3 ("TPS 3"). The existing Structure Plan approved over the site provides for Residential development at R25, R30 and R40 densities, as well as POS and a network of road reserves to service the development. The subject land is located within Development Area 13 ("DA13") as well as Development Contribution Area 1 (Success North) ("DCA 1") and Development Contribution Area 13 (Community Infrastructure) ("DCA 13").

The subject land is currently vacant of built development; however, a Development Application for earthworks was approved in July 2015 and has been substantially carried out, resulting in much of the site being cleared in preparation for future development.

A Conservation Category Wetland (CCW) exists over the north-western portion of the site and the proposed POS (approved under the original Structure Plan) incorporating the CCW and 50m wetland buffer, providing separation to future residential development. Thomsons Lake Nature Reserve exists to the west and south-west of the subject land on the opposite side of Branch Circus.

Hammond Road is located approximately 200m east of the subject land providing access to the regional road network and the Kwinana Freeway via Beeliar Drive to the north and Russel Road to the south. Jandakot Primary School is located approximately 230m east of the subject land. Cockburn Central and Gateway Shopping City are located approximately 1.5km north-east of the subject land and offer a high level of services and community facilities. Cockburn Central Train

Station and Aubin Grove Train Station are located 2.2km north-east and 2.3km south-east of the subject land respectively. Thus, the subject land is well connected and in close proximity to a high level of services and facilities.

Proposed Structure Plan Amendment

The Structure Plan Amendment (Attachment 3) proposes to increase the residential density of the Structure Plan, incorporate a number of laneways and remove a portion of road reserve to replace with POS.

Branch Circus District Structure Plan

The Branch Circus District Structure Plan was adopted by Council at the Ordinary Meeting of 11 August 2011. The District Structure Plan was prepared to facilitate proper and orderly planning across the undeveloped portion of DA13. The District Structure Plan provides guidance for the future preparation and implementation of structure plans, prescribing land uses, the local street network and local parks.

The Branch Circus Draft District Structure Plan provides a greater level of detail to guide structure plans and subdivision compared to other District Structure Plans. This level of detail was considered appropriate due to the highly fragmented nature of the subject area, and the important environmental values associated with extensive wetlands in the area.

The existing approved Structure Plan was prepared generally in accordance with the Branch Circus District Structure Plan in terms of POS location, densities and general road layout. The proposed Structure Plan Amendment only presents a minor variation to the existing approved Structure Plan. Whilst it does not propose R25 coded land at the subject land, as anticipated under the Branch Circus District Structure Plan, this increase in density is considered appropriate as discussed below. The proposed Structure Plan amendment is consistent with the objectives of the Branch Circus District Structure Plan in that it proposes medium density development at the subject land and rear laneways to service R40 coded lots.

Residential Density

Under the existing approved Structure Plan, the majority of residential development was proposed at an R40 density, with a small portion of R30 in the centre of the site and R25 to the south-west, adjoining Branch Circus and Gadd Street. Generally speaking, lots fronting or overlooking POS have been coded R40. The Structure Plan amendment proposes to increase the majority of the R25 coded land to R30 with the small remaining portion of R25 proposed as R40.

The approved Structure Plan prepared over the subject land estimated a total yield of 142 lots based on the area of residential zoned land and the minimum average lot size under the R-Codes. The Structure Plan amendment estimates a total yield of 124 lots over the same area despite the proposed increase in residential densities. This is as a result of further detailed planning of the subject land and the preparation of a Plan of Subdivision depicting a more accurate potential lot yield. Thus, the Structure Plan amendment proposes an overall lot yield of less than originally anticipated by the approved Structure Plan.

Notwithstanding, given the strategic location of the subject land, being in close proximity to Cockburn Central Activity Centre, several areas of POS and a high level of community services and facilities, the proposed increase in density is logical and consistent with State and Local government policies and strategies which promote higher density in close proximity to centres. A key purpose of Liveable Neighbourhoods is *“increased emphasis on achieving density targets and lot diversity, particularly around activity centres and public transport nodes.”* While high density development is generally encouraged within walking distance of activity centres and public transport nodes, medium density development is appropriate at the subject land, as proposed by the Structure Plan amendment, due to it still being relatively close to Cockburn Central Activity Centre and Cockburn Central Train Station.

The subject land and several other lots within the Branch Circus District Structure Plan area are some of the few remaining landholdings capable of being structure planned and developed for residential development in the Success locality. Given much of the land to the east and north-east of the subject land has been developed at a low density (R20) despite being in close proximity to Cockburn Central Activity Centre, the subject land presents an opportunity to provide an increased density and provide more housing diversity within this locality.

Traffic

The expected dwelling yield and the road layout are consistent with Branch Circus District Structure Plan and therefore traffic volumes and flows have already been assessed by the City as acceptable.

However, one submitter raised concerns regarding traffic at the Darlot Avenue/Hammond Road intersection. This is further discussed in the Community Consultation section below.

Public Open Space and Local Road Network

The majority of the POS shown over the Structure Plan consists of CCW and CCW buffer. Further POS has been provided between the CCW buffer and residential zoned land in order to satisfy the 10% requirement set by *Liveable Neighbourhoods* and to provide amenity to residents and facilitate drainage function.

The Structure Plan amendment proposes to increase this portion of POS from 1.8286 hectares to 1.874 hectares by removing a portion of road reserve adjacent to the POS and thus expanding the POS into this area (see Attachment 3). The reason for this amendment is to better accommodate drainage swales within the POS. The increased area of POS provides for an increased separation between the proposed drainage swales and the CCW buffer, thus reducing the risk of disturbance of vegetation and soils within the CCW buffer. It also negates the need for a long lineal area of drainage swale, that would run the length of the POS on the west side of the road, with this now proposed to be constructed as two separate drainage basins, with a path network that can meander between these.

The removal of this section of road will not have a significant impact on the movement network within the Structure Plan area. Lots previously fronting this road reserve are proposed to directly front the POS and be provided with rear access via a laneway. The road network is still sufficiently permeable without this road linkage.

The bushfire risk can be managed through emergency vehicle access being provided through this area of POS. In accordance with *State Planning Policy 3.7 Planning for Bushfire Prone Areas* and the *Guidelines for Planning in Bushfire Prone Areas*, the width of a defensible space for emergency vehicles is required to be a minimum of 3m. Thus, a landscaping plan prepared for the site is required to incorporate a 3m dual use path. Gates into this POS area allowing access for emergency vehicles will be required to be installed to the north and south of the POS and should also be indicated on the landscaping plan. Furthermore, the POS will need to be maintained as low threat vegetation and this should also be indicated on the landscaping plan. These amendments have been required as per recommendation (2)2 above. This adequately addresses the issue of bushfire risk.

In conclusion the proposed Structure Plan amendment is considered to represent an effective planning response to the opportunities present with the land, and adequately addresses issues associated with traffic, bushfire risk and POS interface. It is recommended for approval on this basis.

Strategic Plan/Policy Implications

City Growth

- Ensure planning facilitates a desirable living environment and meets growth targets.
- Ensure a variation in housing density and housing type is available to residents.

Budget/Financial Implications

The Structure Plan amendment fees for this proposal have been calculated in accordance with the *Planning and Development Regulations 2009*, including the cost of advertising and this has been paid by the applicant.

Subdivision and development of the subject land is also subject to the requirements of the City's Development Contribution Plan 13 (Community Infrastructure) and Development Contribution Plan 1 (Success North).

Legal Implications

Planning and Development Act 2005

City of Cockburn Town Planning Scheme No. 3

Planning and Development (Local Planning Schemes) Regulations 2015

Community Consultation

Community consultation was carried out for a period of 28 days from 4 April 2017 until 2 May 2017. The proposal was advertised in the newspaper, on the City's website and letters were sent to affected landowners and relevant government agencies in accordance with the Scheme requirements.

Eight submissions were received during the advertising period, seven from government agencies and one from a nearby landowner. The majority of government agencies provided no objection to the proposal.

However, the Department of Parks and Wildlife expressed concern over the proposal to remove a portion of road adjacent to POS, as discussed in the *Public Open Space and Local Road Network* section above, due to concerns over public safety, protection of bushland and fire safety. The City has undertaken extensive investigations on these issues and the impact of removing this portion of road and has also had a number of discussions with the applicant. As a result of these investigations, it was concluded that the removal of the road would not

have a significant negative impact on public safety, nearby bushland or fire safety as also discussed above.

One objection was received from a landowner on the basis that the proposal will result in unacceptable levels of traffic at the Darlot Avenue/Hammond Road intersection. This submission is directed primarily at the original Structure Plan prepared for the subject land which has now been approved by the Commission. The proposed amendment will not generate significant additional traffic to what was expected under the approved Structure Plan and thus concerns regarding increased traffic as a result of the amendment are not accepted.

However, the City acknowledges that there will be significant traffic impacts on the Darlot Avenue/Hammond Road intersection as part of the existing Structure Plan approval and imminent development of the subject land. The City, in its recommendation to the Commission on the original Structure Plan, requested a condition be placed on any subdivision approval over the subject land that the applicant be required to upgrade the intersection. Despite the City recommending this condition and the applicant being in agreement, the Commission approved subdivision of the subject land on 23 December 2015 without this condition. An amended subdivision application was lodged with the Commission on 15 June 2016 and is yet to be determined pending approval of the proposed Structure Plan Amendment. The City will recommend the same condition be placed on the amended subdivision approval as per recommendation (2)2vii above.

The submissions have been listed in detail within the Schedule of Submissions at Attachment 4.

Risk Management Implications

The proposed Structure Plan amendment presents a good opportunity to increase density in locations close to activity centres in accordance with State Government strategies and policies. The subject land is also one of the few remaining sites within Success to be developed for residential development and the proposed amendment presents an opportunity to better meet density targets prescribed under Directions 2031 and Perth and Peel@3.5million. If the Structure Plan amendment is not approved, this opportunity will be lost.

Attachment(s)

1. Location Plan
2. Existing Structure Plan
3. Proposed Structure Plan
4. Schedule of Submissions

Advice to Proponent(s)/Submissioners

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 8 June 2017 Ordinary Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

15.7 (OCM 08/06/2017) - CITY OF COCKBURN RESPONSE TO ARMADALE ROAD DEVIATION AND NEW NORTH LAKE ROAD BRIDGE/FREEWAY INTERCHANGE ROUTE DEFINITION REPORT (163/011) (A TROSIC)

RECOMMENDATION

That Council support the Route Definition Report for the Armadale Road deviation and new North Lake Road Bridge/Freeway Interchange project, subject to the key comments made in the officer report.

COUNCIL DECISION

Background

The Armadale Road deviation and new North Lake Road Bridge / Freeway Interchange project has recently been committed to funding between the Federal and State Governments. This is a significant piece of infrastructure for the Southern Metropolitan Region, recognising the significance of the Cockburn Activity Centre as a major centre in its own right, and also as a key point along the southern enterprise arc. This ARC links between the major enterprise areas of the Western Trade Coast, Australian Marine Complex, Bibra Lake Industrial Area, Jandakot City, Forestdale Business Park and the Armadale Strategic Centre.

In partnership with Main Roads WA ("MRWA"), Department of Planning, Department of Transport and the Public Transport Authority ("PTA"), a strategic road infrastructure design has been established that will create an appropriately dimensioned centre that supports the needs of regional accessibility balanced with regional mobility. This infrastructure will unlock the intended future land use outcomes not

only with Cockburn Activity Centre, but the broader enterprise precincts that exist along the southern enterprise arc.

As part of progressing the design to its final stage, MRWA have prepared a route definition report for the Armadale Road deviation and new North Lake Road Bridge / Freeway Interchange project. The purpose of this report is for Council to consider its response to this document.

Submission

N/A

Report

The route definition report details the planning study undertaken for the Armadale Road deviation and new North Lake Road Bridge / Freeway Interchange project. Council are very familiar with this project, so rather than re-state a detailed history, it is appropriate that the analysis focus upon the specifics of design, in order for Council to establish its comments on the route definition report. Before this takes place, it is also worth touching on the design concept which features the so called 'duck and dive' intersection treatments.

These are a first for WA, and comprise through traffic movement happening in a trench like expressway, with turning traffic happening via an elevated roundabout type structure. The below image gives an impression of what this generally looks like, being an examples within the US:



This report now proceeds to the analysis proper of the route definition study.

1. *Scope of the Project*

The City understands that the recent funding announcement has been to facilitate the entire Armadale Road deviation and new North Lake Road Bridge / Freeway Interchange project. This had been a point of some confusion in lead up to the funding announcements, as MRWA held a position that in order to deliver the necessary freeway interchange component of the project, the freeway itself needed to be upgraded northbound from Russel Road to the current Roe Highway intersection. The City believes there is now complete clarity on the delivery of the entire project, including freeway connectivity, given that the recent State and Federal Government infrastructure announcements are for:

- Armadale Road/North Lake Road (Kwinana Freeway) – Constructing bridge and collector roads (Project Costs: \$237 million);
- Kwinana Freeway (Russell Road to Roe Highway) – Widening of Northbound Lanes (Project Costs: \$49 million).

Key comment – The City seeks clarity that in light of the recent State and Federal Government announcements, the entire scope of the project is now committed for delivery.

2. *Need for consistent terminology*

The route definition report uses some inconsistent terminology throughout to describe the project. Primarily they use a statement of 'North Lake Road Re-alignment from Kwinana Freeway to east of the intersection of Armadale Road with Verde Drive/Tapper Road in Atwell.' This is not considered the best description of the project, as it doesn't mention the notion of either the freeway bridge or its associated freeway interchange. Accordingly, the terminology for the project should be consistently referred to as 'the Armadale Road deviation and new North Lake Road Bridge / Freeway Interchange project.'

Key comment - The City seeks to have the route definition report amended to provide a consistent name for the project. This should be the Armadale Road deviation and new North Lake Road Bridge / Freeway Interchange project.

3. *Expressed objectives of the City in respect of its design idea*

As the Council are aware, the City provided the idea to MRWA to focus on an Armadale Road deviation for the new North Lake Road bridge and Freeway interchange. The City's objectives for its design, while mentioned in the route definition report, included separating the mix of vehicles with a regional mobility based

objective, with regional vehicles also having an accessibility objective. The way in which the route definition report expressed this objective was that only local vehicles required local access. Part of the justification for the infrastructure delivery is the notion of the enterprise arc for the southern metropolitan region, connecting strategic industrial areas so that supply chains and target markets can more efficiently access and interlink, helping to grow business and employment.

Key comment - The City seeks to have the discussion about mobility and accessibility reflect that regional vehicles are not just seeking mobility to the Kwinana Freeway, but that regional customers also pass through the enterprise arc and seek accessibility in to Cockburn Activity Centre.

4. Reference to the previous 1997 road design option

The route definition report mentions the statement that “it became apparent that long term road planning for the extension of North Lake Road east of the Freeway is being compromised due to the extent of development adjacent to this planned road. It was generally felt that the form and function of the planned North Lake Road as a regional road (Other) as gazetted in the Metropolitan Region Scheme (MRS) was compromised and that a further road network review be required...”

This portrays that development was inappropriately undertaken, and thus inappropriately approved by the City. It should be noted that such development had the involvement of MRWA and the Department of Planning, given it was fronting a ‘Primary Regional Road’ and ‘Other Regional Road’ reservation. It is thus more accurate to represent that it was not development that had compromised the 1997 design, but rather the assumptions that fed the 1997 design had changed significantly.

It is completely reasonable that an infrastructure idea identified in 1997, but not delivered still some 20 years later, would no longer be the most ideal infrastructure design given the context of today. That is, the assumptions made in 1997, would be very different to the assumption made in the present day.

Key comment - The City seeks to have the discussion about the 1997 design be more accurately portrayed as being compromised by the passage of time, rather than development undertaken on the ground.

5. *The objectives for the project*

The objectives for the project according to the route definition report are provided as follows:

- Improve road safety for all road users;
- Additional freeway connectivity;
- Meet future traffic demands and alleviate congestion;
- Improve accessibility by all modes within the Cockburn Central Activity Centre;
- Improve the road network into and out of the Activity Centre for regional traffic;
- Enhance access to public transport and encourage mode share shift toward public transport;
- Provide improved travel time reliability for public transport; and
- Minimise community issues.

It is also worthwhile adding objectives that address the economic return such a project will have in respect of business and jobs growth in the southern metropolitan region. The project has always been about more than congestion, it is about linking strategic industrial areas along the enterprise arc to facilitate business growth, jobs and investment.

Key comment - The City seeks to have the following objectives for the project added under Section 2.4:

- *Deliver a greater ability for businesses to be competitive within the industry sectors of manufacturing, construction and wholesale trade, leading to:*
 - o *a demonstrably positive impact on the Australian economy and;*
 - o *an even higher demonstrably positive impact on Australian jobs;*
- *Improve productivity through more efficient movement of people and goods and provide better access to major enterprise locations in the region;*
- *Provide improved capacity to meet employment self-sufficiency and employment self-containment objectives for the southern metropolitan region.*

6. *Route definition report leading to amendments to the current Planning Control Area*

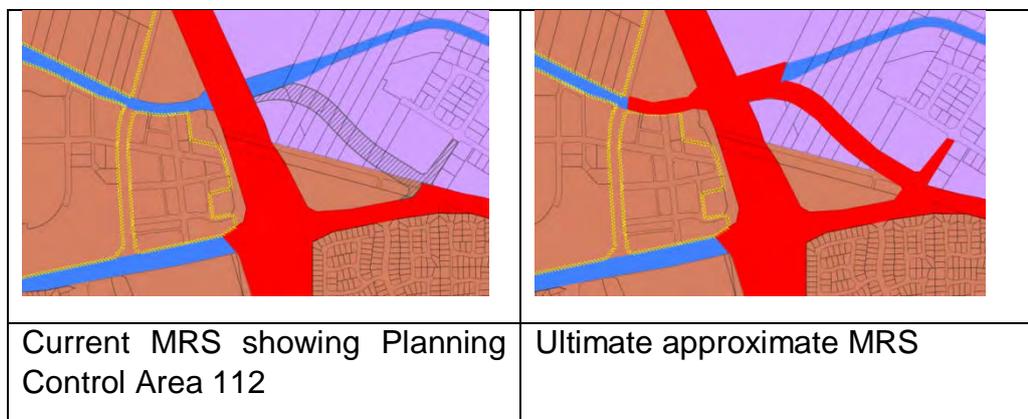
One of the key purposes of the route definition report is to confirm a Planning Control Area Plan and Metropolitan Regional Scheme (“MRS”) modifications that will eventually be gazetted as part of a future MRS amendment. While focussing on the project itself, it is also noted under the previous Kwinana Freeway Route Definition Report – Armadale Road/Beeliar Drive to Berrigan Drive (BG&E

2013) that there was the need to undertake some minor amendments to the Kwinana Freeway ‘Primary Regional Roads’ reservation also. It is noted this yet to be done. Accordingly, it should be reminded to MRWA that this needs to occur.

Key comment - The City seeks to have MRWA undertake the mentioned amendments to the Kwinana Freeway Primary Regional Road reservation, as provided by the BG&E 2013 ‘Kwinana Freeway Route Definition Report – Armadale Road/Beelihar Drive to Berrigan Drive.

7. Existing MRS reserve

Section 3.1 discusses the existing MRS reservations within the project area. It should also be noted under this section of the existence of Planning Control Area 112, given this is a key feature that will ultimately (subject to final design adjustment) lead to a new pattern of road reservations. It should also be noted that the eventual road reservations should be as a ‘Primary Regional Road’ under the MRS, which reverts back to an ‘Other Regional Road’ at the Kentucky Court intersection. This will provide clarity that MRWA are delivering the project, and will maintain it as part of its freeways and highways network. The image below ultimately depicts this reservation outcome:



Key comment - The City seeks to have Section 3.1 of the report reference the existence of Planning Control Area 112. The report should also make it clear that Planning Control Area 112 is intended to revert to a Primary Regional Roads reservation, as per the normal processes for amending the MRS.

8. Data presented in respect of current traffic counts

It is noted that the route definition report presents traffic data that is almost five years old. Its reliability in this regard, especially in potentially under-representing the current traffic volumes, needs to

be addressed. Traffic count data needs to be contemporised, either through replacement with new traffic counts or through addition of an appropriate growth factor. This will be more effective in portraying the need for the project, and also assist in better understanding the management of traffic that will need to occur during the construction phase of the project.

Key comment - The City seeks to have traffic count data brought up to date within the report, either through new counts or through the addition of a growth factor to ensure the data is reliable per the published date of the report.

9. Management of Aboriginal heritage sites

The route definition report mentions the presence of two sites defined in the Department of Aboriginal Affairs Aboriginal Heritage Inquiry System. These need to be appropriately managed as part of the construction and delivery of the project, according to the requirements of the Aboriginal Heritage Act 1972.

Key comment - The City seeks to have the report include reference to the requirement of managing Aboriginal heritage sites in accordance with the requirements of the Aboriginal Heritage Act 1972.

10. Constraints investigation

Under the section of the route definition report titled 'Constraints Investigation', it should be noted that there is a 300m well head protection buffer that extends in to the project areas. This is shown as follows:



Key comment - The City seeks to have the Constraints Investigation section of the report updated to reference the 300m well head protection buffer that extends in to the project area.

11. Noise management

Given the close proximity of current and future sensitive residential development, especially in the section surrounding the Midgegoroo Avenue / North Lake Road and Kentucky Boulevard intersection, it is necessary that noise analysis starts early so that the final design creates the most optimal outcome in respect of noise management, according to the requirements of State Planning Policy 5.4.

Key comment - The City seeks to work with MRWA in respect of the consideration and management of noise issues, with noise to be analysed as early as possible in order to create the opportunity of the most optimal design response to manage noise.

12. Analysis of the recommended design - North Lake Road and Midgegoroo Avenue and Kentucky Court intersection

The proposed design is presented in the following image:

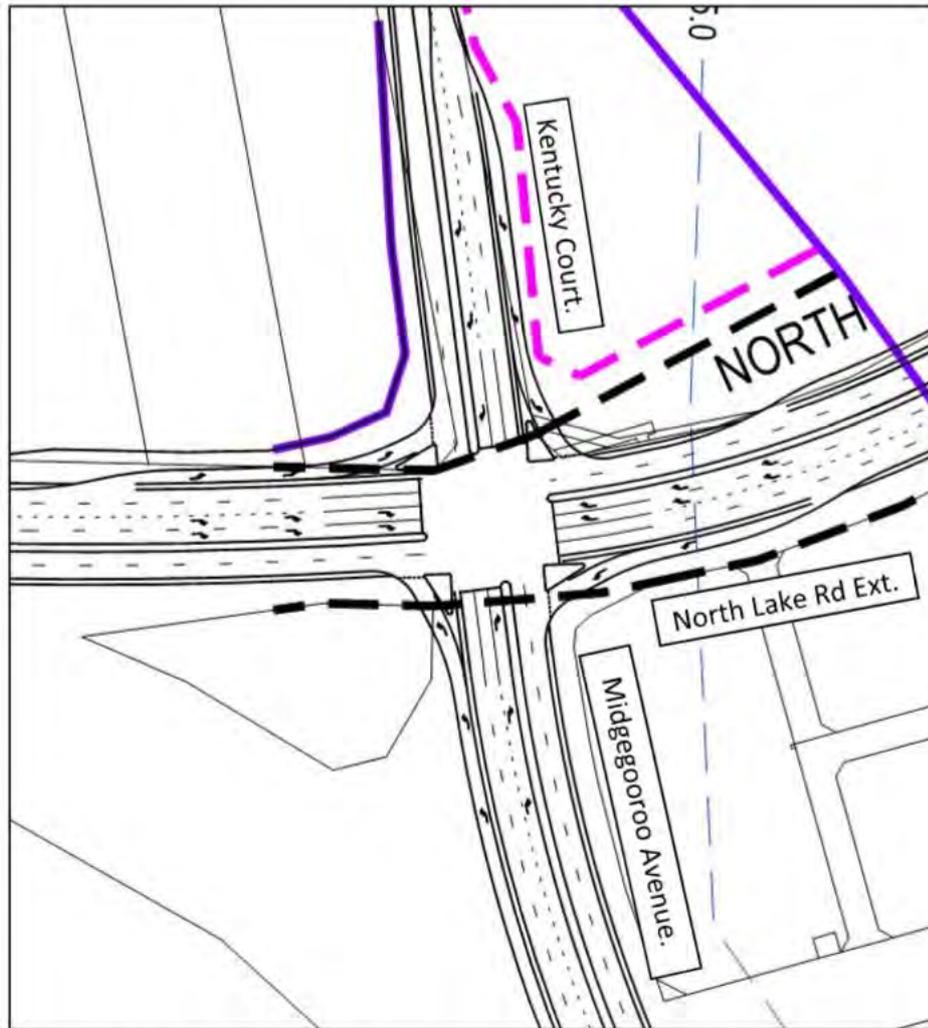


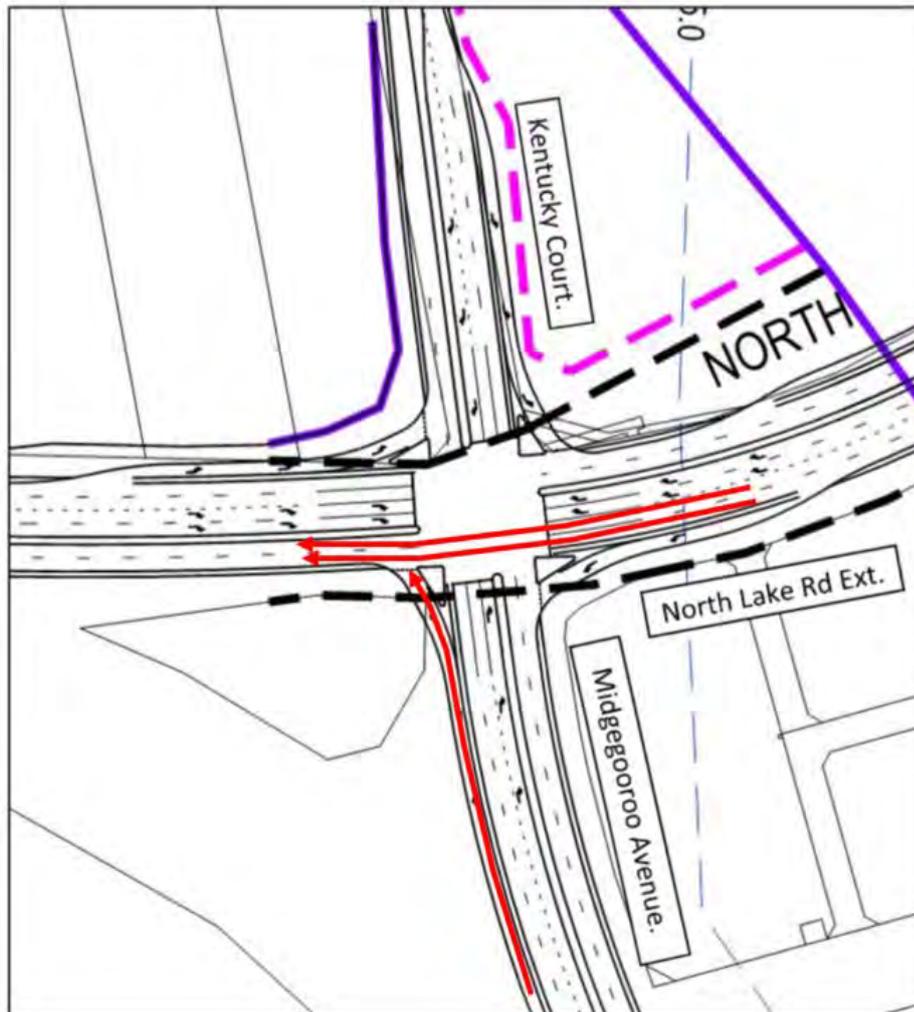
Figure 40 North Lake Road/Midgegooroo Intersection Layout

Key features of this design are identified in the report as follows:

- The northbound approach of Midgegooroo Street is modified by increasing the shared through and right movement into three approach lanes – through, shared through/ right and right only movement, while the existing signalised double left is reduced to a single left movement under give way control;
- The southbound approach of Kentucky Court changes the existing lane (shared left/through/right) movement into three lanes – right and two through movements, while a left only slip under give way control is also provided;
- Westbound lanes on North Lake Road will be modified to increase the current two lanes (left/through /right movement and right only movement) into 4 lanes - two through and two right turn movements, and
- Eastbound lanes on North Lake Road will be modified to mirror that of the westbound lanes including a left slip lane.

Following assessment, there is one small but important concern to raise. This is associated with the lack of priority movement given to the northbound approach for vehicles travelling along Midgegooroo Avenue, intending to turn left in into North Lake Road. This is proposed

as a single left movement under give way control, that is you travel up Midgegooroo Avenue to the intersection, give way to traffic on your right, and then enter North Lake Road to head west. Given the substantial traffic in this area, it will be important to monitor this intersection performance to determine whether sufficient gaps allow a reasonable level of service for that movement left along North Lake Road. While there appears sufficient storage capacity in the left lane, it is unclear to what extent traffic will be backed up along Midgegooroo Avenue due to not having a sufficient gap to enter North Lake Road and head west. This is generally depicted in the following image:



Key comment - The City seeks the report to specifically look at the performance of the northbound left hand turn in to North Lake Road from Midgegooroo Avenue, in order to ascertain whether it will perform adequately.

13. *Analysis of the recommended design - Armadale Road and Solomon Road intersection*

The proposed design is presented in the following image:

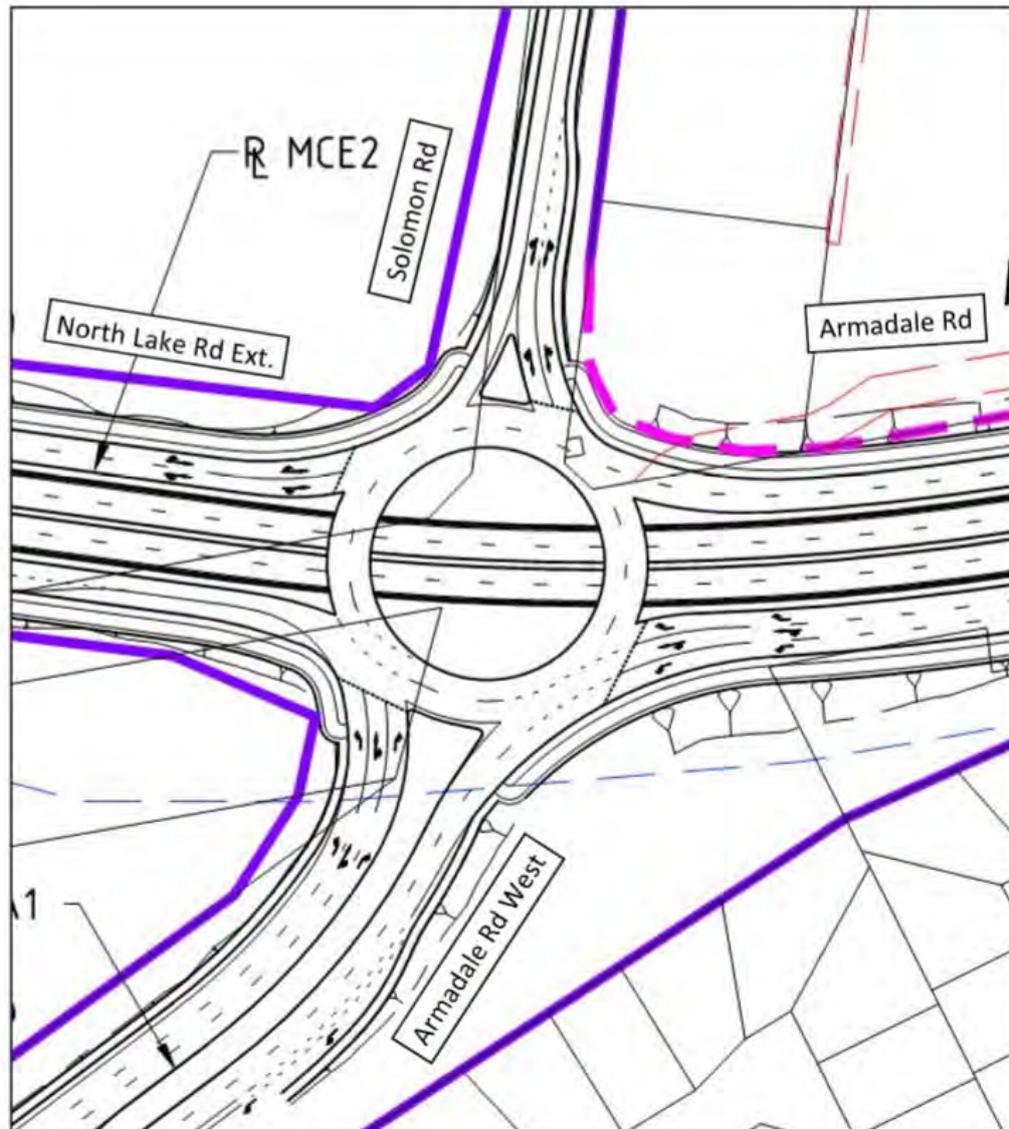


Figure 41 North Lake Road/Armadale Road/Solomon Road Intersection Layout

Key features of this design are identified in the report as follows:

The proposed modifications to the North Lake Road/Armada Road/Solomon Road intersection include:

- The existing T-Intersection of Solomon Road with Armadale Road along with the accesses from Knock Place into Solomon Road (within 40m of the T-intersection) will be reconfigured into a grade separated roundabout. Through traffic from Armadale Road and North Lake Road extension will traverse under the planned roundabout whereas the elevated roundabout will provide vehicle access to existing developments adjacent to the corridor;
- The northbound approach from Armadale Road West to the Roundabout has 3 lanes approaching - left only, shared through/right and right only movements. The double right is required to accommodate the large PM demand;
- The eastbound CD road approach from North Lake Road to the Roundabout has 2 lanes approaching being - left/shared through and shared through/right movements;
- The southbound approach of Solomon Road also has a two lane approach with left/shared through and shared through/right movements;
- The westbound CD road approach of Armadale Road to the Roundabout has 3 lanes approaching - left only, left/shared through and right only movements. The left only movement cannot be a slip lane due to the close proximity of the left turn pocket into Freshwater Drive on Armadale Road West; and
- A spiral will be required within the roundabout for vehicles travelling from the North Lake Road extension or Solomon Road southbound who wish to access North Lake Road Extension westbound.

This is a significant intersection treatment. It appears to primarily affect the existing Puma Service Station on the corner of Solomon Road and Armadale Road, and of course Knock Place and the businesses located along that access. It also appears to limit (but not completely cut off) access to the existing showroom development, which has an access leg coming off Solomon Road via the Puma Service Station site. It will also cause adjustment to the Cockburn Hyundai access arrangements, effectively to push access further north along the Solomon Road frontage. These issues are diagrammatically indicated as follows:



It is recommended that MRWA undertake early and direct engagement with these stakeholders to address concerns they will no doubt raise. The route definition report appears to confirm that the businesses are able to remain, specifically stating:

Puma Service Station - *Two existing accesses to Lot 105 (Puma Service Station) are to be removed as a result of the proposed works. This includes the direct driveway access from Solomon Road and the access from Knock Place, presently east of Solomon Road. An alternative access is suggested from Solomon Road, to the northern extent of Lot 105 (which is the service station site). The secondary access to the service station will remain via an easement shown within Lot 200. The level differential between the proposed roundabout and service station constrain the possible location of alternative access driveways.*

Cockburn Hyundai - *The existing access from Knock Place and southmost access from Solomon Road to Lot 302 (Cockburn Central Hyundai) are required to be removed to accommodate the proposed roundabout. Two alternative driveway accesses are located on Solomon Road which is proposed to remain. Upon liaison with the lot owner, the southern access noted to remain may be deemed too close to the roundabout and will need to be relocated north. It is expected that the northern Solomon Road access can remain, however this appears to service a separate business (Car Giant) within the same lot. It is noted that revised access locations will be the subject of a development application.*

Businesses off Knock Place - *The proposed design will sever access to all lots currently relying on driveways located on Knock Place. These lots have been identified as Lots 14, 64 and 65, based on the proposed design and cadastral / aerial information. The access strategy for these lots comprises utilising existing access via the rear of the lots from Monash Gate for Lots 64 and 65, and access from Verde Drive for Lot 14.*

While this appears positive, it is important to discuss implications with landowners/business owners, as there are likely to be operational issues that cannot be fully understood until discussion occurs. Early discussion will provide the best opportunity for creating an optimal access arrangement.

It will also be important to educate drivers on the use of the roundabout, especially those approaching from Solomon Road southbound intending to access the freeway via the new North Lake Road interchange. Such drivers will need to spiral the roundabout twice in order to move from the inner circle to the outer circle. This is shown following:

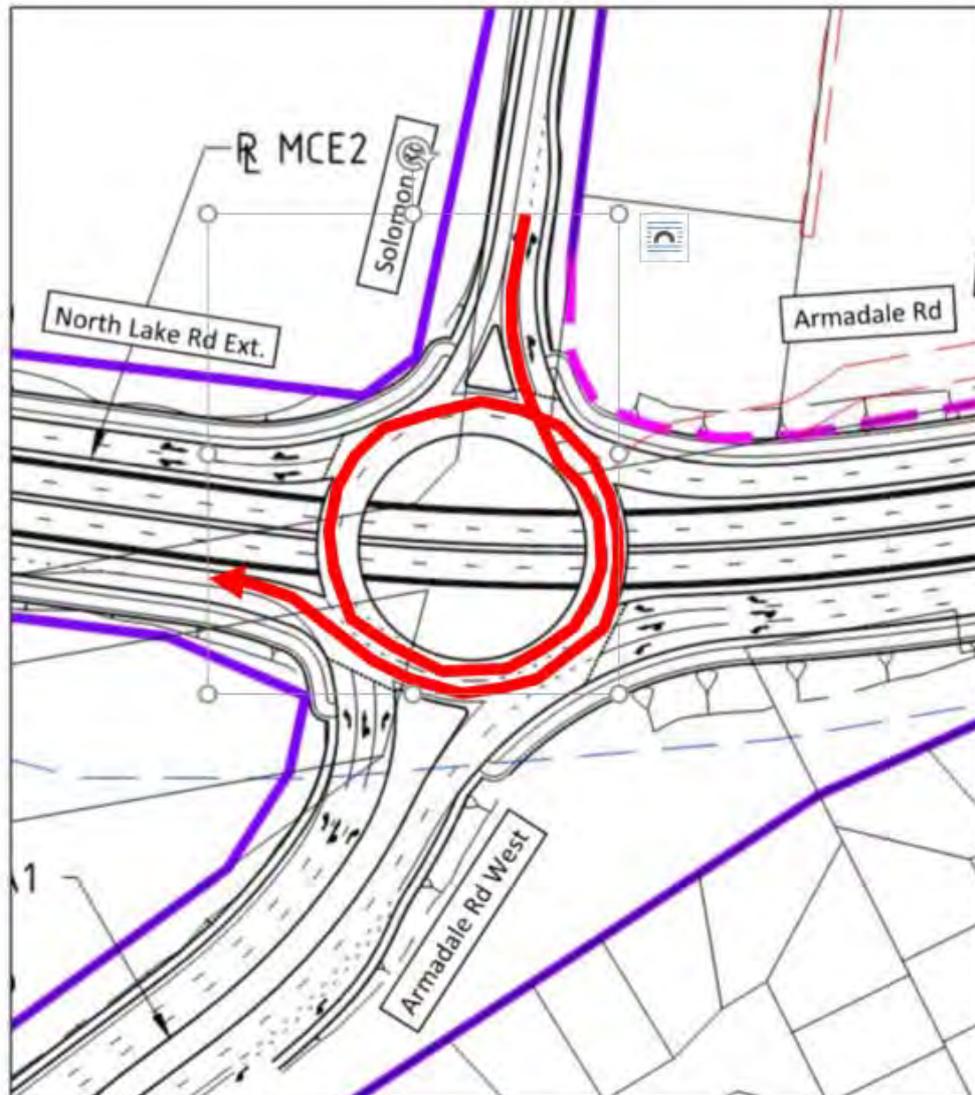


Figure 41 North Lake Road/Armada Road/Solomon Road Intersection Layout

Finally, extensive education will be needed for public transport users to understand how access will be reconfigured for the Cockburn park and ride facility. This will be dramatically improved, with three new access points – from Armadale Road (left in left out), from the new Armadale Road deviation (left in left out), and finally by an underpass to provide northern connection beneath the Armadale Road deviation. It is recommended to also include a clearer diagram depicting this, and how this facilitates a more logical approach to access than what the current Knock Place access provides.

Key comment - The City seeks that in respect of the Solomon Road and Armadale Road intersection treatment, early engagement occur with the affected landowners/business owners, that there be a strategy for driver education (especially due to the use of a roundabout spiral) and finally a strategy for education users of the Cockburn train station park and ride. There should also be an image included within the

document that depicts the new three points of access to the park and ride facility.

14. Analysis of the recommended design - Armadale Road and Tapper Road and Verde Drive intersection

The proposed design is presented in the following image:



Figure 42 Armadale Road/Tapper Road/Verde Drive Intersection Layout

Key features of this design are identified in the report as follows:

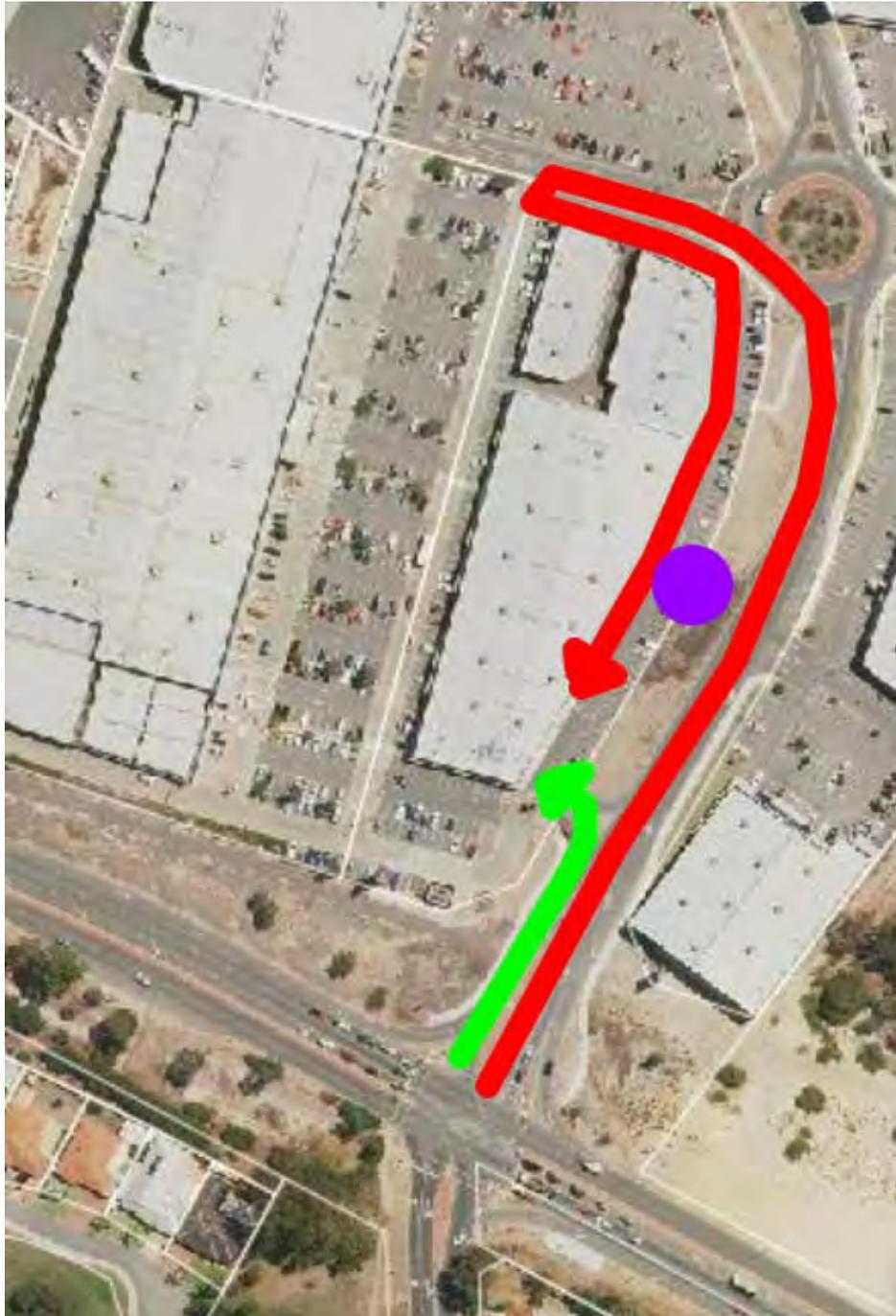
- The existing 4 way intersection of Armadale Road/Tapper Road/Verde Drive will be reconfigured into a grade separated roundabout. Through traffic from Armadale Road will traverse under the planned roundabout whereas the elevated roundabout will provide vehicle access to existing developments adjacent to the corridor;
- The northbound approach of Tapper Road is modified by increasing the two approach lanes of left only and shared through/right movements into three approach lanes – left only, through only and shared through/right movement;
- The eastbound CD road approach of Armadale Road to the Roundabout has 3 lanes approaching – left only, through only and shared through/right movements;

- The southbound approach of Verde Drive is modified from a right only, shared through/right movement and a left only slip lane (under give way control) to only two approach lanes - right only and shared right/through/left lane;
- The existing westbound approach of Armadale Road to the Tapper Road/Verde Drive intersection has 3 lanes approaching (2 through and right only) along with a left only slip under give way control. The revised configuration has 3 lanes approaching – left only, through only and shared through/right movements; and
- A spiral has been provided for vehicles travelling from the Armadale Road eastbound CD road who are turning right to access Tapper Road.

Similar to the Solomon Road and Armadale Road intersection, this is a significant intersection treatment. It appears to make the retention of the existing left in left out treatment as you head north along Verde Drive unachievable, due to proximity of the elevated roundabout and the need for extensive retaining at this point. Likewise, it appears to compromise the current driveway access for the single dwelling located on the south east corner of Tapper Road and Armadale Road. These issues are diagrammatically indicated as follows:



In terms of the single dwelling, there appears the opportunity to take access further south along the Tapper Road frontage. But this requires discussion with that affected landowner. In terms of the impacts on the existing showrooms along Verde Drive, the relocation of the left in left out will have a significant impact on convenient access for them. The image below shows current access in green, and future access in red IF another left in left out access is not provided.



This would appear an unreasonable degree of inconvenience to these businesses. Accordingly, it is recommended that the provision of a relocated (mid-point) left in left out could be a solution (shown in purple). This would again require consideration by MRWA and liaison with the affected businesses.

The design also includes a spiral, as indicated below:



Figure 42 Armadale Road/Tapper Road/Verde Drive Intersection Layout

Key comment - The City seeks that in respect of the Tapper Road / Verde Drive / Armadale Road intersection, consideration be given to relocation of the existing left in left out access as you turn in to Verde Drive from Armadale Road. This left in left out should be located mid-block, in order to maintain a degree of reasonable convenience for these showroom businesses. Direct and early engagement should occur with these businesses, and with the landowner of the single house on the south east corner of the Tapper Road and Armadale Road intersection. There will also need to be a strategy for driver education (especially due to the use of a roundabout spiral).

15. *Analysis of the recommended design - Armadale Road between Solomon Road and Kwinana Freeway*

In looking at the design for the section of (existing) Armadale Road between Verde Drive/Tapper Road and the Kwinana Freeway, it is noted that:

- The existing mid-block left in left out access to businesses between Solomon Road and Verde Drive will remain;
- The existing left in left out access to Lot 500, which is directly on the corner of Kwinana Freeway and Armadale Road, will remain;
- Freshwater Drive access for the suburb of Atwell will be modified such that it becomes a left in/left out and right in only. The right turn movement from Freshwater Drive out has been removed so local residents who need to head east or access the South Central Showroom area and broader industrial precinct will need to do so via Lydon Boulevard and Tapper Road. This is considered a far safer alternative for residents.

Key comment - The City agrees to the modified design for Freshwater Drive, with this considered a greatly improved and safer treatment.

16. *Pedestrian and bike planning*

The proposed adjustments and improvements for pedestrian and bike paths are proposed in the following table:

Road	Location	Type of Path	Average Width (m)	Comments
North Lake Rd, Armadale Rd, CD Roads	North & South verges Between Kwinana Fwy & Fraser Dr	Asphalt Shared Path	3.0	Ties into shared paths on proposed Kwinana Fwy bridge and proposed widening east of Tapper Road
Armadale Rd (southeast)	North & South verges Between Freshwater Dr & Solomon Rd	Asphalt Shared Path	3.0	Ties into existing shared path east of Freshwater Drive. Armadale Road crossing location maintained for connectivity from Atwell suburb.
Solomon Rd Roundabout	Western verge North-South	Asphalt Shared Path	3.0	Shared path constructed as part of the western bridge structure providing connectivity across the dive
Verde Dr Roundabout	Western verge North-South	Asphalt Shared Path	3.0	Shared path constructed as part of the western bridge structure providing connectivity across the dive
Knock Pl	Southern verge	-	-	Existing path removed where clashes with Solomon Rd roundabout. Connectivity provided to the proposed shared path within the southern verge of the westbound CD road.
Solomon Rd	Western verge Between North Lake Rd & Monash Gate	Concrete Footpath	2.0	Reinstatement of existing concrete footpath to suit widening of western kerb alignment and verge
Verde Dr	Western verge Between North Lake Rd & retail access	Concrete Footpath	2.0	Reinstatement of existing concrete footpath to suit widening of western kerb alignment and verge
Tapper Rd	Western verge	-	-	No changes to existing path

Table 6 Summary of Proposed Paths

While noting these, the City requests these to be considered more closely, and it is recommended that the City's TravelSmart section engage directly with MRWA on this. The route definition report has drawn on the City's 1999 Bike Plan, however there have been two further versions since that time. This appears to be a small oversight. While it appears that pedestrian and bike connectivity is being considered, and appears to be improved, it will be important to address the current known issues that the City's residential communities like Atwell and Calleya Estate face in respect of the barrier that the current road environment provides them from safely walking to the train station and other points of interest.

Key comment - The City seeks specific engagement with MRWA to consider the proposed pedestrian and bike network, noting that the latest version of the City's Bike Plan will assist in further considering key connections associated with this project.

17. Public transport impacts

The route definition report notes the following impacts in respect of existing public transport:

There are two existing Transperth bus routes that will be impacted by the proposed North Lake Road alignment works. Both bus routes terminate at Cockburn Central Station and are shown in Appendix H of this report.

The bus routes impacted are as follows:

- Route 527 Cockburn Central Station to Honeywood Estate; and
- Route 518 Cockburn Central Station to Challenger Institute of Technology (via Armadale Road).

Six existing bus stop locations will be impacted during construction, some of which will require relocation as part of the proposed works. These are shown in Appendix G and summarised in Table 7 below:

Bus Stop No.	Location	Existing Layout	Relocation Required	Comments
13281	Armadale Rd southern verge - between Freshwater Dr & Solomon Rd	Bus stop post	Yes	Relocate to proposed northern verge
13282	Armadale Rd northern verge - between Freshwater Dr & Solomon Rd	Bus stop post	Yes	Relocate to proposed southern verge
13280	Armadale Rd southern verge – between Solomon Rd & Tapper Rd	Bus shelter and post within Bus Embayment	Yes	Construct new bus embayment and relocate bus shelter to proposed southern verge, directly west of Tapper Road
26644	Armadale Rd northern verge – between Solomon Rd & Verde Dr	Bus stop post at start of left-slip lane	Yes	Relocate to proposed northern verge at the start of the proposed left-slip lane to Verde Drive (ie. directly east of the left in/left out to Lot 200/400)
21108	Tapper Road western verge	Bus shelter and post	Yes	Relocate to proposed western verge to allow for widening
21104	Tapper Road eastern verge	Bus stop post	No	Bus stop may be affected during construction but does not require relocation as the eastern kerb alignment is not being modified

The route definition report defers consideration of how impacted routes (527 and 518) will be replanned. While it is noted that this is the responsibility of the PTA, the City should seek to have an active role in this also in order to shape the optimal outcome that suits residents and businesses alike.

Key comment - The City seeks to work with PTA in respect of replanning of the two bus routes that appear impacted by the project.

18. Concluding points

This is an infrastructure project that will truly transform Cockburn Activity Centre, the Southern Enterprise Arc and the entire Perth Region. The infrastructure is befitting of the solution needed to see Cockburn Activity Centre become one of the most important strategic centres in the whole of the Perth Region. The City looks

forward to assisting MRWA in any aspect to ensure the successful delivery of this project.

Final key comment - The City seeks to work with MRWA in respect of helping to provide a coordinated education strategy, both in respect of the construct of the project and in the new and much improved regime that vehicles, pedestrians, public transport users, customers, commercial operators and businesses alike will enjoy after delivery of the project.

Strategic Plan/Policy Implications

City Growth

- Ensure planning facilitates a desirable living environment and meets growth targets.

Moving Around

- Reduce traffic congestion, particularly around Cockburn Central and other activity centres.
- Continue advocacy for a better solution to regional freight movement.
- Improve parking facilities, especially close to public transport links and the Cockburn town centre.

Economic, Social & Environmental Responsibility

- Create opportunities for community, business and industry to establish and thrive through planning, policy and community development.

Budget/Financial Implications

This project has been committed for funding by the State and Federal Governments. The City will plan to also address any adjustments needed to the local road network, such that the necessary infrastructure delivery for Cockburn Activity Centre will be done in a coordinated and seamless manner.

Legal Implications

N/A

Community Consultation

The officer recommendation notes the need for detailed engagement and consultation with the community, as the design now progresses past concept.

Risk Management Implications

The risk to Council in not supporting the route definition report subject to the officer comments is that elements that require some further analysis may be missed. This could result in a delay to the delivery of the project.

Attachment(s)

N/A

Advice to Proponent(s)/Submissioners

MRWA will be made aware that the matter is to be considered at the 8 June 2017 Ordinary Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

15.8 (OCM 08/06/2017) - COCKBURN CENTRAL EAST STRUCTURE PLAN (110/155) (R PLEASANT) (ATTACH)

RECOMMENDATION

That Council

- (1) resolves to advertise the proposed Cockburn Central East Structure Plan for the purposes of advertising in accordance with Deemed Provision 18 of City of Cockburn Town Planning Scheme No. 3;
- (2) requests the Western Australian Planning Commission (WAPC) to extend Planning Control Area 122 to include all lots severed to the south by the deviation route including Knock Place, as well as any final adjustments to account for the final alignment of the Armadale Road deviation and new North Lake Bridge / Freeway interchange, including any elements along the Freeway corridor; and
- (3) upon the Planning Control Area amendment declaration by the Western Australian Planning Commission, request that the responsible Minister for Planning approve this declaration to enable it to come in to effect.

COUNCIL DECISION**Background**

The purpose of this report is to seek support for advertising the Cockburn Central East Structure Plan (Structure Plan). The Structure Plan is generally bound by the Kwinana Freeway, Armadale Road, Cutler Road and the western edge of the Banjup Residential Estate. A Local Context Plan is provided at Attachment 1.

The primary objective of the Structure Plan is to ensure the local planning framework is in place to facilitate the delivery of the Armadale Road deviation and new North Lake Bridge / Freeway interchange, of which the alignment cuts through the subject Structure Plan area from east to west.

Main Roads Western Australia (MRWA) has prepared detailed design drawings (85% detail complete) with the design providing for:

- The extension of Armadale Road and the construction of the North Lake Bridge;
- A north bound freeway on ramp and a south bound freeway off ramp;
- The widening of the Armadale Road bridge for vehicles seeking to access Kwinana Freeway north bound;
- Two grade separated roundabouts, and;
- Connections with the existing road network and new access points of which require a local road planning response.

State and Federal Government funding of approximately \$237 million within the recent Federal Government Budget is confirmed for the project and as a result a Structure Plan is required immediately to facilitate land use and road network planning needs in connection with this significant infrastructure upgrade. In addition to this funding is \$49 million for the widening of Kwinana Freeway northbound from Russel Road to Roe Highway, which is also critical to the constructability of the new freeway interchange.

Submission

The proposed Structure Plan has been prepared by the City in consultation with key stakeholders.

Report

Planning framework

Land north of Knock Place is zoned 'Industrial' under the Metropolitan Region Scheme (MRS) with the exception of the Verde Drive alignment of which is reserved 'Other Regional Road'. Land to the south of, and including, Knock Place is zoned 'Urban'. Planning Control Area 122 exists over the Armadale Road alignment, the purpose of the PCA is to ensure land is protected to allow for the investigation and resolution of the Armadale Road deviation and new North Lake Bridge / Freeway interchange design.

The land is zoned 'Development' under City of Cockburn Town Planning Scheme No. 3 (TPS3) and is located within Development Area 20 (DA20) and Development Contribution Areas No. 13 (DCA13). Land west of Solomon Road is also within DCA 8.

The Solomon Road Structure Plan has been in place since 2003 providing a Light and Service Industry Zone over the majority of land alongside a Mixed Business Zone along key road frontages. An indicative Railways reservation exists over Knock Place and the Public Transport Authority (PTA) Commuter Car parks. Three 'Parks and Recreation' zones are located east of Verde Drive, north of Prinsep Road and a site adjacent to the Kwinana Freeway.

For reference the current Solomon Road Structure Plan is provided at Attachment 2.

The Site

Approximately 19 hectares of land within the Structure Plan area is State owned land (Figure 1). Included within this is the 7.5ha site adjacent to the Kwinana Freeway. This land was part of the broader Thomsons Lake Master Plan process, which indicated the land for a potential major sporting activity. Regional sporting needs have since been met by the creation of the new Cockburn ARC and active sporting ovals on the west side of Solomon Road.



Figure 1: Location of - Armadale Road deviation and new North Lake Bridge / Freeway interchange in the context of the Cockburn Central Activity Centre and the Solomon Road Structure Plan area.

A significant amount of land remains underutilised (See Figure 2) with most vacant land being to the west of Solomon Road and within a walkable distance to the train station. The study area supports one geomorphic “Multiple Use” Dampland across approximately one third of the site, in the northern sections.

Many of the lots in this area are long and narrow (some 750m long) with limited access points and as a result hindering the ultimate subdivision and development of these lots. As stated the State Government already have ownership of a large portion of this land, however much is in the hands of a variety of private owners.

3 of 5 Public Transport Authority (PTA) commuter car parks are located within the Structure Plan area with the remaining 2 located within the Town Centre, west of the Train Station (Figure 3). As a result of the Armadale Road realignment, the significant traffic issues currently

experienced along Knock Place in addition to the need to transition the 2 commuter car parks out of the town centre, the PTA car parks require a comprehensive consolidation and redesign.

Lot 500 in the south western corner of the Structure Plan area is bordered by Armadale road, Kwinana Freeway and Knock Place and is currently being developed to include a variety of commercial and office uses. Due to market conditions a planned office component has recently been excluded from the development however noting the ability to transition towards mixed-use developments into the future has been built in to the development approval for this site.



Figure 2: Aerial view of the Structure Plan area containing mostly light industrial, warehouse/large format uses in the eastern portion of the plan area. A considerable amount of vacant and underutilised land remains in the western portion.



Figure 3: Existing location of PTA commuter car parking areas.

Contextual considerations

Gateway to the East

The Structure Plan area is centrally located along Armadale Road at the juncture of the Kwinana Freeway and the Cockburn Central train station. The east-west growth spine of Armadale Road will see up to 20,000 new dwellings delivered along the corridor within the City of Cockburn and the City of Armadale towards 2031. Armadale Road will importantly provide a strong east-west connection for both vehicles with a regional connectivity desire, including access onto the Kwinana Freeway, in addition to those seeking to access Cockburn Central itself.

The eastern precinct will also be an important linking and arrival site for pedestrians and cyclists and therefore footpath, shared paths and public spaces require high levels of amenity to encourage alternative travel options to cars. Major access points for residents entering from Dollier Street and Solomon Road will likely influence both the traffic use patterns and the ultimate land uses in the precinct.

Industrial uses not compatible with residential development

State level strategic planning policy supports and promotes high density mixed-use developments within the walkable 400m-800m catchments of train stations. The expectation is illustrated on the western side of the Kwinana Freeway where the high density developments within the Town Centre and Cockburn Central West illustrate the long term vision of Cockburn Central of being a Transport Oriented Development. However industrial type land uses currently operating within the Structure Plan area and along Cutler Road are

incompatible with residential uses and currently prevent residential development being located within the Structure Plan area.

Residential density targets for Cockburn Central

The Cockburn Central Activity Centre Strategy (2015) identifies that Cockburn Central is currently on track to achieve residential density targets. This recognises the considerable land supply existing west of the Train Station including land remaining to be developed within the Town Centre, and the considerable land supply in the newly created CCW precinct in addition to Muriel Court. As a result, even if residential development could be permitted on planning grounds in Cockburn Central East, it is unlikely residential development will be attracted to the eastern precinct for quite some time.

An important challenge for the City is to promote and facilitate planning decisions that will meet the long term vision for the precinct – that being the remaining half of the Transport Oriented Development for Cockburn Central while also providing opportunities for landowners in the short to medium term. How the City has addressed this within the Structure Plan, and plans to into the future, is further discussed within the body of this report. However to inform these decisions the City sought the advice of Colliers International to undertake a market feasibility assessment to better understand the context of the site and ensure the right land use decisions were importantly supported by economic and market analysis.

The Colliers Report identifies the following preliminary implications for Cockburn Central East –

- *Given the current state of the property markets in Western Australia (WA) and the excess of supply, particularly for residential, industrial and office, it is likely that there will be limited appetite for development within Cockburn Central East in the short term.*
- *Given the office market is likely to take 10 years for vacancy in the Perth CBD to normalise it is unlikely office developments will be attracted to Cockburn Central for some time however noting the most likely way to capture office is to attract and secure a government tenant. Government agencies tend to require larger swathes of space and commit to longer term leases. This aligns with the Government Office Accommodation Master Plan which pushes for the relocation of government agencies from CBD and fringe locations to metropolitan activity centres.*
- *In order for a suburban office building to be considered by the State Government, it must be walking distance from a train station. However, with Cockburn Central East, Murdoch Activity Centre is*

likely to provide significant competition through both the health and education precinct leaving Cockburn Central East as a submarket for local business only. Canning Vale and Jandakot will likely continue to outpace Cockburn Central East and other nearby locations in terms of industrial uses, suggesting that overtime the existing industrial uses may transition more to service commercial uses.

- *In terms of strategic employment regarding health and education, Cockburn Central will be hard pressed to compete against the likes of Murdoch in the short to medium term. Although, as a long term proposition, Cockburn Central is well positioned to capitalise on Murdoch's activities when the MUP is fully developed, although the Murdoch University's ambitions are far greater than the MUP and could reasonably compete well into the 2050's.*

The proposal

As a result of development already delivered in the eastern portion of the Structure Plan area, land use changes for these lots are not proposed. Rather the Structure Plan seeks to propose zone changes to land located between Solomon Road and the Kwinana Freeway where land remains undeveloped and a response is required to address the local road layout, PTA car park requirements and land use planning direction for newly created lots. The exception is for lots located on Verde Drive, between Biscayne Way and Armadale Road where the Mixed-Business Zone boundary is amended to follow recent changes to lot boundaries.

The proposed Cockburn Central East Structure Plan is provided at Attachment 3.

In terms of the road network, provision has been made to connect Verde Drive with the Armadale Road alignment. The Prinsep Road alignment has been modified slightly to connect with Verde Drive. All other more minor roads will require a response by individual landowners at the subdivision stage.

Given the abovementioned reasons supporting the exclusion of residential development and the unlikelihood of attracting office type developments within the short to medium term, despite the overarching vision for Cockburn Central, the findings suggest one of the main objectives of the Structure Plan is to allow for the transitioning of the precinct over time. This requires a response that will protect large government owned landholdings from subdivision and to provide parameters to ensure land uses permitted in the short term do not prevent the ability to transition over time to the ultimate vision. The City therefore proposes the following:

- An extension of the flexible Mixed-Business Zone over underutilised land west of Solomon Road however excluding residential development;
- Permitting lot sizes within the Mixed-Business Zone of between 2,000-4,000sqm. This recognises larger lots provide a greater flexibility for the end user in terms of design, functionality and variety of uses. Furthermore lots within CCW, specifically designed for mixed-use high density residential development, are consistent with this range. Importantly this range is also suitable for lower scale commercial type uses within the short term.
- Promoting opportunities for the retention of the 7.5ha site under WAPC ownership to remain a single lot or a collection of super lots. Including the consideration of ground leases over the shorter term to activate the use of preserved lands including large format warehouses on super lots with 50 year leases, for example a Ikea.
- The newly formed precinct bound by the Armadale Road alignment, the Train station and Lot 500 Armadale Road provides the opportunity to consolidate the 5 PTA commuter car parks including the opportunity to relocate the two PTA commuter car parks out of the Cockburn Town Centre. The relocation of commuter car parking out of the Town Centre will allow for high density residential development of the two lots currently leased to the PTA and under WAPC ownership. Furthermore this will secure the site as a single landholding until such time as the precinct is ready for redevelopment.
- It is noted this response will require the WAPC to acquire the southern portion of lots under private ownership that are severed by the Armadale Road alignment and that this will require an amendment and extension to Planning Control Area 122.
- The newly formed precinct addresses the significant accessibility issues currently experienced by commuters accessing the Cockburn Train Station from the east via Knock Place. The newly formed precinct will provide 3 access points to the commuter car park and the train station.
- Following approval of the Structure Plan and the resolution of land amalgamation and acquisitions, the PTA will be required to submit a development application to the City for the commuter car park. As a result the Structure Plan provides for the following design principles to guide the design for the redevelopment of the proposed precinct of which can easily facilitate up to 2,000 car parking bays:

Requirement	Intent/functional requirements
A concept plan demonstrating staging options for the transition of the precinct over the medium to long term to accommodate mixed-use development.	<p>The subdivision application required to amalgamate lots south of the Armadale Road alignment is to be accompanied by a concept plan that illustrates at least one configuration option for the transition of the precinct to mixed-use. The concept plan should illustrate how development can front the realignment of Knock Place and the new public open space while addressing the primary function related to the accessing for all modes to and from the train station.</p> <p>A key outcome sought is how to deliver development and concurrently consolidate car parking while still addressing access needs.</p>
A public open space area of a similar scale to the Cockburn Town Centre.	<ul style="list-style-type: none"> - Provide a pick up and drop off area for commuters. - Quality design and amenity levels appropriate for a Transport Oriented Development in a town/activity centre environment. - Direct connection with the train station entrance and cycle and pedestrian path network. - Landscaping to: <ul style="list-style-type: none"> o Include significant tree plantings appropriate with the scale of the precinct. o Address the interface between the public space and car parking areas.
Road network connections	<ul style="list-style-type: none"> - Internal movement network to connect with the three MRWA entry points into precinct. - Knock Place to be realigned along Northern boundary of Lot 500 Armadale Road and upgraded to a local road standard and connect to the internal road layout of Lot 500 Armadale Road.
Integrated and connected pedestrian and cycle network	<ul style="list-style-type: none"> - Connects with the wider network via three key connection points. - Street trees along all key cycle and pedestrian footpaths.

The Structure Plan provides for a number of strategic outcomes of which are illustrated in Attachment 4 and further discussed below -

Increased accessibility

A significant outcome is the improved connectivity for the Cockburn Central East precinct, the train station, the wider activity centre core

area and connections with the emerging eastern corridor. The Armadale Road and North Lake Bridge upgrade will alleviate traffic congestion in the core area as a result of the diversion of traffic with a regional trip priority. Proposed changes to the local road network provide for:

- The integration of the MRWA Armadale Road and bridge upgrade project with the existing road network;
- Road typology upgrade requirements including key connections for pedestrians and cyclists and the connection with existing and emerging roads, and;
- Improved access to the PTA car park through the consolidation of PTA car parking on the eastern side of the Cockburn Train Station with three connection points with the surrounding road network. The proposal provides for PTA commuter car parking needs to 2031 and beyond.

The result will contribute to the optimisation of investment in transport and other infrastructure within the activity centre over the long term.

Contributes to the wider Community Connect South initiative

The North Lake Bridge and Armadale Road upgrade is a component of the wider Community Connect South project aiming to deliver economic growth to Perth's South Metropolitan Region through a strategic and regional approach to the provision of transport infrastructure.

The aim is to ease congestion within Cockburn Central and to connect major hubs from Armadale, through Forrestdale Business Parks, future South Forrestdale Industrial Area, Cockburn Central, Jandakot Airport, and the Western Trade Coast to Fremantle, which will enable enterprise, higher productivity and employment growth in the largest subregion of Perth.

Cockburn Central is a regional transport node, linking to key regional enterprise hubs, however the area is one of the worst congestion hot spots in the south metro area with traffic issues experienced beyond peak periods and are significantly impacting on productivity.

Stakeholder consultation

Intergovernmental steering group

An intergovernmental steering group was set up to guide the preparation of the Structure Plan amendment. Representation was provided by the Department of Planning, Public Transport Authority, Main Roads Western Australia and the Department of Transport. The

forming of the group recognises the significant State owned land located within the Structure Plan area, the 5 PTA commuter car park sites requiring consolidation and the integration of access requirements for the Cockburn Train Station. Furthermore the emerging Armadale Road upgrade required direct collaboration with MRWA to ensure integration with the Structure Plan. Following the 3 steering group meetings, in principle support was provided subject to further detailed discussions of which can occur concurrently with the advertising of the Structure Plan amendment.

Approach to land assembly provides significant development options for landholders

The proposed Structure Plan resolves constraints relating to the irregular subdivision layout for lots located between Solomon Road and the Kwinana Freeway. The proposed local road layout divides the long narrow lots and as a result provides new opportunities for developments to front the new road network.

Landowner consultation

The City undertook one-on-one meetings with landowners and business operators located west of Solomon Road and those directly affected by the proposed Structure Plan. The meetings undertaken in late May/Early June informed landowners and business operators of the principles behind the proposed Structure Plan in addition to providing an update on the status of the North Lake Bridge and Armadale Road upgrade project.

All stakeholders will have the opportunity to formally comment on the proposed Structure Plan during formal advertising following support from Council.

Road upgrade considerations

The Traffic Impact Assessment (TIA) prepared in support of the Structure Plan (GTA Consultants. April, 2017) identifies the upgrade requirements to the local road network as a result of a redistribution of regional traffic associated with the Armadale Road realignment, the new PTA commuter car park, in addition to the increase of vehicles forecasted as being generated from the additional Mixed - Business Zoned land. Importantly the TIA recognises the aspiration to transition the precinct over the long term to high density mixed - use development and therefore has built in the relevant forecasting of traffic counts to ensure road upgrades can address future growth requirements over the long term.

This suggests a road upgrade approach that may see certain upgrades occur overtime. Required road upgrades include:

Short term

The extension of Verde Drive between Solomon Road and the realigned Armadale Road

Ultimately Verde Drive is required to be upgraded to a dual carriageway. However noting that as a result of road reserve constraints including the availability of land within the road reserve at the intersection of Verde Drive and Solomon Road it is likely this will not occur in the short to medium term. Rather Verde Drive West of Solomon Road will mirror the single lane typology currently provided east of Solomon along Verde Drive.

The upgrade west of Solomon Road is required to be designed to a standard expected within a town centre, similar to what is delivered within Cockburn Central West, including the integration of significant street trees along the centre line of the road to act as a transitional element, separating the newly formed Mixed - Business precinct from industrial and light and service industry type uses currently operating within and around the Solomon Road and Cutler Road area.

The extension of Prinsep Road down to Verde Drive and upgrade requirements extending to Berrigan Drive

The TIA importantly recognises the necessity to upgrade Prinsep Road so as to reduce the concentration of traffic along Verde Drive and Solomon Road. Without an upgrade to Prinsep Road, daily vehicle trips along Verde Drive and Solomon Road will reach unacceptable levels. As a result the TIA identifies the need to, consistent with the objectives of the existing Solomon Road Structure Plan; connect Prinsep Road with Verde Drive.

Additionally, the constructed component of Prinsep Road also requires upgrading. The road design process to follow the Structure Plan will require the consideration of the increased vehicle trips per day along Prinsep Road recognising the noise complaints currently received from residents fronting Prinsep Road and in proximity to the Glenn Iris Golf course. Complaints relate to noise from trucks accessing the Solomon Road Industrial Area. In response the City recognises that while vehicle trips per day will increase, these numbers relate to an increase in cars associated with the PTA commuter car park. Nonetheless the Prinsep Road upgrade design phase should consider a range of options including the benefits associated with a road realignment, a slip lane and/or appropriate levels of landscaping to act as an edge to the residential area, for example.

Solomon Road south of Cutler Road

Solomon Road requires upgrading consistent with the upgrades currently being delivered to the north of Cutler Road as part of the Calleya Estate.

Medium to long term

Verde Drive

Over time the City will be required to monitor traffic levels along the length of Verde Drive and consider the need to upgrade to a dual carriageway.

Funding considerations for the road network

The abovementioned *short term* road upgrades will be required to be delivered concurrently with the North Lake Bridge and Armadale Road upgrade works of which have an estimated construction commencement date of 2019/2020.

Development Area 20 in the City of Cockburn Town Planning Scheme 3 (TPS3) currently makes provision for landowners whose land is Reserved "Other Regional Road" in the MRS and TPS No. 3 for the purpose of the extension of North Lake Road (Verde Drive) to cede land as a condition of subdivision and to upgrade the land to a two-lane kerbed road. This approach adopted within the current Solomon Road Structure Plan recognises the nexus between development and the need for the road network. Depending on the timing of this work, there may be a requirement for the City to meet some or the entire construction cost element.

The City will need to continue to work with landowners and WAPC to address the required ceding of land in the context of discussions occurring regarding land amalgamations and acquisition options.

Next steps

Following Councils support, the proposed Structure Plan will be advertised for 28 days. The City will consider submissions and report back to Council seeking support to forward to the WAPC for adoption.

Strategic Plan/Policy Implications

City Growth

- Ensure planning facilitates a desirable living environment and meets growth targets.

Moving Around

- Reduce traffic congestion, particularly around Cockburn Central and other activity centres.

Community, Lifestyle & Security

- Provide for community facilities and infrastructure in a planned and sustainable manner.
- Foster a greater sense of community identity by developing Cockburn Central as our regional centre whilst ensuring that there are sufficient local facilities across our community.

Budget/Financial Implications

Infrastructure delivery and upgrades required to meet the land use objectives of the Structure Plan area will be required to be undertaken by individual landowners at the time of subdivision or development, where such relates to local level infrastructure. Currently the Scheme also requires landowners affected by the Other Regional Road reservation to cede the land free of cost, and contribute towards its construction.

However, depending on the timing of this work, there may be a requirement for the City to meet some or the entire construction cost element. This issue will become clearer once advertising of the Structure Plan finishes, and submissions are reviewed. Given the likely short term nature of work beginning on the Armadale Road deviation and new North Lake Bridge / Freeway interchange, it is likely that the City will need to secure the Other Regional Road link in the short term.

Legal Implications

N/A

Community Consultation

The Structure Plan will be formally advertised for 28 days at which time letters will be sent to all affected landowners and residents explaining the structure plan and inviting comment.

Risk Management Implications

If the proposed Structure Plan is not supported, there will be no planning framework in place over the subject land to guide the Armadale Road deviation and new North Lake Bridge / Freeway interchange. This will result in delays in the delivery of the vital piece of infrastructure.

Attachment(s)

1. Local Context Plan
2. Solomon Road Structure Plan
3. Cockburn Central East Structure Plan
4. Strategic outcomes

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

15.9 (OCM 08/06/2017) - PROPOSED AMENDMENT TO THE METROPOLITAN REGION SCHEME FOR LOTS 4, 50, 86, 87 AND 98 (98 & 99 PREVIOUSLY PART OF 333 PRINSEP ROAD) PRINSEP ROAD & LOTS 5, 9, 88, 89 & 99 JANDAKOT ROAD, JANDAKOT (108/001) (C CATHERWOOD)

RECOMMENDATION

That Council write to the Western Australian Planning Commission indicating the following concerns with the draft amendment to the Metropolitan Region Scheme:

- (1) the consideration of this proposal prior to finalisation of the *Perth and Peel @ 3.5 million* suite of documents would be prejudicial to proper and orderly planning for the region;
- (2) should the proposal be progressed irrespective of (1) above, the proposal for 'Urban deferred' is not appropriate given the inability of a wide range of land uses, including sensitive land uses, to be accommodated. The applicant should consider revising their proposal with another MRS zone in mind;
- (3) the submitted documentation is nearly six years old and in that time, the State and local planning policy frameworks have been subject to a number of revisions and additions. Should the proposal be progressed irrespective of (1) above, the documentation should be updated to discuss the current State and local planning frameworks;
- (4) the submitted mapping labelled 'Figure 8 – TPS3 zoning' is incorrect. There is no 'Resource' zone shown in the figure's legend and the colour annotated to the actual 'Resource' zone misrepresents the land as being 'Regional Centre' zone which is

incorrect. The figure also needs updating to reflect the scheme amendments which have occurred in the last six years;

- (5) the submitted documentation is lacking in its discussion of the following specific matters:
1. Traffic analysis with regard to the surrounding network.
 2. Detailed investigations on the water resource given this is a significant constraint.
 3. Road upgrading requirements for Jandakot Road are discussed in relation to future development of the site. This should be elaborated to acknowledge WAPC policy which will impose these requirements at the subdivision stage.
 4. Environmental Report is limited to only a portion of the site and was undertaken in November 2008. It is questionable whether this is sufficient regard to the current environmental policy framework.
 5. Bushfire (all the land is designated as 'Bushfire Prone').
 6. The discussion on the City of Cockburn Town Planning Scheme No. 3 provisions is very limited and does not acknowledge a further local planning scheme amendment would be required to include the land in a 'Development Area' with appropriate provisions to guide the purpose of any structure planning area.
 7. The Jandakot Airport Masterplan has been revised.
 8. Justification against the various planning policies (as opposed to a summary of what they contain).
 9. Acknowledgement of what the various planning strategies contain as they are all contrary to the proposal (as opposed to simply justification).

COUNCIL DECISION

Background

The City of Cockburn has recently received a copy of a request for an amendment to the Metropolitan Region Scheme (“MRS”) dated August 2011. In May 2017, the WA Planning Commission (“WAPC”) advised they would request Council’s preliminary comment. There will still be a further opportunity should the proposal be initiated by the WAPC for a City submission as part of the formal consultation period.

The proposal seeks an ‘Urban Deferred’ zone. It is currently ‘Rural – Water Protection’ zone under the MRS. The current MRS zone reflects the land’s designation under State Planning Policy 2.3 (“SPP2.3”) Jandakot Groundwater Protection as a ‘Priority 2’, which is described as:

“The acceptability of land uses in the Rural-Water Protection zone is based on the objective of risk minimisation. Low risk and intensity of development consistent with the Rural zoning is generally supported, subject to appropriate conditions”.

The amendment deals with a number of lots located on the corner of Prinsep and Jandakot Roads as shown below in the figure extracted from the proposal.



To reflect the MRS, these lots are zoned ‘Resource’ under the City’s Town Planning Scheme No. 3 (“TPS3”).

Submission

The applicant has submitted a planning report to the WAPC requesting the land be rezoned to 'Urban Deferred' under the MRS.

Report

There are a number of concerns with the draft proposal which are worth outlining to the WAPC. This will allow the WAPC to consider whether changes or updates are needed to the document before it is advertised formally. These concerns are set out below and reflected in the officer recommendation.

Consistency with Perth and Peel @ 3.5.million

To realise the vision of Directions 2031 and beyond and the State Planning Strategy 2050, the Western Australian Planning Commission has created a series of detailed draft planning frameworks.

The Perth and Peel@3.5million strategic suite of documents has been developed to engage the community in open discussion on expectations of what our city should look like in the future, on how we can maintain our valued lifestyle and on how we can realistically accommodate a substantially increased population over the next 35 to 40 years.

The South Metropolitan Peel Sub-regional Planning Framework is one of three frameworks prepared for the outer sub-regions of Perth and Peel, which along with the Central Sub-regional Planning Framework establishes a long-term and integrated framework for land use and infrastructure provision.

The framework builds upon the principles of Directions 2031 and will provide guidance for:

- the preparation of amendments to the Perth Metropolitan Region Scheme, local planning schemes, local planning strategies/scheme, and district, local and activity centre structure planning; and
- the staging and sequencing of urban development to inform public investment in regional community, social and service infrastructure.

Importantly the Planning Framework, amongst other things, endeavours to develop a consolidated urban form that limits the identification of new urban areas to where they provide a logical extension to the urban form, and that places a greater emphasis on urban infill and increased residential density.

The following figure is extracted from the Perth and Peel @ 3.5 million spatial plan (the subject land is outlined in red). This indicated the land as primarily 'Rural' with a small area of 'Rural Residential' confined to

what appears to be the corner lots. The land is not shown as 'Urban'. The proposal to rezone to 'Urban Deferred' is not consistent with the draft Perth and Peel @ 3.5 million document.

At this point in time, there is no decision available from the WAPC on Perth and Peel @ 3.5 million.



Suitability of MRS zones

Should the draft proposal be progressed notwithstanding its inconsistency with Perth and Peel @ 3.5 million, consideration should turn to the appropriateness of the MRS zone proposed.

There are a small number of zones and reservations in the MRS, far less than found in local planning schemes, reflective of their broad categories. The MRS text is particularly unhelpful in that it contains no description of the zones or their objectives. The WAPC does provide the following guidance on zones in their MRS proposals:

Urban: areas in which a range of activities are undertaken, including residential, commercial recreational and light industry.

Urban deferred: land identified for future urban uses following the extension of urban services, the progressive development of adjacent urban areas, and resolution of any environmental and planning requirements relating to development. The WAPC must be satisfied that these issues have been addressed before rezoning to urban.

Central city area: strategic regional centres for major retail, commercial and office facilities as well as employment, civic, business and residential uses.

Industrial and special industrial: land on which manufacturing, processing, warehousing and related activities are undertaken.

Rural: land on which a range of agricultural, extractive and conservation uses are undertaken.

Private recreation: areas of significance to the region's recreation resource, which is, or is proposed to be, managed by the private sector.

Rural - water protection: rural land over public groundwater areas, where land use is controlled to avoid contamination”.

As noted in the Background section of this report the current MRS zoning for this land is 'Rural – water protection', reflective of the groundwater constraint.

There are other constraints which must be considered as well, such as aircraft noise. The subject land is affected by aircraft noise and therefore State Planning Policy 5.3 Land use planning in the vicinity of Jandakot Airport, which includes the following objective to:

“protect Jandakot Airport from encroachment by incompatible land use and development, so as to provide for its ongoing, safe, and efficient operation”.

The applicant has acknowledged the aircraft noise as a consideration and suggested uses will be limited to non-sensitive land uses only. This seems to be at cross purposes with the description of the Urban zone which is to facilitate 'a range' of uses including sensitives uses such as residential and recreation.

Submitted documentation – length of time since submission

The submitted documentation is nearly six years old and in that time, the State and local planning policy frameworks have been subject to a number of revisions and additions. Should the proposal be progressed, the documentation should be updated to discuss the current State and local planning frameworks.

Submitted documentation – mapping submitted

The submitted mapping labelled 'Figure 8 – TPS3 zoning' is incorrect. There is no 'Resource' zone shown in the figure's legend and the colour annotated to the actual 'Resource' zone misrepresents the land as being 'Regional Centre' zone which is incorrect. The figure also needs updating to reflect the scheme amendments which have occurred in the last six years.

Submitted documentation – discussions points lacking
Traffic analysis with regard to the surrounding network

There is very little information provided in relation to traffic. Even at the region scheme amendment level, there needs to be proper assessment from a transport planning perspective.

It is important to know what will be required in transport terms, as planning for potential transport impacts at this stage is vital for the more detailed subsequent stages of structure plans, subdivisions and individual developments.

At the very least the proposal should provide sufficient information, as outlined in WAPC's *Traffic Impact Assessment Guidelines – Volume 2 Planning Schemes, Structure Plans and Activity Centre Plans* to determine whether a traffic impact assessment is warranted, or to confirm it is exempted at this stage.

Detailed investigations on the water resource given this is a significant constraint

With the land's designation under State Planning Policy 2.3 ("SPP2.3") Jandakot Groundwater Protection as a 'Priority 2', there needs to be a level of investigation into the land and its relationship to the water resource.

From the draft proposal submitted, no assessment work appears to have been undertaken into this key issue. Instead reliance appears to be made on the (then) upcoming review of SPP2.3 to allow for this area to be urbanised, with little regard placed on the outcome of that SPP review, and rezoning approved given the 'ideal location'.

The review has now occurred and the site remains as a 'Priority 2' water resource. Without the applicant undertaking investigation to prove this environmental concern can be adequately managed, advertising of the proposal would seem futile. It should also be noted that the reviewed version of SPP2.3 sets a series of policy considerations for the consideration to rezone land under the MRS. These are:

h) Discretionary uses

Where an application is made for approval of a land use identified as 'compatible with conditions' in *Water quality protection note 25: Land use compatibility tables for public drinking water source areas*, the relevant decision-maker should refer the application to the Department of Water for advice and recommendation before making a determination.

i) Non-conforming uses

Guidance on non-conforming land uses and injurious affection is provided in the *Planning and Development Act 2005, Planning and Development (Local Planning Schemes) Regulations 2014* and local planning schemes.

Where existing uses are likely to have a detrimental effect on the quality of the groundwater through the discharge of nutrients or other waste matter, the *Environmental Protection Act 1986* may apply.

6.3 Strategic planning and Metropolitan Region Scheme amendments

- a) In order to protect the quality of the public drinking water source, there is a presumption against new urban or industrial land uses in the Water Catchment reservation and the Rural-Water Protection zone of the Metropolitan Region Scheme.
- b) Amendments to the Metropolitan Region Scheme will only be supported where the land has been identified for development in the manner proposed through a strategic planning document approved or prepared by the Western Australian Planning Commission, such as a sub-regional planning framework or sub-regional structure plan.

c) Planning for more intense land uses through strategic planning instruments (such as a sub-regional planning framework or sub-regional structure plan) and subsequent Metropolitan Region Scheme amendments should incorporate the following:

- the subject land is to meet the following criteria:
 - large landholdings that were already substantially cleared at the time this policy was published; and
 - directly adjacent to already developed 'Urban' zoned land;
- the risk to drinking water supplies associated with the proposed development;
- proven efficacy of available risk mitigation measures;
- the net long-term public benefit attributable to the proposed rezoning;
- the need for additional urban land, taking into account the current stock of undeveloped urban zoned land in the sub-region;
- potential alternative locations for proposed land use in the sub-region that would have less or no impact on Public Drinking Water Source Protection Areas;
- any strategic environmental assessment, whether underway or completed; and
- access to infrastructure that is already constructed or scheduled for construction.

When looking at the land in question:

- it has not been identified in a strategic planning document approved or prepared by the WAPC;
- the land is not a large holding which has been substantially cleared but is adjacent to already developed 'Urban' land;
- risk management of the drinking water supply resource has not been researched or proven;
- has not established a net long-term public benefit to support the proposed re-zoning;
- has not analysed the need for additional urban land;
- has not considered potential alternative locations;
- has not been assessed in respect of the Perth and Peel Strategic Environment Assessment process.

It cannot be concluded that the amendment is consistent with SPP2.3

Road upgrading requirements for Jandakot Road

These are discussed in relation to future development of the site. This should be elaborated to acknowledge WAPC policy which may also impose these requirements at the subdivision stage.

Environmental Report

The Environmental Report is limited to only a portion of the site (16ha of the 38.5ha covered by the proposal). The report was undertaken in November 2008. It is questionable whether this is sufficient regard to the current environmental policy framework or if it remains relevant given the passage of time. For example, Banksia Eucalypt Woodland was listed as a threatened ecological community in September 2016.

Bushfire

The subject land is designated as 'Bushfire Prone'. This reflects regulations which have been introduced since the draft proposal was originally prepared.

The draft proposal requires updating to reflect this issue.

City of Cockburn Town Planning Scheme No. 3 provisions

Discussion regarding TPS3 provisions is very limited, indicating an intention for automatic rezoning to the 'Development' zone when urban deferred is lifted. Mention is then made of the need for a structure plan.

This does not acknowledge a further local planning scheme amendment would be required to include the land in a 'Development Area' with appropriate provisions to guide the purpose of any structure planning area. Even if a Council was amendable to allow a zone to be applied under a local planning scheme, there is no ability for a Special Control Area to apply automatically.

'Development Areas' are 'Special Control Areas' and in Cockburn's case, there is associated scheme text to be applied (in Table 9). The draft proposal should be updated to reflect this.

The Jandakot Airport Masterplan has been revised

Since the draft proposal was prepared a review of the Jandakot Airport Masterplan has occurred. The draft proposal should be updated to reflect this.

Justification against the various planning policies (as opposed to a summary of what they contain)

Where policies have been set out in the draft proposal, there is narrative provided about what the policies are for. In most cases, there is very little provided in terms of justification for the proposal and how it will respond to the policy environment.

Acknowledgement of what the various planning strategies contain as they are all contrary to the proposal (as opposed to simply justification).

Discussion on strategies appears to be in reverse to the discussion on policies.

Where strategies have been set out in the draft proposal, there is justification for the proposal. However, there is often no acknowledgement of what the strategy sets out for the subject land. In most cases, the proposal is completely contrary to the strategy.

Strategic Plan/Policy Implications

City Growth

- Ensure planning facilitates a desirable living environment and meets growth targets.

Economic, Social & Environmental Responsibility

- Sustainably manage our environment by protecting, managing and enhancing our unique natural resources and minimising risks to human health.

Budget/Financial Implications

N/A

Legal Implications

If the proposal is ultimately approved by the WAPC, the City will be obliged to update its local planning scheme to reflect the MRS. This is set out in Part 9 of the Planning and Development Act 2005.

There are no legal implications related to the provision of preliminary comments as proposed in the officer recommendation.

Community Consultation

Should the proposal be initiated by the WAPC, there will be a formal opportunity for comment. This will be run by the WAPC.

These preliminary comments are not part of a broader community consultation process.

Risk Management Implications

There is no obligation on the City to provide preliminary comment to the WAPC on a draft MRS proposal. However this is an opportunity to ensure the WAPC have input from the City prior to deciding to formally initiate the proposal.

This opportunity is important considering the legal implications set out above.

Attachment(s)

N/A

Advice to Proponent(s)/Submissioners

The WAPC have been advised that this matter is to be considered at the 8 June 2017 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

15.10 (OCM 08/06/2017) - PROPOSED STRUCTURE PLAN - PART LOT 41 GAEBLER ROAD, HAMMOND PARK - OWNER: BROAD VISION PROJECTS PTY LTD - APPLICANT: RPS GROUP (110/172) (T VAN DER LINDE) (ATTACH)

RECOMMENDATION

That Council

- (1) adopts the Schedule of Submissions prepared in respect to the proposed Structure Plan.
- (2) pursuant to Deemed Provision 20(2)(e) of City of Cockburn Town Planning Scheme No. 3 ("Scheme"), recommends to the Commission the Proposed Structure Plan be refused for the following reasons:
 1. The Proposed Structure Plan is inconsistent with orderly and proper planning in that it does not provide a planning structure over the entirety of Lot 41, by excluding the majority of this lot from the Structure Plan;
 2. The Proposed Structure Plan has not responded to the environmental characteristics of the site. Specifically, the existing Conservation Category Wetland ("CCW") has

been excluded from the Proposed Structure Plan and identified as being “subject to further planning,” rather than being comprehensively addressed as part of this Proposed Structure Plan;

3. The Flora and Vegetation Survey which informs the design of the Proposed Structure Plan is out-dated and therefore unable to be relied upon. This was prepared in 2007, and since that time the environmental characteristics of the land have changed, as well as the regulatory framework dealing with environmental assessment. Environmental considerations for the subject land are fundamental to any assessment of a Proposed Structure Plan;
4. The Bushfire Management Plan does not accurately identify the potential bushfire risk to future dwellings at the subject land due to the land reserved for the future widening of Frankland Avenue being incorrectly excluded from the assessment. This road reservation should be assessed as classified vegetation since the road widening may not occur for a number of years and thus the site may be developed before this bushfire risk is removed;
5. The Proposed Structure Plan prejudices future planning, whether that is associated with needing to adequately respond to the Conservation Category Wetland, or to otherwise respond to a new decision of the Department of Parks and Wildlife in respect of the status of the wetland.

- (2) advise the landowners and those persons who made a submission on the Structure Plan of Council’s recommendation

COUNCIL DECISION

Background

The Proposed Structure Plan applies to a 1.081 hectare portion of Lot 41 Gaebler Road, Hammond Park (“subject land”), with the total lot size being 4.0772 hectares (see Attachment 1 – Structure Plan). In essence, the Structure Plan deals with only 26.5% of the subject land.

This is not a common occurrence or expectation of structure plans, which are to comprehensively deal with the planning of structures for future subdivision and development (i.e. land parts in their general whole).

The subject land is vacant of all development and is bound by Gaebler Road to the north, Frankland Avenue to the west, a vacant lot of a similar size to the south (Lot 9008 Frankland Avenue), and a Conservation Category Wetland ("CCW") to the east, on the balance portion of Lot 41. Attachment 2 – Location Plan shows the location of Lot 41 in the context of the surrounding locality. The CCW exists over the majority of Lot 41 as well as over Lot 9008 Frankland Avenue immediately to the south. The Structure Plan has been prepared over the portion of Lot 41 that does not fall within the CCW or the CCW 50m buffer.

The proponent does not agree with the classification of the CCW and thus has excluded this portion of land. It is unclear what the future planned intent is for this portion of the land, being designated as subject to future planning.

The CCW also extends over a portion of Lot 9008 immediately south of the subject land, and the landowners of Lot 9008 are currently dealing with the Supreme Court disputing the classification of the CCW. The Department of Parks and Wildlife ("DPaW") have been involved in the Supreme Court process. There has been no determination of this matter to date.

In light of the lack of comprehensively planning the whole land, and the risk this poses to prejudicing future planning, it is recommended that Council recommend refusal of the Proposed Structure Plan to the Western Australian Planning Commission.

Submission

NA

Report

Zoning and Context

The majority of the subject land is zoned 'Urban' under the Metropolitan Region Scheme ("MRS") with a 20m wide portion on the western boundary of the lot adjacent to Frankland Avenue being reserved as 'Other Regional Road'. This reservation is to facilitate the widening and upgrade of Frankland Avenue as an extension of Hammond Road, with works estimated to be undertaken during

2019/21. The Structure Plan identifies this portion of the lot as being required to be ceded for the future widening of this road.

The 'Urban' zoned portion of the subject land is zoned 'Development' under the Scheme and is located within Development Area 26 ("DA26"). Thus, a Structure Plan is required to be prepared over the subject land prior to subdivision and development. The subject land falls within Developer Contribution Areas 13 – Community Infrastructure ("DCA 13") and 9 – Hammond Park ("DCA 9") and the developer will be required to satisfy the obligations of both of these DCAs.

Much of the Hammond Park locality has progressively been redeveloped from large rural lots to primarily low to medium density residential development. Land to the north, east and south of the subject land consists of residential development ranging from R20 to R40 densities.

Harry Waring Marsupial Reserve exists further to the west of the subject land, across Frankland Avenue, and consists of approximately 280 hectares of bushland and wetland.

The subject land is in a strategic location, in relatively close proximity to a variety of parks, transport options and community facilities. However, the exclusion of the majority of Lot 41 from the Structure Plan raises a number of broader planning issues, notwithstanding the land's strategic location, that drive a position on it being inconsistent with orderly and proper planning.

Planning Assessment

Under clause 20(2)(d) of the deemed provisions of the *Planning and Development (Local Planning Schemes) Regulations 2015* the City is required to undertake an assessment of the Structure Plan based on appropriate planning principles. This assessment is provided below, the conclusion of which is that the proposal does not comply with the appropriate planning principles and should not be supported.

Southern Suburbs District Structure Plan – Stage 3, Hammond Park/Wattleup

The subject land is located at the most north-western extremity of the Southern Suburbs District Structure Plan ("SSDSP3") and identified as being suitable for medium residential development. The rest of Lot 41 is identified as CCW.

Whilst the proposed medium density coding of R60 is consistent with the SSDSP3, the Structure Plan is not consistent with the intention of

the SSDSP3 to identify the portion of Lot 41 excluded from the Structure Plan area as CCW. In regards to the CCW, section 2.8 of SSDSP3 states:

“Proposed LSP’s will need to ensure these issues are investigated and managed in accordance with relevant government guidance documents, including

- *Position Statement No. 4 – Environmental Protection of Wetlands (EPA 2004).”*

Furthermore, section 3.7 states:

“A dampland lies within the north west portion of the SSDSP3 area, located on Lots 39 and 41 Gaebler Road and Lot 42 Frankland Avenue and is identified as a CCW under the DEC’s Geomorphic Wetlands dataset. The CCW was subject to a wetland classification review in 2010 and the DEC determined that the CCW is a fully functioning wetland and resolved to retain its CCW classification. Careful consideration needs to be given to the wetland at the LSP stage to ensure that subdivision and drainage impacts are minimised and appropriate ongoing management measures are implemented.”

The Structure Plan does not investigate the CCW or implement ongoing management measures but instead identifies this portion of land as being “subject to further planning”. This is not acceptable. Such an environmentally important matter like a CCW, regarded as the highest level of wetland importance on the Swan Coastal Plan, needs to be adequately dealt with by any Proposed Structure Plan relating to the lot. Excluding this feature, and attempting to promote a structure plan over just 26.5% of the subject land, is not consistent with providing a comprehensive structure planned outcome to guide future land use and development according to appropriate planning principles.

Furthermore, the SSDSP3 states *“To progress the subdivision and development of a land holding it will be necessary for landowners or groups of small landowners to prepare and submit a detailed LSP and supporting report for their land. Each structure plan should be generally in accordance with the SSDSP3, and should show detail including the proposed road and lot layout, detail areas of POS, R-Codes and other information set in the Development Area provisions of TPS No. 3”*. The SSDSP3 does not state that landowners may prepare Structure Plans over a portion of their land as has occurred over Lot 41. Thus, the entirety of this lot should be included within the Structure Plan in order to deal with the CCW as an important environmental feature of the land as per the requirements of SSDSP3.

Exclusion of CCW from Structure Plan

The Structure Plan has only been prepared over a portion of Lot 41 to avoid providing a planning structure over the remainder of this lot which comprises the CCW and CCW buffer. As a CCW, the land is intended to be protected and appropriately interfaced in respect of peripheral development surrounding.

Whilst it is acknowledged that the proponent has attempted to respect the CCW classification of this portion of Lot 41 by excluding it from the Structure Plan rather than proposing development over the CCW, the labelling of this portion of land as being "subject to further planning" raises the direct question as to what planning this may or may not be. This question is unclear, unable to be answered and thus any decision now can only be reasonably expected to prejudice future planning, whether that is associated with needing to adequately respond to the Conservation Category Wetland, or to otherwise respond to a new decision of the Department of Parks and Wildlife in respect of the status of the wetland. It is noted that currently the DPaW are involved in review proceedings direct with the applicant to the south regarding the status of the wetland as a CCW.

It is the City's view that since this land is identified as CCW at the time of lodgement and assessment of the Structure Plan, regardless of what may or may not transpire in the future, the Structure Plan must deal with the presence of the wetland. The identification and resolution of issues such as these at the first possible stage of the planning process is important to avoid these issues arising in the future and perhaps no longer being able to be addressed. Thus, it is inconsistent with DPaW's Swan Coastal Plain Geomorphic Wetland dataset, and the planning framework supporting this, to identify this portion of the site as "subject to further planning". It prejudices Council's future ability to secure the most optimal planning outcome for this land and its immediate local context.

The exclusion of this portion of Lot 41 results in the insufficient allocation of a planning structure over the land and ignores the need for the CCW to be identified for protection and managed by the responsible authorities. The exclusion of the CCW within the Structure Plan is an aspect that strikes at why this is not consistent with orderly and proper planning principles.

State Planning Policy No. 2 - Environment and Natural Resources

State Planning Policy No. 2 (Environment and Natural Resources) ("SPP2") defines the principles and considerations that represent good and responsible planning in terms of environment and natural resource issues within the framework of the State Planning Strategy. SPP2 aims

to protect, conserve and enhance the natural environment through planning decision-making by *“actively seek[ing] opportunities for improved environmental outcomes including support for development which provide for environmental restoration or enhancement.”* The Proposed Structure Plan makes no attempt to consider the CCW for restoration or enhancement despite there being an opportunity to do so through the Structure Plan process.

SPP2 also aims for planning decision-making to *“take account of the availability and condition of natural resources, based on best available information at the time.”* DPaW has identified the majority of Lot 41 as a CCW and a site investigation undertaken by the City’s environmental officers has shown the vegetation on site is significant and there are species on site that are predominately found in wetland areas. There is also a stark difference in the vegetation on site found within the CCW and outside the CCW further supporting the wetland classification.

SPP2 supports *“conservation, protection and management of native remnant vegetation where possible, to enhance soil and land quality, water quality, biodiversity, fauna habitat, landscape, amenity values and ecosystem function”* and specifically regarding wetlands encourages planning decision-making to *“consider mechanisms to protect, manage, conserve and enhance...wetlands of importance.”* As stated above, whilst there is an opportunity to protect, conserve and manage the wetland, this has been avoided by the Structure Plan through the exclusion of this portion of Lot 41 from the Structure Plan area.

Furthermore, SPP2 supports the *“use of management plans to protect areas of high biodiversity conservation value in the long term.”* The Structure Plan has made no attempt to protect the wetland or identify the wetland for conservation to be managed for the future.

Based on the above, the Structure Plan is not in accordance with SPP2.

State Planning Policy No. 2.9 - Water Resources

State Planning Policy No. 2.9 (Water Resources) ("SPP2.9") provides clarification and additional guidance to planning decision-makers for consideration of water resources. One of the key objectives of SSP2.9 is to promote and assist in the management and sustainable use of water resources. Particularly in relation to wetlands, SPP2.9 aims to *“protect, manage, conserve and enhance the environmental attributes, functions and values of significant wetlands, such as Ramsar wetlands, conservation category wetlands and wetlands identified in any relevant environmental protection policy.”* SPP2.9 also advocates restoration of

the environmental attributes, functions and values of wetlands where possible.

The CCW is identified as a significant wetland and thus should be protected. In accordance with this policy, the Structure Plan is required to make efforts to enhance and restore the wetland for conservation and management rather than ignore the wetland or argue that the wetland classification be reduced. Thus, the Structure Plan is not in accordance with SPP2.9 in that it does not attempt to protect or enhance the wetland or accurately recognise its environmental value as discussed in the 'Flora and Vegetation Survey' section below.

Council Policy SEW6 - Wetland Conservation

The City's Council Policy SEW6 *Wetland Conservation* ("SEW6") relates to wetland conservation within the City of Cockburn and emphasises the importance of protecting wetlands in the long term amidst ongoing development.

The Statement of Position under SEW6 is as follows:

"Recognising the important environmental, social, cultural, educational and aesthetic values of the range of wetlands which exist within the district, Council will make every reasonable effort to ensure the conservation, protection and management of all wetlands within the municipality."

SEW6 also provides the following policy objective:

"Ensure that wetlands are adequately assessed and their environmental values determined within the development process."

The wetland over Lot 41 has been classified as a CCW, the highest order of conservation due to its particularly important environmental values. Thus, in accordance with SEW6 Statement of Position, it is the City's responsibility to actively seek to ensure the wetland is protected and managed. This would be achieved by including the CCW within the Structure Plan area and for example reserving it as 'Parks and Recreation' or zoning 'Conservation' under the Scheme rather than excluding this portion of Lot 41 from the Structure Plan area.

Furthermore, as per the above stated policy objective, the wetland's value is to be adequately assessed through the planning process. The Structure Plan does not comply with this objective in that it has not addressed the CCW and the Flora and Vegetation Survey provided in support of the Structure Plan is outdated and unable to be relied upon.

Liveable Neighbourhoods

In respect of the provision of public parkland, Element 4 of Liveable Neighbourhoods provides guideline objectives and requirements to inform the structure planning process.

It states in regards to wetlands and buffers that “*an Environmental Protection Policy wetland, conservation category wetland, or wetland of a similar environmental value shall be ceded to the Crown free of cost without payment of compensation by the Crown in addition to the 10 per cent public open space contribution.*”

The exclusion of the CCW portion of Lot 41 from the Structure Plan effectively avoids this requirement and is thus inconsistent with Liveable Neighbourhoods.

Flora and Vegetation Survey

The Flora and Vegetation Survey (“the Survey”) lodged with the Structure Plan was undertaken in 2007 and is outdated. The Survey does not accurately represent the environmental landscape and significance of the site, particularly considering Banksia Woodland, which is prevalent at Lot 41, is now listed as a Threatened Ecological Community (“TEC”) under the *Environment Protection and Biodiversity Conservation Act 1999*. The Survey states that currently there are no TECs located at the site.

As stated within the Structure Plan report, there is also the potential that groundwater levels have changed in the area which is likely to also have an impact on species and frequency of species located at the subject land.

Furthermore, due to the presence of a TEC, the Survey is required to state the proponent’s obligations regarding referral to the Federal Department of Energy and Environment as advised by Department of Parks and Wildlife in their submission included at Attachment 3.

Given the significance of the environmental assets at Lot 41 in determining whether the proposed Structure Plan is appropriate, the City cannot support a proposal that does not provide an accurate reflection and assessment of these environmental assets.

Bushfire Management Plan

The Bushfire Management Plan (“BMP”) prepared to support the Structure Plan is not acceptable due to the portion of the subject land reserved for the future widening of Frankland Avenue being excluded from the assessment. This should remain as classified vegetation for

the purposes of assessing bushfire risk to future dwellings, as the road widening may not occur for a number of years and the subject land may be developed before this bushfire risk is removed.

Assessing this reserved land as classified vegetation is likely to impact the Bushfire Attack Level (“BAL”) ratings across the subject land and may result in an increase in risk to future dwellings. This could potentially result in future dwellings being required to construct to a higher BAL to manage the increased risk of bushfire, or the risk may be unacceptable under State Planning Policy 3.7 *Planning in Bushfire Prone Areas* to allow subdivision or development. Thus, the BMP does not adequately address the bushfire risk across the subject land and does not accurately demonstrate whether this bushfire risk is acceptable for development.

Conclusion

Based on the above, the Structure Plan does not comply with deemed provision 20(2)(d) of the *Planning and Development (Local Planning Schemes) Regulations 2015* in that it is not consistent with the planning framework and appropriate planning principles. On this basis it is recommended for refusal.

Strategic Plan/Policy Implications

City Growth

- Ensure planning facilitates a desirable living environment and meets growth targets
- Ensure a variation in housing density and housing type is available to residents

Economic, Social & Environmental Responsibility

- Sustainably manage our environment by protecting, managing and enhancing our unique natural resources and minimising risks to human health

Budget/Financial Implications

The required fee was calculated on receipt of the proposed Structure Plan and has been paid by the proponent. There are no other direct financial implications associated with the Proposed Structure Plan.

Legal Implications

Clause 20(1) of the deemed provisions requires the City to prepare a report on the proposed structure plan and provide it to the Commission no later than 60 days following the close of advertising.

Community Consultation

In accordance with clause 18(2) of the deemed provisions, the Structure Plan was advertised for a period of 28 days commencing on 11 April 2017 and concluding on 9 May 2017. Advertising included a notice in the Cockburn Gazette and on the City's webpage, letters to landowners in the vicinity of the Structure Plan area, and letters to relevant government agencies.

Council received a total of fourteen submissions, two from landowners, one from a planning firm on behalf of a landowner and eleven from government agencies. One of the landowners supported the proposal while another objected on the basis that the proposed density was out of character within the suburb and created traffic issues. The proposed density is, however, consistent with the SSDSP3 and appropriate in this location due to the proximity to local and regional parks and community facilities. Traffic generated by the proposed development can easily be accommodated by the existing street network and is not expected to have any impact on the performance of the roads. The submission prepared by a planning firm on behalf of a landowner also provided no objection to the proposal.

No government agencies provided objections to the proposal but a number of agencies provided recommendations and advice to the proponent regarding future works and implementation of the Structure Plan.

Further analysis of the submissions has been undertaken within the attached Schedule of Submissions. See Attachment 3 for details.

Risk Management Implications

The Structure Plan excludes the majority of Lot 41 which falls within the CCW and does not deal with this very important environmental feature. Excluding this land also does not allow the interface between the development and the CCW/buffer to be addressed sufficiently. If the Structure Plan is approved in its current state, this would result in a lack of planning structure over this portion of Lot 41, and will also prejudice future planning.

Furthermore, since the Flora and Vegetation Survey and the Bushfire Management Plan prepared in support of the Structure Plan are not

adequate, approving the Structure Plan may result in the environmental assets of the site not being appropriately addressed or protected, and the bushfire risk of future dwellings being unacceptable or higher than expected. This is a significant risk to the Council and community alike.

Attachment(s)

1. Structure Plan
2. Location Plan
3. Schedule of Submissions

Advice to Proponent(s)/Submissioners

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 8 June 2017 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

15.11 (OCM 08/06/2017) - FINAL ADOPTION OF PROPOSED SCHEME AMENDMENT NO. 112; LOCATION: LOT 701 (PREVIOUSLY 101), LOT 703 (PREVIOUSLY 103) AND LOT 702 (PREVIOUSLY 104) JANDAKOT ROAD, JANDAKOT; OWNER: SCHAFFER CORPORATION LTD; APPLICANT: MGA TOWN PLANNERS (109/048) (L SANTORIELLO) (ATTACH)

RECOMMENDATION

That Council:

- (1) in pursuance of the *Planning and Development Act 2005* ('Act') and the *Planning and Development (Local Planning Schemes) Regulations 2015*, resolves to adopt the amendment to City of Cockburn Town Planning Scheme No. 3 ("Scheme"), with modifications, as follows:

1. Amending Additional Use No. 1 ('AU 1') contained in the table of Additional Uses to read and to be amended as follows:

No.	Description of Land	Additional Use	Conditions
AU 1	Lots 701, 702 and 703 (excluding Bush Forever Area 388) Jandakot	<ul style="list-style-type: none"> • Nursery; • Masonry Production; • Warehouse, Showroom and Storage where the display, selling, 	Development Approval for Lots 701, 702 and 703 Jandakot Road, Jandakot, are subject to;

	<p>Road, Jandakot. [Formerly Lots 101, 103 and 104 Jandakot Road, Jandakot.]</p>	<p>hiring or storage of goods, equipment, plant or materials and the incidental site activities do not pose risk of pollution to the below ground public drinking water source.</p> <p>The Use Class Definition's for 'Warehouse', 'Showroom' and 'Storage' are defined in Schedule 1 of the Scheme inclusive of the supplementary restrictions as mentioned above which limit the nature of the permissible goods, equipment, plant or materials to those which do not pose risk of pollution to the below ground public drinking water source.</p> <p>1. Environmental Requirements Industrial Wastewater: All wastewater produced from activities on-site must be disposed of to a system approved by the Local Government and in liaison with the Department of Water.</p> <p>Site Chemical Risk: A Site Chemical Risk Assessment Report being prepared and implemented and regularly updated.</p> <p>Dust Management: No visible dust generated by any aspect of operations on-site is to leave the subject land. The operator is required to submit to the Local Government, after consultation with the Department of Environment Regulation a</p>	<p>a) Due consideration to groundwater risk minimisation.</p> <p>b) No bulk storage of green-waste, compost or 'Toxic and Hazardous Substances' ('THS') are permitted above 25 litres in total volume, excluding fuel within vehicle fuel tanks. THS includes pesticides, herbicides, fuel (storage), explosives, flammable liquids, cleaners, alcohols, fertilisers (other than on lot 702 under current development approvals), medical or veterinary chemicals, pool chemicals and corrosive substances; inclusive of the substances listed in the Poisons Act 1964 (Appendix B). These substances may only be stored in volumes above 25 litres if contained within domestic sized packages ready for end-use in domestic situations.</p> <p>c) Due consideration and compliance with the Western Australian Planning Commission's 'Transport Assessment</p>
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		<p>Dust Management Plan. The Dust Management Plan must be to the satisfaction of the Local Government, and upon approval by the Local Government, is to be adhered to by the land owner/(s) at all times.</p> <p>Noise Emissions: The development is to comply with the <i>Environmental Protection Act 1986</i>, which contains penalties where noise limits exceed those, prescribed by the <i>Environmental Protection (Noise) Regulations 1997</i>. If noise emissions from loading operations and the block plant fail to comply with the <i>Environmental Protection Act 1986</i>, additional acoustic measures must be carried out as soon as reasonably practical to ensure the use complies with the Act.</p> <p>Lighting: The installation and maintenance of lighting must at all times comply with the requirements of Australian Standard AS 4282-1997 "Control of the Obstructive Effects of Outdoor Lighting".</p> <p>Complaints: The operator must prepare a "Complaints Handling Procedure" to ensure that there is a process for administering any complaints including the recording, investigation and</p>	<p><i>Guidelines for Developments'</i>, where appropriate.</p> <p>d) The prior preparation and approval of a Local Development Plan ('LDP') detailing;</p> <ul style="list-style-type: none"> i. The standards to be applied for physical development in order to ensure the protection of the below ground public drinking water source; ii. Vehicle access and egress arrangements; iii. Noise mitigation measures pursuant to the details of an acoustic report where required (refer to point 'e' below); iv. Interface controls and/ or measures with regard to Bush Forever Area 388, including, but not limited to; a hard road edge within the AU1 area abutting the Bush Forever area and/or bushland identified for protection; Bushfire mitigation measures being provided outside the Bush Forever
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		<p>response to any concern regarding the operation.</p> <p>2. Design Requirements Building design and location shall minimise the visual impact of the development from surrounding residents inclusive of appropriate buffers, noise bunds and vegetation (light and visual) screening. Building materials and colours must be clad or coloured to complement the surroundings, and/or adjoining developments in which it is located, and shall use non-reflective materials and colours. Regard shall be had to the screening of product storage. Staging Plan in the form of a Local Development Plan ('LDP') shall be prepared by the applicant and approved by the Local Government prior to any development within Additional Use area 1.</p> <p>3. Traffic requirements Planning proposals shall demonstrate appropriate traffic generation calculations and traffic impact assessments on the current and future planned road network. Mitigation measures shall demonstrate viability and road upgrade responsibilities. The</p>	<p>area within the AU1 area; an appropriate wetland buffer, if considered relevant by the assessing authority, and; drainage to be contained within the AU 1 area.</p> <p>e) With regard to any application for 'Warehouse', 'Showroom' or 'Storage', the preparation and lodgement of a report prepared by a suitably qualified acoustic consultant detailing the potential noise impact on noise sensitive land uses. The report shall demonstrate how the proposed development has been acoustically assessed and designed for the purposes of minimising the effects of noise intrusion and/or noise emissions. The report must demonstrate the measures required to address noise to the Local Government's satisfaction and be implemented and maintained as part of the development of the land</p> <p>f) Development of any 'Warehouse', 'Showroom' or 'Storage' must:</p>
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		<p>extent of all traffic related considerations should be identified and agreed upon early in the planning process to the satisfaction of the Local Government.</p>	<ul style="list-style-type: none"> i. Be connected to a reticulated sewer system; ii. Have all lighting comply with the requirements of Australian Standard AS-4282-1997 <i>"Control of the Obstructive Effects of Outdoor Lighting"</i> and the <i>Civil Aviation Regulations 1988</i> and the <i>Civil Aviation Safety Authority Manual of Standards</i> in accordance with the details prescribed within the <i>Jandakot Airport Masterplan</i>; iii. Have all structures comply with the <i>Obstacle Limitation Surfaces</i> in accordance with the details prescribed within the <i>Jandakot Airport Masterplan</i>; iv. Have a 'Site Chemical Risk Assessment Report' prepared, implemented and regularly Including annual reporting to the Local Government and the
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			<p>Department of Mines and Petroleum.</p> <p>v. Lodge a Dust Management Plan for approval by the Local Government and ongoing compliance by the property owner/(s).</p> <p>g) Building design, internal vehicles access ways, and locations shall minimise the amenity impact of the development from surrounding residents.</p> <p>h) Building materials and colours must be clad or coloured to complement the surroundings, and/or adjoining developments in which it is located, and shall use non-reflective materials and colours.</p> <p>i) No below ground storage is permitted.</p> <p>j) Stormwater from roofs and clean paved areas should be directed away from potentially contaminated areas where THS (below 25 litres in total volume) are stored or handled. Stormwater from carpark areas is to be managed as recommended</p>
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			<p>in the Stormwater Management Manual for Western Australia (reference 8d) or relevant equivalent.</p> <p>k) Any liquids discharged to the environment (via soakage or ground application) should have been tested as compatible with downstream water resource values. Discharge to drains or waterways should not occur due to the risk of release of contaminated water. The effluent quality should be determined by sampling in accordance with Australian Standard 5667 Water quality sampling (reference 9b) or relevant equivalent.</p> <p>l) As part of future development and/or subdivision of the subject land, the applicant shall; provide the land for the Bush Forever site (as agreed) free of cost and ceded to the Crown. This is to be provided at the first available planning opportunity.</p> <p>m) As part of future development and/or subdivision of the subject</p>
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			<p>land, the land owner/ applicant will be expected to:</p> <ul style="list-style-type: none"> i. Provide the land for the widening of the adjoining section of Jandakot Road from a single carriageway road to a dual carriageway road free of cost to the City of Cockburn; ii. Upgrade the adjoining section of Jandakot Road from a single carriageway to a dual carriageway. <p>n) Appropriate native vegetation planning/ planting consideration and conditioning within the area of land east of the AU1 boundary and the adjacent rural residential 'Resource' zoned lots. This land is considered to be a 'rural amenity buffer'. Accordingly, its embellishment should be proportionally reflective of the scale of the proposed development.</p> <p>o) The minimum subdivision and development application lot size</p>
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			<p>requirements and leasehold lot size requirements are as per <i>State Planning Policy 2.3 (Jandakot Groundwater Protection)</i> minimum lot size requirements.</p> <p>p) The subject site is likely to be affected by aircraft noise as the 20, 25 and 30 Australian Noise Exposure Forecast ('ANEF') contours falls within the AU1 area. Acceptable land use and building types should be compliant with regard to <i>State Planning Policy 5.3 (Land Use Planning in the Vicinity of Jandakot Airport)</i> and the Building site acceptability table from AS2021.</p>
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COUNCIL DECISION

Background

The subject land is located broadly on the corner of Jandakot Road and Berrigan Drive and is commonly known as the "Urbanstone" site. Jandakot Airport is situated directly to the North of the subject site.

Lot 701 is approximately 6.2097ha in area and is occupied by the "Urbanstone" factory producing masonry products. Lot 702, being approximately 3.2442ha, sits at the corner of Jandakot Road and Berrigan Drive and is currently occupied by a nursery. The remainder of the subject site is located on Lot 703, located north and east of the

“Urbanstone” plant, and is approximately 44.9639ha in area and partially cleared, having been previously mined for sand resources and since revegetated.

The northern portion of Lot 703 is heavily vegetated and occupied by Bush Forever Site 388, which has an area of approximately 12.97ha.

Additional Use No.1 (“AU1”) of the Scheme is currently located over Lots 702, 701 and approximately 2.5ha of Lot 703 and allows for the use of the land for “Nursery”, “Masonry Production”, “Warehouse only where ancillary to Masonry Production” and “Showroom only where ancillary to Masonry Production”. Masonry Production and Warehouse are restricted to Lot 701 (Scheme refers to previous lot number, lot 101).

Council at its meeting of 13 December 2012 resolved to adopt Scheme Amendment No. 91 which extended the then AU 1 area and introduced the additional uses of “Nursery”, “Showroom” and “Warehouse”, where “Warehouse” and “Showroom” are ancillary to Masonry production. Prior to Amendment 91 “Masonry Production” was the only additional use and it applied only to the then Lot 77 on Diagram 86541 Jandakot Road, Jandakot.

On 8 September 2016 Council initiated item number 14.2; Scheme Amendment No. 112. Following Councils initiation in September, the Amendment was subsequently forwarded to the Environmental Protection Authority (‘EPA’). This was to enable the EPA to comply with section 48A of the *Environmental Protection Act 1986 (the ‘EP Act’)*, in relation to the proposed scheme amendment.

Pursuant to Part 5 Division 2 Regulation 37 (2) of the *Planning and Development (Local Planning Schemes) Regulations 2015* (‘the Regulations’) the City forwarded 2 copies of the [then] proposed Amendment No. 112 to the Commission following Council’s initiation. This was to provide the Commission with the opportunity to examine the documents and advise the City of Cockburn if the Commission considered any modification/(s) to the documents were required prior to the amendment, to the local planning scheme, being advertised.

After consideration the EPA formally responded on 24 October 2016 indicating the proposed scheme amendment should not be assessed under Part IV Division 3 of the EP Act and that the EPA did not consider it necessary to provide any advice or recommendations. The EPA did advise, however, the requirements as stipulated by Part IV Division 4 of the EP Act. This section stipulates “a responsible authority shall monitor or cause to be monitored the implementation of its assessed schemes and of proposals under its assessed scheme”. On

this basis the EPA referral requirements have been met by the City of Cockburn under the proposed Scheme text.

Pursuant to Regulation 37(4), the Commission examined the documents provided by the City to determine whether any modifications were required before the amendment was permitted to be advertised.

On 25 October 2016 the Commission wrote to the City advising the City that the amendment was suitable to be advertised subject to modification. The modifications were administrative in nature and related primarily to terminology rather than the planning content. These modifications have since been incorporated into the above resolution of Council. The modifications are specifically identified in detail in the accompanying schedule of modifications table under item number 39 (Attachment No. 5).

Following the above mentioned support from both the EPA and the Commission, the City subsequently advertised the proposed scheme amendment pursuant to the advertising requirements of the Regulations. The report before Council aims to address the comments raised as a result of the advertising period.

Submission

The proposed Scheme Amendment was project managed by MGA Town Planners on behalf of the landowner Schaffer Corporation Ltd. The proposal seeks to extend the AU 1 area covering Lots 702, 701 and portion of Lot 703, Jandakot Road to include the whole of Lots 702, 703 and 701 excluding Bush Forever Site 388. (Refer to attachment 2).

Report

Perth and Peel at 3.5 Million and supporting documentation

Council, at its meeting of 8 September 2016 under item 14.2 in relation to the initiation of this proposed Scheme Amendment, made mention of Councils previous submission on the 'Perth and Peel @ 3.5 Million documentation'.

Council's resolution with respect to Perth and Peel emphasised nine (9) points in particular, of which four (4) are considered to be relevant to the Urbanstone site at Lots 703, 701 & 702 Jandakot Road, Jandakot. These points are listed, as extracted, below for convenience;

1. *“For the future development of the Banjup north precinct, a more legible spatial boundary should be adopted based upon Armadale Road; Warton Road; Jandakot Road; Berrigan Drive and; the Kwinana Freeway. This will enable a further strategic planning element to take place by local government, working with landowners and the community to determine the ultimate nature of land use and development in the precinct;*
2. *Questions are raised about what happens in the area north of Jandakot Road and particularly surrounding Jandakot Airport. Is it realistic that the document seek to retain a rural setting, typified by 2ha lots sizes with the landscape containing buildings, or will this area be unable to support required levels of rural amenity given its proximity to the airport and urban development to the south;*
7. *Further work is needed to analyse the regional sports needs of the sub-region, before deciding whether the location on Jandakot Road as currently designated by the document is appropriate; and*
8. *The delivery of a future Jandakot Road Other Regional Road will need to be based upon developer contributions, and need to limit land impacts to the north, given it is the southern adjoining land use that is changing from rural to urban.”*

The City has not yet received a formal response from the WAPC regarding the City's submission on the 'Perth and Peel @ 3.5 Million documents'. The City has met, however, with WAPC staff in relation to the proposed Amendment and its placement within the scope of Perth and Peel. The Commission indicated a general, without prejudice, support for the proposal based purely from a broad land use planning perspective. The Commission indicated to Council staff, the Scheme Amendment would need to demonstrate compliance with State Planning policy and will be subject to later assessment, by the Commission, in accordance with proper and orderly planning principles.

Notwithstanding the above, as mentioned within Appendix 5 of the Scheme Amendment application report, the proposal is accompanied by two separate letters of support from the Chairperson of the Western Australian Planning Commission both dated 23 June 2015.

These letters identify, in the view of the Chairman, the approach of this amendment may have strong merits in terms of its current and future uses for purposes associated with Jandakot Airport; in particular the 'Specialised Centre' which is identified by a yellow circle on the *South Metropolitan Peel Sub-regional Planning Framework Towards Perth and Peel @ 3.5 million* document.

The Chairman advises, the Department and subsequently the Commission will need to re-examine its proposals to not only relocate the recreational site but also give consideration to the site being considered more as a commercial site due to its proximity to Jandakot Airport, Roe Highway and Kwinana Freeway; and the proposed freight link extension network of the Government.

From a strategic perspective Jandakot Airport Holdings (JAH), in their letter dated 1 September 2014, believes the subject land should be regarded as part of the airport site for operational and commercial reasons in conjunction with the proposed freight link extension.

The City formally referred the proposed Scheme Amendment to the WAPC prior to advertising, as per Regulation 37 (4) and Resolution (7) of Council's Initiation report. The WAPC provided a formal response to the City of Cockburn indicating *"the Commission has examined the documents provided to determine whether any modification is required before the amendment is advertised. The Commission advises that the amendment is suitable to be advertised subject to [four (minor) administrative changes]."* These details are located under Submission number 39 of the attached Schedule of submissions table under Attachment No. 5.

The Commission did not raise any points, in their letter, in relation to the proposed Scheme Amendment in the context of the State governments Perth and Peel @ 3.5 Million documents.

In addition to the above, the City formally referred the proposed Scheme Amendment to the Commission via two separate letters of correspondence. This was in addition to the referral under Regulation 37 (4). Specifically the City of Cockburn referred the proposed Scheme Amendment to 'the Department of Planning' (generic address) and also specifically to the 'Bush Forever Branch' (within the WAPCs offices) as part of the Amendment's advertising process.

In relation to the above two referrals, the Commission provided one response in reply which was from the 'Bush Forever Branch'. This submission is provided for under submission number 35 of attachment No. 5, the Schedule of submissions response table. It is noted this submission made mention;

"Please note this is officer level advice with regard to Bush Forever and State Planning Policy 2.8 only and does not reflect comments of other branches within the Department of Planning and is not a formal position of the Western Australian Planning Commission."

On this basis the City of Cockburn has referred the proposed Scheme Amendment to the WAPC as per the Scheme Amendment advertising requirements, as set by the Regulations. The Commission has not indicated any conflicts with the proposed Scheme Amendment and the Commissions draft Perth and Peel @ 3.5 Million documents. Notwithstanding it is understood the Commission will undertake a further assessment of the proposed Amendment prior to their determination.

As mentioned in the 8 September 2016 Council report for the Initiation of this Amendment, City officers have met with the Commission in the early stages of this Amendment. The Commission expressed a general, without prejudice, level of support for the proposal within the context of the subject land and the States broader strategic objectives.

It is important to note the framework for a growing City of 3.5 Million people is currently in draft stage and will be further developed and finalised as sub-regional structure plans that will provide guidance for:

- *“the preparation of amendments to the Perth metropolitan and Peel region schemes, local planning strategies/schemes and district, local and activity centre structure plans; and”*
- *“the staging and sequencing of urban development to inform public investment in regional community, social and service infrastructure.”*

At this early stage in (State) Strategic Planning, the overarching framework, particularly at a more specific Statutory level, has not yet been finalised. In this respect, the City received objections as per the below. The below objection was supported by 21 further Objections (signatories). It is respectfully recommended by the Banjup Residents Group, Council;

“Either:

- *Defer any decisions on Schaffer’s application and on alternative land uses for current [rural] residential landowners in rural Jandakot until after the publication of the WAPC’s final South Metropolitan & Peel Planning Framework; then*
- *Develop a formal structure plan for the whole of the Jandakot rural area from Berrigan Drive to Warton Road that includes:*
 - *Schaffer’s application*
 - *Residents’ considerations*

Or, [alternatively]:

- *Defer a decision on Schaffer’s application until the City of Cockburn has developed a formal structure plan for the whole of*

the Jandakot rural area from Berrigan Drive to Warton Road that includes:

- *Schaffer's application*
- *Residents' considerations."*

The full submission, as provided by the Banjup Residents Group is provided for under submission number 8 of the attached Schedule of Submissions response table.

With respect to the planning framework, as mentioned above, the Perth and Peel at 3.5 Million documents provide a broad strategic perspective as follows;

"The draft South Metropolitan Peel Sub-regional Planning Framework has been developed by the Department of Planning, on behalf of the Western Australian Planning Commission. It represents a whole-of-State Government approach to managing the future urban form within the sub-region. It will be subject to further refinement prior to its finalisation and endorsement as a sub-regional structure plan."

In relation to the request for deferral, it is important to note the WAPC has indicated an in principle, without prejudice, Strategic Planning level of support for the proposed Scheme Amendment. This has been provided by the above mentioned letters from the Chairman of the WAPC and in relation to subsequent officer level meetings with the Department of Planning staff.

It is respectfully not considered appropriate for the City to prepare its own "formal structure plan for the whole of the Jandakot rural area from Berrigan Drive to Warton Road". This is on the basis the City of Cockburn is expected to be guided by the planning decisions of the State Government and therefore any Local Government district structure plan (if one were to be applicable) would need to have due regard to the State level policy. In this respect the City of Cockburn's hypothetical contemplation of a 'Structure Plan' for the area is considered to be premature with respect to the current position of the State government.

The State planning framework is in a draft stage and whilst it is not yet complete the author, the Department of Planning/ WAPC has indicated a general, without prejudice, level of support with respect to the proposed Amendment. From a Local Government Scheme perspective, the draft scheme text, as proposed inclusive of modifications as a result of the advertising process, aims to ensure appropriate levels of land use compatibility to the benefit of the adjacent community members.

The Scheme text aims to ensure any future (modified) 'Showroom', (modified) 'Warehouse' and/ or (modified) 'Storage' land uses will be compatible, by way of future Development Application and approval controls, with the surrounding rural residential land uses. The draft Scheme planning controls address the following points by way of scheme text provisions and/ or scheme map provisions;

- Appropriate native vegetation planning/ planting consideration and conditioning within the area of land east of the Additional Use No. 1 ('AU1') boundary and the adjacent rural residential 'Resource' zoned lots. This land is considered to be a 'rural amenity buffer'. Accordingly, its embellishment should be proportionally reflective of the scale of the proposed development.
- In addition, as per resolution number 3 above, it is proposed to reduce the proposed AU1 area (Scheme Map) by relocating the eastern boundary westwards by 100 metres. This is to ensure the adjacent eastern 'Resource' zoned rural/ residential lots are provided with an appropriate 'rural amenity buffer'.
- Pursuant to resolution 5 above, it is proposed to amend the proposed AU1 area (Scheme Map) by deleting the eastern portion proposed for 'road link' to Jandakot Airport. This is on the basis of protecting the rural amenity of the adjacent rural/ residential lots.
- Noise mitigation requirements pursuant to the details of an acoustic report (Scheme text requirement).
- The minimum subdivision and development application lot size requirements and leasehold lot size requirements are as per *State Planning Policy 2.3 (Jandakot Groundwater Protection)* minimum lot size requirements. This mandates a minimum 2ha lot size.
- Interface controls and/ or measures with regard to Bush Forever Area 388 (see resolution above for details).
- All lighting is to comply with the requirements of Australian Standard AS- 4282-1997 "*Control of the Obstructive Effects of Outdoor Lighting*" and the *Civil Aviation Regulations 1988* and the *Civil Aviation Safety Authority Manual of Standards* in accordance with the details prescribed within the *Jandakot Airport Masterplan*.
- Development may require a 'Site Chemical Risk Assessment Report' prepared, implemented and regularly updated, including annual reporting to the City of Cockburn and the Department of Mines and Petroleum.

- The Lodgement of a Dust Management Plan for approval by the Local Government and ongoing compliance by the property owner/(s) may be required at development application stage.
- Building design and internal vehicles access ways are required to minimise the amenity impact of the development from surrounding residents.
- Building materials and colours must be clad or coloured to complement the surroundings, and/ or adjoining developments in which it is located, and shall use non-reflective materials and colours.

In addition to the above, the Scheme Amendment proposal includes a (now revised) Noise and Vibration Impact Assessment which has been supported by Council staff. The updated and revised Noise and Vibration assessment concludes under section 6, in relation to 'showrooms', the Regulations are expected to be complied with at all times.

In relation to the 'warehouse' land use, the type and location of activity may influence whether compliance with the Regulations can be achieved or not. Forklift work, for example, in an open yard associated with warehouse and outdoor storage areas, and the use of refrigerated trucks for deliveries (should they be required by the future developments) can potentially result in exceedances of the assigned noise levels.

On this basis the revised acoustic report, as provided within the Amendment proposal, mandates the requirement for future warehouse/ logistic premises development proposals (located adjacent to Rural/Residential properties) to be accompanied by a site specific acoustic assessment. This future acoustic report will be required to be assessed and approved by City officers and the outcome of the final report will be required to be conditioned with regard to any future Development Application/(s).

On this basis, the above mentioned objection in relation to the proposal is duly noted. The Scheme Amendment Map and Scheme text has been amended accordingly to protect the rural amenity of the existing Rural/Residential community. The request for deferral is however respectfully not considered to be appropriate, as indicated above, as provided for within the WAPC's document;

"The draft South Metropolitan Peel Sub-regional Planning Framework is subject to further refinement prior to its finalisation and endorsement as a sub-regional structure plan."

In addition to the above it is important to note, under Part 5 Division 2 Regulation 41 (3) of the *Planning and Development (Local Planning Schemes) Regulations 2015* the City is obligated to proceed with the Scheme Amendment application on the basis the formal advertising period has now concluded.

On this basis the broad strategic planning objectives are not considered by the City to be compromised by the proposed Scheme Amendment. The Commission will ultimately determine the proposal prior to the Hon. Minister for Planning. In this respect the objectives of the State Government will be met by the State government under their assessment of the proposed Amendment following Councils consideration.

Noise and Vibration Impact Assessment

Resolution '(1)1' of Councils' 8 September 2016 Initiation report made mention of the following:

"The Noise and Vibration Impact Assessment (under Appendix 1) to be updated to incorporate the advice from the City of Cockburn's Health Services dated 9 August 2016. This aims to make it clear under page 21 that any future application will require a development specific Acoustic report, including the site identified by dot point 1 on page 21 of the report. This is to be to the satisfaction of the Chief Executive Officer."

The '*Noise and Vibration Impact Assessment*' as originally lodged with the Scheme Amendment proposal is dated 7 June 2016. Following Councils' resolution, as extracted above, the applicant later submitted a revised *Noise and Vibration Impact Assessment* to City staff for assessment. City Staff, under delegation from the CEO, informally adopted a later version of the *Noise and Vibration Impact Assessment*, the 21 October 2016 report.

Resolution number eight (8) above, of this report, aims to finalise Councils adoption of the (revised) 21 October 2016 *Noise and Vibration Impact Assessment*. It is proposed for the updated report to be included within the updated Scheme Amendment documentation prior to its forwarding to the WAPC for final consideration.

The revised sections of the *Noise and Vibration Impact Assessment* have been extracted from the (revised) 21 October 2016 *Noise and Vibration Impact Assessment* and provided as Attachment number 4 to this Council report for ease of reference.

The subject site will need to address the impacts of noise, at the future detailed 'Development Application' stage, so that the adjacent

rural/residential 'Resource' zoned lots are not negatively impacted by noise and vibration.

In addition to the above, please note the draft Scheme text under column 4 section 'e' of the Additional Use No. 1 provisions specifies the following;

“With regard to any application for ‘Warehouse’, ‘Showroom’ or ‘Storage’, the preparation and lodgement of a report prepared by a suitably qualified acoustic consultant detailing the potential noise impact on noise sensitive land uses. The report shall demonstrate how the proposed development has been acoustically assessed and designed for the purposes of minimising the effects of noise intrusion and/or noise emissions. The report must demonstrate the measures required to address noise to the Local Government’s satisfaction and be implemented and maintained as part of the development of the land.”

On this basis, the issue of noise and vibration is expected to be addressed appropriately at the next stage of planning, the 'Development Application' stage. At this early stage, the Strategic 'Scheme Amendment' stage the application does not include the specifics of the intended operations. The Scheme Amendment process relates only to broad land use permissibility, as such the objective of the Amendment is to ensure a clear outline of objectives for the next stage of planning.

This directly relates to some of the concerns of the Community as provided for during the advertising period by way of submissions to Council.

Environmental Assessment

Resolution '(1)2' of Councils' 8 September 2016 Initiation report made mention of the following:

“The Environmental Assessment (under Appendix 8) to be updated to incorporate the advice from the City’s Environmental Services dated 17 August 2016. This aims to ensure further investigation is required with regard to the remnant vegetation directly to the south of the Bush Forever Site. It is noted a firebreak will be required to the south of the Bush Forever site. The identified adjacent bushland is considered to be an appropriate strip for such purposes. This is to be to the satisfaction of the Chief Executive Officer.”

Figure 3 of Councils 8 September 2016 initiation report has been extracted and inserted below for illustration purposes. The area in

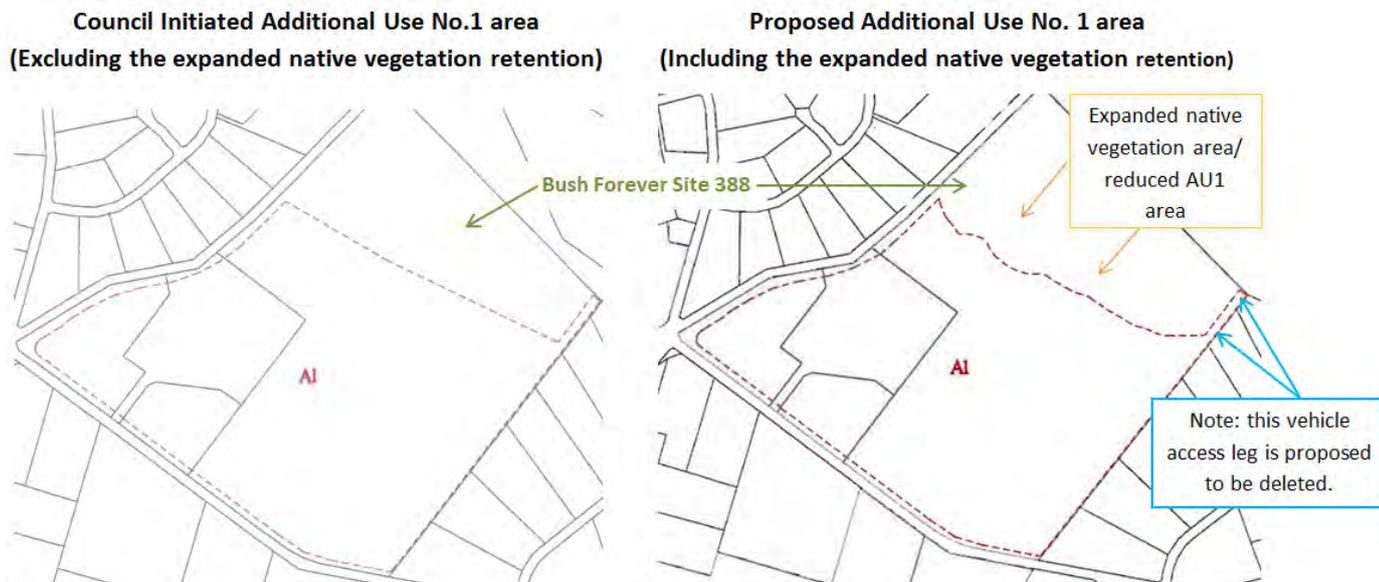
question identified in-between the 'Bush Forever' site (in Orange) and the red line beneath have since been resolved.

Figure 1: Bush Forever Site (orange) with remnant vegetation beneath (red).



As a result of such discussions the Applicant provided an updated draft Scheme Amendment Map to reduce the Additional Use No. 1 area. This will effectively reduce the area of the developable land which will ensure the protection of the above mentioned vegetation in perpetuity. The below image provides a visual illustration of the agreed outcome.

Figure 2: Draft proposed Additional Use area No. 1 incorporating the expanded native vegetation retention.



It is important to note the above draft 'A1' (Additional Use No. 1) area proposes a road connection to the north east of the site. This is proposed to be deleted via resolution number five (5) above. This is

discussed and elaborated on in further detail in the below “Jandakot Airport Masterplan (2014)” section of this report.

Council’s objective with respect to the ‘Environmental Assessment’ as outlined within the Initiation report has been addressed by the Applicant. This has also been conditioned under Resolution four (4) above to ensure the Scheme Map reflects the agreed Environmental Assessment outcome.

It is important for the Community to be aware; the native vegetation within the property boundary from the red line in figure 1 north (including the Orange area) is proposed to be retained in perpetuity. This is given specific consideration under the proposed Scheme map and also the Scheme text. Please refer to column 4 point ‘1’ which specifies the following;

“As part of future development and/or subdivision of the subject land, the applicant shall; provide the land for the Bush Forever site (as agreed) free of cost and ceded to the Crown. This is to be provided at the first available planning opportunity.”

This point was raised by a number of submitters. It is important to note, for those members of the Community that are concerned, the above mentioned ‘Bush Forever Site 388’ is proposed to be retained and protected in perpetuity.

The WAPC’s Bush Forever section requested the last sentence above [*“This is to be provided at the first available planning opportunity”*] to be included within the revised scheme text. As such, the scheme text has been amended accordingly to the WAPC’s Bush Forever sections satisfaction. Please refer to submission number 35 within Attachment 5 for further details.

Traffic Report

Resolution ‘(1)3’ of Councils’ 8 September 2016 Initiation report made mention of the following:

“The Traffic Report (under Appendix 6) to be updated to incorporate the advice from the City dated 22 August 2016. This aims for the report to be updated to identify how the extensive queue lengths expected by 2031 can be reduced by maybe providing additional road capacity on the approaches to the intersection, and/or any other measures. This is to be to the satisfaction of the Chief Executive Officer.”

The ‘Traffic Report’ as originally lodged with the Scheme Amendment proposal is dated June 2016. Following Councils resolution, as extracted above, the applicant later submitted a revised *Traffic Report*

to City staff for assessment. City Staff, under delegation from the CEO, informally adopted a later version of the *Traffic Report, the November 2016 report*.

Main Roads Western Australia ('MRWA') provided two submissions in relation to the Traffic Report. Submission Number 36 of Attachment No. 5 provides MRWA's initial submission. This submission objected to the proposal as follows:

"Main Roads is interested in better understanding the impact of this development on the operation of the Kwinana Freeway / Berrigan Drive interchange. To this end, Main Roads requests the network volumes (existing and at 2031) for the area extending to and including the interchange of Kwinana Freeway / Berrigan Drive."

Following discussions between the City of Cockburn Staff, MRWA and the Applicant and their Traffic Consultants, MRWA provided a second submission in response to the revised Traffic Report. MRWA's second submission is provided for under submission number 37. The second submission from MRWA made mention of the following:

"Main Roads now wishes to revise its response to this proposed scheme amendment and advise that we have no objections to the proposed increase of land use permissibility over the above lots."

Main Roads notes that the traffic distribution assumed by [the Applicants Traffic Engineers] on the Berrigan Drive / Pilatus Street routes differs from our understanding from other investigations in this area.

Whilst content that this should not be critical to this scheme amendment, it will have a bearing on the form and function of intersections along Berrigan Drive and Pilatus Street - provided for information to City of Cockburn."

City Staff are aware of the comments as provided by MRWA and aim to ensure any future development application appropriately addresses these concerns.

Pursuant to the above, the draft Scheme text specifies under column 4 point 'm' of Additional Use No. 1 as follows:

"As part of future development and/or subdivision of the subject land, the land owner/ applicant will be expected to:

- i. Provide the land for the widening of the adjoining section of Jandakot Road from a single carriageway road to a dual carriageway road free of cost to the City of Cockburn;*

- ii. *Upgrade the adjoining section of Jandakot Road from a single carriageway to a dual carriageway.”*

Resolution number nine (9) above, of this report, aims to finalise Councils adoption of the (revised) *November 2016 Traffic Report*, as supported by MRWA. It is proposed for the updated report to be included with the updated Scheme Amendment documentation prior to its forwarding to the WAPC for final consideration. Additionally pursuant to Resolution No. 5 above the revised Traffic Report is expected to be amended further. This is discussed below in the “Jandakot Airport Master Plan (2014) section”.

State Planning Policy 2.3 Jandakot Groundwater Protection Policy January 2017 (WAPC)

The subject site falls within the boundary of the Jandakot groundwater protection area and as such SPP 2.3 is a relevant, and crucial, consideration in the assessment of the proposed Scheme Amendment.

The objectives of SPP 2.3 are as follows:

- *“To ensure that all development and changes to land use within the policy area are compatible with maximising the long-term protection and management of groundwater, in particular for public drinking water supply;*
- *To protect groundwater quality and quantity in the policy area in order to maintain the ecological integrity of important wetlands that are hydraulically connected to that groundwater, including wetlands outside the policy area;*
- *To prevent, minimise, and manage in defined locations development and land uses that may result in contamination of groundwater; and*
- *To maintain or increase natural vegetation cover over the policy area”.*

The Perth and Peel section of this report, as per above, makes mention; the Scheme text aims to ensure any future ‘modified’ Showroom, ‘modified’ Warehouse and/ or ‘modified’ Storage land uses will be compatible, by way of development application and approval controls, with the surrounding rural residential land uses. The word ‘modified’ is included as the proposed Scheme text aims to restrict the land uses so that the land use permissibility is consistent with the objectives of SPP 2.3. The Scheme text proposal specifies under column 3 of the proposed Additional Use No. 1 provisions that the following land uses are proposed to be permissible.

“Warehouse, Showroom and Storage where the display, selling, hiring or storage of goods, equipment, plant or materials and the incidental site activities do not pose risk of pollution to the below ground public drinking water source.”

Each and every (future) ‘Development Application’ within the AU1 area (Lots 702, 701 & 703) will be assessed, from a land use perspective, on its merits in accordance with the above mentioned proposed Scheme text provisions. The City will, in this way, ensure each and every (future) Development Application meets the land use requirements as per the objectives of SPP 2.3.

The Department of Water (‘DoW’) was consulted during the preparation of the proposed Amendment and during the Advertising process to ensure compliance with SPP 2.3. The DoW is identified under the WAPC’s SPP 2.3 as the ‘responsible authority’ with respect to protection of Jandakot Groundwater.

The DoW made a submission with respect to Amendment No. 112 which has been included as item number 5 of the attached Schedule of submissions table. The DoW indicated support for the proposed Scheme Amendment as follows;

“The Urbanstone facility that currently operates on the site is deemed an incompatible land use in the Jandakot UWPCA. However, it is an operation that is a pre-existing, non-conforming land use that was established prior to the gazettal of SPP 2.3. Therefore the land use is permitted to continue to operate in line with the best management practices under non-conforming use rights of this policy.

Thus with regards to the proposed amendment to allow new provisions and expand the existing land use over the lots, the Department has no objections subject to the employment of best management practices outlined in the following

Water Quality Protection Notes (WQPN) found at www.water.wa.gov.au

- WQPN 32: Nurseries and garden centres*
- WQPN 52: Stormwater management at industrial sites*
- WQPN 65: Toxic and hazardous substances - storage and use*
- WQPN 90: Organic material - storage and recycling*
- WQPN 93: Light industry near sensitive waters”*

In addition to the above, the ‘Department of Environment Regulation’ (‘DER’) made a submission in support of the proposal as follows;

“DER has no comment on this matter in reference to regulatory responsibilities under the Environmental Protection Act 1986 and the Contaminated Sites Act 2003.”

The DER’s submission is provided for as submission number two (2) of the Schedule of submissions table.

Submission number 31 of the Schedule of submissions table included the following objection, as extracted below;

“...We continue to be impacted, inconvenienced and squeezed by the relatively unconstrained developments on surrounding land by Jandakot Airport Holdings (Precincts 5 & 6 clearing and development), Stockland’s Calleya/Treeby residential development, Jandakot road widening, and now Schaffer’s proposed developments. It is unreasonable and unfair that these developments are allowed on similar adjacent land, while stringent restrictions remain imposed on our land...”

With respect to the above objection; the subject site, at Lots 702, 701 & 703/ the proposed expanded AU1 scheme area, is subject to SPP 2.3 requirements, as mentioned above. It is important to note each application is assessed on its merits. This applies to Stockland’s Calleya/Treeby residential development, the surrounding rural/residential lots and also Schaffer’s proposed development. Each of these application processes requires independent review with respect to the Planning system which includes SPP 2.3.

The below section aims to address the above mentioned objection further.

Jandakot Airport Master Plan (2014)

With respect to the *“Jandakot road widening”* component of the above mentioned objection, this form of development is separate to the requirements of SPP 2.3 as road widening is considered to be public works. Road upgrades are not a land use planning consideration in this respect and falls beyond the scope of the Scheme Amendment assessment. The road widening benefits the wider community and is not specifically associated with this development. To some extent some road upgrades relate to population growth (as a State) and is therefore potentially an incidental component of a growing population.

The above mentioned objection refers also to *“the Jandakot Airport Holdings (Precincts 5 & 6 clearing and development)”*. In this respect it is important to note the development of airports is undertaken within the regulatory framework of the Airports Act 1996, and the following key legislation and regulations:

- Airports Regulations 1997;
- Airports (Building Control) Regulations 1996;
- Airports (Control of On-Airport Activities) Regulations 1997;
- Airports (Protection of Airspace) Regulations 1996; and
- Airports (Environment Protection) Regulations 1997.

The Airports Act is the principal statute regulating the ownership, management and conduct of federally leased airports. Part 5 of the Act prescribes a number of controls over land use, planning and building at airports and Part 6 details environmental management.

Under Section 70 of the Act, each commonwealth airport is required to produce a final master plan. A final master plan is one which has been approved by the Federal Minister of Infrastructure and Regional Development. Prior to submitting a draft master plan to the Minister, the airport is required to take into account public comments. Subsequent development at the airport must be consistent with the final master plan.

Council item number 14.6 of the 13 November 2014 meeting provided Councils formal submission with respect to the [then] draft 2014 Jandakot Airport Master Plan.

Council's report in 2014 made mention, in terms of land use, the draft 2014 Master Plan indicated some key differences to the 2009 Master Plan. The most significant difference was in the way in which 'Precinct 6 and 6A' were being subject to future development considerations and had shifted to become a "Mixed Business" (37ha) and "Aviation Operation" (10ha) precinct, (and at one point) proposed to be connected to a low scale rural community at the end of Solomon Road north of Jandakot Road.

Council identified two issues with the above, back in 2014, the first being the contemplation of a major road link through what is (and what was planned to remain) a quiet rural community, and the second being the contemplation of this land for a Mixed Business development outcome. The below image provides a visual representation of the evolution of the Federal Government's JAH Master Plan.

Figure 3: Federal Government’s JAH Master Plan progression; 2009, draft 2014 and final 2014.



As mentioned above, the Community has expressed grievances with the recent developments and vegetation clearing by JAH adjacent to the rural lots. The below image provides a current aerial photograph indicating the full extent of the clearing associated with Precinct 6 and 6A.

Figure 4: Aerial Photograph of cleared vegetation within Jandakot Airport, Precincts 6 and 6A.



It is important to note Council objected in part, back in 2014, to the above clearing (within the green area) directly abutting the rural/residential lots (indicated in orange). Council *“formally requested, of Jandakot Airport, the reconfiguration of Precinct 6, 6A and 2A so as*

to achieve an approximate 200m conservation zone separation from rural development and future mixed business development.”

JAH, back in 2014, acknowledged their obligation to *“mitigate the potential impact that any development within Precinct 6 might have on the adjoining rural living properties”*. JAH believes this *“can be adequately addressed by the location, design and layout of individual developments within the precinct and are prepared to undertake such an assessment prior to undertaking each respective development.”*

Council recommended under point 4 of the 13 November 2014 report, *“in order to assess the suitability of any noise attenuation measures incorporated into any development in Precinct 6, Jandakot Airport Holdings shall engage an Acoustic Consultant/Engineer with relevant qualifications and experience, and who is a member of either the Australian Acoustical Society and/or the Association of Australian Acoustical Consultants to undertake an assessment of that development.”*

Pursuant to the above, the Solomon Road access as shown on the DRAFT 2014 master plan was successfully deleted from the FINAL 2014 Master Plan, as per figure 3 above. On this basis the resolution, under point five (5) above, aims *“to amend the proposed AU1 area by deleting the eastern portion proposed for ‘road link’ to Jandakot Airport. This is on the basis of protecting the rural amenity of the adjacent rural/residential lots.”*

On the above basis, whilst it is acknowledged objections were received regarding the changing nature of the locality, the changes within JAH are, unfortunately, beyond the control of Local Government. Likewise the upgrades to Jandakot Road are beyond the scope of land use considerations associated with this Scheme Amendment. It is important to note in this respect the Urban Stone land, as per the above, meets State planning controls as per recent submissions received from the various government agencies.

For those members of the respective Community that are concerned, and seek further clarity, they may wish to/ and may benefit from reviewing Council’s previous report on the submission to the Federal Government. As mentioned above, this was Item No.14.6 of 13 November 2014 Meeting.

WAPC Subdivision Application No.154019 – Lot No. 703 (previously Lot 103) Jandakot Road, Jandakot

In recent history members of the respective Community have noted a ‘roundabout’ is currently under construction abutting the western boundary of the subject land. Members of the Community have made

enquiries in this respect as to whether Scheme Amendment No. 112 has been actioned already. Specifically there is concern that due process may not have been followed.

In relation to the above, it is important for the Community to note the roundabout in question has been facilitated through a WAPC (State government) subdivision. The Subdivision provided the land in question as 4,134m² of 'road widening'. This was approved by the WAPC on 22 September 2016 subject to three conditions as extracted below, which were delegated to the control of the City of Cockburn as indicated by "(Local Government)".

CONDITION(S):

1. Lauanders Street being widened in accordance with the approved plan of subdivision to the satisfaction of the City by the landowner/applicant transferring the land required to the Crown free of cost for the purpose of widening for the construction of a roundabout. (Local Government)
2. Satisfactory arrangements being made with the local government for the full cost of construction of the proposed roundabout. (Local Government)
3. Measures being taken to ensure no vegetation within Bush Forever Site No. 388 is removed or disturbed during subdivisional works, including any secondary impacts from works to provide service infrastructure and drainage to implement the approved plan of subdivision. (Local Government)

The corresponding condition numbers have been illustrated on Figure 5 below for ease of reference. It is important to note Condition number two (2) above specifies the full cost of the roundabout is at the land owner/ applicants cost, and not at the cost of the City of Cockburn. The risk with respect to the roundabout and the need for the roundabout is therefore at the risk of the owner/ applicant. The WAPC subdivision approval as indicated above is on a without prejudice basis and does not in itself indicate a support for Scheme Amendment No. 112, by either the Hon. Minister for Planning, the WAPC or the Elected members of Council at the City of Cockburn.

Figure 5: Roundabout in question currently under construction as a result of WAPC subdivision Application 154 019



As can be seen above, the roundabout is currently under construction and has been paid for by the owner/ applicant pursuant to the above WAPC condition No. 2. The provision of the roundabout at this early stage is not binding with respect to Amendment No. 112.

Conclusion

Proposed Scheme Amendment No. 112 has been forwarded to the EPA, WAPC, respective members of the Community, Government Agencies and Service providers. This report before Council aims to identify and address the Submissions received during the advertising period.

It is understood the Community is concerned with the changing environment with respect to the locality. The above report aims to dissect each component that makes up the planning system in this respect. The above report then aims to express that the proposed Scheme Amendment is considered to be consistent with proper and orderly planning principles.

Please refer to the below section, the 'Community Consultation' section for further details.

Strategic Plan/Policy Implications

City Growth

- Ensure planning facilitates a desirable living environment and meets growth targets.

Moving Around

- Improve connectivity of transport infrastructure.

Economic, Social & Environmental Responsibility

- Create opportunities for community, business and industry to establish and thrive through planning, policy and community development.
- Increase local employment and career opportunities across a range of different employment areas through support for economic development.

Budget/Financial Implications

The required fee was calculated on receipt of the proposed Scheme Amendment and has been paid by the proponent. There are no other direct financial implications associated with the Proposed Scheme Amendment.

Legal Implications

N/A

Community Consultation

Advertising was undertaken pursuant to the advertising requirements prescribed within Regulation 38 of the *Planning and Development (Local Planning Schemes) Regulations 2015*. This was for 'not less than a period of 60 days'.

On this basis, advertising included a notice in the newspaper, a hard copy of the report displayed in the City's offices, displayed in full on the City's website and a copy (letter) provided to each public authority and nearby landowner.

Advertising commenced on 22 November 2016. On the basis the advertising period extended over the Christmas/ Holiday period, which is generally accepted as a 'down period', advertising was extended to provide the community and government agencies/ service providers with a prolonged advertising period as per contextual procedural fairness.

In addition to the above the Banjup Residents Group requested of Council a further advertising extension period. This was in regard to the timing of the State governments overarching strategic document which covers the subject site and the adjacent landholdings; The 'Perth and Peel @ 3.5 Million' documentation (as discussed above).

As a result of the above advertising extension requests, advertising formally concluded on 7 February 2017.

Council received a total of 39 submissions of which 12 submissions were in support of the proposal. Of these submissions in support of the proposal one submission, submission number 35 of the attached table was 'in support subject to modification'. This submission was provided by the WAPC and has generally been incorporated into the revised Scheme Text (above). See Attachment No. 3 for details.

In total Council received 27 objections of which 20 objections were signatories to submission number 10. Submission number 10 was submitted by R & J Kroon of 97 Jandakot Road, Jandakot. This submission provides the following partial extract;

"We fully support the submission made by the Banjup Residents Group that covers our main concerns. Please refer to the BRG submission, as attached, for full justifications. In summary they state that: either all Jandakot rural properties are permitted additional commercial land uses, or none are. We request the Council of the City of Cockburn to ensure equal opportunities are given both to residential ratepayers and commercial developers in the squeeze between Jandakot City and urban Treeby."

The submission from the Banjup Residents Group, as referred to above, is provided for as submission number 8 in the Schedule of Submission response table. This submission provides the following position, as partially extracted;

"Our position is simple: by whatever means either all Jandakot rural properties are permitted commercial land uses or none are. We urge the Council of the City of Cockburn to ensure equality of opportunity between residential ratepayers and commercial developers, as formalised in a clear Structure Plan for the whole area. To realise this, we recommend these resolutions to the Council of the City of Cockburn:

Either -

- *Defer any decisions on Schaffer's application and on alternative land uses for current residential landowners in rural Jandakot until*

after the publication of the WAPC's final South Metropolitan & Peel Planning Framework; then

- *Develop a formal structure plan for the whole of the Jandakot rural area from Berrigan Drive to Warton Road that includes:*
 - *Schaffer's application*
 - *Residents' considerations*

Or, alternatively -

- *Defer a decision on Schaffer's application until the City of Cockburn has developed a formal structure plan for the whole of the Jandakot rural area from Berrigan Drive to Warton Road that includes:*
 - *Schaffer's application*
 - *Residents' considerations"*

The comments of the Banjup Residents Group are discussed above throughout this report.

One of the objections was a two part submission. This objection was provided by Main Roads Western Australia ('MRWA') whom provided an initial objection and later a letter of support. These submissions are provided for as submissions numbers 36 and 37. As per the above, the Schedule of Submissions table records 12 submissions in support and 27 submissions objecting to the proposal. The details of these submissions are summarised above within the 'Report' section.

Risk Management Implications

The officer's recommendation takes into consideration all the relevant planning factors associated with this proposal, including State Planning Policy 2.3 and Perth and Peel @ 3.5 Million.

It is considered that the officer recommendation is appropriate in recognition of making the most appropriate planning decision.

It is recommended that Council adopts the proposed amendment subject to the above mentioned Scheme Map and Scheme Text modifications. This is partially to address the various submissions received during the advertising period. These modifications are required prior to the forwarding of the proposed Scheme Amendment to the WAPC. On this basis the associated risks in not achieving these planning outcomes is considered minimal.

Attachment(s)

1. Location Plan.
2. Current and Proposed Scheme Map.
3. Modified proposed Scheme Text.

4. Noise report extract.
5. Schedule of Submissions

Advice to Proponent(s)/Submissioners

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 8 June 2017 Ordinary Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

16. FINANCE AND CORPORATE SERVICES DIVISION ISSUES

16.1 (OCM 08/06/2017) - LIST OF CREDITORS PAID - APRIL 2017 (076/001) (N MAURICIO) (ATTACH)

RECOMMENDATION

That Council adopt the List of Creditors Paid for April 2017, as attached to the Agenda.

COUNCIL DECISION

Background

It is a requirement of the Local Government (Financial Management) Regulations 1996, that a List of Creditors be compiled each month and provided to Council.

Submission

N/A

Report

The list of accounts for March 2017 is attached to the Agenda for consideration. The list contains details of payments made by the City in relation to goods and services received by the City.

Strategic Plan/Policy Implications

Leading & Listening

- Deliver sustainable governance through transparent and robust policy and processes.
- Listen to and engage with our residents, business community and ratepayers with greater use of social media.

Budget/Financial Implications

N/A

Legal Implications

N/A

Community Consultation

N/A

Risk Management Implications

The list of accounts for April 2017 is attached to the Agenda for consideration. The list contains details of payments made by the City in relation to goods and services received by the City.

Attachment(s)

List of Creditors Paid – April 2017.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

16.2 (OCM 08/06/2017) - STATEMENT OF FINANCIAL ACTIVITY AND ASSOCIATED REPORTS - APRIL 2017 (071/001) (N MAURICIO) (ATTACH)

RECOMMENDATION

That Council

- (1) adopt the Statement of Financial Activity and associated reports for April 2017, as attached to the Agenda; and
- (2) amend the 2016/17 Municipal Budget in accordance with the detailed schedule in the report as follows:

Revenue Adjustments	Increase	2,548,109
Expenditure Adjustments	Increase	1,381,029
TF to Reserve Adjustments	Increase	1,504,102
TF from Reserve Adjustments	Increase	193,000
Net change to Municipal Budget Closing Funds	Decrease	144,022

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COUNCIL DECISION

Background

Regulations 1996 prescribes that a local government is to prepare each month a Statement of Financial Activity.

Regulation 34(2) requires the Statement of Financial Activity to be accompanied by documents containing:–

- (a) *details of the composition of the closing net current assets (less restricted and committed assets);*
- (b) *explanation for each material variance identified between YTD budgets and actuals; and*
- (c) *any other supporting information considered relevant by the local government.*

Regulation 34(4)(a) prescribes that the Statement of Financial Activity and accompanying documents be presented to Council within 2 months after the end of the month to which the statement relates.

The regulations require the information reported in the statement to be shown either by nature and type, statutory program or business unit. The City chooses to report the information according to its organisational business structure, as well as by nature and type.

Local Government (Financial Management) Regulations - Regulation 34 (5) states:

- (5) *Each financial year, a local government is to adopt a percentage or value, calculated in accordance with the AAS, to be used in statements of financial activity for reporting material variances.*

This regulation requires Council to annually set a materiality threshold for the purpose of disclosing budget variances within monthly financial reporting. At its August meeting, Council adopted to continue with a materiality threshold of \$200,000 for the 2016/17 financial year.

Detailed analysis of budget variances is an ongoing exercise, with any required budget amendments submitted to Council each month in this report or included in the City's mid-year budget review as considered appropriate.

Submission

N/A

Report

Opening Funds

The opening funds of \$9.27M representing closing funds brought forward from 2015/16 have been audited and the budget amended to reflect this final position.

Closing Funds

The City's closing funds position of \$36.50M was \$7.66M higher than the YTD budget forecast. This result reflects net favourable cash flow variances across the operating and capital programs as detailed in this report.

The 2016/17 revised budget reflects an EOFY closing position of \$0.17M, down \$0.14M from last month as a result of budget amendments processed during the month.

Operating Revenue

Consolidated operating revenue of \$128.62M was ahead of the YTD budget target by \$1.05M.

The following table shows the operating revenue budget performance by nature and type:

Nature or Type Classification	Actual Revenue \$M	Revised Budget YTD \$M	Variance to Budget \$M	FY Revised Budget \$M
Rates	94.27	93.36	(0.92)	95.70
Specified Area Rates	0.31	0.33	0.02	0.33
Fees & Charges	18.62	19.59	0.97	23.27
Service Charges	0.44	0.45	0.01	0.45
Operating Grants & Subsidies	9.62	9.24	(0.38)	11.11
Contributions, Donations, Reimbursements	0.87	0.56	(0.31)	0.77
Interest Earnings	4.48	4.04	(0.44)	4.87
Total	128.62	127.57	(1.05)	136.51

The significant variances at month end were:

- Rates – Part year rating was \$0.96M ahead of the YTD budget setting.
- Fees & Charges –
 - Leisure Centre fee income was \$0.74M behind YTD budget.
 - Development application fees were \$0.23M behind YTD budget.
 - Commercial leasing income was \$0.20M behind YTD budget.
- Operating Grants & Contributions – Child care fee subsidies were \$0.32M ahead of the YTD budget.
- Interest Earnings – Investment earnings from the City's financial investments were \$0.42M ahead of the YTD budget.

Operating Expenditure

Reported operating expenditure (including asset depreciation) of \$105.56M was under the YTD budget by \$3.09M.

The following table shows the operating expenditure budget variance at the nature and type level. The internal recharging credits reflect the amount of internal costs capitalised against the City's assets:

Nature or Type Classification	Actual Expenses \$M	Revised Budget YTD \$M	Variance to Budget \$M	FY Revised Budget \$M
Employee Costs - Direct	40.00	39.83	(0.18)	49.78
Employee Costs - Indirect	0.64	0.80	0.15	1.41
Materials and Contracts	31.00	33.26	2.26	40.79

Nature or Type Classification	Actual Expenses \$M	Revised Budget YTD \$M	Variance to Budget \$M	FY Revised Budget \$M
Utilities	3.83	3.82	(0.01)	4.70
Interest Expenses	0.48	0.48	(0.00)	0.93
Insurances	2.32	2.43	0.11	2.43
Other Expenses	6.45	6.82	0.37	8.48
Depreciation (non-cash)	21.97	22.29	0.32	26.83
Amortisation (non-cash)	0.91	0.99	0.09	1.19
Internal Recharging-CAPEX	(2.04)	(2.06)	(0.03)	(2.59)
Total	105.56	108.65	3.09	133.95

The significant variances at month end were:

- Material and Contracts - were \$2.26M under the YTD budget with the significant variances being:
 - IT & IS projects under by \$0.49M
 - Ranger & Community Safety projects collectively under by \$0.25M
 - Waste Disposal costs under by \$0.28M,
 - Council promotion projects under by \$0.20M
 - Child care subsidy payments over by \$0.50M.
 - Parks maintenance over by \$0.35M.

Capital Expenditure

The City's total capital spend at the end of the month was \$75.5M, representing an under-spend of \$17.3M against the YTD budget.

The following table details the budget variance by asset class:

Asset Class	YTD Actuals \$M	YTD Budget \$M	YTD Variance \$M	FY Revised Budget \$M	Commit Orders \$M
Roads Infrastructure	12.2	16.9	4.7	17.9	3.1
Drainage	0.5	1.1	0.7	1.6	0.1
Footpaths	0.7	1.0	0.3	1.1	0.1
Parks Infrastructure	6.3	8.8	2.5	10.7	2.1
Landfill Infrastructure	0.2	0.5	0.4	1.2	0.1
Freehold Land	0.7	1.5	0.9	1.9	0.0
Buildings	48.4	52.4	4.0	55.3	4.5
Furniture & Equipment	0.9	1.8	0.9	3.0	0.6
Information Technology	0.5	1.0	0.4	1.9	0.6
Plant & Machinery	5.2	7.9	2.7	8.3	2.9
Total	75.5	92.8	17.3	103.1	14.0

These results included the following significant project variances:

- Roads Infrastructure under YTD budget by \$4.7M – including Berrigan Drive Jandakot Improvement Works (\$2.1M), Lyon & Gibbs Signalisation and Upgrade (\$0.7M), Mayor Rd [Rockingham to Fawcett] (\$0.5M), Gibbs & Liddelow Roundabout (\$0.4M) and Beeliar Drive [Spearwood to Stock] (\$0.2M).
- Parks Infrastructure – the capital program was behind the YTD budget by \$2.5M with CY O'Connor Improvements (\$0.3M), Coogee Beach master plan (\$0.5M), Dixon Reserve works (\$0.2M) and Jarvis Park landscaping (\$0.3M) the significant contributing projects.
- Freehold Land – various land acquisition & development projects were collectively \$0.9M behind the YTD budget with lot 915 Goldsmith (\$0.36M) the main contributor.
- Buildings – collectively \$4.0M behind YTD budget with Cockburn ARC (\$2.7M), Community Men's Shed (\$0.44M) and Bibra Lake sewer connection (\$0.5M) the significant variances. The new depot was ahead of YTD budget (\$0.45M) but within overall budget.
- Furniture & Equipment – was \$0.86M behind YTD budget, consisting of the fitout of the Cockburn ARC.
- Information Technology – was collectively \$0.41M under YTD budget due to a number of under spent software and website projects.
- Plant & Machinery – replacement program was behind YTD budget by \$2.7M, with most items representing this variance being on order and awaiting delivery.

Capital Funding

Capital funding sources are highly correlated to capital spending, the sale of assets and the rate of development within the City (developer contributions received).

Significant variances for the month included:

- Capital grants were \$2.34M behind YTD budget mainly due to timing issues for Cockburn ARC state and federal grants (\$1.6M), various road grants (\$0.40M) and Lotterywest funding for the men's shed (\$0.48M)

- Development contributions for the Cockburn ARC project (\$3.2M) and Jandakot Rd Improvement project (\$1.0M) were outstanding due to timing.
- Developer Contribution Area (DCA) contributions for road and community assets were collectively behind YTD budget by \$0.86M.
- Transfers from financial reserves were \$6.2M behind the cash flow budget due to the capital program under spending for buildings, parks, plant and roads (timing issue).
- Proceeds from the sale of assets were \$2.22M behind the YTD budget comprising of land (\$1.67M) and plant (\$0.55M).

Transfers to Reserve

Transfers to financial reserves were \$1.98M behind the YTD budget mainly due to unrealised land sales of \$1.67M.

Cash & Investments

The closing cash and financial investment holding at month's end totalled \$138.72M (down from \$147.62M last month).

\$97.50M of this balance represents the current amount held for the City's cash/investment backed financial reserves. The balance of \$41.22M is available to meet operational liquidity needs (down from \$49.49M last month).

Investment Performance, Ratings and Maturity

The City's investment portfolio made a weighted annualised return of 2.73% for the month, decreased from 2.75% last month and from 2.80% the month before. However, this still compares quite favourably against the UBS Bank Bill Index (2.03%) and has been achieved through careful management of the City's cash flow requirements. The cash rate was most recently reduced 25bp to 1.50% at the August 2016 meeting of the Reserve Bank of Australia and this reduction has since impacted the investment rates achieved for new deposits.

However, the City's interest revenue from investments to month's end was ahead of the YTD budget target by \$0.42M. This was primarily due to the retention of a large investment pool, as capital outflows have been somewhat delayed. Also assisting this result was a conservative budget setting anticipating more rate cuts.

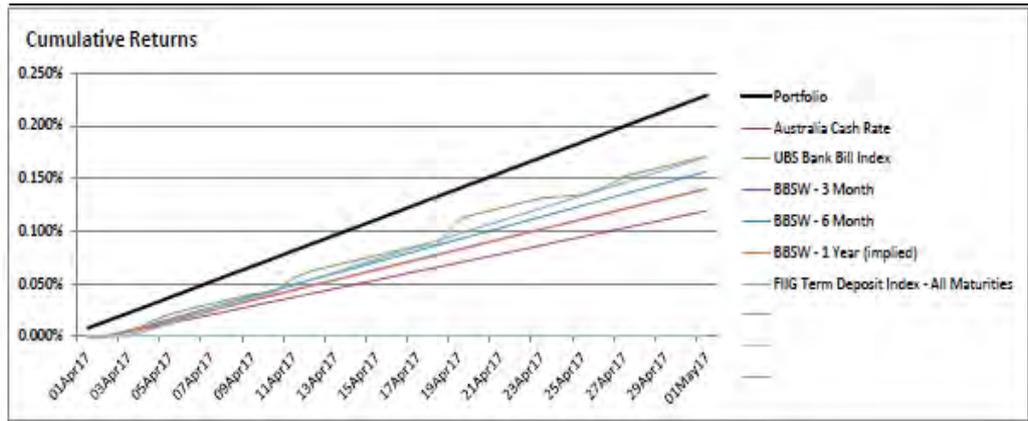


Figure 1: COC Portfolio Returns vs. Benchmarks

The majority of investments were held in term deposit (TD) products placed with highly rated APRA (Australian Prudential Regulation Authority) regulated Australian and foreign owned banks. These were invested for terms ranging from three to twelve months. All investments comply with the Council’s Investment Policy other than those made under previous statutory provisions and grandfathered by the new ones.

The City’s TD investments fall within the following Standard and Poor’s short term risk rating categories. The A-1+ investment holding decreased marginally from 33% to 31% during the month, whilst the A-1 holding increased from 13% to 14%. The amount invested with A-2 banks also increased to 51% (from 50%), comfortably below the policy limit of 60%:

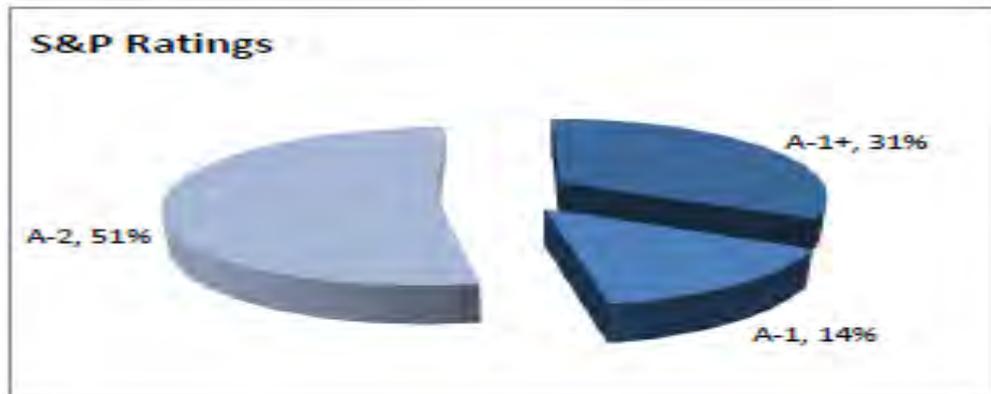


Figure 2: Council Investment Ratings Mix

The current investment strategy seeks to secure the highest possible rate on offer (up to 12 months for term deposits), subject to cash flow planning and investment policy requirements. Value is currently being provided within the 3-12 month investment range.

The City's TD investment portfolio currently has an average duration of 138 days or 4.5 months (reduced from 153 days last month) with the maturity profile graphically depicted below:

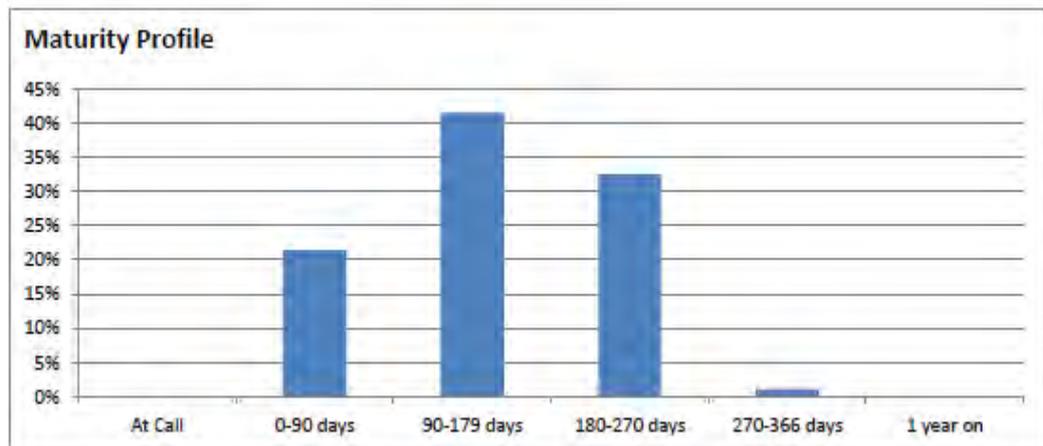


Figure 3: Council Investment Maturity Profile

Investment in Fossil Fuel Free Banks

At month end, the City held 56% (\$75.66M) of its TD investment portfolio of \$135.26M with banks deemed as free from funding fossil fuel related industries. This was slightly up from 55% the previous month.

Budget Revisions

Budget amendments identified during the month and requiring Council adoption are as per the following schedule:

PROJECT/ACTIVITY LIST	USE OF FUNDING +/-		FUNDING SOURCES (+)/(-)		
	EXP	TF to RESERVE	TF FROM RESERVE	REVENUE	MUNI
	\$	\$	\$	\$	\$
Increase funding for fitout at ARC for Curtin University	193,000		(193,000)		
Fence replacement for Friendship Way project (OCM 9/3/17)	80,000				(80,000)
ARC Opening – increase budget	47,932			(7,980)	(39,952)
New gifted DFES vehicle	548,243			(548,243)	
Berrigan Drive Jandakot Improvement Works – balancing and funding adjustments	435,154	1,504,102		(1,939,256)	
ARC - Discover Community Event	76,700			(52,630)	(24,070)

PROJECT/ACTIVITY LIST	USE OF FUNDING + / (-)		FUNDING SOURCES (+) / (-)		
	EXP	TF to RESERVE	TF FROM RESERVE	REVENUE	MUNI
	\$	\$	\$	\$	\$
Totals	1,381,029	1,504,102	(193,000)	(2,548,109)	(144,022)

Description of Graphs & Charts

There is a bar graph tracking Business Unit operating expenditure against budget. This provides a quick view of how the different units are tracking and the comparative size of their budgets.

The Capital Expenditure graph tracks the YTD capital spends against the budget. It also includes an additional trend line for the total of YTD actual expenditure and committed orders. This gives a better indication of how the capital budget is being exhausted, rather than just purely actual cost alone.

A liquidity graph shows the level of Council's net current position (adjusted for restricted assets) and trends this against previous years. This gives a good indication of Council's capacity to meet its financial commitments over the course of the year. Council's overall cash and investments position is provided in a line graph with a comparison against the YTD budget and the previous year's position at the same time.

Pie charts included show the break-up of actual operating income and expenditure by nature and type and the make-up of Council's current assets and liabilities (comprising the net current position).

Trust Fund

At month end, the City held \$10.82M within its trust fund. \$5.85M was related to POS cash in lieu and another \$4.97M in various cash bonds and refundable deposits.

A summary of the POS cash in lieu held follows:

<u>Suburb</u>	<u>\$</u>
Aubin Grove	845,930
Atwell	172,320
Beeliar	2,259,820
Cockburn Central	161,832
Coolbellup	167,369
Coogee	378,850
Hamilton Hill	565,254
Hammond Park	29,936

Jandakot	258,119
Bibra Lake (East)	124,374
Munster	604,164
South Lake	56,023
Yangebup	221,286
Total	5,845,276

Strategic Plan/Policy Implications

Leading & Listening

- Deliver sustainable governance through transparent and robust policy and processes.
- Ensure sound long term financial management and deliver value for money.

Budget/Financial Implications

The 2016/17 budget surplus reduced from \$313,158 last month to \$169,136 due to the \$144,022 net adjustment included in this report.

Legal Implications

N/A

Community Consultation

N/A

Risk Management Implications

Council's budget for revenue, expenditure and closing financial position will be misrepresented if the recommendation amending the City's budget is not adopted.

Attachment(s)

Statement of Financial Activity and associated reports – April 2017.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

17. ENGINEERING AND WORKS DIVISION ISSUES

17.1 (OCM 08/06/2017) - TENDER NO. RFP 11/2017 - GREENWASTE DECONTAMINATION PLANT - DESIGN, FABRICATION, INSTALLATION, TESTING AND COMMISSIONING. (RFP 11/2017) (L DAVIESON)

RECOMMENDATION

That Council accept the tender submitted by EMER Pty Ltd , T/A Focus Enviro for Tender No.RFP 11/2017 – Greenwaste Decontamination Plant for the total lump-sum of \$689,105.46 (GST-exclusive).

COUNCIL DECISION

Background

Over the next four (4) years, the City will roll out a 240 litre garden waste bin to all properties greater than 400m². This bin will be serviced fortnightly and as the program proceeds, greater tonnages will require decontamination.

To assist in this endeavour, the City requires the design, fabrication, installation, testing and commissioning of a greenwaste picking station/decontamination plant for its Henderson Waste Recovery Park (HWRP) located at 920 Rockingham Road, Wattleup.

The greenwaste picking station will be designed to allow the HWRP's excavator to place the garden waste unloaded from the waste collection compaction truck directly into the hopper. The Principal's staff will remove contaminants from the greenwaste as it passes through the plant and the decontaminated product will be discharged to a stockpile. The plant will have an average throughput capacity of 20 to 25 tonnes of greenwaste per hour.

The proposed scope of works/services includes; concept / design development, foundations, plant fabrication, installation, testing, training, commissioning and other associated works. The City will be responsible for the all approvals and the preparation of the site.

The City's scope embodies best practice sustainability principles throughout, in particular for energy and water efficiency; and potentially powered by renewable energy.

It is expected that the proposed Contract shall be awarded in June 2017 with work commencing immediately upon appointment so that the plant is installed and commissioned by 15 December 2017.

Tender No. RFP 11/2017 – Greenwaste Decontamination Plant was advertised on Saturday 25 March 2017 in the Local Government Tenders section of “The West Australian newspaper. The RFP was also displayed on the City’s E-Tendering website between 25 March and 11 April 2017.

Submission

The Request for Proposal closed at 2:00PM (AWST) Tuesday, 11 April 2017. Six (6) submissions were received from the following companies:

Business Name	Respondent’s Name
Australian Bale Press Company Pty Ltd	Australian Bale Press
Alawite Pty Ltd	Australian Project Management
Focus Enviro	EMER
Dieselcraft	Pipecraft
RDT Engineering Pty Ltd	RDT
Wastech Engineering Pty Ltd	Wastech

Report

Compliance Criteria

The following criteria were used to determine whether the submissions received were compliant:

	Description of Compliance Criteria
A	Compliance with the Conditions of Responding (Part 1).
B	Compliance with the Brief (Part 2) contained in the Request.
C	Completion of Section 3.1 . – Form of Response
D	Compliance Section 3.2 . – Respondent’s Contact Person
E	Compliance with Sub-Contractors requirements and completion of Section 3.5.3 .
F	Compliance with Financial Position requirements and completion of Section 3.5.5 .
G	Compliance with Insurance requirements and completion of Section 3.5.6 .

Description of Compliance Criteria	
H	Compliance with Qualitative Criteria and completion of Section 3.6.2.
I	Compliance with Fixed Price and completion of Section 3.7.2.
J	Compliance with and completion of the Price Schedule (including the breakdown of Lump Sum) in the format provided in Part 4.
K	Compliance with ACCC Requirements and completion of Appendix A.
L	Acknowledgement of any Addenda issued.

Compliant Tenderers

All six (6) submissions were deemed compliant and were evaluated.

Evaluation Criteria

Evaluation Criteria	Weighting Percentage
Demonstrated Experience	15%
Respondent's Resources	10%
Design Requirements, Compliance and Features	25%
Methodology	5%
Sustainability	5%
Tendered Price	40%
Total	100%

Tender Intent/Requirements

The City is seeking the services of a suitably experienced Contractor for the design, fabrication, installation, testing and commissioning of a greenwaste picking station / decontamination plant for its Henderson Waste Recovery Park (HWRP).

The proposed scope of works/services includes:

- a) Concept / preliminary design,
- b) Design development and documentation;
- c) Site foundations; and
- d) Plant fabrication, installation, testing, training, commissioning and other associated works.

The City will be responsible for the following works/services:

- a) Obtaining any necessary planning or building approvals;
- b) DER design compliance/works approval;
- c) Underground services location;
- d) Site preparation and earthworks; and
- e) Electrical connections.

Evaluation Panel

The tender submissions were evaluated by:

1. Lyall Davieson (Chair) – Waste Manager
2. Michael Haynes – Recovery Park Coordinator
3. Margot Tobin (SBMG Rep) – Executive Manager Strategy & Civic Support

Probity: Gary Ridgway, Contracts Specialist and Caron Peasant, Contracts Officer – Procurement Services

Scoring Table - Combined Totals

Respondent's Name	Percentage Score		
	Non-Cost Evaluation	Cost Evaluation	Total
	60%	40%	100%
EMER Pty Ltd**	43.78%	20.28%	64.06%
Wastech Engineering Pty Ltd	33.57%	29.36%	62.93%
RDT Engineering Pty Ltd	35.35%	23.10%	58.45%
Pipecraft Pty Ltd	16.63%	40.00%	56.63%
Alawite Pty Ltd	15.20%	38.61%	53.81%
Australian Bale Press Company Pty Ltd	30.10%	15.32%	45.42%

**** Recommended Submission**

Evaluation Criteria Assessment

Demonstrated Experience

Of the Six tenderers, three have never built a greenwaste decontamination plant, two (Wastech and RDT) had constructed plants in Australia that were yet to be commissioned at the time of the tender and only one (EMER) demonstrated significant experience in this type

of plant. EMER has built, operated and supplied plants in Victoria, Birmingham (UK) and general waste stream plants locally. As a result, EMER was able to describe in detail the problems that arose during the commissioning and operation of the plant. EMER scored well above the others in this criterion. Australian Bale Press, Wastech and RDT all demonstrated a track record in the construction of Material Recovery Facility for the sorting of comingled recyclables.

Respondent's Resources

Wastech and Australian Bale Press demonstrated the most sustained company history and support. RDT also scored well in this criterion. Whilst EMER recently formed in 2015, they indicated an install base of 150 units in Australia and provided a detailed response to address this criterion. Pipecraft and Australian Project Management did not demonstrate the same level resources in their response to backup services, training and spare part supply.

Design Requirements, Compliance and Features

All Tenderers scored well in this section in understanding the City's minimum design requirements. The Panel paid particular attention to the responses relating to how their plant would prevent glass breakage. As broken glass in greenwaste renders the product unusable in compost production, a detailed response was required. EMER provided the most conclusive response on this criterion.

Methodology

The Panel was seeking an understanding of how the greenwaste would be delivered and presented for the pickers to ensure contaminants could be easily identified on the moving belt. EMER scored highest in this criteria followed closely by RDT and Wastech.

EMER recommended the removal of fines (lawn clippings) by introducing a trommel unit in front of the picking station. This approach will reduce the volume of waste to be decontaminated but also provide the pickers greater visibility and ease to remove contaminants.

Sustainability

Most Tenderers scored poorly in this section with the exception of RDT followed by Focus and Wastech. This is mostly due to their processes and plants they manufactured. Whilst their plants deliver sustainable environmental outcomes few tenderers were able to demonstrate a record of social enterprise, community benefit or sustainability awards.

Summation

Whilst RDT and Wastech could deliver a greenwaste decontamination plant meeting the City's specific guidelines, EMER provided a superior submission with demonstrated ability to manage and handle greenwaste. Referees were consulted and the information gathered confirmed EMER was experienced in delivering this type of project on time and on budget. EMER may be used for the purchase of spare part if they are not available locally. EMER provided the best score against all the selection criteria. As a consequence, EMER submission represents the best value for the City should be supported.

Strategic Plan/Policy Implications

Economic, Social & Environmental Responsibility

- Sustainably manage our environment by protecting, managing and enhancing our unique natural resources and minimising risks to human health.
- Improve water efficiency, energy efficiency and waste management within the City's buildings and facilities and more broadly in our community.
- Further develop adaptation actions including planning; infrastructure and ecological management to reduce the adverse outcomes arising from climate change.

Leading & Listening

- Deliver sustainable governance through transparent and robust policy and processes.

Budget/Financial Implications

In the Implementation Plan for the rollout of garden waste bins presented to Council in May 2016, a picking station to remove contamination from the greenwaste was identified as an essential tool to ensure a valuable end product.

In the Mid-Year Budget Review of January 2017, \$700,000 was set aside for the purchase of a greenwaste decontamination plant. The recommendation identifies a lump sum total of \$ 689,105.46. All costs associated with this Tender will be funded from CW 1982.

Due to the long lead time to manufacture the plant (6 months), these funds will be carried forward into the 2017/18 FY.

An independent financial risk assessment is currently being undertaken and will be available for the Council meeting.

Legal Implications

Section 3.57 of the Local Government Act 1995 and Part 4 of the Local Government (Functions and General) Regulations 1996 refers.

Community Consultation

N/A

Risk Management Implications

The risk of Council not awarding the tender is that the decontamination of the future green waste collected by the third bin would still be carried out by hand which becomes very inefficient for the tonnages involved. The cost would also be prohibitive for the number of staff required as well as the workplace health and safety risks of manual picking. From a practical perspective, hand picking would never be able to remove all the contamination that mechanical separation achieves via the trammel and hence would result in a lower quality of decontaminated green waste for processing into compost.

Attachment(s)

The following attachments are provided under confidential cover:

1. Compliance Evaluation;
2. Consolidated Evaluation Panel Score Sheet; and
3. Tendered Prices.

Advice to Proponent(s)/Submissioners

Those who lodged a tender submission have been advised that this matter is to be considered at the 8 June 2017 Ordinary Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

18. COMMUNITY SERVICES DIVISION ISSUES

18.1 (OCM 08/06/2017) - WETLANDS TO WAVES EVENT CONCEPT DEVELOPMENT (152/010) (M LA FRENAIS) (ATTACH)

RECOMMENDATION

That Council

- (1) not proceed with a 'Wetlands to Waves' Adventure Challenge

event at this time; and

- (2) reconsider the concept as a potential inclusion in the Calendar of Events for 2018/19.

COUNCIL DECISION

Background

Project 3 was appointed to undertake a review of the City's event calendar. This included making suggestions for improvement to event delivery planning and internal procedures as well as proposing a number of new event concepts and opportunities.

Council adopted the annual events program at its June 2016 Meeting. This included a budget to develop a detailed scope for a 'Wetlands to Waves' Adventure Challenge event. The concept would utilise Cockburn's natural environment and provide a unique and engaging event for the Cockburn community while also showcasing the Cockburn lifestyle.

The resolution was:

that the City commissions an external report on how it could hold an adventure race in the future, with said report being presented to Council by July 2017.

Submission

N/A

Report

Through a consultancy brief process, Sports Performance was appointed to develop a report on how the City of Cockburn could hold an adventure challenge event. Their findings provided the following recommendations/observations:

1. The City of Cockburn contains areas that can include the following disciplines in an event:
 - Swim and Paddle - With some 6.5 kilometres of beach stretching from Rollinson Road (North Coogee) to Woodman

Point (Jervoise Bay Sailing Club) there is adequate safe areas in which to conduct these sections.

- Run – Run sections involving both path and off road areas are available and can be organised in a safe environment with minimal road traffic management or can incorporate areas that will require significant traffic management or procedures.
- Cycle – This section provides the greatest challenge and could include both on road and off road sections. There are limitations in off road areas depending on the final distance of the event. A combination of on road and off road would enable a longer course to be designed. This section will also involve traffic management and potentially increased signage. A further alternative is a complete on road course.

2. An adventure race involves activities in a natural setting using a variety of disciplines over a course of 1 to 8+ hour duration. This proposal does not recommend an event of longer than 4 hours duration. The event would include two distinct distances / times (to cater for the general public as well as professional athletes) operating concurrently and involving more than the normal numbers in a team structure in addition to individuals. A team generally comprises of 2 to 4, however, for this event it could be expanded to 6 to 8 persons, making it a unique event to include a wider range of entrants.
3. The event could be geared to be cost neutral as it is not considered uncommon for entrants to be charged \$120 per person for this type of event.

Cost

A definitive budget can only be set when the event and its requirements are finalised. Major items are listed below with potential costs and variations. If the City did not gain sponsorship and could provide a subsidy the event could still proceed. Costs are based on similar high profile events of this nature held in Australia:

Income:

- | | |
|--|----------|
| • Sponsorship – includes cash and offsets | \$20,000 |
| • Entry Fees – based on 1000 competitors of 200 individuals (\$120 each) and 200 Teams of four (\$200 per team). | \$64,000 |
| • Contribution by the City of Cockburn | \$32,000 |

TOTAL	\$116,000
--------------	------------------

Expenditure:

- | | |
|--|-----------|
| • Event Director and Event Operations Crew | \$50,000+ |
|--|-----------|

(Event management and Sponsorship Management - Race director and 4 section assistant race directors)	
• Medical – St John Ambulance	\$10,000
• Water Safety – SLSC or similar	\$5,000
• Traffic Management – including plans	\$15,000
• Equipment – including bike racks, marquees	\$5,000
• Event specific signage	\$3,000
• Public address and commentator	\$3,000
• Event timing	\$10,000
• Marketing/PR	\$10,000
• Competitor race bibs, swim caps, bike numbers	\$5,000
• Community consultation	\$2,000
• Contingencies	\$6,000
TOTAL	\$116,000

Sponsorship/ Stakeholder Feedback

A number of potential key partners were approached for an initial opinion on the likelihood of sponsoring with indicative amounts. Only the following feedback was received:

- Department of Sporting and Recreation don't normally support community events. Exceptions to this rule would be if the event were to fit in with their trails project, which is not the case on this occasion.

Conclusion

Having considered the Sports Performance report, the City does not recommend hosting the "Wetlands to Waves" Adventure event at this stage for the following reasons:

- Budget restrictions for 2017/18 and current lack of other resources.
- There would be minimal economic benefit.
- A more detailed concept could be considered in the future.

Strategic Plan/Policy Implications

Community, Lifestyle & Security

- Provide residents with a range of high quality, accessible programs and services.

Budget/Financial Implications

The cost of undertaking an adventure race event is estimated at \$116,000. (Costs minus entry fee income; sponsorship income is not definite).

Legal Implications

The City would need to draw up a legal agreement in regard to the sponsorship and outsourcing of the event management.

Community Consultation

Some local consultation was undertaken onsite during trail investigations, with further advice required to be issued should the event proceed.

Risk Management Implications

Should the Council resolve to undertake the event when it is not properly resourced to do so, providing a subpar experience, there is a "Moderate" level of Reputational Risk associated with this decision.

Attachment(s)

Sports Performance Wetlands to Waves Proposal.

Advice to Proponent(s)/Submissioners

The Proponent has been advised that this matter is to be considered at the 8 June 2017 Ordinary Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

19. EXECUTIVE DIVISION ISSUES

Nil

20. MOTIONS OF WHICH PREVIOUS NOTICE HAS BEEN GIVEN**20.1 (OCM 08/06/2017) - COMMUNITY POLL - ROE HIGHWAY STAGE 8 (006/002); 007/008) (D GREEN) (ATTACH)****RECOMMENDATION**

That Council

- (1) not undertake a community poll of City of Cockburn residents on the construction of the Roe Highway Stage 8 at this stage, and
- (2) monitor the progress of the rehabilitation program for the cleared alignment being undertaken by the State Government, to assess the potential to engage with the community on related issues, as appropriate.

COUNCIL DECISION**Background**

By Notice received on 11 May 2017, Councillor Portelli has submitted the following Motion for Council consideration, which is to be included in the Agenda for the Ordinary Council Meeting to be held on 8 June 2017:

That a community poll on Roe 8 – 3,000 with 1,000 from each Ward and spread evenly over each Ward:

- (1) *undertake the Poll in relation to ratepayers position on Roe 8;*
- (2) *approve an amount of \$14,400 +GST to conduct the Poll as outlined above; and*
- (3) *submit results of Poll to Council as soon as practical for their consideration and action.*

Submission

N/A

Report

The City of Cockburn has consistently stated its opposition to the construction of Roe Highway Stage 8, supported by resolutions of Council over a period of more than 17 years.

As recently as November 2016, Council also resolved to decline the opportunity to canvass the opinion of its community on the matter. One of the reasons provided for supporting this position was that such a poll was “moot”, given that the State Government at the time had awarded Contracts to commence the construction of the project prior to the March 2018 general election and that Council funds and administrative time should not be “wasted” on such a futile exercise.

Since that time, the State Government of the day has lost office and the incoming Government has halted the Project and announced it will not be proceeding with its construction. In addition, it has established a Working Group to immediately oversee a rehabilitation program of the alignment land, from Kwinana Freeway to Stock Road, which was cleared in the weeks immediately prior to the election.

The City of Cockburn is represented on this Working Group as a key stakeholder and has appointed Mayor Howlett as its elected member delegate to the Group, with additional technical and advisory support to be provided by a Senior Environmental Officer of the City. The composition of the Working Group is multi – disciplinary and also comprises the construction Contractor, Building Roe 8, to assist with the revegetation and other improvements proposed for the site.

The Group`s Terms of Reference are extensive and includes objectives to ensure the refurbishment examines the potential for extensive public spaces, including footpaths, cycle ways, trails and other landscaping attractions to be included. The Group has already commenced the planning phase of the project and regular meetings are scheduled to ensure a timely commencement of the revegetation program, to ensure that benefits of the seasonal growth timeframe can be maximised.

Accordingly, it is apparent that the current State Government is totally committed to a regime of high profile and timely environmental improvements to the alignment and that it will not in any way consider a continuation of the previous construction program, with the exception of the southern connection to the Murdoch Activity Centre, including the Health and Knowledge Precinct, at the extreme eastern end of the Roe 8 alignment. This connection is considered necessary infrastructure to support the considerable public and private investment currently being undertaken and being complemented in the short term future.

Against all these mitigating factors, it seems incongruous for Council to contemplate further investigation of this matter, for which no foreseeable outcome is likely to be supported, even if a community poll was conducted and resulted in the majority of respondents favouring the construction of Roe 8.

In the highly unlikely event that the current situation is reversed in the interim period before the next State election in 2021, it is recommended that Council maintain a “watching brief” on the redevelopment program for the alignment, to ensure that the community’s interests are protected and preserved consistent with Council’s aspirations.

Strategic Plan/Policy Implications

Moving Around

- Identify gaps and take action toward extending the coverage of the cycle way, footpath and trails network.
- Continue advocacy for a better solution to regional freight movement.

Community, Lifestyle & Security

- Provide for community facilities and infrastructure in a planned and sustainable manner.

Economic, Social & Environmental Responsibility

- Improve the appearance of streetscapes, especially with trees suitable for shade.

Leading & Listening

- Deliver sustainable governance through transparent and robust policy and processes.

Budget/Financial Implications

Funding of \$14, 400 to undertake the survey has not been provided for in the 2016/17 budget and therefore Council would need to include this amount in the 2017/18 budget, if it was to proceed.

Legal Implications

N/A

Community Consultation

N/A

Risk Management Implications

There is a "Moderate" level of reputational risk to the City in determining an outcome (either for or against) on this matter.

Attachment(s)

Notice of Motion as forwarded by Councillor Steve Portelli.

Advice to Proponent(s)/Submissioners

Councillor Portelli has been advised that this matter is to be considered at the 8 June 2017 Ordinary Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

21. NOTICES OF MOTION GIVEN AT THE MEETING FOR CONSIDERATION AT NEXT MEETING

22. NEW BUSINESS OF AN URGENT NATURE INTRODUCED BY MEMBERS OR OFFICERS

23 (OCM 08/06/2017) - MATTERS TO BE NOTED FOR INVESTIGATION, WITHOUT DEBATE

Clr Lee-Ann Smith has requested in writing the following be noted for investigation:

1. A report be prepared for an investigation into traffic movements and congestion on Lyon Road from the intersection with Gibbs Road to the intersection with Alliance Entrance.
2. A report exploring options for Cockburn residents to submit e-petitions. The report to include the changes needed to the City's Standing Orders Local Law as well as the model used by the City of Brisbane.

24. CONFIDENTIAL BUSINESS

25 (OCM 08/06/2017) - RESOLUTION OF COMPLIANCE**RECOMMENDATION**

That Council is satisfied that resolutions carried at this Meeting and applicable to items concerning Council provided services and facilities, are:-

- (1) integrated and co-ordinated, so far as practicable, with any provided by the Commonwealth, the State or any public body;
- (2) not duplicated, to an extent Council considers inappropriate, services or facilities as provided by the Commonwealth, the State or any other body or person, whether public or private; and
- (3) managed efficiently and effectively.

COUNCIL DECISION**26. CLOSURE OF MEETING**

Don Green

From: Don Green
Sent: Thursday, 1 June 2017 1:52 PM
To: Don Green
Subject: RE: Maternity leave letter for Centrelink

From: Cr Dr Chamonix Terblanche
Sent: Thursday, 1 June 2017 12:17 PM
To: Don Green
Subject: Re: Maternity leave

LOA will be from 28/6 until 18/10 (16weeks).

Thanks again

Chamonix

Sent from my iPhone

On 1 Jun 2017, at 11:57 am, Don Green <don@cockburn.wa.gov.au> wrote:

Hi Chamonix,

If you could please advise the dates to be covered by your maternity leave, I will add an item to the Agenda for Council to formally grant you leave of absence.

Thanks,

<image001.png>

Don Green

Director, Governance & Community Services

9 Coleville Crescent, Spearwood WA 6163

PO Box 1215, Bibra Lake DC WA 6965

P 08 9411 3444

don@cockburn.wa.gov.au

cockburn.wa.gov.au

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CITY OF COCKBURN



DELEGATED AUTHORITIES, POLICIES & POSITION STATEMENTS COMMITTEE

MINUTES

FOR

THURSDAY, 18 MAY 2017

These Minutes are subject to Confirmation

Presiding Member's Signature

Date: 24 August 2017

CITY OF COCKBURN

SUMMARY OF MINUTES OF THE DELEGATED AUTHORITIES, POLICIES & POSITION STATEMENTS COMMITTEE MEETING HELD ON THURSDAY, 18 MAY 2017 AT 6:00

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CITY OF COCKBURN

MINUTES OF THE DELEGATED AUTHORITIES, POLICIES AND POSITION STATEMENTS COMMITTEE MEETING HELD ON THURSDAY, 18 MAY 2017 AT 6:00

PRESENT:

Mrs C Reeve-Fowkes	-	Deputy Mayor (Presiding Member)
Mr L. Howlett	-	Mayor
Mrs L. Sweetman	-	Councillor
Mr S. Portelli	-	Councillor
Mr B. Houwen	-	Councillor

IN ATTENDANCE:

Mr S. Cain	-	Chief Executive Officer
Mr D. Green	-	Director, Governance & Community Services
Mr S. Downing	-	Director, Finance & Corporate Services
Mr D. Arndt	-	Director, Planning & Development Services
Mr C. Sullivan	-	Director, Engineering & Works
Ms M. Tobin	-	Executive Manager, Strategy & Civic Support
Mr J Ngoroyemoto	-	Governance & Risk Co-ordinator
Mrs B. Pinto	-	Executive Assistant to Directors – Fin. & Corp. Services/Gov. & Comm. Services

1. DECLARATION OF MEETING

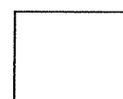
The Presiding Member declared the meeting open at 6.00 pm.

2. APPOINTMENT OF PRESIDING MEMBER (If required)

Nil.

3. ACKNOWLEDGEMENT OF RECEIPT OF WRITTEN DECLARATIONS OF FINANCIAL INTERESTS AND CONFLICT OF INTEREST (BY PRESIDING MEMBER)

Nil



4 (DAPPS 18/05/2017) - APOLOGIES & LEAVE OF ABSENCE

Clr Stephen Pratt	-	Apology
Clr Phil Eva	-	Apology
Clr Kevin Allen	-	Apology

5. CONFIRMATION OF MINUTES

5.1 **(MINUTE NO 404) (DAPPS 18/05/2017) - MINUTES OF THE DELEGATED AUTHORITIES, POLICIES & POSITION STATEMENTS COMMITTEE MEETING - 23 FEBRUARY 2017**

RECOMMENDATION

That Council confirms the Minutes of the Delegated Authorities, Policies and Position Statements Committee meeting held on Thursday, 23 February 2017 as a true and accurate record.

COMMITTEE DECISION

MOVED Clr S Portelli SECONDED Clr B Houwen that the recommendation be adopted.

CARRIED 5/0

6. DEPUTATIONS

Nil

7. PETITIONS

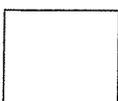
Nil

8. BUSINESS LEFT OVER FROM THE PREVIOUS MEETING (IF ADJOURNED)

Nil

9. DECLARATION OF COUNCILLORS WHO HAVE NOT GIVEN DUE CONSIDERATION TO MATTERS IN THE BUSINESS PAPER

Nil



10. COUNCIL MATTERS

AT THIS POINT IN THE MEETING, THE TIME BEING 6.03 PM THE FOLLOWING ITEMS WERE CARRIED BY 'EN BLOC' RESOLUTION OF COMMITTEE

10.2	11.1	13.1
10.4		13.2
		13.3

10.1 (MINUTE NO 405) (DAPPS 18/05/2017) - ANNUAL REVIEW OF DELEGATED AUTHORITIES EXTRANEIOUS TO THE LOCAL GOVERNMENT ACT 1995 (086/003) (J NGOROYEMOTO) (ATTACH)

RECOMMENDATION

That Council

- (1) adopt proposed amendments to Delegated Authorities made under Acts, extraneous to the Local Government Act 1995, as shown in the attachments to the Agenda; and
- (2) update the Delegated Authorities Register accordingly.

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COMMITTEE RECOMMENDATION

MOVED Mayor L Howlett SECONDED Cllr S Portelli that the recommendation be adopted.

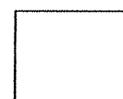
CARRIED 5/0

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COUNCIL DECISION

Background

Section 5.46 of the *Local Government Act 1995* requires local governments to keep and maintain a Register of Delegated Authority. In accordance with section 5.46(2), Governance Services have



coordinated a full review of the Delegations made under Acts extraneous to the Local Government Act, as required.

The review of the Delegations made under Acts extraneous to the Local Government Act is now complete. DAPPS Committee is now required to consider and recommend adoption of the proposed amendments to Council.

Submission

N/A

Report

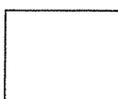
In accordance with s5.46 of the *Local Government Act 1995* the City currently maintains a Register of Delegated Authority.

Section 5.46(2) of the Act requires a complete review of the Register of Delegated Authority to be conducted at least once every financial year. While the review does not extend to those Delegations made under Acts extraneous to the Local Government Act 1995, it has been done to conform to the principle of good governance.

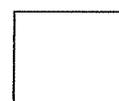
In consultation with the relevant officers, a comprehensive review of the Delegations made under other Acts was undertaken. Very few changes were made to delegations, indicating they are still in line with their objective.

Listed in the table below are the proposed amended delegations for consideration of Council.

<u>Delegation</u>	<u>Proposed Amendment</u>	<u>Reason for Amendment</u>
OLCS2 'Bush Fires Act 1954 – Prohibited and Restricted Burning Period'	Under sub heading conditions/guidelines after the last row in the table insert another role with the words 'Rottnest Island Authority'	Rottnest Island is under the municipal boundary of the City of Cockburn under the Bush Fire Act, this allows for notice of prohibited burnings to be published in all parts of the district of Cockburn
OLCS3 – Bush Fires Act 1954 - Legal Proceedings	Under sub heading conditions/guidelines delete the note referencing s 59A(3) Under the sub heading sub – delegate/s after the	To clarify and remove any confusion, as to the conditions that applies to this delegation. S 59A(3) relates to withdrawal of Fines, which is not relevant to this delegation This is to ensure separation of powers and



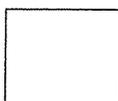
<u>Delegation</u>	<u>Proposed Amendment</u>	<u>Reason for Amendment</u>
	<p>words Director, Governance & Community Services and Ranger and Community Safety Services Manager positions insert the words ‘– institute legal proceedings only’</p> <p>Under the sub heading sub – delegate/s insert ‘Manager Recreation & Community Safety– institute legal proceedings only’ ‘Emergency Management Coordinator’ ‘Bushfire Risk Assessment Officer’</p>	<p>remove conflict of interest in duties, to ensure that position that are authorised to withdraw infringement fines, are not also delegated to issue infringements under the Bush Fires Act</p> <p>To allow additional officers that deal with Bush Fires Act 1954 in their regular work, to be able to enforce the provisions of this act in relation to issuing fine and initiating legal proceedings. To capture and include the new created role of Bushfire Assessment Officer enforcement powers</p>
OLCS14 ‘Cat Act 2011- Administration and Enforcement’	Under the sub heading sub – delegate/s , insert the words ‘Manager Recreation & Community Safety,	To provide coverage for the delegates under this delegation. Allows for an extra officer position to be able to enforce and administer powers of under the Cat Act 2011.
DA-OLCS15 – Dog Act 1976 – Administration and Enforcement	Under the sub heading sub – delegate/s , insert the words ‘Manager Recreation & Community Safety,	To provide coverage for the delegates under this delegation. Allows for an extra officer position to be able to enforce and administer powers of under the Dog Act 1976.
OLPD20 ‘Building Act 2011 – Approve or Refuse Building Permit’	<p>Under the sub heading Function Delegated after the words ‘ Design Compliance’ insert ‘as prescribed by s20 of the Building Act 2011’</p> <p>Under the sub heading Condition/Guidelines condition (1) Delete the words ‘District Town Planning Scheme 3’, and replace with ‘Local Planning Scheme No 3.</p>	<p>This nominates the specific section of the Act that addresses the authority to approve or refuse to approve building plans, specifications, Building Permit and Certificate of Design Compliance</p> <p>To reflect Western Australia Planning Commission recent changes from referencing Town Planning Scheme Number 3 to Local Planning Scheme Number 3</p>



<u>Delegation</u>	<u>Proposed Amendment</u>	<u>Reason for Amendment</u>
OLPD21 – Building Act 2011 - Approve or Refuse a Demolition Permit	Under the sub heading Condition/Guidelines condition (2) Delete the words 'District Town Planning Scheme 3', and replace with 'Local Planning Scheme No 3.	To reflect Western Australia Planning Commission recent changes from referencing Town Planning Scheme Number 3 to Local Planning Scheme Number 3
OLPD22 'Building Act 2011 – Approve or Refuse an Extension of Time for Building and Demolition Permits'	Under the sub heading Condition/Guidelines condition (1) Delete the words 'District Town Planning Scheme 3', and replace with 'Local Planning Scheme No 3.	To reflect Western Australia Planning Commission recent changes from referencing Town Planning Scheme Number 3 to Local Planning Scheme Number 3
OLPD26 'Building Act 1954 – Approve of Refuse an Extension of Time for an Occupancy Permit and Building Approval Certificate	Under the sub heading Condition/Guidelines condition (1) Delete the words 'District Town Planning Scheme 3', and replace with 'Local Planning Scheme No 3.	To reflect Western Australia Planning Commission recent changes from referencing Town Planning Scheme Number 3 to Local Planning Scheme Number 3

The Instruments of delegation listed below have been reviewed by staff and no changes are required, as they are considered appropriate for Council to adopt in their present form.

- OLCS1 'Bush Fires Act 1954 – Abatement of a Fire'
- OLCS5 'Bush Fires Act 1954 – Powers and Duties'
- OLEW1 'Graffiti Vandalism Act 2016- Administration & Enforcement'
- OLPD17 'City of Cockburn – Town Planning Scheme No.3 – Development Contributions'
- OLPD23 'Building Act 2011 - Issue An Occupancy Permit Or Building Approval Certificate'
- OLPD24 'Building Act 1952- Make an Order for Building or Demolition Work'
- OLPD25 'Building Act 2011 – Revoke Order for Building or Demolition Work'
- OLPD27 'Building Act 2011 - Appoint Authorised Persons'
- OLPD28 'Building Act 2011 – Legal Proceedings'
- OLPD29 'Food Act 2008 – Prosecution'
- OLPD30 'Food Act 2008 - Prohibition Orders'
- OLPD31 'Food Act 2008 – Registration of Food Business'
- OLPD32 'Food Act 2008 – Appointment of Authorised Persons and Designated Officers'
- OLPD33 'Local Planning Scheme No. 3 – Development Control'



- OLPD34 'Public Health Act 2016- Appointment Of Authorised Officers'

Strategic Plan/Policy Implications

Leading & Listening

- Deliver sustainable governance through transparent and robust policy and processes

Budget/Financial Implications

N/A

Legal Implications

Sec. 5.46(2) of the Local Government Act, 1995 refers.

Community Consultation

N/A

Risk Management Implications

Failure to adapt the recommendation may result in inconsistent Instruments of Delegation that do not reflect current practices and positions, thus not adhering to principles of good governance.

Attachment(s)

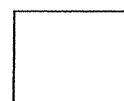
Various instruments of Delegated Authorities - 8

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil



10.2 **(MINUTE NO 406)** (DAPPS 18/05/2017) - ANNUAL REVIEW OF DELEGATED AUTHORITIES PURSUANT TO THE LOCAL GOVERNMENT ACT 1995 (086/003) (J NGOROYEMOTO) (ATTACH)

RECOMMENDATION

That Council

- (1) adopt proposed amendments to Delegated Authorities pursuant to the Local Government Act, 1995, as shown in the attachments to the Agenda; and
- (2) update the Delegated Authorities Register accordingly.

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COMMITTEE RECOMMENDATION

MOVED Clr S Portelli SECONDED Clr L Sweetman that the recommendation be adopted.

CARRIED 5/0

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COUNCIL DECISION

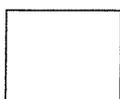
Background

Section 5.46 of the *Local Government Act 1995* requires local governments to keep and maintain a Register of Delegated Authority. In accordance with section 5.46(2), Governance Services have coordinated a full review of the Delegations made under the Local Government Act, as required.

The review of the Delegations made under the Local Government Act is now complete. DAPPS Committee is now required to consider and recommend adoption of the proposed amendments to Council.

Submission

N/A



Report

In accordance with s5.46 of the *Local Government Act 1995* the City currently maintains a Register of Delegated Authority.

Section 5.46(2) of the Act requires a complete review of the Register of Delegated Authority to be conducted at least once every financial year. The review of the delegations made under the Local Government Act has now been undertaken and is now being presented for adoption.

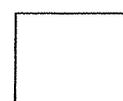
In consultation with the relevant officers, a comprehensive review of these delegations was undertaken. Very few changes were made to delegations, indicating they are still in line with their objective.

Listed in the table below are the proposed amended delegations for consideration of Council.

<u>Delegation</u>	<u>Proposed Amendment</u>	<u>Reason for Amendment</u>
LGACS3 – City Of Cockburn (Local Government Act) Local Laws – Notices	Under sub heading sub-delegate/s , Insert the words '& Risk Management', after the word Governance	Cosmetic change to reflect the correct position title as per the City of Cockburn organisational Structure
LGAFCS4 – Local Government Act, 1995 – Payments From Municipal and Trust Funds	Under the sub heading sub – delegate/s , delete the words 'Budgeting and Financial' before 'Manager' and replace with the words 'Accounting Services'	Cosmetic change to reflect the correct position title as per the City of Cockburn organisational Structure
LGAFCS9 – Acquisition and Disposal of Property (Land)	In the Title of the Delegation Document delete LGAFCS9, and replace with LGAES12	To change the responsible section for managing this delegation document from Financial Services to Executive Services. As the responsible officer for this delegation and delegate is the CEO.

The Instruments of delegation, listed below have been reviewed by staff and are considered appropriate for Council to adopt in their present form.

- LGACS2 'City of Cockburn (local Government Act) Local Laws – Grants'.
- LGACS4 – 'City of Cockburn (Local Government Act) Local Laws – Stallholders'.

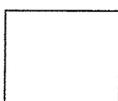


- LGACS5 – ‘City of Cockburn (Local Government Act) Local Laws – Use of Reserves;
- LGACS7 ‘Local Government Act, 1995 – Funding Assistance – Community Associations – Publication & Distribution of Newsletters’.
- LGACS9 ‘Local Government Act, 1995 – Youth Sports Travel Assistance Grant’.
- LGACS10 –‘Local Government Act, 1995 – Youth Art Scholarship Programme’.
- LGACS11 – ‘City of Cockburn (Local Government Act) Local Laws – Applications to keep more than two (2) dogs at a residential property’.
- LGAES2 ‘Local Government Act, 1995 – Appointment of Authorised Persons’.
- LGAES3 ‘Local Government Act, 1995 – Calling of Tenders of Expressions of Interest’.
- LGAES4 ‘Local Government Act, 1995 – Contract Variation’.
- LGAES5 ‘Local Government Act, 1995 – Legal Proceedings’.
- LGAES6 ‘Local Government Act, 1995 – Authority to Call Public Meetings’.
- LGAES11 ‘Local Government Act, 1995 – Execution of Documents’.
- LGAEW1 ‘City of Cockburn (Local Government Act) Local Laws – Traffic and Vehicles’.
- LGAEW2 Local Government Act 1995 – Temporary Road Closures’.
- LGAEW3 ‘Local Government Act, 1995 – Dangerous Trees’.
- LGAEW4 Local Government Act 1995 – Sand Drift’.
- LGAEW5 – ‘Local Government Act (Uniform Local Provisions) Regulations, 1996 - Obstruction of Streets’.
- LGAFCS1 – ‘Local Government Act, 1995 – Advertising Proposed Differential Rates’.
- LGAFCS5 ‘Local Government Act, 1995 – Recovery of Rates and Services Charges – Leased Properties’.
- LGAFCS8 – ‘Corporate Credit Cards’.
- LGAFCS10 ‘Objections to the Rate Record and Rateable Status of Land’.
- LGAPD1 ‘City of Cockburn (Local Government Act) Local Laws 2000(as amended) – Signs, Hoardings, Bill Postings’
- LGAPD4 – Preparation of Business Plans for Disposal of Land

Strategic Plan/Policy Implications

Leading & Listening

- Deliver sustainable governance through transparent and robust policy and processes



Budget/Financial Implications

N/A

Legal Implications

Sec. 5.46 (2) of the Local Government Act, 1995 refers.

Community Consultation

N/A

Risk Management Implications

Failure to adapt the recommendation may result in non-compliance with the local Government Act 1995 requirement to complete review of the Register of Delegated Authority at least once every financial year.

Attachment(s)

Various instruments of Delegated Authority – 3.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

10.3 (MINUTE NO 407) (DAPPS 18/05/2017) - PROPOSED AMENDMENT TO POLICY SC9 'REPRESENTATION AT COUNCIL RELATED FORUMS' (182/001) (D GREEN) (ATTACH)

RECOMMENDATION

That Council adopt proposed amendments to Policy SC9 'Representation at City Related Forums and Site Visits', as shown in the attachments to the Agenda.

COMMITTEE RECOMMENDATION

MOVED Mayor L Howlett SECONDED Cllr B Houwen that the recommendation be adopted subject to the amendments as shown in the attachments to the Minutes.

CARRIED 5/0



COUNCIL DECISION

Reason for Decision

Mayor Howlett requested that in instances where an Elected Member makes such a request, there is no process in place for the Mayor or other Elected Members to be notified of this request. So that this may be addressed the Policy should be amended to reflect that where the request is supported all Elected Members be notified of the outcome.

Background

Council's Policy on this matter was first adopted in 2000 and was designed to provide for occasions where constituents / stakeholders and City representatives could meet informally, usually in an "office" situation to discuss a particular issue. More recently, with the rapid development of the Cockburn district, it has become more commonplace for such occasions to be held "on site" to enable participants to better comprehend the details of the subject under discussion.

Submission

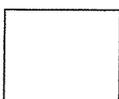
N/A

Report

Given the time elapsed since this Policy was first adopted by Council, it is considered appropriate for the intent of the Policy to reflect more relevant practices associated with community engagement.

The increased pace of the development of Cockburn requires the City's representatives (elected and administrative) to consult with its stakeholders in more flexible circumstances than are provided for within the terms of the current Policy. Consequently, it is recommended that the Policy include "site visits" as a legitimate meeting option, in addition to clarifying a process by which these meetings could be initiated.

In addition, the amended Policy emphasises that under no circumstances should any City of Cockburn representatives at a meeting involving third party stakeholders commit to or advocate a particular outcome. This is considered necessary given that the Rules of Conduct Regulations are now applicable and requires members to



adopt a neutral position on matters relative to their decision making responsibilities, external to the formal meeting proceedings of Council.

Strategic Plan/Policy Implications

Leading & Listening

- Deliver sustainable governance through transparent and robust policy and processes

Budget/Financial Implications

N/A

Legal Implications

Regulation 7 of the Local Government (Rules of Conduct) Regulations refers.

Community Consultation

N/A

Risk Management Implications

A “Substantial” level of non-compliance risk would result if Council was to reject or ignore the proposed amendments to the Policy.

Attachment(s)

Proposed amended Policy SC9 ‘Representation at City Related Forums and Site Visits’

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

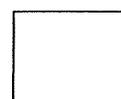
Nil.

10.4 (MINUTE NO 408) (DAPPS 18/05/2017) - PROPOSED AMENDMENTS TO POSITION STATEMENT PSPD25 'RESPONSE TO APPEALS' (182/002) (D GREEN) (ATTACH)

RECOMMENDATION

That Council

- (1) adopt proposed new Policy SC58 ‘Response to Appeals’; and



(2) delete Position Statement PSPD25 'Response to Appeals'
as shown in the attachments to the Agenda.

COMMITTEE RECOMMENDATION

MOVED Clr S Portelli SECONDED Clr L Sweetman that the recommendation be adopted.

CARRIED 5/0

COUNCIL DECISION

Background

Since 2012, the State Administrative Tribunal (SAT) has had the authority to hear appeals against certain types of Council decisions by aggrieved persons or applicants affected by these decisions.

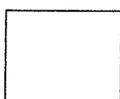
As a means of managing the procedures associated with the appeals process, Council adopted a Position Statement (PSPD25 "Response to Appeals") which provided for Elected Members to represent the City of Cockburn at the Tribunal, in the event of the relevant Council decision being the opposite to the recommendation contained in officer report provided.

However, the Position Statement has been found to be difficult to implement on some occasions and has led to some lack of communication on the Tribunal outcomes being provided.

Accordingly, some fundamental changes to this process are recommended to clarify the roles and responsibilities of participants involved.

Submission

N/A



Report

The majority of matters associated with third party appeals to the SAT against Council decisions are related to planning considerations by Council and more particularly to development applications.

When such appeals are in response to officer recommendations which are **supported** by Council, the ensuing process of responding to the SAT advice is relatively straightforward, as it is primarily managed by the appropriate City's officer/s.

However, when the appeal is as the result of a Council decision made **contrary** to officer advice, the ensuing process is much more complicated, as the current Position Statement emphasises that **Council** nominates one or more Elected Members to essentially frame the case for responding to the appeal. This mechanism is flawed to the extent that there is no way of knowing in advance when an appeal may be lodged to SAT and thereafter whether there would be time for Council to convene and nominate its advocates to attend the Tribunal proceedings. In addition, the current Position does not stipulate that the outcomes of the Tribunal should be advised to the Elected Members. Such information is fundamental to enabling Elected Members an oversight of the impact of the decisions endorsed at a previous Council Meeting.

Accordingly, a number of clarifying statements are recommended for inclusion to the current process which better define and streamline the responsibilities of the City's representatives as they affect the SAT appeal procedures.

This includes identifying the mover and seconder of dissenting motions adopted by Council as the City's advocates at any subsequent SAT proceedings in addition to specifying a notification process to ensure Elected Members are kept apprised of the SAT outcomes.

Strategic Plan/Policy Implications

City Growth

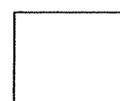
- Ensure planning facilitates a desirable living environment and meets growth targets

Leading & Listening

- Deliver sustainable governance through transparent and robust policy and processes

Budget/Financial Implications

N/A



Legal Implications

State Administrative Tribunal Act 2004; and
Local Government Administration Regulation 11 (da) refers.

Community Consultation

N/A

Risk Management Implications

A “Moderate” level of non – compliance risk would result if the proposed amendments were rejected by Council

Attachment(s)

1. Proposed new Policy SC58 ‘Response to Appeals’
2. Proposed superseded Position Statement PSPD25 ‘Response to Appeals’

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

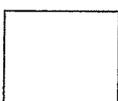
Nil.

11. PLANNING & DEVELOPMENT DIVISION ISSUES

- 11.1 **(MINUTE NO 409)** (DAPPS 18/05/2017) - ADOPTION OF PROPOSED MODIFICATIONS TO LOCAL PLANNING POLICY LPP 4.6 ‘COCKBURN COAST DESIGN GUIDELINES FOR ROB JETTY AND EMPLACEMENT PRECINCTS’ (182/001) (C DA COSTA) (ATTACH)

RECOMMENDATION

That Council adopt the proposed modifications to Local Planning Policy LPP 4.6 ‘Cockburn Coast Design Guidelines for Rob Jetty and Emplacement Precincts’ for finalisation in accordance with Clause 4 of the *Planning and Development (Local Planning Schemes) Regulations 2015*, as shown in the attachment to the Agenda.



COMMITTEE RECOMMENDATION

MOVED Clr S Portelli SECONDED Clr L Sweetman that the recommendation be adopted.

CARRIED 5/0

COUNCIL DECISION**Background**

Modifications to the City's Local Planning Policy 4.6 'Cockburn Coast Design Guidelines for Robb Jetty and Emplacement Precincts' was adopted by Council for the purposes of advertising in accordance with Clause 4 (1) of the *Planning and Development (Local Planning Schemes) Regulations 2015* at its meeting held on 09 March 2017.

The policy was subsequently advertised and no comments were received during the advertising period.

The Local Planning Policy LPP 4.6 provides guidance for the creation of quality development that ensures the design principles of the Robb Jetty and Emplacement Local Structure Plan are achieved.

Submission

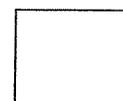
N/A

Report

The purpose of this policy is to provide guidance for applicants, Council and the community in the assessment and determination of applications for development within the Robb Jetty and Emplacement Local Structure Plan.

The non-administrative changes to the policy include:

- Clarification of setback requirements for buildings particularly relating to balconies;
- Reduction of setbacks to public open space from 4m to 3m;
- Clarification of building heights to avoid confusion in the interpretation of the building heights plans (figure 14);
- Fencing requirements relocated to a new section in each typology;
- Clarification that the Mixed Use – Cockburn Road Typology includes both Mixed Use and Mixed Business zones;



- Clarification of open space provisions for grouped dwellings and single houses;
- Introduction of a requirement for communal open space for multiple dwelling developments which is consistent with the State Government's draft Apartment Design Guide.

The above modifications to the policy are considered relatively minor and were recommended in collaboration with Landcorp to provide more clarity and consistency across the various planning documents applicable to the area.

It is envisaged however that further modifications to the design guidelines may be warranted once the State Government's Apartment Design Guide is finalised to provide greater consistency between apartment development in this area and in other areas around Western Australia.

Strategic Plan/Policy Implications

City Growth

- Ensure planning facilitates a desirable living environment and meets growth targets.

Economic, Social & Environmental Responsibility

- Create opportunities for community, business and industry to establish and thrive through planning, policy and community development.

Leading & Listening

- Deliver sustainable governance through transparent and robust policy and processes.

Budget/Financial Implications

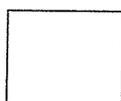
N/A

Legal Implications

N/A

Community Consultation

Consultation was undertaken in accordance with the requirements of the *Planning and Development (Local Planning Schemes) Regulations 2015* and no submissions were received.



Risk Management Implications

If the modifications are not adopted, it could result in an inconsistent approach to decision making, which is undesirable and could damage the brand and/or reputation of the City.

Attachment(s)

Proposed modified Local Planning Policy LPP 4.6 'Cockburn Coast Design Guidelines for Rob Jetty and Emplacement Precincts'.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

11.2 **(MINUTE NO 410) (DAPPS 18/05/2017) - ADOPTION OF MINOR MODIFICATIONS TO LOCAL PLANNING POLICIES (182/001) (C DA COSTA) (ATTACH)**

RECOMMENDATION

That Council adopt minor modifications to the following Local Planning Policies:

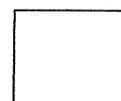
- LPP 1.1 'Residential Design Codes Alternative Deemed to Comply Provisions';
- LPP 1.2 'Residential Design Guidelines';
- LPP 2.5 'Relocation of Building Envelopes';
- LPP 3.7 'Signs and Advertising'

In accordance with Clause 5(2) of the Planning and Development (Local Planning Schemes) Regulations 2015, as shown in the attachment to the Agenda.

COMMITTEE RECOMMENDATION

MOVED Cllr S Portelli SECONDED Mayor L Howlett that the recommendation be adopted.

CARRIED 5/0



COUNCIL DECISION

Background

The local planning policies, the subject of this report, require minor modifications. The modifications are minor in nature, and serve to clarify certain elements of the policies. A table depicting the changes to each policy is clarified in the 'Report' section.

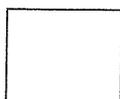
Submission

N/A

Report

The local planning policies proposed to be amended are reflected in the table below:

Ref. No.	Local Planning Policy	Change Summary
LPP 1.1	Residential Design Codes Alternative Deemed to Comply Provisions	<ul style="list-style-type: none"> Clarifying the provisions for the outbuilding boundary walls.
LPP 1.2	Residential Design Guidelines	<ul style="list-style-type: none"> Clarifying the requirements for walls on the boundary abutting Public Access Ways (PAW), Right of Ways (ROW), Public Open Space (POS), and any other reservations.
LPP 2.5	Relocation of Building Envelopes	<ul style="list-style-type: none"> Changing title of Policy from 'Relocation of Building Envelopes' to 'Building Envelopes'; and Clarify the requirements for the allocation of building envelopes in line with the requirements of LPS 3.
LPP 3.7	Signs and Advertising	<ul style="list-style-type: none"> Minor grammatical and spelling errors; Further clarification on variable message signs in relation to Service Stations (Sign Type 17); and Addition of image and clarification to pylon signs (Sign Type 12).



Strategic Plan/Policy Implications

City Growth

- Continue revitalisation of older urban areas to cater for population growth and take account of social changes such as changing household types

Community, Lifestyle & Security

- Provide for community facilities and infrastructure in a planned and sustainable manner

Budget/Financial Implications

N/A

Legal Implications

N/A

Community Consultation

N/A

Risk Management Implications

If the subject changes to the policies are not adopted and therefore not progressed, some inconsistencies would occur in relation to existing practices. This practice needs to be formalised in a policy for consistency and reliability.

Attachment(s)

Proposed amendments to the following Local Planning Policies:

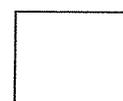
1. LPP 1.1 'Residential Design Codes Alternative Deemed to Comply Provisions'
2. LPP 1.2 'Residential Design Guidelines'
3. LPP 2.5 'Relocation of Building Envelopes'
4. LPP 3.7 'Signs and Advertising'

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.



12. FINANCE & CORPORATE SERVICES DIVISION ISSUES

12.1 **(MINUTE NO 411)** (DAPPS 18/05/2017) - PROPOSED AMENDMENTS TO POLICY AES8 'COUNCIL OWNED VEHICLE USAGE POLICY (182/001) (S BARON) (ATTACH)

RECOMMENDATION

That Council adopt proposed amendments to Policy AES8 'Council Owned Vehicle Usage' and associated Delegated Authority, as shown in the attachment to the Agenda.

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COMMITTEE RECOMMENDATION

MOVED Mayor L Howlett SECONDED Cllr S Portelli that the recommendation be adopted, subject to the amendments as shown in the attachments to the Minutes.

CARRIED 5/0

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

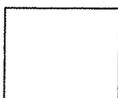
COUNCIL DECISION

Reason for Decision

Mayor Howlett noted that the Policy does not reflect where designated drivers who have been approved by the Chief Executive Officer that they have a valid WA driver's licence. It mentions that other employees must have a valid licence for the class of vehicle being driven. Mayor Howlett felt that the Policy needs to be amended to incorporate those employees who have been allocated a vehicle must have a valid WA driver's licence.

Background

A number of incidents have occurred where unauthorised use of Council vehicles has occurred, incidents and damage to vehicles have not been reported and drug and alcohol testing has not been carried out.



Submission

N/A

Report

Proposed changes include:

- stipulating that damage and incidents involving a Council owned vehicle must be reported immediately, in line with the City's incident reporting procedure;
- the inclusion of 'for cause' drug and alcohol testing to be administered after an accident;
- unauthorised use of a Council owned vehicle may result in disciplinary action; and

These changes were designed to address the issues of damage and accidents involving Council vehicles not being reported.

Strategic Plan/Policy Implications

City Growth

- Maintain service levels across all programs and areas

Economic, Social & Environmental Responsibility

- Sustainably manage our environment by protecting, managing and enhancing our unique natural resources and minimising risks to human health

Leading & Listening

- Deliver sustainable governance through transparent and robust policy and processes

Budget/Financial Implications

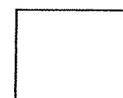
N/A

Legal Implications

N/A

Community Consultation

N/A



Risk Management Implications

Should the changes not be adopted, unauthorised use and under reporting of incidents may continue.

Attachment(s)

1. Proposed amendments to AES8 'Council Owned Vehicle Usage'
2. Delegated Authority AES8 'Council Owned Vehicle Usage'

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

13. ENGINEERING & WORKS DIVISION ISSUES

13.1 (MINUTE NO 412) (DAPPS 18/05/2017) - PROPOSED AMENDMENT TO POSTION STATEMENT PSEW21 'TRAILER PASSES' (182/002) (L DAVIESON) (ATTACH)

RECOMMENDATION

That Council adopt proposed amendments to Position Statement PSEW21 'Trailer Passes', as shown in the attachment to the Agenda.

COMMITTEE RECOMMENDATION

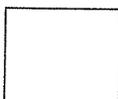
MOVED Clr S Portelli SECONDED Clr L Sweetman that the recommendation be adopted.

CARRIED 5/0

COUNCIL DECISION

Background

Position Statement PSEW21 "Trailer Passes" was reviewed and approved in 2013 to more accurately reflect the content of the Policy.



Submission

N/A

Report

The City has recently upgraded the Henderson Waste Recovery Park weighbridge software program. A key feature in this upgrade allowed for the bar code scanning of Trailer Passes. This feature serves to protect the “highly valued” Cockburn privilege of 6 Trailer Passes annually.

This new software is connected to TechOne and allows a simpler and more accurate assessment of a property’s Trailer Pass usage at a given time. It allows our Waste Staff to issue, replace, “top up”, register its use or cancel Trailer Passes quickly and efficiently.

Prior to the new weighbridge system, the 6 Trailer Passes attached to the Rates Notice were not individually marked with the property address. The barcode printed on each Trailer Pass now automatically identifies the property to which they relate.

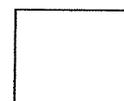
In recent years, residents have resorted to social media platforms and Gumtree to advertise and sell their unused Trailer Passes. The Trailer Pass itself, has for decades, identified that these passes are not transferable, nor valid if separated from the other Trailer Passes on the issue page.

The City already maintains an element of surveillance of Twitter, Facebook and other social media platforms (Cockburn Chat). When trading or selling of Trailer Passes is identified, the Waste Services Administration Team is notified. The seller is contacted and advised of the City’s Policy.

The ability to scan bar codes has meant that the requirement to keep Trailer Passes attached to each other is no longer necessary.

The changes to PSEW21 are summarised as follows in Section (1) 7:

- Upon presentation, Trailer Passes will be deemed as invalid if it is proven that they do not relate to the issued property. If any resident attempts to sell, trade or transfer passes to others beyond the residence to which they are issued, the City will cancel the relevant Trailer Passes.
- Part of sub-clause (8) and sub-clause (9) has been removed as it is considered procedural in nature.



Strategic Plan/Policy Implications

Community, Lifestyle & Security

- Provide residents with a range of high quality, accessible programs and services

Leading & Listening

- Deliver sustainable governance through transparent and robust policy and processes
- Ensure sound long term financial management and deliver value for money

Budget/Financial Implications

6 Trailer Passes are issued to all residential properties annually. This cost is incorporated in the Waste and Recycling Removal Charge Levied (GL 480 5550) as part of the Waste Management Budget build.

Legal Implications

N/A

Community Consultation

N/A

Risk Management Implications

Ensuring that Trailer Passes are used correctly by the residential property owners or tenants is fundamental to our commitment to excellent customer service to all residents. The sale and transference of Trailer Passes to ineligible residents will result in additional unnecessary costs to the Waste Service Unit and therefore the wider Cockburn community.

Attachment(s)

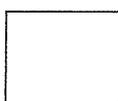
Proposed amendments to Position Statement PSES21 'Trailer Passes'.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.



13.2 **(MINUTE NO 413)** (DAPPS 18/05/2017) - PROPOSED AMENDMENTS TO DELEGATED AUTHORITY AEW3 'STREET & PUBLIC AREA LIGHTING' (086/003) (J KIURSKI) (ATTACH)

RECOMMENDATION

That Council adopt proposed amendments to Delegated Authority AEW3 'Street and Public Area Lighting', as shown in the attachment to the Agenda.

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COMMITTEE RECOMMENDATION

MOVED Clr S Portelli SECONDED Clr L Sweetman that the recommendation be adopted.

CARRIED 5/0

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COUNCIL DECISION

Background

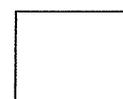
In accordance with the annual review of Council Delegated Authorities, Policies and Position Statements relevant officers have reviewed the Delegated Authorities, Policies and Position Statements and are presented for adoption.

Submission

N/A

Report

The Engineering and Works Policy and Delegated Authority AEW3 'Street and Public Area Lighting' was reviewed and adopted on 10 September 2015 however the the titles of delegate/s were omitted, and were not reviewed at the time to reflect current organisation structure.



This delegated authority has now been reviewed and the titles of delegate/s authorised changed to suite the current organisation structure, and is presented for Committee consideration.

Strategic Plan/Policy Implications

City Growth

- Ensure planning facilitates a desirable living environment and meets growth targets

Moving Around

- Improve connectivity of transport infrastructure

Community, Lifestyle & Security

- Provide for community facilities and infrastructure in a planned and sustainable manner

Budget/Financial Implications

N/A

Legal Implications

N/A

Community Consultation

N/A

Risk Management Implications

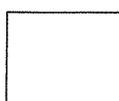
The adoption of the recommendation allows for correct positions to be delegated, failure to adopt the recommendation will result in delegation document that refers to incorrect position titles, and ultimately affect the management of requests for additional street lighting and alterations within the City of Cockburn and the appropriate exercise of the delegation.

Attachment(s)

Proposed amendments to Delegated Authority AEW3 'Street and Public Area Lighting'.

Advice to Proponent(s)/Submissioners

N/A



Implications of Section 3.18(3) Local Government Act, 1995

Nil.

13.3 (MINUTE NO 414) (DAPPS 18/05/2017) - PROPOSED AMENDMENTS TO POLICY SC37 'SUSTAINABILITY' (064/121; 182/001) (M BAINBRIDGE) (ATTACH)

RECOMMENDATION

That Council adopt proposed amendments to Policy SC37 'Sustainability', as shown in the attachments to the Agenda.

COMMITTEE RECOMMENDATION

MOVED Clr S Portelli SECONDED Clr L Sweetman that the recommendation be adopted.

CARRIED 5/0

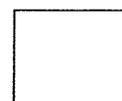
COUNCIL DECISION

Background

In 2006 Council adopted a Sustainability policy to inform its commitment to implement sustainability measures across the organisation and work with the community towards an environmentally, socially and economically sustainable future.

The City has, in the interim, developed a full suite of strategic and informing documents to complement this policy, which now form the basis of the City's Integrated Reporting Framework for sustainability.

The suite of accompanying documents (Sustainability – Integrated Reporting Framework) is currently under review as follows:



Sustainability – Integrated Reporting Framework – Review Process

1. Sustainability Policy (SC37)

To ensure alignment with Strategic Community Plan and Sustainability Strategy and Action Plan.

Currently the policy includes elements that represent operational targets and actions which should be captured in the Strategy and Action Plan. The policy requires simplification and review to align with the current Strategic Community Plan and Sustainability Strategy, as well as to more clearly define the City's high level policy position / commitments on sustainability (principles).

This avoids having to review the policy each time strategic or operational documents are reviewed and ensures consistency of approach.

2. Sustainability Strategy

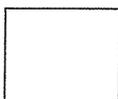
To ensure integration with Strategic Community Plan and align the integrated reporting framework for sustainability's objectives, targets and KPI's with the four year review period set against the Strategic Community Plan.

As the Strategic Community Plan has recently been reviewed, the current Sustainability Strategy does not adequately align with articulated City objectives. To address this inconsistency, and to ensure that objectives, targets and KPI's outlined in the Strategic Community Plan are achieved in a sustainable manner, the Sustainability Strategy will be reviewed to set City-wide, measurable targets which will support the City in meeting its high level strategic objectives.

3. Sustainability Action Plan

To ensure integration with Strategic Community Plan and align the integrated reporting framework for sustainability's objectives, targets and KPI's with the four year review period set against the Strategic Community Plan.

The Sustainability Action Plan is currently reviewed annually, and new business unit KPI's are set each year. These targets do not carry over from year to year. This means that tracking City-wide progress towards longer term targets is not currently possible.



By reviewing the Action Planning process, and aligning it to the Strategic Community Plan's four year review period, we can set achievable four year, measurable targets and annual KPI's to track these (i.e. percentage renewables developed / percentage green space improved etc). This way we can ensure that each year our progress is tracked against agreed benchmarks and communicated as a process of continual improvement.

4. State of Sustainability Reporting Structure

To measure, monitor and communicate progress towards agreed targets set out in Strategy and Action Plan.

The State of Sustainability reporting structure will be retained largely as is, with an annual review (predominantly online, rather than printed) to show annual progress towards key targets and assess any gaps or resourcing issues.

The tracking process however will be aligned to the longer term targets (four year).

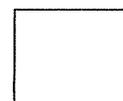
Submission

N/A

Report

The main proposed amendments for Sustainability Policy SC37 are:

Policy	Proposed Amendment	Reason for Amendment
SC37	<ol style="list-style-type: none"> 1. Reordering of the BACKGROUND section to reference the City's position above the Local Government Act: 2. Expansion of the BACKGROUND section to include key principles of sustainability for local government as referenced in the Local Government Act: 3. Substantial editing of the PURPOSE section to simplify and outline the City's high level policy and practice commitments to sustainability: 	<p>Making the City's position upfront in the document.</p> <p>To provide Council, staff and the community with clear direction around the City's and local government's legislative responsibility with respect to sustainability.</p> <p>To ensure that the policy deals only with policy level matters (not procedural / practical matters) and clearly illustrates its purpose. Specific reference to the reporting</p>



	<p>4. Expansion of the POLICY section to provide a clear and concise outline of the six specific policy commitments that the City makes on sustainability.</p>	<p>framework will be captured within the strategy and will reference other strategic documents as required.</p> <p>To make the policy more clear, concise and easier to comprehend.</p>
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Strategic Plan/Policy Implications

Economic, Social & Environmental Responsibility

- Sustainably manage our environment by protecting, managing and enhancing our unique natural resources and minimising risks to human health
- Improve water efficiency, energy efficiency and waste management within the City’s buildings and facilities and more broadly in our community
- Further develop adaptation actions including planning; infrastructure and ecological management to reduce the adverse outcomes arising from climate change

Budget/Financial Implications

N/A

Legal Implications

N/A

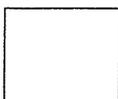
Community Consultation

N/A

Risk Management Implications

The full integration of sustainability across the organisation and at an ‘intra’ business unit level is necessary for the City’s sustainability objectives as outlined in the Sustainability Strategy, and the Strategic Community Plan 2016 - 2026 to be achieved.

The amendments to SC37 reduce the risk of falling short of meeting articulated commitments and KPI’s and also reduce the reputational and potentially economic risks of making decisions that do not consider broader sustainability impacts.



Attachment(s)

Proposed amendments to Policy SC37 'Sustainability'.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

14. COMMUNITY SERVICES DIVISION ISSUES

Nil

15. EXECUTIVE DIVISION ISSUES

Nil

16. MOTIONS OF WHICH PREVIOUS NOTICE HAS BEEN GIVEN

Nil

17. NOTICES OF MOTION GIVEN AT THE MEETING FOR CONSIDERATION AT NEXT MEETING

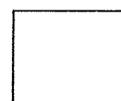
Nil

18. NEW BUSINESS OF AN URGENT NATURE INTRODUCED BY DECISION OF MEETING BY COUNCILLORS OR OFFICERS

- 18.1 (MINUTE NO 415) (DAPPS 18/05/2017) - PROPOSED NEW POLICY SC59 'PUBLIC INTERNET USE & WIRELESS ACCESS' (182/001; 191/017) (K FITZPATRICK) (ATTACH)**

RECOMMENDATION

That Council adopt proposed new Policy SC59 'Public Internet Use and Wireless Access', as attached to the Agenda.



COMMITTEE RECOMMENDATION

MOVED Clr S Portelli SECONDED Mayor L Howlett that the recommendation be adopted.

CARRIED 5/0

COUNCIL DECISION

Background

The City proposes to offer a free wireless public Internet access service to the community. This service begins at Bibra Lake Regional Playground and Cockburn Aquatic and Recreation Centre (ARC), then progressing to other areas of the City.

It is planned for this service to be available ready for Cockburn ARC opening to the community. Therefore, the relevant Policy and Terms and Conditions have been drafted to minimise the risks associated in providing this service.

Submission

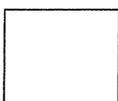
N/A

Report

The City is committed to serving the information access needs of all members of the community. The City is helping to bridge the digital divide in its community by providing to the public free wireless service to access the Internet ("the Service"). Public Wi-Fi can help create new opportunities for community engagement and collaboration, provide new services and is a means of encouraging people to spend more time in communal areas, such as Cockburn ARC and Bibra Lake Regional Playground.

In time, this Service will also enable the City to equip its field workers (e.g. Rangers,) to securely connect to the City's information systems as and when required. This service also enables the City to engage more with the government's 'Smart Cities' initiative at the appropriate time.

With the pending opening of Cockburn ARC and re-opening of Bibra Lake Regional Playground, the City will be providing public free wireless Internet access. It is common practice across Australian and international Public Wi-Fi service providers to require Users to accept a



set of terms and conditions before using the Service for the first time. The City has drafted a Policy and Terms of Conditions to minimise the risks associated with providing this Service and to meet collection and disclosure requirements (as required under the Privacy Act, 1988) as well as to notify Users of limitations in liability.

The proposed Policy and Terms of Conditions outline the level of service offered and sets Service expectations. It also summarises the salient points of Public Wi-Fi Terms and Conditions of Services across Australia for the purposes of identifying aspects that are common and should be included in Terms and Conditions for new Public Wi-Fi Services in the City of Cockburn.

Due to the ever-changing nature of the Internet, the Terms and Conditions are subject to change without notice and at the discretion of the City's Administration.

Strategic Plan/Policy Implications

City Growth

- Ensure growing high density living is balanced with the provision of open space and social spaces

Community, Lifestyle & Security

- Create and maintain recreational, social and sports facilities and regional open space
- Advocate for improvements to information technology infrastructure such as the NBN rollout

Budget/Financial Implications

Currently budgeted for Cockburn ARC and Ilbra Lake Regional Playground deployment. Additional budgeting required for future locations.

Legal Implications

Privacy Act 1988

Privacy and Personal Information Protection Act 1998

Telecommunications Act 1997

Copyright Act 1968

Community Consultation

As this is a Service Provider requirement, no community consultation is required.



Risk Management Implications

If the recommendation is not adopted the City of Cockburn could be held liable for any damage or offence caused by the risks from Internet content.

With the impending opening of Cockburn ARC and the intent to provide free public Internet usage within the facility, a "Substantial" level of Compliance risk could result.

Attachment(s)

1. Proposed new Policy SC59 'Public Internet Use and Wireless Access'.
2. City of Cockburn Acceptable Use of Public WiFi Internet – Terms and Conditions.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

19. MATTERS TO BE NOTED FOR INVESTIGATION, WITHOUT DEBATE

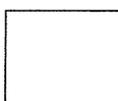
Nil

20. CONFIDENTIAL BUSINESS

Nil

21 (DAPPS 18/05/2017) - CLOSURE OF MEETING

6.28 pm.



POL	REPRESENTATION AT COUNCIL RELATED FORUMS & SITE VISITS	SC9
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POLICY CODE:	SC9
DIRECTORATE:	Executive Services
BUSINESS UNIT:	Executive Support
SERVICE UNIT:	Executive Support
RESPONSIBLE OFFICER:	Director, Governance & Community Services
FILE NO.:	086/001
DATE FIRST ADOPTED:	15 August 2000
DATE LAST REVIEWED:	12 December 2013
ATTACHMENTS:	N/A
DELEGATED AUTHORITY REF.:	N/A
VERSION NO.	2

Dates of Amendments / Reviews:	
DAPPS Meeting:	22 March 2012 28 November 2013
OCM:	17 September 2002 12 April 2012

BACKGROUND:

Council often seeks, or is requested or required, to arrange or attend meetings or visits extraneous to the normal business of Council.

PURPOSE:

To stipulate a protocol by which Council representation to attend occasional meetings, forums or visits as deemed appropriate, can be effected.

POLICY:

Meetings, other than Council Meetings, which are either required to be conducted by Council (e.g. Elector Meetings) or are sought by, or requested of, City representatives to be held for any specific purpose (e.g. Public Meetings / Meetings with Ratepayers / Site Visits / Developers / Politicians) will be chaired, by the Mayor or nominee and attended, as appropriate, by Councillors and facilitated by nominated staff.

Elected Members wishing to initiate such meetings/visits, shall send a request, in writing to the Chief Executive Officer for consideration. Where the requested is supported all Elected Members shall be notified of the outcome.

POL	REPRESENTATION AT COUNCIL RELATED FORUMS & SITE VISITS	SC9
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Staff attendance at any such meetings will be at the discretion of the Chief Executive Officer and where staff do attend it will be to provide technical advice only.

It will be the responsibility of the Mayor and Councillors present at these meetings to respond to or promote any Council statements or position.

Unless a formal position on the subject matter has been previously resolved by Council, no indication of Council support or otherwise can be portrayed by any Council representatives to third parties in attendance.

POL	COUNCIL OWNED VEHICLE USAGE	AES8
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POLICY CODE:	AES8
DIRECTORATE:	Executive Support Department
BUSINESS UNIT:	Executive Support Department
SERVICE UNIT:	Executive Support Services
RESPONSIBLE OFFICER:	Chief Executive Officer
FILE NO.:	182/001
DATE FIRST ADOPTED:	17 June 1997
DATE LAST REVIEWED:	10 March 2016
ATTACHMENTS:	N/A
DELEGATED AUTHORITY REF.:	AES8
VERSION NO.	4

Dates of Amendments / Reviews:	
DAPPS Meeting:	31 January 2013 27 February 2014 25 February 2016
OCM:	12 October 2006 14 February 2013 13 March 2014

BACKGROUND:

The City of Cockburn is required to provide a light vehicle fleet to ensure its functions can be performed in an efficient and effective manner.

PURPOSE:

To provide for out of hours allocation and usage of Council's light vehicle fleet.

POLICY:

- (1) The Chief Executive Officer and Directors have unrestricted use of a Council vehicle within the State of Western Australia of a suitable standard with all costs of purchasing or leasing and operating the vehicle being met by Council. These officers may also seek to have their contracts amended to allow for provision of a motor vehicle allowance, in lieu of a Council supplied vehicle. In such a circumstance, approval will require confirmation that this arrangement does not increase the City's costs, nor affect its operations.
- (2) Other officers of Council may be granted unrestricted private, restricted private or commuter use of a Council vehicle, such arrangements and standard of vehicle to be negotiated between the officer and the Chief Executive Officer. Occasional requests for usage outside these guidelines must be approved by the Divisional Director, and recorded on the relevant personal file of the employee prior to such extraordinary usage occurring.

POL	COUNCIL OWNED VEHICLE USAGE	AES8
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- (3) Where the Chief Executive Officer agrees to allow an officer who would normally only be allocated a vehicle with commuting use, access to restricted private use, then the Chief Executive Officer is to ensure a financial contribution is obtained from the officer that fully reimburses the City for any additional costs, eg FBT and ancillary operating expenses.
- (4) The vehicles allocated to the Chief Executive officer and Directors, or other officers granted unrestricted use, may be driven by:
 1. their partner;
 2. designated drivers approved by the Chief Executive Officer (or Mayor when applicable for the Chief Executive Officer) in writing; and
 3. other persons who have valid Western Australian driver's licences for the class of vehicle being driven and are accompanied by the officer when the vehicle is being driven.
- (5) Vehicles allocated to other officers who have restricted private use may be driven by:
 1. their partner if they are accompanied by the officer;
 2. their partner if they are picking up or dropping off the officer;
 3. their partner within 20 kilometres of their residence; and
 4. by designated drivers approved by the Chief Executive Officer in writing, who have a valid Western Australian driver's licence for the class of vehicle being driven and are accompanied by the officer when the vehicle is being driven,
- (6) Vehicles allocated to officers for commuting purposes only are to be driven by that officer only, or other persons who have valid Western Australian driver's licenses for the class of vehicle being driven and accompanied by the officer, between the place of residence and Council work place only and / or return, unless occasional permission is granted in accordance with part (2) of the Policy.
- (7) Officers allocated Council-owned and leased vehicles for private or commuting use are responsible for regular internal and external cleaning of the vehicle, maintaining it in a clean and tidy state and ensuring it is serviced at the correct intervals.
- (8) Should the Chief Executive Officer or an Officer with private use of a vehicle resign, the provisions of Part a) and b) of the Policy are not to apply and the Council vehicle is not to be taken prior to the employment termination date of the officer, in the event that accrued leave is taken by the Officer prior to the termination date.
- (9) Any Officer of Council having any form of private usage or commuting rights of a Council vehicle may, when absent from duty, grant these rights to another Officer of Council, as nominated by the authorising Officer and approved by

POL	COUNCIL OWNED VEHICLE USAGE	AES8
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the relevant Supervising Staff Member and provided to the delegated Officer in writing.

- (10) Any officer who is required to carry Council owned property within the vehicle shall at all times secure that property in the boot of the vehicle, (or some other such secure space within the vehicle), so as to reduce the likelihood of such equipment being stolen. Similarly, all officers should take the same precaution with their own personal effects, as the City of Cockburn's insurance policies will not cover the loss of such goods.
- (11) All damage to or incidents involving a Council owned vehicle must be reported immediately in accordance with OSH 4.5 Incident Reporting. Failure to comply with this requirement may result in disciplinary action.
- (12) 'For Cause' drug and alcohol testing will be carried out on the driver of any Council owned vehicle involved in an incident.
- (13) Unauthorised use of a Council owned vehicle will result in disciplinary action.

DA	COUNCIL OWNED VEHICLE USAGE	AES8
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DELEGATED AUTHORITY CODE:	AES8
DIRECTORATE:	Executive Services
BUSINESS UNIT:	Executive Services
SERVICE UNIT:	Executive Services
RESPONSIBLE OFFICER:	Chief Executive Officer
FILE NO.:	086/003
DATE FIRST ADOPTED:	1997
DATE LAST REVIEWED:	10 March 2016
ATTACHMENTS:	N/A
VERSION NO.	4

Dates of Amendments / Reviews:	
DAPPS Meeting:	24 May 2012 27 February 2014 25 February 2016
OCM:	9 April 2009 14 June 2012 13 March 2014

FUNCTION DELEGATED:

The authority to approve or negotiate private and commuting vehicle usage to staff to whom Council owned or leased vehicles are allocated.

CONDITIONS/GUIDELINES:

- (1) As provided in Policy AES8.
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegations Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Policy AES8.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AES8 "Council Owned Vehicle Usage" refers.

DELEGATE:

Chief Executive Officer.

DELEGATE/S AUTHORISED:

N/A

POL	PUBLIC INTERNET USE & WIRELESS ACCESS	SC59
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POLICY CODE:	SC59
DIRECTORATE:	Finance & Corporate Services
BUSINESS UNIT:	Information Services
SERVICE UNIT:	Information & Communication Technology
RESPONSIBLE OFFICER:	Manager, Information Services
FILE NO.:	182/001; 019/017
DATE FIRST ADOPTED:	
DATE LAST REVIEWED:	
ATTACHMENTS:	N/A
DELEGATED AUTHORITY REF.:	N/A
VERSION NO.	1

Dates of Amendments / Reviews:	
DAPPS Meeting:	
OCM:	

BACKGROUND:

The City of Cockburn is committed to serving the information access needs of all members of the community. The City is helping to bridge the digital divide in its community by providing to the public free wireless service to access the Internet ("the Service").

PURPOSE:

The City of Cockburn ("City") recognises that internet access is of growing importance to an increasingly mobile community and offers free public wireless service to access the Internet for basic browsing. The City's Guest Wireless ('Cockburn Free Wi-Fi') Access allows persons with wireless-enabled devices to connect to the Internet through the City's network resources. The City requires that public users ("Users") abide by the City's Public Internet Use and Wireless Access Policy ("the Policy"), as set forth herein. By use of public wireless service, Users acknowledge that they have read and agree to abide by the Policy.

POLICY:

City of Cockburn provides free wireless access to the internet through Wi-Fi. Although the Internet generally provides access to information that is valuable and enlightening, the User may encounter information that is controversial, offensive, disturbing, erroneous or illegal. Apart from basic firewall filtration, the City does not actively monitor and has no control over the information available on the Internet and is not responsible for the content, accuracy, or quality of information retrieved or

POL	PUBLIC INTERNET USE & WIRELESS ACCESS	SC59
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images viewed. Access to the internet by children under the age of 16 is the responsibility of the child's parent or guardian.

As a user of the City's free Wi-Fi service to the Internet, the User must abide by the Acceptable Use of Public Wi-Fi Internet Terms and Conditions. The City cannot guarantee the security of the wireless network at all times. The access and use of the Internet service is at the user's own risk.

The City respects the user's right to privacy and will not capture any personal information while the user is accessing the wireless Internet service unless consent is granted



ACCEPTABLE USE OF PUBLIC WI-FI INTERNET

TERMS & CONDITIONS

This Service (known as ‘Cockburn Free WiFi’) is provided to you free of charge by the City of Cockburn in areas enabled with a wireless local area network.

In these Terms and Conditions, any reference to “you” or “your” includes the owner and/or user of the computer or mobile device used to access this Service.

Access to and use of this Service is subject to the Terms and Conditions set out below. By accessing and using this Service, you agree to accept these Terms and Conditions without limitation. Each time you access and use the Service you are bound by The City of Cockburn’s Terms and Conditions. If you do not agree to these Terms and Conditions, you will not be able to access or use this Service.

LOGGING IN/OUT OF THE SERVICE

To use Service, you simply need to access this Service with your wireless enabled device and agree to accept these Terms and Conditions. You are not required to pay any fee to access the Service.

The City of Cockburn can only support a certain number of users accessing the Service at any one time. The City of Cockburn does not promise that you will be able to access and/or use the Service immediately or each time you visit a Service area. To log out or quit using the Service, you simply need to turn the Wi-Fi setting off on your mobile device or computer.

USER OBLIGATIONS AND LIMITATIONS

The Service is intended to be used in a fair and “community-minded” manner. To this end, The City of Cockburn may, in its absolute and sole discretion, limit or block certain websites and/or content that it considers not appropriate or may bring negative exposure or harm to the City of Cockburn’s reputation and/or brand. This includes but is not limited to websites and/or content that:

- Could cause The City of Cockburn to be in breach of any law, code or instrument which governs its conduct or to incur a liability to any third party;
- Could interfere with the integrity and/or performance of the Service or The City of Cockburn’s other networks or equipment;
- Depicts, alludes to or promotes offensive or illegal behaviour;
- Is offensive or promotes racism, bigotry, hatred or physical harm of any kind against any group or individual;
- Harasses or advocates harassment of another person;
- Exploits people in a sexual or violent manner;
- Contains nudity, violence, or offensive subject matter or which may contain links to adult websites;
- Promotes conduct that is abusive, threatening, obscene, defamatory or libellous;
- Promotes an illegal or unauthorised copy of another person’s copyrighted work (including but not limited to file sharing applications used for Bit-Torrent downloads/uploads or peer-to-peer applications generally);
- Involves the transmission of “junk mail”, “chain letters”, or mass mailing, instant messaging or “spamming”;

- Furthers or promotes any criminal activity or enterprise or provides instructional information about illegal activities including, but not limited to making or buying illegal weapons, violating someone's privacy, or providing or creating computer viruses;
- Contains any unwanted malicious software agents, such as viruses, Trojan horses, worms, etc. or other computer programming routines that may damage, modify, delete, detrimentally interfere with, surreptitiously intercept, access without authority or expropriate any system, data or personal information; or
- Uses sexually suggestive imagery or any other inappropriate, misleading or deceptive content.

The City of Cockburn may immediately terminate and block your future access to the Service if you use the Service in contravention of these Terms and conditions, attempt to manipulate or bypass any limitations of the Service by any means or behave in an aggravating, illegal, inappropriate or unsociable manner as a user of the Service.

SPEED AND RELIABILITY OF THE SERVICE

The City of Cockburn will use reasonable endeavours to provide you with a reliable, stable and secure access to the Service but does not promise that the access to this shared Service will be continuous, fault-free, secure or accessible at all times. This Service access is not suitable for supporting any application or use which requires continuous, fault-free network connectivity or uninterrupted data traffic flow. Without limiting any other provision of these Terms and Conditions, The City of Cockburn will not be liable if the Service access becomes unstable, un-secured, slow or unavailable for any reason whatsoever.

You should be aware that your free broadband internet access is dependent on several factors that govern its performance, such as the speed and throughput of each Service and the capabilities of your wireless enabled device; the physical location of the wireless access points in the area; Service user traffic during time of access, wireless signal interference and the location of your wireless enabled device within the area whilst accessing the Service. Each Service has limited range and to gain optimal access, it is recommended that your computer or mobile device accesses the Service within the confines of the area.

Service faults, malfunctions or other problems associated with the Service access will be attended to by the City of Cockburn at its earliest reasonable convenience. The City of Cockburn will not be liable if such faults, malfunctions or problems occur with the Service and is not obligated to rectify any such faults, malfunctions or problems associated with the Service within any specified timeframe.

SECURITY

Whilst The City of Cockburn will use reasonable endeavours to provide secure access through the Service, it cannot guarantee the security of the Service at all times. You are solely responsible for any information or data uploaded, downloaded or otherwise communicated via the Service and you are responsible for keeping all

usernames, passwords and other security-based information secure and private at all times.

Without limiting any other provisions of these Terms and Conditions, The City of Cockburn shall not, in any way, be liable to you for any kind of loss or damage incurred as a result of your use of the Service, including but not limited to any viruses you may become subject to during your use of the Service.

The City of Cockburn strongly recommends you install a personal firewall on your wireless enabled device to block unwanted traffic or downloads to your device.

PRIVACY

The City of Cockburn will only collect, use and store your personal information for the purposes of delivering the Service in accordance with applicable legislation and these Terms and Conditions. Without limiting the above, The City of Cockburn will not capture or attempt to capture any personal information whilst you access and/or use the Service without your consent.

However, once you access the Service and open your Internet Browser application to view a website, The City of Cockburn will capture and process information regarding your web browser type and/or operating system information as used by your wireless enabled device, in order to determine the most effective and/or customised way to display the requested webpage on your device.

The City of Cockburn may also collect and store the IP and MAC address of the wireless enabled device that has accessed the Service, once the Terms and Conditions of access have been agreed to.

The City of Cockburn is not in any way responsible for how any third party (including but not limited to any website that you access through the Service) collects and/or uses your personal information. You are solely responsible for checking the terms of use and privacy policy of each website you visit or online service used on a case by case basis and determining whether you accept such terms of use and/or privacy policies.

LIMITATION OF LIABILITY

The City of Cockburn makes no warranties or representations as the accessibility, security, stability or reliability of this Service and expressly disclaims any liability or responsibility for any Service faults, failures or interruptions or the accuracy, timeliness, completeness, security or reliability of any communications (including without limitation, any transactions) made by using Service.

Neither The City of Cockburn nor any other party involved in delivering the Service is liable for any direct, incidental, consequential, indirect or punitive damages arising out of your access to, or use of, or inability to use or access, the Service for any reason whatsoever, even if The City of Cockburn has been notified of, or advises of the possibility of such damages.

The City of Cockburn assumes no responsibility, and makes no warranty or representation in relation to, and shall not be liable in respect of your use of any third party proprietary software.

The City of Cockburn is not responsible for the content of any website accessed or used via the Service. Your access to and use of any website or network connection whilst using the Service is entirely at your own risk.

INDEMNITY

You agree to indemnify and hold harmless, The City of Cockburn and its personnel, from and against all loss, damage, liability, charge, expense or cost (including all reasonable legal and other professional costs on a full indemnity basis) of any nature or kind arising from your breach of these Terms and Conditions.

The following Legislative Framework underpins the authority of the City of Cockburn's 'Cockburn Free WiFi' service:

- Privacy Act 1988
- Privacy and Personal Information Protection Act 1998
- Telecommunications Act 1997 (Cth)
- Copyright Act 1968

While this position will endeavour to be implemented with consistency, due to the ever-changing nature of the Internet, all positions, rules and conditions are subject to change without notice and at the discretion of the City's Information Services management.

CITY OF COCKBURN



COCKBURN COMMUNITY EVENTS COMMITTEE

MINUTES

FOR

TUESDAY, 16 MAY 2017

CITY OF COCKBURN

SUMMARY OF MINUTES OF THE COCKBURN COMMUNITY EVENTS COMMITTEE MEETING HELD ON TUESDAY, 16 MAY 2017 AT 6:00 PM

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CITY OF COCKBURN**MINUTES OF THE COCKBURN COMMUNITY EVENTS
COMMITTEE MEETING HELD ON
TUESDAY, 16 MAY 2017 AT 6:00 PM**

PRESENT

Mrs C. Reeve-Fowkes	-	Deputy Mayor (Presiding Member)
Mr K. Allen	-	Councillor
Mrs L. Sweetman	-	Councillor

IN ATTENDANCE

Mr D. Green	-	Director, Governance & Community Services
Ms M. La Frenais	-	Events & Culture Co-ordinator
Ms Sandy Edgar	-	Events Officer

1. DECLARATION OF MEETING

The Presiding Member declared the meeting open at 6.30 pm.

2. APPOINTMENT OF PRESIDING MEMBER (If required)

Nil.

3. ACKNOWLEDGEMENT OF RECEIPT OF WRITTEN DECLARATIONS OF FINANCIAL INTERESTS AND CONFLICT OF INTEREST (by Presiding Member)

Nil

4. APOLOGIES AND LEAVE OF ABSENCE

Clr Stephen Pratt	-	Apology
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5. CONFIRMATION OF MINUTES

5.1 (CCEC 16/5/2017) – MINUTES OF THE COCKBURN COMMUNITY EVENTS COMMITTEE MEETING - 30 MAY 2016

RECOMMENDATION

That Council confirm the Minutes of the Cockburn Community Events Committee Meeting held on Thursday, 30 May 2016 as a true and accurate record.

COMMITTEE RECOMMENDATION

MOVED Clr L Sweetman SECONDED Clr K Allen that the recommendation be adopt.

CARRIED 3/0

6 BUSINESS LEFT OVER FROM THE PREVIOUS MEETING (If adjourned)

Nil

7. DECLARATION OF COUNCILLORS WHO HAVE NOT GIVEN DUE CONSIDERATION TO MATTERS IN THE BUSINESS PAPER

Nil

8. COUNCIL MATTERS

8.1 (CCEC 16/5/2017) – PROPOSED CALENDAR OF EVENTS 2017/18 (152/010) (S SEYMOUR-EYLES) (ATTACH)

RECOMMENDATION

That Council adopt the proposed 2017/18 Events Calendar, as identified in the report.

COMMITTEE RECOMMENDATION

MOVED Clr L Sweetman SECONDED K Allen that the recommendation be adopt, subject to the following amendments:

- (1) minor amendments to the schedule to correct typographical and formatting errors; and



(2) the following artists be prioritized for the community concerts:

1. John Farnham
2. San Cisco
3. Human Nature
4. Birds of Tokyo
5. Joe Camilleri
6. Sheppard
7. Hoodoo Gurus
8. Jessica Mauboy
9. Guy Sebastian
10. Troye Sivan
11. Leo Sayer

CARRIED 3/0

COUNCIL DECISION

Reason for Decision

1. There were minor typographical errors in the comment column for the Christmas on the Green event and an error in the sequencing number for the Seniors Social Evenings.
2. It is a function of the Committee to recommend preferred artists for the Community Concerts.

Background

Council is required to determine the Calendar for the 2017/18 events season, as per Budget Policy SC34, which states a "Provisional allocation for Community Events is to be a maximum of 1.0% of Rates Revenue. Council to approve the calendar of events."

The Community Events and related expenses tabled below are funded from this budget. Any other City run events are funded from separate budgets.

The Events team has developed the following proposal for the 2017/18 program of events, based on:

- A review of the 2016/17 season
- Feedback from surveys
- Staff de-brief of the events

- Feedback from people at events/on social media

It is necessary to consider the calendar early in the financial year (July), because:

- It is preferable that marketing for the season commences in September (Fur Run), therefore adequate time is required for marketing material to be produced in advance.
- October-November Events are included in Cockburn Soundings October-November edition, which is prepared in August.
- Corporate Communications will apply to Healthway for funding for the 2017/18 season. Council needs to have determined the season of events before applications are submitted. These applications require around four months lead-in time and then adequate time to feature these organisations on promotional material should a sponsorship agreement require.

Submission

N/A

Report

Proposed 2017/18 Events

Below is the proposed calendar of events. This includes events for the coming financial year and related budget.

Event Name Location	Date	Budget ex-GST	Comments
<i>Fur Run</i>	24 September 2017	OP 8992 \$9,000	Encourage healthy dogs and provide information for dog owners. Manning Park
<i>Seniors Social Evening 3</i>	September 2017	OP 9492 \$12,000	Different theme; entertainment, buffet meal, raffles & prizes. 5.30pm – 11pm. Dalmatinac Club Tickets \$10.00.
<i>Side Splitter</i>	28-29 October and 4-5 November 2017	OP 8854 \$25,000	Comedy festival to be held at Memorial hall. Includes a 16+ free comedy workshop.
<i>Teddy Bears Picnic</i>	25 October	OP 9307	10am - 1pm –



Event Name Location	Date	Budget ex-GST	Comments
	2017	\$27,000	Manning Park Entertainment and rides free for pre-school kids, activities, amusements, arts, parenting information.
<i>Christmas on the Green</i>	9 December 2017	OP 9460 \$33,000	Indigenous Christmas at MacFaull Park.
<i>Australia Day Coogee Beach Festival</i>	26 January 2018	OP 9107 \$83,000	8am – 12am. Free sausage sizzle, free rides, entertainment, family activities. Coogee Beach Reserve.
<i>Community Concert</i>	February 2018	OP 9476 \$160,000	Cockburn Central Legacy Park, 7pm – 10pm.
<i>Coogee Live</i>	March 2018	\$160,000	Coogee Coast, 3pm – 10pm daily (3 days).
<i>Cultural Fair</i>	7 April 2018	OP 9108 \$46,000	Harmony Oval Harvest Lakes. Flavours of Cockburn theme. Have stalls selling a mixture of cuisines. Gourmet food and cooking demonstrations as well as arts and crafts. Final of Cockburn's Got Talent.
<i>Seniors Social Evening 1</i>	May 2018	OP 8855 \$12,000	Different theme; entertainment, buffet meal, raffles & prizes. 5.30pm – 11pm. Dalmatinac Club Tickets \$10.00.
<i>Seniors Social Evening 2</i>	July 2018	OP 8856 \$12,000	Different theme; entertainment, buffet meal, raffles & prizes. 5.30pm – 11pm. Dalmatinac Club. Tickets cost \$10.00 to purchase.
<i>Marketing, research, detailed concept and Insurance for the major events plus</i>		OP9021 \$125,000	Marketing for all events, insurance and miscellaneous expenses which may include research.

Event Name Location	Date	Budget ex-GST	Comments
<i>miscellaneous</i>			
Pop up events x 3 Various locations	TBA to fit in with reserve availability and other events	OP 8857 \$10,500	Simple pop up art and music with coffee and stall food offering. Sites to be determined at a later date
Total		\$714,500	

All acts are subject to availability and budgets, as staff can only confirm and book them after a Council decision. The date for the main concert is subject to change as the availability of acts dictates this to some extent. These are finalised before the annual printed calendar is distributed.

The number of events in this program is designed according to budget and to enable the two Event Staff to manage them, in addition to the other events that they organise or assist with during the year. These include Celebrate Ability, Bibra Lake Fun Run, Hiroshima Day, Cockburn's Got Talent heats x 2, Show Off, Spring Fair, ANZAC Services and official openings. Funding of these events is not taken from the allocated budget for those captured by Policy SC34.

In 2017/18, it is proposed that the events calendar program follows a new format in terms of introducing a new event and reducing the number of community concerts to one.

Dates have been considered in light of key events around Perth that are currently known, such as major sporting events and community events, as well as other City of Cockburn events which the City supports.

The recommendation is that the City continues with one major concert to be held at Cockburn Central, Legacy Park and one major arts and cultural light festival, Coogee Live, along the Cockburn coast. This proposed event is further detailed under the heading 'New Events 2017/18' later in this report.

The list of proposed artists for the concert can be found as an attachment to this document. The potential considerations and nomination of top four concert artists is to be decided in this meeting for presentation to Council in June 2017.

The support act choice would be determined by the cost of the main act. It would be a local Perth band or tribute style.



The Program retains three seniors' evening events. The tickets currently cost the City \$45 per person and each person pays a subsidised cost per ticket, which goes towards prizes and giveaways on the night. Last year the cost was \$10.00 per ticket. It is proposed to keep the price to \$10.00 per person for 2017-18. This is based on the capacity of the Dalmatinac Club and the sale of 270 tickets. This means each ticket will cost the City \$35 per person. As the tickets are highly sought after, the process is refined each year to ensure, as far as possible, that only Cockburn residents attend and that there is a waiting list for those who miss out on the previous event.

The following events are retained in current format due to their popularity and good attendance:

- Teddy Bear's Picnic;
- Seniors Events;
- Pop Ups x3;
- Side Splitter;
- Christmas on the Green;
- Cultural Fair;
- Fur Run;
- Australia Day Coogee Beach Festival

New events 2017/18

Coogee Live

Coogee Live is proposed to be a three-day festival that will showcase the Cockburn coast through creative activities such as an innovative lighting and laser display, theatre and art exhibitions, and a hawkers market.

Letters of support have been received from Coogee Beach Progress Association, Friends of the Community and Coogee Beach Surf Life Saving Club.

Comment on Cockburn results- survey open from 5 May – 10 April

- 97.1% of respondents support the idea of Coogee Live
- 80.8% of respondents support the City hosting one concert, instead of two, to enable Coogee Live to proceed, 12.5% said no and 6.7% had no opinion.
- 28.8% of respondents were from Coogee, 25.4% from Spearwood, 18.6% from North Coogee, 16.9% from Yangebup and 10.2% from Success

Preliminary sponsorship funding yet to be finalised however a possible budget has been formulated on the following:

- \$170K (COC, \$160K events budget plus \$10K from cultural budget/grant for artists in residence program)
- \$45K Lotterywest
- \$45K Healthway
- \$35K Fremantle Ports, Landcorp, Murdoch University.

If sponsorship is not gained from either Lotterywest or Healthway the event would not be able to proceed and the City will revert back to a second concert sourcing an artist from the approved four as recommended by the committee. If sponsorship was approved from Healthway and Lotterywest yet were unable to gain support from some of the smaller organisations the event could still proceed on a smaller scale.

Marketing / Insurance/ Research/ Concept Development

The marketing plan will include traditional advertising, use of Facebook, the annual calendar, mini billboards in parks, posters and promoted at other events. New event detailed concept design (for 17-18) and event surveying as well as insurance is covered in this component of the budget (\$125K).

Healthway Funding

The City was successful in securing \$8,000 in sponsorship funding for the Cultural Fair and will continue to seek this partnership in 2018. Healthway have indicated that Coogee Live would not impact Cultural Fair funding but this is not confirmed at this stage.

Strategic Plan/Policy Implications

Community & Lifestyle

- Community environments that are socially cohesive and embrace diversity.
- Communities that are connected, inclusive and promote intergenerational opportunities.
- Communities that take pride and aspire to a greater sense of community.
- People of all ages and abilities to have equal access to our facilities and services in our communities.
- Promotion of active and healthy communities.
- The significance and richness of our local Indigenous people and diverse multicultural community will be recognised and celebrated.



Budget/Financial Implications

\$714,500 including marketing, event concept development and insurance.

Legal Implications

N/A

Community Consultation

In 2016 the Community Perceptions Survey (Catalyse) showed 89% of those surveyed were familiar with festivals, events and cultural opportunities in the City of Cockburn. 22% responded excellent, 42% responded Good and a further 25% responded ok.

Survey research was undertaken specifically for the Australia Day Coogee Beach Festival. 45% responded 'very satisfied' and 47% 'satisfied'.

Survey research was undertaken specifically for the 'Success Community (Dami Im) in February. 47% responded 'very satisfied' and 42% 'satisfied'.

Risk Management Implications

The risk in not making a decision on the program, enabling the program to be adopted at the June 2017 Council meeting is a delay in booking acts, information being left out of the annual City of Cockburn calendar, and preventing the events team from starting to plan the event.

The risk of not considering new events is that Council is not seen to be listening to community or market trends.

Attachment(s)

1. List of Performers
2. Coogee Live final proposal
3. Consultation analysis Coogee Live
4. Side Splitter post event analysis

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

It is appropriate for Council to provide entertainment activities for its community on a free or subsidised cost basis.

9. NEW BUSINESS OF AN URGENT NATURE INTRODUCED BY DECISION OF MEETING BY MEMBERS OR OFFICERS

Nil

10. CLOSURE OF MEETING

8.26 pm.



EXIST. SITE CALCULATIONS

SITE AREA: 1978m²

DEMOLITION NOTES

- ALL REDUNDANT SERVICES TO BE REMOVED. SERVICES ARE TO BE TERMINATED AT SOURCE (WHERE APPLICABLE) AND MADE SAFE.
- MODIFIED BRICKWORK TO BE TOOTHED INTO EXISTING WHERE APPLICABLE AND CAVITY TO REMAIN CONTINUOUS AT ALL TIMES.
- MATERIALS REMOVED FROM SITE MUST BE DISPOSED OF AS PER COUNCIL REGULATIONS.
- INVESTIGATION SHOULD BE UNDERTAKEN BEFORE ALL WORKS THAT REQUIRES EXCAVATION.
- CONTRACTOR TO CLEANUP AND MAKE GOOD TO ALL AREAS AFFECTED BY THE DEMOLITION.

NOTES:

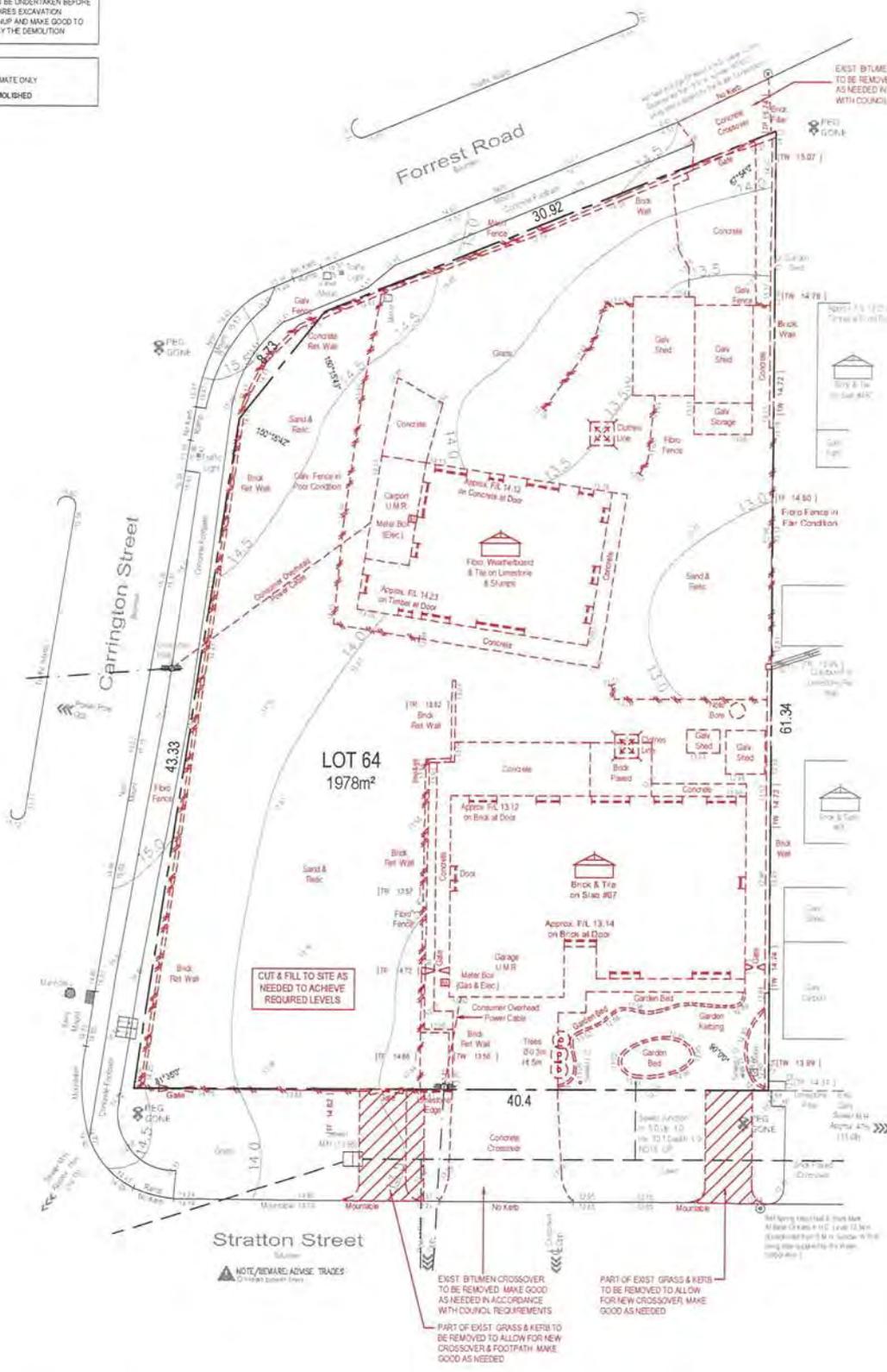
DIMENSIONS ARE APPROXIMATE ONLY

--- TO BE DEMOLISHED

OCM 8/6/2017 - Item No.15.1 Attach 1

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EXIST. / DEMOLITION SITE PLAN
SCALE: 1:200

SA1	PLANNING APPROVAL - COUNCIL RESPONSE	2014	2014.10
SA2	SOURCE FURNISHING MATERIAL RESPONSES	2014	2014.11
SA3	PLANNING APPROVAL - BIDE TO GOVMSL	2014	2014.11
REV	ISSUED FOR DETAILS	01	2017

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www.rmilford.com.au info@rmilford.com.au

PROP. CHILD CARE CENTRE
LOT 64 (NO. 67) CNR STRATTON & CARRINGTON ST, HAMILTON HILL

ROTTO INVESTMENTS P/L
EXIST. / DEMOLITION SITE PLAN

DESIGNED BY
R.MILFORD

SCALE
1:200

DATE
A2

PROJECT NO.
16.009

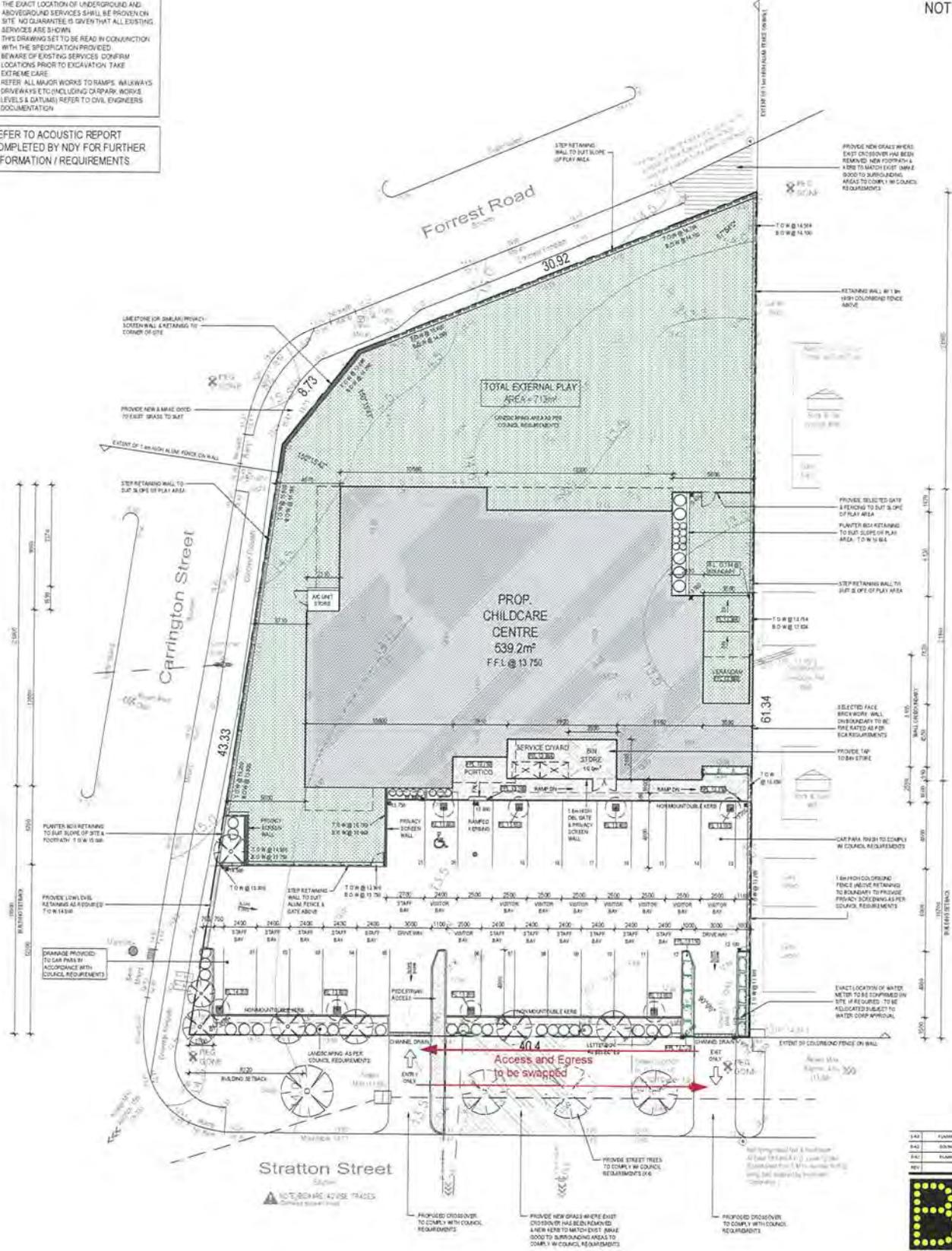
DATE OF ISSUE
A1.01

REVISION
DA.3

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- SITE NOTES:**
- CONFIRM ALL DIMENSIONS ON SITE PRIOR TO COMMENCEMENT OF ANY WORK.
 - RELEVANT PLUMBING AND ELECTRICAL WORKS TO BE REMOVED AND MADE GOOD AS NECESSARY.
 - THIS PLAN IS TO BE READ IN CONJUNCTION WITH LANDSCAPING DETAIL PLANS FOR SPECIFIC PLANTING LOCATIONS.
 - THE EXACT LOCATION OF UNDERGROUND AND ABOVEGROUND SERVICES SHALL BE PROVIDED ON SITE. NO GUARANTEE IS GIVEN THAT ALL EXISTING SERVICES ARE SHOWN.
 - THIS DRAWING SET TO BE READ IN CONJUNCTION WITH THE SPECIFICATION PROVIDED.
 - BE AWARE OF EXISTING SERVICES. CONFIRM LOCATIONS PRIOR TO EXCAVATION. TAKE EXTREME CARE.
 - REFER ALL MAJOR WORKS TO RAMPS, WALKWAYS, DRIVEWAYS ETC. (INCLUDING CURBWORK, MOWLA LEVELS & DATUMS) REFER TO CIVIL ENGINEERS DOCUMENTATION.

REFER TO ACOUSTIC REPORT COMPLETED BY NDY FOR FURTHER INFORMATION / REQUIREMENTS.



PROPOSED SITE PLAN
SCALE - 1:200

CHILD CARE CALCULATIONS

TOTAL # OF STAFF = 12
TOTAL # OF CHILDREN = 190

AREA PROVIDED

ROOM	CHILD QTY	SIZE
PLAYROOM 001	x 12	42.3m²
PLAYROOM 002	x 16	65.1m²
PLAYROOM 003	x 20	82.0m²
PLAYROOM 004	x 22	77.7m²
PLAYROOM 005	x 20	54.2m²
INTERACTIVE KIT (inc. Above)		20.0m²

TOTAL INTERNAL PLAY AREA = 326.3m²
@ 20m² per child
TOTAL EXTERNAL PLAY AREA = 713.0m²
@ 3.7m per child

PLEASE NOTE:
Floor area shown include unutilized space only.

PROP. SITE CALCULATIONS

SITE AREA	1979m²
BUILDING AREAS	
FLOOR AREA	529.2m²
VERANDA/ PORCH	11.4m²
BNR STORE	10.0m²
SERVICE YARD	12.7m²
ANG STORE	3.0m²
TOTAL	588.2m²
CAR PARKING (LPP 3:1 - Child Care Centres)	
CAR BAYS REQUIRED	
1 BAY PER 10 CHILDREN (x 20)	x 9
1 BAY PER STAFF MEMBER (x 12)	x 12
TOTAL	x 21
CAR BAYS PROVIDED (Including 1 x PWD Bay)	x 21

PROP. CHILD CARE CENTRE
LOT 84 (NO. 67) CNR STRATTON &
CARRINGTON ST, HAMILTON HILL

ROTTO INVESTMENTS P/L

PROP. SITE PLAN

R MILFORD
R MILFORD

1:200 A2

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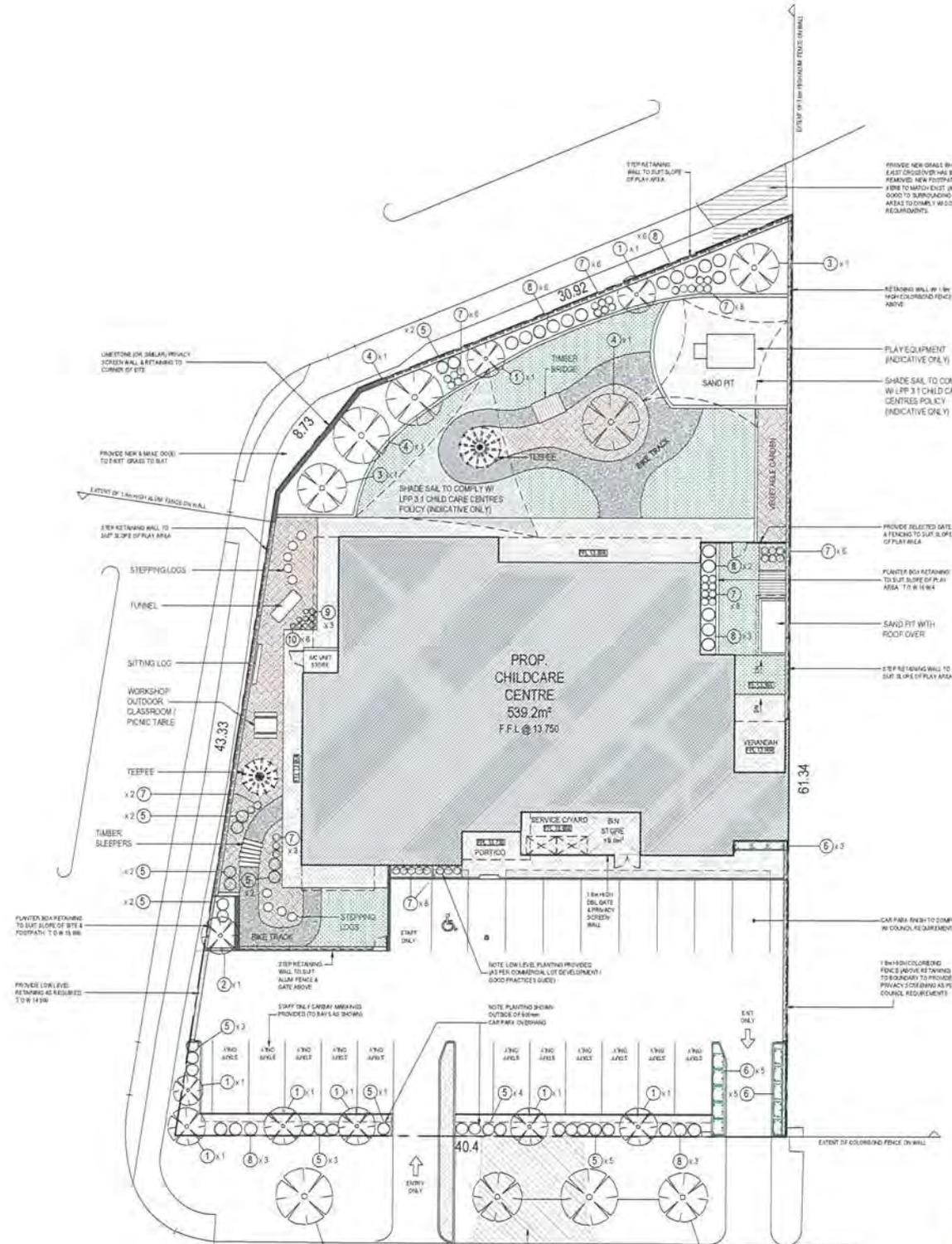
343	PLANNING APPROVAL - COUNCIL REVIEW	09/04/20	01/02/20
342	GENERAL PLANNING APPROVAL REVIEW	08/04/20	01/02/20
341	PLANNING APPROVAL - CNR TOWNCLERK	08/04/20	01/02/20
340	ARCHITECTURAL DETAILS	01/02/20	01/02/20



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SCALE	PLANNING APPROVAL - COUNCIL PERMIT ONLY	DATE	16.09.17
SCALE	QUANTITY SCHEDULE APPROVAL - PERMIT ONLY	DATE	16.09.17
SCALE	LANDSCAPE APPROVAL - SEND TO COUNCIL	DATE	16.09.17
REF	LANDSCAPE DETAILS	BY	DA3



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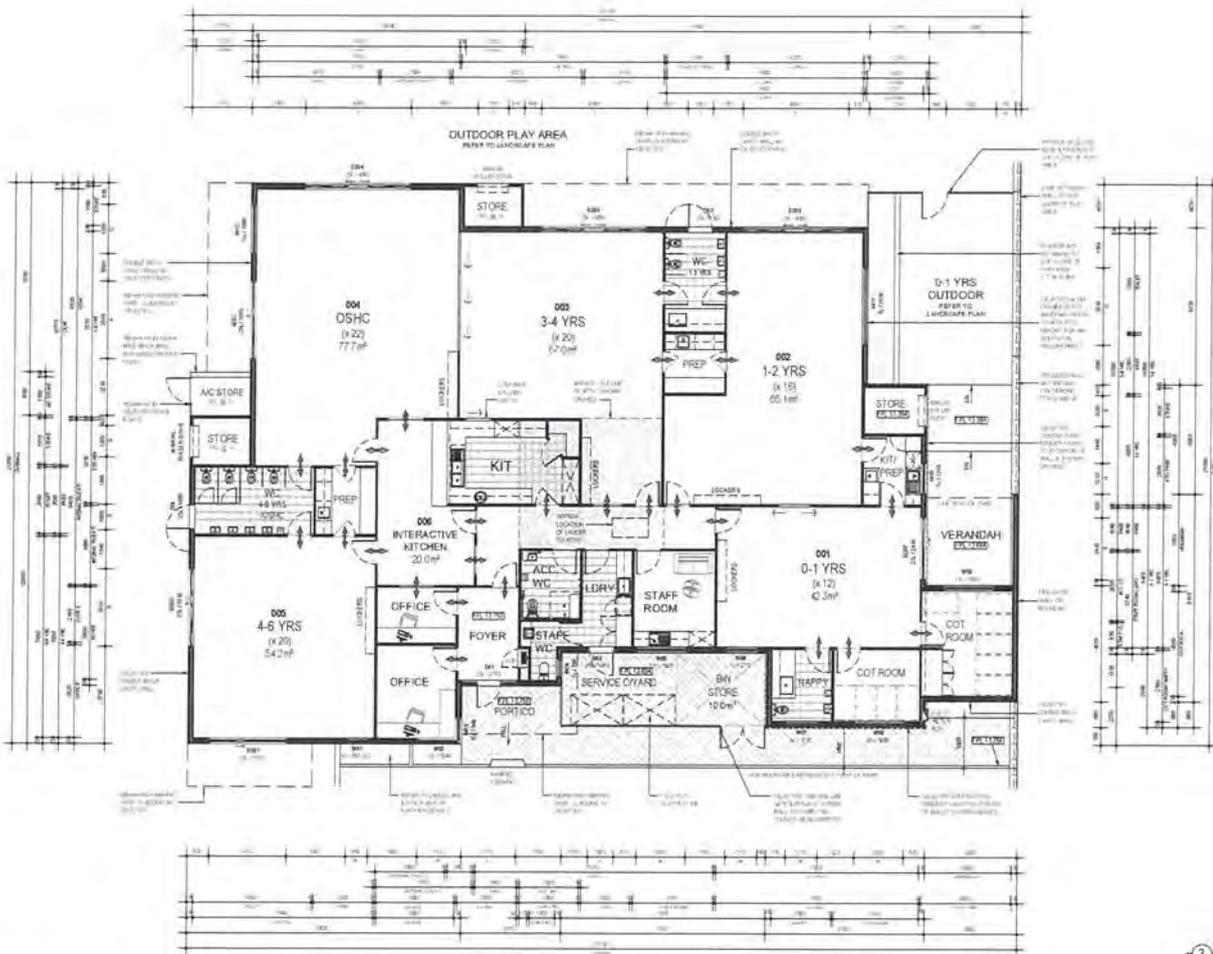
ROTTO INVESTMENTS P/L
PROP. LANDSCAPE PLAN

1:200 A2
16.009 A1.03 DA.3
16.009 A1.03 DA.3

PROPOSED LANDSCAPE PLAN
SCALE - 1:200

REF	SPECIES	SIZE	SPACING	QTY
TREES				
1	PRUNUS CALLERYANA REDSPINE OR CHANTICLEER	30-40L	AS SHOWN	x 6
2	PRUNUS CERASIFERA 'NIGRA'	30-40L	AS SHOWN	x 1
3	PRUNUS CALLERYANA REDSPINE OR CHANTICLEER	100L	AS SHOWN	x 2
4	SAPINDUS SERRIFOLIUM 'CHINESE TALLOW'	10L	AS SHOWN	x 3
PLANTINGS				
5	WESTRINGIA LOW HORIZON	150mm	800mm	x 26
6	SYZYGIUM TINY TREY	150mm	AS SHOWN	x 13
7	DIANELLA LITTLE JEES	150mm	500mm	x 47
8	POLYGALA LITTLE BOP	170mm	300mm	x 22
9	GERBERA EVERLAST	150mm	500mm	x 3
10	DIANELLA 'COOL VISTA'	150mm	500mm	x 6

LANDSCAPE FINISHES LEGEND	
	CONCRETE (SELECTED COLOUR)
	BRICK PAVING
	TIMBER DECKING / PLATFORM BRIDGE (AS NOTED)
	GRASS (PROVIDE ASTRO TURF AS NOTED IN DRAWINGS)
	WOOD CHIPS OR MULCH (PROVIDED TO SUIT)



PROPOSED FLOOR PLAN
SCALE 1:100

NOT FOR CONSTRUCTION

CHILD CARE CALCULATIONS

TOTAL # OF CHILDREN 78

TOTAL # OF CHAIRS 18

AREA CALCULATIONS

ROOM	AREA	NO.
CLASSROOMS	4.0	42.5m²
CLASSROOMS	4.0	63.5m²
CLASSROOMS	4.0	57.5m²
CLASSROOMS	4.0	77.5m²
CLASSROOMS	4.0	54.2m²
CLASSROOMS	4.0	20.0m²

TOTAL INTERNAL PLAY AREA 28.3m²
22.2m² net area

TOTAL EXTERNAL PLAY AREA 70.0m²
(170m² net)

Area = 100
Net area value includes construction of glass only

LEGEND

→ INDICATES VISUAL ACCESS BETWEEN ROOMS

REFER TO ACUSTIC REPORT COMPLETED BY NDV FOR FURTHER INFORMATION / REQUIREMENTS



PROP. CHILD CARE CENTRE
LOT 64 (NO. 67) CHELSEA STRATTON &
CARRINGTON - HAMILTON HILL

ROTTI INVESTMENTS PT. A.

PROP. FLOOR PLAN

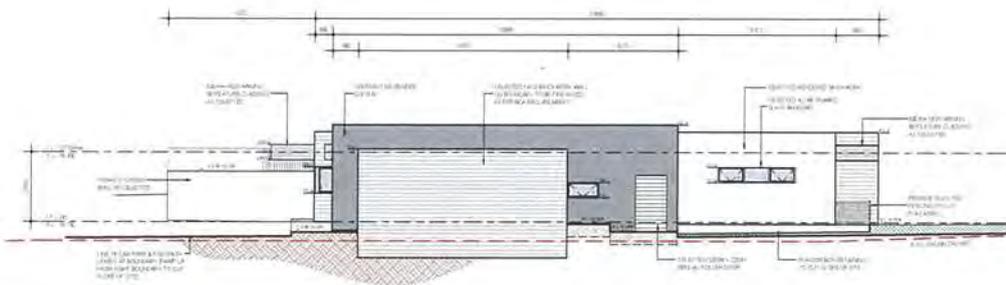


8 MELFORD
8 MELFORD
SCALE 1:100
16.009 A2.01 DA.3

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1. FRONT ELEVATION
SCALE: 1:100



2. SIDE ELEVATION
SCALE: 1:100

NO.	DESCRIPTION	DATE	BY
1	ISSUED FOR PERMIT	16/06/2017	RM
2	ISSUED FOR PERMIT	16/06/2017	RM
3	ISSUED FOR PERMIT	16/06/2017	RM



COMMERCIAL & RESIDENTIAL BUILDING DESIGN
PROPERTY DEVELOPMENT & PROJECT MANAGEMENT

PROP. CHILD CARE CENTRE
LOT 64 (INC. ST) OUR STRATTON &
CARRINGTON ST. HAMILTON HILL

ROTTO INVESTMENTS P/L

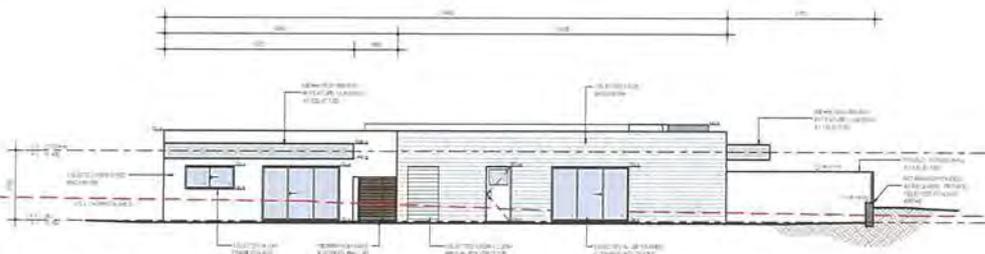
PROP. ELEVATIONS

RM/LP/RD
RM/LP/RD
SCALE: 1:100
DATE: 16/06/2017
16 009 A3.01 DA.2

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3. REAR ELEVATION
SCALE 1:100



4. SIDE ELEVATION
SCALE 1:100

NO.	DESCRIPTION	DATE	BY
1	ISSUED FOR PERMIT	16/06/17	R.M.F.
2	REVISED PER PERMIT	16/06/17	R.M.F.
3	REVISED PER PERMIT	16/06/17	R.M.F.
4	REVISED PER PERMIT	16/06/17	R.M.F.

COMMERCIAL & RESIDENTIAL BUILDING REGULATIONS
SECTION 71(1)(b) - BUILDING HEIGHTS

PROP. CHILD CARE CENTRE
LOT 54 (NO. 87) CARRINGTON & CARRINGTON ST. HAMILTON HILL

ROTTO INVESTMENTS P/L

PROP. ELEVATIONS

R.M.F. 16.009
R.M.F. 16.009
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A2
16.009 A3.02 DA.2



Perspective 1



Perspective 2

Report

Acoustics

HAMILTON HILL CHILDCARE CENTRE DA

Peter Betz



CONFIDENTIAL

Revision: 3.0 - Issue
Issued: 11 May 2017





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1 EXECUTIVE SUMMARY

NDY has undertaken a noise assessment of a proposed Childcare Centre at the corner of Carrington Street and Stratton Street in Hamilton Hill WA.

1.1 Purpose

The purpose of this report is to provide acoustic design input into the following areas as requested by council:

- Noise emission from outside play areas to neighbouring noise sensitive properties
- Noise emission from outdoor mechanical services units to neighbouring noise sensitive properties
- Control of external noise intrusion and facade design

1.2 Major Recommendations

NDY have modelled environmental noise emissions for various scenarios and we forecast compliance with the applicable noise limits with the following operational measures implemented:

- Children aged 2-6 years playing outside are limited to:
 - No more than 25 children playing outside for no more than 1 hour for every 4 hours.
- Toddlers aged 1-2 years playing outside are limited to:
 - No more than 15 toddlers playing outside (in the toddlers fenced area) for no more than 1 hour for every 4 hours.
- That is a total 40 for 1 hour of every 4 hours, and 120 for a full day if you consider 3 x 1 hour outdoor sessions spaced by 3 hours each (e.g. 1x hour at 08.00; 1x hour at 12.00; 1x hour at 16.00)

To control noise to the adjacent properties, the play area is recommended to be enclosed with the following construction (boundary facing Forrest Road):

- 400mm high retaining wall or higher; and,
- 1.6m high solid continuous barrier (e.g. Colorbond fence) or higher – extend highlighted in blue in Figure 5.

In addition, NDY has made recommendations on facade and glazing to control noise intrusion to the internal noise levels that are specified in AS/NZS 2107:2000 as follows:

- Minimum 10.38mm laminated glazing (or equivalent to R_w 36) for the windows of sleeping rooms (W08 and W09)
- Minimum 6mm float glazing (or equivalent to $R_w + C_{tr}$ 28) for the all other windows
- Minimum $R_w + C_{tr}$ 35 (e.g. 90mm acoustibrick -50mm air gap – 90mm acoustibrick) for all non-vision facade areas facing the road.



To control noise emissions from outdoor mechanical plant NDY recommends the following:

- Limit use of air conditioning to day time. If used outside daytime hours (07.00 to 19.00 hours) ensure the silent option is enabled.
- Units to be selected so that their combined noise emission is no more than 65dBA @ 1 metre (all units in operation). If units are selected to be louder further acoustic review is required to demonstrate compliance with WA environmental noise regulations.

1.3 Conclusion

Environmental noise emissions with the operation limits and recommended Colorbond fence along Forrest Road are forecast to result in compliance with the WA Environmental Protection (Noise) Regulations 1997 at all nearby noise sensitive receivers.

The mechanical contractor is required to select units with a combined noise emissions of no more than 65dBA sound pressure level at 1 metre. If a noisier outdoor unit is selected further input from an accredited acoustic consultant will be required to demonstrate compliance with the environmental noise regulations.

NDY have undertaken background noise levels measurements over an extended period of time and note that the average daytime L_{A10} noise levels are above the assigned noise levels for this premises.

Noise emissions from the child care centre are therefore not considered to be intrusive to neighbouring houses. However, it is likely that children playing noises will be audible in the adjacent premises. In view of the *2009 AAAC Child care centre noise assessment technical guideline (November 2009)*, this forms a suitable outcome because it does not result in any practical increase of daily background noise levels.



2 INTRODUCTION

This report documents environmental noise performance requirements and compliance measures for the proposed Childcare Centre on the corner of Carrington Street and Stratton Street in Hamilton Hill WA.

This acoustic report outlines how the proposed development has been assessed for the purposes of minimising the effects of noise intrusion and / or noise emission, covering state noise policy and local planning policy requirements.

2.1 Required and Recommended Design Standards

This report has been written with reference to the following mandatory and recommended documents, which set out various acoustic criteria for residential buildings.

Required:

- *WA Environmental Protection (Noise) Regulations 1997*

Recommended:

- Australian / New Zealand Standards including *AS/NZS 2107:2000 – Recommended design sound levels and reverberation times for building interiors*
- *AAAC Child care centre noise assessment technical guideline (November 2009)*

2.2 Authority

- Authority to undertake this report was provided by Peter Betz of Rotto Investments Pty Ltd.

2.3 Information Sources

This report was written with reference to the following documents:

- Updated Architectural Drawings, received by email from Rebecca Milford on 27 January 2017.

2.4 Acoustic Engineer Accreditation

Modelling and reporting was conducted by Ben Hillion of Norman Disney & Young. The author holds relevant qualifications from undergraduate and postgraduate study in engineering acoustics. The author is a full Member of the Australian Acoustical Society (MAAS).

NDY Acoustics Perth is a member firm of the Association of Australian Acoustical Consultants (AAAC).

2.5 Revision History

Revision	Date Issued	Comment
1.0	04 February 2017	Draft issue - For client comments
2.0	07 February 2017	Final Issue –For DA submission
3.0	11 May 2017	Final issue – includes car park noise assessment



The assigned noise levels were determined on the basis of an Influencing Factor of 6dB applicable at the nearest noise receivers, comprising:

- A transport factor of 6 dB on the basis of Carrington Road being a major road within the inner circle;
- A minimum commercial zoning factor of 0 dB, due to absence of land zoned commercial within either circle and;
- A minimum industrial zoning factor of 0 dB, due to absence of land zoned industrial within either circle.

Table 1 – Preliminary Assigned Noise Levels Summary

Part of Premises Receiving Noise	Time of Day	Assigned Level (dBA)		
		L _{A10}	L _{A1}	L _{Amax}
Noise sensitive premises at locations within 15 metres of a building directly associated with a noise sensitive use	0700 to 1900 hours Monday to Saturday	51	61	71
	0900 to 1900 hours Sunday and public holidays	46	56	71
	1900 to 2200 hours all days	46	56	61
	2200 hours on any day to 0700 Monday to Saturday and 0900 hours Sunday and public holidays	41	51	61
Noise sensitive premises at locations further than 15 metres from a building directly associated with a noise sensitive use.	All hours	60	75	80
Commercial premises	All hours	60	75	80
Industrial and utility premises	All hours	65	80	90



4 ASSESSMENT

4.1 Source identification and scope

We identify from the planning submission documents considered noise sources with potential to create noise impact as including (in no set order):

- Children playing outdoors in children play area, situated at the back of the development
- Plant associated with the main building – air conditioning outdoor units are proposed to be housed along the Carrington Road facade on ground level.

Following preliminary consideration of all services and activities and the times at which they occur, scenarios likely to cause significant / notable impact have been considered as per Table 2.

Table 2 - Assessment scope -week

Scenario	Source / activity	Expected operation	Scenario(s) assessed
1	Children playing Outdoor	07.00 to 18.00 hrs Monday to Friday	Up to 25 children playing up to 1 hour per half day (i.e. no more than 1 hour every 4 hours)
2	Toddlers playing Outdoor	07.00 to 18.00 hrs Monday to Friday	Up to 15 toddlers playing up to 1 hour per half day (i.e. no more than 1 hour every 4 hours)
3	Air conditioning outdoor unit	07.00 to 18.00 hrs Monday to Friday	All units in use (assumed worst case)
4	Cars activity in car park	07.00 to 18.00 hrs Monday to Friday	Voices from small groups of people accessing/leaving Noise from door closing

Table 3 - Assessment scope -weekend

Scenario	Source / activity	Expected operation	Scenario(s) assessed
1	Children playing Outdoor	08.00 to 16.00 hrs Saturdays	Up to 25 children playing up to 1 hour per half day (i.e. no more than 1 hour every 4 hours)
2	Toddlers playing Outdoor	08.00 to 16.00 hrs Saturdays	Up to 15 toddlers playing up to 1 hour per half day (i.e. no more than 1 hour every 4 hours)
3	Air conditioning outdoor unit	08.00 to 16.00 hrs Saturdays	All units in use (assumed worst case)
4	Cars activity in car park	08.00 to 16.00 hrs Saturdays	Voices from small groups of people accessing/leaving Noise from door closing



From a review of all environmental noise emissions anticipated from the proposed development, we find that compliance with set criteria is expected where the comparison level for each scenario does not exceed the Assigned levels listed in Table 1 for each time period and assessment location (indicated in Figure 1).

The comparison level of each source has been developed with regard to the most 'at-risk' and relevant assigned level descriptor.

4.2 Basis of Childcare Centre Operations

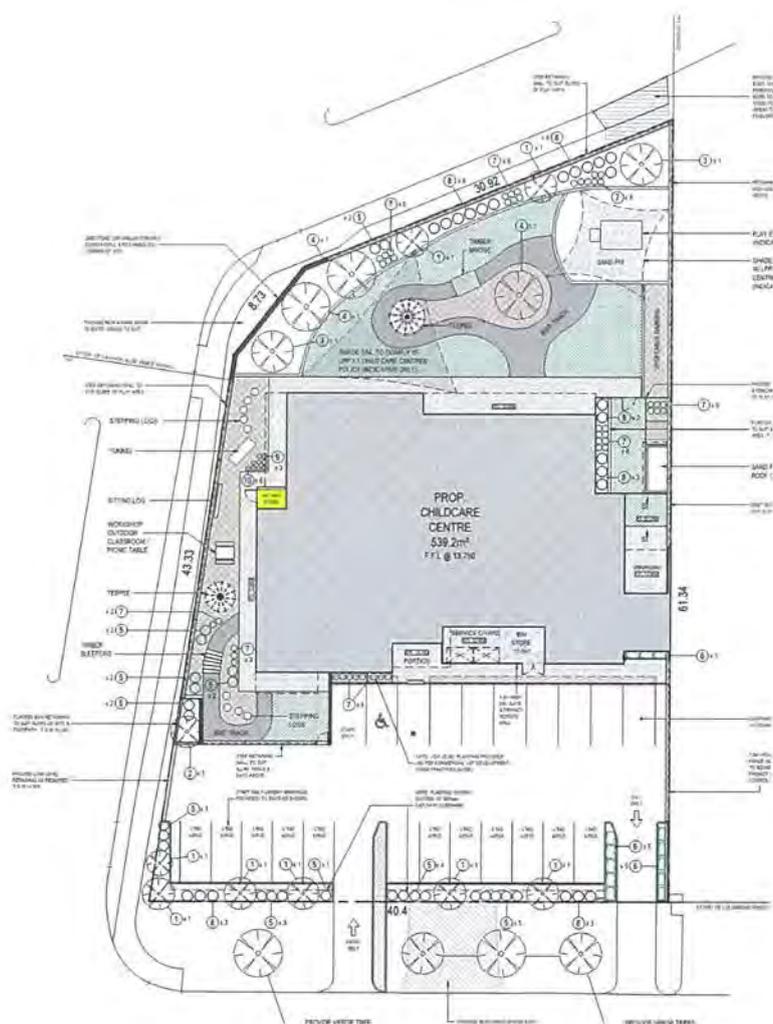
For the purpose of this assessment it was assumed that the childcare centre will be operating between 07.00 to 18.00 hours during week days; strictly no weekend and public holiday operation.

Based on the operation described above and provided by the client, the worst case scenario in terms of noise is when the children aged 3-6 years are in the outdoor area.

In addition to children playing outside, the following noise sources have been identified.

- Outdoor condenser for air conditioning units with an A/C unit facing Carrington Road - not exceeding 65dBA @ 1 metre.

Figure 2: Location of outdoor mechanical plant – condenser units





4.3 Data sources

Source power levels are modelled based on proprietary field data and/or professional/industry publications.

Table 3 lists the modelled sound power levels for identified noise sources.

Table 4. Selected and modelled noise source levels, dB re 1pW

Item reference	Octave band centre frequency (Hz)								Sum, dB	A-weighted Sound pressure level at 3m, dB(A) ¹
	63	125	250	500	1k	2k	4k	8k		
15 x 1-2 yrs old toddlers playing	88	82	77	81	83	80	80	68	87	70
25 x 2-6 yrs old children playing	95	89	84	88	90	87	87	75	94	77
outdoor condenser units	82	81	78	73	72	65	58	53	76	62
Group of 4 people – instantaneous*	89	84	79	82	84	82	81	70	89	74
Doors closing - instantaneous*	98	91	88	93	91	83	80	77	94	86

A summary of key results are listed in Section 4.5, following assessment on the basis in the following section.

Noise sources with an asterisk are considered as short events for the purpose of this assessment and are assessed in terms of instantaneous values (i.e. LAmax).

4.4 Basis of modelling

The acoustic propagation model used (ISO 9613) considered:

- the three-dimensional location and orientation (directivity) of each source aspect,
- the individual shielding / reflection effects due to surrounding structures (assumed 2m high solid continuous fence- e.g. colorbond - provides some screening of play area).

The forecast outcomes of this study are formed on the following basis:

- The entire wall extent to the south boundary (i.e. to the rear of No.67 Stratton St.) is continuous (without gaps) and no less than 2.0m above finished ground level (1.6m Colorbond fence above a 0.4m retaining wall) – refer to Appendix C.
- Under application of Clause 3(a) of the Environmental Protection (Noise) Regulations 1997, noise emissions from local vehicle traffic (propulsion or braking systems) operating on all thoroughfares of the site open to or used by the public are not assessed.
- A +5dB penalty is applied on the expected presence of tonal characteristics from air conditioning plant as defined in Regulation 9 of the Western Australia Environmental Protection (Noise) Regulations 1997. Under this regulation, additional penalties apply if noise emissions from the site contain intrusive noise characteristics described as impulsive or modulating, e.g. plant is observed to constantly switch on and off. These characteristics are objectively defined within state noise policy but may be applied subjectively at the discretion of the Authority.
- All scenarios consider the most stringent condition of ambient noise levels being equal to or less than the Assigned Level.

¹ Provided for indicative comparison purposes under free field conditions, dB referenced to 20µPa.



- The day period scenario assumes all air conditioning plant operating simultaneously at the highest nominal settings.
- No PA or loudspeaker system to be used outdoors.

4.5 Results summary

Results of the assessment are summarised in Table 5. A discussion of these results is presented in Section 6.

With the implementations as outlined in this report in place, NDY modelled the following scenarios:

Table 5 - Assessment results summary

Scenario	Aspect	Criteria	Forecast noise level at NSR
1	Up to 25 children playing no more than 1 hour per any 4 consecutive hours	Does not exceed L_{A10} assigned noise level for the time period 0700 to 1900 hours Mon. to Fri.	$L_{Aeq, 4hrs}$ equal to or less than 51dB
2	Up to 15 toddlers playing no more than 1 hour per any 4 consecutive hours		$L_{Aeq, 4hrs}$ equal to or less than 48dB
3	Evaporative coolers		L_{A10} equal to or less than 43dB²
4	Car park	Does not exceed L_{Amax} assigned noise level for the time period 0700 to 1900 hours Mon. to Fri.	L_{Amax} equal to or less than 59dB
Outside of building operation hours		Does not exceed L_{A10} assigned level for the time period 0900 to 1900 hours Sun. and public holidays, or 1900 to 2200 hours all days	Outside of building operation hours
		Does not exceed L_{A10} assigned noise level for the time period 2200 hours on any day to 0700 Mon. to Sat. and 0900 hours Sun. and public holidays	

The predicted noise emission from outdoor child play area with all recommendations implemented is therefore forecast to be below assigned noise levels.

2 Including applicable penalties for undesirable characteristics (refer Section 4.2)



5 FACADE DESIGN

5.1 Internal Noise Criteria and reverberation times

Table 6 below outlines the recommended project internal noise levels, which are based on AS/NZS 2107:2000 - *Acoustics - Recommended design sound levels and reverberation times for building interiors*. These criteria are inclusive of both external noise intrusion and building services noise contributions.

Table 6 – Overall Internal Noise Criteria

Type of Occupancy	Overall Internal Noise Criteria L_{eq} (dBA)
Sleeping Areas	30 - 40 near major roads
Living Areas, including Kitchen*	35 - 45 near major roads
Bathrooms / Toilets	45 - 55
Offices	35 - 45
Staff Rooms	40 - 45
Foyer / Corridors / Lift Lobbies	45 - 50

*Criterion for kitchens excludes noise from range hood; range hoods to be designed to 55 dBA.

5.2 External Noise Levels

We expect external noise levels to be governed primarily by traffic along Carrington Road (intermittent traffic), but also by traffic along Forrest Road, which may experience less continuous string of traffic.

A type 1 noise logger was left on site for 7 consecutive days to quantify existing noise levels roads. Monitoring location is shown on the map below.

Figure 3 – Noise Measurement Location (Image Source: Intramaps)





5.3 Noise Measurement Results Summary

A summary of the measured noise levels (location 1) are given in Table 7 as follows.

Table 7 – Environmental Noise Levels at Various Site Locations

Time	dBA				
	L _{Aeq}	L _{A10}	L _{A01}	L _{A90}	L _{Amax}
Monday to Friday					
07.00-22.00 hrs	60	62	69	49	78
22.00-07.00 hrs	55	57	65	44	74
Weekend					
07.00-22.00 hrs	59	61	69	46	79
22.00-07.00 hrs	52	55	63	38	72

It is noted that the ambient noise levels measured were dominated by road traffic. In the absence of proposed activities as outlined, the above measured noise levels are above the assigned noise levels for the State Noise Regulations (refer to Table 1).



6.1.2 Children outdoor activities

Compliance is generally forecast for the hours of operation between 07.00 to 18.00 hours provided the two following measures are implemented:

- Boundary fences at the back to be extended to a total height of 2 metres above ground comprising of:
 - 400mm high retaining wall
 - 1.6m high Colorbond fence

It is recommended a qualified structural engineer reviews the design prior to construction. Considerations to storm water relief should also be assessed by a qualified specialist.

- Children ages 2-6 years playing outside are limited to:
 - No more than 25 children playing outside for no more than 1 hour for every 4 hours.
 - Toddlers ages 1-2 years playing outside are limited to:
 - No more than 15 toddlers playing outside (in the toddlers fenced area) for no more than 1 hour for every 4 hours.

6.1.3 Ancillary activities

Ancillary activities with a potential to emit noise such as mowing the grass and building maintenance are exempt from the noise regulations provided they are limited to a total of 1 hour per day.



6.2 Facade Constructions

Based on the conducted noise survey, glazing sound insulation recommendations are provided below.

Recommended constructions are modelled to achieve the recommended internal noise levels outlined in AS/NZS 2107:2000, as documented in Table 6.

6.2.1 Vision Areas

Table 8 outlines the recommended minimum glazing performance, including example (single) glazing constructions for rooms which include sleeping (understood to be 2 x cott rooms).

We note that double glazing may be required for thermal requirements. Any commercially available double glazed system is expected to meet the nominated acoustic performance.

Table 8 - Glazing Requirements

façade aspect	Minimum weighted sound reduction, dB	Example glazing construction
Habitable Rooms		
Sleep areas	Rw 36	10.38mm laminated glass
All other	Rw 32	6mm float glass
Lobby / Corridors/Kitchens		
All	Rw 32	6mm float glass

Glazing to the facade is assumed to be sealed, as ventilation and cooling/heating will be provided mechanically.

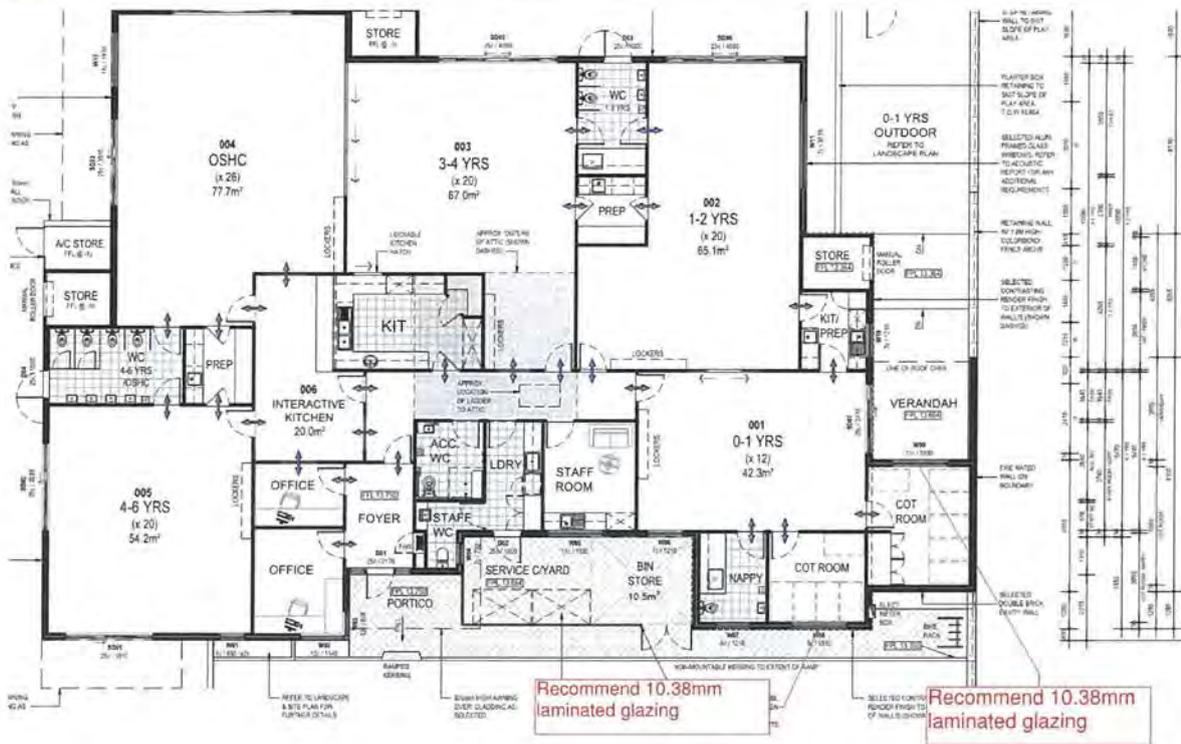
All framing for housing glazing will need to be constructed to such a standard that the sound performance of the glazing is not significantly degraded. Recommended glazing for the two cott been provided in



Figure 6.



Figure 6 - Recommended glazing to the two cott rooms



6.2.2 Non Vision Areas

It is recommended specifying a facade that meets $R_w + C_{tr} 35$ or higher (e.g. 90mm acoustibrick -50mm air gap – 90mm acoustibrick would be suitable as it would exceed this requirement).



A GLOSSARY

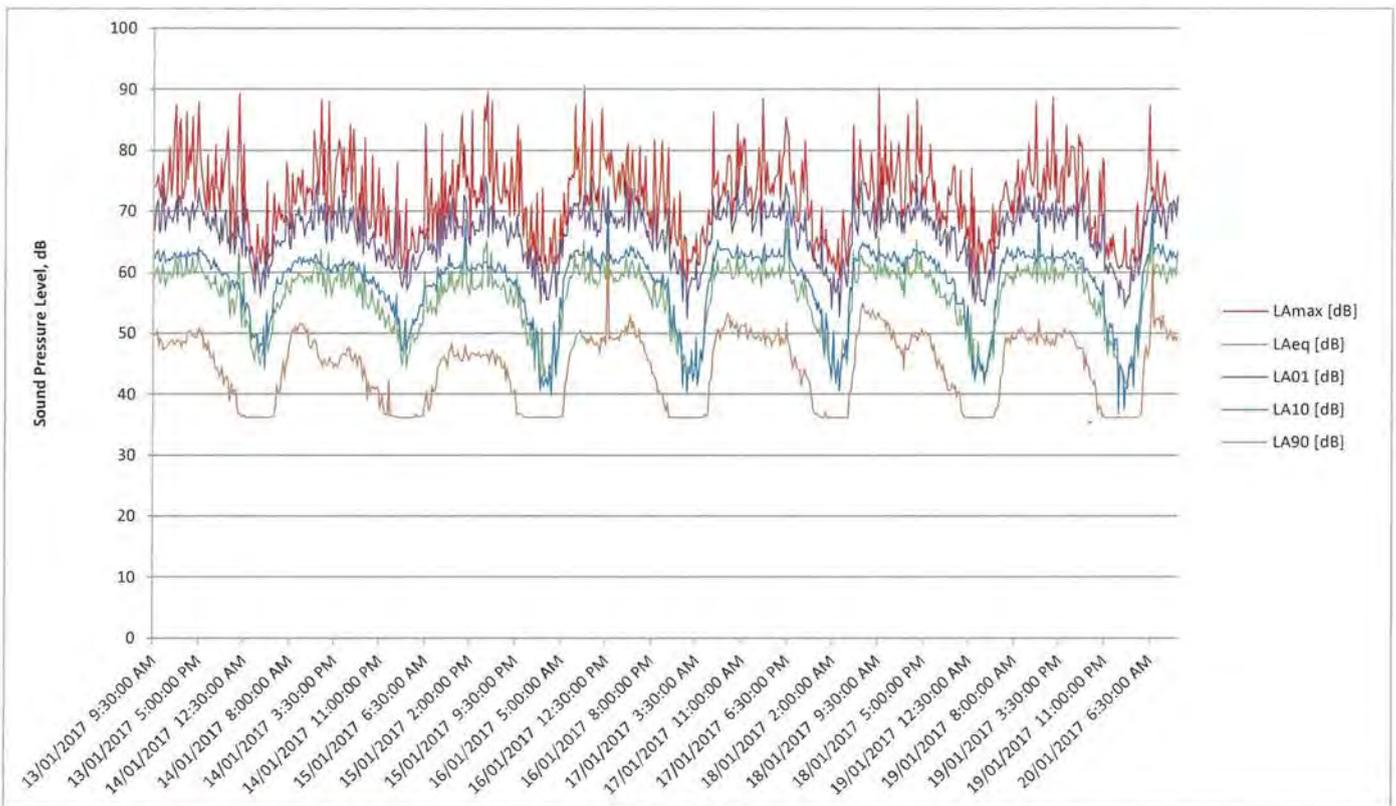


due to flanking sound transmission and imperfect site construction.

Sound Isolation	A reference to the degree of acoustical separation between any two areas. Sound isolation may refer to sound transmission loss of a partition or to noise reduction from any unwanted noise source. The term 'sound isolation' does not specify any grade or performance quality and requires the units and measurement conditions to be specified.
Sound Pressure Level L_p, dB	A measurement obtained directly using a microphone and sound level meter. Sound pressure level depends on the distance from a source and on the measuring environment. Sound pressure level equals 20 times the logarithm to the base 10 of the ratio of the rms. sound pressure to the reference sound pressure of 20 microPascals - $20\log_{10}(\text{measured rms pressure}/2 \times 10^{-6})$
Sound Power Level L_w, dB	Sound power level is a measure of the sound energy emitted by a source. It does not change with distance, and is not directly measured. Sound power level of a machine may vary depending on the actual operating load and is calculated from sound pressure level measurements with appropriate corrections for distance and/or environmental conditions. Sound power level is equal to 10 times the logarithm to the base 10 of the ratio of the sound power of the source to the reference sound power of 1 picoWatt. - $10\log_{10}(\text{Sound Power}/1 \times 10^{-12})$
Speech Privacy	A non-technical term but one of common usage. Speech privacy is opposite to speech intelligibility - a high level of speech privacy means a low level of speech intelligibility. It should be recognized that acceptable levels of speech privacy do not require that speech from an adjacent room is inaudible.



B UNATTENDED NOISE DATA SUMMARY







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NDY QA SYSTEM

Revision No: 3.0
Revision Date: 11 May 2017
Reason Description: Issue
File Location: W:\P676xx\P67674\001\J-124_Reports
Filename: rp170203p0013

Client Name: Peter Betz
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ROADMILES

Consulting Civil & Traffic Engineers

CHILD CARE FACILITY

67 Stratton Street, Hamilton Hill

Intersection with Carrington Street and Forrest Road

(90 Places)

TRANSPORT IMPACT STATEMENT

ROTTO INVESTMENTS

03 May 2017

Document Status

Rev No.	Author	Reviewed by	Date	Issued for	Signature	Date
A	G Miles			Client review		05/04/17
1	G Miles			Final		07/04/17
2	G Miles			Reduced Places		03/05/17

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1. INTRODUCTION

The statement considers the impact that the development of the child day care facility at 67 (Lot 64) Stratton Street, Hamilton Hill will have on the road network through increased traffic and parking demands. The site is on the one property that spans between the intersection of Forrest Road / Carrington Street and the intersection of Carrington Street / Stratton Street.

The report is prepared in response to a request from Rotto Investments P/L as the developer of the property.

The development site is shown on Figure 1. It is located centrally within an established residential area on a District Distributor B Road between two intersections with the other boundary roads being a Local Distributor Road and a Local Access Road. The site is approximately 4.5km from the Fremantle business district.

Nearby are the Port School (150m), the Kerry Street Community School (380m) and the Fremantle Christian College (380m). The Hamilton Senior High School is 1.6km away.

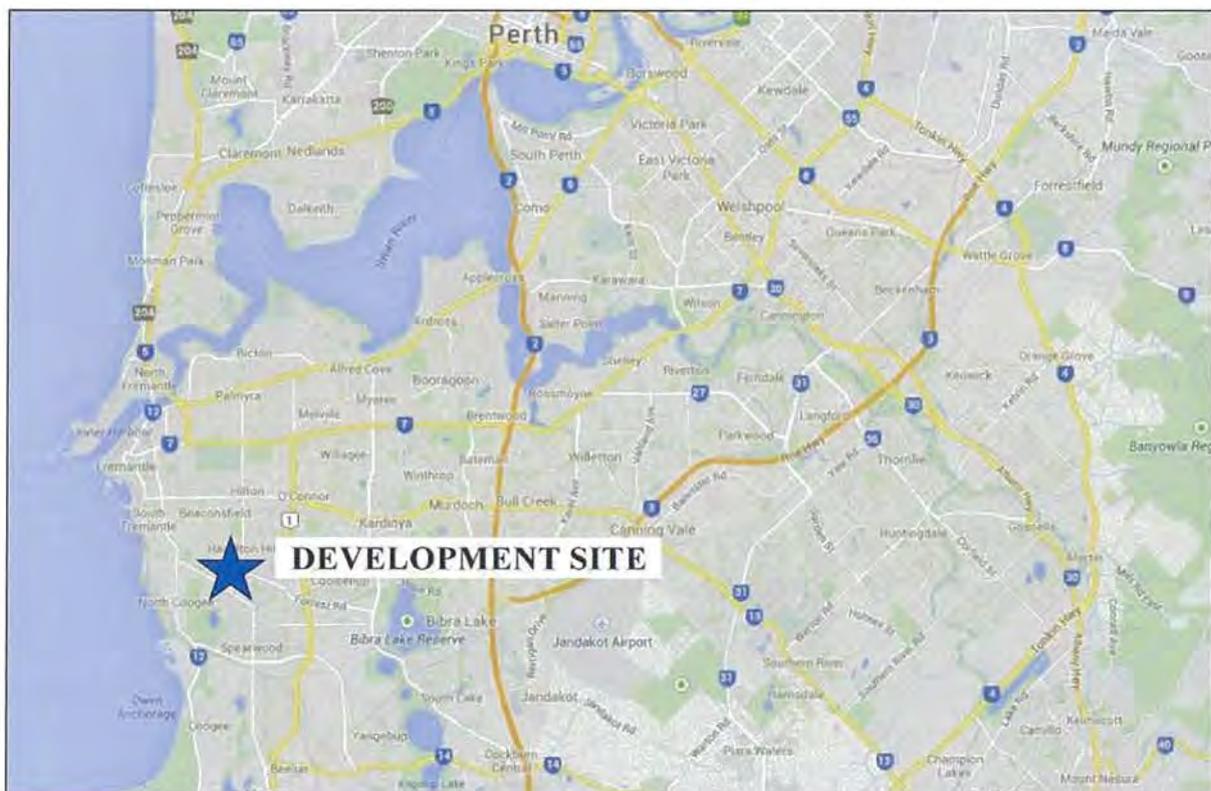


Figure 1 General Development Location

ROADMILES

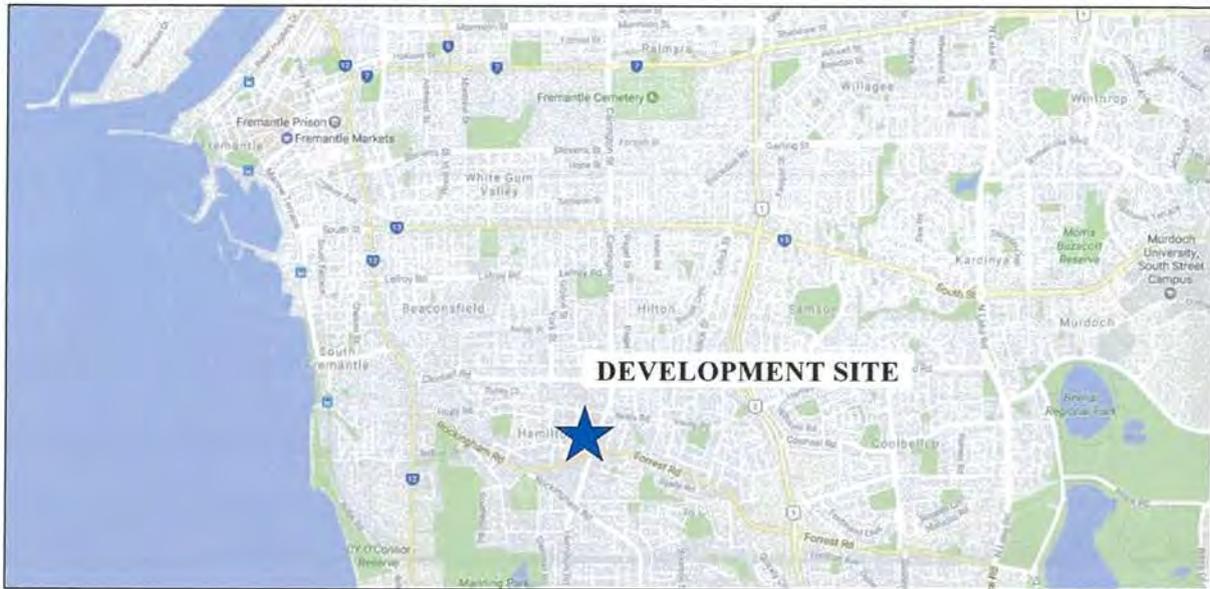


Figure 2 Development Location

The site is in an area identified by the City of Cockburn in the Local Planning Scheme No.3 as being zoned for residential purposes. The land use as a child day care facility is permitted in the scheme with the Council's discretionary approval.



Figure 3 Town Planning Zoning

The site is within a general residential area that had a variety of lot sizes when the different land subdivisions were created. There has been some progression with redevelopment from single residential to multi-residential R30, R40 or R60 with multiple dwelling units or properties replacing the previous individual residence on one property.

The extent of development within the 400m and 800m walking radii can be gauged from Figure 4. The

area is nearly fully developed for residential purposes. The major undeveloped strip was the proposed route for the Roe Highway extension.

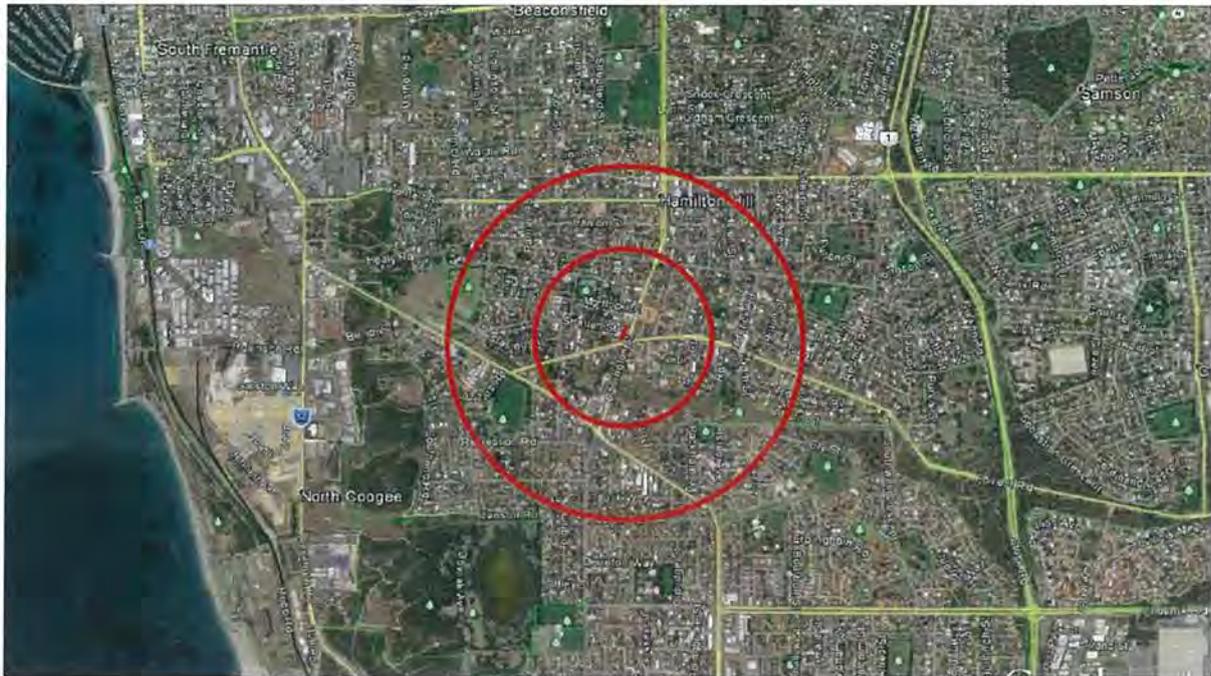


Figure 4 Aerial photograph of general area with 400m and 800m walking radii

The proposed development site is located on the westerly side of Carrington Street between Forrest Road and Stratton Street.

Stratton Street is a 735m long residential through road that parallels the major route of Forrest Road. There is a small shopping area about midway along Stratton Street. Stratton Street has no other attraction for additional traffic other than the local property access.

Forrest Road is a 4.5km long road that once was a major 4 lane east – west route. For a long while the importance has been declining due to more attractive alternate routes.

Carrington Street is 5.5km long and is a major north – south connector road extending between Rockingham Road and Canning Highway.

Stratton Street is a 6.5m wide two lane undivided road with a 1.5m wide pathway on the northern side along the edge of road.

Forrest Road comprises two through traffic lanes, a median area and two on-road cycle lanes away from the intersection with Carrington Street. At the traffic signal controlled intersection the configuration changes to two lanes for vehicles in each direction.

Carrington Street comprises two 5m wide through traffic lanes away from the intersection with Forrest Road. At the traffic signal controlled intersection the configuration changes with a widening of the road to two lanes for vehicles in each direction. There are 1.5m pathways on both sides of the road.



Figure 5 Existing Site Development

The present site is used as for two individual residential buildings with associated outbuildings. Typical residential traffic generation from the previous land use would be 20 vehicle movements per day.

This statement's purpose is to identify specific transport issues with respect to this site and is not to assess the overall transport impacts generated by the other land uses close to this property.

2. PROPOSED DEVELOPMENT

The site currently has a two individual single residential buildings and a car port. There is no significant vegetation remaining on the site or the adjacent nature strips. The ground slopes from the Carrington Street frontage where the public road level is 2.5m higher than the north-western corner of the property.

There are existing internal retaining walls. The existing vehicle access is by a sealed crossover from Stratton Street.



Figure 6 Existing Access on Stratton Street

The proposed redevelopment incorporates a new single storey custom purpose building and a 21 bay car park including a universal bay suitable for use by drivers who have a disability.

The proposed redevelopment site is shown in concept in Figure 7 and with more detail in Attachment 1.

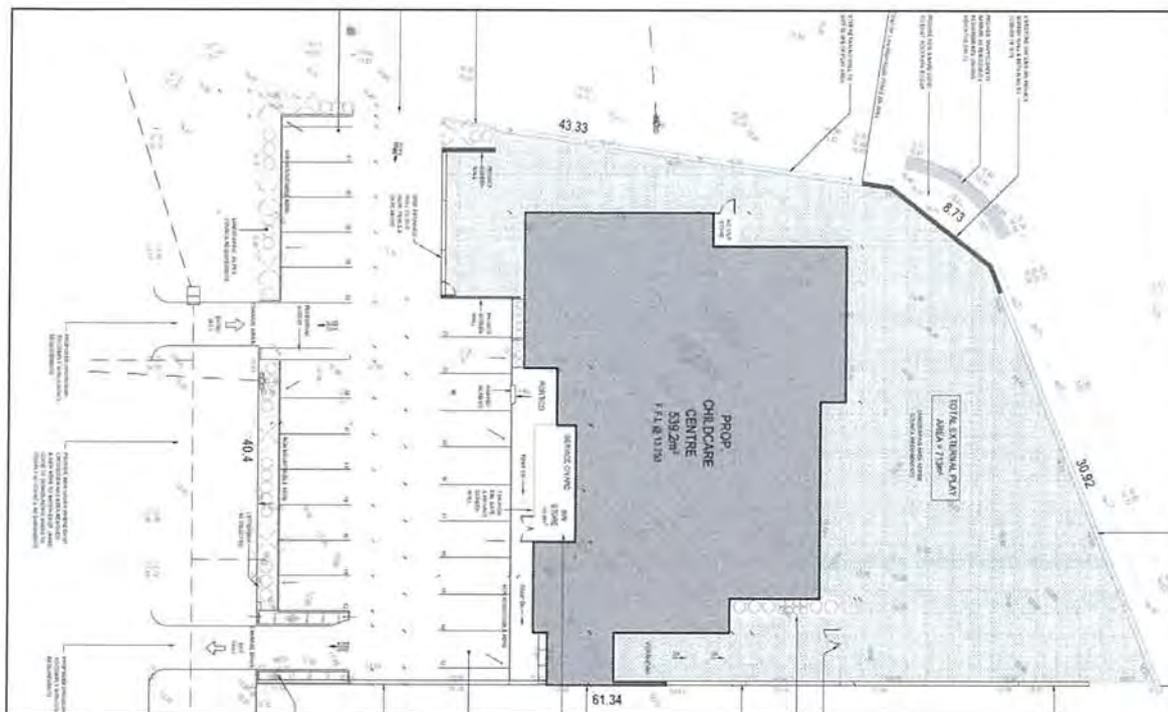


Figure 7 Concept Development

The redevelopment will cater for up to 90 children of varying age groups up to 6 years old and a maximum of 12 staff plus four trainees at any time. Hamilton Hill is a typical metropolitan suburb close to Fremantle but distant from Perth and the proposed child care facility is to operate within the hours from 6:30am to 6:30pm weekdays.

3. TRAFFIC GENERATION

3.1. Child Care Centre Use

The peak number of vehicle trips generated by a child day care facility is estimated from the RTA guide to be 0.8 times the number of children and to occur between 7am and 9am. The evening peak generation is lesser at a factor of 0.7. The peak increase in traffic is calculated to be 72 vehicles over the morning two hours.

Traffic generation rates

Centre Type	Peak Vehicle Trips / Child		
	7.00-9.00am	2.30-4.00pm	4.00-6.00pm
Pre-school	1.4	0.8	-
Long-day care	0.8	0.3	0.7
Before/after care	0.5	0.2	0.7

Figure 8 RTA NSW Guide to Traffic Generation

The RTA guide survey found that 93% of day-long child care children were transported by vehicles. The maximum daily number of vehicles accessing the site can then be calculated based on numbers of children and staff to be 181 vehicles per day.

The calculation is:

Morning drop-off of children:	90 x 0.93 =	84 vehicles
Evening pick-up of children:	90 x 0.93 =	84 vehicles
Operational Staff	12 x 1 =	12 vehicles
TOTAL		180 vehicles

This is not the increase in traffic on the adjacent road as some of the local residents using the new child care facility would presently be using Stratton Street as the access route to Carrington Street

It is expected that 20% to 40% of children would be from the local catchment between Healy Road and Rockingham Road. To be conservative the additional traffic generation has been calculated based on 10% of traffic to the child care centre presently using Stratton Street and would only be breaking an existing trip.

The total additional daily traffic along external roads expected to be generated from this development is 163vpd.

The movements of vehicles within the car park has to consider the maximum of 180 vehicles and that would generate 360 trip end movements per day in the car park. That is an indication of the level of use of the crossovers / accesses to the adjacent roads.

ROADMILES

Data is available from surveys of existing child care centres close to the centre of a metropolitan area where the child care centres are operating between 7am and 6pm.

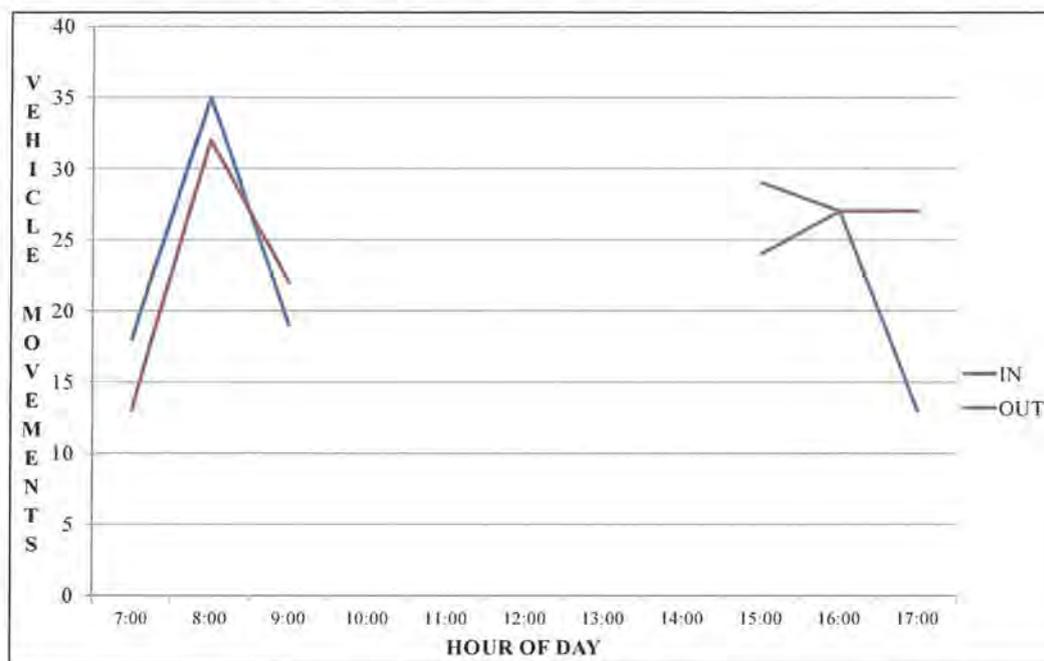


Figure 9 Metropolitan Surveys of Vehicle Movement Times for 90 Places

The morning arrival of at least 7 staff is expected to occur between 6:30am and 7:30am. Other care staff would arrive progressively until 8:45am. Kitchen staff would arrive about 10:00am.

The additional morning peak is expected to be 67vph between 8:00am and 9:00am representing the in (35) and out (32) movements of late city commuters and people also taking other children to primary school.

The Main Roads WA traffic data for Carrington Street shows that the residential commuter traffic this distance from Perth CBD peaks between 7:30am and 8:30am.

3.2. Residential Use

The development site could be used for at least seven residential units in accordance with the R30 / R40 zoning around this area and as has occurred on several of the nearby properties.

The traffic generation associated with a single residential lot is 9vpd.

For residential redevelopment the Stratton Street traffic would increase by 63vpd and the morning peak by 6vph between 7:30am and 8:30am.

3.3. Comparison

The net traffic increase by use as a child care centre instead of residential would be 100vpd and the morning peak increase would be 29vph between 8:00am and 9:00am.

4. VEHICLE ACCESS AND PARKING

4.1. Access and Aisles

The concept shows the vehicle access is proposed on Stratton Street with separate entry and exit crossovers to create a through flow in the car park for the drop-off and pick-up traffic.

Access on Forrest Road is not considered due to the interaction with the traffic signal queuing traffic within 20m of the intersection.

Access on Carrington Street has internal level constraints and also might replicate or aggravate existing traffic movement issues observed between Forrest Road and Stratton Street.

There are some drainage, sewer and telecommunication service pit covers that would limit access locations from the adjacent streets.



Figure 10 Forrest Road Frontage



Figure 11 Carrington Street Frontage



Figure 12 Stratton Street Frontage

The Stratton Street accesses are proposed to be 19.5m from Carrington Street for the entry and about 40m from Carrington Street for the exit.

The two accesses are proposed to be 3m wide and cater for one-directional traffic flow.

The concept building finished floor level is designed to be about half way between the upper and lower levels external to the site.

The car park level at centre of aisle can be sufficiently below building floor level to provide for gradients to provide drainage.

The Australian Standard AS2890.1:2004 Parking Facilities Part 1 Off-Street Parking Facilities recommends the crossover gradients to be 1 in 20 as specified in Section 3.3(a) -

3.3 GRADIENTS OF ACCESS DRIVEWAYS

At entry and exit points, the access driveway should be graded to minimize problems associated with crossing the footpath and entering the traffic in the frontage road.

Maximum gradients on and near access driveways, other than at domestic properties (see Clause 2.6), shall be as follows:

- (a) *Property line/building alignment/pedestrian path*—max. 1 in 20 (5%) between edge of frontage road and the property line, building alignment or pedestrian path (except as provided in Item (d)), and for at least the first 6 m into the car park (except as provided below).

The grade of the first 6 m into the car park may be increased to 1 in 8 (12.5%) under the following conditions:

- (i) The grade is a downgrade for traffic leaving the property and entering the frontage road.
- (ii) The user class is Class 1, 1A or 2 only.
- (iii) The maximum car park size is—
 - (1) for entry into an arterial road—25 car spaces, or
 - (2) for entry onto a local road—100 car spaces.

The maximum grade across the property line shall remain at 1 in 20 (5%).

The AS2890.1:2004 specifies the minimum gradient within an off-street car park should be 1% to allow for drainage. The maximum gradients should be 5% parallel to the angle of parking and 6.25% in any other direction. Within a universal bay AS2890.6:2009 Parking Facilities - Off-street Parking for People with Disabilities specifies the maximum gradient to be 3% if the bay is out of doors and has a bituminous sealed surface. The internal car park level requirements will need to be considered in the detailed design prepared for constructed.

The car parking is to be with bays at right angles to the car park access aisle. The classification of the off-street car park in accordance with AS2890.1 is User Class 1 as it is mostly for employee parking and is not for high turnover / short term stopping all through the day. The drop-off and pick-up bays do operate as short term parking, however are not high turnover such as expected in shopping centres and public car parks. Therefore the design of the drop-off and pick-up bays needs to consider the criteria for opening the car doors at full width as parents would need to assist children into and out of vehicles. The car park design can be in two areas split between User Class 1 and User Class 3 to reflect the appropriate use of bays on-site.

The crossover width is described in AS2890.1 as the Access Driveway Width. For a User Class 1 and for a User Class 3 parking facility with less than 25 bays accessing onto a local road the access driveway is considered to be an AS2890.1 Category 1. The entry and exit are normally combined with a width from 3m to 5.5m.

As the proposed design has individual widths for entry and exit of 3.0m each, it complies with the minimum requirements.

The car park configuration is designed to have a through aisle for easy circulation of drop-off / pick-up traffic with most of the staff parking in a dedicated area away from the through movements to reduce the potential conflict within the car park.

ROADMILES

The car parking bays for staff can be 5.4m long by 2.4m wide as they are for all day parking. The length of 5.4m is required where parking is to a wall or high kerb. If the vehicle can overhang a low kerb then the length can be reduced by up to 600mm. The overhang area can only be hard landscaping, grass or very low growing ground cover plants.

The car parking bays for drop-off and pick-up should be 5.4m long by at least 2.5m wide as they should allow for the full opening of all doors. These bays are for use in the four hours of the day when most non-staff activity occurs. AS2890.1 in Table 1.1 does clearly identify User Class 2 (that is a 2.5m width) as being the minimum for single manoeuvre entry and exit and for full opening of all doors.

If the bays were to be in use continuously through the whole business day, or to be in continuous high demand when in use, then the preference may be to have a slightly wider 2.6m bay. As those criteria do not occur the 2.5m wide bay will operate satisfactorily.

The car parking is to be with bays at right angles to a central aisle that is to be 6.0m wide. AS2890.1 recommends a minimum distance of 5.8m clear area in front of 2.4m and 2.5m wide bays. An extra 0.2m of aisle width would make the car park easier to use.

The City of Cockburn Scheme 3 does not have specific design requirements for car parking. The Residential Design Codes are referenced and the R-Codes section refers to the Australian Standard AS2890.1 for design guidelines.

The City does have planning policies with general comments that can affect the design of the car park and access locations. Those policies are:

- LPP3.1 Child Care Centres
- LPP5.6 Vehicle Access
- LPP1.2 Residential Design Guidelines

The policy LPP3.1 has the most relevant criteria In sections (3) 1 (a) & (b).

Where applications for child care premises are received, the following criteria should be taken into account:-	
1.	Location and Residential Amenity
(a)	Sites in close proximity to busy intersections or Primary Regional Roads and Other Regional Roads are not preferred due to issues of traffic conflict, noise and possible health implications associated with vehicle emissions.
(b)	Child care premises should be located adjacent or within close proximity to public transport routes.

Figure 13 Extract City of Cockburn Policy LPP3.1

4.2. Access Vehicle Sight Distance

Sight distance from the car park access along the street is defined in AS2890.1 as being a minimum of 45m in a 50km/h speed zone area and 65m in a 60km/h zone. The extract from AS2890.1 is shown below.

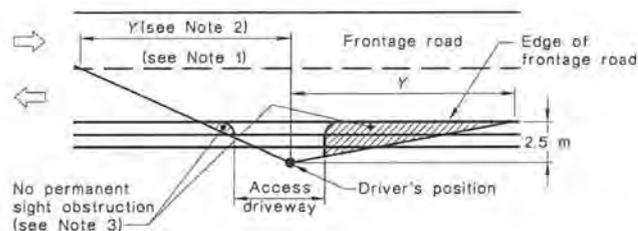
For existing roads the design speed and frontage road speed are based upon recorded traffic data when available. Austroads AGRD03/09 Guide to Road Design Part 3: Geometric Design 2009 states on the matter of design speed that “designers need to ensure that the design speed of every element is either equal to, or greater than, the 85th percentile operating speed”. The approach speed along Stratton Street is controlled by the gradient and the T-intersection configuration.

Reference to Note 4 in the AS2890.1 section 3.2.4 - the frontage speed is the posted speed limit unless the 85th percentile is more than 5km/h higher.

3.2.4 Sight distance at access driveway exits

Access driveways need to be located and constructed so that there is adequate entering sight distance to traffic on the frontage road and sight distance to pedestrians on the frontage road footpath for traffic entering the frontage road, as follows:

- Entering sight distance* Unsignalized access driveways shall be located so that the intersection sight distance along the frontage road available to drivers leaving the car park or domestic driveway is at least that shown in Figure 3.2.
- Sight distance to pedestrians* Clear sight lines as shown in Figure 3.3 shall be provided at the property line to ensure adequate visibility between vehicles leaving the car park and pedestrians on the frontage road footpath.



Frontage road speed (Note 4) km/h	Distance (Y) along frontage road m		
	Access driveways other than domestic (Note 5)		Domestic property access (Note 6)
	Desirable 5 s gap	Minimum SSD	
40	55	35	30
50	69	45	40
60	83	65	55
70	97	85	70
80	111	105	95
90	125	130	Use values from 2 nd and 3 rd columns
100	139	160	
110	153	190	

The speed for calculating the maximum required sight distance is taken to be the speed limit of 50km/h along Stratton Street. Therefore a minimum sight distance along Stratton Street of 45m is required when vehicles approach at 50km/h. The desirable sight distance is 69m.

ROADMILES

The sight distance requirements specified in detail in the Austroads Intersection Design Guide have also been considered. The Austroads AGRD04A/09 Guide to Road Design Part 4A: Unsignalised and Signalised Intersections 2009 provided more details about sight distances.

Sight distance is measured in three components:

- i) The approach sight distance (ASD) is the minimum level of sight distance that must be available on the minor road approaches to all intersections to ensure that drivers are aware of the presence of an intersection. The ASD varies with the design speed of the road:

ASD at 40km/h 34m

ASD at 50km/h 48m

ASD at 60km/h 64m

In this proposed development the minor road is the property access for exiting the site, the approach speed would be 10km/h and the presence of the road intersection is apparent to all drivers from within the car park. Therefore the design provides for adequate ASD.

- ii) The Safe Intersection Sight Distance (SISD) is the minimum distance that should be provided on the major road at any intersection. The SISD provides for sufficient distance for a driver of a vehicle on the major road to observe a vehicle on a minor road approach moving into a collision situation and to decelerate to a stop before reaching the collision point. The SISD varies with the design speed of the road:

SISD at 15km/h 24m

SISD at 20km/h 32m

SISD at 30km/h 52m

SISD at 40km/h 73m

SISD at 50km/h 97m

SISD at 60km/h 123m

At this proposed development site the sight distance has different conditions depending if the approaching vehicle is a through vehicle along Stratton Street from the west or has turned into Stratton Street from either direction along Carrington Street.

Stratton Street is straight to the west to a hill with the clear straight sight distance being 200m. Approach speeds can be expected at 50km/h. The view along Stratton Street to the west is shown in Photo 1.



Stratton Street view to West past the development site

Drivers turning from Carrington Street into Stratton Street would be at 25km/h and have direct clear sight distance of 40m.

The desirable SISD is exceeded along Stratton Street.

- iii) The minimum gap sight distance (MGSD) is based on distances corresponding to critical gaps in traffic flows along the road that drivers entering that road are prepared to use when turning.

MGSD is measured from the theoretical point of conflict if a crash was to occur. That is not always the same point as where the entering vehicle starts from. The SISD varies with the type of turning movement being made and the speed environment. For turns on a two lane two way road the turning gap times are listed as 5seconds for a left or right turn.

Stratton Street

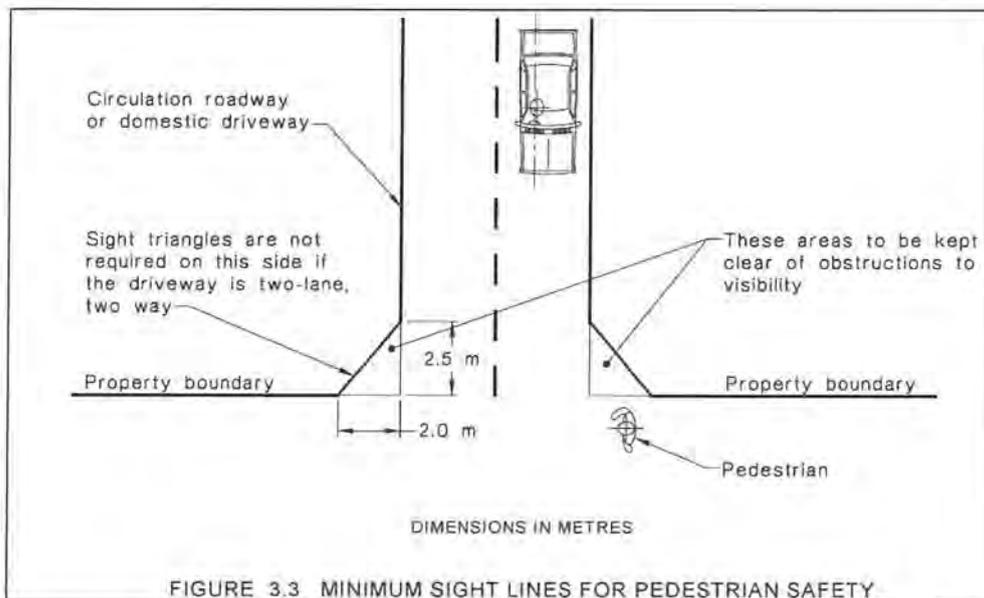
MGSD for 5 seconds at 20km/h 28m

MGSD for 5 seconds at 50km/h 69m

The site inspection showed these distances and gaps were achievable in Stratton Street in both the morning and afternoon peak hours. The probability of a delay occurring is greatest in the afternoon peak being 10.5% with the west bound traffic being the highest volume. The time of delay is miniscule being an average expected delay of 0.3 seconds.

4.3. Access Pedestrian Sight Distance

The Australian Standard AS2890.1:2004 also provides details for sight lines and distances for pedestrian movements across an access to a car park. Those details are shown in the AS2890.1 Figure 3.3 extract.



The design drawings provided indicates there would be landscape areas alongside the accesses that could be planted so that there would be no restriction on pedestrian sight distances.

4.4. Parking Bay Dimensions

As the predominant (longest time) use of the car park is for employee parking, the classification of the off-street car park in accordance with AS2890.1 is a User Class 1 facilities and for 90° bay the minimum parking bay dimensions are to be 2.4m wide by 5.4m long and an aisle width of 6.2m. An aisle width of as little as 5.8m can be used if partial three-point turns are permissible for employee parking.

However, the drop-off and pick-up bays do operate as short term parking although they are not high turnover such as expected in shopping centres and public car parks. Therefore the design of the drop-off and pick-up bays needs to consider the criteria for opening the car doors at full width as parents would need to assist children into and out of vehicles. The car park design can be in two areas split between User Class 1 and User Class 3 to reflect the appropriate use of bays on-site.

For an area of staff only parking, the proposed bay size is 2.4m wide by 5.4m deep off a 6.0m wide aisle.

For an area with bay widths of 2.5m, AS2890.1 allows for the aisle width to be a minimum of 5.8m.

The proposed parking space suitable for use by persons with disabilities is located close to the building access and the Australian Standard AS2890.6:2009 Off-Street Parking for People with Disabilities indicates the dedicated bay width should be 2.4m plus an adjoining shared area for access at 2.4m wide.

The concept design has a mix of bay widths that are arranged to provide the 2.5m width for the drop-off and pick-up bays.

The length of the bays at 5.4m can be varied by up to 0.6m if a low kerb and level area behind the kerb allows for a vehicle overhang without obstructing areas of pedestrian pathway. The level area can be a

hard surface, lawn or very low ground cover. There can be no raised / hard objects that would stop or damage a vehicle that moved all the way into the parking bay and overhung the end.

4.5. Number of Parking Bays

The number of parking bays required for the operation of a child day care facility varies according to the reference sources however most are within the same range.

- The WAPC Transport Assessment Guidelines for Developments August 2006 recommends an actual monitoring of a similar development or the application of parking rates from sources including the Guide to Traffic Generating Developments produced by the RTA NSW. The WAPC 2016 Guidelines retains the RTA NSW as a reference document. The RTA guide for child care centres is parking at 1 space per every 4 children in attendance and could be reduced “if convenient and safe on-street parking is available”. Staff parking is included in that provision.
- The WAPC released Planning Bulletin Number 72 in June 2005 regarding child care centres. The parking minimum requirement “including staff parking, will be one space per five children”. The number of bays may be varied by the local government considering local town planning schemes or relevant local policies.
- The City of Cockburn in the Town Planning Scheme No. 3 lists parking requirements for Child Care Premises at 1 bay for every 10 children plus 1 bay for every employee.

The proposed redevelopment will include 21 off-street car parking bays. This equates to 1 bay for 4.7 children.

As there are expected to be a maximum of 90 children, 12 adult staff and 4 non-adult trainees, the City of Cockburn TPS No.3 requirements would be a total of 21 bays.

In comparison the RTA rate calculates at 24 bays and the WAPC planning bulletin rate calculation is 20 bays.

The City of Cockburn does not allow the use of on-street parking to be used in the calculations.

4.6. Parking Utilisation

The RTA Guide indicates the peak traffic generation is in the morning period between 7am and 9am. The lesser peak occurs in the evening between 4pm and 6pm. There is little traffic generation during the middle of the day. The parking demand is closely associated with the traffic generation times as the children must be delivered and collected in person at the reception area of the facility.

The RTA review of existing facilities indicated the peak parking demand at ‘long-day care’ facilities could be considered to be 0.23cars per child at any one time. The average length of stay of parked vehicles was less than 6.8minutes. The RTA review established that at most long-day care facilities 93% of children were transported by car.

The inference from that data is that for this proposed child day care facility:

The total number of cars requiring to be parked in the morning drop-off period would be :

$$90 \text{ children} \times 0.93 \text{ 'transport by car' factor} = 84 \text{ cars}$$

That number of vehicles would arrive, park and depart over typically a two hour period.

The peak number of cars requiring to be parked in the morning drop-off period would be :

$$90 \text{ children} \times 0.23 \text{ factor} = 21 \text{ cars}$$

The RTA guidance for the maximum demand to be calculated includes staff and drop-off vehicles.

The proposal has 12 bays dedicated for staff parking. The other 9 bays are to be dedicated for drop-off / pick-up use.

The RTA survey data indicates the provision of 21 bays would be sufficient for the peak demand for 21 parking bays.

The ability for the car bays to be utilised was assessed by the use of a tracked path computer program using the Australian Standards Design Vehicle. The assessment showed the bays can be used by the large size 85th percentile passenger car with the normal expectations found in car parks that some waiting for the aisle to be clear is necessary.

The tracked path shown in Figure 14 is for the most difficult bay to enter and exit.

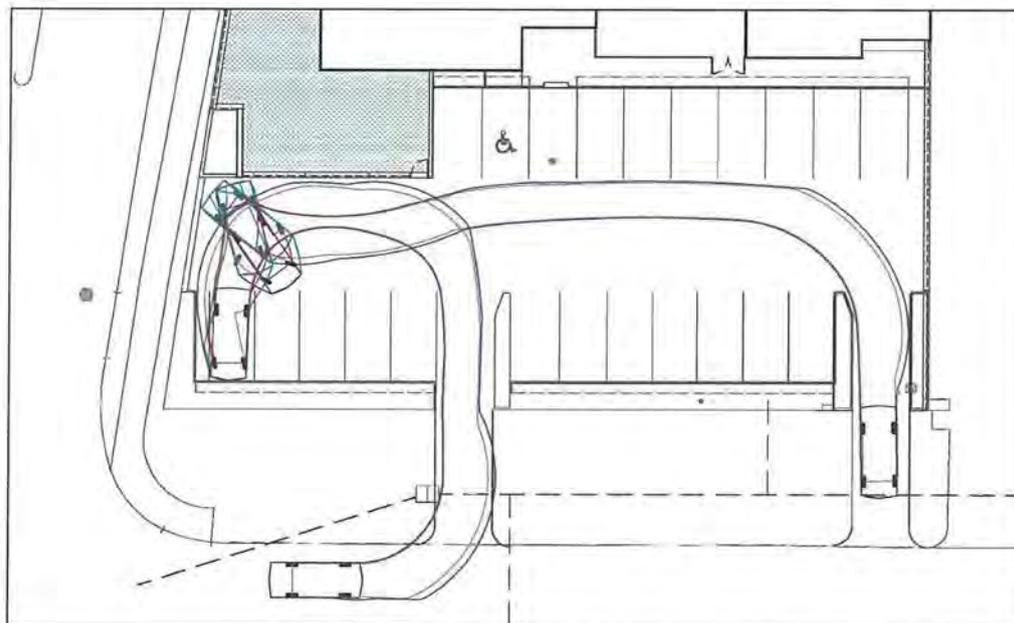


Figure 14 Car Tracked Path – last Staff bay

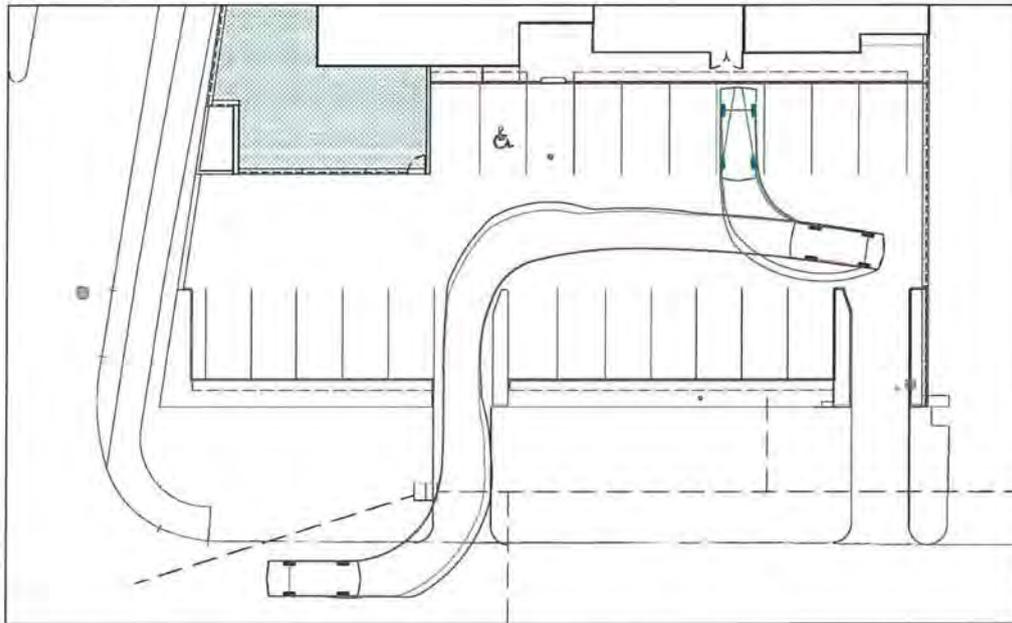


Figure 15 Car Tracked Path – Typical Drop-off – reverse parking

There were no bays identified that could not be used by forward or reverse movements. All vehicles could leave the site in a forward direction.

The tracked path shown in Figure 15 is indicative of the movement that frequent visitors will determine for themselves to be the easiest manoeuvre. Reversing into the visitor bays is the easiest for parking and then driving forwards leads simply to the exit access / crossover.

4.7. Parking on-Road

On-street parking occurs in many locations along Stratton Street. It is not required for the operation of this child care centre.

The on-street parking observed by the residential properties is for long stay parking and utilises the nature strip adjacent to the properties.

The small shopping area at the intersection of Stratton Street and Kerry Street utilises paved sections of nature strip for right angle parking off the roads adjacent to the shops.

5. PROVISION FOR SERVICE VEHICLES

The demand for access by service vehicles to a child day care facility is similar to any very small business and residential properties. There will be a regular weekly collection of 'household' refuse and an occasional delivery of goods.

The access and car park is not suitable for medium or large commercial vehicles. Vehicles of that size are not expected to be required in the operation of a child day care facility.

Waste collection from mobile garbage bins will need to be by a kerb-side pick-up in conjunction with the normal residential streets waste collection.

6. HOURS OF OPERATION

This proposed child day care centre facility's hours of operation are between 6:30am and 6:30pm on weekdays and 8:00am to 4:00pm on Saturdays. The traffic flow associated with a child day care facility is usually distributed across two hours in the morning and 3 hours in the afternoon. The morning traffic for this site will be over a longer period. For this metropolitan location there will be overlap with the normal residential commuter peak traffic times.

7. DAILY TRAFFIC VOLUMES AND VEHICLE TYPES

The daily traffic along Carrington Street has been determined from the Main Roads WA records for a weekly count in 2015 to be 16,779vpd.

The peak volume identified was 1,506vph total traffic between 3:00pm and 4:00pm.

The traffic flow along Carrington Street north of the site split into the two directions is shown in Figure 14. There is the typical residential commuter peak flow pattern. The northbound traffic in the morning is significantly higher than the south bound. The afternoon trend is the reverse.

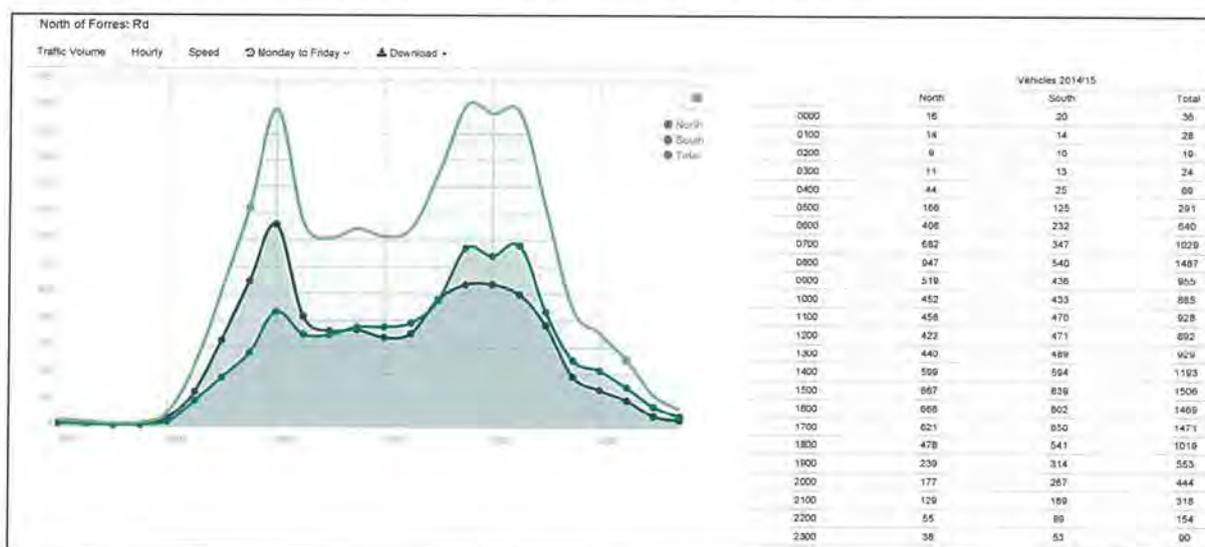


Figure 16 Traffic Volumes Along Carrington Street by direction

The traffic flow along Stratton Street was determined from manual turning movement surveys conducted in the morning and afternoon peak hours on Tuesday 28 March 2017. The survey results were:

	West Bound	East Bound	Total
Morning	32	48	80
Afternoon	80	46	126

Table 1 Peak Hour Traffic in Stratton Street

From the rule of thumb that the peak volume is close to 10% of the daily volume, the 2017 Stratton Street traffic would be 1,250vpd.

In physical capacity a road with single lanes in both directions can accommodate up to 9,000vpd in each direction depending upon the interaction of traffic flows at intersections, reference Austroroads Guide to Traffic Engineering Practice Part 2 Roadway Capacity. The physical capacity exists in Stratton Street to carry more traffic.

ROADMILES

The traffic capacity along Carrington Street has the ability to accept some additional traffic, however it is nearing the desirable maximum volume.

The vehicle types generated from this proposed redevelopment will be almost all passenger cars with the very occasional light commercial vehicle use.

8. TRAFFIC MANAGEMENT ON FRONTAGE STREETS

The site is located on a straight section of Stratton Street at the T-intersection with Carrington Street.

The speed limit along the road is the default 50km/h built-up area limit. The observed traffic behaviour was of speeds in this area considerably below 50km/h.

There are no existing traffic management treatments along this section of Stratton Street.

Pedestrian movement is accommodated with an existing footpath along one side of Stratton Street and both sides of Carrington Street. No deficiencies in pedestrian or cyclist facilities were identified.

Specific additional treatments for the control of vehicle speed along Stratton Street are not identifiable from the proposal for a child day care facility.

9. PUBLIC TRANSPORT ACCESS

The site is serviced by bus routes along Carrington Street and Forrest Road with stops close to this site.



Figure 17 Public Bus Routes

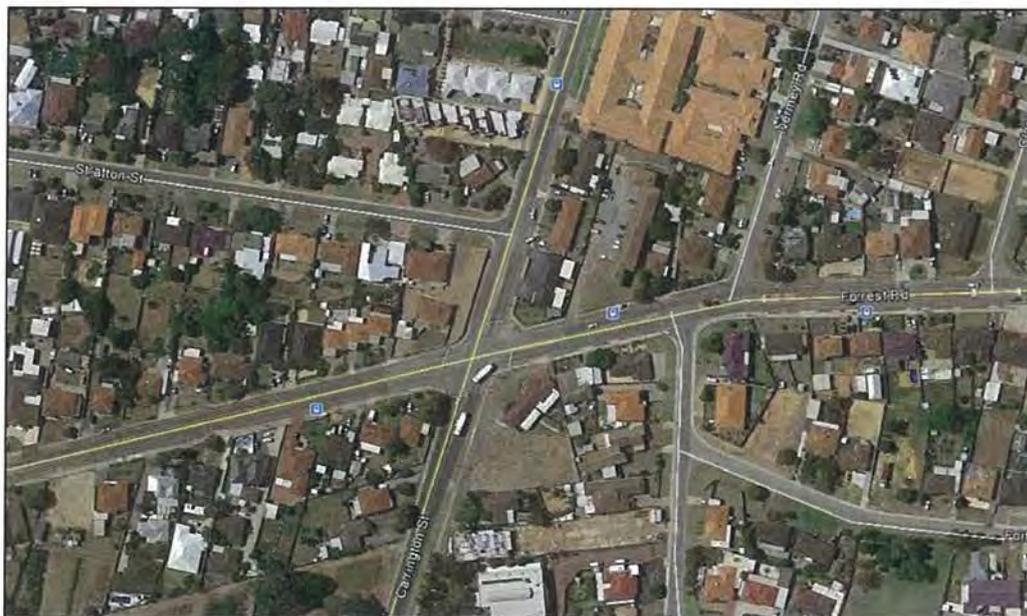


Figure 18 Three bus stops close to site

Pedestrian and cycle access is available to the bus services by existing footpaths.

There is a bus transfer station 400m to the south at Rockingham Road / Carrington Street.

10. PEDESTRIAN ACCESS

Pedestrian access to the proposed new developed building is by at-grade access from Stratton Street to the entry area. The pedestrian access is adjacent to the vehicular entry and aligns with the shared area access to the front building entry.

Pedestrian or cycle drop-off and pickup is expected to be less than 7% of the placements or 6 children.

From the north side of the car park aisle pedestrians need to cross the aisle to reach the clear zone with kerb ramp.

As is expected in any car park, adults will have to provide guidance to young children while crossing an area where cars might be moving.

11. CYCLE ACCESS

Cycle accessibility is directly from Stratton Street by the vehicle access.

Cycling is on-road along the residential streets.

The Perth Bike Network plans show the site is served by a network of cycle suitable roads around the general residential precinct. There are no off-road shared paths within the residential precinct.



Figure 19 Perth Bike Network extract

12.SITE SPECIFIC ISSUES

There is no expectation that traffic associated with this proposal should impact on the residential area around of the site.

Vehicle noise generated from any additional traffic will be restricted to normal day time hours due to the restrictions on the operating hours of child day care facilities.

13.SAFETY ISSUES

A review of the road network in the vicinity of the redevelopment site did identify one traffic movement that was undertaken by some drivers with excessive caution. That was during the afternoon peak period and was the right turn from Stratton Street into Carrington Street east-most lane followed by a left turn into Forrest Road.

There were no indications of any unacceptable risks that may arise from other vehicle movements on roads around the site.

The increased traffic flows associated with the development would not noticeably increase the risks in movements at the existing road intersections.

14. CONCLUSIONS

The redevelopment of the property at 67 Stratton Street, Hamilton Hill is not expected to adversely increase peak traffic flows on the adjacent road network. The maximum peak increase in traffic may be 29vph and will be between 8:00am and 9:00am. That is would be after the existing morning peak traffic flow period.

The child day care facility will generate a low traffic volume expected between 6:30am and 7:30am that would partially interact with the existing morning peak traffic flow along Carrington Street.

The total traffic generation is calculated to be 180vpd from a 90 place child care centre. An estimated 17vpd would be from within the existing traffic along Stratton Street.

The existing site use for two residences would have generated 20 movements on site or 10vehicles on the adjacent roads that no longer would occur.

The resulting increase would be a maximum of 163 additional vehicles along the adjacent roads.

This compares to a multi-residential land use on the property that would generate 63vpd. So the net additional traffic due this child care land use would be 100vpd.

Stratton Street presently has an estimated 1,250vpd along a road with an infrastructure capacity of over 18,000vpd

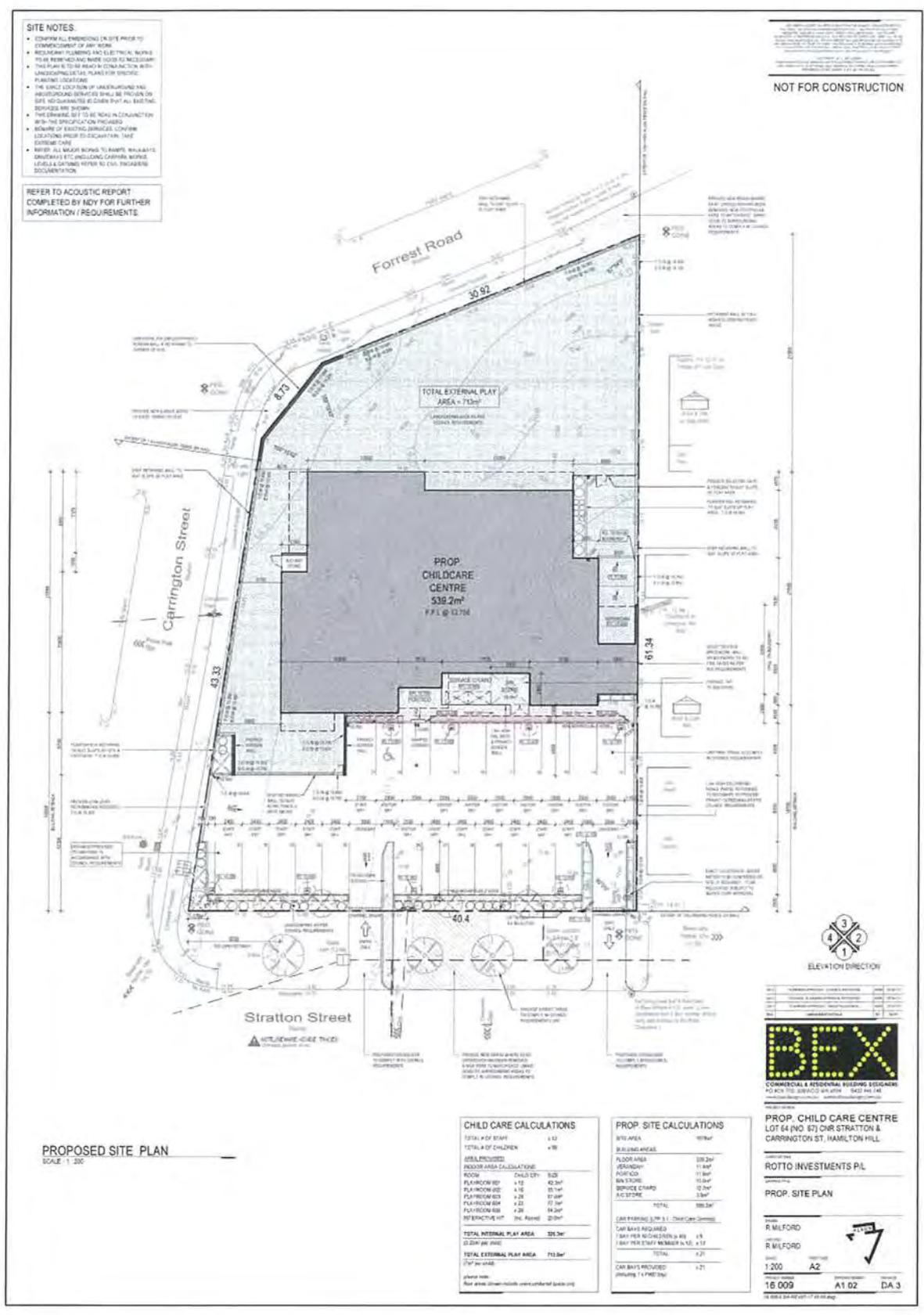
Carrington Street presently has an estimated 17,000vpd along a road with an infrastructure capacity of over 18,000vpd.

The total traffic flows from existing sources plus the traffic generated by a child day care facility would not exceed the road capacity

The proposed parking off-street could cater for the generated parking demand based on an even distribution of vehicles across the morning 2.5 hours expected drop-off time.

Existing public transport services, cyclist and pedestrian facilities servicing this area are considered to be adequate and do not require adjustment or improvement.

No changes to the existing surrounding major transport network are proposed due to the redevelopment proceeding.



SITE NOTES

- CONFIRM ALL DIMENSIONS IN SITE PRIOR TO COMMENCEMENT OF ANY WORK
- NECESSARY PLUMBING AND ELECTRICAL WORKS TO BE REVEALED AND MADE GOOD BY MCDONALD
- THIS PLAN IS TO BE READ IN CONJUNCTION WITH UNDERLYING LITTEL PLANS FOR SPECIFIC PLANNING CONSIDERATIONS
- THE EXISTING LEVEL OF UNDERGROUND SERVICES AND UTILITIES SHOULD BE PROVIDED BY THE PROPOSER AND SHOWN ON THE DRAWING SET TO BE READ IN CONJUNCTION WITH THE SPECIFICATION PROVIDED
- REMOVAL OF EXISTING SERVICES TO BE LOCATED PRIOR TO COMMENCEMENT OF CONSTRUCTION
- REFER ALL MAJOR WORKS TO RAMP, WALLS, DRIVEWAYS ETC INCLUDING CAPPING WORKS, LEGAL CAPPING REFER TO CIVIL ENGINEERING DOCUMENTATION

REFER TO ACOUSTIC REPORT COMPLETED BY MDV FOR FURTHER INFORMATION / REQUIREMENTS.

NOT FOR CONSTRUCTION

CHILD CARE CALCULATIONS

TOTAL # OF STAFF: 4 LP
 TOTAL # OF CHILDREN: 48

APPLICABLE ROOMS

ROOM	CHILDS CAP	SIZE
PLAY ROOM 001	+ 10	42.0m²
PLAY ROOM 002	+ 20	85.0m²
PLAY ROOM 003	+ 20	87.0m²
PLAY ROOM 004	+ 20	77.0m²
PLAY ROOM 005	+ 20	84.0m²
OFFICE/STAFF	(SEE PLAN)	20.0m²

TOTAL INTERNAL PLAY AREA: 355.0m²

TOTAL EXTERNAL PLAY AREA: 713.0m²

PROP. SITE CALCULATIONS

SITE AREA: 1070m²

AREA	SIZE
FLOOR AREA	538.2m²
ROADWAY	11.0m²
PARKING	10.0m²
SEALED DRIVE	10.0m²
SERVICE YARD	10.0m²
ACCESS DRIVE	2.0m²
TOTAL	583.2m²

CAR PARKING: 27 (SEE 3.1 Child Care Centres)

CAR BAYS REQUIRED: 27

1 BAY PER 10 CHILDREN BY 4.5m x 2.5m

1 BAY PER STAFF MEMBER BY 2.5m x 2.5m

TOTAL: 27

CAR BAYS PROVIDED (INCLUDING 1 PMB Bay): 27

PROP. CHILDCARE CENTRE
 LOT 64 (NO. 87) CHR STRATTON & CARRINGTON ST, HAMILTON HILL

ROTO INVESTMENTS PL

PROP. SITE PLAN

PROJECT NO: R MIFORD
 CLIENT: R MIFORD
 SCALE: 1:200
 DATE: 16.09.2016
 DRAWING NO: A2
 SHEET NO: DA.3



		PRINTED ON: 19/05/2017	SCALE = 1:850	
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FRANKLAND AVE

GAEBLER ROAD

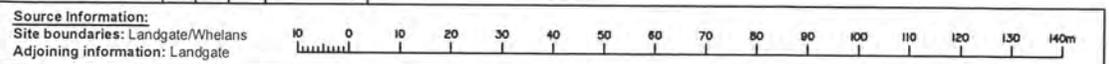
FRANKLAND AVENUE

20m ROAD WIDENING

R60

LOT 41
SUBJECT TO
FURTHER PLANNING

LOT 42



Source Information:
Site boundaries: Landgate/Whelans
Adjoining information: Landgate

- NOTES**
- The Public Open Space requirement will be made up either land ceded through subdivision and/or communal open space within a survey strata subdivision (DC1.3) and/or cash-in-lieu contribution.
 - 10% credited P.O.S. to be provided in accordance with provisions of Western Australian Planning Commission Liveable Neighbourhoods.

Legend

--- Structure Plan Boundary

ZONE

General Residential

OTHER

←-- Private Road

PROJECT		LOT 41 FRANKLAND AVENUE HAMMOND PARK	
Job Ref.	131336-1	Date	15 March 2017
Comp By.	LCFV	DWG Name.	131336-1-001h.dwg
Locality	HAMMOND PARK	Local Authority	CITY OF COCKBURN

CLIENT	BROADVISION PROJECTS PTY LTD
STRUCTURE PLAN MAP	



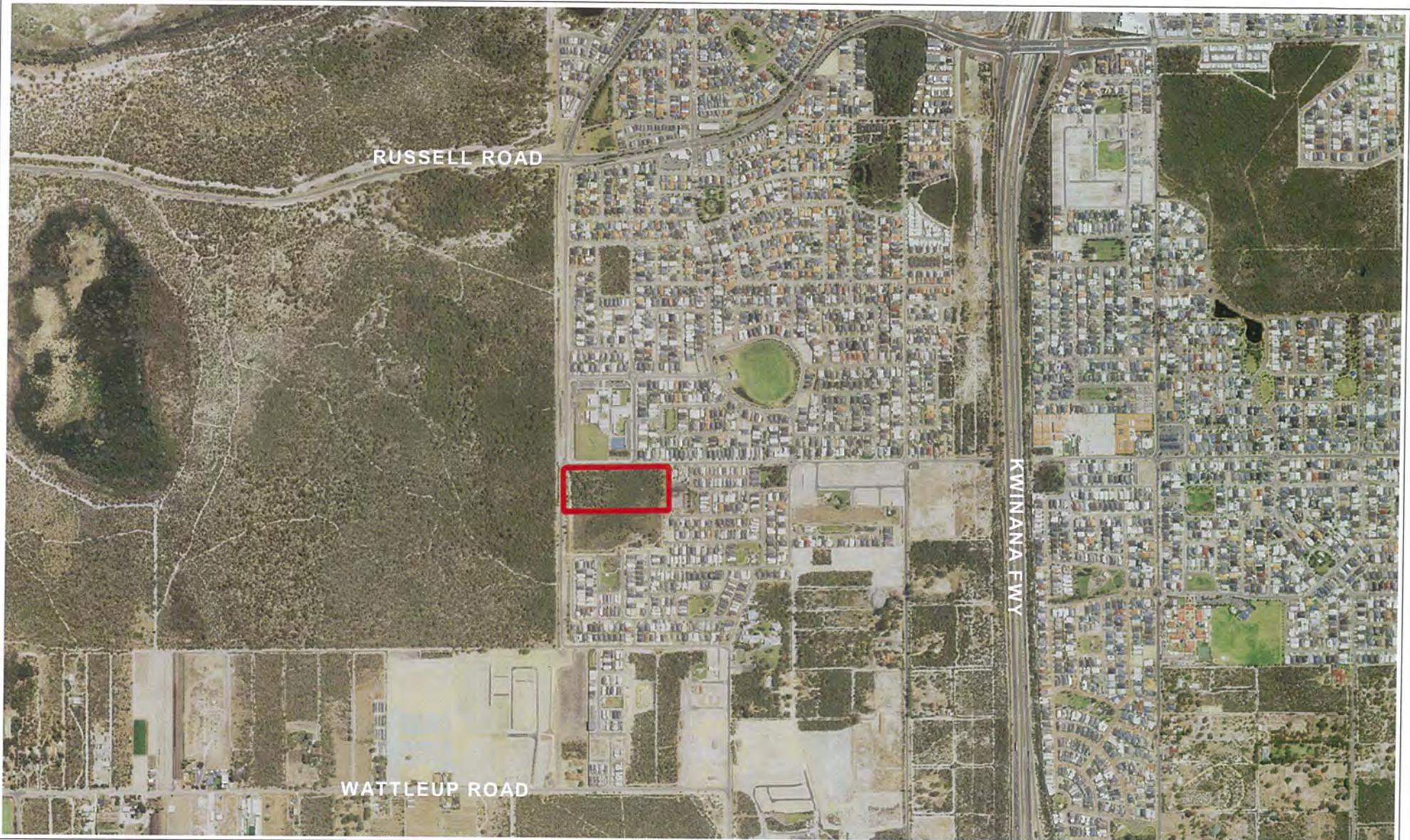
RPS Australia East Pty Ltd
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Scale	Sheet	Plan Ref	Rev
1:1000	A3	131336-1	H

OCM 8/6/2017 - Item No.15.10 Attach 1



RUSSELL ROAD

KWINANA FWY

WATTLEUP ROAD



City of Cockburn
G.I.S Services Department

Attachment 1 – Location Plan

PRINTED ON:
27/04/2017

SCALE = 1:15925

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SCHEDULE OF SUBMISSIONS
PROPOSED STRUCTURE PLAN – LOT 41 GAEBLER ROAD, HAMMOND PARK

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
1	Telstra, Forecasting & Area Planning Locked Bag 2525 PERTH 6001	<p>Thank you for the above advice. At present, Telstra Corporation Limited has no objection. I have recorded this in our Development database and look forward to further correspondence in the future. Should you require any more information regarding Telstra's new infrastructure policy, please read below or contact me.</p> <p><u>Latest Telecommunications Policy</u></p> <p>The Federal Government has deemed developers are now responsible for telecommunications infrastructure on all developments, i.e. conduits, pits and the cost of the cable installation by Telstra or other carrier. Telstra can provide a quote for the pit and pipe and/or cable. This is explained on the Telstra Smart Community website below. The owner/developer will have to submit an application <u>before construction is due to start</u> to Telstra (less than 100 lots or living units) or NBN Co. (for greater than 100 lots or living units in a 3 year period).</p> <p>Applications to Telstra can be made on the Telstra Smart Community website: http://www.telstra.com.au/smart-community</p> <p>More information regarding NBN Co. can be found on their website http://www.nbnco.com.au/develop-or-plan-with-the-nbn.html</p> <p>Please dial 1100 (Dial before You Dig) for location of existing services.</p> <p><u>Federal Government Telecommunications Infrastructure in New Developments Policy May 2015</u> https://www.communications.gov.au/policy/policy-listing/telecommunications-new-developments</p> <p><u>STATE PLANNING POLICY 5.2 Telecommunications Infrastructure August 2015</u> http://www.planning.wa.gov.au/dop_pub_pdf/Telecommunications_Infrastructure.pdf</p> <p><u>Communications Alliance - G645:2011 Fibre Ready Pit and Pipe Specification</u></p>	Noted.

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>for Real Estate Development Projects http://www.commsalliance.com.au/Documents/all/guidelines/g645</p>	
2	<p>Department of Water PO Box 332 MANDURAH WA 6210</p>	<p>The Department of Water (DoW) has reviewed the application and provides the following advice.</p> <p>LWMS Urban Water Management</p> <p>Consistent with Better Urban Water Management (BUWM) (NAPC, 2008) and policy measures outlined in State Planning Policy 2.9, Water Resources, the proposed Structure Plan should be supported by a Local Water Management Strategy (LWMS) prior to final approval of the Structure Plan.</p> <p>The LWMS should demonstrate how the subject area will address water use and storm water management. It should contain a level of information that demonstrates the site constraints and the level of risk to the water resources.</p> <p>The DoW reviewed the supporting document, <i>Lot 41 Frankland Avenue, Hammond Park - Local Water Management Strategy</i> (DEC, January 2017) and it was deemed satisfactory to the DoW in January 2017. Accordingly, the DoW has no objections to this proposal.</p>	Noted.
3	<p>Department of Education 151 Royal Street EAST PERTH WA 6004</p>	<p>Thank you for your letter dated 10 April 2017 regarding the Proposed Structure Plan for Lot 41 Gaebler Road, Hammond Park.</p> <p>The Department has reviewed the document and advises that it has no objection to this proposal.</p>	Noted.
4	<p>Jovito & Sarah Enriquez 3 Gaebler Road HAMMOND ROAD WA 6164</p>	I support	Noted.
5	<p>Water Corporation PO Box 100 LEEDERVILLE WA 6902</p>	<p>Depending on the proposed form of subdivision and development of this site, the developers may need to fund the extension of reticulation-sized water mains and gravity sewers from the established networks nearby.</p> <p>The Water Corporation has prepared conceptual water and wastewater planning, which provides a guide for the future servicing of the area. This planning is summarized in the proponent's servicing report. The developer's consulting engineers may adapt or modify the Corporation's planning in consultation with the Corporation.</p>	Noted. The applicant has been made aware of this requirement via this attachment to the Council Report.

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
6	Brad and Taneel Petersen 27 Gaebler Road HAMMOND ROAD WA 6164	<p>Regarding the proposed structure plan and zoning of Lot 41 Gaebler road to R60.</p> <p>I, and my wife strongly oppose this proposal. However, will support the initial plan as proposed in the stage 3 structure plan where Lot 41 Gaebler Rd is planned for medium density housing.</p> <p>It is our opinion that having the block of units proposed by the developer will degrade the family nature of Hammond park, which we, and our neighbours specifically bought in to.</p>	<p>Due to the exclusion of a large portion of Lot 41 from the Structure Plan area as well as inadequate supporting appendices, the Structure Plan has been recommended for refusal. However, the proposed density was not a concern of the City's for the reasons stated below.</p> <p>The 'stage 3 structure plan' referred to is the Southern Suburbs District Structure Plan – Stage 3 Hammond Park/Wattleup (SSDSP3) and is not a statutory document but rather a guidance document assisting the preparation and coordination of local Structure Plans. The SSDSP3 proposes the site to be developed at a medium residential density. Part 10.4 of the Department of Planning's Structure Plan Framework defines medium density residential development as R25-R60. Thus, the proposed R60 is consistent with the medium residential density prescribed under the SSDSP3.</p> <p>The subject land has always been intended for medium density residential development (R25-R60) under the SSDSP3. It is not supported that if the subject land was to be developed as grouped or multiple dwellings, that this would compromise the amenity, security or enjoyment of existing residents. Medium density development at an R60 coding will not facilitate high rise development, but will allow for the opportunity for more diversity of housing within the locality which is primarily coded R20-R40. The State Government's strategic planning document <i>Perth and Peel@3.5million</i>, as well as <i>Liveable Neighbourhoods</i>, encourages diversity in housing choice to cater for a diversity of household typologies and incomes. Increased density is also encouraged in close proximity to</p>

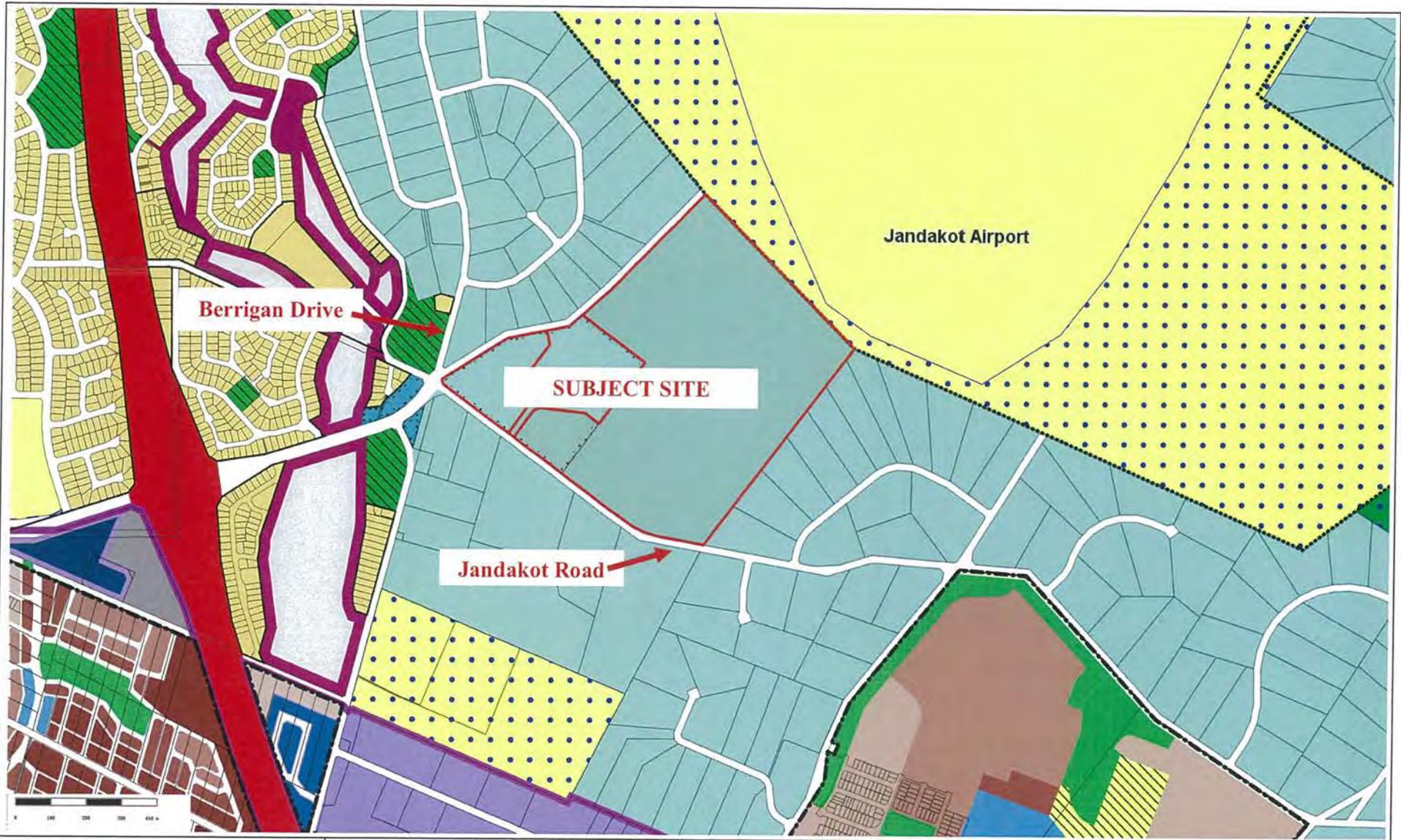
NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>Furthermore, we think that having such a high population density so close to a primary school is a poor planning decision as it will increase the traffic in an area where speeding cars are common.</p>	<p>areas of open space (Harry Waring Marsupial Reserve, Johnsonia Park, Botany Park), community facilities (Hammond Park Primary School, future local centre to the south, local centre to the north on Russell Road), and transport options (Russell Road, Rowley Road, Kwinana Freeway and Aubin Grove Train Station). Furthermore, the City's Housing Affordability and Diversity Strategy promotes a mix in land tenure and housing stock, and particularly recognises the shortfall of smaller housing options in the locality. Thus, R60 is an appropriate density in this location.</p> <p>The Transport Assessment Report lodged with the Structure Plan predicts that the subject site could generate 400 vehicle trips per day, with 15% of that traffic assumed to travel along Gaebler Road to/from the east of the site. In theory, this equates to an additional 60 vehicle trips on Gaebler Road each day, or approximately 6 vehicles in the peak hour, which would have no impact on the performance of the road and is unlikely to be discernible to residents from the existing volume of traffic. Even if this 15% was increased to a more conservative 30%, this would still only generate 120 vehicles on Gaebler Road to the east of the site, or 12 vehicles in the peak hour. This would still have no noticeable impact on the performance of the road. The Transport Assessment Report also recommends an appropriate location for vehicle access to the subject land that would provide the best separation from the Gaebler Road/Hammond Road intersection and the Gaebler Road/Murrumbidgee Drive intersection. This will minimise conflict with traffic turning at these intersections including school traffic and will allow sufficient queuing and storage distance</p>

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>We respectfully request that you deny the planning application and keep Hammond park a family friendly medium density suburb.</p>	<p>for vehicles turning in to the subject land. Speeding vehicles is not a planning matter that should be considered when assessing rezoning of land. Rather this is an enforcement matter not the responsibility of the City of Cockburn.</p> <p>There is no evidence to suggest that an R60 coding at the subject land will compromise the family friendly nature of the locality. Regardless, the Structure Plan has been recommended for refusal for a number of other reasons as identified in the Council Report.</p>
7	<p>WA Gas Networks (ATCO Australia) PO Box 3006 SUCCESS WA 6964</p>	<p>ATCO Gas does not currently operate gas mains nor infrastructure within the adjacent road reserve of Gaebler Road nor Frankland Avenue,</p> <p>ATCO Gas does not have any objection to this proposal for Lot 41. ATCO Gas will not be lodging a submission to the City of Cockburn regarding this proposal.</p> <p>Map included</p>	<p>Noted.</p>
8	<p>Department of Transport GPO Box C102 PERTH WA 6839</p>	<p>The Department of Transport (DoT) has no comment to provide. DoT recommend the City of Cockburn to forward the above application to Department of Planning (Infrastructure and Land Use Coordination team) to obtain their comments.</p> <p>Thank you for the opportunity to comment on the proposal</p>	<p>Noted. The application has been forwarded to the Department of Planning for their comment.</p>
9	<p>Main Roads WA PO Box 6202 EAST PERTH WA 6892</p>	<p>Main Roads has now had the opportunity to review the documentation and attached Plans.</p> <p>Frankland Avenue is designated as a blue road "Other Regional Road" within the Metropolitan Regional Scheme (MRS) under the control of the Department of Planning.</p> <p>AS the proposal is not adjacent to, nor will it impact, any roads under Main Roads control. Main Roads has no comment or objection to the proposal.</p> <p>It is advised to contact the Department of Planning and Infrastructure, Land Use Coordination (ILUC) Branch, for review and comment on the proposal.</p>	<p>Noted. The application has been forwarded to the Department of Planning for their comment.</p>
10	<p>Department of Health PO Box 8172 Perth BC WA 6849</p>	<p>Thank you for your letter of 10 April 2017 requesting comment from the Department of Health (DOH) on the above proposal.</p>	<p>Noted. The applicant has been made aware of this requirement via this attachment to the Council Report.</p>

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>The DOH has no objection provided the Structure Plan is to require that all developments be required to connect to scheme water and reticulated sewerage as required by the <i>Government Sewerage Policy - Perth Metropolitan Region</i>.</p>	
11	<p>Rowe Group Level 3 369 Newcastle Street NORTHBRIDGE WA 6003</p>	<p>Rowe Group acts on behalf of the owners of Lot 9008 Frankland Avenue, Hammond Park in respect to a submission on the Local Structure Plan for Lot 41 Frankland Avenue (the LSP). Our Client's landholding is located directly to the south of the LSP area.</p> <p>Our Client does not raise an objection to the proposed LSP. However, provides the following comments in respect to the Conservation Category Wetland (CCW) that is "mapped" within the LSP area and on our Client's landholding.</p> <p>On page "13" of the LSP Report it is advised that:</p> <p><i>The area noted as subject to further study which is excluded from the SP area contains a mapped Conservation Category Wetland (dampland). No portion of this mapped wetland area or associated 50m buffer encroaches into the boundaries of the SP. The proponent does not agree with the dampland classification and is continuing to study the vegetation, soil and hydrology of the area mapped as damp land to better ascertain its status.</i></p> <p>The "mapped" CCW mentioned above encroaches onto our Client's landholding (Lot 9008 Frankland Avenue). Our Client's position is that there is no CCW on their landholding.</p> <p>"Our Client is currently challenging the "mapped" CCW classification as it relates to their landholding through proceedings within the Supreme Court.</p> <p>Our Client takes this opportunity to re-affirm its position that a CCW is not extant on their landholding, thus the "mapped" CCW classification is considered to be incorrect and the CCW should be discounted for the purposes of structure planning."</p>	<p>Noted. The City is aware of the landowner's point of view regarding the CCW and their dealings with the Supreme Court. However, only the Department of Parks and Wildlife have the authority to change the classification of a wetland. At the time of lodgement of this Structure Plan, the wetland is classified as a CCW under DPaW's <i>Geomorphic Wetlands Swan Coastal Plain</i> dataset and the Structure Plan will be assessed on this basis.</p>
12	<p>Department of Aboriginal Affairs PO Box 3153 EAST PERTH WA 6892</p>	<p>The Department of Aboriginal Affairs (DAA) advises there are no reported Aboriginal sites or Aboriginal heritage places within the areas of the Proposal.</p> <p>The DAA recommends that proponents undertaking works within this area have consideration for the DAA's Aboriginal Heritage Due Diligence Guidelines when planning specific developments within the Proposal area. These guidelines have been developed to assist proponents to identify any risks to Aboriginal heritage and</p>	<p>Noted. The applicant has been made aware of this advice via this attachment to the Council Report.</p>

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		to mitigate risk where heritage sites may be present. The guidelines are available at: https://www.daa.wa.gov.au/heritage/land-use/ .	
13	Department of Parks and Wildlife Locked Bag 104 BENTLEY DC WA 6983	<p>In response to your request for comment on the above Proposed Structure Plan, the Department of Parks and Wildlife provides the following advice.</p> <p>Lot 42 Frankland Avenue</p> <p>The city's correspondence advised of the current Supreme Court processes relating to the Conservation Category Wetland (CCW) - UFI 14104 located on Lot 42 Frankland Avenue (Lot 42). Parks and Wildlife has been involved in a mediation process with the proponents for Lot 42. The mediation was the result of a directive given by the Supreme Court to explore possibilities for future development options through the planning process, rather than via the current pathway for a wetland reclassification. A 'without prejudice' proposal was presented through mediation to Parks and Wildlife for comment on the potential impacts to the CCW.</p> <p>Parks and Wildlife advised the proponents that the best way forward to reach an amicable outcome would be to contact the relevant decision making authorities to organise a meeting to discuss options for a suitable development footprint on Lot 42. Parks and Wildlife has had no further correspondence with the proponents since the mediation meeting that was held on 16 March 2017.</p> <p>Lot 41 Gaebler Road</p> <p>Wetlands</p> <p>Parks and Wildlife acknowledges that the proposed Structure Plan map in Attachment A of the Structure Plan Report, shows the Structure Plan boundary being located to the west of CCW UFI 14101 and its associated 50m buffer. However, the map doesn't identify CCW UFI 14101 and states that the remainder of Lot 41 is subject to further planning. This is presumably due to the current Supreme Court proceedings in relation to the wetland classification on Lot 42.</p> <p>Parks and Wildlife suggests that a notation is put into the Structure Plan that acknowledges the current Supreme Court process and informs further planning that should the wetland classification remain as a CCW, then the Structure Plan should identify the area of CCW inclusive of a 50m buffer.</p>	<p>Noted. In regards to Lot 41, the City has met with the proponent and advised that as long as the CCW classification remains over the site, the portion of Lot 41 within the CCW will be assessed as a CCW and required to be reserved or zoned for conservation.</p> <p>Noted and supported. However, since the Structure Plan has been lodged and is being assessed with the CCW classification still in place over Lot 41, the proposal needs to acknowledge that this is the case. The proposal is required to reflect the planning and environmental landscape and legislation at the time of lodgement, not anticipate future outcomes that may not come to fruition ie. removal of CCW classification over the land. There has been no move to reclassify this wetland classification thus far by DPaW. Therefore, whilst acknowledgement of the Supreme Court process</p>

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>Remnant Vegetation</p> <p>Aerial mapping suggests that the proposed development will impact on approximately 0.75ha of vegetation, containing <i>Banksia sp.</i> that is likely to be indicative of the Banksia Woodlands Threatened Ecological Community (TEC). The Banksia Woodlands TEC is federally listed under the <i>Environment Protection and Biodiversity Conservation Act 1999</i> (EPBC Act).</p> <p>Parks and Wildlife suggests that the proponents give due consideration as to whether the proposal should be referred to the Federal Department of Energy and Environment for assessment under the EPBC Act.</p>	<p>provides further information on this issue, it is not necessary to include this detail since at the time of assessment of this Structure Plan the CCW still exists over Lot 41.</p> <p>Noted. The Flora and Vegetation Survey is outdated since the previous survey was prepared in 2007. The survey also does not acknowledge that Banksia Woodland is now listed as a Threatened Ecological Community and occurs onsite. The Survey also needs to state the proponent's obligations in terms of referral to the Federal Department of Energy and Environment. Thus, the Flora and Vegetation Survey is insufficient and cannot be approved in support of the Structure Plan.</p>
14	Bush Forever Locked Bag 2506 PERTH WA 6001	<p>Thank you for referring the proposed structure plan for Lot 41 Gaebler Road, Hammond Park to the Policy branch at the Department of Planning (previously Bush Forever office). Lot 41 is adjacent to Bush Forever area 392 and is separated by a road. No comments are provided at this point in time.</p> <p>The South West Metropolitan Statutory Planning team within the Department of Planning will liaise with the Policy branch, if required, when/if the structure plan is sent to the WAPC for endorsement.</p> <p>It is advised however the Department of Parks and Wildlife is consulted given the remnant vegetation and conservation category wetland located in the subject site.</p> <p>Please contact me if you have any further queries.</p>	<p>Noted. The Structure Plan has been referred to DPaW for comment.</p>



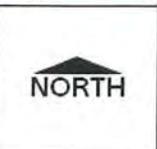

City of Cockburn
C.I.S Services Department

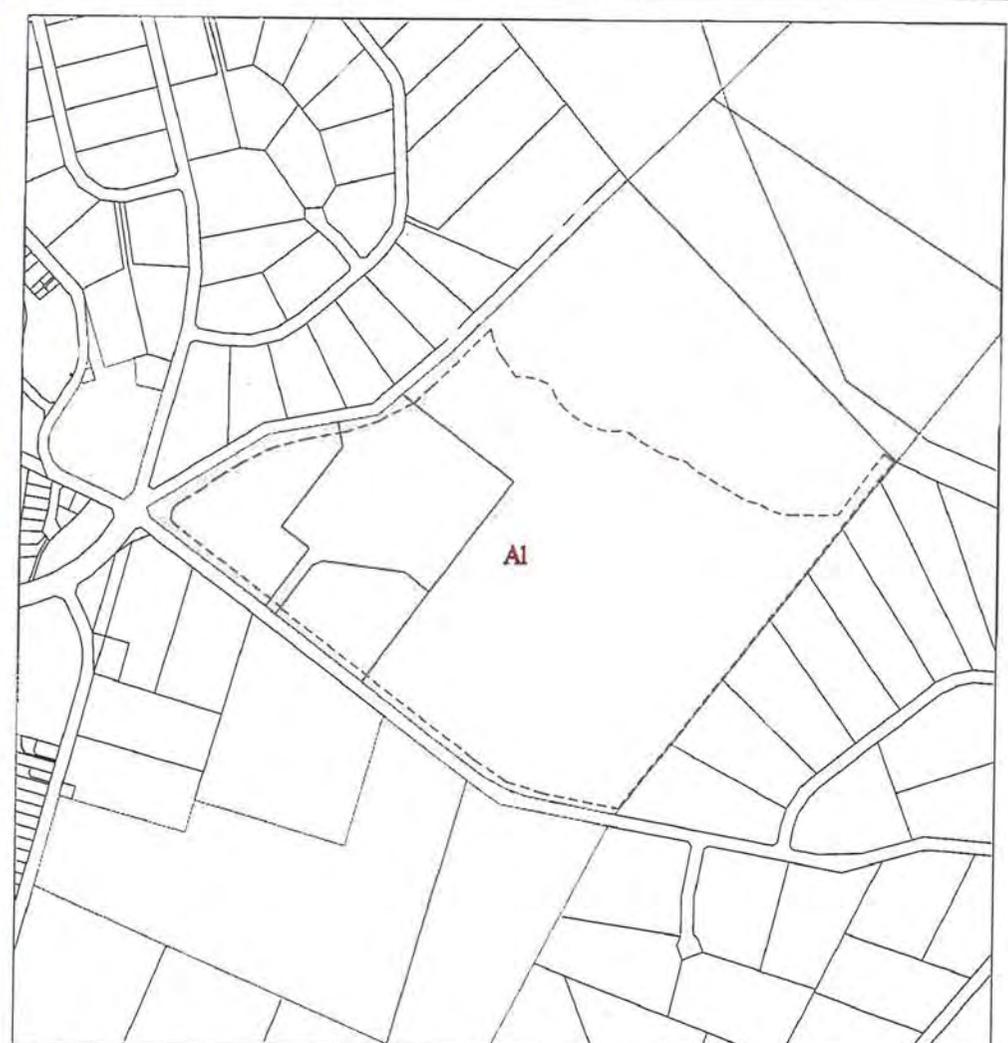
Attachment 1: Location Plan
Scheme Amendment (Adoption) Urban Stone
Lots 701, 703 (excluding Bush Forever Area 388) and Lot 702
Jandakot Road, Jandakot
109/ 048 – Additional Use No. 1 ('AU 1')

PRINTED ON:
24/03/2017

SCALE = 1:15000

DISCLAIMER - The City of Cockburn provides the information contained herein and bears no responsibility or liability whatsoever for any errors, faults, defects or omissions of information contained in this document.





EXISTING ZONING MAP

- Region Scheme Reserves**
- CG Commonwealth Government
 - SEC Commonwealth Government
 - Water Catchments

- Local Scheme Reserves**
- Parks and Recreation
 - Local Road

- Local Scheme Zones**
- Development
 - Residential
 - Resource
 - Special Use

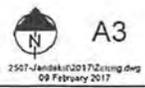
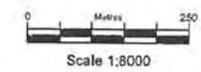
- Other Categories**
- R20 R Codes
 - AI Additional Uses
 - RI Restricted Uses
 - SU1 Special Use Area
 - Building Envelopes

- BVA Bushfire Vulnerability Area
- JA Jandakot Airport
- DCA Development Contribution Area
- No Zone

MGA
TOWN PLANNERS

Ph: (08) 9321 3011
Fax: (08) 9324 1961
email: mga@global.net.au

CITY OF COCKBURN
AMENDMENT No. __
DISTRICT PLANNING SCHEME No. 3



ATTACH 2

2507-Jandakot-02017Zoning.dwg
09 February 2017

Scheme Amendment No. 112

Proposed Scheme Text Schedule of Modifications identified below as those changes highlighted in yellow.

No.	Description of Land	Additional Use	Conditions
AU 1	<p>Lots 701, 702 and 703 (excluding Bush Forever Area 388) Jandakot Road, Jandakot. [Formerly Lots 101, 103 and Jandakot Road, Jandakot.]</p>	<p>• Nursery; • Masonry Production; • Warehouse, Showroom and Storage where the display, selling, hiring or storage of goods, equipment, plant or materials and the incidental activities do not pose risk of pollution to the below ground public drinking water source.</p> <p>The Use Class Definition's for 'Warehouse', 'Showroom' and 'Storage' are defined in Schedule 1 of the Scheme inclusive of the supplementary restrictions as mentioned above which limit the nature of the permissible goods, equipment, plant or materials to those which do not pose risk of pollution to the below ground public drinking water source.</p> <p>1. Environmental Requirements Industrial Wastewater: All wastewater produced from activities on-site must be disposed of to a system approved by the Local Government and in liaison with the</p>	<p>Development Approval for Lots 701, 702 and 703 Jandakot Road, Jandakot, are subject to;</p> <p>a) Due consideration to groundwater risk minimisation.</p> <p>b) No bulk storage of green-waste, compost or 'Toxic and Hazardous Substances' ('THS') are permitted above 25 litres in total volume, excluding fuel within vehicle fuel tanks. THS includes pesticides, herbicides, fuel (storage), explosives, flammable liquids, cleaners, alcohols, fertilisers (other than on lot 702 under current development approvals), medical or veterinary chemicals, pool chemicals and corrosive substances; inclusive of the substances listed in the Poisons Act 1964 (Appendix B). These substances may only be stored in volumes above 25 litres if contained within domestic sized packages ready for end-use in domestic situations.</p> <p>c) Due consideration and compliance with the Western Australian Planning Commission's 'Transport Assessment Guidelines' for appropriate.</p> <p>d) The prior preparation and approval of a Local Development Plan ('LDP') detailing;</p>

	<p>Department of Water.</p> <p>Site Chemical Risk: A Site Chemical Risk Assessment Report being prepared and implemented and regularly updated.</p> <p>Dust Management: No visible dust generated by any aspect of operations on-site is to leave the subject land. The operator is required to submit to the Local Government, after consultation with the Department of Environment Regulation a Dust Management Plan. The Dust Management Plan must be to the satisfaction of the Local Government, and upon approval by the Local Government, is to be adhered to by the land owner(s) at all times.</p> <p>Noise Emissions: The development is to comply with the <i>Environmental Protection Act 1986</i>, which contains penalties where noise limits exceed those, prescribed by the <i>Environmental Protection (Noise) Regulations 1997</i>. If noise emissions from loading operations and the block plant fail to comply with the <i>Environmental Protection Act 1986</i>, additional acoustic measures must be carried out as soon as reasonably practical to ensure the</p>	<p>i. The standards to be applied for physical development in order to ensure the protection of the below ground public drinking water source;</p> <p>ii. Vehicle access and egress arrangements;</p> <p>iii. Noise mitigation measures pursuant to the details of an acoustic report where required (refer to point 'e' below);</p> <p>iv. Interface controls and/ or measures with regard to Bush Forever Area 388, including, but not limited to; a hard road edge within the AU1 area abutting the Bush Forever area and/or bushland identified for protection; Bushfire mitigation measures being provided outside the Bush Forever area within the AU1 area; an appropriate wetland buffer, if considered relevant by the assessing authority, and; drainage to be contained within the AU 1 area.</p> <p>e) With regard to any application for 'Warehouse', 'Showroom' or 'Storage', the preparation and lodgement of a report prepared by a suitably qualified acoustic consultant detailing the potential noise impact on noise sensitive land uses. The report shall demonstrate how the proposed development has been acoustically assessed and designed for the purposes of minimising the effects of noise intrusion and/or noise emissions. The report must demonstrate the measures required to address noise to the Local Government's satisfaction</p>
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	<p>use complies with the Act.</p> <p>Lighting: The installation and maintenance of lighting must at all times comply with the requirements of Australian Standard AS 4282-1997 "Control of the Obstructive Effects of Outdoor Lighting".</p> <p>Complaints: The operator must prepare a "Complaints Handling Procedure" to ensure that there is a process for administering any complaints including the recording, investigation and response to any concern regarding the operation.</p> <p>2. Design Requirements Building design and location shall minimise the visual impact of the development from surrounding residents inclusive of appropriate buffers, noise bunds and vegetation (light and visual) screening. Building materials and colours must be clad or coloured to complement the surroundings, and/or adjoining developments in which it is located, and shall use non-reflective materials and colours. Regard shall be had</p>	<p>and be implemented and maintained as part of the development of the land</p> <p>f) Development of any 'Warehouse', 'Showroom' or 'Storage' must:</p> <ol style="list-style-type: none"> i. Be connected to a reticulated sewer system; ii. Have all lighting comply with the requirements of Australian Standard AS-4282-1997 "Control of the Obstructive Effects of Outdoor Lighting" and the Civil Aviation Regulations 1988 and the Civil Aviation Safety Authority Manual of Standards in accordance with the details prescribed within the <i>Jandakot Airport Masterplan</i>; iii. Have all structures comply with the Limitation Surfaces in accordance with the details prescribed within the Jandakot Airport Masterplan; iv. Have a 'Site Chemical Risk Assessment Report' prepared, implemented and regularly updated, including annual reporting to the Local Government and the Department of Mines and Petroleum. v. Lodge a Dust Management Plan for approval by the Local Government and ongoing compliance by the property owner(s). <p>g) Building design, internal vehicles access ways, and locations shall minimise the amenity impact of the development from surrounding residents.</p>
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	<p>to the screening of product storage. Staging Plan in the form of a Local Development Plan ('LDP') shall be prepared by the applicant approved by the Local Government prior to any development within Additional Use area 1.</p> <p>3. Traffic requirements</p> <p>Planning proposals shall demonstrate appropriate traffic generation calculations and traffic impact assessments on the current and future planned road network. Mitigation measures shall demonstrate viability and road upgrade responsibilities. The extent of all traffic related considerations should be identified and agreed upon early in the planning process to the satisfaction of the Local Government.</p>	
	<p>h) Building materials and colours must be clad or coloured to complement the surroundings, and/ or adjoining developments in which it is located, and shall use non-reflective materials and colours.</p> <p>i) No below ground storage is permitted.</p> <p>j) Stormwater from roofs and clean paved areas should be directed away from potentially contaminated areas where THS (below 25 litres in total volume) are stored or handled. Stormwater from carpark areas is to be managed as recommended in the Stormwater Management Manual for Western Australia (reference 8d) or relevant equivalent.</p> <p>k) Any liquids discharged to the environment (via soakage or ground application) should have been tested as compatible with downstream water resource values. Discharge to drains or waterways should not occur due to the risk of release of contaminated water. The effluent quality should be determined by sampling in accordance with Australian Standard 5667 Water quality sampling (reference 9b) or relevant equivalent.</p> <p>l) As part of future development and/or subdivision of the subject land, the applicant shall; provide the land for the Bush Forever site (as agreed) free of cost and ceded to the Crown. This is to be provided at the first available planning opportunity.</p>	

			<p>m) As part of future development and/or subdivision of the subject land, the land owner/ applicant will be expected to:</p> <ol style="list-style-type: none"> i. Provide the land for the widening of the adjoining section of Jandakot Road from a single carriageway road to a dual carriageway road free of cost to the City of Cockburn; ii. Upgrade the adjoining section of Jandakot Road from a single carriageway to a dual carriageway.
			<p>n) Appropriate native vegetation planning/ planting consideration and conditioning within the area of land east of the AU1 boundary and the adjacent rural residential 'Resource' zoned lots. This land is considered to be a 'rural amenity buffer'. Accordingly, its embellishment should be proportionally reflective of the scale of the proposed development.</p>
			<p>o) The minimum subdivision and development application lot size requirements and leasehold lot size requirements are as per <i>State Planning Policy 2.3 (Jandakot Groundwater Protection)</i> minimum lot size requirements.</p>
			<p>p) The subject site is likely to be affected by aircraft noise as the 20, 25 and 30 Australian Noise Exposure Forecast ('ANEF') contours falls within the AU1 area. Acceptable land use and building types should be compliant with regard to <i>State Planning</i></p>

Policy 5.3 (Land Use Planning in the Vicinity of Jandakot Airport) and the Building site acceptability table from AS2021.

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6 DISCUSSION

It is understood the zoning of Lot 103 Jandakot will be changed to allow for showrooms and warehouse/logistics types of premises.

The potential for adverse impacts from ground vibration emanating from the site was considered negligible given that significant sources of vibration such as crushers, metal shredders or other heavy industrial equipment are not expected on the development site, and the separation distances to the nearest receivers.

In relation to noise, an assessment of typical noise emissions for the type of premises proposed was conducted. In relation to showrooms, the Regulations are expected to be complied with at all times since noise emissions from this type of premises would only include air conditioning noise, daytime deliveries and daytime waste collections.

In relation to warehouses, the type and location of activity dictates whether compliance with the Regulations can be achieved or not. Forklift work in an open yard associated with warehouses and outdoor storage areas, and the use of refrigerated trucks for deliveries can result in exceedances of the assigned noise levels at all times.

Based on preliminary planning and results of the assessment carried out, the following levels of acoustic assessment are recommended depending on the premises type:

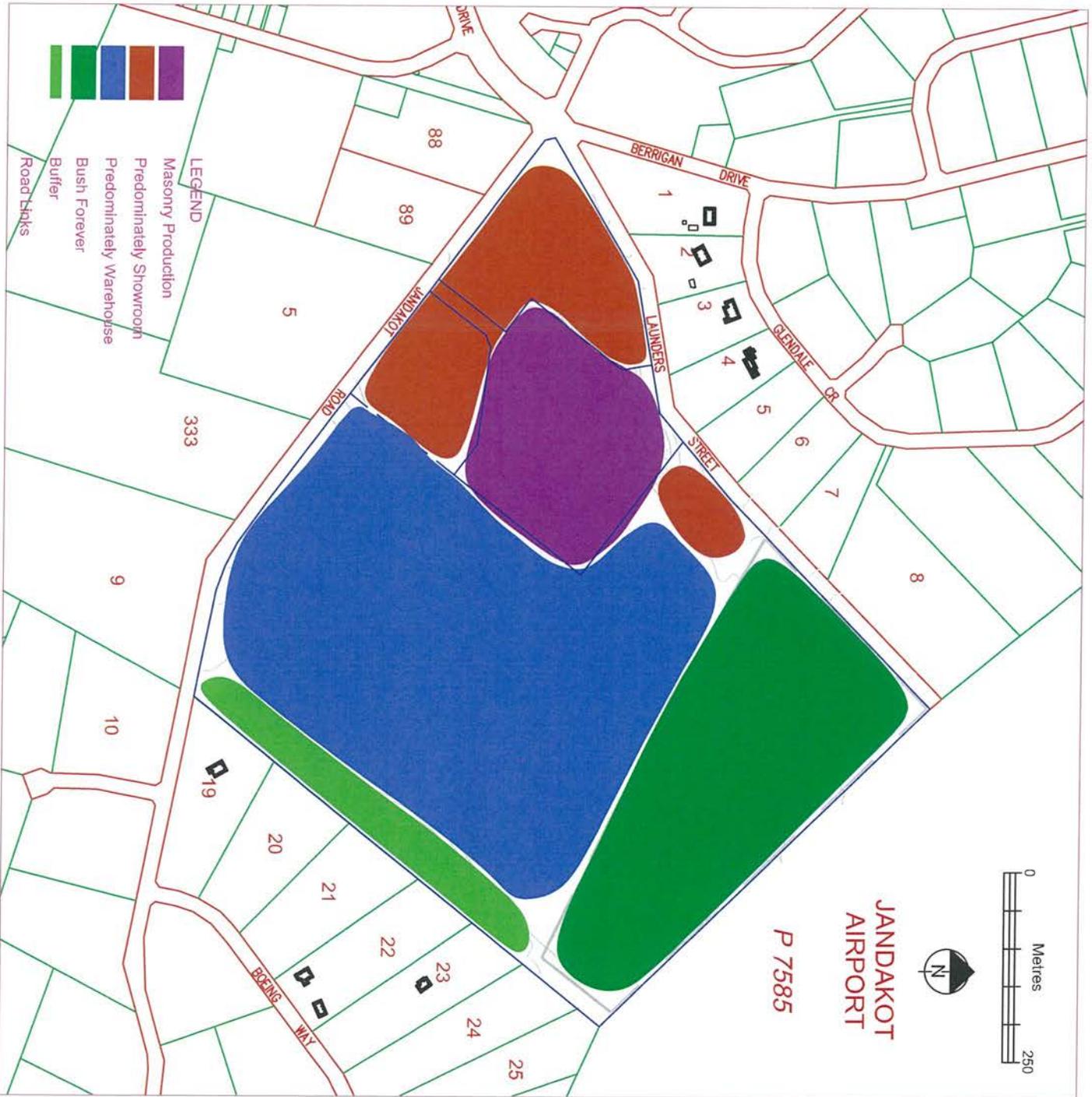
- For the showroom premises in the north-west of the site, a site specific acoustic assessment is not expected to be required provided the relevant provisions of the Noise Management Plan shown in *Appendix B* are implemented.
- For warehouse/logistics premises located behind showrooms or other large buildings e.g. central to the development, a site specific acoustic assessment is not expected to be required provided the relevant provisions of the Noise Management Plan shown in *Appendix B* are implemented.
- For warehouse/logistics premises on the east side, or any premises where the relevant provisions of the Noise Management Plan shown in *Appendix B* cannot be implemented, a site specific acoustic assessment should be undertaken.

Finally, it is noted each premises occupier will be required to comply with the Regulations and, in accordance with the City of Cockburn local planning policy 1.12, detailed acoustic assessments may be required by the City for specific occupiers.

Lloyd George Acoustics

Appendix A

Preliminary Development Plan



Concept Plan

Lloyd George Acoustics

Appendix B

Noise Management Plan

This Plan presents generic noise control mitigation measures that are to be considered by each premises occupier in order to minimise noise impacts to adjacent premises and nearby residential receivers.

Air-Conditioning and Refrigeration Plant

- Locate plant on roof and/or away from residential receivers e.g. side or back of building
- Mechanical plant should be selected so as to be able to operate at lower capacity during evening / night-time

Deliveries

- All deliveries to occur Monday to Saturday between 07.00 and 19.00 and to maximise time between trucks in order to avoid multiple noise sources and trucks idling/queuing
- In addition to above, refrigeration plant mounted to delivery trucks are not to be operated for more than 24 minutes in any 4 hours
- Where reversing is required, delivery vehicles should have broadband type reversing alarms fitted rather than standard tonal alarms. Where a safe work practice can be provided, for example, route not requiring reversing or use of spotters, reversing alarms should be turned off.
- Consider impact noise mitigation in loading/off-loading areas such as rubber matting to minimise impact noise of bins, pallets, etc.

Plant Yard / Outdoor Storage Areas

- Open plant yards and outdoor storage areas within which forklifts or other mobile equipment will be used on a regular basis to be located behind large buildings and away from sensitive receivers to maximise noise barrier effects
- Forklift or similar mobile equipment operations in outdoor areas are to be daytime operations i.e. 07.00 to 19.00 Monday to Saturday
- Mobile equipment and trucks should have broadband type reversing alarms rather than standard tonal alarms
- Consider use of impact matting in lay down / storage areas, especially in cases where hollow metals are stored, to minimise impact noise

'Warehouse/Logistics' Building Design and Sitting

- Building(s) to be designed and oriented to maximise noise attenuation effects to nearby residential receivers and adjacent premises
- For building(s) on the east side of the development, large openings such as roller shutter doors will preferably be facing away from residential receivers

**SCHEDULE OF SUBMISSIONS
PROPOSED AMENDMENT NO. 112**

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
1	WA Gas Networks (ATCO Australia) Locked Bag 2507, Perth Business Centre PERTH WA 6849	SUPPORT ATCO Gas operates Medium Pressure Gas Mains within Jandakot Road and PEHP Gas Mains and infrastructure within the Orion Road, road reserve in the near vicinity. ATCO Gas does not have any objection to the proposed Amendment 112. Please see the attached Figure for your record (image provided).	Noted.
2	Department of Environment Regulation Locked Bag 33, Cloisters Square PERTH WA 6850	SUPPORT DER has no comment on this matter in reference to regulatory responsibilities under the Environmental Protection Act 1986 and the Contaminated Sites Act 2003.	Noted.
3	Department of Fire and Emergency Services (DFES) GPO Box P1174 PERTH WA 6844	SUPPORT Our Land Use Planning Officer will review the proposed Scheme Amendment No. 112 to Lots 101, 103 & 104 (Now lots 701, 702 and 703) Jandakot Road and provide DFES comment by no later than 7 February 2017 as requested. (No further submission received by the City of Cockburn).	Noted. No further submission received by the City of Cockburn from DFES.
4	Department of Transport GPO Box C102 PERTH WA 6839	SUPPORT The Department of Transport (DoT) has no comment to provide for the above. I note we have also forwarded a copy of the letter to Main Roads to respond to you directly with any comments they may have.	Noted.
5	Department of Water PO Box 332 MANDURAH WA	SUPPORT The Department of Water (DoW) has reviewed the proposal and has the	Noted. The DoW and the City of Cockburn were in close

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
	6210	<p>following advice.</p> <p>Priority 2 Public Drinking Water Source Area The land subject to this proposal is located within the Jandakot Underground Water Pollution Catchment Area (UWPCA), which has been declared for Priority 2 (P2) source protection. P2 areas are defined and managed to maintain or improve the quality of the drinking water source with the objective of risk minimisation. P2 areas occur within PDWSAs where the land is zoned rural and the risks need be minimised. Low levels of development consistent with the rural zoning are considered appropriate (generally with conditions) in P2 areas.</p> <p>The Jandakot UWPCA is managed in accordance with the Western Australian Planning Commission's Statement of Planning Policy No 2.3 <i>Jandakot groundwater protection policy</i> (SPP 2.3) and the DoW's Water Quality Protection Note (WQPN 25) <i>Land use compatibility tables for public drinking water source areas</i> (DoW, 2016). The Urbanstone facility that currently operates on the site is deemed an incompatible land use in the Jandakot UWPCA. However, it is an operation that is a pre-existing, non-conforming land use that was established prior to the gazettal of <i>SPP 2.3</i>. Therefore the land use is permitted to continue to operate in line with the best management practices under non-conforming use rights of this policy.</p> <p>Thus with regards to the proposed amendment to allow new provisions and expand the existing land use over the lots, the Department has no objections subject to the employment of best management practices outlined in the following</p> <p>Water Quality Protection Notes (WQPN) found at www.water.wa.gov.au</p> <ul style="list-style-type: none"> • WQPN 32: Nurseries and garden centres • WQPN 52: Stormwater management at industrial sites • WQPN 65: Toxic and hazardous substances - storage and use • WQPN 90: Organic material - storage and recycling • WQPN 93: Light industry near sensitive waters 	<p>liaison in the early stages/ formulation of this amendment. The City and applicant sought DoW in principle approval prior to initiation. The DoW support in this regard is understood, accepted and it is therefore advised by way of this schedule of submission that the developer adhere to the WQPN as listed by the DoW.</p>
6	Landowner (Confidential)	<p>OBJECTION</p> <p>(No further comment provided).</p>	Noted.
7	Department of Education 151 Royal Street	<p>SUPPORT</p> <p>The Department has reviewed the document and advises that it has no</p>	Noted.

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
	EAST PERTH WA 6004	objection to the proposal.	
8	Banjup Residents Group 207 Liddelow Road BANJUP WA 6164	<p>OBJECTION</p> <p>1. Current Position All of Jandakot east of the Kwinana Freeway is on the Jandakot Water Mound and is so subject to State Planning Policy 2.3 – Jandakot Groundwater Protection. Almost all properties along Jandakot Road are rural in nature, with banksia woodland predominating. Houses are typically on 2 hectare lots that were subdivided in the 1980s. There is no gas, mains water, or main sewerage. All are zoned 'Resource' in the City of Cockburn's Town Planning Scheme.</p> <p>Schaffer Corporation owns Lots 101, 103, and 104 Jandakot Road, Jandakot at the Berrigan Drive end. In Cockburn's Town Planning Scheme they are all zoned 'Resource' but have an additional use AU1 – Nursery, Masonry Production, Warehouse only where ancillary to Masonry production, and Showroom only where ancillary to Masonry Production. None of the rural residential properties along Jandakot Road and its side streets has additional uses permitted.</p> <p>2. Jandakot Rural Residents' Submission on the South Metropolitan & Peel Planning Framework In July 2015, the Banjup Residents Group held 3 public meetings and on behalf of the overwhelming majority of Jandakot rural property owners made a submission to the WA Planning Commission in relation to the South Metropolitan & Peel Planning Framework that is a sub-set of the Perth@3.5million planning strategy.</p>	<p>Noted. It is understood none of the 'rural' residential properties (zoned 'Resource' under TPS No. 3) along Jandakot Road and its side streets has additional uses permitted.</p> <p>The existing AU1 provisions (prior to this Scheme Amendment to modify the AU1 provisions) resulted from a separate Scheme Amendment (Amendment 91) which was gazetted on 21 June 2013 by the then Hon. Minister for Planning.</p> <p>Both the previous Amendment and the current amendment were lodged by consultants engaged by the landowner.</p> <p>The City has not received any proposed amendments, for consideration, to TPS No. 3 for any of the 'Resource' zoned lots. The City is not the applicant for the proposed amendment and does generally not prepare amendments of this nature for multiple landholdings unless that amendment is a revitalisation strategy. In which case, revitalisation strategies and the associated scheme amendments are undertaken with extensive forms of community consultation and broadly in accordance with State planning documents, such as <i>Perth and Peel @ 3.5 Million</i> or an adopted Local Planning Strategy.</p> <p>It is noted the objection speculates "<i>it is doubtful that rural lifestyles can be sustained when surrounded by urban, commercial, and industrial activity and their associated day and night noise and traffic.</i>" This speculation is noted. With regards to the proposed Scheme Amendment the document provides for an Environmental Assessment, Noise and Vibration Impact Assessment, Bushfire Management Plan, Traffic Report and an Engineering servicing report. The suite of supplementary appendices</p>

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>In its submission, the BRG said that the 150 or so rural residential properties (typically 2 hectare) sandwiched between Jandakot Road, Berrigan Drive, and the airport would be severely affected by the developments proposed in the draft Planning Framework. The Framework did not address the adjoining and conflicting land uses in the north Jandakot area. It is doubtful that rural lifestyles can be sustained when surrounded by urban, commercial, and industrial activity and their associated day and night noise and traffic.</p>  <p>The rural residents' land areas 1, 2, and 3 that are affected are shaded yellow below. Schaffer's land is shaded red:</p> <p>The Banjup Residents Group urged the WAPC to rescue the residents of north Jandakot from their planning predicament. The area is close to the Roe Highway, the Kwinana Freeway, the railway, and the commercial hubs of Jandakot City and Cockburn Central. The BRG said that the area should be rezoned for urban or commercial development, contiguous then with the commercial park of Jandakot City and the urban housing arising from the old sand quarries.</p>	<p>aims to address the relevant State Planning Policies to which they relate. The proposed amendment and the Council report, outlining Councils assessment, aims to demonstrate compliance in these respective areas. Ultimately the proposal needs to adequately demonstrate that the proposed amendment can be accommodated whilst protecting an appropriate level of 'rural' amenity. This balance of consideration and the associated appendices have been assessed by the various technical experts in the respective fields. The outcome of the assessment has resulted in modification. For example the Acoustic report has been modified as a result of assessment, in addition to the environmental assessment. Similarly the appendices have been advertised to the full suite of government agencies and service providers. Their responses are provided within this table of submissions which is public information. Additionally the scheme text provides a list of future development application condition requirements. These conditions aim to protect the surrounding existing rural amenity.</p> <p>In relation to Point 3 and the reference to Councils submission to the WAPC with respect to Item 14.4 of 9/7/2015 OCM, the City posed a question to the WAPC for two considerations. It may be appropriate that 'this rural ribbon be maintained' however 'if the amenity in this area is so far removed from a rural setting, then should consideration be given to an alternative land use?' This was a question from the CoC to the WAPC from a single perspective in the land use planning framework. The City does note the changing character of the area. Notwithstanding it is important to note the context in which this statement was raised. Perth and Peel @3.5 million is a broad strategic document. Its implementation is generally the result of separate MRS and TPS amendments, such as this amendment. It is also important to note any MRS and TPS amendments will be subject to 'due process' which involves the scrutiny of proposals. In this example regarding AU1 the Amendment, as mentioned above, is reviewed along with its technical appendices from various government agencies in the formulation and recommendation to the</p>

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		<p>3. Cockburn's Submission to WAPC</p> <p>The City of Cockburn also made a submission in in relation to the South Metropolitan & Peel Planning Framework. In it the City expressed its concerns about the future of the land surrounding Jandakot Airport:</p> <p><i>Do we want to see this retained in a rural setting, typically 2ha lot sizes with the landscape containing buildings, or is this rural setting to the point that it won't deliver the intended rural amenity? It may be appropriate that this rural ribbon be maintained around Jandakot Airport, as a limitation to urban development encroaching closer to the airport. However, if the amenity in this area is so far removed from a rural setting, then should consideration be given to an alternate land use?</i></p> <p>Clearly, Cockburn is concerned about the viability of the Jandakot rural areas in the face of the increasing urbanisation that surrounds them.</p> <p>4. Schaffer's Application to the City of Cockburn</p> <p>Schaffer Corporation proposes to develop about 26 hectares of land adjoining its Urban Stone operation on Jandakot Road for warehousing and showroom purposes. To facilitate this, Schaffer is requesting Cockburn to permit additional land uses.</p> <p>In the WAPC's first draft for public comment of the Perth and Peel @ 3.5 million Planning Framework, Schaffer's lot 103 was proposed for 'public purposes'. However, correspondence in July 2015 between the WAPC chairman and consultants for the Schaffer Corporation (see attachment copied from Schaffer's application) indicated that an alternative site more consistent with the Canning Vale Sports Master Plan would be considered by the WAPC. In that correspondence, the WAPC chairman also encouraged Schaffer not to "constrain" its plans for lot 103 to become an industrial site.</p> <p>What Schaffer submitted in relation to the Planning Framework is unknown but is not likely that they would have ignored the WAPC chairman's prompting.</p> <p>5. South Metropolitan & Peel Planning Framework Outcomes</p> <p>Our submission could be overtaken by events but in the absence of any guidance from the WAPC, we see 3 possible outcomes.</p>	<p>Minister for planning. It is also important to note the draft Perth and Peel @ 3.5 Million is one document which provides some guidance on land use planning. It is neither the single decision making instrument nor does it provide an exhaustive list of detailed site specific assessments.</p> <p>In relation to point 5. As mentioned above, the Banjup Residents Group may decide to lodge a Scheme Amendment for their various landholdings. Should this be acted upon it is recommended to do so following the finalisation of the broad strategic document Perth and Peel @ 3.5 Million. Any (hypothetical) proposed Amendments by the Group, for their land, will need to follow the same process as the AU1 amendment. Any future hypothetical amendment on this basis would be offered the same level of attention by the City with respect to its assessment. The City does not generally, as discussed above, prepare amendments such as the one being suggested by the Group on behalf of landowners.</p> <p>It is understood under 5.2 of the submission that there is a notion being expressed by the Group which suggests "WAPC has ignored the plight of residents sandwiched between Treeby and Jandakot Airport". With respect to this point it is noted Perth and Peel @ 3.5 Million is in its draft format. At this point in time, as far as the City is aware, the document has not yet been finalised by the WAPC.</p> <p>It is understood there is a request by the Group seeking the permitting of "all rural property owners in Jandakot an additional land use of 'Showroom and Warehouse/ Storage". It is not understood from the City at this point as to how this is intended to work from a land use planning perspective? One of the advantages of the Schaffer land is that it is roughly 40ha in area. The benefit of the larger property is that separation distances from noise permitting sources are more manageable. If the same land use/(s) were to be provided to the 2ha lots, noting some residents you would assume would be seeking to maintain a 'rural/residential' lifestyle, the associated land use conflicts would be magnified. This is an issue, among others,</p>

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		<p>1. All land rezoned urban 2. No land rezoned 3. Schaffer land only rezoned urban</p> <p>The 3 possible outcomes have different impacts upon nearby Jandakot rural residents and cause those residents to submit 3 different recommendations to Cockburn Council.</p> <p>We recognise that if Cockburn Council did adopt our recommendations as set out below, then they would need to be endorsed by the WAPC. However, we trust that Council would prevail on behalf of its ratepayers.</p> <p>5.1. All Land Rezoned Urban</p> <p>This would be the best outcome for Jandakot rural residents. It would release them from the planning blight they currently suffer and allow them to make practical plans for their futures.</p> <p>In this case, with the same level playing field for all, the Schaffer application for an additional land use would be superseded by the rezoning of the whole rural Jandakot area to urban, so allowing Schaffer to develop its property for commercial purposes.</p> <p>However, there are transitional implications for rural property owners that are discussed in Attachment 1 of our submission.</p> <p>5.2. No land rezoned</p> <p>This would be a great disappointment to Jandakot rural residents. It would mean that the WAPC has ignored the plight of residents sandwiched between Treeby and Jandakot Airport. Their rural amenity will be continuously eroded; their lifestyles would be detrimentally affected by ever increasing traffic and noise on their doorsteps; and their properties will be unsaleable because of the WAPC induced planning blight.</p> <p>In this case, Cockburn councillors could remedy the invidious position of their ratepayers by permitting to all rural property owners in Jandakot an additional land use of 'Showroom and Warehouse/Storage', exactly as requested by Schaffer in their own planning application to the City of Cockburn. There would be some transitional implications that are discussed later in our submission.</p> <p>Jandakot rural residents would be even further disappointed if Council did not</p>	<p>which the Group would need to consider as part of any proposed Amendment of their own.</p> <p>In relation to point 6, please note the subject area to which the Group refers is not zoned 'Development' under TPS No. 3. On this basis the City is not able to develop a 'Structure Plan' to govern the land in question. The City could consider however to prepare a District (non-statutory) structure plan. This however would need to consider the outcome of the final version of Perth and Peel @ 3.5 Million document. As mentioned, at this point in time the document is in draft format and therefore it is not appropriate for a lower order local government hypothetical DSP to be prepared prior to the finalisation of the state governments final Perth and Peel @ 3.5 Million document.</p> <p>Section 7 of the submission makes reference to 'equality of opportunity'. As mentioned above, the applicant on behalf of the landowner has submitted a scheme amendment proposal. This proposal is the subject of this application. On this basis the Planning and Development Act 2005 offers the same equity to all landowners. Specifically, should the Group seek to lodge their own Amendment, they are permitted to do so. Such applications would be subject to the same application fee, assessment process and general procedures as is the case with all amendment submitted to the CoC for consideration.</p> <p>It is noted the submission recommends the 'deferral' of any decision with regards to this amendment. It is noted however the WAPC is the final assessing authority in this respect. The WAPC has indicated an in principle support of the proposed Amendment on the basis that the WAPC considers the proposal to be consistent with their Perth and Peel @ 3.5 Million documentation. This is noted within the Amendment documentation. On this basis the City is not inclined to defer the proposed Scheme Amendment however please note the WAPC and the Hon. Minister of Planning will ultimately determine the proposed Scheme Amendment.</p>

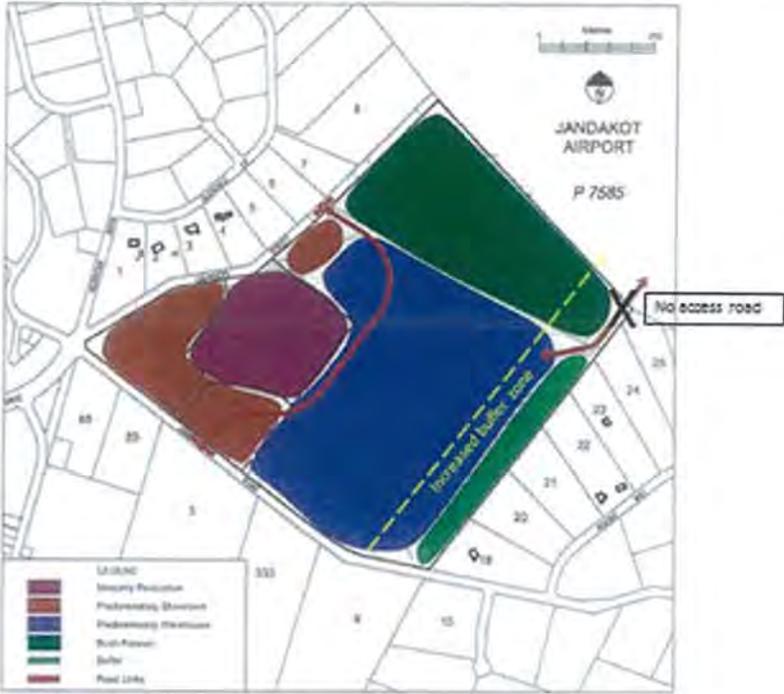
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		<p>adopt this recommendation. However, if that were to occur, then, in all conscience, Council should not permit the additional land use to Schaffer – it would only exacerbate Jandakot rural residents’ plight by squeezing the urban sandwich around them with a further 26 hectares of commercial activity to their west.</p> <p>In summary, in this outcome all rural Jandakot properties should be permitted the same additional land use or none should.</p> <p>5.3. Schaffer land only rezoned urban</p> <p>This would be a travesty.</p> <p>In this case, Jandakot rural residents would query the WAPC decision as showing clearly the influence of the ‘big end of town’ over the WAPC.</p> <p>As with the no land rezoned outcome, Cockburn councillors could remedy the invidious position of their ratepayers by permitting to all rural property owners in Jandakot an additional land use of ‘Showroom and Warehouse/Storage’. This should have the effect of ‘homogenising’ the land uses between urban Treeby and the Airport commercial park.</p> <p>As before, this recommendation would have some transitional implications as discussed in Attachment 1.</p> <p>6. Future of Jandakot Rural Areas and Need for a Structure Plan</p> <p>Clearly, Schaffer’s application and the outcome of the WAPC’s deliberations call into question the future of the whole of the Jandakot rural area. Rather than the City of Cockburn approach piecemeal the planning of the area, it would be a far better outcome if the City were to develop a formal structure plan for the whole area from Berrigan Drive to Warton Road. The Schaffer application would form an important part of the structure plan but so, too, would residents’ considerations.</p> <p>7. Resolutions for Council</p> <p>Our position is simple: by whatever means either all Jandakot rural properties are permitted commercial land uses or none are. We urge the Council of the City of Cockburn to ensure equality of opportunity between residential ratepayers and commercial developers, as formalised in a clear Structure Plan for the whole area. To realise this, we recommend these resolutions to</p>	<p>Please note the Acoustic report submitted as part of the proposed Scheme Amendment stipulates under section 6 ‘Discussion’ suggests a ‘site specific acoustic assessment’ (at Development stage) may be required. It also provides for general guidance for decision making at DA stage. On this basis the expected noise outcomes is unknown at this conceptual stage. Notwithstanding the specific noise/ disturbance considerations with regard to any future developments will be assessed at DA stage. Please note also under point ‘e’ of the proposed Scheme Text under the “conditions” column, “with regard to any application for ‘Warehouse’, ‘Showroom’, or ‘Storage’, the preparation and lodgement of a report prepared by a suitably qualified acoustic consultant detailing the potential noise impact on noise sensitive land uses”. In this regard development applications will be subject to further statutory scrutiny at DA stage.</p>

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>the Council of the City of Cockburn:</p> <p>EITHER</p> <ul style="list-style-type: none"> • Defer any decisions on Schaffer's application and on alternative land uses for current residential landowners in rural Jandakot until after the publication of the WAPC's final South Metropolitan & Peel Planning Framework; then • Develop a formal structure plan for the whole of the Jandakot rural area from Berrigan Drive to Warton Road that includes: <ul style="list-style-type: none"> ○ Schaffer's application ○ Residents' considerations <p>OR</p> <ul style="list-style-type: none"> • Defer a decision on Schaffer's application until the City of Cockburn has developed a formal structure plan for the whole of the Jandakot rural area from Berrigan Drive to Warton Road that includes: <ul style="list-style-type: none"> ○ Schaffer's application ○ Residents' considerations <p>ATTACHMENT 1</p> <p>Transitional Implications</p> <p>Even if changes in land use or zoning are approved for all Jandakot rural properties and for Schaffer's land, it is likely that current rural property owners would reside in their properties for several years. During this interim period, the commercial development of Schaffer's land will proceed apace and create considerable noise nuisance for the nearby residents that remain.</p> <p>As Schaffer concedes in its application, the proposed developments would cause excessive noise levels in nearby homes. Based on our experience with Jandakot Airport operations, we know that it is very difficult to mitigate noise once it is in place, and very difficult even to monitor or effectively raise and manage complaints.</p> <p>Significant noise sources should be prohibited initially from Schaffer's developments. Schaffer's application identified refrigerated truck operations as a major source of excessive noise nuisance. Consequently, we recommend that over a transitional period this should be a condition of any approval:</p> <p>1. Construction of refrigerated facilities must be specifically excluded</p>	

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		<p>from any land use and any access to the site by refrigerated trucks prohibited.</p> <p>Even without refrigerated trucks, noise limits would be exceeded. Schaffer's own noise modelling studies show that, even with a buffer zone of 50 metres around its property, neighbours would experience excessive noise, as conceded in Schaffer's own Noise Report in Appendix 1 of its application.</p> <p>To reduce noise levels to an acceptable level will therefore require a wider buffer zone; we suggest 120 metres on the south-eastern boundary of Lot 103, subject to detailed noise studies. Consequently, we recommend that over a transitional period this should be a condition of any approval :</p> <p>2. Any development or amendment to land use on Lot 103 should is not permitted in a buffer zone of at least 120 m to the north-west of the boundary with the rear of Lots 19 to 25 Boeing Way. This buffer zone, including the existing incline escarpment and vegetation, must be excluded from any land use changes and remain intact and untouched.</p> <p>(Note: Attached WAPC letters provided with highlights).</p>	
9	C Elpitelli 135 Jandakot Road JANDAKOT WA 6164	<p>OBJECTION</p> <p>Confirming that I reside at 135 Jandakot Rd, Jandakot, approx. 800 metres from the above and we submit the following which we trust the City of Cockburn will take into consideration;</p> <p>Request The Cockburn City Council to Defer</p> <ul style="list-style-type: none"> • Defer any decisions on Schaffer's application and on alternative land uses for current residential landowners in rural Jandakot until after the publication of the WAPC' s final South Metropolitan & Peel Planning Framework; then • Develop a formal structure plan for the whole of the Jandakot rural area from Berrigan Drive to Warton Road that includes: <ul style="list-style-type: none"> ○ Schaffer's application ○ Residents' considerations <p>OR</p> <ul style="list-style-type: none"> • Defer a decision on Schaffer's application until the City of Cockburn has developed a formal structure plan for the whole of the Jandakot rural area from Berrigan Drive to Warton Road that includes: 	<p>Noted. The submission recommends the 'deferral' of any decision with regards to this amendment. It is noted however the WAPC is the final assessing authority in this respect. The WAPC has indicated an in principle support of the proposed Amendment and on the basis the WAPC considers the proposal to be consistent with their Perth and Peel @ 3.5 Million documentation. This is noted within the Amendment documentation. On this basis the City is not inclined to defer the proposed Scheme Amendment however please note the WAPC and the Hon. Minister of Planning will ultimately determine the proposed Scheme Amendment.</p> <p>Please note 135 Jandakot Road and the surrounding land is not zoned 'Development' under TPS No. 3. On this basis the City is not able to develop a statutory 'Structure Plan' to govern the land in question.</p> <p>Should you/ the wider 'Resource' zoned lot owners seek</p>

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		<ul style="list-style-type: none"> ○ Schaffer's application ○ Residents' considerations ○ Attach Plan showing area 1,2 & 3 	<p>to lodge your own Scheme Amendment, you are permitted to do so. Such applications would be subject to the same application fee, assessment process and general procedures as is the case with all amendment submitted to the CoC for consideration.</p>
10	<p>R & J Kroon 97 Jandakot Road JANDAKOT WA 6164</p>	<p>OBJECTION</p> <p>Residents and landowners to the east of the area, who would be directly impacted, object to this Amendment.</p> <p>We fully support the submission made by the Banjup Residents Group that covers our main concerns. Please refer to the BRG submission, as attached, for full justifications. In summary they state that: either all Jandakot rural properties are permitted additional commercial land uses, or none are. We request the Council of the City of Cockburn to ensure equal opportunities are given both to residential ratepayers and commercial developers in the squeeze between Jandakot City and urban Treeby.</p> <p>We also request the City of Cockburn to put this proposed Scheme Amendment No. 112 on hold until such time as the West Australian Planning Commission has considered all submissions on land use in Jandakot and has developed an integrated strategic approach applicable to all the land.</p> <p>The main relevant submissions to the WAPC are those from several of the current property owners, the Banjup Residents Group (following several public meetings), and the City of Cockburn, each in response to the "Perth and Peel at 3.5 Million" consultation in July 2015.</p> <p>We would expect that the WAPC should find similar and as "strong merits" in broadening land use across the whole area as the Chairman so promptly stated to Mr Peter Goff (representing Shaffer Corporation) in their letter of June 23rd, 2015 (as attached).</p> <p>Even if changes in land use or zoning are approved for all properties, it is likely that current residential owners may still reside in their properties for several years. Therefore, if and when any changes in use or development on Lot 103 are approved, transitional issues must still be addressed and we make recommendations accordingly:</p> <p>NOISE POLLUTION</p>	<p>Noted. The applicant on behalf of the landowner has submitted a scheme amendment proposal at their own cost. On this basis the <i>Planning and Development Act 2005</i> offers the same equity to all landowners. Specifically, should you seek to lodge your own Amendment, you are permitted to do so. Such applications would be subject to the same application fee, assessment process and general procedures as is the case with all amendment submitted to the CoC for consideration.</p> <p>The submission recommends the deferral of any decision with regards to this amendment. It is noted however the WAPC is the final assessing authority in this respect. The WAPC has indicated an in principle support of the proposed Amendment and on the basis the WAPC considers the proposal to be consistent with their Perth and Peel @ 3.5 Million documentation. This is noted within the Amendment documentation. On this basis the City is not inclined to defer the proposed Scheme Amendment however please note the WAPC and the Hon. Minister of Planning will ultimately determine the proposed Scheme Amendment.</p> <p>It is noted the submission makes mention "<i>it is likely that current residential owner may still reside in their properties for several years....therefore transitional issues must be addressed.</i>" The submission then lists various 'recommendations'. These sorts of considerations are, in part, better placed in a separate formal scheme amendment by the respective landowners seeking to amend the zoning of their properties. The proposal before Council relates specifically to the Schaffer landholdings. The proposal does not propose to amend the zones or</p>

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		<p>As stated in the consultant's report, the proposed developments would cause excessive noise levels in adjacent homes. Based on our experience with Jandakot Airport operations, we know that it is very difficult to mitigate noise once it is in place, and very difficult even to monitor or effectively raise and manage complaints.</p> <p>a) Significant noise sources must therefore be prohibited from the developments. The consultant's report identified refrigerated truck operations as a major source of excessive noise nuisance.</p> <p>Recommendation 1. Construction of refrigerated facilities must be specifically excluded from any land use and access prohibited to the site by refrigerated trucks.</p> <p>b) Even without refrigerated trucks, noise limits would be exceeded. The distance from the noise source to homes must be increased and suitable sound barriers put in place. The best way to achieve this is to retain the existing elevated escarpment and existing vegetation along the South-Eastern boundary, providing a natural barrier. A buffer zone of 50 - 60 m width is already shown on the concept plan drawings and has been used in the noise modelling studies. However, this still results in excessive noise levels. To reduce noise levels below limits will therefore require a wider buffer zone; we suggest 120 metres on the south-eastern boundary of Lot 103 subject to detailed noise studies. This buffer zone must be enshrined in any approvals:</p> <p>Recommendation 2. Any development or amendment to land use on Lot 103 should not be permitted in a buffer zone of at least 120 m to the north-west of the boundary with the rear of Lots 19 to 25 Boeing Way. This buffer zone, including the existing incline escarpment and vegetation, must be excluded from any land use changes and remain intact and untouched (see Fig.1)</p>	<p>permissibility of land use over the adjacent 'Resource' zoned land.</p> <p>The specific noise/ disturbance considerations with regard to any future developments will be assessed at DA stage. Please note also under point 'e' of the proposed Scheme Text under the "conditions" column, <i>"with regard to any application for 'Warehouse', 'Showroom', or 'Storage', the preparation and lodgement of a report prepared by a suitably qualified acoustic consultant detailing the potential noise impact on noise sensitive land uses"</i>. In this regard development applications will be subject to further statutory scrutiny at DA stage. The Scheme Amendment therefore meets 'Recommendation 1' as outlined by the submission.</p> <p>In regards to 'Recommendation 2' the Council report addresses this issue by ensuring the AU1 scheme area is reduced in width resulting in a buffer between existing abutting residences. This is included in the formal resolution of Council. On this basis Recommendation 2 is supported.</p> <p>In regards to 'Recommendations 3 and 4' recommending no access road should be allowed between Lot 103 and Jandakot Airport land. This too is supported by the City of Cockburn and it too is addressed in the formal resolution of Council. On this basis Recommendations 3 and 4 are supported. Please note also the associated traffic report which will be implemented more specifically at development application stage.</p>

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		 <p>The map shows the Jandakot Airport (P 7585) with various noise buffer zones. A legend in the bottom left corner identifies the zones: <ul style="list-style-type: none"> Secondary Protection (Red) Proximate, Showroom (Purple) Proximate, Warehouse (Blue) Sub-Primary (Green) Buffer (Light Green) Road Links (Yellow) A dashed yellow line indicates an 'Increased buffer zone'. A road connecting the airport to the Schaffer Lots is marked with a red 'X' and labeled 'No access road'. A scale bar and north arrow are also present. </p> <p>FIG. 1: Estimated noise buffer zone requirements and deletion of JAH access road</p> <p><u>ROADS AND TRAFFIC</u></p> <p>Concept plans include a road linking Jandakot Airport Holdings land to the Schaffer Lots. There appears little valid justification for this connection, which has not previously been envisaged in local road plans and designs, or in the Jandakot Airport Master Plan. The multi-national oil and gas companies mentioned by JAH do not require showrooms and use highly specialised equipment, so have minimal requirements from local suppliers. There would be no land use requirement for businesses to be linked to the airport or JAH, so most or all of the development could be taken by local independent companies with no link to JAH businesses or operations. A road link to Jandakot Airport land has not be identified as essential, or critical, in any of the communications or studies presented, but merely as having potential for “providing possible integration”.</p>	

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		<p>The limited gap between the Bush Forever area and the boundary makes it unlikely that an access road from Airport land could be constructed without destroying more of the Bush Forever site.</p> <p>Furthermore the need for a buffer zone to reduce noise below limits means an access road could not be constructed at all along this boundary, as shown in Fig. 1.</p> <p>The overall proposal also requires the destruction of some of the western parts of the Bush Forever site for construction of an access road junction with Pilatus Street. This loss must be offset. It is recommended that an equivalent area to that destroyed should be created as Bush Forever and used to fill the gap at the north-east corner of Lot 103. This would then provide a continuous link from the present Bush Forever Site 388c to the extensive bush in the Lots along Boeing Way. This makes much more ecological and environmental sense and avoids net destruction of Bush Forever area.</p> <p>Recommendation 3. No access road should be allowed between Lot 103 and Jandakot Airport land. The existing gap should be added to the Bush Forever Site 388c.</p> <p>The traffic studies indicate that all traffic volumes can be adequately handled by the specifically designed exit onto Pilatus Street. This suggests that only limited or minor access is needed to and from Jandakot Road (i.e. mainly for any showrooms). Construction of a major junction and roundabout onto Jandakot Road would increase traffic volumes and congestion as it would be used as "short-cut", require resumption of significant land area, and incur considerable cost.</p> <p>JAH have previously sought additional access from the Airport land to Jandakot Road to help market their current Precinct 5 and 6 developments. Their previous proposal for access via Solomon Road was rejected by the City of Cockburn and had to be removed from the Jandakot Airport Master Plan. Traffic analysis in the 2014 Master Plan shows that the currently planned road improvements are adequate for predicted traffic volumes from Airport businesses. Access to Jandakot Road via Lot 103 is therefore not needed, is not included in the Master Plan, and would circumvent the previous objections from the City of Cockburn.</p> <p>Recommendation 4. Only a minor entry/exit to Jandakot Road for access to showrooms</p>	

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		should be allowed. General access and larger vehicle movements should be via the specifically designed new junction on Pilatus Street.	
11	AH & LM Rowland 28 Boring Way JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.
12	George Yacoub 27 Boeing Way JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.
13	Seng Peh 39 Boeing Way JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.
14	Ronald Fry 134 Jandakot Road JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.
15	Yvonne & Tom Miller 1 Coonadoo court JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.
16	Glenn Rideout 237 Jandakot Road JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/

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			recommendation in this regard adjacent to submission number 10.
17	Michael Clark 253 Jandakot Road JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.
18	Mark Taylor 287 Jandakot Raod JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.
19	Maureen & Gordon Fleet 46 Boeing Way JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.
20	Gary & Maria Clark 47 Boeing Way JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.
21	Greg & Dianne Goodchild 8 Falcon Place JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.
22	David & Fran Martin Lot 20 Boeing Way JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/

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			recommendation in this regard adjacent to submission number 10.
23	Malcolm & Vicky Cole 197 Jandakot Road JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.
24	Stephen & Melissa Masters 218 Solomon Road JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.
25	Ewe Huat The & Yok Hoe The 2 Cessna Drive JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.
26	Malcolm & Louise Dobson 139 Jandakot Road JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.
27	Pieter, Sue & Ian Bezuidenhout 147 Solomon Road JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.
28	Vivian Sloss 13 Falcon Place JANDAKOT WA 6164	OBJECTION This submission was a signatory to submission 10 above.	Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/

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			recommendation in this regard adjacent to submission number 10.
29	Douglas & Lynne Smith 227 Jandakot Road JANDAKOT WA 6164	<p>OBJECTION</p> <p>This submission was a signatory to submission 10 above.</p>	<p>Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.</p>
30	Tim & Delys Bovell 145 Solomon Road JANDAKOT WA 6164	<p>OBJECTION</p> <p>This submission was a signatory to submission 10 above.</p>	<p>Noted. As this submission was a signatory to submission 10 above, please refer to the associated response/recommendation in this regard adjacent to submission number 10.</p>
31	M & K Wilcox – Marris 35 Boeing Way JANDAKOT WA 6164	<p>OBJECTION</p> <p>We object to this proposed Amendment 112.</p> <p>We fully support the position being submitted by Ron Kroon for the group of directly affected residents (including us), and also the document submitted by the Banjup Residents Group.</p> <p>We continue to be impacted, inconvenienced and squeezed by the relatively unconstrained developments on surrounding land by Jandakot Airport Holdings (Precincts 5 & 6 clearing and development), Stockland's Caleya/Treeby residential development, Jandakot road widening, and now Schaffer's proposed developments. It is unreasonable and unfair that these developments are allowed on similar adjacent land, while stringent restrictions remain imposed on our land.</p> <p>The City of Cockburn, in submissions to the WA Planning Commission, has agreed that our area no longer enjoys rural amenity. The special appeal and lifestyle of our supposedly "rural" properties has already been grossly eroded. These restrictions must therefore be removed to ensure that our properties do not become stranded, worthless or unsaleable.</p> <p>We are concerned that Schaffer's amendment is being fast-tracked based on the encouragement and support they have already been given directly by the Chairman of WAPC. We are therefore relying on the Council of the City of</p>	<p>Noted. It is understood this submission identifies the changing nature of the locality by way of Treeby/ Caleya, JAH and now the proposed Amendment. Please note the City of Cockburn is the initial assessing authority of the proposed Scheme Amendment.</p> <p>The Scheme Amendment process involves meeting criteria set by the <i>Planning and Development Act 2005</i> and a suite of planning documentation including State Planning Policies. The specific scheme text proposed within draft AU1 outlines numerous restrictions with regard to the 'showroom', 'warehouse' and 'storage' land uses. These restrictions have been designed to comply with the relevant SPP and DoW requirements.</p> <p>The City has liaised closely with the DoW in the formulation of the draft scheme text (see this schedule number 5 for their response). As a result of such discussions the draft scheme text has been refined to ensure proper and orderly planning. Similarly the Treeby/ Caleya estate was the subject of a series of planning amendments and the 'structure planning process'. In both examples the change in planning requirements has been</p>

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		<p>Cockburn to ensure that resident ratepayers are not disadvantaged and are provided equal opportunity, with no preference or bias being given to commercial developers.</p> <p>In the short to medium-term, we have to co-exist with these incompatible developments surrounding us. We would therefore require interim restrictions and controls to be placed on any developments on Lot 103 (as detailed in Ron Kroon's submission) to minimise detrimental impacts on existing residents.</p>	<p>actioned by a statutory process. With regards to JAH please note this is Commonwealth land and managed under the details prescribed within the JA Master Plan 2014. The City of Cockburn's Town Planning Scheme has minimal statutory control/ influence over the land use outcomes in the airport area. On this basis following the final outcome of Perth and Peel @ 3.5 Million you and your neighbours are entitled under the <i>Planning and Development Act 2005</i> to prepare your own scheme amendment. In this regard the City is able to provide some guidance when and if you decide to act upon this. Please note the scheme amendment process involves legislated fees, statutory timeframes, due process and community consultation. All amendments are ultimately determined by the Hon. Minister for Planning with input from both the City of Cockburn and the WAPC.</p> <p>On the above basis the resident ratepayers are not disadvantaged and are provided equal opportunity, with no preference or bias being given. Ratepayers are entitled to engage a Planning consultant on their behalf to prepare and lodge a scheme amendment for their land. The City recommends early discussion prior to finalisation and submission of any hypothetical amendment.</p> <p>The resolution of Council involves the increasing of the buffer between the Schaffer landholdings and the adjacent 'Resource' zoned land. On this basis your concerns have been actively addressed in some capacity. In addition to this please note the various land use restrictions/ practices as outlined within the draft AU1 scheme provisions. These provisions will be assessed by the WAPC and ultimately determined by the Hon. Minister for Planning. For more details on the advice from DoW and other agencies please refer to this schedule of submissions for direct details. Should you have any questions please do not hesitate to contact the City of Cockburn Strategic Planning Staff.</p>
32	Christine Horton & Ian Dober 41 Boeing Way	<p>OBJECTION</p> <p>As discussed earlier in the week we would like to forward the following</p>	Noted. Please note the resolution of Council involves the

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	JANDAKOT WA 6164	<p>comments onto Cockburn City Council for consideration.</p> <p>We the property owners and residents of Lot 25, 41 Boeing Way Jandakot oppose the proposed Amendment 112 for the following reasons:</p> <ol style="list-style-type: none"> 1. We are concerned about the increased noise levels and heavy traffic that will occur directly behind our property from heavy trucks and vehicles accessing the area 24hrs a day. Increased traffic noise and congestion on Jandakot Road is already posing a problem as a result of other nearby new developments. 2. The proposed plan has no buffer between us (lot 25) and the proposed road. 3. The proposal poses greater security and safety issues for our family and our property. We have already experienced security issues since the new housing estate went in down the road (Off Jandakot & Solomon Roads). 4. When we purchased our property in 2007 we never envisaged intense commercial development would occur adjacent to us. The zoning then was "Forever Bush" which we believed meant it would be "Forever Bush". 5. Our lifestyle choice was to raise our family in a rural environment, but this development, together with others already impacting on us, will certainly take that lifestyle choice away. 6. Wildlife in our area has been greatly affected by recent land clearing and developments, this amendment will only impact on them further. 7. Fragile Water-table - It is difficult for us to understand how this proposal can be given serious consideration and/or how others have 	<p>deletion of the proposed access road behind your property. This has been actioned according to your request.</p> <p>In addition the AU1 scheme map area is required under the resolution to be reduced in width to provide a greater separation distance. This has been actioned according to your request.</p> <p>Please note the bush forever land is proposed to be retained. The proposed scheme amendment does not involve the clearing of the associated native vegetation to which you refer to in your submission. The 'bush forever' land is not only proposed to be retained, it is proposed to be subdivided (as a super lot) and given up by the landowner as a separate landholding and given to the Crown for ongoing management and protection. For details regarding this please refer to the draft AU1 scheme text under point 'L' which mandates the ongoing retention of the 'bush forever' vegetation. On this basis your concerns with regard to the 'bush forever' land has been addressed to your preference, as understood by the City.</p> <p>As discussed on the telephone and via email, the clearing to which you refer is on the JAH land which is governed as per the details prescribed in the JAH Master Plan 2014. The City of Cockburn's Town Planning Scheme No. 3 does not dictate land use or development control over this area. On this basis the City is unable to restrict the clearing of land on JAH landholdings. Please refer to the Master Plan for more details.</p> <p>Please note the ground water restrictions apply also to the AU1 area. In this respect your property and their property are similar.</p> <p>The proposed Scheme Amendment addresses SPP 2.3 by restricting land use activities. For details please refer to the scheme text within the OCM resolution. You will note any future development on the AU1 land will need to comply with the same restrictions as your property is</p>

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		<p>already been approved, given how strict Cockburn Council have been on residents in our area over many years. The "Fragile Water Table" has been used as the reason why residents have been refused approval to have livestock, or subdivide, or in our case convert our existing tiny house of 120sq into a granny flat and build a new average size residence to accommodate a family of four. There is no doubt that this proposed development will be detrimental to the water table, and cause far greater damage than anything we wanted to do. It just doesn't make sense how there are rules for us and rules for developers?!?</p> <p>8. When purchasing our property we were aware there would be noise from aircraft taking off and landing at the airport and prepared to accept that knowing that the vegetation ("Forever Bush") that "was" along our rear boundary fence would help block ground noise and lights. With Jandakot Holdings recently clearing a huge area of that vegetation, increased noise levels and lighting from the airport are already having a negative impact on our lives and further development will make the problem much worse.</p> <p>9. We have already endured over 18 months of being woken in the early hours of the morning by heavy machinery clearing land, excavating sand and the carrying out other site works at the airport. Our quality of life has suffered with us unable to enjoy a sleep in while on holidays or to spend time outside due to the noise and dust associated with those works. In fact although I'm currently sitting in my home writing this letter, instead of wildlife all I can hear is the continual beeping sound of heavy machinery right now. So much for the rural lifestyle!! Further clearing and developments will only exacerbate the problem causing us more stress and inconvenience.</p> <p>10. The amendment if approved will not only be detrimental to our lifestyle choice, but also greatly devalue our property and reduce the number of potential buyers and our ability to sell. Who is going to want to buy a 5 acre property that can't be sub-divided, can only have</p>	<p>required to comply with. In addition the SPP mandates a minimum lot size of 2ha. Noting the AU1 land area is in excess of 40ha. It is noted subdivision of your 2ha property is generally (under State government requirement) not permitted. This is the case for your property and for the AU1 area.</p> <p>There are therefore no separate rules for you/ the developer. The same 'rules' apply to both lands as per the various SPP and DoW water quality protection requirements. For details on the advice from DoW please refer to point 5 above for details. Should you wish to lodge any applications for your property regarding extensions or new dwellings, please do not hesitate to contact the City of Cockburn's Statutory Planning department who are best placed to provide you with advice in this respect. This way you will be able to seek advice prior to engaging the services of any builders, tradespersons or draftspersons. The City aims to assist its ratepayers as much as possible to guide them through the statutory processes as set by the Town Planning Scheme and the relevant planning documentation as set by the State government. A solution may be possible subject to discussion with the City. This is entirely up to you for your consideration and initial contact.</p>

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		<p>one residence, is surrounded by: warehouses (including refrigerated); an airport (with no buffer); busy main roads; heavy vehicle traffic; industrial buildings; housing estates; and of which you can do little with due to strict council regulations?</p> <p>11. There are already numerous other projects underway that are impacting on our lifestyle and value of our property, ie the Jandakot Airport Precinct Plan, the widening of Jandakot Road, new housing estates, the proposed extension of Solomon Rd at the end of our street and the State Government is soon to release another paper which we believe is likely to have further implications for our property.</p> <p>12. The uncertainty of what is happening around our home is unsettling, stressful and has resulted in us feeling the need to put all future plans on hold. Our plans to improve our living conditions by putting up a shed, new outdoor entertaining area, continue with internal renovations/extensions or to make any further improvements seem pointless. We feel stuck, stressed and uncertain about our future which seems to be in the hands of the Council and other Government Departments.</p> <p>13. 13. We may be minority but have genuine concerns and hope that Council will give them due consideration and are not swayed by Developers who offer to pay for roads and/or other costs in order to have their proposal/s approved.</p>	
33	Department of Parks and Wildlife Locked Bag 104 Bentley Delivery Centre WA 6983	<p>SUPPORT</p> <p>In response to your correspondence dated 22 November 2016 seeking comments on the above proposed scheme amendment, the Department of Parks and Wildlife provides the following advice.</p> <p>The preliminary development plan provided with the proposed scheme amendment report indicates that some remnant banksia woodland vegetation will be cleared.</p>	<p>Noted. The majority of the site has been previously cleared of native vegetation and now consists of replanted Australian native species, generally not native to the site. Please refer to the Environmental Assessment for details. The 'bush forever land' is proposed to be given to the Crown pursuant to the draft Scheme Text as provided for under point 'L' of the AU1 provisions. On this basis the concerns of DPaW as raised in their submission</p>

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		<p>Level 2 flora surveys, including targeted searches for the Declared Rare Flora species <i>Caladenia huegelii</i> should be undertaken prior to the structure planning stage. A fauna survey and a black cockatoo habitat assessment should also be undertaken prior to structure planning.</p> <p>The proposed development may result in the loss of habitat for threatened black cockatoos as defined by the Commonwealth Environmental Protection and Biodiversity Conservation Act 1999 (EPBC Act) referral guidelines for threatened black cockatoo species. These black cockatoo species are listed under the EPBC Act as endangered and are also listed as 'Specially Protected' under the Western Australian Wildlife Conservation Act 1950.</p> <p>The Banksia Woodlands of the Swan Coastal Plain was also recently included on the list of Threatened Ecological Communities under the EPBC Act as Endangered on 16 September 2016.</p> <p>Planning for the area should make provision to retain as much of the banksia woodland and cockatoo habitat as possible, and identify and quantify habitat that will be lost.</p> <p>The proposal should also be discussed with the Federal Department of Environment and Energy as there may be a requirement to refer the proposal under the EPBC Act.</p>	<p>will be met.</p> <p>The applicant is made aware of the advice from DPaW by way of this schedule of submissions. The applicant has been made aware and has acknowledged their responsibilities under the EPBC Act.</p>
34	Department of Health PO Box 8172, Perth Business Centre PERTH WA 6849	<p>SUPPORT</p> <p>The DOH has no objection to the proposed amendment subject to:</p> <ul style="list-style-type: none"> • Appropriate noise attenuation is incorporated into the building design of future developments to mitigate noise impact from Jandakot Airport. • All developments are required to connect to scheme water and reticulated sewerage (if available) as required by the Government Sewerage Policy - Perth Metropolitan Region. <p>The amendment to include that approval is required for anyone-site waste water treatment process. The requirements may be referenced and downloaded from:</p> <p>http://www.public.health.wa.gov.au/3/672/2/wastewaterlegislation_and_guidelines_pm http://www.public.health.wa.gov.au/3/1430/2/subdivisions_and_town_plannin</p>	<p>Noted. The requirements of SPP 5.4 are addressed under the accompanying Noise and Vibration Assessment which meets the requirements of SPP 5.4. Please note also any future development application within the AU1 area will require compliance with draft scheme text as outlined under point 'e' of the proposed alternative AU1 provisions. This scheme text requires...</p> <p><i>"the preparation and lodgement of a report prepared by a suitably qualified acoustic consultant detailing the potential noise impact on noise sensitive land uses. The report shall demonstrate how the proposed development has been acoustically assessed and designed for the purposes of minimising the effects of noise intrusion and/ or noise emissions. The report must</i></p>

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		<p>g_approvals.pm http://www.public.health.wa.gov.au/3/1275/2/recycled_water_guidelines_and_publications.pm</p>	<p><i>demonstrate the measures required to address noise to the LG's satisfaction and be implemented and maintained as part of the development of the land."</i></p> <p>On this basis the first point outlined by the DoH is considered to be addressed by the proposed Scheme Amendment.</p> <p>The proposed scheme text under point 'f 'l' makes mention</p> <p><i>"Development of any 'Warehouse', 'Showroom', or 'Storage' must be connected to a reticulated sewer system."</i></p> <p>On this basis the second point outlined by the DoH is considered to be addressed by the proposed Scheme Amendment.</p>
35	<p>Department of Planning Bush Forever Section Locked Bag 2506 Perth WA</p>	<p>SUPPORT – Subject to modification</p> <p>Thank you for sending the above town planning scheme amendment to the Policy team at the Department of Planning for comments with regard to Bush Forever and <i>State Planning Policy 2.8 - Bushland Policy for the Perth Metropolitan Region</i> (SPP 2.8).</p> <p>The proposed amendment is to increase land use permissibility over an expanded area of the 'Urban Stone' land at Lots 701, 702 and 703 Jandakot Road, Jandakot. Including (modified) 'Warehouse' 'Showroom' and 'Storage' land use permissibility as defined by the draft scheme text. The area of land use permissibility is defined as Additional Use area A1 (AU1) in the scheme text.</p> <p>Bush Forever area 388 - Jandakot Airport, Jandakot is within Lot 703 to the north. The expansion of the AU1 site excludes the Bush Forever area. Proposed condition 'l)' of the scheme text states: <i>'As part of future development and/or subdivision of the subject land, the applicant will be expected to; Provide the land for the Bush Forever site (as agreed) free of costs to the Crown'</i>. The ceding of all the Bush Forever area free of cost to the Crown is supported. However, it is noted the proposal has not included a figure of the AU1 and the notation of 'as agreed to' should be clarified.</p>	<p>Noted. It is not considered necessary to define the Bush Forever site under a 'figure' for the purposes of point 'L' of the draft scheme text. The Bush forever boundary is clearly an identifiable land mass. In addition the AU1 area excludes the bush forever land on the subject lot. The notion of 'as agreed' is as per previous discussions with the applicant which has been file noted. The applicant has not objected to this statement as they have not made a submission to that effect. On this basis the current scheme text 'L' is considered appropriate and these two suggestions have not resulted in any further modifications to the scheme text. The issues however will be addressed to the desires of the WAPC as the current draft scheme text is considered adequate in this (specific) regard.</p> <p>The notion of ceding the land <i>'at the first available opportunity'</i> is considered to be a valuable addition and is supported by the City of Cockburn. This has therefore been included in the draft scheme text as part of the final adoption of Council. Accordingly the Scheme Amendment</p>

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		<p>Furthermore, clarification is sought on the mechanism of ceding land through a development application process. It is recommended at the first available planning opportunity that the Bush Forever area be excised through subdivision and ceded free of cost to the Crown.</p> <p>Proposed scheme condition 'd) states: <i>'The prior preparation and approval of a Local Development Plan detailing iv) interface controls and/or measures with regard to Bush Forever area 388'</i>. This is supported and should include, but not limited to, a hard road edge within the AU1 abutting the Bush Forever area and/or bushland identified for protection; Bushfire mitigation measures being provided outside the Bush Forever area within the development area and drainage to be contained within the development area.</p> <p>From the aerial photography and native vegetation mapping, the area outside Bush Forever area 388 subject to this amendment does contain some remnant vegetation. An environmental assessment has been undertaken for the subject area which indicates a Level 2 flora and vegetation survey will be undertaken in spring of 2016 for the subject site, which has not been provided for review.</p> <p>However, the environmental assessment indicates the floristic community type of the remnant vegetation within the proposed expanded AU1 is 23a, which has been identified as a sub community of the Banksia Woodlands of the Swan Coastal Plain Threatened Ecological Community (TEC) listed under the <i>Environment Protection and Biodiversity Conservation (EPBC) Act 1999</i>. SPP 2.8 Appendix 2 (ix) (b) seeks to avoid unacceptable losses, which includes a general presumption against clearing bushland and other degrading activities for areas containing TECs. Any adverse impacts on the TEC may require referral to the federal Department of the Environment and Energy under the EPBC Act.</p> <p>In addition, the entire Jandakot Airport Bush Forever area is known to contain rare, threatened and priority flora and fauna species. Remnant vegetation abutting the Bush Forever area has the potential to also contain, or be habitat for, significant flora and fauna species. Given the wide spread clearing at Jandakot airport, remnant vegetation in the vicinity in good or better condition should be considered for protection in the AU1. Further advice from Department of Parks and Wildlife (DPaW) should be sought with regard to retention of additional vegetation over and above the Bush Forever area.</p> <p>Supplementary information has been provided by the City of Cockburn and DPaW which indicates the wetland within the Bush Forever area has been</p>	<p>report will be modified accordingly when presented to the WAPC.</p> <p>The notion of including ...<i>'but not limited to, a hard road edge within the AU1 abutting the Bush Forever area and/or bushland identified for protection; bushfire mitigation measures being provided outside the Bush Forever area within the development area and drainage to be contained within the development area.'</i>... is considered to be a valuable addition and is supported by the City of Cockburn. This has therefore been included in the draft scheme text as part of the final adoption of Council. Accordingly the Scheme Amendment report will be modified accordingly when presented to the WAPC.</p> <p>The applicant and the sub-consultants are aware of their requirements under the EPBC Act. The WAPCs comments in this regard are noted.</p> <p>With regard to the comments concerning DPaW and retention of vegetation, this has since been addressed separately by the City as per the amended vegetation boundary. This is secured via an alternative AU1 area which is identified under the final adoption of Council. On this basis this matter is incorporated into the amendment as per the request of the WAPC.</p> <p>Please note the proposed Scheme Amendment was advertised to DPaW. DPaW's submission is provided in this table under submission No. 33. Please refer to their advice. Please note the areas of (significant) remnant vegetation on the subject site are proposed to be retained. The majority of the vegetation within the new (see updated) AU1 area is degraded. Please refer to the associated environmental reports in this regard for details.</p> <p>With regard to the WAPCs comments regarding connection to deep sewerage, please note point 'f' 'l' of the scheme text. The WAPC will note ...any development ...via scheme text...must be connected to a reticulated sewer system.</p>

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		<p>upgraded to a Conservation Category Wetland (CCW) and the wetland boundary now aligns with the Bush Forever boundary abutting the AU1. SPP 2.8 Appendix 2 (ix) (b) seeks to avoid unacceptable losses, which includes a general presumption against clearing bushland and other degrading activities for areas containing CCWs. The scheme amendment and development area should take into consideration measures to avoid any indirect impacts to the wetland such as vegetation clearing, drainage and hydrological changes and provide a suitable buffer as advised by DPaW.</p> <p>The subject site is a priority 1 & 2 drinking water area. The priority 1 area is confined to the Bush Forever area. Deep sewerage should be considered to the subject site and the additional use of 'Nursery' should be contained within the P2 area. The Department of Water should be consulted on the proposed amendment with regard to ground water protection.</p> <p>As the subject site is within a bushfire prone area, <i>State Planning Policy 3.7 - Planning in Bushfire Prone Areas</i> will apply. The Department of Fire and Emergency Services should be consulted with regard to the bushfire management plan.</p> <p>The subject site is likely to be affected by aircraft noise as the 20, 25 and 30 Australian Noise Exposure Forecast contour falls within the subject site. Acceptable land use and building types should give consideration to <i>State Planning Policy 5.3 - Land Use Planning in the Vicinity of Jandakot Airport</i> and the Building site acceptability table from AS2021.</p> <p>Please note this is officer level advice with regard to Bush Forever and SPP 2.8 only and does not reflect comments of other branches within the Department of Planning and is not a formal position of the Western Australian Planning Commission.</p>	<p>Please note the DoW and Water Corp were consulted during the advertising process. DoW comments can be found under point 5 of this submission table. It is noted the Water Corporation did not provide a submission to the City of Cockburn (to date) despite being sent a letter during the advertising period. With regard to the use of the 'nursery' please note this is an existing use and as such unless the landowner develops further there is no (appropriate) mechanism to trigger connection to sewer for that specific operation. As such the draft scheme text has not been modified in this regard as suggested by the WAPC.</p> <p>Please note DFES was consulted with regard to the BMP. Please refer to submission number 3 above for details. Please note the CoC undertook an assessment of the BMP of their own.</p> <p>The comments regarding SPP 5.3 is considered to be a valuable addition and is supported by the City of Cockburn. This has therefore been included in the draft scheme text as part of the final adoption of Council. Accordingly the Scheme Amendment report will be modified accordingly when presented to the WAPC.</p>
36	Main Roads Western Australia Don Alken Centre, Waterloo Crescent, East Perth WA 6004 PO Box 6202, East Perth WA 6892.	<p>OBJECTION</p> <p>Thank you for your letter of 22 November 2016 inviting Main Roads comments on the above proposed scheme amendment.</p> <p>Main Roads is <u>unable to support</u> this amendment as presented since the traffic report prepared by Transcore looks at impacts to the local network only.</p> <p>Main Roads is interested in better understanding the impact of this development on the operation of the Kwinana Freeway I Berrigan Drive</p>	<p>Noted. Please refer to the MRWA comment provided below. It is noted MRWA has since supported the proposed scheme amendment following much discussion with the applicants Traffic consultant. The specific comments from MRWA in relation to their support are provided below.</p>

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>interchange. To this end, Main Roads requests the network volumes (existing and at 2031) for the area extending to and including the interchange of Kwinana Freeway I Berrigan Drive. Volumes on each ramp should be presented as per figures 7-1 o in Transcore's report.</p> <p>There is a risk that the distribution of traffic assumed is too strongly biased to and from the south east along Jandakot Road. If this distribution is more balanced, the volumes to and from the west will further exacerbate the function of the Freeway interchange. Similarly, it is considered that the distribution to and from Berrigan Drive does not reflect the hierarchy planned for the network. This would result in more demand being directed to Pilatus Street as the dual carriageway - with resultant impacts to the function of the intersection.</p> <p>Therefore it is recommended that the traffic report is revised to report on:</p> <ul style="list-style-type: none"> • the volumes predicted to occur on a broader area of the network • the rationale that would corroborate why the trip distribution is predominantly directed to and from the south east • the volumes predicted to occur on each of the ramps at the Kwinana Freeway / Berrigan Drive interchange • a trip distribution scenario which is balanced at least 50/50 east/west and therefore the sensitivity of the network in this area • the route choice between Berrigan Drive and Pilatus Street and the subsequent function of the intersection. <p>When Main Roads is provided with the above information and afforded the opportunity to consider the implications on the broader road network then Main Roads may be in a position to support this proposed Scheme Amendment.</p>	
37	Main Roads Western Australia Don Altken Centre, Waterloo Crescent, East Perth WA 6004 PO Box 6202, East Perth WA 6892.	<p>SUPPORT</p> <p>I refer to our letter of 13 February 2017 regarding the above proposed scheme amendment.</p> <p>You may be aware that since our letter, we have been provided additional information from Robin White of Transcore that clarified a number of our queries.</p> <p>Therefore, Main Roads now wishes to revise its response to this proposed scheme amendment and advise that we have no objections to the proposed</p>	Noted. The City is aware of the most recent discussions between the applicant and MRWA. The final outcome reflected as a support for the proposal is therefore noted.

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>increase of land use permissibility over the above lots.</p> <p>Main Roads notes that the traffic distribution assumed by Transcore on the Berrigan Drive/ Pilatus Street routes differs from our understanding from other investigations in this area. Whilst content that this should not be critical to this scheme amendment, it will have a bearing on the form and function of intersections along Berrigan Drive and Pilatus Street - provided for information to City of Cockburn.</p>	
38	<p>Environmental Protection Authority Locked Bag 10, East Perth WA 6892</p>	<p>SUPPORT</p> <p>Thank you for referring the above scheme to the Environmental Protection Authority (EPA).</p> <p>After consideration of the information provided by you, the EPA considers that the proposed scheme should not be assessed under Part IV Division 3 of the Environmental Protection Act 1986 (EP Act) and that it is not necessary to provide any advice or recommendations.</p> <p>Please note the following:</p> <ul style="list-style-type: none"> • For the purposes of Part IV of the EP Act, the scheme is defined as an assessed scheme. In relation to the implementation of the scheme, please note the requirements of Part IV Division 4 of the EP Act. • There is no appeal right in respect of the EPA's decision to not assess the scheme. <p>The EPA supports retention of remnant vegetation in the northern section of Lot 104 and around Lot 103 where the lot borders Jandakot Road, rural residential land and Bush Forever site 388.</p>	<p>Noted. The City acknowledges the responsibility as prescribed under 48H (1) 'Control of implementation of assessed schemes' as per Part IV Division 4 of the EP Act 1986. The Bush Forever vegetation is proposed to be retained as per the EPA's</p>
39	<p>Western Australian Planning Commission Locked Bag 2506 Perth WA</p>	<p>SUPPORT</p> <p>It is acknowledged that Amendment No. 112 to the City of Cockburn Town Planning Scheme No. 3 has been forwarded to the Western Australian Planning Commission (the Commission) in accordance with clause 37(2) of the Planning and Development (Local Planning Schemes) Regulations 2015 (the Regulations), as the proposed amendment is not consistent with any local planning strategy and is of a scale that is significant relative to</p>	<p>Noted. The requested changes including points 1 to 3 and the point relating to the 'dust management section' have been addressed in the formal resolution of Council. The City understands its obligations regarding Clause 44 of the Regulations with regards to 'information on complex amendment to be provided to the Commission.' These</p>

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>development in the area.</p> <p>Pursuant to clause 37(4) of the Regulations, the Commission has examined the documents provided to determine whether any modification is required before the amendment is advertised.</p> <p>The Commission advises that the amendment is suitable to be advertised subject to the following modifications:</p> <ol style="list-style-type: none"> 1. Replace references to the Department of Conservation and Environment with reference to the current equivalent Department. Reason – The Department of Conservation and Environment has been replaced by the Department of Environment Regulation and the Department of Parks and Wildlife. 2. Replace references to “planning approval” with references to “development approval”. Reason: Development approval replaced planning approval with the introduction of the deemed provisions of the Regulations. 3. Replace the word “repot” with “report” in condition e). Reason – typographical error. <p>The Commission also notes that the last sentence in the Dust Management section of the Additional Use column seems to have an error where it states “is to be implemented and all times”. The City should rectify this.</p> <p>Whilst the Commission has undertaken a preliminary assessment, this should not be construed as support for the amendment or that further modifications may not be sought following advertising.</p> <p>The City is reminded that Local Planning Scheme (LPS) amendments need to be accompanied by sufficient documentation as detailed in clause 44 of the Regulations, to enable the Commission to provide a recommendation to the Minister for Planning.</p> <p>An LPS Amendment Checklist has been prepared to assist Local Government in providing the necessary information when submitting LPS amendments to the Commission, and is provided as an attachment to this letter.</p>	<p>requirements will be met as part of the formal referral to the Commission. Please note the modifications as requested will be identified in the Council report attachment ‘Proposed Scheme Text Schedule of Modifications’. The requested changes are highlighted in yellow amongst other changes.</p>

Contact Officer: Giselle Allieux (08) 9411 3444
Our Ref: 2212413 - DA16/0100



Records
4643681

29 April 2016

Cadds Compliance (Attn: Scott Geere)
1/2 Ambitious Link
BIBRA LAKE WA 6163

**Retrospective Change of Use from Private Recreation to Health Studio
9/153 Rockingham Road HAMILTON HILL**

I refer to your application received 18/02/2016 for the above and advise that Council at its meeting held on 14 April 2016 resolved to grant conditional approval in accordance with the attached Notice of Determination on Application for Planning Approval – Schedule 8. This approval was granted pursuant to clause 60 of the Planning and Development (Local Planning Schemes) Regulations 2015.

PRIOR TO ANY WORKS WHATSOEVER COMMENCING ON-SITE, AN APPLICATION FOR A BUILDING PERMIT MUST BE SUBMITTED TO THE COUNCIL UNDER THE PROVISIONS OF THE BUILDING REGULATIONS AND APPROVAL FROM COUNCIL IS REQUIRED IN THE FORM OF A BUILDING PERMIT. AS PART OF THE BUILDING PERMIT APPLICATION, ALL INFORMATION NECESSARY FOR THE PURPOSE OF ADDRESSING CONDITIONS IN THE ATTACHED APPROVAL SHOULD ALSO BE PROVIDED.

Should you be aggrieved by the City's decision or a condition there is a right to apply for a review under Part 14 of the Planning and Development Act 2005. The application for review must be submitted within 28 days of the date of this decision to the State Administrative Tribunal, 6th Floor, 565 Hay Street Perth. It is recommended that you contact the State Administrative Tribunal for further details (telephone 9219 3111) or <http://www.sat.justice.wa.gov.au>

Planning approval does not remove the need for any other approvals, licences or permits that may be required.

Regards,

Giselle Allieux
PLANNING OFFICER



REF No: 2212413 - DA16/0100
2016

Issue Date: 29 April

SCHEDULE 8

NOTICE OF DETERMINATION ON APPLICATION FOR DEVELOPMENT APPROVAL

PLANNING AND DEVELOPMENT ACT 2005

Owner Name & Address Worldclass Holdings Pty Ltd

LOCATION: 9/153 Rockingham Road HAMILTON HILL WA 6163

LOT: 7

PLAN/DIAGRAM: S / 33014

FOLIO NO: 239

VOL NO: 2112

Application Date: 16/02/2016

Received on: 18/02/2016

Description of proposed development: Modification Change of Use -
Recreational / Dance Studio

The application for planning approval is **granted** subject to the following conditions:

CONDITIONS

1. Development may be carried out only in accordance with the details of the application as approved herein and any approved plan. This schedules the use of the land and/or a tenancy. The approved development has approval to be used for Health Studio only. In the event it is proposed to change the use of the tenancy, a further application needs to be made to the City for determination.
2. Hours of operation are restricted from between 9.00am to 9.00pm Monday to Saturday.
3. Classes after 7:00pm between Monday to Saturday shall adhere to the Noise Management Plan as detailed in the Environmental Noise Assessment prepared by Lloyd George Acoustics (Ref 16023492-01c; dated 10 March 2016)
4. The approved change of use to Health Studio is restricted to a maximum of 15 persons at any one given time.
5. The development shall be retrofitted within 30 days of the date of this approval to comply with the requirements of the Environmental Noise Assessment prepared by Lloyd George Acoustics (Ref 16023492-01c; dated 10 March 2016). The Building Permit Application is to

demonstrate that all recommendations made in the Environmental Noise Assessment prepared by Lloyd George Acoustics (Ref 16023492-01c; dated 10 March 2016) have been incorporated into the proposed development.

6. The premises shall be kept in a neat and tidy condition at all times by the owner/occupier to the satisfaction of the City.
7. All outdoor lighting must be installed and maintained in accordance with Australian Standard AS 4282 – 1997 "Control of the Obtrusive Effects of Outdoor Lighting".
8. Within 60 days from the date of this approval (or another timeframe agreed to by the City), an acoustic wall shall be constructed along the boundary of the subject site and the adjoining residential zoned property to the west as marked in red on the approved plan to the satisfaction of the City. In this regard detailed plans for the wall shall be submitted to and approved by the City prior to construction

FOOTNOTES

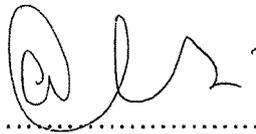
- a) This is a Planning Approval only and does not remove the responsibility of the applicant/owner to comply with all relevant building, health and engineering requirements of the Council, or with any requirements of the City of Cockburn Town Planning Scheme No. 3 or with the requirements of any external agency. Prior to the commencement of any works associated with the development, a Building Permit is required.
- b) The applicant/owner is advised that the approved change of use will generate the requirement for an Occupancy Permit to be obtained from the City's Building Services Department prior to commencement of use. In this regard, please contact the City's Building Services on 9411 3444 to confirm.
- c) The development shall comply with the requirements of the Building Code of Australia.
- d) The development shall comply with the Environmental Protection Act 1986 which contains penalties where noise limits exceed those prescribed by the Environmental Protection (Noise) Regulations 1997.
- e) You are advised that all waste and recycling must be contained within bins. These must be stored within the buildings or within an external enclosure. Should an internal fitout be required, this information should be submitted for approval at the building permit stage

Note 1: If the development the subject of this approval is not substantially commenced within a period of 2 years, or such other period as specified in the approval after the date of the decision, the approval shall lapse and be of no further effect.

Note 2: Where an approval has so lapsed, no development shall be carried out without the further approval of the Council having first been sought and obtained.

Note 3: If the applicant or owner is aggrieved by this determination there is a right of review by the State Administrative Tribunal in accordance with the *Planning and Development Act 2005* Part 14. An application must be made within 28 days of determination.

SIGNED:



DATED: 29/4/16

.....
Giselle Allix

PLANNING OFFICER

for and on behalf of the City of Cockburn.



Lloyd George Acoustics

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Hillarys WA 6923

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Environmental Noise Assessment

Kelete Theatre and Performing Arts Company

Reference: 16023492-01c.docx

Prepared for:

CADDs Architectural



Member Firm of Association of Australian Acoustical Consultants

Report: 16023492-01c.docx

Lloyd George Acoustics Pty Ltd ABN: 79 125 812 544 PO Box 717 Hillarys WA 6923 T: 9300 4188 / 9401 7770 F: 9300 4199					
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This report has been prepared in accordance with the scope of services described in the contract or agreement between Lloyd George Acoustics Pty Ltd and the Client. The report relies upon data, surveys, measurements and results taken at or under the particular times and conditions specified herein. Any findings, conclusions or recommendations only apply to the aforementioned circumstances and no greater reliance should be assumed or drawn by the Client. Furthermore, the report has been prepared solely for use by the Client, and Lloyd George Acoustics Pty Ltd accepts no responsibility for its use by other parties.

Prepared By:	Olivier Mallié 
Position:	Project Director
Date:	10 March 2016

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1 INTRODUCTION

Kelete Theatre and Performing Arts Company is a community based venture wishing to seek permission to operate the following land use activities at Unit 9/ 153 Rockingham Road:

- 9.00am till 4.00pm – use of Music and Dance therapy for people with disabilities; and
- 4.00pm till 9.00pm – theatrical/dance/singing classes for children and fitness classes for adults.

Lloyd George Acoustics was engaged to identify noise mitigation measures for the music noise to comply at the nearest residences. In addition, a noise management plan addressing noise from patrons leaving the venue as well as car doors closing is presented.

The Kelete premises ('Dance Studio') is located within a commercial complex and is bounded by a residential property to the south-west, vacant land to the east and other commercial tenancies to the north and south - refer *Figure 1.1*.



Figure 1-1 Location of Kelete Dance Studio and Closest Residence

Appendix A shows the development plans used as the basis for this assessment.

Appendix C contains a description of some of the terminology used throughout this report.

2 CRITERIA

Environmental noise in Western Australia is governed by the *Environmental Protection Act 1986*, through the *Environmental Protection (Noise) Regulations 1997* (the Regulations).

Regulation 7 defines the prescribed standard for noise emissions as follows:

"7. (1) Noise emitted from any premises or public place when received at other premises –

- (a) Must not cause or significantly contribute to, a level of noise which exceeds the assigned level in respect of noise received at premises of that kind; and
- (b) Must be free of –
 - i. tonality;
 - ii. impulsiveness; and
 - iii. modulation,
 when assessed under regulation 9"

A "...noise emission is taken to significantly contribute to a level of noise if the noise emission ... exceeds a value which is 5 dB below the assigned level..."

Tonality, impulsiveness and modulation are defined in Regulation 9. Noise is to be taken to be free of these characteristics if:

- (a) The characteristics cannot be reasonably and practicably removed by techniques other than attenuating the overall level of noise emission; and
- (b) The noise emission complies with the standard prescribed under regulation 7 after the adjustments of *Table 2-1* are made to the noise emission as measured at the point of reception.

Table 2-1 Adjustments Where Characteristics Cannot Be Removed

Where Noise Emission is Not Music			Where Noise Emission is Music	
Tonality	Modulation	Impulsiveness	No Impulsiveness	Impulsiveness
+ 5 dB	+ 5 dB	+ 10 dB	+ 10 dB	+ 15 dB

Note: The above are cumulative to a maximum of 15 dB.

The baseline assigned levels (prescribed standards) are specified in Regulation 8 and are shown in *Table 2-2*.

Table 2-2 Baseline Assigned Noise Levels

Premises Receiving Noise	Time Of Day	Assigned Level (dB)		
		L _{A10}	L _{A1}	L _{Amax}
Noise sensitive premises: highly sensitive area ¹	0700 to 1900 hours Monday to Saturday (Day)	45 + influencing factor	55 + influencing factor	65 + influencing factor
	0900 to 1900 hours Sunday and public holidays (Sunday)	40 + influencing factor	50 + influencing factor	65 + influencing factor
	1900 to 2200 hours all days (Evening)	40 + influencing factor	50 + influencing factor	55 + influencing factor
	2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and public holidays (Night)	35 + influencing factor	45 + influencing factor	55 + influencing factor
Noise sensitive premises: any area other than highly sensitive area	All hours	60	75	80
Commercial	All hours	60	75	80
Industrial	All hours	65	80	90

1. *highly sensitive area* means that area (if any) of noise sensitive premises comprising —
- (a) a building, or a part of a building, on the premises that is used for a noise sensitive purpose; and
 - (b) any other part of the premises within 15 metres of that building or that part of the building.

The influencing factor, applicable at the nearest noise sensitive premises has been calculated as 4.5 dB by the City of Cockburn.

Table 2-3 shows the assigned noise levels including the influencing factor at the receiving location.

Table 2-3 Assigned Noise Levels

Premises Receiving Noise	Time Of Day	Assigned Level (dB)		
		L _{A10}	L _{A1}	L _{Amax}
Noise sensitive premises: highly sensitive area ¹	0700 to 1900 hours Monday to Saturday (Day)	49.5	59.5	69.5
	0900 to 1900 hours Sunday and public holidays (Sunday)	44.5	54.5	69.5
	1900 to 2200 hours all days (Evening)	44.5	54.5	59.5
	2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and public holidays (Night)	39.5	49.5	59.5

¹ *highly sensitive area* means that area (if any) of noise sensitive premises comprising –
 (a) a building or a part of a building on the premises that is used for a noise sensitive purpose; and
 (b) any other part of the premises within 15 metres of that building or that part of the building.

It must be noted the assigned noise levels above apply outside the receiving premises and at a point at least 3 metres away from any substantial reflecting surfaces. Given the close proximity of existing buildings and fences, the noise emissions were assessed at a point 1 metre away from building facades and a -2 dB adjustment was made to the predicted noise levels to account for reflected noise.

It is further noted the assigned noise levels are statistical levels and therefore the period over which they are determined is important. The Regulations define the Representative Assessment Period (RAP) as *a period of time of not less than 15 minutes, and not exceeding 4 hours*, which is determined by an *inspector or authorised person* to be appropriate for the assessment of a noise emission, having regard to the type and nature of the noise emission. An *inspector or authorised person* is a person appointed under Sections 87 & 88 of the *Environmental Protection Act 1986* and include Local Government Environmental Health Officers and Officers from the Department of Environment Regulation. Acoustic consultants or other environmental consultants are not appointed as an *inspector or authorised person*. Therefore, whilst this assessment is based on a 4 hours RAP, which is assumed to be appropriate given the nature of the operations, this is to be used for guidance only.

Under regulation 3, nothing in the Regulations applies to the following noise emissions –

- (a) noise emissions from the propulsion and braking systems of motor vehicles operating on a road;
- (b) noise emissions from a safety warning device, other than a reversing alarm, fitted to a motor vehicle operating on a road;
- (c) noise emissions from trains or aircraft (other than model aircraft and trains operating on railways with a gauge of less than 70 cm);
- (d) noise emissions from a safety warning device fitted to a train or vessel;

- (e) noise emissions from an emergency vehicle as defined in the Road Traffic Code 2000 regulation 3(1);
- (f) noise emissions from the propulsion system or the movement through the water of a vessel operating in water other than water on private premises;
- (g) noise emissions –
 - (i) from a device for warning pedestrians installed at a pedestrian crossing on a road; or
 - (ii) from a device for warning of the passage of a train installed at a level crossing; or
 - (iii) from a safety warning device fitted to a building as a requirement of the Building Code as defined in the *Building Regulations 2012* regulation 3; or
 - (iv) for the purpose of giving a warning required under the *Mines Safety and Inspection Regulations 1995* regulation 8.26,if every reasonable and practicable measure has been taken to reduce the effect of the noise emission consistent with providing an audible warning to people;
- (h) noise emissions from –
 - (i) a reversing alarm fitted to a motor vehicle, mobile plant, or mining or earthmoving equipment; or
 - (ii) a startup or movement alarm fitted to plant,if
 - (iii) it is a requirement under another written law that such an alarm be fitted; and
 - (iv) it is not practicable to fit an alarm that complies with the written law under which it is required to be fitted and emits noise that complies with these Regulations;
- (i) noise emissions from an engine, equipment, machinery or plant on a vessel while the vessel is in a port.

Port is defined in either the *Port Authorities Act 1999* section 3(1) or the *Shipping and Pilotage Act 1967* section 3.

3 METHODOLOGY

3.1 Site Inspection

The premises were inspected on the Friday 5 February 2016 and the following was noted in relation to the dance studio and its surroundings:

- The nearest residence is located approximately 17 metres to the south-west of the dance studio west facade;
- A large roller shutter door is located on the west wall;
- The current entry door is a hollow core without seals;

- There is a large window on the west facade;
- The roof construction is metal sheet decking with insulation under the purlins;
- The ground level of the dance studio is elevated compared to the nearest residence; and,
- There is a solid 1.8 metre high wall on the east boundary of the residence.

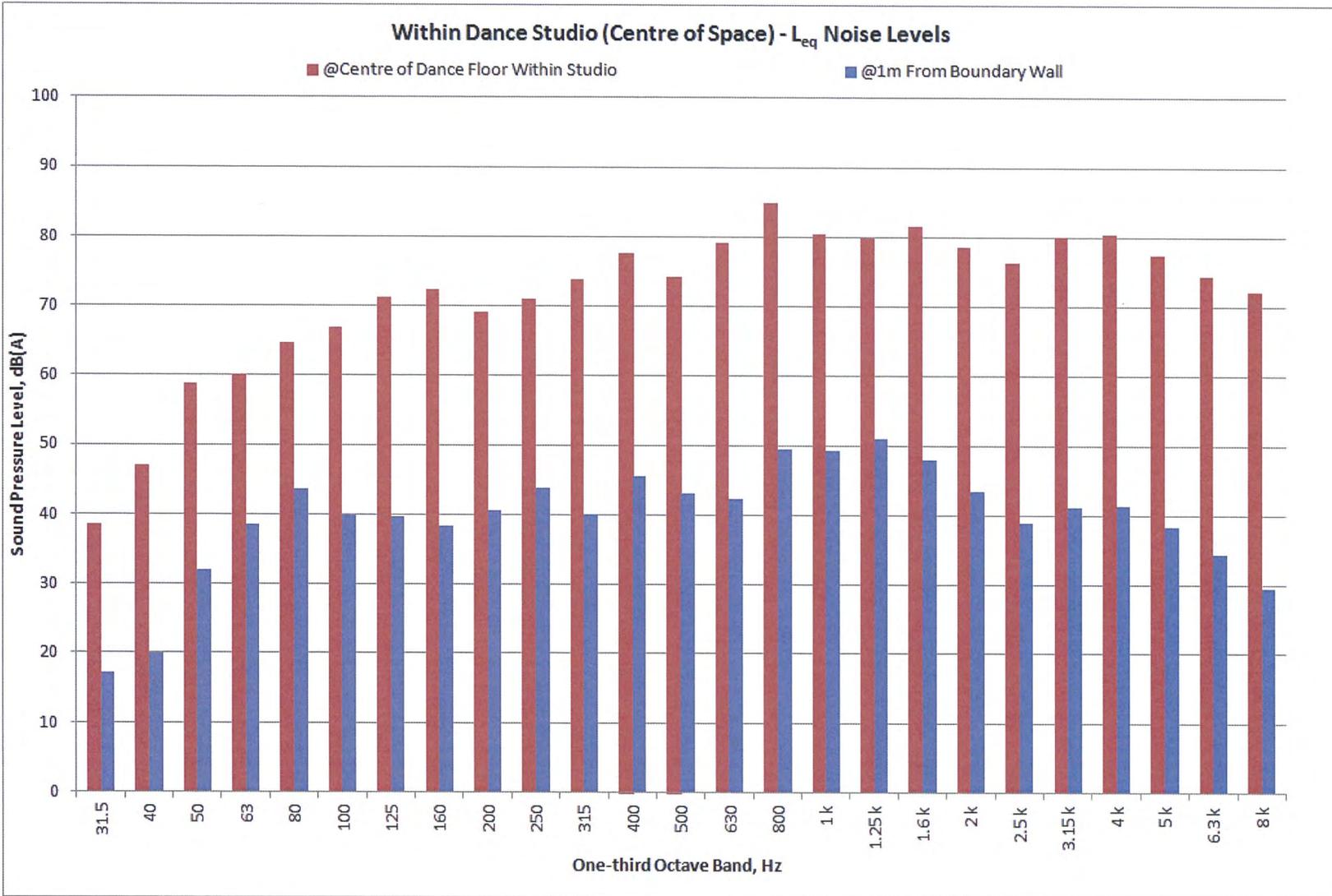
While on site, noise measurements were also conducted to establish typical noise levels within and outside of the dance studio and later used for noise model calibration. The noise measurements were conducted with a Rion NA-28 hand held sound level meter while loud music, of a type which included a good amount of low frequency noise, was being played within the dance studio. The noise levels recorded are summarised in *Table 3-1* with the spectral data of the first and last measurements shown in *Figure 3-1*.

Table 3-1 Summary of Noise Levels Recorded On Site

Location	Measurement Duration	Measured Level
Within Dance Studio at Centre of Floor space. Microphone was 1.5 metre above dance floor.	1 min	91.4 dB L _{Aeq}
Within Dance Studio 2 metres from the Roller Shutter Door. Microphone was 1.5 metre above dance floor.	1 min	88.0 dB L _{Aeq}
Within Office Space of Dance Studio near entry door. All doors and windows to dance floor shut. Microphone was 1.5 metre above dance floor.	1 min	66.0 dB L _{Aeq}
Outside Dance Studio, 7 metres directly in front of Roller Shutter Door. Microphone was 1.5 metre above local ground.	1 min	60.4 dB L _{Aeq}
Outside Dance Studio 1 metre from Boundary Wall (approximately 17 metres from Roller Shutter Door). Microphone was 1.5 metre above local ground.	1 min	57.8 dB L _{Aeq}

It is noted that music was clearly audible at both outside measurement locations.

Figure 3-1 Recorded Spectral Data



3.2 Noise Modelling

Computer modelling has been used to support the site measurements and identify noise mitigation measures. The advantage of modelling is that it is not affected by background noise sources and can provide the individual noise contribution of each source in the model.

The software used was *SoundPLAN 7.4* with the ISO 9613-2 algorithms selected. These algorithms have been selected given the relative short distance between source and receiver (less than 20 metres) and as they are one of the few that include the influence of wind. Input data required in the model are:

- Meteorological Information;
- Topographical data and Buildings;
- Ground Absorption; and
- Noise Sources.

An overview of the noise model showing the extent and location of the various model inputs is shown in *Appendix B*.

3.2.1 Meteorological Information

Meteorological information utilised (*Table 3-1*) is based on that specified in the *draft EPA Guidance for the Assessment of Environmental Factors No.8 Environmental Noise*. These conditions are considered the worst-case for noise propagation. At wind speeds greater than those shown, sound propagation may be further enhanced, however background noise from the wind itself and from local vegetation is likely to be elevated and dominate the ambient noise levels.

Table 3-2 Modelling Meteorological Conditions

Parameter	Evening and Night (1900-0700)	Day (0700-1900)
Temperature (°C)	15	20
Humidity (%)	50	50
Wind Speed (m/s)	3	4
Wind Direction*	All	All

* Note that the modelling package used allows for all wind directions to be modelled simultaneously.

The EPA policy is that compliance with the assigned noise levels needs to be demonstrated for 98% of the time, during the day and night periods, for the month of the year in which the worst-case weather conditions prevail. In most cases, the above conditions occur for more than 2% of the time and therefore must be satisfied.

3.2.2 Topographical Data and Buildings

Given the short source to receiver distance and local surrounding topography, flat ground was assumed over the study area.

Buildings and boundary walls and fences were also included in the noise model as they can provide noise attenuation effects as well as reflection paths. However, the roof of pergolas were not included. The model include the following buildings and fences:

- Commercial complex, of which the dance studio is part of, modelled as 5 metres high and with the southern-most section modelled as 2.5 metres high;
- 6 metre high commercial building on 11 Marryat Court;
- Residence to the south-west modelled as single storey, 3.5 metres high; and,
- 1.8 metres high solid wall on east boundary of residence (between residence and car park).

3.2.3 Ground Absorption

Ground absorption varies from a value of 0 to 1, with 0 being for an acoustically reflective ground (e.g. bitumen or asphalt) and 1 for acoustically absorbent ground (e.g. grass). In this instance, a value of 0 has been used as an average across the study area.

3.2.4 Noise Sources

The noise sources incorporated in the model were based on that observed on site and the development plans in *Appendix A*. The source sound power levels were derived based on the measurements taken on site and the estimated transmission loss of the various construction elements. The following noise sources were included in the model:

- Existing Building
 - Roller Shutter Door of steel construction and surface area of 15.2 m², estimated R_w rating of 22;
 - Roof is steel construction with insulation under the purlins, of surface area of 132 m² and with an estimated R_w rating of 18; and,
 - Large office window with 4mm thick glass of surface area of 3.9 m² rated R_w 25, with the bottom sill approximately 0.8 metres above ground.
- Re-furbished Dance Studio
 - Stud wall partition in front of roller shutter door comprising:
 - Proprietary 8mm thick Soundstop plasterboard directly fixed to 90mm steel stud,
 - 30mm gap between studs,
 - Hardiflex 7.5mm thick cement fibre sheet directly fixed to 90mm steel stud,
 - Cavity insulation to be double layer of Bradford Soundscreen insulation.
 Above construction estimated rating R_w (C , C_{tr}) of 48 (-4, -11)
 - Double door to be incorporated in new stud wall partition comprising of two 44mm solid core doors with drop seal, astragal and full perimeter seals with overall estimated R_w rating of 32;
 - Office window to be upgraded to double glazed unit (10.38mm laminated / 12mm air gap / 6.38mm laminated) with estimated R_w rating of 41; and,

- Ceiling to be Gyprock perforated ceiling panels on suspended steel grid system. It is noted the perforated ceiling will have negligible transmission loss and therefore it was taken to be standard 10mm plasterboard with an estimated R_w rating of 26 as a basis for this assessment.
- Patrons leaving the venue - as a guide to assess the potential impact of patrons leaving the Venue, the noise model include 3 patrons standing and chatting at once as follows:
 - Three point sources of sound power level L_w of 77 dB(A) each;
 - Sources are 1.5 metres above ground level; and,
 - Sources were located by the entrance/exit door, and also near a car park spot.
- Car doors closing - this was also included in the model to provide guidance on the potential impact from cars using the car park as follows:
 - One point source per car door with a sound power level of 84 dB(A);
 - Sources are 1 metre above local ground; and,
 - Sources were located in several car park spot to identify 'worst' spot.

It is noted that with the exception of the roof, all noise paths are located on the west facade, that is, the east wall is of solid construction with no penetrations to outside.

Once refurbished, it is understood the roller door will remain fully open when the Studio is in use.

4 ASSESSMENT

4.1 Existing Building

The noise contribution of each noise paths was determined using the noise model. They are presented in *Table 4-1* along with the assessment of the overall predicted noise levels against the evening L_{A10} assigned noise levels.

Table 4-1 Noise Modelling Results and Assessment - Existing Building

Source	Predicted Noise Levels At Receiver	Evening Assigned Noise Levels at Receiver ¹	Adjusted Noise Levels ²	Calculated Exceedance
Roller Shutter Door (Closed)	45.4 dB L_{A10}	-	-	-
Roof	44.2 dB L_{A10}			
Office Window	16.0 dB L_{A10}			
Overall Levels	48 dB L_{A10}	45 dB L_{A10}	58 dB L_{A10}	13 dB

Notes:

1. The assigned noise level is as defined in *Table 2-3*.
2. Predicted existing noise levels adjusted by - 10 dB for music.

From the results, it can be seen that the noise contribution from the roller shutter door and roof are similar and dominate the overall noise levels, while the contribution from the office window is not significant.

Then, it can also be seen the adjusted noise levels exceed the evening L_{A10} assigned noise levels by 13 dB.

4.2 Future Building

The noise contribution from the various noise paths was determined using the noise model. They are presented in *Table 4-2* along with the assessment of the predicted overall noise levels against the evening L_{A10} assigned noise levels.

The predicted noise levels are also presented in *Figure 4-2* as noise contours.

Table 4-2 Noise Modelling Results and Assessment - Future Building

Source	Predicted Noise Levels At Receiver	Evening Assigned Noise Levels at Receiver ¹	Adjusted Noise Levels ²	Calculated Exceedance
Roof	32.6 dB L_{A10}			
Double Door	23.8 dB L_{A10}			
New Partition	20.1 dB L_{A10}			
Office Window	5.7 dB L_{A10}			
Overall Levels	33 dB L_{A10}	45 dB L_{A10}	43 dB L_{A10}	Complies

Notes:

1. The assigned noise level is as defined in *Table 2-3*
2. Predicted existing noise levels adjusted by + 10 dB for music.

From the results, it can be seen that the noise contribution from the roof is predicted to dominate the overall noise levels, while the contribution from the other building elements are less significant.

Then, it can also be seen the adjusted noise levels would comply with the evening L_{A10} assigned noise levels.

However, while the overall noise levels have significantly reduced compared to existing building condition, it is noted the predicted levels of low frequency noise from the music are still relatively high. This is illustrated in *Figure 4-2* which shows the difference between predicted noise levels for 'existing building' and 'future building' conditions.

In addition, it must be noted this assessment is based on the internal noise levels recorded during the site visit (refer *Table 3-1* and *Figure 3-1*). From the results above, the predicted overall levels are close to the assigned levels and therefore there is not a lot of room for the internal noise levels to increase, either overall or at low frequency, which could still lead to exceedances.

It is therefore recommended to further mitigate the low frequency noise by upgrading the ceiling construction using two layers of fire rated 13mm plasterboards. Doing so results in the overall predicted noise levels to decrease to 28 dB L_{A10} and the low frequency noise to decrease by a further 6 dB approximately in the 31.5, 63 and 125 Hz octave bands. This is also shown in *Figure 4-1* and the predicted noise levels with the upgraded ceiling are also shown in *Figure 4-3*.

4.3 Car Park and Patrons Noise

The highest predicted noise level from a car door closing is 47.5 dB(A). Car doors closing could be considered impulsive as per the Regulations however, it is considered unlikely this noise will be impulsive when assessed at the receiver, which is behind the existing 1.8 metre high brick wall. Given the short duration of such events, car doors closing would have to comply with the evening L_{Amax} assigned level of 59.5 dB(A). The highest predicted level is well below that and therefore no specific noise mitigation would be required.

The highest predicted noise level from people talking is 47 dB(A). For this source, it is noted the applicable assigned noise level will depend on how long the patrons are talking. The L_{A10} assigned noise level is 44.5 dB(A) which means that a level of 44.5 dB(A) cannot be exceeded for more than 10% of the representative period (RAP). As such, for compliance with the Regulations to be achieved patrons cannot talk for more than:

- 24 minutes if the RAP is taken as 4 hours; or,
- 6 minutes if the RAP is taken as 1 hour; or,
- 1.5 minutes if the RAP is taken as 15 minutes.

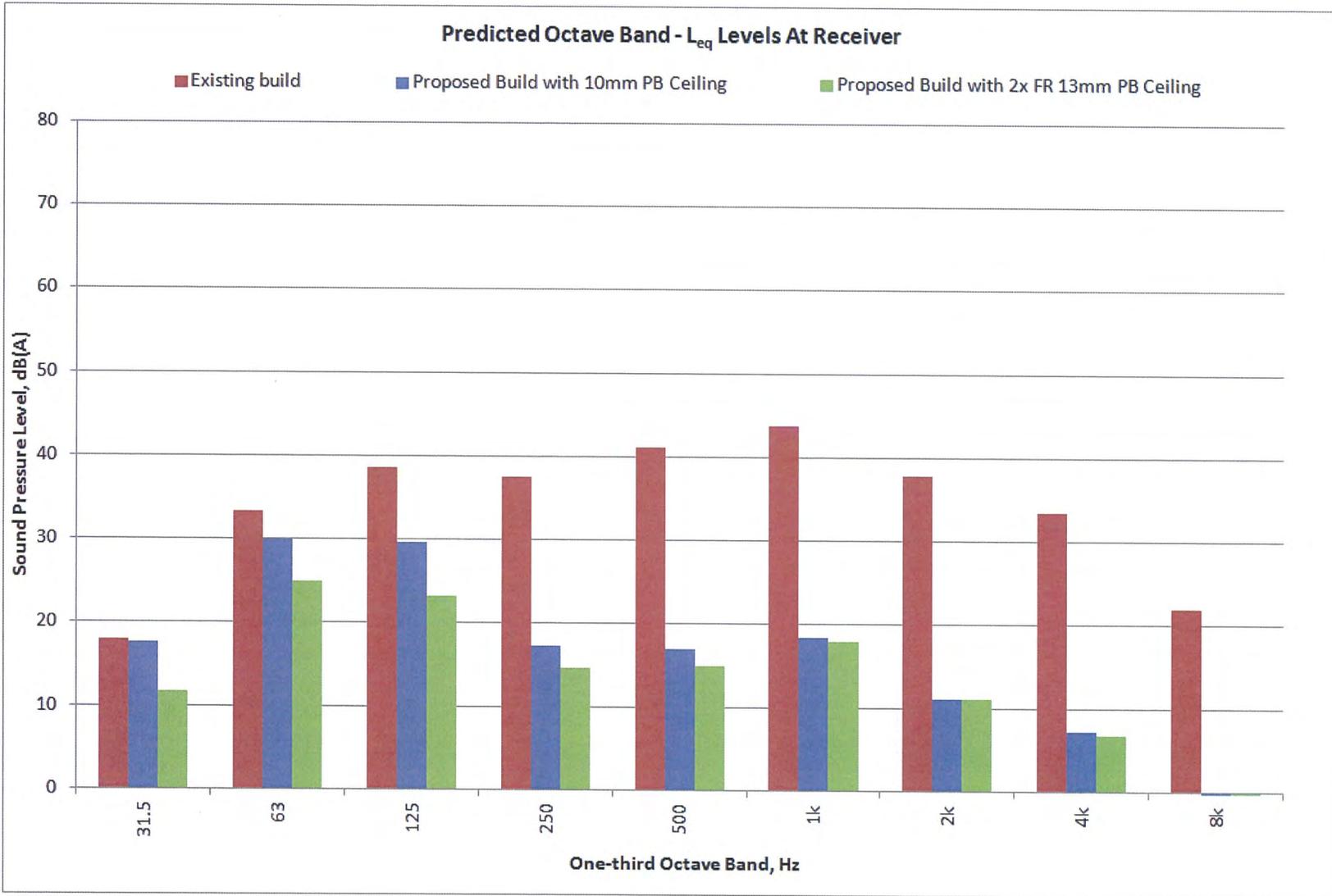
To minimise the impact from car doors closing and/or patrons talking, it is proposed to implement a noise management plan to encourage patrons to park further north and discourage small gatherings in the car park after the last session. This plan is presented in *Section 5.0*.

Alternatively to the above, increasing the boundary wall height was also investigated to reduce the impact from both sources. It was found that:

- Increasing wall height to 2.1m high wall result in predicted car doors closing and people talking noise levels of 45 dB(A) and 44 dB(A) respectively; and,
- Increasing wall height to 2.4m high wall result in predicted car doors closing and people talking noise levels of 43 dB(A) and 42 dB(A) respectively.

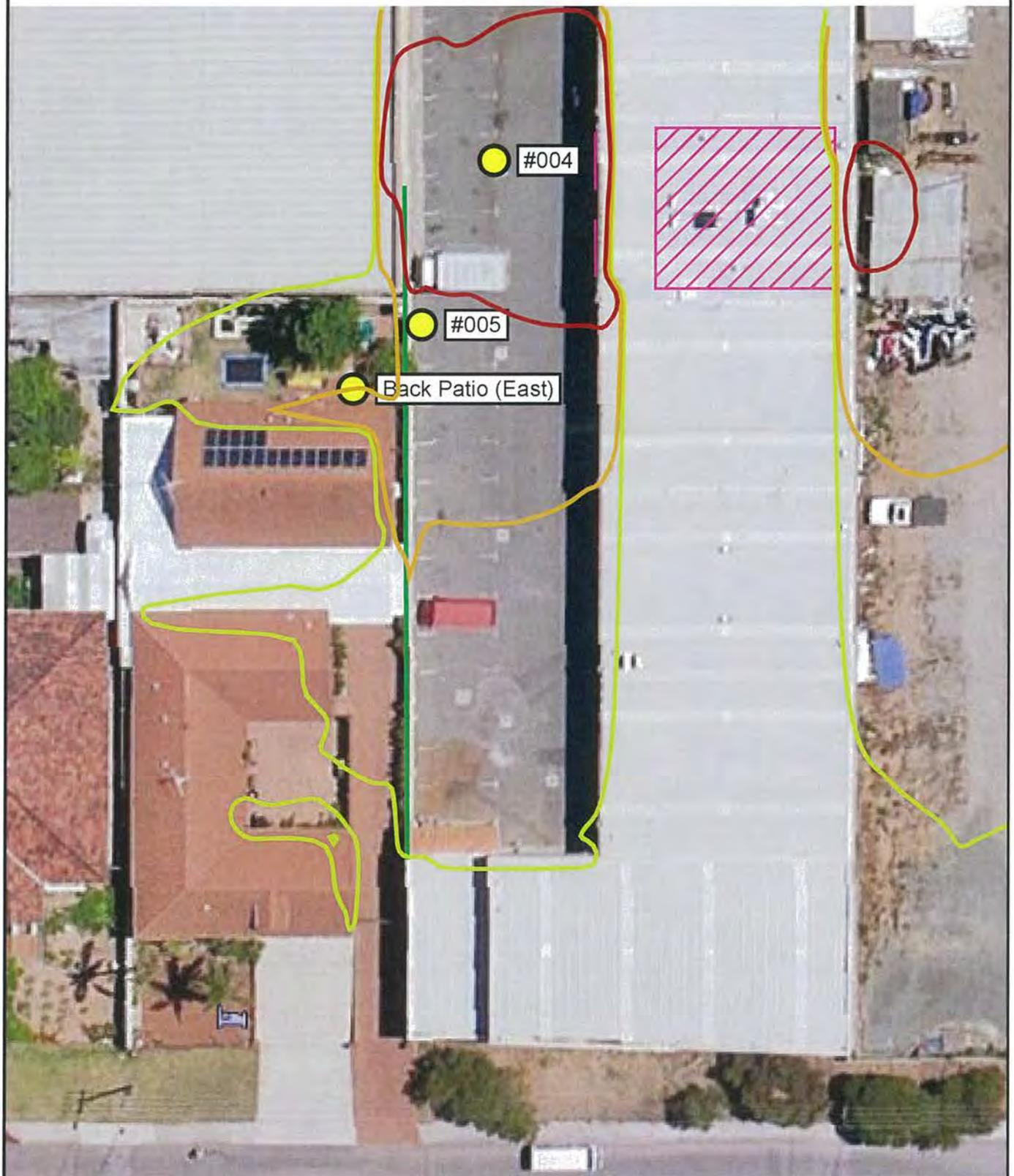
Based on the above, it is noted that increasing the height of the existing boundary wall to 2.1 metres would result in the above noise sources to comply with the Regulations.

Figure 4-1 Predicted Spectral Noise Levels

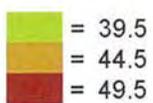


Kelete Dance Studio - Proposed Alterations
 Music Noise Levels with 10mm ceiling and +10 dB Music Penalty Added

Figure 4-2



Noise Levels
 dB LA10



Signs and symbols

-  Receiver
-  Area Source
-  Existing Fences/Walls



11 February 2016

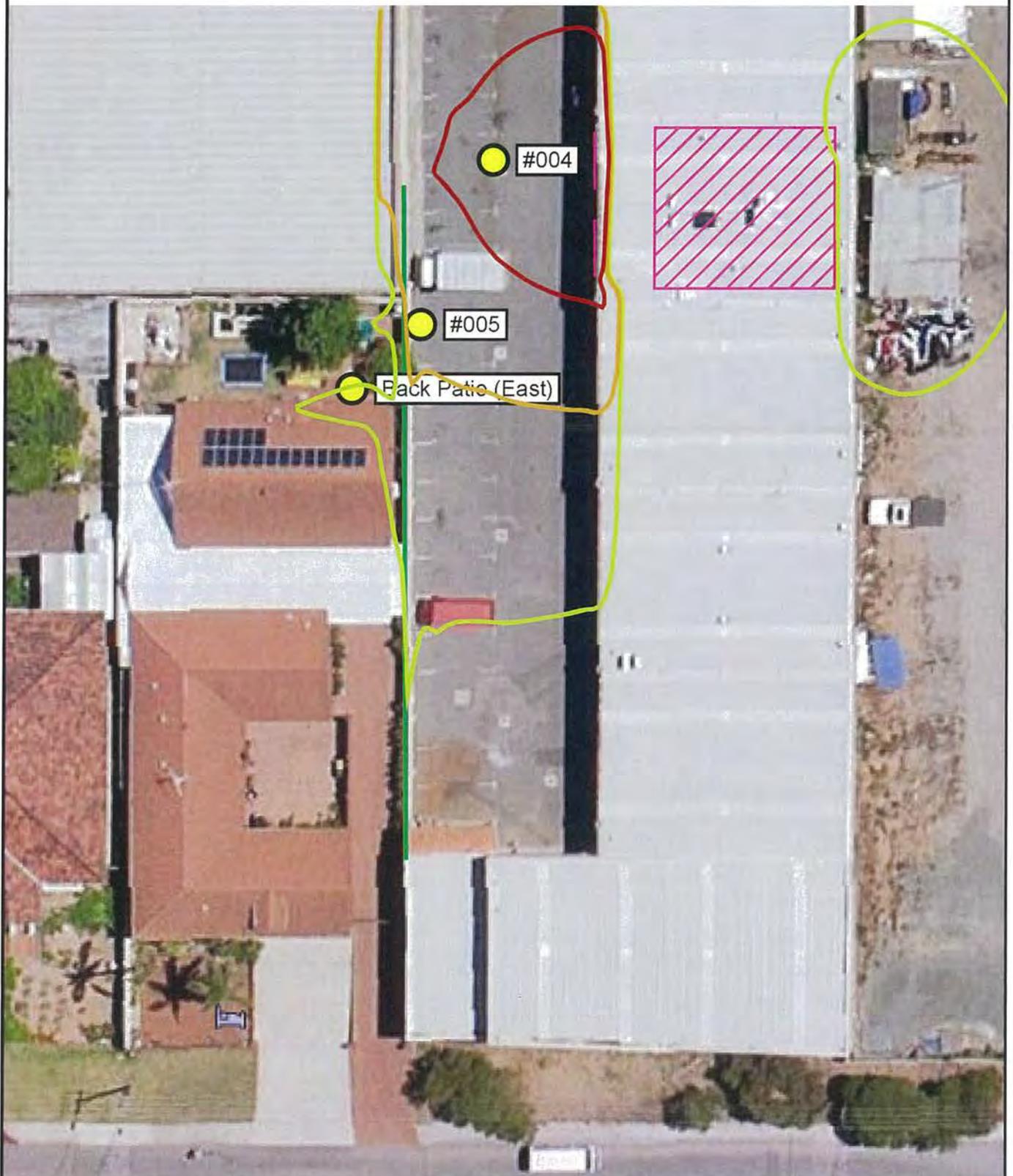
Length Scale 1:360



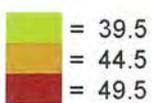
Lloyd George Acoustics
 by Olivier Mallié
 olivier@lgacoustics.com.au
 0439 987 455

Kelete Dance Studio - Proposed Alterations
 Music Noise Levels with upgraded ceiling and +10 dB Music Penalty Added

Figure 4-3



Noise Levels
 dB L_{A10}



Signs and symbols

- Receiver
- Area Source
- Existing Fences/Walls



11 February 2016

Length Scale 1:360



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5 RECOMMENDATIONS

5.1 Alterations to Existing Building

The proposed alterations to the existing building were predicted to mitigate noise levels so that compliance with the evening L_{A10} assigned noise levels are complied with, even with the music penalty added.

However, the following is noted in regard to the proposed new construction:

- The ceiling is of solid construction, that is, perforated ceiling cannot be used. The minimum construction required is 10mm thick plasterboard suspended ceiling. It is however recommended to upgrade the ceiling construction to incorporate two layers of 13mm thick fire rated plasterboards to significantly increase the attenuation of low frequency noise.
- The new wall partition in the roller door opening as proposed is considered sufficient however it is recommended to be upgraded as follows to provide additional low frequency attenuation:
 - One layer of standard 10mm thick plasterboard to inside directly fixed to 90mm steel stud,
 - 20 to 30 mm gap between studs with 2 layers of acoustic insulation,
 - One layer of 13mm fire rated plasterboard directly fixed to 90mm steel stud and covered with one layer of Hardiflex 7.5mm cement fibre sheet,
 - Cavity insulation to be double layer of Bradford Soundscreen insulation.

Above construction estimated rating is R_w (C, C_{tr}) of 58 (-4, -11)

- New double door to be built from pair of 44mm solid core doors minimum and fitted with RP10/RP10 Si perimeter seal, RP99Si bottom seal and RP71 or RP71Si astragal (source Raven). In addition, all gaps are to be filled and ensure all door seals are adequately fitted.
- The windows, including the external office window, are proposed to be double glazed which a significant improvement from existing. Given the relative noise contribution from the external office window, it is noted though a single pane of 6.38mm laminated glass would be sufficient.
- To allow for fresh air circulation and prevent noise breakout from the existing ventilation outlets, it is recommended to incorporate air grilles in the new ceiling and connect to the existing vents via at least 2 metres of flexible acoustic ducts. Additional fresh air inlets should not be incorporated in either the roof or walls however, if more vents are specifically required, a detailed review of the acoustic requirements for any additional vents should be carried out.

It is noted that with the installation of the new ceiling, the acoustic absorption provided by the insulation blanket under the purlins will be lost, resulting in a more reverberant space. It is therefore recommended incorporating an equal amount of acoustic absorption within the space than was available to the underside of the ceiling (approximately 130 m²). Typical suitable products include Ecophon Solo baffles or Armstrong Soundscape baffles which could be hang underneath the ceiling or Autex wall panels.

Finally, this assessment and recommendations are based on the internal noise levels as recorded on site and presented in this report. It is therefore recommended to monitor internal noise levels and/or setup the audio system in such a way as to prevent excessive overall or low frequency noise levels being played back within the Dance Studio.

5.2 Noise Management Plan

In order to minimise noise from car parking and patrons leaving the venue, the procedures presented below will be put in place.

- Kelete Theatre and Performing Arts Company (Kelete Company) will physically place cones on car parking spaces from Unit 7 down toward the rear of the subject lot adjacent to the residential properties and advertise on site that all classes after 7.00pm are to utilise car parking spaces:
 - From Unit 6 to the front of the subject lot toward Rockingham Road, which accommodates 14 car parking spaces prior to the start of residential properties. Refer to Photo 1 below.

Photo 1



- As an alternative measure (and if the City accepts this method), Kelete Company can direct people by advertisement and word of mouth to park at 145 Rockingham Road. There are 15-20 car parking spaces available for public use adjacent to Rockingham Road and Kelete Company has received verbal agreement from the owner that this is suitable. The alternative car park location is shown below in red.



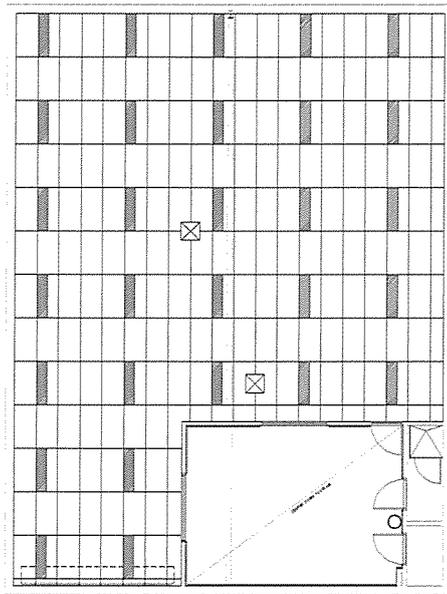
- Any patrons using the subject premises after 7.00pm will be advised of the car parking rules, and a representative from the studio will also be present. Maps will also be supplied to all patrons to clearly show which car parking bays are not to be used after 7.00pm. Finally, signage will be erected inside the premises to reinforce these parking rules.
- Similar to the above, signage will also be erected to request patrons are to keep noise to a minimum when attending the Venue, and in particular after 9.00pm in order to be considerate to the surrounding residents. Signage to that effect will also be erected on the external wall by the entry door to reinforce this message.
- The main entry / exit door will be kept closed at all times during Classes and access managed at end of classes to ensure the door is not propped open or left open for extended periods of time.
- Kelete Company will inform clients and patrons in written and verbal form that alternative transportation methods are encouraged where possible, including car-pooling, walking, catching public transportation and cycling.

- To manage traffic effectively Kelete Company will ensure ALL classes do not exceed the following maximum number of patrons:
 - Classes from 4.00pm – 7.00pm = 15 patrons; and,
 - Classes after 7.00pm = 20 patrons.

Appendix A

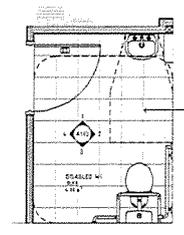
Development Plans

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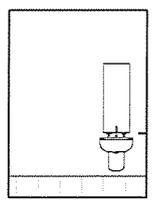
NOTE:
 PROPOSED SUSPENDED CEILING THROUGHOUT MAIN AREA, FLOOR TO EXISTING STEEL ROOF STRUCTURE TO MAINTAIN CLEAR OPERATIONS HEIGHT TO CLEAR EXISTING ROLLER DOOR W/ OF POOLROOM. EXISTING REFRIGERATE CEILING PANELS INSTALLED IN PROPRIETARY SUSPENSION SYSTEM BRASSFORD BEE KINETIC GAFFNEY 1/2" - 1/4" HANGING TIE.

NOTE:
 CONTRACTOR TO NOTE POSITIONS OF OTHER AIR CONDITIONER PUMP TERMINALS & ALLOW FOR IN PROPOSED CEILING DESIGN.

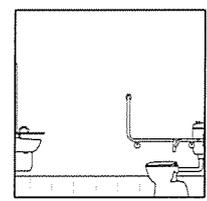


FOR CUP RECAST PIPE TO PROPOSED WALL SHALL BE FINISHED TO MATCH SURROUNDING WALL.

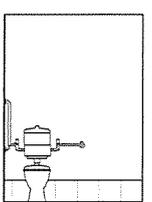
BATH - INTERNAL LAYOUT
 PLAN 1/4" = 1'-0"



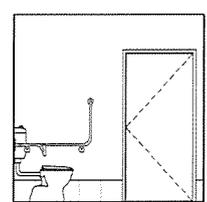
INTERNAL ELEVATION 2
 PLAN 1/4" = 1'-0"



INTERNAL ELEVATION 3
 PLAN 1/4" = 1'-0"



INTERNAL ELEVATION 4
 PLAN 1/4" = 1'-0"



INTERNAL ELEVATION 1
 PLAN 1/4" = 1'-0"

PROPOSED FIRST FLOOR
 PLAN 1/4" = 1'-0"

1. 1/4" = 1'-0"

CADDS ARCHITECTURAL
 1000 10th Street, Suite 100
 Vancouver, BC V6J 1A1
 TEL: 604.681.1111
 FAX: 604.681.1112
 WWW.CADDSARCHITECTURAL.COM

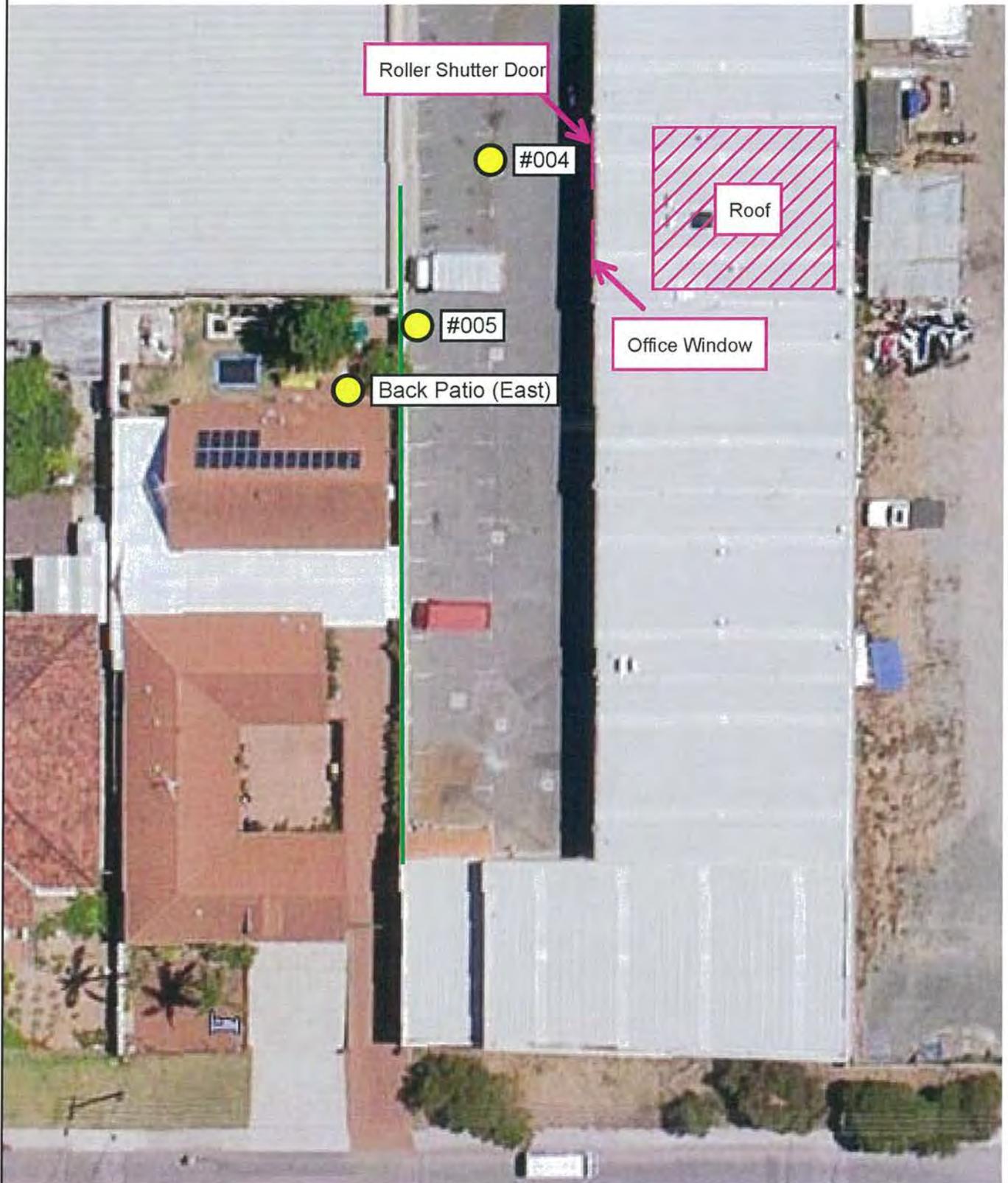
INFORMATION	
Project Name	A1.03
Sheet No.	A

Appendix B

Noise Model Overview

Kelete Dance Studio - Noise Model Overview - Building Noise Sources

Unit 9, 153 Rockingham Road, Hamilton Hill



Signs and symbols



-  Receiver
-  Area Source
-  Existing Fences/Walls

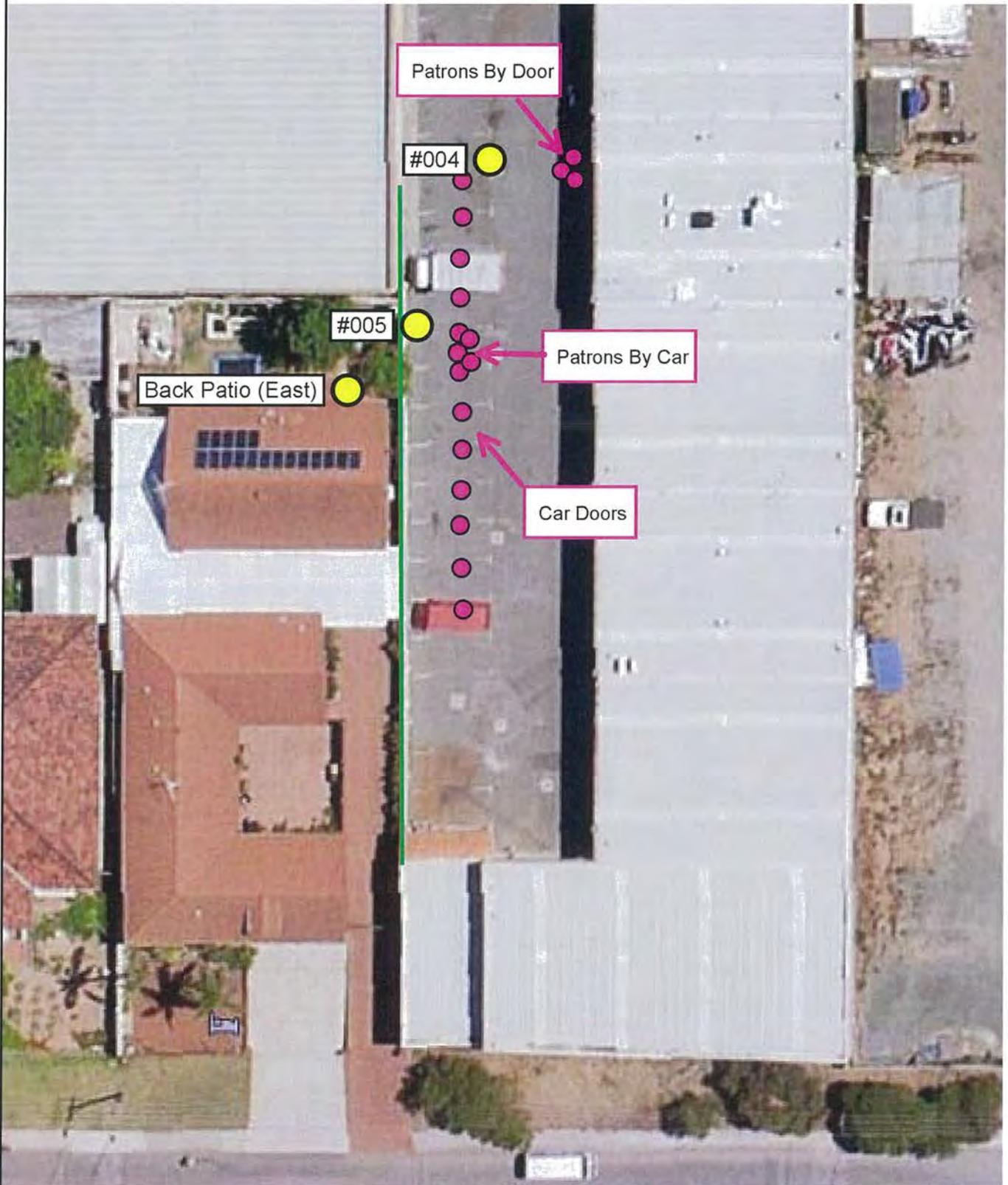
10 March 2016

Length Scale 1:360



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Kelete Dance Studio - Noise Model Overview - Car Park Noise Sources
 Unit 9, 153 Rockingham Road, Hamilton Hill



Signs and symbols



-  Receiver
-  Existing Fences/Walls
-  Point Source

10 March 2016

Length Scale 1:360



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 olivier@lgacoustics.com.au
 0439 987 455

Appendix C

Terminology

The following is an explanation of the terminology used throughout this report.

Decibel (dB)

The decibel is the unit that describes the sound pressure and sound power levels of a noise source. It is a logarithmic scale referenced to the threshold of hearing.

A-Weighting

An A-weighted noise level has been filtered in such a way as to represent the way in which the human ear perceives sound. This weighting reflects the fact that the human ear is not as sensitive to lower frequencies as it is to higher frequencies. An A-weighted sound level is described as L_A dB.

Sound Power Level (L_w)

Under normal conditions, a given sound source will radiate the same amount of energy, irrespective of its surroundings, being the sound power level. This is similar to a 1kW electric heater always radiating 1kW of heat. The sound power level of a noise source cannot be directly measured using a sound level meter but is calculated based on measured sound pressure levels at known distances. Noise modelling incorporates source sound power levels as part of the input data.

Sound Pressure Level (L_p)

The sound pressure level of a noise source is dependent upon its surroundings, being influenced by distance, ground absorption, topography, meteorological conditions etc and is what the human ear actually hears. Using the electric heater analogy above, the heat will vary depending upon where the heater is located, just as the sound pressure level will vary depending on the surroundings. Noise modelling predicts the sound pressure level from the sound power levels taking into account ground absorption, barrier effects, distance etc.

L_{ASlow}

This is the noise level in decibels, obtained using the A frequency weighting and the S time weighting as specified in AS1259.1-1990. Unless assessing modulation, all measurements use the slow time weighting characteristic.

L_{AFast}

This is the noise level in decibels, obtained using the A frequency weighting and the F time weighting as specified in AS1259.1-1990. This is used when assessing the presence of modulation only.

L_{APeak}

This is the maximum reading in decibels using the A frequency weighting and P time weighting AS1259.1-1990.

L_{Amax}

An L_{Amax} level is the maximum A-weighted noise level during a particular measurement.

L_{A1}

An L_{A1} level is the A-weighted noise level which is exceeded for one percent of the measurement period and is considered to represent the average of the maximum noise levels measured.

L_{A10}

An L_{A10} level is the A-weighted noise level which is exceeded for 10 percent of the measurement period and is considered to represent the "intrusive" noise level.

L_{Aeq}

The equivalent steady state A-weighted sound level (“equal energy”) in decibels which, in a specified time period, contains the same acoustic energy as the time-varying level during the same period. It is considered to represent the “average” noise level.

L_{A90}

An L_{A90} level is the A-weighted noise level which is exceeded for 90 percent of the measurement period and is considered to represent the “background” noise level.

One-Third-Octave Band

Means a band of frequencies spanning one-third of an octave and having a centre frequency between 25 Hz and 20 000 Hz inclusive.

L_{Amax} assigned level

Means an assigned level which, measured as a $L_{A\ Slow}$ value, is not to be exceeded at any time.

L_{A1} assigned level

Means an assigned level which, measured as a $L_{A\ Slow}$ value, is not to be exceeded for more than 1% of the representative assessment period.

L_{A10} assigned level

Means an assigned level which, measured as a $L_{A\ Slow}$ value, is not to be exceeded for more than 10% of the representative assessment period.

Tonal Noise

A tonal noise source can be described as a source that has a distinctive noise emission in one or more frequencies. An example would be whining or droning. The quantitative definition of tonality is:

the presence in the noise emission of tonal characteristics where the difference between -

- (a) the A-weighted sound pressure level in any one-third octave band; and
- (b) the arithmetic average of the A-weighted sound pressure levels in the 2 adjacent one-third octave bands,

is greater than 3 dB when the sound pressure levels are determined as $L_{Aeq,T}$ levels where the time period T is greater than 10% of the representative assessment period, or greater than 8 dB at any time when the sound pressure levels are determined as $L_{A\ Slow}$ levels.

This is relatively common in most noise sources.

Modulating Noise

A modulating source is regular, cyclic and audible and is present for at least 10% of the measurement period. The quantitative definition of modulation is:

a variation in the emission of noise that —

- (a) is more than 3 dB $L_{A\ Fast}$ or is more than 3 dB $L_{A\ Fast}$ in any one-third octave band;
- (b) is present for at least 10% of the representative.

Impulsive Noise

An impulsive noise source has a short-term banging, clunking or explosive sound. The quantitative definition of impulsiveness is:

a variation in the emission of a noise where the difference between $L_{A\ peak}$ and $L_{A\ Max\ slow}$ is more than 15 dB when determined for a single representative event;

Major Road

Is a road with an estimated average daily traffic count of more than 15,000 vehicles.

Secondary / Minor Road

Is a road with an estimated average daily traffic count of between 6,000 and 15,000 vehicles.

Influencing Factor (IF)

$$= \frac{1}{10} (\% \text{ Type A}_{100} + \% \text{ Type A}_{450}) + \frac{1}{20} (\% \text{ Type B}_{100} + \% \text{ Type B}_{450})$$

where :

% Type A₁₀₀ = the percentage of industrial land within
a 100m radius of the premises receiving the noise

%TypeA₄₅₀ = the percentage of industrial land within
a 450m radius of the premises receiving the noise

% Type B₁₀₀ = the percentage of commercial land within
a 100m radius of the premises receiving the noise

%TypeB₄₅₀ = the percentage of commercial land within
a 450m radius of the premises receiving the noise

+ Traffic Factor (maximum of 6 dB)

= 2 for each secondary road within 100m

= 2 for each major road within 450m

= 6 for each major road within 100m

Representative Assessment Period

Means a period of time not less than 15 minutes, and not exceeding four hours, determined by an inspector or authorised person to be appropriate for the assessment of a noise emission, having regard to the type and nature of the noise emission.

Background Noise

Background noise or residual noise is the noise level from sources other than the source of concern. When measuring environmental noise, residual sound is often a problem. One reason is that regulations often require that the noise from different types of sources be dealt with separately. This separation, e.g. of traffic noise from industrial noise, is often difficult to accomplish in practice. Another reason is that the measurements are normally carried out outdoors. Wind-induced noise, directly on the microphone and indirectly on trees, buildings, etc., may also affect the result. The character of these noise sources can make it difficult or even impossible to carry out any corrections.

Ambient Noise

Means the level of noise from all sources, including background noise from near and far and the source of interest.

Specific Noise

Relates to the component of the ambient noise that is of interest. This can be referred to as the noise of concern or the noise of interest.

Peak Component Particle Velocity (PCPV)

The maximum instantaneous velocity in mm/s of a particle at a point during a given time interval and in one of the three orthogonal directions (x, y or z) measured as a peak response. Peak velocity is normally used for the assessment of structural damage from vibration.

Peak Particle Velocity (PPV)

The maximum instantaneous velocity in mm/s of a particle at a point during a given time interval and is the vector sum of the PCPV for the x, y and z directions measured as a peak response. Peak velocity is normally used for the assessment of structural damage from vibration.

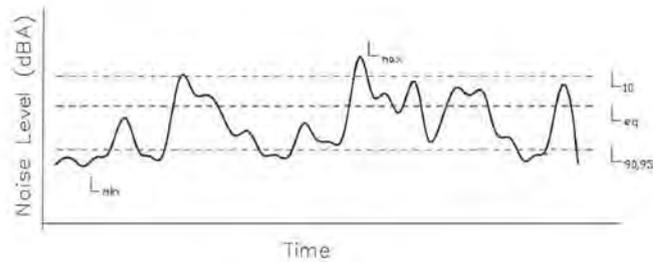
RMS Component Particle Velocity (PCPV)

The maximum instantaneous velocity in mm/s of a particle at a point during a given time interval and in one of the three orthogonal directions (x, y or z) measured as a root mean square (rms) response. RMS velocity is normally used for the assessment of human annoyance from vibration.

Peak Particle Velocity (PPV)

The maximum instantaneous velocity in mm/s of a particle at a point during a given time interval and is the vector sum of the PCPV for the x, y and z directions measured as a root mean square (rms) response. RMS velocity is normally used for the assessment of human annoyance from vibration.

Chart of Noise Level Descriptors

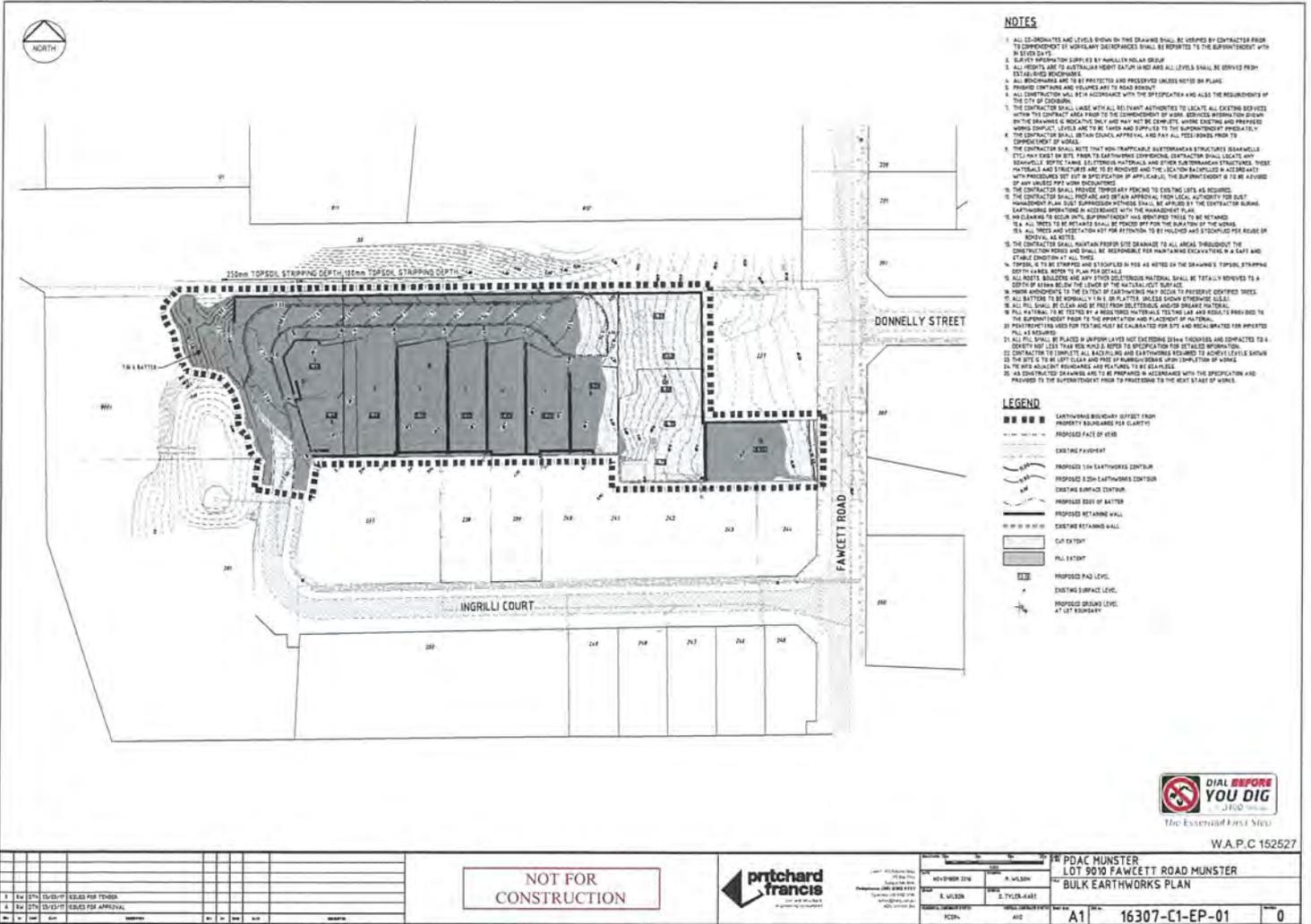


Typical Noise Levels





		<p>PRINTED ON: 19/05/2017</p>	<p>SCALE = 1:1347</p>	<p>NORTH</p>
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NOTES

1. ALL DIMENSIONS AND LEVELS SHOWN IN THIS DRAWING SHALL BE INTERPRETED BY THE CONTRACTOR PRIOR TO COMMENCEMENT OF WORK. ANY DISCREPANCIES SHALL BE REPORTED TO THE SUPERINTENDENT WITHIN 48 HOURS.
2. SURVEY INFORMATION SUPPLIED BY MURKIN & ASSOCIATES SHALL BE USED AS SHOWN. ALL LEVELS SHALL BE DERIVED FROM THE PROVIDED BENCHMARKS.
3. ALL BENCHMARKS ARE TO BE PROTECTED AND PRESERVED UNLESS NOTED ON PLANS.
4. PROPOSED CUTTING AND FILLING ARE TO BE ACCORDING TO THE SPECIFICATIONS AND ALSO THE REQUIREMENTS OF THE CITY OF COVINGTON.
5. THE CONTRACTOR SHALL MAINTAIN ALL NECESSARY ACCESS TO ALL EXISTING UTILITIES WITHIN THE CONTRACT AREA PRIOR TO THE COMMENCEMENT OF WORK. SERVICES INFORMATION FROM ALL UTILITIES SHALL BE OBTAINED PRIOR TO THE COMMENCEMENT OF WORK. EXISTING AND PROPOSED WORKS SHALL BE CLEARLY MARKED AND IDENTIFIED TO THE SUPERINTENDENT PRIOR TO THE COMMENCEMENT OF WORK.
6. THE CONTRACTOR SHALL OBTAIN NECESSARY APPROVALS AND PERMITS PRIOR TO THE COMMENCEMENT OF WORK.
7. THE CONTRACTOR SHALL NOTE THAT NON-TRAFFICABLE SUBSTRUCTURES (BEARINGS, TIES) MAY EXIST IN THE AREA. PRIOR TO THE COMMENCEMENT OF WORK, THE CONTRACTOR SHALL LOCATE ANY SUCH EXISTING STRUCTURES AND REPORT THEM TO THE SUPERINTENDENT PRIOR TO THE COMMENCEMENT OF WORK.
8. THE CONTRACTOR SHALL PROVIDE TEMPORARY FENCING TO THE EXISTING LOTS AS SHOWN.
9. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY EROSION CONTROL MEASURES THROUGHOUT THE CONSTRUCTION PERIOD AND SHALL BE RESPONSIBLE FOR MAINTAINING EROSION CONTROL IN A SAFE AND STABLE CONDITION AT ALL TIMES.
10. THE CONTRACTOR SHALL MAINTAIN PROPER SITE DRAINAGE TO ALL AREAS THROUGHOUT THE CONSTRUCTION PERIOD AND SHALL BE RESPONSIBLE FOR MAINTAINING EROSION CONTROL IN A SAFE AND STABLE CONDITION AT ALL TIMES.
11. THE CONTRACTOR SHALL MAINTAIN PROPER SITE DRAINAGE TO ALL AREAS THROUGHOUT THE CONSTRUCTION PERIOD AND SHALL BE RESPONSIBLE FOR MAINTAINING EROSION CONTROL IN A SAFE AND STABLE CONDITION AT ALL TIMES.
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18. THE CONTRACTOR SHALL MAINTAIN PROPER SITE DRAINAGE TO ALL AREAS THROUGHOUT THE CONSTRUCTION PERIOD AND SHALL BE RESPONSIBLE FOR MAINTAINING EROSION CONTROL IN A SAFE AND STABLE CONDITION AT ALL TIMES.
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LEGEND

- EARTHWORK BOUNDARY - OFFSET FROM PROPERTY BOUNDARY (SEE CLARIFICATION)
- PROPOSED FACE OF EMBANKMENT
- EXISTING PAVEMENT
- PROPOSED 10% EARTHWORKS CONTOUR
- PROPOSED 1.5% EARTHWORKS CONTOUR
- EXISTING SURFACE CONTOUR
- PROPOSED EARTHWORKS CONTOUR
- PROPOSED RETAINING WALL
- EXISTING RETAINING WALL
- CUT EXTENT
- FILL EXTENT
- PROPOSED FINISH LEVEL
- EXISTING SURFACE LEVEL
- PROPOSED GROUND LEVEL
- AT LOT BOUNDARY



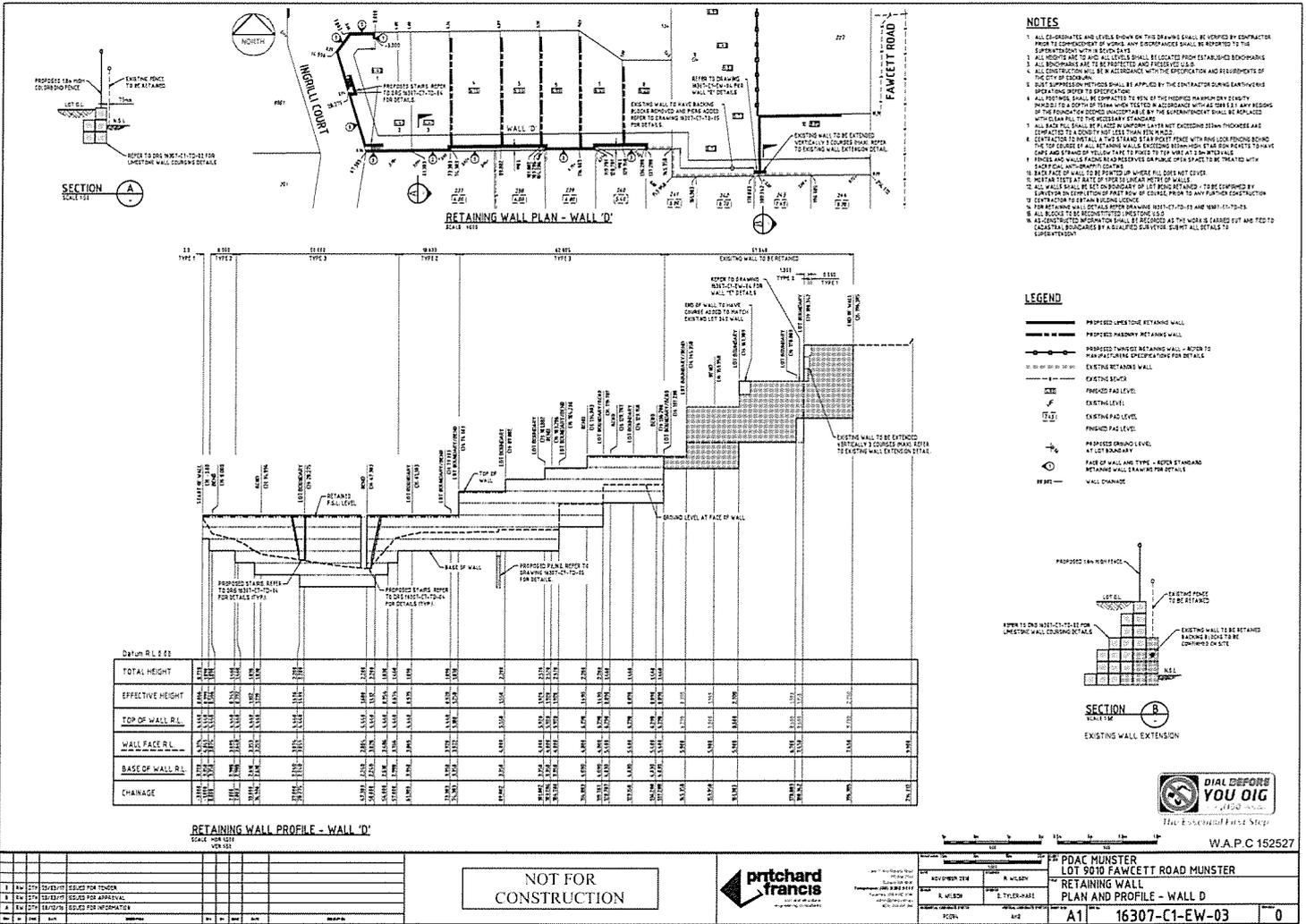
W.A.P.C 152527

NOT FOR CONSTRUCTION



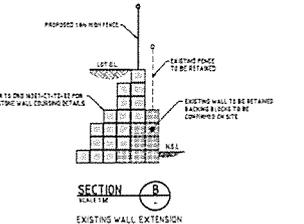
DATE: 08/06/2017
 DRAWN BY: A. WILSON
 CHECKED BY: S. TUCKER
 PROJECT NO: 16307-C1-EP-01

PDAC MUNSTER
 LOT 9010 FAWCETT ROAD MUNSTER
 BULK EARTHWORKS PLAN
 SHEET NO: A1
 PROJECT NO: 16307-C1-EP-01
 TOTAL SHEETS: 0



- NOTES**
1. ALL DIMENSIONS AND LEVELS SHOWN ON THIS DRAWING SHALL BE VERIFIED BY CONTRACTOR PRIOR TO COMMENCEMENT OF WORK. ANY DISCREPANCIES SHALL BE REPORTED TO THE SUPERINTENDENT WITHIN 24 HOURS.
 2. ALL HEIGHTS ARE TO AND ALL LEVELS SHALL BE LOCATED FROM ESTABLISHED BENCHMARKS.
 3. ALL DIMENSIONS ARE TO BE PROTECTED AND MAINTAINED THROUGHOUT CONSTRUCTION.
 4. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS AND REQUIREMENTS OF THE CITY OF LOS ANGELES.
 5. ALL CONSTRUCTION METHODS SHALL BE APPROVED BY THE CONTRACTOR THROUGH THE SUPERINTENDENT PRIOR TO IMPLEMENTATION.
 6. ALL EXCAVATIONS SHALL BE PROTECTED BY SHIELDING OR SHIELDING EQUIPMENT TO A DEPTH OF 10 FEET UNLESS OTHERWISE SPECIFIED IN THE SPECIFICATIONS.
 7. ALL EXCAVATIONS SHALL BE PROTECTED BY SHIELDING OR SHIELDING EQUIPMENT TO A DEPTH OF 10 FEET UNLESS OTHERWISE SPECIFIED IN THE SPECIFICATIONS.
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 16. ALL EXCAVATIONS SHALL BE PROTECTED BY SHIELDING OR SHIELDING EQUIPMENT TO A DEPTH OF 10 FEET UNLESS OTHERWISE SPECIFIED IN THE SPECIFICATIONS.
 17. ALL EXCAVATIONS SHALL BE PROTECTED BY SHIELDING OR SHIELDING EQUIPMENT TO A DEPTH OF 10 FEET UNLESS OTHERWISE SPECIFIED IN THE SPECIFICATIONS.
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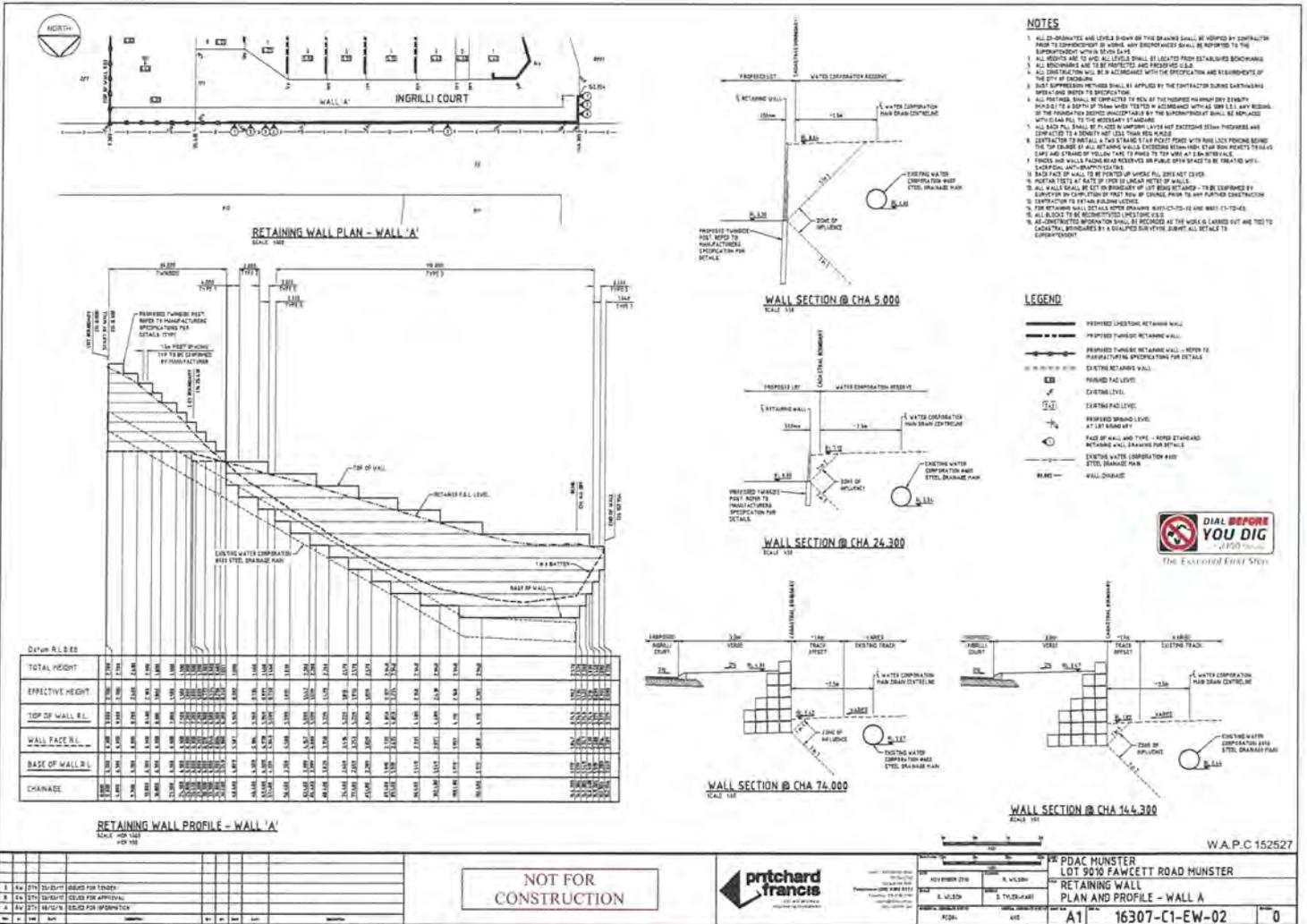
- LEGEND**
- PROPOSED Limestone RETAINING WALL
 - PROPOSED Masonry RETAINING WALL
 - PROPOSED TWISTED RETAINING WALL - REFER TO PLAN FOR CHANGES AND DIMENSIONS FOR DETAILS
 - EXISTING RETAINING WALL
 - EXISTING DRIVE
 - PROPOSED PAD LEVEL
 - EXISTING PAD LEVEL
 - PROPOSED BRIDGE LEVEL AT INTERSECTION
 - FACE OF WALL AND TYPE - REFER STANDARDS DRAWING FOR DETAILS
 - WALL CHANGE



PROJECT NO.	16307-C1-EW-03
DATE	02/06/2017
SCALE	AS SHOWN
DESIGNED BY	R. WILSON
CHECKED BY	E. TYLER-HART
DATE	02/06/2017
PROJECT	PDAC MUNSTER LOT 5010 FAWCETT ROAD MUNSTER
DESCRIPTION	RETAINING WALL PLAN AND PROFILE - WALL D
SCALE	AS SHOWN
DATE	02/06/2017
PROJECT	PDAC MUNSTER LOT 5010 FAWCETT ROAD MUNSTER
DESCRIPTION	RETAINING WALL PLAN AND PROFILE - WALL D
SCALE	AS SHOWN
DATE	02/06/2017
PROJECT	PDAC MUNSTER LOT 5010 FAWCETT ROAD MUNSTER
DESCRIPTION	RETAINING WALL PLAN AND PROFILE - WALL D
SCALE	AS SHOWN
DATE	02/06/2017

NOT FOR CONSTRUCTION



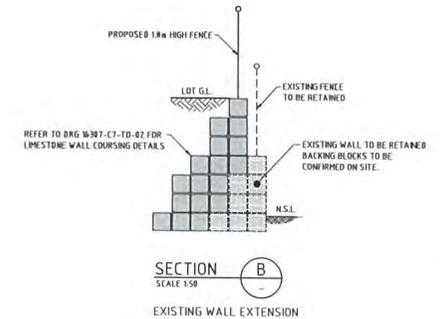


NOTES

1. ALL ELEVATIONS AND LEVELS SHOWN ON THIS DRAWING SHALL BE VERIFIED BY CONTRACTOR PRIOR TO COMMENCEMENT OF WORKS. ANY DISCREPANCIES SHALL BE REPORTED TO THE SUPERINTENDENT WITHIN SEVEN DAYS.
2. ALL HEIGHTS ARE TO AND ALL LEVELS SHALL BE LOCATED FROM ESTABLISHED BENCHMARKS.
3. ALL BENCHMARKS ARE TO BE PROTECTED AND PRESERVED U.S.D.
4. ALL CONSTRUCTION WILL BE IN ACCORDANCE WITH THE SPECIFICATIONS AND REQUIREMENTS OF THE CITY OF COCKBURN.
5. DUST SUPPRESSION METHODS SHALL BE APPLIED BY THE CONTRACTOR DURING EARTHWORKS OPERATIONS (REFER TO SPECIFICATION).
6. ALL FOOTINGS SHALL BE COMPACTED TO 95% OF THE MODIFIED MAXIMUM DRY DENSITY (MDD) TO A DEPTH OF 750mm WHEN TESTED IN ACCORDANCE WITH AS 1289.5.2.1 ANY REGIONS OF THE FOUNDATION DEEMED UNACCEPTABLE BY THE SUPERINTENDENT SHALL BE REPLACED WITH CLEAN FILL TO THE NECESSARY STANDARD.
7. ALL BACK FILL SHALL BE PLACED IN UNIFORM LAYER NOT EXCEEDING 300mm THICKNESS AND COMPACTED TO A DENSITY NOT LESS THAN 95% MDD.
8. CONTRACTOR TO INSTALL A TWO STRAND STAR PICKET FENCE WITH RING LOCK FENCING BEHIND THE TOP COURSE OF ALL RETAINING WALLS EXCEEDING 900mm HIGH. STAR IRON PICKETS TO HAVE CAPS AND STRAND BY YELLOW TAPE TO FIX TO TOP WIRE AT 2.0m INTERVALS.
9. FENCES AND WALLS FACING ROAD RESERVES OR PUBLIC OPEN SPACE TO BE TREATED WITH SACRIFICIAL ANTI-GRAFFITI COATING.
10. BACK FACE OF WALL TO BE POINTED UP WHERE FILL DOES NOT COVER.
11. MORTAR TESTS AT RATE OF 1 PER 50 LINEAR METRE OF WALLS.
12. ALL WALLS SHALL BE SET ON BOUNDARY OF LOT BEING RETAINED - TO BE CONFIRMED BY SURVEYOR ON COMPLETION OF FIRST ROW OF COURSE; PRIOR TO ANY FURTHER CONSTRUCTION.
13. CONTRACTOR TO OBTAIN BUILDING LEISURE.
14. FOR RETAINING WALL DETAILS REFER DRAWING 16307-C7-TD-02 AND 16307-C7-TD-03.
15. ALL BLOCKS TO BE RECONSTITUTED LIMESTONE U.S.D.
16. AS-CONSTRUCTED INFORMATION SHALL BE RECORDED AS THE WORK IS CARRIED OUT AND TIED TO CADASTRAL BOUNDARIES BY A QUALIFIED SURVEYOR. SUBMIT ALL DETAILS TO SUPERINTENDENT.

LEGEND

- PROPOSED LIMESTONE RETAINING WALL
- - - PROPOSED MASONRY RETAINING WALL
- PROPOSED TWINGIDE RETAINING WALL - REFER TO MANUFACTURERS SPECIFICATIONS FOR DETAILS
- - - EXISTING RETAINING WALL
- - - EXISTING SEWER
- 6.500 FINISHED PAD LEVEL
- 6.745 EXISTING LEVEL
- 6.745 EXISTING PAD LEVEL
- 6.745 FINISHED PAD LEVEL
- ⊕ PROPOSED GROUND LEVEL AT LOT BOUNDARY
- ⊕ FACE OF WALL AND TYPE - REFER STANDARD RETAINING WALL DRAWING FOR DETAILS
- 89.882 WALL CHANGING

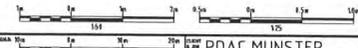


SECTION B
SCALE 1:50

EXISTING WALL EXTENSION



W.A.P.C 152527



PDAC MUNSTER
LOT 9010 FAWCETT ROAD MUNSTER
RETAINING WALL
PLAN AND PROFILE - WALL D

DATE: NOVEMBER 2016
DRAWN BY: R. WILSON
CHECKED BY: D. TYLER-HARRIS
PROJECT: PDAC MUNSTER
SHEET NO: A1
PROJECT NO: 16307-C1-EW-03

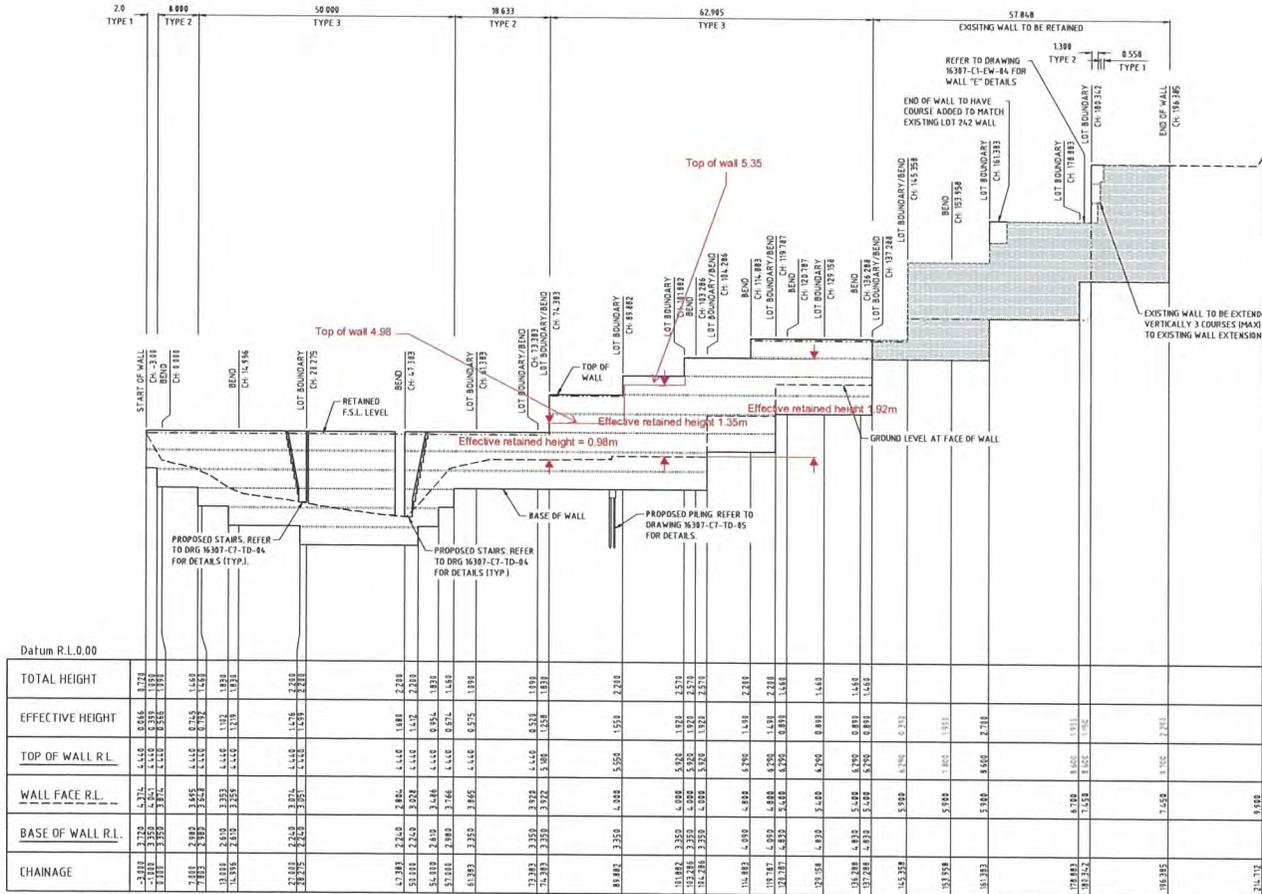


Level 1, 439 Pridmore Road
PO Box 2150
Salisbury WA 8041
Telephone: (81) 9222 5114
Facsimile: (81) 9222 5100
info@pritchard.com.au
ACN: 008 839 104

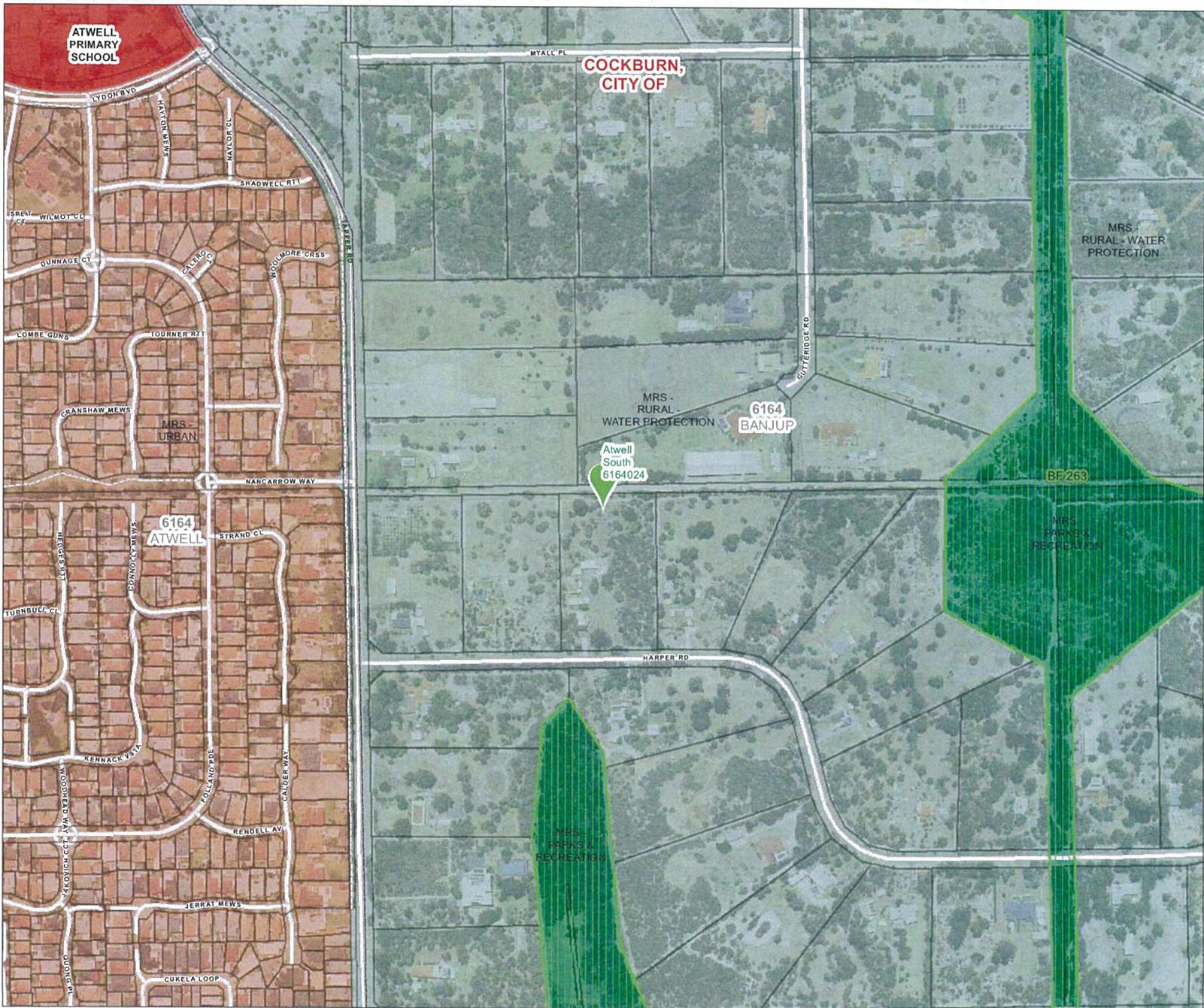
NOT FOR CONSTRUCTION

RETAINING WALL PROFILE - WALL 'D'

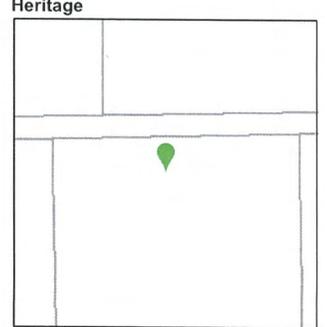
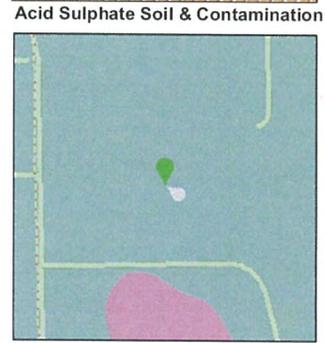
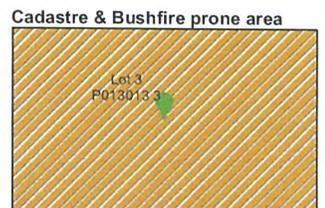
SCALE: HOR 1:500
VER 1:50







- Legend**
- LGA Boundary
 - Suburb Boundary
 - Cadastre
 - Community Sensitive Location
 - State Heritage Office Registered Site
 - Indigenous Heritage Site (Registered)
 - Bush Forever
 - RAMSAR Sites
 - BushFire Prone Areas 2016
 - DER Contaminated Site
 - Airport Facility
 - Reserves (LGATE123)
 - Acid Sulphate Soil
 - High to moderate risk
 - Moderate to low risk

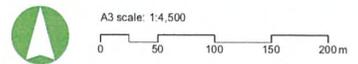


Date: 17/11/2016 Version: 1

Site Name: Atwell South
RFNSA ID: 6164024

Site Address: 25 Harper Road BANJUP WA 6164

Map by: J.P. L:\GIS\Projects\026428_Telstra\General_Sites_Manager\mod_02\02\2016_11\31



Job No: 252977
Coordinate system: MGA Zone 50

A3

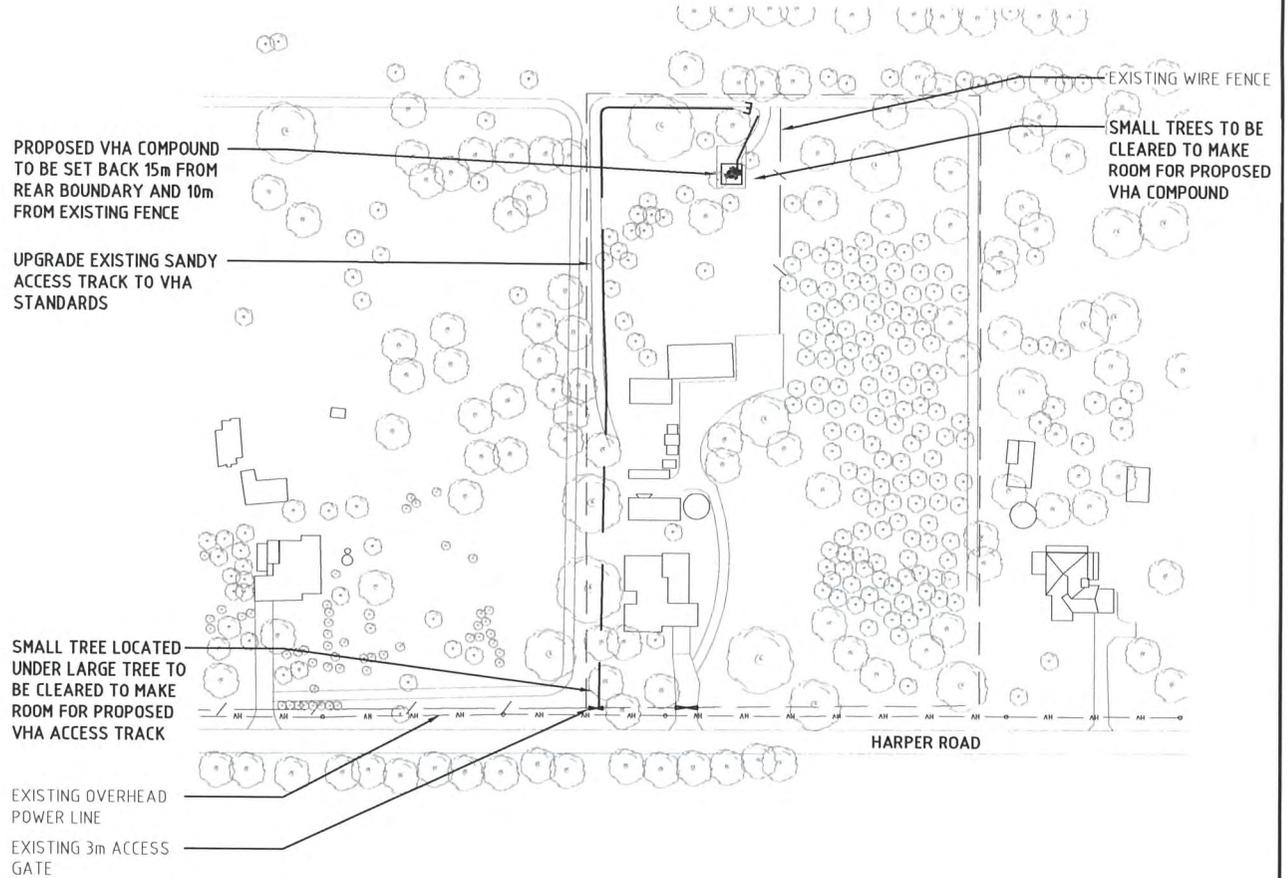


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 license, the full license of which can be accessed at <http://creativecommons.org/licenses/by-sa/2.0/>

SITE LOCALITY PLAN

NOT TO SCALE

RFNSA NUMBER: TBC		
CARRIER	SITE NAME	SITE ID
VHA	ATWELL SOUTH	640129



SITE ACCESS PLAN

SCALE 1:1500

Attach 2

A	15.11.16	PRELIMINARY / FOR APPROVAL (METRO EXPANSION - SOLE RISK)	HUAWEI	NY			 Huawei Technologies (Australia) Pty Ltd 15 Spring Level 4, Tower 11, 100 Pitt Street Sydney NSW 2000 Tel: +61 2 9038 7000 Fax: +61 2 9038 7001 Melbourne Level 24, 455 Collins Street Melbourne VIC 3000 Tel: +61 3 9078 0200 Fax: +61 3 9078 1975	 VODAFONE SITE 640129 METRO EXPANSION - SOLE RISK ATWELL SOUTH 25 HARPER ROAD BANJUP, WA 6164	TITLE SITE AND LOCALITY PLANS		
	REV	DATE	REVISION DESCRIPTION	VENDOR	DRAWN	DESIGNED			APPROD	DRAWING STATUS PRELIMINARY	DRAWING NO. 640129 - G1

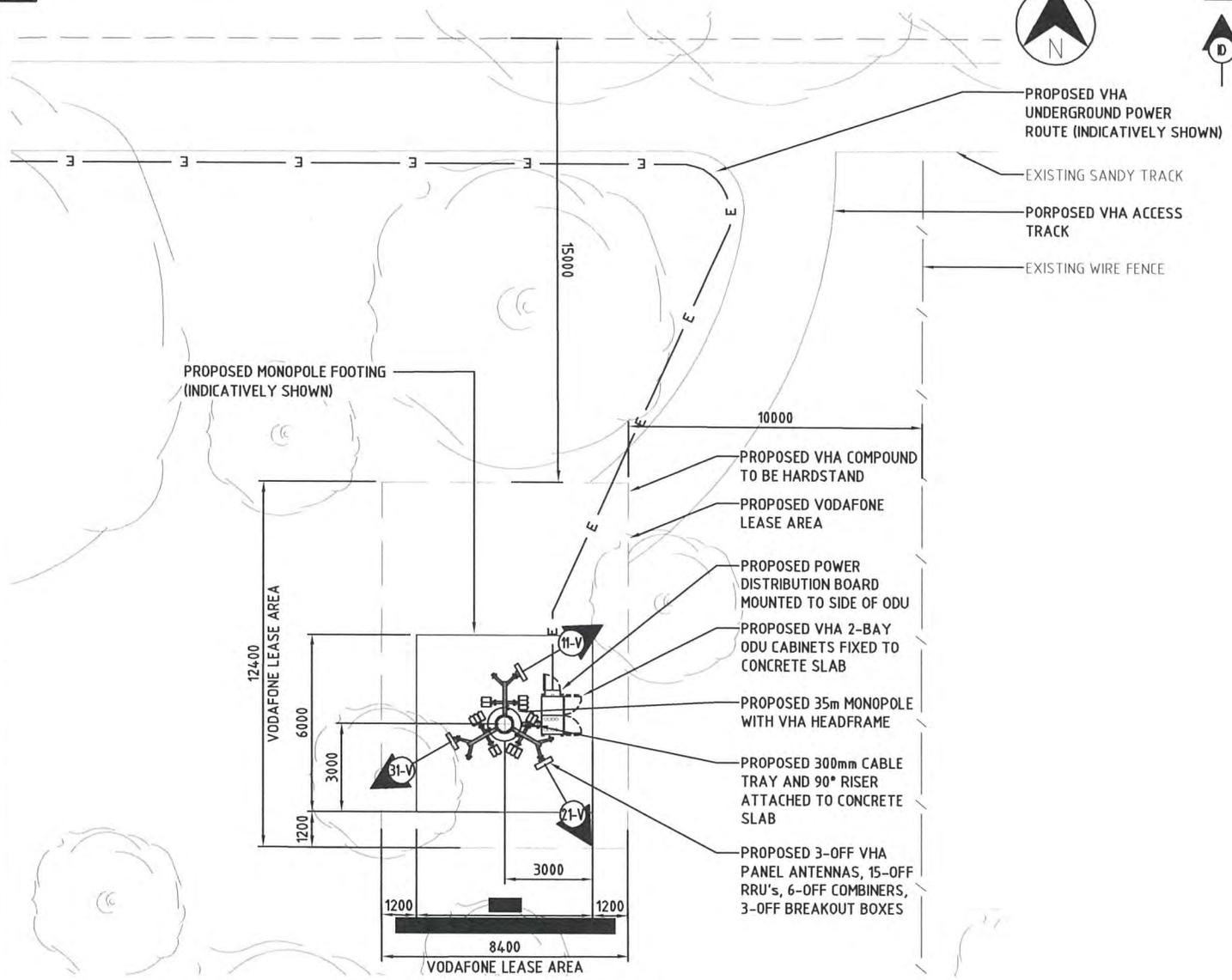
A3



LEGEND



DATUM POINT	GDA94 CO-ORDINATES		GROUND LEVEL
NEW	LATITUDE	-32.140629	A.H.D. RL. 614m
	LONGITUDE	115.874480	EL. 0.0m



NOTES:

1. ALL DIMENSIONS IN mm UNLESS STATED OTHERWISE.
2. THIS DRAWING SET IS PRELIMINARY DRAWINGS ONLY AND ISSUED FOR COMMENT. IT IS NOT A DETAILED SURVEY/STRUCTURAL DRAWING AND THEREFORE COULD BE SUBJECT TO CHANGE.
3. ALL FOOTING TYPES SHOWN ARE SUBJECT TO CONFIRMATION BY GEOTECHNICAL INVESTIGATION. FOOTINGS TO BE DESIGNED IN ACCORDANCE TO LATEST AUSTRALIAN STANDARDS AS RELEVANT TO BUILDING CODE OF AUSTRALIA.

SITE SETOUT PLAN

SCALE 1:150

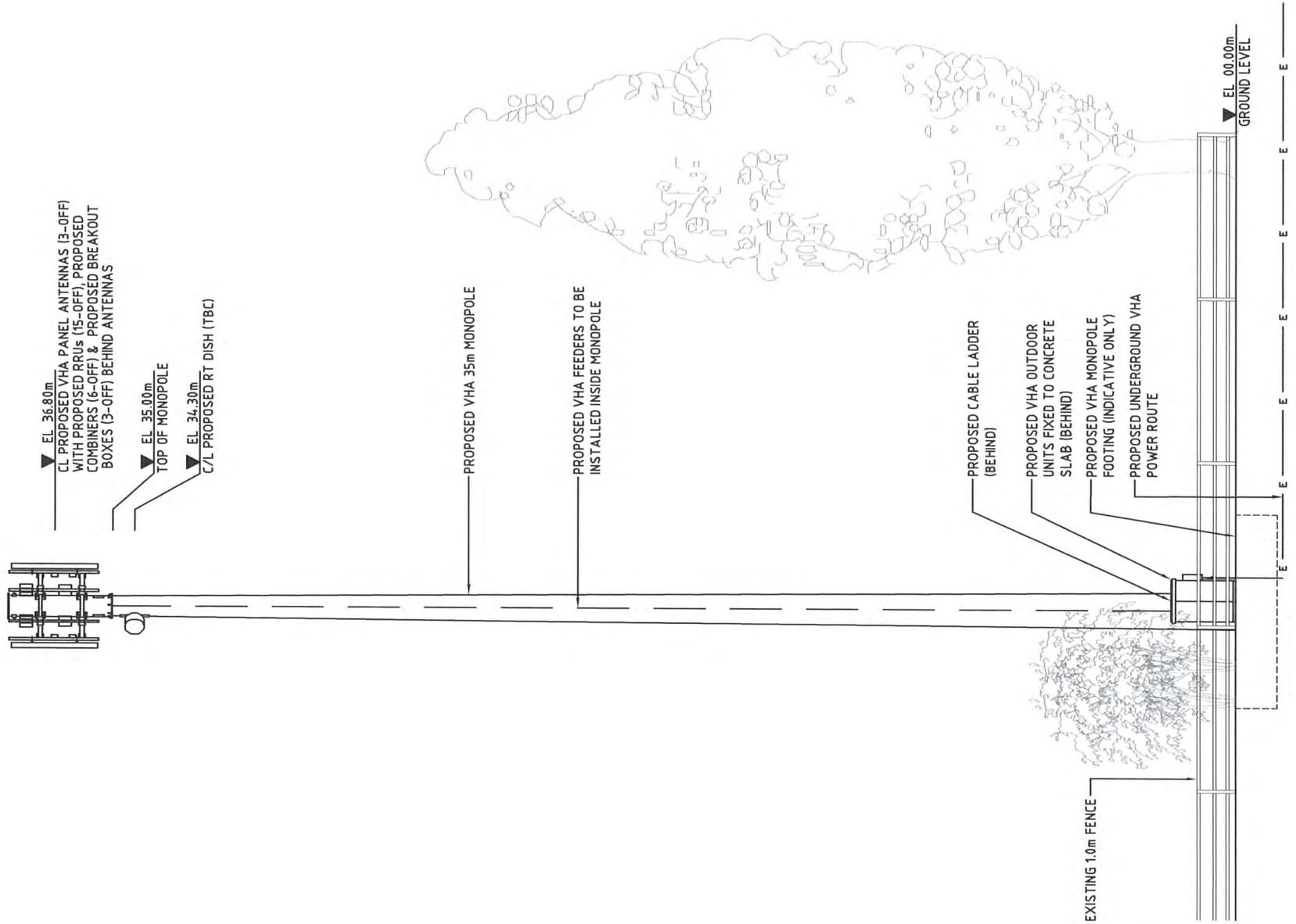
Attach 3

<p>Huawei Technologies (Australia) Pty Ltd Sydney Level 8 Tower 8 Two Park Drive Cherrybrook NSW 2762 Tel: +61 2 9608 3800 Fax: +61 2 9611 9000 Melbourne Level 4 400 Collins Street Melbourne VIC 3000 Tel: +61 3 9614 0000 Fax: +61 3 9611 9000</p>				<p>VODAFONE SITE 640129 METRO EXPANSION - SOLE RISK ATWELL SOUTH 25 HARPER ROAD BANJUP, WA 6164</p>		<p>TITLE SITE SETOUT PLAN</p>			
REV	DATE	REVISION DESCRIPTION	VENDOR	DRAWN	DESIGNED	APPROD	DRAWING STATUS	DRAWING No.	REV
A	15-11-16	PRELIMINARY / FOR APPROVAL (METRO EXPANSION - SOLE RISK)	HUAWEI	INV			PRELIMINARY	64-0129 - G2	A

A3

NOTES:

1. THIS DRAWING SET IS PRELIMINARY DRAWINGS ONLY AND ISSUED FOR COMMENT. IT IS NOT A DETAILED SURVEY/STRUCTURAL DRAWING AND THEREFORE COULD BE SUBJECT TO CHANGE.
2. ALL FOOTING TYPES SHOWN ARE SUBJECT TO CONFIRMATION BY GEOTECHNICAL INVESTIGATION. FOOTINGS TO BE DESIGNED IN ACCORDANCE TO LATEST AUSTRALIAN STANDARDS AS RELEVANT TO BUILDING CODE OF AUSTRALIA.



EAST ELEVATION

SCALE 1:125

Attach 4

01	15/11/16	PRELIMINARY / FOR APPROVAL (METRO EXPANSION - SOLE RISK)	HUAWEI	NV		
REV	DATE	REVISION DESCRIPTION	VENDOR	DRAWN	DESIGNED	APPROD



Huawei Technologies (Australia) Pty Ltd
 Sydney
 Level 8, Tower B, 700 Pitt Street
 Sydney NSW 2000
 Tel: +61 2 9616 3000 Fax: +61 2 9616 3001
 Melbourne
 Level 4, 443 Collins Street
 Melbourne VIC 3000
 Tel: +61 3 9619 0200 Fax: +61 3 9619 0201



VODAFONE SITE 640129 METRO EXPANSION - SOLE RISK
 ATWELL SOUTH
 25 HARPER ROAD
 BANJUP, WA 6164

TITLE SITE ELEVATION		
DRAWING STATUS PRELIMINARY	DRAWING No. 640129 - G3	REV A





Attach 6



Attach 7

Environmental EME Report 25 Harper Road, BANJUP WA 6164

This report provides a summary of Calculated RF EME Levels around the wireless base station

Date 21/11/2016

RFNSA Site No. 6164024

Introduction

The purpose of this report is to provide calculations of EME levels from the existing facilities at the site and any proposed additional facilities.

This report provides a summary of levels of radiofrequency (RF) electromagnetic energy (EME) around the wireless base station at 25 Harper Road BANJUP WA 6164. These levels have been calculated by Huawei using methodology developed by the Australian Radiation Protection and Nuclear Safety Agency (ARPANSA).

The maximum EME level calculated for the proposed systems at this site is 0.5% of the public exposure limit.

The ARPANSA Standard

ARPANSA, an Australian Government agency in the Health and Ageing portfolio, has established a Radiation Protection Standard specifying limits for general public exposure to RF transmissions at frequencies used by wireless base stations. The Australian Communications and Media Authority (ACMA) mandates the exposure limits of the ARPANSA Standard.

How the EME is calculated in this report

The procedure used for these calculations is documented in the ARPANSA Technical Report "Radio Frequency EME Exposure Levels - Prediction Methodologies" which is available at <http://www.arpansa.gov.au>.

RF EME values are calculated at 1.5m above ground at various distances from the base station, assuming level ground.

The estimate is based on worst-case scenario, including:

- wireless base station transmitters for mobile and broadband data operating at maximum power
- simultaneous telephone calls and data transmission
- an unobstructed line of sight view to the antennas.

In practice, exposures are usually lower because:

- the presence of buildings, trees and other features of the environment reduces signal strength
- the base station automatically adjusts transmit power to the minimum required.

Maximum EME levels are estimated in 360° circular bands out to 500m from the base station.

These levels are cumulative and take into account emissions from all wireless base station antennas at this site.

The EME levels are presented in three different units:

- volts per metre (V/m) – the electric field component of the RF wave
- milliwatts per square metre (mW/m²) – the power density (or rate of flow of RF energy per unit area)
- percentage (%) of the ARPANSA Standard public exposure limit (the public exposure limit = 100%).

Results

The maximum EME level calculated for the proposed systems at this site is 3.19 V/m; equivalent to 27.045 mW/m² or 0.5% of the public exposure limit.

Radio Systems at the Site

There are currently no existing radio systems for this site.

It is proposed that this base station will have equipment for transmitting the following services:

Carrier	Radio Systems
Vodafone	WCDMA900 (proposed), LTE850 (proposed), WCDMA2100 (proposed), LTE1800 (proposed)

Calculated EME Levels

This table provides calculations of RF EME at different distances from the base station for emissions from existing equipment alone and for emissions from existing equipment and proposed equipment combined.

Distance from the antennas at 25 Harper Road in 360° circular bands	Maximum Cumulative EME Level at 1.5m above ground – all carriers at this site					
	Existing Equipment			Proposed Equipment		
	Electric Field V/m	Power Density mW/m ²	% ARPANSA exposure limits	Electric Field V/m	Power Density mW/m ²	% ARPANSA exposure limits
0m to 50m				3.083	25.21	0.49%
50m to 100m				2.078	11.45	0.22%
100m to 200m				2.84	21.33	0.41%
200m to 300m				3.19	27.045	0.5%
300m to 400m				2.91	22.42	0.4%
400m to 500m				2.24	13.25	0.23%
Maximum EME level				3.19	27.045	0.5
	249.18 m from the antennas at 25 Harper Road					

Calculated EME levels at other areas of interest

This table contains calculations of the maximum EME levels at selected areas of interest that have been identified through the consultation requirements of the Communications Alliance Ltd Deployment Code C564:2011 or via any other means. The calculations are performed over the indicated height range and include all existing and any proposed radio systems for this site.

Additional Locations	Height / Scan relative to location ground level	Maximum Cumulative EME Level All Carriers at this site Existing and Proposed Equipment		
		Electric Field V/m	Power Density mW/m ²	% of ARPANSA exposure limits
1 No locations identified				

RF EME Exposure Standard

The calculated EME levels in this report have been expressed as percentages of the ARPANSA RF Standard and this table shows the actual RF EME limits used for the frequency bands available. At frequencies below 2000 MHz the limits vary across the band and the limit has been determined at the Assessment Frequency indicated. The four exposure limit figures quoted are equivalent values expressed in different units – volts per metre (V/m), watts per square metre (W/m²), microwatts per square centimetre (μW/cm²) and milliwatts per square metre (mW/m²). Note: 1 W/m² = 100 μW/cm² = 1000 mW/m².

Radio Systems	Frequency Band	Assessment Frequency	ARPANSA Exposure Limit (100% of Standard)
LTE 700	758 – 803 MHz	750 MHz	37.6 V/m = 3.75 W/m ² = 375 μW/cm ² = 3750 mW/m ²
WCDMA850	870 – 890 MHz	900 MHz	41.1 V/m = 4.50 W/m ² = 450 μW/cm ² = 4500 mW/m ²
GSM900, LTE900, WCDMA900	935 – 960 MHz	900 MHz	41.1 V/m = 4.50 W/m ² = 450 μW/cm ² = 4500 mW/m ²
GSM1800, LTE1800	1805 – 1880 MHz	1800 MHz	58.1 V/m = 9.00 W/m ² = 900 μW/cm ² = 9000 mW/m ²
LTE2100, WCDMA2100	2110 – 2170 MHz	2100 MHz	61.4 V/m = 10.00 W/m ² = 1000 μW/cm ² = 10000 mW/m ²
LTE2300	2302 – 2400 MHz	2300 MHz	61.4 V/m = 10.00 W/m ² = 1000 μW/cm ² = 10000 mW/m ²
LTE2600	2620 – 2690 MHz	2600 MHz	61.4 V/m = 10.00 W/m ² = 1000 μW/cm ² = 10000 mW/m ²
LTE3500	3425 – 3575 MHz	3500 MHz	61.4 V/m = 10.00 W/m ² = 1000 μW/cm ² = 10000 mW/m ²

Further Information

The Australian Radiation Protection and Nuclear Safety Agency (ARPANSA) is a Federal Government agency incorporated under the Health and Ageing portfolio. ARPANSA is charged with responsibility for protecting the health and safety of people, and the environment, from the harmful effects of radiation (ionising and non-ionising).

Information about RF EME can be accessed at the ARPANSA website, <http://www.arpansa.gov.au>, including:

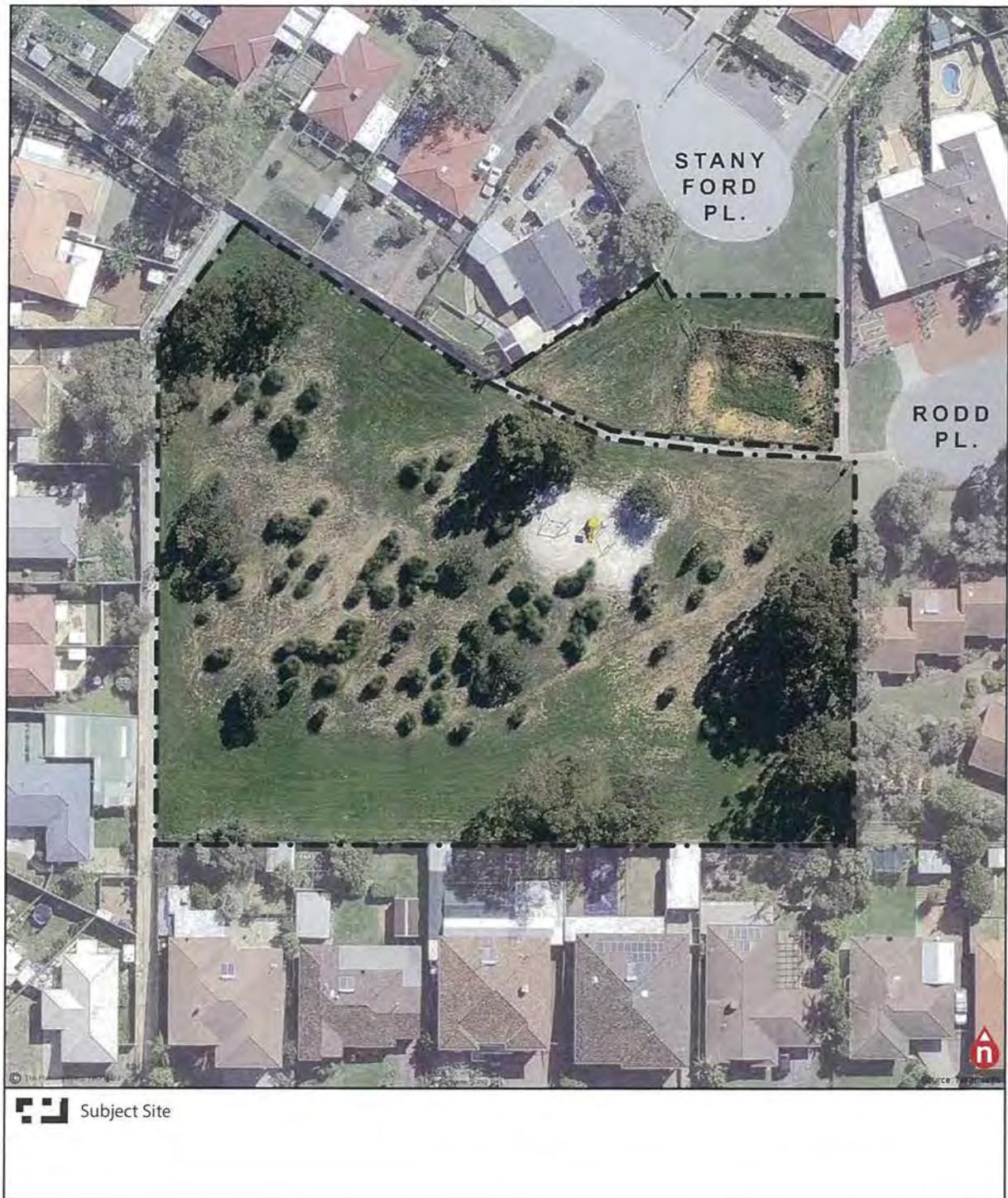
- Further explanation of this report in the document "Understanding the ARPANSA Environmental EME Report"
- The procedure used for the calculations in this report is documented in the ARPANSA Technical Report; "Radio Frequency EME Exposure Levels - Prediction Methodologies"
- the current RF EME exposure standard
Australian Radiation Protection and Nuclear Safety Agency (ARPANSA), 2002, 'Radiation Protection Standard: Maximum Exposure Levels to Radiofrequency Fields — 3 kHz to 300 GHz', Radiation Protection Series Publication No. 3, ARPANSA, Yallambie Australia.
[Printed version: ISBN 0-642-79400-6 ISSN 1445-9760] [Web version: ISBN 0-642-79402-2 ISSN 1445-9760]

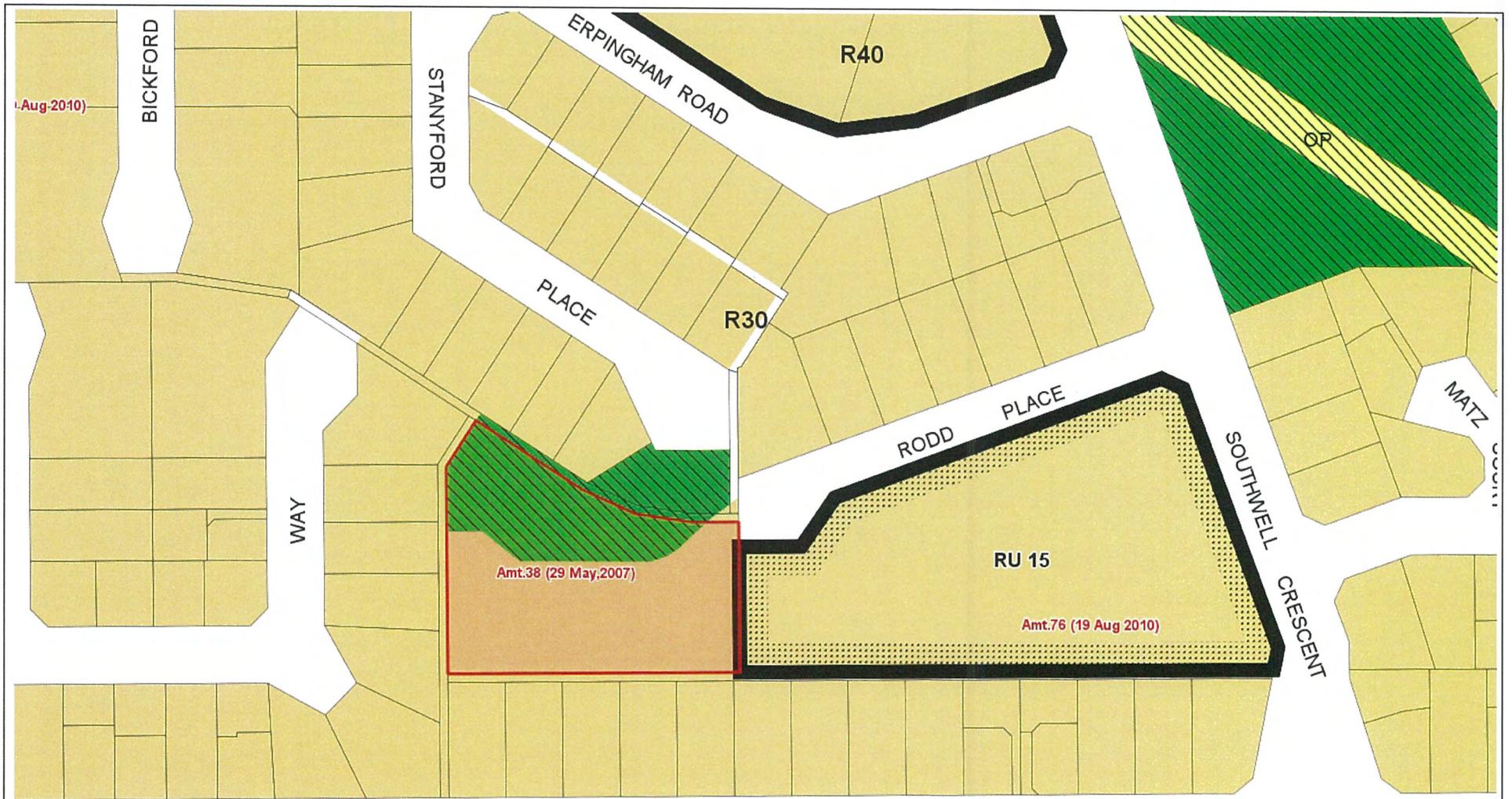
The Australian Communications and Media Authority (ACMA) is responsible for the regulation of broadcasting, radiocommunications, telecommunications and online content. Information on EME is available at <http://emr.acma.gov.au>

The Communications Alliance Ltd Industry Code C564:2011 'Mobile Phone Base Station Deployment' is available from the Communications Alliance Ltd website, <http://commsalliance.com.au>.

Contact details for the Carriers (mobile phone companies) present at this site and the most recent version of this document are available online at the Radio Frequency National Site Archive, <http://www.rfnsa.com.au>.

Figure 3 - Aerial Plan





PHOENIX



City of Cockburn
G.I.S Services Department

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Attach 2



16 Rodd Place, Hamilton Hill

SCHEME AMENDMENT REQUEST

April 2017
715-187A

perth
sydney

Document Control

Document ID: PG 2015/715-187 Hamilton Hill, 16 Rodd Place-DA/7 Final Documents/1 Lodged/Scheme Amendment/715-187 Rodd Place Hamilton Hill 2017.04.21.indd						
Issue	Date	Status	Prepared by		Approved by	
			Name	Initials	Name	Initials
1	13.04.17	Draft	Eleanor Richards		David Read	
2	21.04.17	Final	Eleanor Richards		David Read	
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Introduction

TPG+Place Match (TPG) has prepared this report, on behalf of Rodd Place Development Pty Ltd, in support of a request to amend the City of Cockburn Local Planning Scheme No. 3 (LPS3 or the Scheme), to increase the density coding of the subject site from R30 to R40, to facilitate the development of a multiple dwelling development.

The subject site is currently zoned 'Residential' with an R30 density code pursuant to the City of Cockburn (the City) Local Planning Scheme No. 3 (LPS3 or the Scheme). This scheme amendment seeks to take advantage of the site's location abutting a future public park, by increasing the residential density coding of the subject site to R40 to facilitate the development of a multiple (rather than grouped) dwelling proposal, and provide an appropriate transition between the higher density coded Bethanie Illawong Retirement Village to the east and the lower coded areas to the west.

The proposed scheme amendment is considered to be highly consistent with the intent and objectives of the City's Scheme, which advocates for a variety of housing typologies, as well as the City's strategic plan for the Hamilton Hill area, Directions 2031 and the latest strategic direction of the State Government as outlined within the draft *Perth and Peel @ 3.5 million* document.

This report outlines the relevant planning and site considerations for the proposed amendment, which has been prepared in consideration of LPS3 and the State and local strategic and statutory planning framework applicable to the site.

Background

The subject site was formally owned by the City of Cockburn, and has historically been used as an informal park, though the southern portion of the site has always been zoned for residential purposes.

In 2014, the site was sold to Rodd Place Development Pty Ltd on the understanding that the southern portion

of the site would be developed for an architecturally designed, three storey multiple dwelling development comprising 47 dwellings, and the northern portion of the site be redeveloped for an ungraded public park which would be transferred back to the City to ensure its ongoing use and enjoyment by the local public.

A development application for this proposal was subsequently lodged with the City of Cockburn, and approved at the end of 2014.

At the time, *State Planning Policy 3.1 – Residential Design Codes* (the R-Codes) required the 'density' of development in R30 coded areas be assessed under the 'plot ratio' controls specified in Part 6 of the R-Codes, allowing for a plot ratio of 0.5:1 on the subject site. As the development proposed a plot ratio of 0.44:1, the proposal was considered compliant in this respect.

Since the approval was issued, and despite objections received from various local governments, including the City of Cockburn, the Western Australian Planning Commission (the WAPC) has amended the R-Codes, to require development within areas coded less than R40 (i.e. including the subject site) be assessed under Part 5 of the R-Codes rather than Part 6. Part 5 contains 'minimum site area per dwelling' requirements which are not able to be varied, and would limit the number of dwellings that could be approved on this site to around 25, significantly less than the existing approval.

It is understood that the City of Cockburn was not supportive of the WAPC's proposed amendments due to concerns that it would decrease the diversity and quality of dwellings within the City of Cockburn.

It is important to note that the maximum number of dwellings is unchanged whether or not the site is developed for a 'grouped' or 'multiple' dwelling typology, despite the latter typology typically being significantly smaller in terms of the average floor area per dwelling compared to grouped dwellings. It is likely that if the site were developed under the current R30

coding, a 'grouped' dwelling typology (already common in this area) would be the most feasible option, though it would not be the best outcome in terms of providing surveillance over the adjoining Park, appropriate setbacks to neighbouring lots or provide the dwelling diversity advocated by the City.

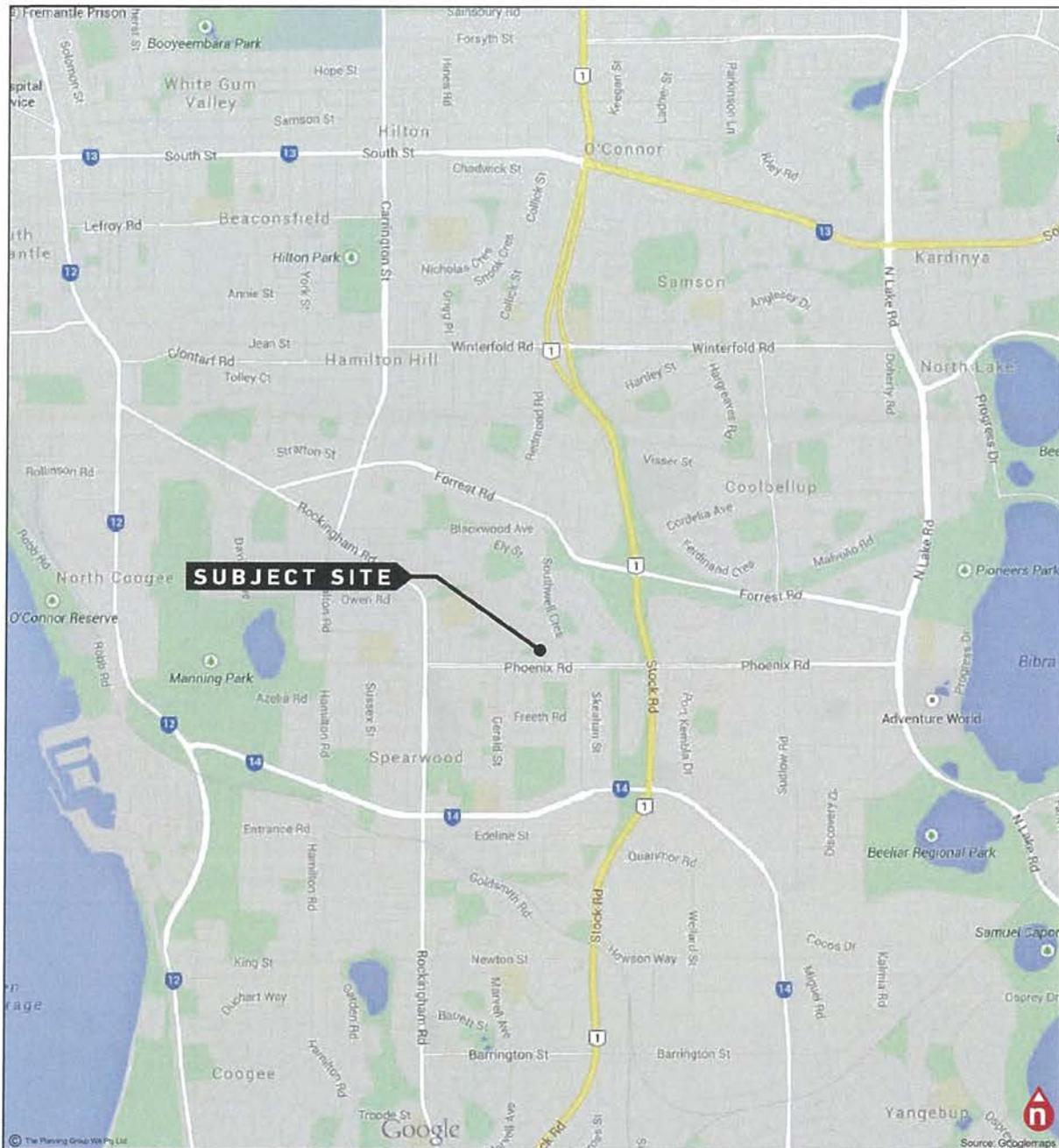
Since the approval has issued, Rodd Place Development Pty Ltd have been working with the City's staff to clear the conditions of approval, including conditions 4 and 10, which required the developer to liaise with the City's Engineering staff to design the public open space area including the required swales, prior to the issuing of a Building Permit. Despite continued liaison between the various parties with respect to these matters, the calculation of the required size and capacity of the swales and the refinement of this element of the design has been more complicated than originally anticipated, and as a result, these conditions have not yet been able to be formally 'cleared'. In addition, the 'market' for apartments in Perth has dropped slightly, resulting in a slower 'presale' process than originally anticipated.

As a result of these issues, the two year 'default' approval timeframe for the development application has now passed and, as the development has not been 'substantially commenced' within that timeframe, the development application has expired.

Advice from the City indicates that based on the recent amendments to the R-Codes, the existing approval for a multiple dwelling development is not able to be extended until the site's density coding under the Scheme is amended. This scheme amendment is therefore sought to allow for the site to be developed for the previously approved, high quality 'multiple dwelling' outcome, which will significantly enhance the open space on the northern portion of the site, deliver a bona fide 'public park', provide passive surveillance over that park, and provide an appropriate transition between the higher coded Bethanie Illawong Retirement Village to the east and the lower coded areas to the west, while also helping to deliver a diverse range of dwellings advocated by the City of Cockburn.

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Figure 1 - Location Plan



Subject Site

Property Description

No. 16 (Lot 432) Rodd Place, Hamilton Hill is located within the municipality of the City of Cockburn, within close proximity to the Phoenix Shopping Centre and the Southwell Local Centre. The subject site is 7,639m² in area and bound on all sides, except the east (and portions of the north) by a series of Public Access Way's (PAW).

The surrounding area is dominated by single or two storey houses, with the exception of the Bethanie II lawong Retirement Village which abuts the site's eastern boundary and has a split density code of R35/80. While the current development on this property comprises a series of single storey units it has the potential to be developed for a significantly greater density.

Refer to Figure 1 - Location Plan

Refer to Figure 2 - Site

Refer to Figure 3 - Aerial Plan

The subject site is currently used as an informal park, though only the northern portion is zoned for recreation purposes. The only improvement on the site is a small sand pit and play area located towards the centre.

To the west and north, a paved path has been constructed in the PAWs. It is understood the PAW along the southern edge of the site has recently been offered to the adjoining owners for sale, many of whom have expressed an interest in purchasing a portion. This PAW is therefore likely to soon to be extinguished, though the others will remain in place to ensure ongoing access to the park.

The particulars of the Certificate of Title are detailed in Table 1.

Table 1 - Certificate of Title Details

Lot	Plan/ Diagram	Area	Vol/Fol	Registered Proprietor
432	D5/125	7,639m ²	1596/853	Rodd Place Development Pty Ltd

Refer to Appendix A - Certificate of Title

Context

The subject site is located approximately 17 kilometres south west of the Perth CBD and six kilometres south east of Fremantle. Access to the surrounding road network is provided by Phoenix Road to the south of the site and Stock Road. The surrounding area is predominately characterised by a mix of low and medium density single residential developments. The Bibra Lake industrial/commercial area is less than one kilometre east of the site on the other side of Stock Road.

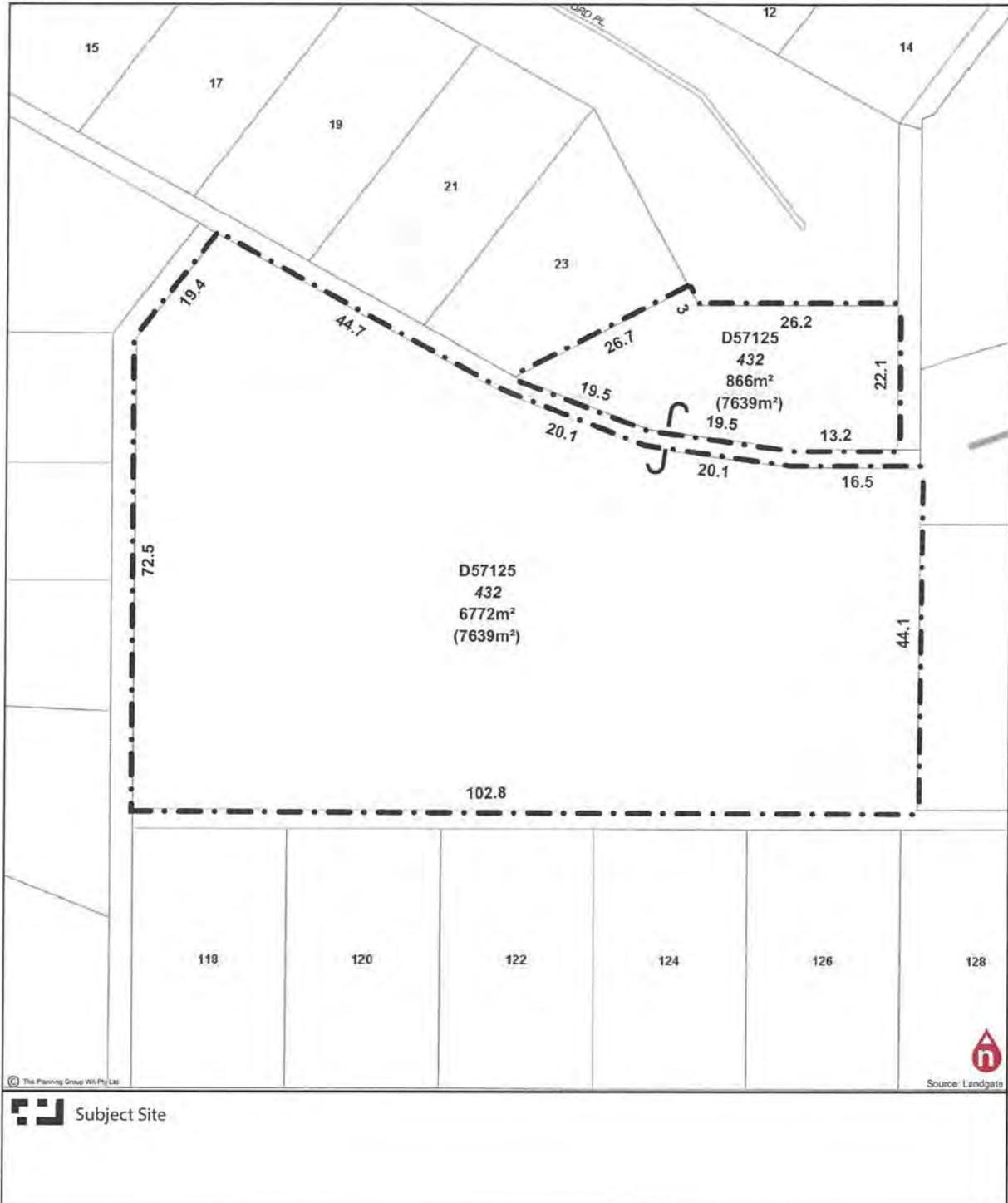
There are several schools in proximity including Southwell, Phoenix and Spearwood Primary Schools, which are all within 800 metres. The Phoenix Shopping Centre is located around 500 metres south west of the site and includes both Coles and Woolworths supermarkets and many other specialty stores. The Southwell Local Centre is also located with 400 metres of the subject site.

The site has relatively good access to public transport with buses running along Southwell Crescent at the other end of Rodd Place and others along Phoenix Road. These buses run between Fremantle and Cockburn Central Train Station and Murdoch University and Train Station, where trains link to the CBD and Mandurah and run regularly.

In terms of access to open space and recreation areas there are several local parks in the area. Manning Park, a large area which incorporates natural bush land, a lake, paths and play equipment is also located only two kilometres to the west of the site. Beyond the park, CY O'Connor reserve and North Coogee beach provide more passive recreation opportunities. 2.5 kilometres to the east of the site, close to Bibra Lake is Adventure World which is open in the warmer months.

Refer to Figure 4 - Bus and Cycle Routes

Figure 2 – Site

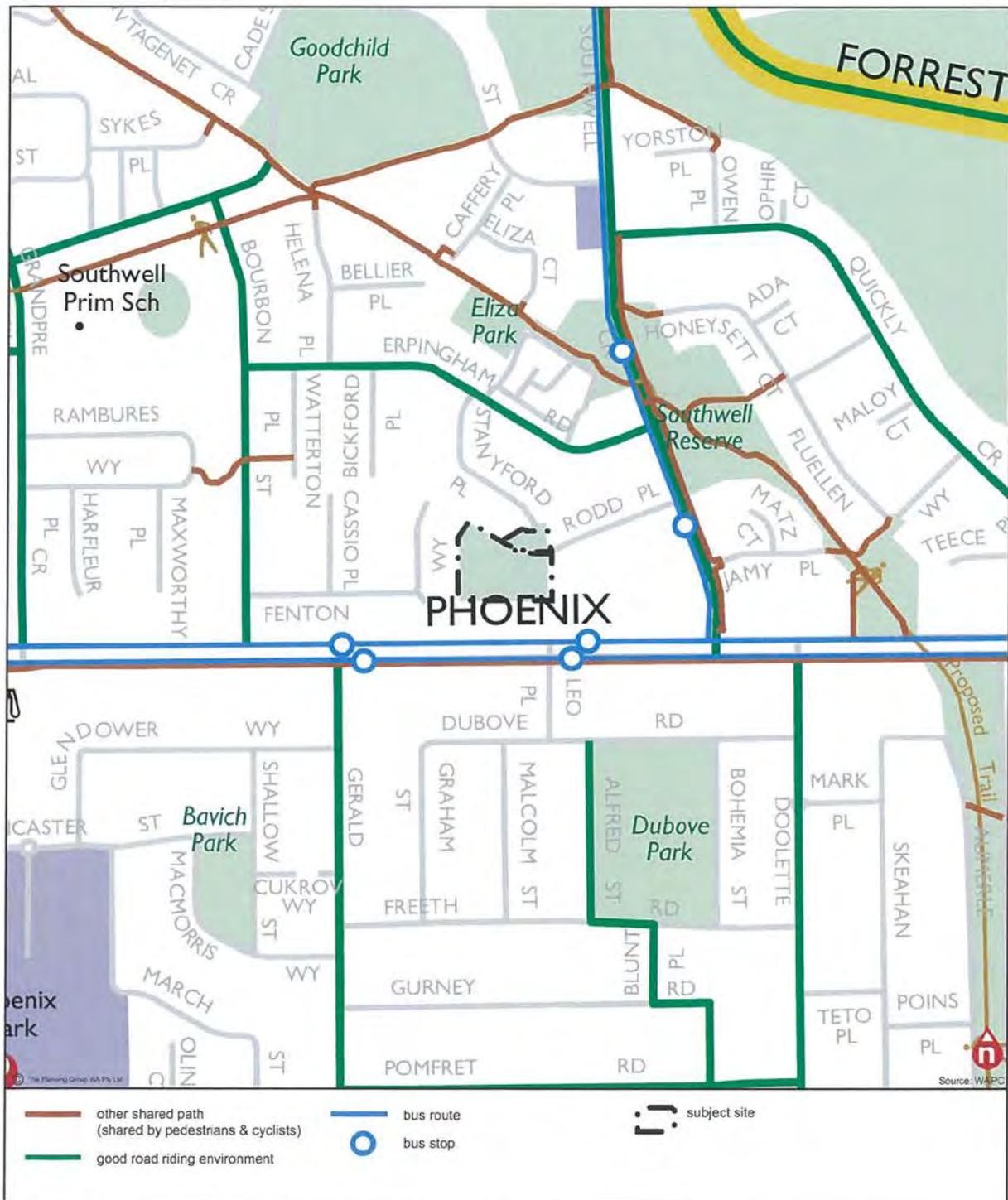


 Subject Site

Figure 3 - Aerial Plan



Figure 4 - Bus and Cycle Routes

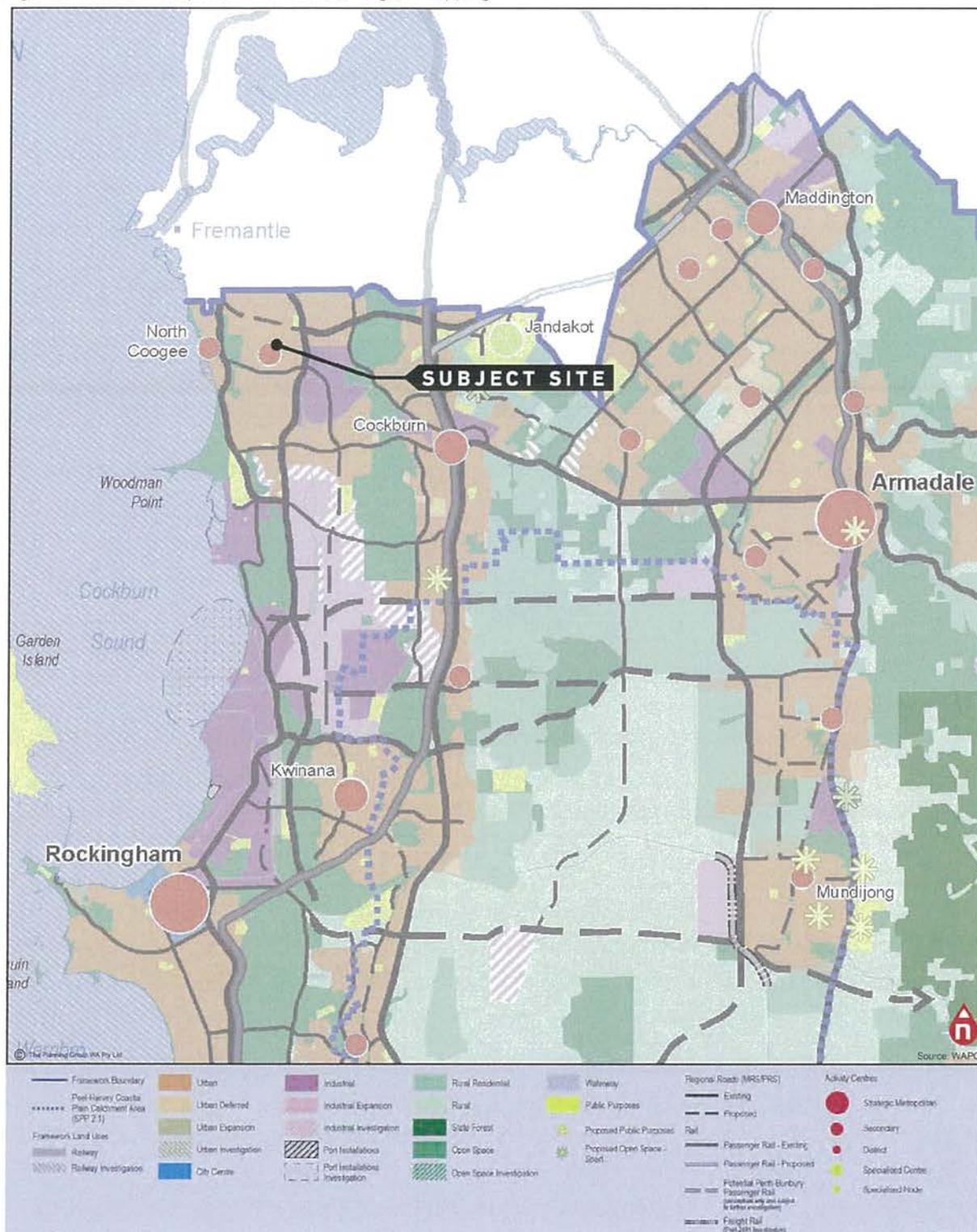


Acid Sulphate

Acid sulphate soils (ASS) are naturally occurring soils and sediments containing sulphide minerals. In an undisturbed state below the water table, these soils are not acidic, however, if the soils are drained, excavated or exposed, the sulphides will react with oxygen to form sulphuric acid.

A desktop search of Landgate's Shared Land Information Platform (SLIP) indicates that the site has a low risk of ASS occurring beyond 3m of natural ground level. It is not envisaged that acid sulphate soils will be disturbed by excavation works required as part of the future proposed development on the site.

Figure 5 - 'South Metropolitan and Peel Sub-Region' Mapping



Planning Framework

Strategic Planning Documents

Directions 2031 and Beyond

The Western Australian Planning Commission (WAPC)'s Directions 2031 and Beyond (Directions 2031) document is a spatial framework; a high level strategic plan that establishes a vision for the future growth of the Perth and Peel metropolitan region. Directions 2031 provides a framework to guide the detailed planning and delivery of housing, infrastructure and services necessary to accommodate growth.

Directions 2031 establishes a vision for the future growth of the Perth and Peel region:

"By 2031, Perth and Peel people will have created a world-class liveable city: green, vibrant, more compact and accessible with a unique sense of place."

The above vision is supported by a number of key objectives. The objectives relevant to this scheme amendment request are as follows:

- *Sustainable: We should grow within the constraints placed on us by the environment we live in; and*
- *Responsible: We have a responsibility to manage urban growth and make the most efficient use of available land and infrastructure.*

A primary strategy underpinning the document is establishing targets to improve upon current infill development trends to assist in accommodating the rapidly growing population of the Perth and Peel region. Specifically, Directions 2031 sets a target of 47% or 154,000, of the required 328,000 dwellings, being provided as infill development as a way of managing growth. The subject site is identified by Directions 2031 as being within the 'South West Sub-Region' in an existing urban area, which Directions 2031 indicates should be developed for targeted for infill development.

This scheme amendment application directly responds to these aspirations by facilitating the development of a high quality, infill proposal in an established suburb within close proximity to a local and district centre, high quality public open space and public transport.

Draft Perth and Peel @ 3.5 Million

The Draft Perth and Peel @ 3.5 Million framework is an overarching suite of documents which builds on the vision established under Directions 2031. It aims to achieve a more consolidated urban form to meet long-term housing needs and strengthen key activity centres and employment nodes as the Perth and Peel population grows to 3.5 million. The framework guides infill development to increase residential housing choice and to reduce the need for people to commute long distances for work.

Similar to Directions 2031, the subject site is located within the 'South Metropolitan and Peel Sub-Region', which is identified as having the following priorities considered relevant to the proposed development:

- *Develop a consolidated urban form that limits the identification of new green field areas to where they provide a logical extension to the urban form, and that places a greater emphasis on urban infill and increased residential density; and*
- *Maximise the use of existing infrastructure, including transport, community/social and service, where there is a concentration of urban and employment opportunities.*

Refer to Figure 5 – 'South Metropolitan and Peel Sub-Region' Mapping

The 'South Metropolitan and Peel Sub-Regional Planning Framework' expects the sub-region to experience relatively strong population growth through to 2050, influenced by, among other things, the provision of

relatively affordable housing as well as employment and lifestyle opportunities. The sub-regional population is predicted to grow considerably by 2050 from 523,400 to 1,264,400 people. It is critical that planning for the predicted population growth focuses on making the most efficient use of transport networks, service infrastructure, employment and key community/social infrastructure facilities. A foundation of the proposed consolidated urban form is to concentrate the majority of the population within reasonable proximity of a wide range of services and activities.

The proposed scheme amendment is highly consistent with these objectives as it will facilitate urban infill development, providing an increasing density and promote more diversity of housing form in a locality within close proximity to a number of commercial and civic destinations, employment areas, areas serviced well by public transport and infrastructure and overlooking a public park.

State Planning Policy 4.2 – Activity Centres for Perth and Peel

State Planning Policy 4.2 – Activity Centres for Perth and Peel (SPP4.2) details broad planning requirements for the planning and development of new activity centres and the redevelopment and renewal of existing centres in Perth and Peel. It is mainly concerned with the distribution, function, broad land use and urban design criteria of activity centres, and with coordinating their land use and infrastructure planning.

SPP4.2 also reflects the WAPC's intention to encourage and consolidate residential and commercial development in activity centres so that they contribute to a balanced network.

SPP4.2 identifies Phoenix Shopping Centre (formerly Spearwood) as a 'District Centre' within the Activity Centre Hierarchy. District Centres have a walkable catchment of 400 metres and provide a range of daily and weekly shopping services. The subject site is located just beyond the catchment area (520 metres).

While not dealt with directly by SPP4.2 the subject site is also located within close proximity to the Southwell Local Centre (further discussed below). Local Centres provide lower order services to the immediate surrounding residents and are complementary to larger,

higher order Centres. As shown in Figure 6 below, the subject site is located right between both centres and in close proximity to high frequency public transport with buses running along Phoenix Road and Southwell Crescent, the subject site is considered to meet the objectives of SPP4.2 and is an appropriate location for increased density development.

City of Cockburn Local Commercial and Activity Centres

The City of Cockburn's Local Commercial and Activity Centres Strategy (LCACS) is an important planning document for implementing the new direction for the planning of activity centres in Perth and Peel set by the refreshed policy context outlined in Directions 2031 and beyond and State Planning Policy No. 4.2 – Activity Centres for Perth and Peel.

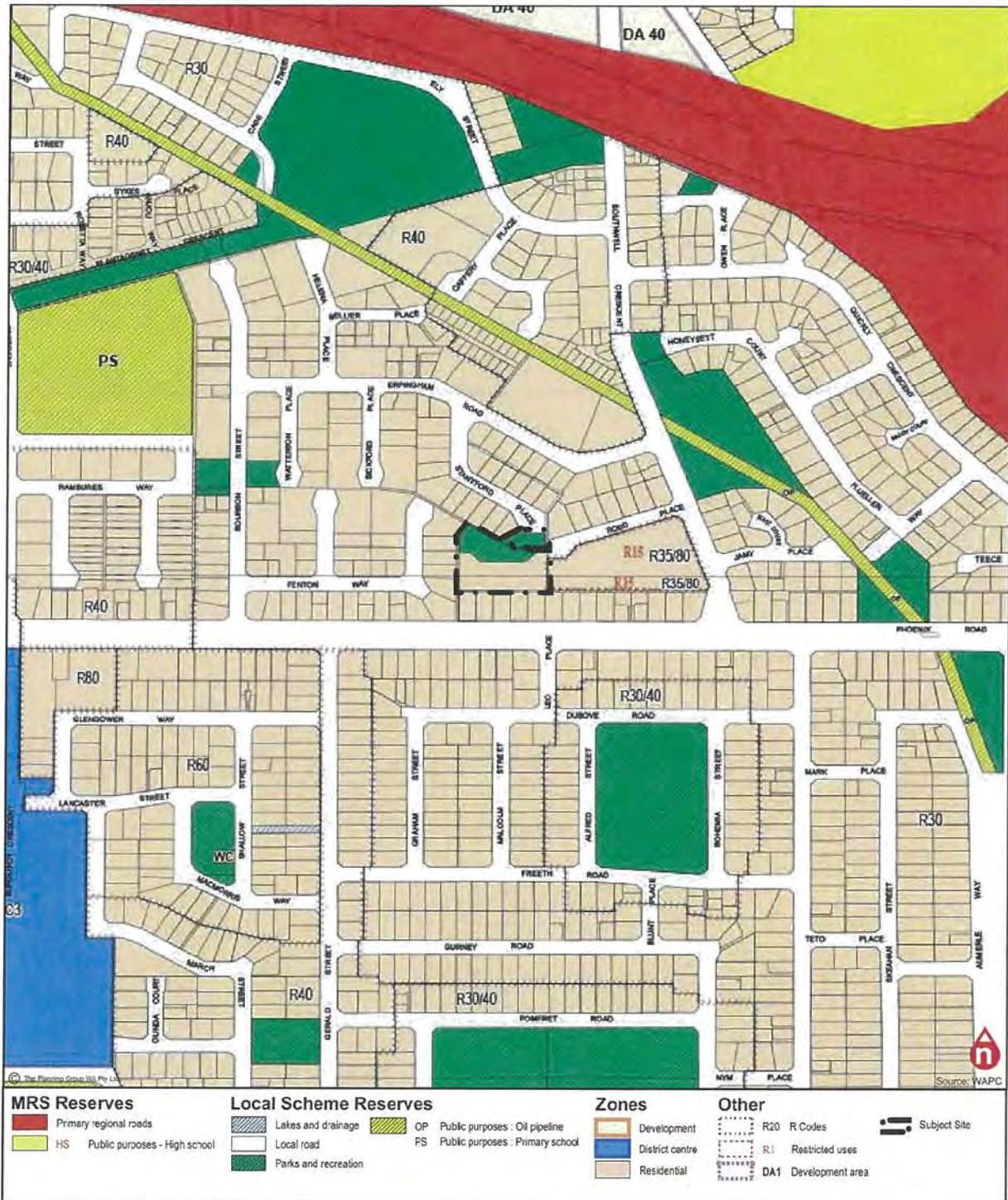
Pursuant to the LCACS the subject site located between the Phoenix District Centre and the Southwell Local Centre. The LCACS identified that the Phoenix District Centre currently has a shortfall of 802 dwellings when assessed against the desired dwelling targets of the LCACS. The LCACS does not provide details on desired dwelling targets for centres lower than District in the Activity Centres hierarchy. As such, no current housing numbers or infill targets are provided for the Southwell Local Centre.

The proposed scheme amendment affords an opportunity to provide an increase in residential density at a site strategically located between two complementing centres, which will increase the service base for both centres and assist with realising the resident and housing infill targets for the Phoenix District Centre as detailed in the LCACS.

Phoenix Central Revitalisation Strategy and Activity Plan

The Phoenix Central Revitalisation Strategy and Activity Plan (PCRS) was adopted by the City in 2009 with the aim of providing a strategic framework for improvements to the Phoenix town centre and parts of the suburbs of Spearwood and Hamilton Hill for the next ten years. The study area covers an approximately 800m walkable catchment from the Phoenix town centre, which the subject site is located within.

Figure 7 – Local Planning Scheme No. 3 Map Extract



The PCRS recommended increasing residential densities from R20 to R40 in the 400m walkable catchment of the Phoenix town centre and around local centres, parks and along major public transport routes to facilitate a greater range of housing options within a close proximity to commercial and civic destinations.

Refer to Figure 6 - Phoenix Revitalisation Strategy

The PCRS recommended increasing the residential density of the subject site to R30. At the time of the PCRS's adoption, the R30 density coding was an appropriate density for multiple dwellings. The proposed scheme amendment to recode the subject site to R40 is therefore considered to be very well aligned with the PCRS, as it will allow for multiple dwellings to be developed at the subject site which responds appropriately to the overarching objective of the Strategy by providing increased density around high quality POS and within close proximity to the Southwell Local Centre and Phoenix District Centre.

Statutory Planning Framework

City of Cockburn Local Planning Scheme No. 3

The City of Cockburn Local Planning Scheme No. 3 (LPS3 or the Scheme) was gazetted in 2002 and is a statutory Scheme that provides guidance for the development and use of land and buildings. The general aims of the Scheme are to:

- a) *ensure that development and the use of land within the district complies with accepted standards and practices for public amenity and convenience;*
- b) *ensure that the future development and use of land within the district occurs in an orderly and proper way so that the quality of life enjoyed by its inhabitants is not jeopardised by poor planning, unacceptable development and the incompatible use of land.*

Under LPS3, the northern portion of the site is reserved 'Parks & Recreation' and the southern portion zoned 'Residential R30'. The land to the west of the site is predominantly coded R30, with multiple pockets of higher coded areas around local parks (such as the properties around Dubove Park to the south of the site), commercial centres and the like. To the east of the site

is the Bethanie Illawong Retirement Village, which has a split density coding of R30/80, and is therefore likely to be re-developed in the future for a significantly greater density of development than the current single storey units.

Refer to Figure 7 - Local Planning Scheme No. 3 Map Extract

The objective of the residential zone is to "...provide for residential development at a range of densities with a variety of housing to meet the needs of different household types through the application of the Residential Design Codes."

Table 1 of LPS3 details the permissibility of land uses within the City. Pursuant to Table 1 of LPS3, 'Multiple Dwellings' are deemed a 'Discretionary' ('D') use within the Residential zone. A discretionary use means "that the use is not permitted unless the local government has exercised its discretion by granting planning approval". Note 3 of Clause 3.3 of LPS3 states "in considering a 'D' use, the local government will have regard to the matters set out in clause 67 of the Deemed Provisions". As such, the amendment will not affect the City's ability to have discretion over any future development applications for the subject site.

The proposed scheme amendment responds to the aims and objectives of the Scheme and the Residential zone by:

- Facilitating the development of a slightly higher density multiple dwelling development which will provide an appropriate interface between future higher density development on the Bethanie Illawong Retirement Village to the east and lower coded areas to the west;
- Adding to the diversity of dwelling typologies in the area and providing a development offering a variety of housing options in one, two and three bedroom units in a locality close to a local park, the Southwell Local Centre, the Phoenix Shopping Centre, and a number of employment, civic and recreational destinations; and
- Facilitating a high quality development which will deliver a quality public park and improve the amenity and safety of that space by providing passive surveillance in the form of balconies and major openings overlooking and addressing it.

Planning and Development (Local Planning Scheme) Regulations 2015

In August 2015, the Planning and Development (Local Planning Scheme) Regulations 2015 were gazetted. In addition to other matters, the Regulations introduce new requirements to standardise the processing of scheme amendments throughout the State. The Regulations divide scheme amendments into 'simple', 'standard' and 'complex' amendments, which each have different processing requirements and timeframes. The City have advised this amendment would be assessed as a 'standard' amendment.

density residential developments in the City. The Policy provides a number of provisions relating to; sustainable building design, design of outdoor living areas and façade treatments. Clause 15 of the Policy provides the specific requirements for split coded R30/40 lots fronting POS in order to develop to the higher density, which generally require higher density developments to provide a high standard of amenity for residents, provide passive surveillance over adjacent streets and POS, and other related provisions. It is noted that the previously approved development, which the proposed amendment aims to facilitate, is highly consistent with these objectives.

Residential Design Codes

The Residential Design Codes of Western Australia (R-Codes) provide a statutory framework for the development of residential dwellings across the State. The R-Codes provide provisions in relation to different types of residential development including controls over minimum lot size, building size and height, setbacks, open space, streetscape, outdoor living areas, landscaping, parking, access, privacy, solar access and the like.

In terms of the density of development permitted on site, the R-Codes require that single, grouped dwellings and multiple dwellings in areas coded R30 be controlled by minimum site area per dwelling requirements (which cannot be varied), whereas multiple dwellings in areas coded R40 or higher are controlled via plot ratio (which is able to be varied). The current R30 density requirements limit the development opportunities of the subject site to no more than 25 dwellings (potentially less depending on the configuration). As the development of 25 multiple dwellings (which are typically smaller in floor area than grouped dwellings of single houses) would not be feasible, the only likely development outcome for the site under the R30 coding would be for single or grouped housing, which we believe would be a lost opportunity on a site such as this.

Local Planning Policy 1.2 – Residential Design Guidelines

Local Planning Policy 1.2 – Residential Design Guidelines (LPP1.2 or the Policy) was prepared by the City to assist in improving the design quality of medium and high

Proposal

This application seeks support to amend the City of Cockburn Local Planning Scheme No. 3 to increase the density coding of the southern portion of No. 16 (Lot 432) Rodd Place, Hamilton Hill from R30 to R40. The proposed rezoning would facilitate the development of the site for a high quality multiple dwelling development overlooking a park and in particular allow our Client to implement the development application already approved by the City in 2014.

Discussions with City's planning officers has identified the proposed scheme amendment would be considered a 'Standard scheme amendment' pursuant to the Regulations 2015.

Table 2 details the difference in site requirements between the current R30 density coding and the proposed R40 density coding.

The table below demonstrates the scheme amendment will result in minor changes to the site development requirements for the subject site apart from the ability for multiple dwellings to be assessed against the plot ratio controls rather than the 'average site area'.

As mentioned previously, the scheme amendment will not limit or reduce the City's control over residential development at the subject site, instead it provides an opportunity to consider a greater range of housing options at the subject which responds to the growing diversity in housing options sought by home buyers in the area.

Table 2 - Residential Design Codes Requirements for R30 and R40

Density Coding	Dwelling Type	Minimum Site Area	Average Site Area	Maximum Plot Ratio	Minimum Open Space	Minimum Primary Street Setback	Side and Rear Setbacks
R30	Single/Grouped Dwelling	260m ²	300m ²	N/A	45% of site area.	4 metres	Based on the height and length of the wall.
	Multiple Dwelling	N/A	300m ²	N/A			
R40	Single/Grouped Dwelling	180m ²	220m ²	N/A	45% of site area.	4 metres	Unchanged from R30 coding.
	Multiple Dwelling	N/A	N/A	0.6:1			

Future Development

Under the current R30 density coding, the subject site is only able to be feasibly developed for single or grouped dwellings which, when considering the context of the subject site, is not considered to be its highest or best use. The scheme amendment presents an opportunity for the subject site to be developed for multiple dwellings which appropriately responds to the objectives of the State's strategic planning framework and the objective of the Residential zone pursuant to LPS3.

As mentioned previously in this report, it is the intention of the landowner to seek an extension of approval for a previously approved development application once the scheme amendment is gazetted. The previously approved development is a quality, architecturally designed residential development comprising 47 one and two bedroom multiple dwellings which overlooks the upgraded public park. The proposed development is three storeys in height. High quality landscaping will also be provided both within the public park and throughout the development, including within the private courtyards. Existing trees in the park will be retained wherever possible, as will the large tree on the southern boundary of the site.

The development was designed to address and overlook the park which comprises the northern portion of the site that will be enhanced and upgraded by the developer as part of the development works. In doing so it is anticipated that the proposed development will decrease the amount of antisocial behaviour which takes place in the area by increasing opportunities for passive surveillance.



Expected Traffic Impact

The scheme amendment seeks to increase the residential density coding of 16 Rodd Place, Hamilton Hill from its existing R30 to an increased R40 coding to allow for a multiple dwelling development to be constructed on site.

Vehicle Access and Parking

Access Arrangements

Vehicle access to the site will be provided via a single crossover, on Rodd Place.

Rodd Place is considered an 'Access Road' as per the Main Roads Western Australia (MRWA) Road Information Mapping System and therefore has design capacity in the order of 3,000 vehicles per day (VPD).

The following section details the carrying capacity of the road when assessed against an R30 grouped dwelling development, R40 grouped dwelling development and the proposed R40 multiple dwelling development.

Expected Dwelling Yield

The assessment in Table 3 is based on the average lot size requirements of the R-Codes and the subject site total area of 7,639m².

Vehicle Parking

An assessment of the required parking against the relevant R-Codes requirements is presented in Table 4. It

should be noted the assessment of grouped dwellings is assuming all dwellings contain two or more bedrooms.

Anticipated Traffic Volumes and Hours of Operation

The NSW Roads and Traffic Authority's Guide to Traffic Generating Developments (which is used as a standard reference by traffic consultants in Perth) indicates that typical 'medium density residential flat buildings' generate vehicular movements in accordance with the following:

- Smaller Units and Flats (up to two bedrooms): 4-5 daily vehicle trips per dwelling, with 0.4-0.5 trips per dwelling occurring during peak periods.
- Larger Unit and Town Houses (three or more bedrooms): 5-6.5 daily vehicle trips per dwelling, with 0.5-0.65 trips per dwelling occurring during peak periods.

Table 5 details the expected vehicle trips per day (VTPD) and vehicle trips per hour (VPH) for a grouped dwelling development at the current R30 density coding, a grouped dwelling development at the proposed R40 density coding and the proposed multiple dwelling development.

As noted above, the MRWA Road hierarchy carrying capacity for Rodd Place is in the order of 3,000 vehicles per day. Based on the above figures, the additional traffic generated by the increased density would only

Table 3 - Expected Dwelling Yield

Density Coding	Dwelling Type	Minimum Site Area	Average Site Area	Maximum Expected Dwelling Yield (based on average lot size)
R30	Grouped/Multiple Dwelling	260m ²	300m ²	25 dwellings.
R40	Single/Grouped Dwelling	180m ²	220m ²	34 grouped dwellings.
	Proposed Multiple Dwelling	0.6:1	N/A	47 multiple dwellings.

Table 4 - State Planning Policy 3.1 – Residential Design Codes: Clause 6.3.3 Parking

Requirement	Required
Grouped Dwellings (R30)	
<p>Resident Parking</p> <p>Where the development is not located within 250m of a high frequency bus route, parking to be provided in accordance with the following:</p> <p>2 bedrooms or more: 2 parking bays required</p>	A total of 50 resident parking bays would be required for a grouped dwelling development containing 25 dwellings.
Grouped Dwelling (R40)	
<p>Resident Parking</p> <p>Where the development is not located within 250m of a high frequency bus route, parking to be provided in accordance with the following:</p> <p>2 bedrooms or more: 2 parking bays required</p>	A total of 68 resident parking bays would be required for a grouped dwelling development containing 34 dwellings.
Resident Parking (Multiple Dwelling)	
<p>Resident Parking</p> <p>Where the development is located within 250m of a high frequency bus route, parking to be provided in accordance with the following:</p> <p>Less than 110m² and/or 1 or 2 bedrooms: 1 bay per dwelling</p> <p>More than 110m² and/or 3 bedrooms plus: 1.25 bays per dwelling.</p>	A total of 47 resident parking bays would be required for a multiple dwelling development containing 47 one or two bedroom units of less than 110m ² .
Visitor Parking	
<p>Visitor Parking (Grouped Dwelling)</p> <p>Visitor parking is to be provided at a rate of one bay per four dwellings</p>	<p>A total of 5.25 (6) visitor parking bays would be required for a grouped dwelling development containing 34 dwellings.</p> <p>A total of 7.5 (8) visitor parking bays would be required for a grouped dwelling development containing 34 dwellings.</p>
<p>Visitor Parking (Grouped Dwelling)</p> <p>Visitor parking is to be provided at a rate of 0.25 bays per dwelling.</p>	A total of 10.75 (11) visitor parking bays would be required for a multiple dwelling development containing 47 units.

Table 5 - Expected Vehicle Trips

Density Coding	Dwelling Type	No. Dwellings	Min VTPD	Max VTPD	Min VTPH	Max VTPH
R30	Single/Grouped Dwelling (3 bedroom)	25	125	150	12.5	15
R40	Single/Grouped Dwelling (3 bedroom)	34	170	204	17	20.4
	Proposed Multiple Dwelling	47	188	235	18.8	23.5

be between 63-85 vtpd. Given the cul-de-sac design of Rodd Place and the relatively limited number of crossovers onto the road, it is expected that the existing traffic on the road would be significantly less than 3,000 vehicles per day, so expected daily trip generation by the proposed development of between 188 and 235 vtpd is not considered to have any material or adverse impact on the operation of Rodd Place.

promote alternative methods of transport for short journey trips.

Location of nearest bus stops, train stations and level of accessibility

The site is well located in terms of access major roads and bus routes, being approximately 100 metres from Phoenix Road and Southwell Crescent. Both roads service a number of bus routes connecting to train stations and the wider bus network (refer to Figure 4 published earlier in this report).

Pedestrian Access

There are existing pedestrian facilities running along the boundary of the subject site connecting the site to Phoenix Road, Fenton Way, Bickford Place and Rodd Place. Both Phoenix Road and Rodd Place provide access to bus services, with a high frequency service running along Phoenix Road connecting Phoenix Shopping Centre and Murdoch Train Station.

Cycle Access

The subject site has access to a number of high quality bicycle facilities (refer Figure 4). Department of Transport (DoT) mapping identifies Phoenix Road having a 'High Quality Shared Path' running along its entirety and connecting to wider shared path network. A number of the surrounding streets are also identified as being 'good on road cycling environments' including Eppingham Road and Bourbon Street. These roads also provide connection to a number of shared paths and 'hard shoulder on road cycling lanes'.

Overall, proposed development is considered to be relatively minor in scale and when assessed against expected group developments does not pose a significant increase in traffic. The above assessment has been conservative in its assessment of vehicle trips, with a high likelihood that the subject site's close proximity to activity centres and alternative transport options would

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Justification

The proposed amendment will allow for a previously approved and appropriate development to occur at the subject site and does not pose a risk to the City in their ability to have discretion over future development of the subject site. An R40 coding has been sought due to the relatively minor increase in density and its ability to facilitate the multiple dwelling development previously approved. The R40 coding will also provide an appropriate transition from the existing split coded R35/80 site to the east of the site and the existing R30 properties to the west.

Currently under the R30 density coding, the only likely feasible development option for the site would be for grouped dwellings. Depending on the configuration of these dwellings, setbacks of as little as one metre to neighbouring properties could be permitted under the R-Codes which while deemed to comply, would most likely have a detrimental impact on the existing residential properties that surround the site through the increase in building bulk and reduction in ventilation and access to sunlight to these existing properties.

A grouped dwelling outcome would most likely also configure in the rear of properties to face the existing POS. This would result in negative outcomes in terms of surveillance of the POS as no active frontage or interaction between the development and the POS would occur. The vertical nature of multiple dwelling developments means that greater setbacks can be provided to neighbouring development.

A multiple dwelling development, which would be facilitated by the proposed amendment is considered to be the best outcome of the site as it provides the opportunity for smaller dwellings stacked above one another development to address the public park, provide greater setbacks to adjoining properties (particularly the residential properties to the south), provides a better outcome in terms of privacy and overshadowing, as well as providing more open space and allowing for the retention of the mature trees on the site itself wherever possible.

Refer to Figure 8 – Grouped and Multiple Dwelling Development Comparison

The proposed amendment responds to the aims and objectives of the Scheme and the Residential zone and is considered entirely consistent with the orderly and proper planning for the site as follows:

- It will assist with realising a number of residential infill and increased density planning objectives as detailed within Directions 2031 and Beyond and draft Perth and Peel @ 3.5 Million which identifies the south metropolitan region as a key infill location for increased housing density;
- The proposed amendment is very well aligned with the general intent of the Phoenix Central Revitalisation Strategy and Activity Plan as it allows for multiple dwellings to be developed at the subject site which responds appropriately to the overarching objective of the strategy by providing increased density around high quality POS and within close proximity to the Southwell Local Centre and Phoenix District Centre;
- It will facilitate development which provides an appropriate transition down in density and interface between future higher density development on the Bethanie Illawong Retirement Village to the east, which has a split density code of R30/80, and lower coded areas to the west;
- It will facilitate the development of multiple dwellings at the subject site which adds to the diversity of dwelling typologies on a site directly adjacent a local park, as well as the Southwell Local Centre, Phoenix Shopping Centre and a number of employment, civic and recreational destinations; and
- It will facilitate the construction of a high quality development which will also deliver a quality public park for the use and enjoyment of all surrounding residents and improve the amenity and safety of that space by providing passive surveillance in the form of balconies and major openings overlooking and addressing it.

Figure 8 – Grouped and Multiple Dwelling Development Comparison



Grouped dwelling configuration.

- Reduced setbacks to neighbouring Lots.
- Greater overshadowing.
- Less open space.



Multiple dwelling configuration.

- Greater height.
- Greater setbacks to neighbouring Lots.
- Less overshadowing.
- More open space.

Conclusion

This request to amend the City of Cockburn Local Planning Scheme No. 3 by increasing the density from R30 to R40, has been prepared by TPG+Place Match on behalf of Rodd Place Development Pty Ltd. The proposal will facilitate the development of the previously approved multiple dwelling development on the site in close proximity to the Phoenix Shopping Centre, the Southwell Local Centre, multiple bus services and POS. This development will in turn provide direct community development by including the redevelopment of the public park.

The proposed scheme amendment is considered to be highly consistent with both the City's Local Planning Scheme and strategic plan for the Hamilton Hill area as well as Directions 2031 and the latest strategic direction of the State Government as outlined within the draft Perth and Peel @ 3.5 million document.

The proposed scheme amendment is therefore considered to be consistent with the orderly and proper planning of the area, ensuring that future development will not be out of place with the established amenity of the area and will enhance the amenity of the subject site including the future public park, and the surrounding area.

For these reasons, we respectfully request the City initiate the scheme amendment request.

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Appendix A

Certificate of Title

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WESTERN



AUSTRALIA

REGISTER NUMBER 432/D57125	
DUPLICATE EDITION 1	DATE DUPLICATE ISSUED 13/4/2015

RECORD OF CERTIFICATE OF TITLE
UNDER THE TRANSFER OF LAND ACT 1893

VOLUME **1596** FOLIO **853**

The person described in the first schedule is the registered proprietor of an estate in fee simple in the land described below subject to the reservations, conditions and depth limit contained in the original grant (if a grant issued) and to the limitations, interests, encumbrances and notifications shown in the second schedule.



REGISTRAR OF TITLES

LAND DESCRIPTION:

LOT 432 ON DIAGRAM 57125

REGISTERED PROPRIETOR:
(FIRST SCHEDULE)

RODD PLACE DEVELOPMENT PTY LTD OF SUITE 11, 1ST FLOOR, 40 ST QUENTIN AVENUE, CLAREMONT
(T M926845) REGISTERED 3 MARCH 2015

LIMITATIONS, INTERESTS, ENCUMBRANCES AND NOTIFICATIONS:
(SECOND SCHEDULE)

1. M926846 MORTGAGE TO SOVEREIGNTY MORTGAGE NO. 24 PTY LTD REGISTERED 3.3.2015.
2. *M926847 CAVEAT BY CITY OF COCKBURN LODGED 3.3.2015.

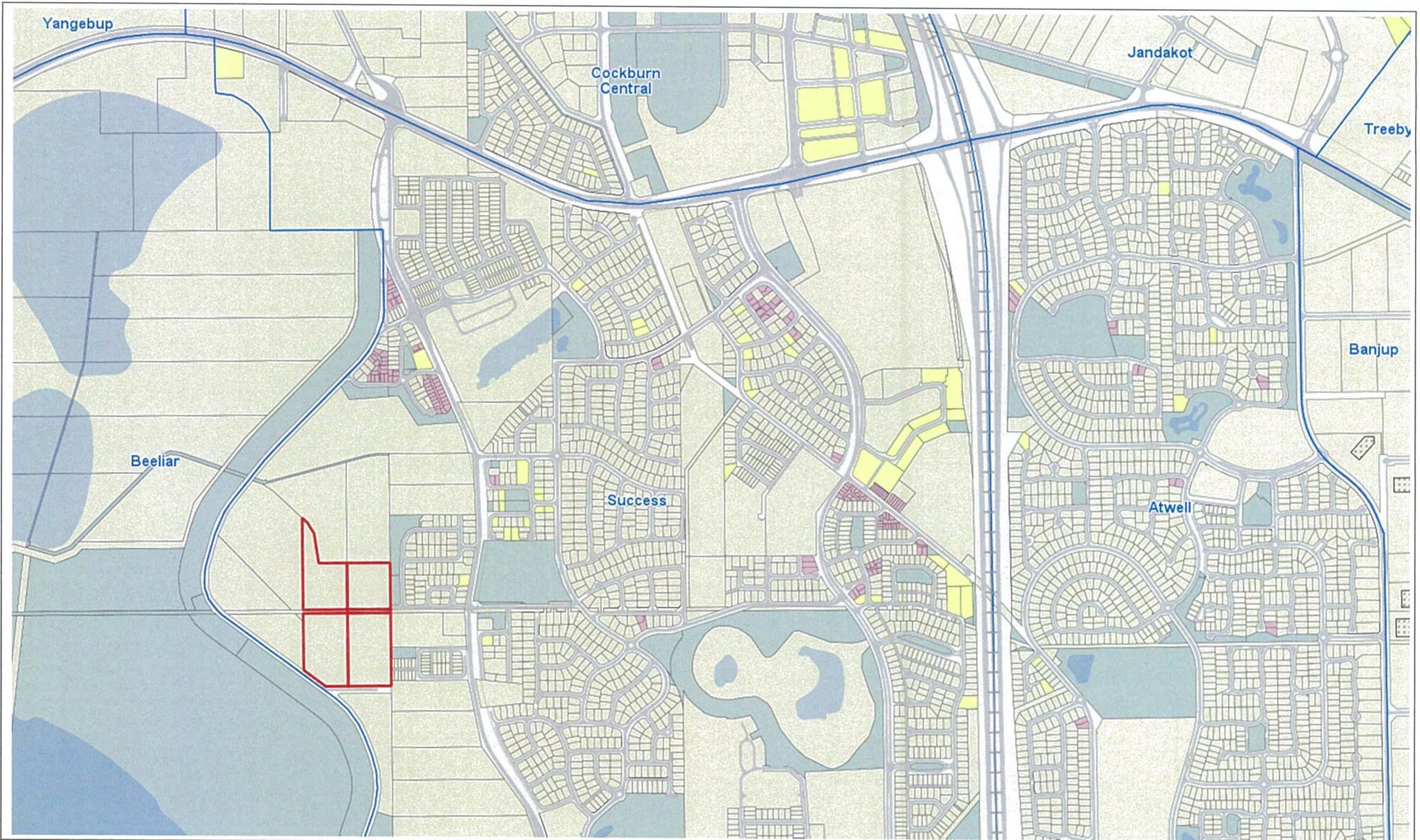
Warning: A current search of the sketch of the land should be obtained where detail of position, dimensions or area of the lot is required.
* Any entries preceded by an asterisk may not appear on the current edition of the duplicate certificate of title.
Lot as described in the land description may be a lot or location.

-----END OF CERTIFICATE OF TITLE-----

STATEMENTS:

The statements set out below are not intended to be nor should they be relied on as substitutes for inspection of the land and the relevant documents or for local government, legal, surveying or other professional advice.

SKETCH OF LAND: 1596-853 (432/D57125).
PREVIOUS TITLE: 1564-801, 1555-331, 1543-357, 1410-808.
PROPERTY STREET ADDRESS: 16 RODD PL, HAMILTON HILL.
LOCAL GOVERNMENT AREA: CITY OF COCKBURN.



Attachment 1 - Location Plan

PRINTED ON:
10/04/2017

SCALE = 1:15000

DISCLAIMER - The City of Cockburn provides the information contained herein and bears no responsibility or liability whatsoever for any errors, faults, defects or omissions of information contained in this document.



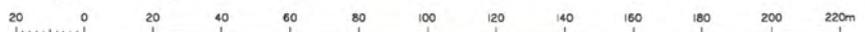
EXISTING STRUCTURE PLAN

Attach 2



- Legend:**
- Local Structure Plan Boundary
 - Zones**
 - Residential R25
 - Residential R30
 - Residential R40
 - Reserves**
 - Parks and Recreation
 - Public Purpose

Source Information:
 Site boundaries: PGS Surveys / RPS
 Adjoining information: City of Cockburn



PROJECT GADD STREET SUCCESS		CLIENT MUNTOC PTY LTD		 RPS	RPS Australia East Pty Ltd ACN 140 292 762 ABN 44 140 292 762	
Job Ref. 119136	Date 29 APRIL 2015	LOCAL STRUCTURE PLAN			Subiaco Office 38 Station Street Subiaco WA 6008	
Comp By. BC	DWG Name. 119136-2-001e.dwg			T +61 8 9211 1111 F +61 8 9211 1122 W rpsgroup.com.au		
Checked By. RS	Locality SUCCESS			Scale 1 : 1500	Sheet A3	Plan Ref 119136-2-001
Local Authority CITY OF COCKBURN						

AMENDED STRUCTURE PLAN

Attach 3



PROJECT GADD STREET SUCCESS		CLIENT MAIR PROJECTS		 RPS Australia East Pty Ltd ACN 140 292 762 ABN 44 140 292 762 Level 2/27/31 Troode Street West Perth WA Australia 6005 PO Box 170 West Perth WA 6872 T +61 8 9211 1111 F +61 8 9211 1122 W rpsgroup.com.au
Job Ref. 119136 Date 26 JUNE 2016	GADD STREET SUCCESS STRUCTURE PLAN		Scale 1 : 1500 Sheet A3 Plan Ref 119136-3-002 Rev <input type="checkbox"/>	
Comp By. LCFV DWG Name. 119136-2-001H.dwg	Local Authority CITY OF COCKBURN			
Checked By. RS Locally SUCCESS				

SCHEDULE OF SUBMISSIONS
PROPOSED STRUCTURE PLAN – LOT 1 GADD STREET, PORTION OF LOT 80 AND LOT 761 BRANCH CIRCUS, SUCCESS

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
1	Robert Dunn 256 Hammond Road SUCCESS WA 6164	<p>I would like to state my objection to the above proposal.</p> <p>Please note that my concerns about this proposal with regard to the Hammond Rd and Darlot Ave intersection safety and access etc have not changed and are on record at the City.</p> <p>The fact that increasing housing density from R25 to R30/R40 will only make the problem worse.</p> <p>Currently in peak traffic it is often dangerous and can take a long time to enter Hammond Rd from Darlot due to the unbroken line of traffic, visibility and speed.</p> <p>Only the few local residents in the vicinity of Darlot Ave currently negotiate the congestion.</p> <p>The addition of 124 plus residents(original plan) with two cars per residence will create massive congestion and safety issues at the intersection of Darlot and Hammond Rd.</p> <p>It will be impossible to make a timely and safe entry or exit with the enormous pressure of the additional traffic without a controlled intersection.</p>	<p>Noted. The City understands and agrees with the concerns raised previously regarding the Hammond Road/Darlot Avenue intersection. The City attempted to address this issue previously by coming to an agreement with the developer that they be responsible for upgrading the Hammond Road/Darlot Avenue intersection as a condition of subdivision approval over the subject land. However, this condition of subdivision was removed by the WAPC from the final approval, negating any obligation for the developer to upgrade this intersection. The City will again recommend this condition be placed on a decision to approve the amended Plan of Subdivision over the subject land lodged with the WAPC by the applicant in June 2016 as per recommendation (2)2 of the Council Report.</p> <p>However, the proposed amendment will not significantly increase traffic volumes in comparison to traffic expected to be generated by development of the land in accordance with the approved Structure Plan. Since the WAPC has approved the existing Structure Plan in spite of these traffic concerns, it is unlikely that this amendment proposal will present any unacceptable traffic impacts to prevent approval.</p> <p>Notwithstanding, the City is planning to</p>

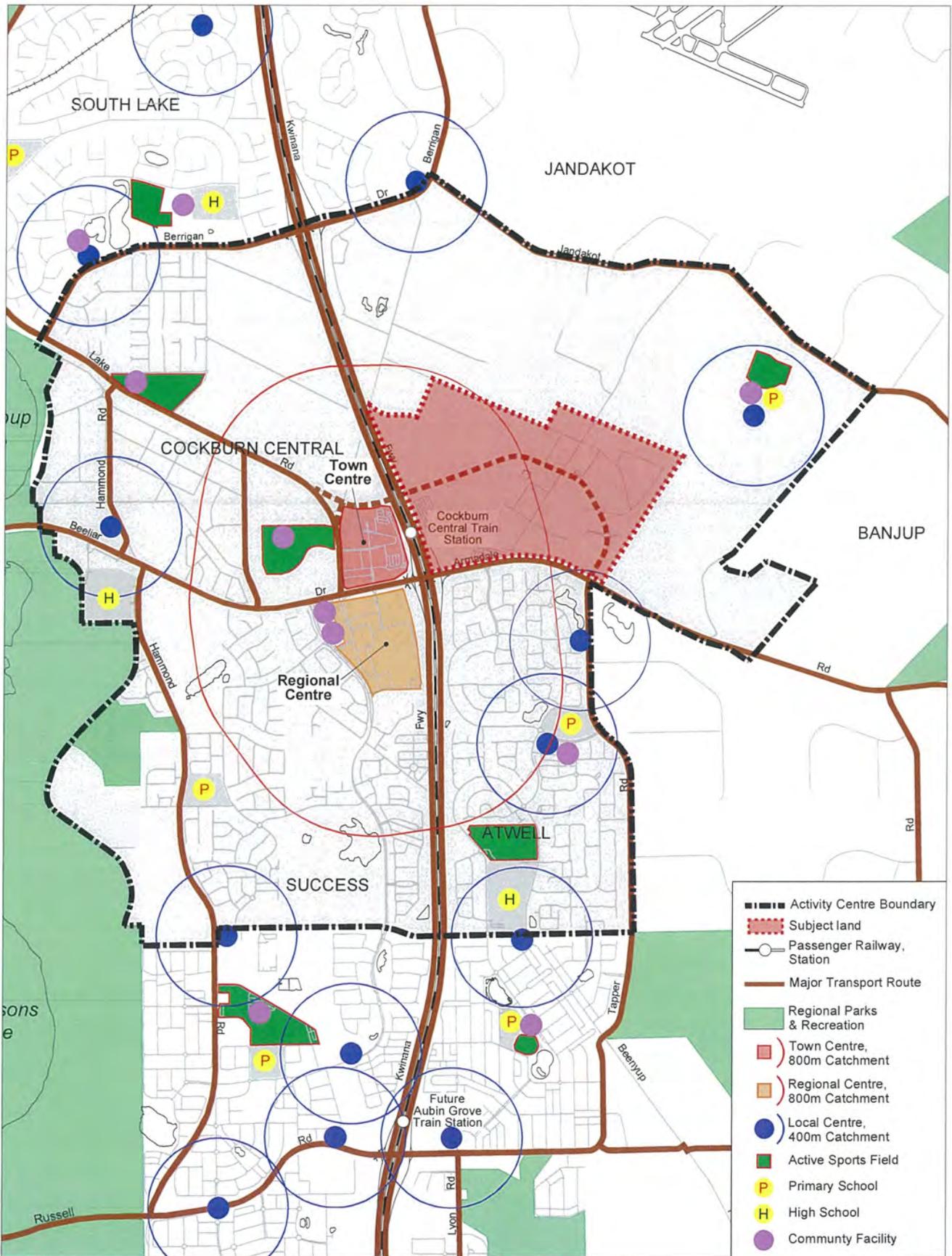
NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>In the event of fire, Branch Circus is surrounded by parkland and the road is not an escape route but more of a service road for watercorp that is un safe as has been demonstrated on several occasions over previous years with fires in the Beeliar Reserve burning up to and across Branch Circus as dense bushland surrounds the service road.</p> <p>Grass tree lane is the only other fire escape access to Hammond Rd via Windermere Circuit from this development.</p> <p>The laneway is narrow and often congested with street parking.</p> <p>I would encourage you to experience current peak traffic conditions so that you are under no illusions of the future problem that will occur without modification and a controlled intersection or roundabout.</p> <p>I direct you to the City's previous response below from Road Design Services.</p> <p>The opportunity is now available to address and rectify the intersection.</p>	<p>upgrade Hammond Road from Jubilee Avenue to Bartram Road. The works are anticipated to commence by 2019, but this is subject to funding. The works along Hammond Road would also address the intersection treatment at Darlot Avenue.</p> <p>In terms of fire, the applicant has prepared a Bushfire Management Plan (BMP) to accompany the existing/approved Structure Plan which has been approved by the City. The BMP incorporates adequate separation from residential zoned land to surrounding parkland via hazard separation zones consisting of road reserves or public open space maintained as low risk vegetation. Whilst Darlot Avenue is expected to be the primary access route from the subject land to Hammond Road, there will also be several other possible routes to Hammond Road available once structure planning and development of surrounding lots is completed. This includes Gadd Street to the south of the subject land, and an additional road connection adjacent to the small area of POS on the eastern boundary of the subject land as depicted on the Branch Circus District Structure Plan.</p> <p>The City is aware of the traffic issues at this intersection and is looking to upgrade the intersection to full movement access (rather than left-in, left-out) or potentially a roundabout. The City has planned upgrades to Hammond Road as stated above but unfortunately this can only proceed where funding becomes available, particularly if the developer is not required to upgrade this intersection.</p>
2	WA Gas Networks	ATCO Gas has existing PE High Pressure (DN63PE 1.5PEHP 350kPa) gas mains	Noted.

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
	(ATCO Australia) PO Box 3006 SUCCESS WA 6964	<p>and infrastructure within the Grasstree Lane road reserve and proposed gas mains within th designated area.</p> <p>ATCO Gas does not have any objection to lodge with the City of Cockburn after considering the advertised amended Structure Plan and map.</p> <p>Please see the attached Figure for your record. ATCO Gas will not be lodging a completed Submission Form – Structure Plan Amendment Proposal – Lot 1 Gadd Street, Portion of Lot 80 and Lot 761 Branch Circus, Success.</p>	
3	Water Corporation PO Box 100 LEEDERVILLE WA 6902	<p>The proposed changes are minor in nature and should not affect the ability to provide water and wastewater services to the proposed subdivision.</p> <p>The area of POS proposed over the westernmost road crossing over the Water Corporation's pipe reserve is acceptable. It is assumed that the land tenure of this portion of the pipe reserve will therefore remain unchanged. The proponent should be advised to consult with the Water Corporation regarding any proposed use or development of the portion of POS over the pipe reserve.</p>	Noted. The proponent has been made aware of this requirement via this attachment to the Council Report.
4	Telstra, Forecasting & Area Planning Locked Bag 2525 PERTH 6001	<p>At present, Telstra Corporation Limited has no objection. I have recorded this in our Development database and look forward to further correspondence in the future. Should you require any more information regarding Telstra's new infrastructure policy, please read below or contact me.</p> <p><u>Latest Telecommunications Policy</u></p> <p>The Federal Government has deemed developers are now responsible for telecommunications infrastructure on all developments, i.e. conduits, pits and the cost of the cable installation by Telstra or other carrier. Telstra can provide a quote for the pit and pipe and/or cable. This is explained on the Telstra Smart Community website below. The owner/developer will have to submit an application before construction is due to start to Telstra (less than 100 lots or living units) or NBN Co. (for greater than 100 lots or living units in a 3 year period).</p> <p>Applications to Telstra can be made on the Telstra Smart Community website: http://www.telstra.com.au/smart-community</p> <p>More information regarding NBN Co. can be found on their website http://www.nbnco.com.au/develop-or-plan-with-the-nbn.html</p> <p>Please dial 1100 (Dial before You Dig) for location of existing services.</p>	Noted.

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>Federal Government Telecommunications Infrastructure in New Developments Policy May 2015 https://www.communications.gov.au/policy/policy-listing/telecommunications-new-developments</p> <p>STATE PLANNING POLICY 5.2 Telecommunications Infrastructure August 2015 http://www.planning.wa.gov.au/dop_pub_pdf/Telecommunications_Infrastructure.pdf</p> <p>Communications Alliance - G645:2011 Fibre Ready Pit and Pipe Specification for Real Estate Development Projects http://www.commsalliance.com.au/Documents/all/guidelines/g645</p>	
5	Department of Health PO Box 8172 Perth BC WA 6849	Thank you for your letter of 3 April 2017 requesting comment from the Department of Health (DOH) on the above proposal. The DOH has no objection to the proposed amendment.	Noted.
6	Department of Transport GPO Box C102 PERTH WA 6839	The Department of Transport (DoT) has no comment to provide on this application. However, as the subject site is in close proximity to Hammond Road which is categorised as an Other Regional Road (ORR) in the Metropolitan Region Scheme (MRS) and is under the responsibility of Department of Planning. Therefore, comment should be sought from Department of Planning on this application.	Noted. The application has been referred to the Department of Planning for comment.
7	Department of Environment Regulation Locked Bag 33 Cloisters Square WA 6850	<p>The Department of Environment Regulation (DER) has reviewed the information submitted in regard to the structural plan amendment for Lot 1 Gadd Street, Portion of Lot 80 and Lot 761 Branch Circus, Success (Application No: 110/162).</p> <p>Based on the available information, consistent with advice provided on 28 April 2016, DER recommends that model acid sulfate soils (ASS) and contaminated sites conditions EN8 and EN9 and advices ENa1 and ENa2, respectively are applied to the approval as published in 'Model Subdivision Conditions Schedule' (Department of Planning and WAPC, October 2012).</p> <p>Please note that as requirement of model acid sulfate soil condition EN8, DER provided the attached advice following review of the acid sulfate soil management for proposed development associated with the former structure plan.</p> <p>This advice relates to acid sulfate soils and Contaminated Sites issues only. If additional advice is required in relation to other factors within the jurisdiction of DER, please contact the Land Use Planning Advice Coordinator on advice.coordinator@der.wa.gov.au.</p>	Noted. These requirements have been addressed in recommendation (2)2 of the Council Report.

NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
		<p>If you have any further clarification, please contact DER Contaminated Sites on 1300 762 982.</p>	
8	<p>Department of Parks and Wildlife Locked Bag 104 Bentley Delivery Centre WA 6983</p>	<p>The amendment area abuts portions of Beeliar Regional Park to the south west (Thomsons Lake Nature Reserve) and north (Branch Circus wetlands), both of which are managed by the department for conservation purposes and which form part of Bush Forever Site No. 391 – “Thompsons Lake Nature Reserve and Adjacent Bushland, Beeliar”. The site contains a portion of Conservation category wetland, as well as some significant areas of remnant vegetation.</p> <p>Background The department provided advice to the City of Cockburn in relation to the existing local structure plan (LSP) on 21 August 2015. It is understood that the LSP was endorsed by the WAPC in March 2017.</p> <p>In recognition that the LSP has been endorsed by the WAPC the department provides the following advice in relation to one of the proposed amendments only, consistent with previous advice.</p> <ul style="list-style-type: none"> • Amendment to remove road linkage and replace with POS <p>The amended structure plan depicts some residential development fronting the POS. WAPC's draft Liveable Neighbourhoods (2015) policy outlines that "Streets with lots fronting them should surround the majority of public open space (POS) as this provides amenity, safety and security for both POS users and residents.".... "Lots that are to directly front POS may require further detailed planning (facilitated by a local development plan) to address siting, amenity and safety issues both for the development, POS and the street..." Parks and Wildlife supports having a perimeter road between residential development and POS, for reasons of public safety, protection of bushland within the POS and fire safety for residents.</p> <p>The advice given above is consistent with the department's response to the WAPC in relation to the subdivision plan for Lot 1 and Lot 761 Gadd Street (WAPC reference 153467).</p>	<p>Noted. The removal of this road was extensively investigated by the City in terms of wetland management, bushfire risk and amenity. It was concluded that the proposal to remove this road would not have a negative impact on any of these factors with the proper conditions in place as stated in recommendation (2)2 of the Council Report. The POS will be maintained and managed to ensure invasive species do not make their way into the wetland vegetation, whilst also ensuring the bushfire risk is not increased to nearby dwellings. Dual use paths wide enough to allow emergency access vehicles to enter the POS will be required, as well as access gates from nearby local roads to further decrease the risk to dwellings (see recommendation (2)2). Dwellings will also be required to be constructed in accordance with AS 3959 - <i>Construction of buildings in bushfire-prone areas</i> at Building Permit stage.</p> <p>Furthermore, Local Development Plans will be required to be prepared for lots directly fronting POS to address amenity and security. Permeable fencing on the lot boundary adjacent to the POS will be required to increase security of POS through passive surveillance.</p> <p>The removal of this road will allow for a more efficient and effective drainage function within the POS and result in a more useable area of POS for future residents as described in more detail in the Council</p>

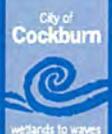
NO.	NAME/ADDRESS	SUBMISSION	RECOMMENDATION
			Report. Thus, with the above conditions in place, the removal of the road adjacent to the POS is an acceptable outcome from the City's point of view.

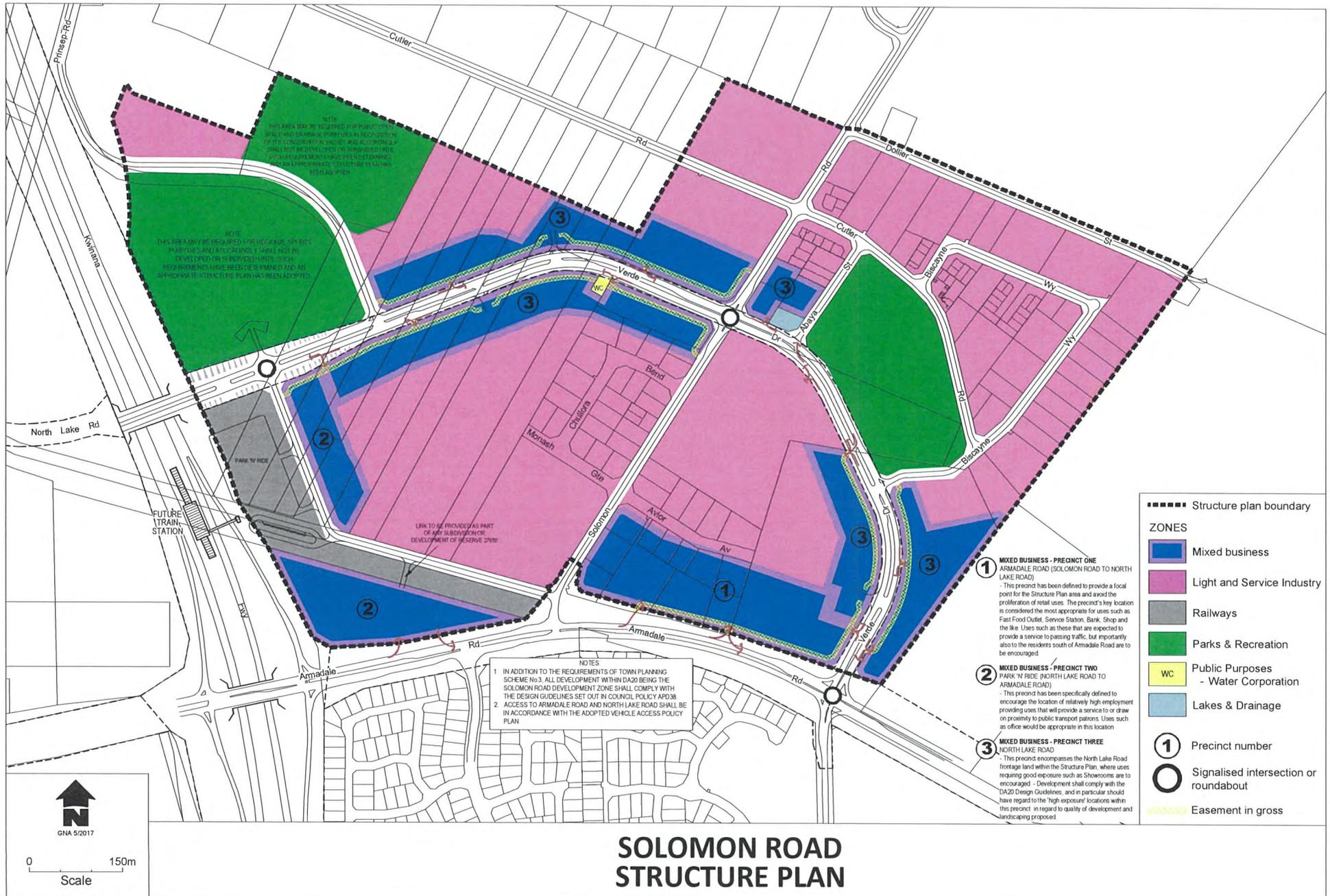




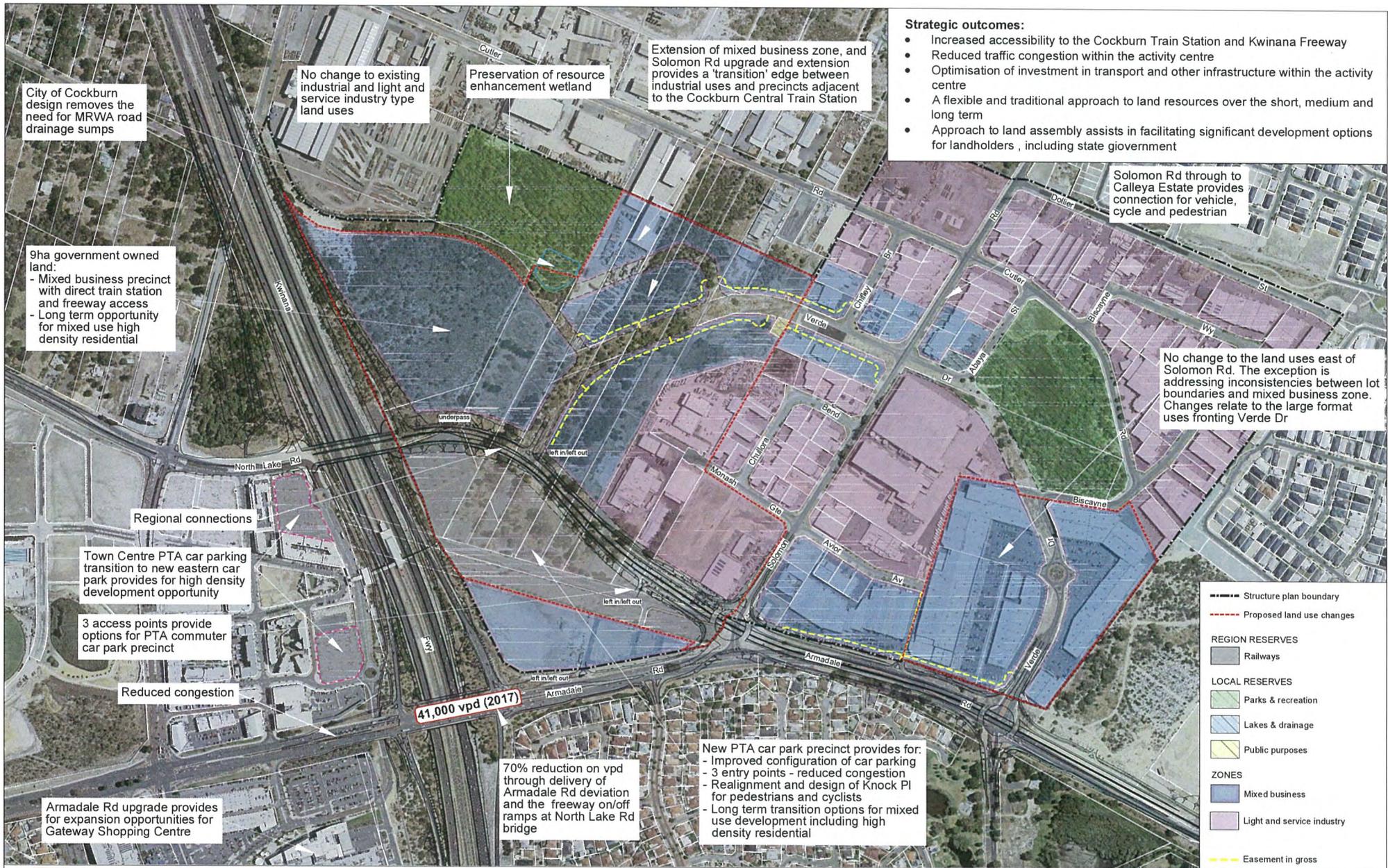
LOCAL CONTEXT

COCKBURN CENTRAL - SUBJECT LAND





SOLOMON ROAD STRUCTURE PLAN



STRUCTURE PLAN STRATEGY BENEFITS

CITY OF COCKBURN
MUNICIPAL BANK ACCOUNT

OCM 8/6/2017 - Item No.16.1

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF101844	10244	BUILDING & CONST INDUSTRY TRAINING FU LEVY PAYMENT	6/04/2017	43,899.79
EF101845	23302	BUILDING SERVIC BUILDING SERVICES LEVIES	6/04/2017	51,954.15
EF101846	26673	PROJECT 3 PTY LTD EVENT AND MARKETING AGENCY	6/04/2017	3,300.00
EF101847	26696	CHAMONIX TERBLANCHE MONTHLY COUNCILLOR ALLOWANCE	6/04/2017	729.00
EF101848	27138	MARINA INDUSTRIES ASSOCIATION LTD ANNUAL MEMBERSHIP SUBSCRIPTION	6/04/2017	520.00
EF101849	99996	INVESTWISE PROPERTY MANAGEMENT RATES REFUND	6/04/2017	2,112.83
EF101850	99997	JACK COLLINS JUNIOR SPORT TRAVEL ASSISTANCE	6/04/2017	400.00
EF101851	99997	RYAN NUTTALL JUNIOR SPORT TRAVEL ASSISTANCE	6/04/2017	400.00
EF101852	99997	KEVIN KALEMBER - CANCELLED JUNIOR SPORT TRAVEL ASSISTANCE	6/04/2017	400.00
EF101853	18314	GLOBETROTTER TRAVEL SERVICES	6/04/2017	22,761.38
EF101854	10118	AUSTRALIA POST POSTAGE CHARGES	10/04/2017	34,302.08
EF101855	10152	AUST SERVICES UNION PAYROLL DEDUCTIONS	10/04/2017	1,471.15
EF101856	10154	AUSTRALIAN TAXATION OFFICE PAYROLL DEDUCTIONS	10/04/2017	378,800.00
EF101857	10305	CHILD SUPPORT AGENCY PAYROLL DEDUCTIONS	10/04/2017	3,633.66
EF101858	10733	HOSPITAL BENEFIT FUND PAYROLL DEDUCTIONS	10/04/2017	295.05
EF101859	10888	LJ CATERERS CATERING SERVICES	10/04/2017	4,480.31
EF101860	11001	LOCAL GOVERNMENT RACING & CEMETERI PAYROLL DEDUCTIONS	10/04/2017	307.50
EF101861	11857	CHAMPAGNE SOCIAL CLUB PAYROLL DEDUCTIONS	10/04/2017	563.20
EF101862	11860	45S CLUB PAYROLL DEDUCTIONS	10/04/2017	18.00
EF101863	18553	SELECTUS PTY LTD PAYROLL DEDUCTIONS	10/04/2017	13,953.93
EF101864	19726	HEALTH INSURANCE FUND OF WA PAYROLL DEDUCTIONS	10/04/2017	1,339.80
EF101865	23250	DEPARTMENT OF PLANNING DAP APPLICATIONS & DAP FEES	10/04/2017	5.00
EF101866	25987	TOYOTA FLEET MANAGEMENT PAYROLL DEDUCTIONS - NOVATED LEASE	10/04/2017	567.62
EF101867	26568	UNITED DIAMOND TOOLS DIAMOND CUTTING TOOLS	10/04/2017	2,720.00
EF101868	99997	KEVIN KALEMBER LGACS9 - JUNIOR SPORT TRAVEL ASSISTANCE	10/04/2017	400.00
EF101869	18314	GLOBETROTTER TRAVEL SERVICES	13/04/2017	8,746.70
EF101870	99997	RETHINK EVENTS LTD WORLD WASTE TO ENERGY RESOURCES	18/04/2017	14,453.18
EF101871	10152	AUST SERVICES UNION PAYROLL DEDUCTIONS	24/04/2017	1,418.45
EF101872	10154	AUSTRALIAN TAXATION OFFICE PAYROLL DEDUCTIONS	24/04/2017	377,987.00

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EF101873	10305	CHILD SUPPORT AGENCY PAYROLL DEDUCTIONS	24/04/2017	3,633.66
EF101874	10733	HOSPITAL BENEFIT FUND PAYROLL DEDUCTIONS	24/04/2017	317.80
EF101875	10888	LJ CATERERS CATERING SERVICES	24/04/2017	3,572.80
EF101876	11001	LOCAL GOVT. RACING & CEMETERIES EMPLOYEES PAYROLL DEDUCTIONS	24/04/2017	307.50
EF101877	11857	CHAMPAGNE SOCIAL CLUB PAYROLL DEDUCTIONS	24/04/2017	560.00
EF101878	11860	45S CLUB PAYROLL DEDUCTIONS	24/04/2017	16.00
EF101879	18553	SELECTUS PTY LTD PAYROLL DEDUCTIONS	24/04/2017	12,507.83
EF101880	19726	HEALTH INSURANCE FUND OF WA PAYROLL DEDUCTIONS	24/04/2017	1,563.95
EF101881	25987	TOYOTA FLEET MANAGEMENT PAYROLL DEDUCTIONS - NOVATED LEASE	24/04/2017	567.62
EF101882	11867	KEVIN JOHN ALLEN MONTHLY COUNCILLOR ALLOWANCE	28/04/2017	2,613.67
EF101883	12740	MAYOR LOGAN HOWLETT MONTHLY COUNCILLOR ALLOW. & REIMB.	28/04/2017	11,658.73
EF101884	19059	CAROL REEVE-FOWKES MONTHLY COUNCILLOR ALLOWANCE	28/04/2017	4,465.00
EF101885	20634	LEE-ANNE SMITH MONTHLY COUNCILLOR ALLOWANCE	28/04/2017	2,613.67
EF101886	21185	BART HOUWEN MONTHLY COUNCILLOR ALLOWANCE	28/04/2017	2,613.67
EF101887	23338	STEVE PORTELLI MONTHLY COUNCILLOR ALLOWANCE	28/04/2017	2,613.67
EF101888	23339	STEPHEN PRATT MONTHLY COUNCILLOR ALLOWANCE	28/04/2017	2,613.67
EF101889	25352	LYNDSEY SWEETMAN MONTHLY COUNCILLOR ALLOWANCE	28/04/2017	2,613.67
EF101890	25353	PHILIP EVA MONTHLY COUNCILLOR ALLOWANCE	28/04/2017	2,613.67
EF101891	26696	CHAMONIX TERBLANCHE MONTHLY COUNCILLOR ALLOWANCE	28/04/2017	2,613.67
EF101892	10047	ALINTA ENERGY NATURAL GAS & ELECTRCITY SUPPLY	28/04/2017	68,915.48
EF101893	11794	SYNERGY ELECTRICITY USAGE/SUPPLIES	28/04/2017	306,217.70
EF101894	12025	TELSTRA CORPORATION COMMUNICATIONS SERVICES	28/04/2017	16,983.71
EF101895	10035	ADVENTURE WORLD WA PTY LTD ENTERTAINMENT SERVICES	28/04/2017	588.00
EF101896	10058	ALSCO PTY LTD HYGIENE SERVICES/SUPPLIES	28/04/2017	535.57
EF101897	10071	AUSTRALASIAN PERFORMING RIGHT ASSOCIATION LICENCE - PERFORMING RIGHTS	28/04/2017	561.81
EF101898	10079	ARCUS AUSTRALIA REFRIGERATION REPAIRS	28/04/2017	426.95
EF101899	10086	ARTEIL WA PTY LTD ERGONOMIC CHAIRS	28/04/2017	497.20
EF101900	10091	ASLAB PTY LTD ASPHALTING SERVICES/SUPPLIES	28/04/2017	7,856.46
EF101901	10160	DORMA AUSTRALIA PTY LTD AUTOMATIC DOOR SERVICES	28/04/2017	11,287.98

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EF101902	10201	BIG W DISCOUNT STORES VARIOUS SUPPLIES	28/04/2017	115.40
EF101903	10207	BOC GASES GAS SUPPLIES	28/04/2017	449.72
EF101904	10220	BOYA EQUIPMENT EQUIPMENT SUPPLIES	28/04/2017	519.61
EF101905	10221	BP AUSTRALIA LIMITED DIESEL/PETROL SUPPLIES	28/04/2017	25,893.30
EF101906	10226	BRIDGESTONE AUSTRALIA LTD TYRE SERVICES	28/04/2017	26,746.50
EF101907	10239	BUDGET RENT A CAR - PERTH MOTOR VEHICLE HIRE	28/04/2017	1,280.00
EF101908	10246	BUNNINGS BUILDING SUPPLIES PTY LTD HARDWARE SUPPLIES	28/04/2017	3,532.25
EF101909	10247	BUNZL AUSTRALIA LTD PAPER/PLASTIC/CLEANING SUPPLIES	28/04/2017	555.21
EF101910	10255	CABCHARGE AUSTRALIA PTY LTD CABCHARGES	28/04/2017	95.70
EF101911	10279	CASTROL AUSTRALIA PTY LTD GREASE/LUBRICANTS	28/04/2017	2,817.55
EF101912	10333	CJD EQUIPMENT PTY LTD HARDWARE SUPPLIES	28/04/2017	1,831.46
EF101913	10348	COCA COLA AMATIL SOFT DRINK SUPPLIES	28/04/2017	310.60
EF101914	10353	COCKBURN CEMENT LTD RATES REFUND	28/04/2017	736.56
EF101915	10358	COCKBURN LIQUOR CENTRE LIQUOR SUPPLIES	28/04/2017	1,280.33
EF101916	10359	COCKBURN PAINTING SERVICE PAINTING SUPPLIES/SERVICES	28/04/2017	7,458.00
EF101917	10368	COCKBURN WETLANDS EDUCATION CENTRE COMMUNITY GRANT	28/04/2017	600.00
EF101918	10375	VEOLIA ENVIRONMENTAL SERVICES WASTE SERVICES	28/04/2017	7,948.60
EF101919	10384	PROGILITY PTY LTD COMMUNICATION SERVICES	28/04/2017	4,059.33
EF101920	10386	COMMUNITY NEWSPAPER GROUP ADVERTISING SERVICES	28/04/2017	17,383.74
EF101921	10456	DATANET PTY LTD MOBILITY IT DEVICES	28/04/2017	17,689.10
EF101922	10459	DAVID GRAY & CO MOBILE GARBAGE BINS	28/04/2017	173.25
EF101923	10483	LANDGATE MAPPING/LAND TITLE SEARCHES	28/04/2017	9,720.42
EF101924	10526	E & MJ ROSHER PTY LTD MOWER PARTS	28/04/2017	8,234.30
EF101925	10535	WORKPOWER INCORPORATED EMPLOYMENT SERVICES - PLANTING	28/04/2017	8,457.36
EF101926	10580	FC COURIERS COURIER SERVICES	28/04/2017	1,948.27
EF101927	10597	FLEXI STAFF PTY LTD EMPLOYMENT SERVICES	28/04/2017	78,093.89
EF101928	10609	FORESTVALE TREES P/L PLANTS - TREES/SHRUBS	28/04/2017	2,288.00
EF101929	10636	FUJI XEROX AUSTRALIA PTY LTD PHOTOCOPY CHARGES	28/04/2017	11,732.14
EF101930	10679	GRASSTREES AUSTRALIA PLANTS & PLANTING SERVICES	28/04/2017	8,382.00

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EF101931	10709	HECS FIRE FIRE SYSTEM MAINTENANCE	28/04/2017	8,317.54
EF101932	10726	HOLTON CONNOR ARCHITECTS & PLANNERS ARCHITECTURAL SERVICES	28/04/2017	14,008.72
EF101933	10757	INDOOR KART HIRE O'CONNOR ENTERTAINMENT SERVICES	28/04/2017	1,600.00
EF101934	10779	J F COVICH & CO PTY LTD ELECTRICAL SERVICES	28/04/2017	92,574.50
EF101935	10783	JANDAKOT METAL INDUSTRIES METAL SUPPLIES	28/04/2017	1,660.00
EF101936	10787	JANDAKOT ACCIDENT REPAIR CENTRE PANEL BEATING SERVICES	28/04/2017	2,000.00
EF101937	10794	JASON SIGNMAKERS SIGNS	28/04/2017	6,293.88
EF101938	10814	JR & A HERSEY PTY LTD SAFETY CLOTHING SUPPLIES	28/04/2017	759.00
EF101939	10879	LES MILLS AEROBICS INSTRUCTION/TRAINING SERVICES	28/04/2017	1,142.79
EF101940	10913	BUCHER MUNICIPAL PTY LTD PURCHASE OF NEW PLANT / REPAIR SERVICES	28/04/2017	12,090.88
EF101941	10918	MAIN ROADS WA REPAIRS/MAINTENANCE SERVICES	28/04/2017	34,544.91
EF101942	10923	MAJOR MOTORS PTY LTD REPAIRS/MAINTENANCE SERVICES	28/04/2017	165.18
EF101943	10944	MCLEODS LEGAL SERVICES	28/04/2017	27,349.75
EF101944	11022	NATIVE ARC GRANTS & DONATIONS	28/04/2017	450.00
EF101945	11026	NESTLE FOOD SERVICES CATERING SUPPLIES	28/04/2017	1,032.00
EF101946	11028	NEVERFAIL SPRINGWATER LTD BOTTLED WATER SUPPLIES	28/04/2017	1,565.19
EF101947	11036	NORTHLAKE ELECTRICAL ELECTRICAL SERVICES	28/04/2017	46,789.46
EF101948	11039	NOVUS AUTO GLASS WINDSCREEN REPAIR SERVICES	28/04/2017	696.50
EF101949	11068	VODAFONE HUTCHISON AUSTRALIA PTY LTD PAGING SERVICES	28/04/2017	665.50
EF101950	11070	OTIS ELEVATOR COMPANY ELEVATOR REPAIRS/MAINTENANCE	28/04/2017	2,129.19
EF101951	11077	P & G BODY BUILDERS PTY LTD PLANT BODY BUILDING SERVICES	28/04/2017	2,219.80
EF101952	11093	PARKLAND MAZDA LIGHT FLEET REPLACEMENT	28/04/2017	24,158.75
EF101953	11205	QUALITY TRAFFIC MANAGEMENT PTY LTD TRAFFIC CONTROL SERVICES	28/04/2017	33,988.39
EF101954	11208	QUICK CORPORATE AUSTRALIA PTY LTD STATIONERY/CONSUMABLES	28/04/2017	6,610.59
EF101955	11307	SATELLITE SECURITY SERVICES PTY LTD SECURITY SERVICES	28/04/2017	8,104.64
EF101956	11308	BOSS INDUSTRIAL FORMALLY SBA SUPPLIES HARDWARE SUPPLIES	28/04/2017	1,389.60
EF101957	11361	SIGMA CHEMICALS PTY LTD CHEMICAL SUPPLIES	28/04/2017	722.97
EF101958	11373	SKIPPER TRUCK PARTS SPARE PARTS & MAINTENANCE SERVICES	28/04/2017	945.47
EF101959	11425	SOUTHERN METROPOLITAN REGIONAL COUNCIL WASTE DISPOSAL GATE FEES	28/04/2017	661,242.90

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EF101960	11459	SPEARWOOD VETERINARY HOSPITAL VETERINARY SERVICES	28/04/2017	480.00
EF101961	11483	ST JOHN AMBULANCE AUST WA OPERATION FIRST AID COURSES	28/04/2017	683.01
EF101962	11496	STANLEE WA LTD CATERING EQUIPMENT/SUPPLIES	28/04/2017	867.84
EF101963	11546	T FAULKNER & CO INSTALLATIONS/SUPPLY OF HAND RAILS	28/04/2017	13,135.00
EF101964	11557	TECHNOLOGY ONE LTD IT CONSULTANCY SERVICES & SOFTWARE LICENCE	28/04/2017	65,677.08
EF101965	11594	MEN OF THE TREES SUPPLY OF PLANTS	28/04/2017	120.00
EF101966	11625	TOTAL EDEN PTY LTD RETICULATION SUPPLIES	28/04/2017	24,909.17
EF101967	11642	TRAILER PARTS PTY LTD TRAILER PARTS	28/04/2017	534.84
EF101968	11651	TREE WATERING SERVICES TREE WATERING SERVICES	28/04/2017	62,720.00
EF101969	11667	TURFMASTER FACILITY MANAGEMENT TURFING SERVICES	28/04/2017	8,360.00
EF101970	11699	VERNON DESIGN GROUP ARCHITECTURAL SERVICES	28/04/2017	26,730.00
EF101971	11701	VIBRA INDUSTRIA FILTER SUPPLIES	28/04/2017	1,378.30
EF101972	11708	VITAL PACKAGING PTY LTD PACKAGING SUPPLIES	28/04/2017	4,675.00
EF101973	11715	WA BLUEMETAL ROADBASE SUPPLIES	28/04/2017	7,418.29
EF101974	11722	WA HINO SALES & SERVICE PURCHASE OF NEW TRUCKS / MAINTENANCE	28/04/2017	1,978.71
EF101975	11749	WARRENS EARTHMOVING CONTRACTORS EARTHMOVING SERVICES	28/04/2017	12,672.00
EF101976	11753	WASTE MANAGEMENT & RECYCLING FUND QUARTERLY LANDFILL LEVY PAYMENT	28/04/2017	687,870.84
EF101977	11773	WESFARMERS LANDMARK LIMITED CHEMICAL SUPPLIES	28/04/2017	2,002.00
EF101978	11787	DEPT OF TRANSPORT VEHICLE SEARCH FEES	28/04/2017	158.40
EF101979	11789	WALGA ADVERTISING/TRAINING SERVICES	28/04/2017	685.05
EF101980	11793	WESTERN IRRIGATION PTY LTD IRRIGATION SERVICES/SUPPLIES	28/04/2017	42,090.45
EF101981	11795	WESTERN POWER ELECTRICAL SERVICES	28/04/2017	6,890.00
EF101982	11828	WORLDWIDE ONLINE PRINTING - O'CONNOR PRINTING SERVICES	28/04/2017	149.50
EF101983	11835	WURTH AUSTRALIA PTY LTD HARDWARE SUPPLIES	28/04/2017	32.45
EF101984	11841	YANGEBUP FAMILY CENTRE INC VENUE HIRE / GRANTS & DONATIONS	28/04/2017	425.00
EF101985	11854	ZIPFORM PRINTING SERVICES	28/04/2017	1,458.01
EF101986	11990	EARTHCARE (AUSTRALIA) P/L LANDSCAPING SERVICES	28/04/2017	35,215.29
EF101987	12193	SAGE CONSULTING ENGINEERS P/L CONSULTANCY SERVICES - LIGHTING	28/04/2017	4,235.00
EF101988	12219	PARKS AND LEISURE AUSTRALIA SUBSCRIPTION RENEWAL	28/04/2017	132.00

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EF101989	12394	MP ROGERS & ASSOCIATES PTY LTD CONSULTANCY SERVICES - MARINE	28/04/2017	1,097.36
EF101990	12458	KITE KINETICS ENTERTAINMENT SERVICES	28/04/2017	400.00
EF101991	12497	TROPHY CHOICE TROPHY SUPPLIES	28/04/2017	939.00
EF101992	12560	AUSTSWIM LTD TRAINING SERVICES	28/04/2017	280.00
EF101993	12589	AUSTRALIAN INSTITUTE OF MANAGEMENT TRAINING SERVICES	28/04/2017	2,926.00
EF101994	12672	NORMAN DISNEY & YOUNG CONSULTANCY SERVICES	28/04/2017	484.00
EF101995	12779	WESTERN RESOURCE RECOVERY PTY LTD WASTE DISPOSAL SERVICES	28/04/2017	7,459.50
EF101996	12796	ISENTIA PTY LIMITED MEDIA MONITORING SERVICES	28/04/2017	1,673.85
EF101997	12883	CONSERVATION VOLUNTEERS AUSTRALIA ENVIRONMENTAL SERVICES	28/04/2017	4,125.00
EF101998	12998	PLAYRIGHT AUSTRALIA PTY LTD INSPECTION SERVICES - PLAYGROUNDS	28/04/2017	576.40
EF101999	13056	CLEANDUSTRIAL SERVICES PTY LTD CLEANING SERVICES	28/04/2017	71,381.14
EF102000	13102	MICHAEL PAGE INTERNATIONAL EMPLOYMENT SERVICES	28/04/2017	2,097.34
EF102001	13111	OCE-AUSTRALIA LIMITED COPIERS/PRINTERS	28/04/2017	477.17
EF102002	13393	SOUTH WEST GROUP CONTRIBUTION - NRM FACILITATOR	28/04/2017	35,750.00
EF102003	13462	ATI-MIRAGE PTY LTD TRAINING SERVICES	28/04/2017	989.41
EF102004	13521	DIAMOND CUT CONCRETE CONCRETE CUTTING SERVICES	28/04/2017	660.00
EF102005	13563	GREEN SKILLS INC EMPLOYMENT SERVICES	28/04/2017	10,992.80
EF102006	13670	HISCO PTY LTD HOSPITALITY SUPPLIES	28/04/2017	233.02
EF102007	13671	STAPLES AUSTRALIA PTY LTD OFFICE/STATIONERY SUPPLIES	28/04/2017	27.67
EF102008	13860	KRS CONTRACTING WASTE COLLECTION SERVICES	28/04/2017	33,427.35
EF102009	13873	COCKBURN SES TRAFFIC MANAGEMENT SERVICES	28/04/2017	800.00
EF102010	13937	HIND'S TRANSPORT SERVICES TRANSPORT SERVICES	28/04/2017	1,874.88
EF102011	14305	ACHIEVEABILITY PTY LTD TRAINING SEMINAR	28/04/2017	2,200.00
EF102012	14311	BBC ENTERTAINMENT ENTERTAINMENT SERVICES	28/04/2017	880.00
EF102013	14350	BAILEYS FERTILISERS FERTILISER SUPPLIES	28/04/2017	18,059.18
EF102014	14476	COCKBURN PLEASURE BOAT STORAGE STORAGE SERVICES	28/04/2017	1,980.00
EF102015	14593	AUSTREND INTERNATIONAL PTY LTD ALUMINIUM SUPPLIES	28/04/2017	11,355.05
EF102016	14667	APPEALING SIGNS SIGNS	28/04/2017	495.00
EF102017	14700	KINGMAN SIGNS & GRAPHICS SIGNWRITING/SIGNMAKING	28/04/2017	22,000.00

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EF102018	14777	LGIS INSURANCE BROKING INSURANCE PREMIUMS	28/04/2017	154.95
EF102019	15224	GILBARCO EQUIPMENT REPAIRS	28/04/2017	1,301.30
EF102020	15393	STRATAGREEN HARDWARE SUPPLIES	28/04/2017	371.16
EF102021	15541	JANDAKOT NEWS NEWSPAPER SUPPLIERS	28/04/2017	129.46
EF102022	15571	ADASOUND PUBLIC ADDRESS PA REPAIRS	28/04/2017	3,222.50
EF102023	15588	NATURAL AREA HOLDINGS PTY LTD WEED SPRAYING	28/04/2017	17,351.68
EF102024	15609	CATALYSE PTY LTD CONSULTANCY SERVICES	28/04/2017	1,980.00
EF102025	15678	A2Z PEST CONTROL THE TRUSTEE FOR CAL PEST CONTROL	28/04/2017	4,772.00
EF102026	15786	AD ENGINEERING INTERNATIONAL PTY LTD SIGNS - ELECTRONIC	28/04/2017	132.00
EF102027	15868	CARDNO (WA) PTY LTD CONSULTANCY SERVICES - ENGINEERING	28/04/2017	20,039.80
EF102028	15914	T-QUIP MOWING EQUIPMENT	28/04/2017	3,464.15
EF102029	16064	CMS ENGINEERING PTY LTD AIRCONDITIONING SERVICES	28/04/2017	32,583.31
EF102030	16107	WREN OIL WASTE DISPOSAL SERVICES	28/04/2017	165.00
EF102031	16257	THE FINISHING TOUCH GALLERY FRAMING SERVICES	28/04/2017	1,284.36
EF102032	16291	WA PROFILING ROAD PROFILING SERVICES	28/04/2017	3,630.00
EF102033	16363	ATCO GAS AUSTRALIA GAS SUPPLIES/SERVICES	28/04/2017	481.40
EF102034	16396	MAYDAY EARTHMOVING ROAD CONSTRUCTION MACHINE HIRE	28/04/2017	49,360.86
EF102035	16648	YOUTH AFFAIRS COUNCIL OF WA INC YOUTH AFFAIRS	28/04/2017	3,700.00
EF102036	16704	ACCIDENTAL FIRST AID SUPPLIES - PERTH S MEDICAL SUPPLIES	28/04/2017	1,016.50
EF102037	16894	TREBLEX INDUSTRIAL PTY LTD CHEMICALS - AUTOMOTIVE	28/04/2017	1,859.00
EF102038	16985	WA PREMIX CONCRETE SUPPLIES	28/04/2017	8,537.76
EF102039	16997	AUS SECURE SECURITY SERVICES/PRODUCTS	28/04/2017	100.00
EF102040	17213	COCKBURN CITY SOCCER CLUB INC SPORT EQUIPMENT GRANT	28/04/2017	760.00
EF102041	17279	AUSSIE COOL SHADES SHADE SAILS & AWNINGS	28/04/2017	25,010.97
EF102042	17471	PIRTEK (FREMANTLE) PTY LTD HOSES & FITTINGS	28/04/2017	4,764.52
EF102043	17600	ERLECTIONS (WA) GUARD RAILS	28/04/2017	7,755.00
EF102044	17608	NU-TRAC RURAL CONTRACTING BEACH CLEANING/FIREBREAK CONSTRUCTION	28/04/2017	9,586.00
EF102045	17798	WESTERN DIAGNOSTIC PATHOLOGY ANALYTICAL SERVICES	28/04/2017	88.00
EF102046	17887	RED SAND SUPPLIES PTY LTD MACHINERY HIRE	28/04/2017	7,458.00

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EF102047	17942	MRS MAC'S FOOD SUPPLIES	28/04/2017	47.00
EF102048	18126	DELL AUSTRALIA PTY LTD COMPUTER HARDWARE	28/04/2017	2,090.90
EF102049	18147	AURECON AUSTRALASIA PTY LTD CONSULTANCY - CIVIL ENGINEERING	28/04/2017	6,200.98
EF102050	18249	LASSO KIP PTY LTD ADVERTISING	28/04/2017	440.00
EF102051	18286	IW PROJECTS PTY LTD CONSULTANCY SERVICES - CIVIL ENGINEERING	28/04/2017	17,207.58
EF102052	18314	GLOBETROTTER TRAVEL SERVICES	28/04/2017	9,735.76
EF102053	18407	RIPE ART CATERING SERVICES - EDIBLE ART	28/04/2017	400.00
EF102054	18508	JOHN TURNER BRICK LAYING SERVICES	28/04/2017	3,338.00
EF102055	18533	FRIENDS OF THE COMMUNITY INC. DONATION	28/04/2017	376.80
EF102056	18559	LORRAINE'S PARTY PONIES PARTY HIRE	28/04/2017	500.00
EF102057	18628	UNILEVER AUSTRALIA LTD BEVERAGES	28/04/2017	227.76
EF102058	18734	P & R EDWARDS ENTERTAINMENT SERVICES	28/04/2017	345.00
EF102059	18760	MELVILLE SUBARU VEHICLE PURCHASE	28/04/2017	36,633.64
EF102060	18764	AFFIRMATIVE GROUP BRICK PAVING SERVICES	28/04/2017	4,713.50
EF102061	18801	FREMANTLE BIN HIRE BIN HIRE - SKIP BINS	28/04/2017	250.00
EF102062	18962	SEALANES (1985) P/L CATERING SUPPLIES	28/04/2017	1,332.20
EF102063	19058	VALENTINE'S CAMERA HOUSE FREMANTLE PHOTOGRAPHIC EQUIPMENT	28/04/2017	45.00
EF102064	19107	FOREVER SHINING STAINLESS STEEL BIN ENCLOSURES	28/04/2017	90,991.00
EF102065	19306	ZIP HEATERS (AUST) PTY LTD HEATERS	28/04/2017	481.19
EF102066	19505	ADVANCED WINDOW SHUTTERS WINDOW SHUTTERS	28/04/2017	300.00
EF102067	19533	WOOLWORTHS LTD GROCERIES	28/04/2017	1,957.63
EF102068	19541	TURF CARE WA PTY LTD TURF SERVICES	28/04/2017	10,029.80
EF102069	19546	THE BIG PICTURE FACTORY PRINTING SERVICES	28/04/2017	306.90
EF102070	19649	TELSTRA NETWORK INTEGRITY SERVICES ASSET RELOCATION - BERRIGAN & JANDAKOT DRV	28/04/2017	1,390,275.57
EF102071	19718	SIFTING SANDS CLEANING SERVICES - SAND	28/04/2017	29,901.55
EF102072	19847	PFD FOOD SERVICES PTY LTD CATERING SERVICES	28/04/2017	775.75
EF102073	20000	AUST WEST AUTO ELECTRICAL P/L AUTO ELECTRICAL SERVICES	28/04/2017	17,987.89
EF102074	20146	DATA#3 LIMITED CONTRACT IT PERSONNEL & SOFTWARE	28/04/2017	7,657.46
EF102075	20238	MY DELICIOUS CAKE DECORATING SERVICES	28/04/2017	550.00

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EF102076	20307	OCTAGON-BKG LIFTS MAINTENANCE SERVICES	28/04/2017	1,422.84
EF102077	20321	RIVERJET P/L EDUCTING-CLEANING SERVICES	28/04/2017	25,220.25
EF102078	20535	HOME-GROWN THEATRE DRAMA CLASSES	28/04/2017	2,310.00
EF102079	20549	A1 CARPET, TILE & GROUT CLEANING CLEANING SERVICES - TILES/CARPET	28/04/2017	6,033.50
EF102080	20622	BLADE SKATE SPORTING EQUIPMENT	28/04/2017	2,000.00
EF102081	20786	THE BUTCHER SHOP ARTISTIC SUPPLIES	28/04/2017	1,207.94
EF102082	20867	FARM INFORMATION SERVICES TRAINING/INSTRUCTION	28/04/2017	11,299.40
EF102083	21005	BRAIN TEASERS OZ PTY LTD EDUCATIONAL PRODUCTS	28/04/2017	66.00
EF102084	21010	REDMAN SOLUTIONS PTY LTD COMPUTER SOFTWARE	28/04/2017	1,180.25
EF102085	21101	AMY WARNE COMPOST WORKSHOPS	28/04/2017	400.00
EF102086	21139	AUSTRAFFIC WA PTY LTD TRAFFIC SURVEYS	28/04/2017	1,815.00
EF102087	21287	T.J.DEPIAZZI & SONS SOIL & MULCH SUPPLIES	28/04/2017	56,885.40
EF102088	21291	CHITTERING VALLEY WORM FARM ENVIRONMENTAL EDUCATION	28/04/2017	1,643.00
EF102089	21294	CAT HAVEN ANIMAL SERVICES	28/04/2017	1,188.00
EF102090	21371	LD TOTAL SANPOINT PTY LTD LANDSCAPING WORKS/SERVICES	28/04/2017	37,903.25
EF102091	21469	JOHN HUGHES VOLKSWAGON PURCHASE OF NEW VEHICLE	28/04/2017	21,250.70
EF102092	21627	MANHEIM PTY LTD IMPOUNDED VEHICLES	28/04/2017	2,138.40
EF102093	21664	ACT INDUSTRIAL PTY LTD SKIP BINS - MANUFACTURE	28/04/2017	9,630.50
EF102094	21665	MMJ REAL ESTATE (WA) PTY LTD PROPERTY MANAGEMENT SERVICES	28/04/2017	25,531.03
EF102095	21697	ICT EXPRESS PTY LTD CONSULTANCY SERVICES - IT	28/04/2017	3,344.00
EF102096	21744	JB HI FI - COMMERCIAL ELECTRONIC EQUIPMENT	28/04/2017	1,840.00
EF102097	21782	WEST COAST TIMBER FLOORING FLOORING SUPPLIES	28/04/2017	10,967.00
EF102098	21844	DRAIN PAVE ASPHALT STAMPING SERVICES	28/04/2017	2,952.96
EF102099	21868	AUSTRALIAN BARBELL COMPANY P/L FITNESS EQUIPMENT	28/04/2017	2,088.90
EF102100	21946	RYAN'S QUALITY MEATS MEAT SUPPLIES	28/04/2017	1,225.94
EF102101	22106	INTELIFE GROUP SERVICES - DAIP	28/04/2017	13,063.16
EF102102	22119	BINDI BINDI DREAMING MARISSA VERMA CONSULT - ABORIGINAL EDUCATION/ENT	28/04/2017	1,320.00
EF102103	22182	K-LINE FENCING GROUP FENCING SERVICES	28/04/2017	17,647.30
EF102104	22337	SEGAFREDO ZANETTI AUSTRALIA PTY LTD COFFEE & COFFEE MACHINES	28/04/2017	1,648.45

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EF102105	22400	FIRE & SAFETY WA PTY LTD FIRE SAFETY EQUIPMENT	28/04/2017	214.50
EF102106	22448	CAKES WEST PTY LTD CATERING	28/04/2017	156.82
EF102107	22553	BROWNES FOOD OPERATIONS CATERING SUPPLIES	28/04/2017	1,950.55
EF102108	22569	SONIC HEALTH PLUS PTY LTD MEDICAL SERVICES	28/04/2017	3,569.50
EF102109	22589	JB HI FI - COCKBURN ELECTRICAL EQUIPMENT	28/04/2017	2,229.00
EF102110	22613	VICKI ROYANS ARTISTIC SERVICES	28/04/2017	450.00
EF102111	22619	KSC TRAINING TRAINING SERVICES	28/04/2017	1,485.00
EF102112	22624	AUSSIE EARTHWORKS PTY LTD EARTHWORKS	28/04/2017	2,028.00
EF102113	22682	BEAVER TREE SERVICES PTY LTD TREE PRUNING SERVICES	28/04/2017	29,246.11
EF102114	22805	COVS PARTS PTY LTD MOTOR PARTS	28/04/2017	2,140.06
EF102115	22806	PUMA ENERGY (AUSTRALIA) FUELS PTY LTD FUEL SUPPLIES	28/04/2017	66,246.85
EF102116	22903	UNIQUE INTERNATIONAL RECOVERIES LLC DEBT COLLECTORS	28/04/2017	460.80
EF102117	22913	AUSTRALIAN OFFICE LEADING BRANDS.COM ENVELOPES	28/04/2017	3,005.86
EF102118	22949	TASHANA LEE TWEDDLE HEAVENSTRINGS ENTERTAINMENT SERVICES - MUSIC	28/04/2017	1,010.00
EF102119	23213	SPOTLESS FACILITY SERVICES PTY LTD (LA LAUNDRY SERVICES	28/04/2017	247.90
EF102120	23253	KOTT GUNNING LEGAL SERVICES	28/04/2017	16,848.66
EF102121	23254	IBIS INFORMATION SYSTEMS PTY LTD COMPUTER SOFTWARE	28/04/2017	9,334.85
EF102122	23351	COCKBURN GP SUPER CLINIC LIMITED T/A LEASING FEES	28/04/2017	7,500.26
EF102123	23450	CLEVER DESIGNS UNIFORMS	28/04/2017	1,044.50
EF102124	23457	TOTALLY WORK WEAR FREMANTLE CLOTHING - UNIFORMS	28/04/2017	1,893.15
EF102125	23550	HENRICKS CONSULTING PTY LTD CONSULTANCY SERVICES - HUMAN RESOURCES	28/04/2017	715.00
EF102126	23570	A PROUD LANDMARK PTY LTD LANDSCAPE CONTRUCTION SERVICES	28/04/2017	37,522.29
EF102127	23579	DAIMLER TRUCKS PERTH PURCHASE OF NEW TRUCK	28/04/2017	275.00
EF102128	23685	ASTRO SYNTHETIC TURF PTY LTD SITE INSPECTIONS	28/04/2017	1,391.50
EF102129	23849	JCB CONSTRUCTION EQUIPMENT AUSTRALI PLANT/MACHINERY	28/04/2017	2,380.60
EF102130	23854	FRATELLE GROUP PTY LTD ARCHITECTUAL SERVICES - VISKO PARK FACILITY	28/04/2017	112,574.00
EF102131	23858	SPECIALISED SECURITY SHREDDING DOCUMENT DESTRUCTION SERVICES	28/04/2017	50.60
EF102132	23971	FIND WISE LOCATION SERVICES LOCATING SERVICES - UNDERGROUND	28/04/2017	4,140.40
EF102133	23974	CONCEPT AUDIO VISUAL (NSW) TRUST AUDIO VISUAL PRODUCTION SERVICES	28/04/2017	649.00

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EF102134	24156	MASTEC AUSTRALIA PTY LTD PURCHASE OF NEW BINS	28/04/2017	41,883.60
EF102135	24183	WELLARD GLASS GLASS REPAIR SERVICES	28/04/2017	5,063.30
EF102136	24275	TRUCK CENTRE WA PTY LTD PURCHASE OF NEW TRUCK	28/04/2017	377.04
EF102137	24430	DOCTOR HOME CAR DOCTOR CARE	28/04/2017	450.00
EF102138	24506	AMARANTI'S PERSONAL TRAINING PERSONAL TRAINING SERVICES	28/04/2017	300.00
EF102139	24508	REBECCA FLANAGAN EDUCATIONAL MUSICAL LESSONS	28/04/2017	600.00
EF102140	24524	CALO HEALTH HEARTMOVE CLASSES	28/04/2017	3,522.50
EF102141	24527	AUSTRALIAN ASSOCIATION FOR ENVIRONMI COURSE REGISTRATION	28/04/2017	572.00
EF102142	24595	CONTEMPORARY IMAGE PHOTOGRAPHY PT PHOTOGRAPHY SERVICES	28/04/2017	3,190.00
EF102143	24599	POOLWERX SPEARWOOD ANALYTICAL SERVICES	28/04/2017	1,959.95
EF102144	24655	AUTOMASTERS SPEARWOOD VEHICLE SERVICING	28/04/2017	4,458.00
EF102145	24724	QUALITY MARINE COATING SYSTEMS P/L CLEANING SERVICES - ROAD SURFACES	28/04/2017	2,860.00
EF102146	24736	ZENIEN CCTV CAMERAS & INSTALLATION	28/04/2017	34,212.04
EF102147	24748	PEARMANS ELECTRICAL & MECHANICAL SE ELECTRICAL SERVICES	28/04/2017	13,734.16
EF102148	24886	A NATURAL SELF ENTERTAINMENT SUPPLIES	28/04/2017	337.00
EF102149	24902	CRIMSON WOLF FINE ART ARTISTIC SERVICES	28/04/2017	1,830.00
EF102150	24945	NS PROJECTS PTY LTD PROJECT MANAGEMENT SERVICES	28/04/2017	14,850.00
EF102151	24949	BITUMEN SURFACING THE TRUSTEE FOR CO BITUMEN SUPPLIES	28/04/2017	2,097.15
EF102152	24959	PERTH TEMPORARY AIRBRUSH TATTOOS ENTERTAINMENT SERVICES	28/04/2017	300.00
EF102153	24974	SCOTT PRINT PRINTING SERVICES	28/04/2017	891.00
EF102154	24976	SNAP PRINTING - COCKBURN CENTRAL PRINTING SERVICES	28/04/2017	803.16
EF102155	25060	DFP RECRUITMENT SERVICES EMPLOYMENT SERVICES	28/04/2017	6,452.27
EF102156	25063	SUPERIOR PAK PTY LTD VEHICLE MAINTENANCE	28/04/2017	11,249.00
EF102157	25092	LINKS MODULAR SOLUTIONS PTY LTD COMPUTER SOFTWARE	28/04/2017	28,930.00
EF102158	25102	FREMANTLE MOBILE WELDING WELDING SERVICES	28/04/2017	1,859.00
EF102159	25115	FIGG INVESTMENT MANAGEMENT SERVICES	28/04/2017	2,750.00
EF102160	25121	IMAGESOURCE DIGITAL SOLUTIONS BILLBOARDS	28/04/2017	8,352.61
EF102161	25128	HORIZON WEST LANDSCAPE & IRRIGATION LANDSCAPING SERVICES	28/04/2017	3,787.39
EF102162	25263	SYSTEM MAINTENANCE SEWERAGE PUMP MAINTENANCE	28/04/2017	515.79

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EF102163	25264	ACURIX NETWORKS PTY LTD WIFI ACCESS SERVICE	28/04/2017	1,615.90
EF102164	25418	CS LEGAL LEGAL SERVICES	28/04/2017	57,031.80
EF102165	25586	ENVIROVAP PTY LTD HIRE OF LEACHATE UNITS	28/04/2017	24,557.50
EF102166	25645	YELAKITJ MOORT NYUNGAR ASSOCIATION I WELCOME TO THE COUNTRY PERFORMANCES	28/04/2017	900.00
EF102167	25713	DISCUS ON DEMAND THE TRUSTEE FOR DIS PRINTING SERVICES	28/04/2017	5,133.55
EF102168	25733	MIRACLE RECREATION EQUIPMENT PLAYGROUND INSTALLATION / REPAIRS	28/04/2017	5,368.88
EF102169	25795	DEPARTMENT OF LANDS TRANSFER OF LAND - LOT 2718 BENEDICK RD	28/04/2017	18,000.00
EF102170	25813	LGCONNECT PTY LTD SOFTWARE DEVELOPMENT CONSULTANCY	28/04/2017	23,760.00
EF102171	25822	FIT2WORK.COM.AU MERCURY SEARCH AND EMPLOYEE CHECK	28/04/2017	37.18
EF102172	25832	EXTERIA STREET AND PARK INFRASTRUCTURE	28/04/2017	23,463.00
EF102173	25940	LEAF BEAN MACHINE COFFEE BEAN SUPPLY	28/04/2017	200.00
EF102174	25962	ALL LINES LINEMARKING SERVICES	28/04/2017	7,920.00
EF102175	26029	AUTOSWEEP WA SWEEPING SERVICES	28/04/2017	3,707.00
EF102176	26067	SPRAYKING WA PTY LTD CHEMICAL WEED CONTROL SERVICES	28/04/2017	3,685.00
EF102177	26090	FREMANTLE MILK DISTRIBUTORS MILK DISTRIBUTORS	28/04/2017	56.50
EF102178	26110	DASH CIVIL CONTRACTING CONCRETING SERVICES	28/04/2017	1,458.93
EF102179	26114	GRACE RECORDS MANAGEMENT RECORDS MANAGEMENT SERVICES	28/04/2017	1,448.44
EF102180	26160	CORROSION CONTROL ENGINEERING (WA) P CONSULTANCY SERVICES	28/04/2017	2,614.00
EF102181	26173	SOUTHSIDE PLUMBING PLUMBING SERVICES	28/04/2017	7,982.07
EF102182	26195	PLAY CHECK CONSULTING SERVICES	28/04/2017	330.00
EF102183	26257	PAPERBARK TECHNOLOGIES ARBORICULTURAL CONSULTANCY SERVICES	28/04/2017	3,875.00
EF102184	26303	GECKO CONTRACTING TURF & LANDSCAPE TURF & LANDSCAPE MAINTENANCE	28/04/2017	120,994.50
EF102185	26314	CPE GROUP TEMPORARY EMPLOYMENT SERVICES	28/04/2017	3,089.06
EF102186	26323	AT THE KITCHEN CATERING SERVICES	28/04/2017	7,410.00
EF102187	26330	KENNARDS HIRE - BIBRA LAKE EQUIPMENT HIRE	28/04/2017	2,603.00
EF102188	26354	ELECTROFEN REPAIR SERVICES - SECURITY FENCES	28/04/2017	308.55
EF102189	26359	WILSON SECURITY SECURITY SERVICES	28/04/2017	8,997.84
EF102190	26369	ALL RETAINING SYSTEMS CONSTRUCTION SERVICES	28/04/2017	5,390.00
EF102191	26382	RANGS GRAPHICS AND DESIGN SOFTWARE LICENCES	28/04/2017	33.00

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EF102192	26386	AIRMASTER AUSTRALIA PTY LTD AIRCONDITIONING MAINTENANCE SERV	28/04/2017	694.75
EF102193	26403	CHES POWER GROUP ENGG SOLUTIONS / BACK UP GENERATO	28/04/2017	1,295.10
EF102194	26416	COOLBELLUP NEWSAGENCY THE TRUSTEE I NEWSPAPER DELIVERY SERVICES	28/04/2017	1,384.25
EF102195	26418	INTEGRANET TECHNOLOGY GROUP PTY LTD ICT CONSULTANCY SERVICES	28/04/2017	10,868.00
EF102196	26423	ALPHA PEST ANIMAL SOLUTIONS INVASIVE PEST CONTROL SERVICES	28/04/2017	1,870.00
EF102197	26442	BULLANT SECURITY PTY LTD KEY WEST LOC LOCKSMITH & SECRUITY SERVICES	28/04/2017	1,436.81
EF102198	26460	KISS PHOTOBOOTHS PHOTOBOOTH HIRE	28/04/2017	825.00
EF102199	26461	777 MAINTENANCE PTY LTD MAINTENANCE SERVICES	28/04/2017	5,544.00
EF102200	26470	SCP CONSERVATION AND LAND MANAGEME FENCING SERVICES	28/04/2017	7,736.30
EF102201	26480	MATTRESS REMOVAL WA MATRESS REMOVAL SERVICES	28/04/2017	6,819.60
EF102202	26549	SHARON GREGORY (KOORT-KADAK CONSUL CONSULTANCY SERVICES	28/04/2017	100.00
EF102203	26550	GAME VAULT PTY LTD AMUSEMENT SERVICES	28/04/2017	1,200.00
EF102204	26568	UNITED DIAMOND TOOLS DIAMOND CUTTING TOOLS	28/04/2017	1,400.00
EF102205	26574	EVA BELLYDANCE ENTERTAINMENT - BELLY DANCING	28/04/2017	300.00
EF102206	26586	WA TEMPORARY FENCING SUPPLIES FENCING - TEMPORARY	28/04/2017	346.50
EF102207	26603	3E CONSULTING ENGINEERS PTY LTD ELECTRICAL CONSULTANCY SERVICES	28/04/2017	880.00
EF102208	26606	ENVIRO INFRASTRUCTURE PTY LTD CONSTRUCTION & FABRICATION	28/04/2017	32,006.87
EF102209	26610	TRACC CIVIL PTY LTD CIVIL CONSTRUCTION - BERRIGAN DRV	28/04/2017	879,159.00
EF102210	26613	AVE BIN AND BBQ CLEANING PTY LTD CLEANAING SERVICES (BBQ - BINS)	28/04/2017	1,784.25
EF102211	26614	MARKETFORCE PTY LTD ADVERTISING	28/04/2017	7,951.41
EF102212	26619	SPEARWOOD NEWS DELIVERY NEWSPAPER DELIVERY	28/04/2017	320.30
EF102213	26620	GRA EVERINGHAM PTY LTD CONSULTING/ADVISORY	28/04/2017	5,225.00
EF102214	26625	ANDOVER DETAILERS CAR DETAILING SERVICES	28/04/2017	1,259.02
EF102215	26626	SEVERSA PTY LTD ENVIRONMENTAL AUDITING	28/04/2017	5,445.00
EF102216	26639	SAFEGUARD INDUSTRIES SECURITY DOORS, SCREENS & ROLLER SHUTT	28/04/2017	700.00
EF102217	26647	MULTIPLEX CONSTRUCTIONS PTY LTD BUILDING CONST. - COCKBURN ARC	28/04/2017	306,182.26
EF102218	26649	KUBED MEDIA HIRE OF ADVERTISING LED SCREENS	28/04/2017	792.00
EF102219	26663	PERTH KIDS PARTY HIRE CHILDRENS PARTY HIRE	28/04/2017	330.00
EF102220	26667	TANGELO CREATIVE GRAPHIC DESIGN	28/04/2017	968.00

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EF102221	26669	TURNER DESIGN PTY LTD GRAPHIC DESIGN CONSULTANCY	28/04/2017	3,179.00
EF102222	26709	TALIS CONSULTANTS PTY LTD WASTE CONSULTANCY	28/04/2017	3,971.00
EF102223	26721	QUAD SERVICES PTY LTD CLEANING SERVICES	28/04/2017	19,598.64
EF102224	26731	OLD MACDONALDS TRAVELLING FARMS WA AMUSEMENT - ANIMAL FARM	28/04/2017	550.00
EF102225	26739	KERB DOCTOR KERB MAINTENANCE	28/04/2017	8,665.25
EF102226	26743	STATEWIDE TURF SERVICES TURF RENOVATION	28/04/2017	21,571.60
EF102227	26746	MOWER CITY LAWN MAINTENANCE	28/04/2017	3,850.50
EF102228	26747	BELL-VISTA FRUIT & VEG FRUIT AND VEGETABLES.	28/04/2017	634.50
EF102229	26750	KLEENIT PTY LTD GRAFFITTI REMOVAL	28/04/2017	3,558.50
EF102230	26757	INCREDIBLE CREATURES MOBILE FARM BRINGING ANIMALS TO SHOWS FOR PUBLIC INT	28/04/2017	655.00
EF102231	26759	METRO FILTERS CANOPY, FLUE AND FANS CLEANING AND FILTE	28/04/2017	481.80
EF102232	26765	GLOBAL UNMANNED SYSTEMS PTY LTD DRONE MEDIA/DATA SERVICES	28/04/2017	1,100.00
EF102233	26770	LAKESIDE MOWERS & MOTORCYCLES REPAIRS/MAINTENANCE SERVICES	28/04/2017	934.00
EF102234	26774	NATURALISTE LAND SURVEYS SURVEYING SERVICES	28/04/2017	22,426.80
EF102235	26780	METROPOLITAN OMNIBUS COMPANY BUS HIRE	28/04/2017	462.00
EF102236	26786	NUTURF HERBICIDE PRODUCTS	28/04/2017	15,730.00
EF102237	26791	MONSTERBALL AMUSEMENT & HIRE AMUSEMENT HIRE	28/04/2017	590.00
EF102238	26797	DONEGAN ENTERPRISES PTY LTD INSTALL PARK FURNITURE	28/04/2017	1,815.00
EF102239	26811	ROMERI MOTOR TRIMMERS UPHOLSTERY REPAIR	28/04/2017	440.00
EF102240	26818	THE MAZE AMUSEMENT	28/04/2017	535.00
EF102241	26819	JANDAKOT EARTHMOVING & RURAL CONTR HIRE SERVICES - EARTHMOVING EQUIPMENT	28/04/2017	2,244.00
EF102242	26823	THE FREEDOM FAIRIES AMUSEMENT	28/04/2017	825.00
EF102243	26824	WEB KEY IT PTY LTD WEBSITE CONSULTANCY	28/04/2017	1,815.00
EF102244	26826	PINDAN CONTRACTING PTY LTD BUILDING CONST. SERV. - OP. CENTRE	28/04/2017	500,970.85
EF102245	26830	ECO EATS CATERING CATERING	28/04/2017	492.70
EF102246	26831	AFL SPORTS READY LTD EDUCATION & TRAINING	28/04/2017	1,647.64
EF102247	26833	DRILLING CONTRACTORS OF AUSTRALIA DRILLING SERVICES - COCKBURN ARC	28/04/2017	603,894.39
EF102248	26843	ERGOLINK ERGONOMIC OFFICE FURNITURE	28/04/2017	670.91
EF102249	26854	IFAP TRAINING	28/04/2017	1,335.00

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EF102250	26855	MKDC INTERIOR DESIGN SERVICES	28/04/2017	17,050.00
EF102251	26858	BIKE DR BICYCLE ITEMS	28/04/2017	594.00
EF102252	26888	MEDIA ENGINE GRAPHIC DESIGN, MARKETING, VIDEO PRODUCT	28/04/2017	14,410.00
EF102253	26889	AE SMITH SERVICE PTY LTD PAINTING SERVICES	28/04/2017	2,277.00
EF102254	26893	THE BASKETBALL MAN SPORTING GOODS	28/04/2017	1,250.70
EF102255	26898	SPANDEX ASIA PACIFIC PTY LTD SIGNAGE SUPPLIER	28/04/2017	4,259.35
EF102256	26901	ALYKA PTY LTD DIGITAL CONSULTANCY AND WEB DEVELOPMENT	28/04/2017	19,083.90
EF102257	26914	NATURE CALLS PORTABLE TOILETS SUPPLY OF PORTABLE TOILETS	28/04/2017	231.00
EF102258	26917	CIRRUS NETWORKS PTY LTD IT SERVICES	28/04/2017	3,187.79
EF102259	26921	JULIET COGHLAN - PHOTOGRAPHER PHOTOGRAPHY SERVICES	28/04/2017	640.00
EF102260	26928	PATHTECH PTY LTD SCIENTIFIC & DRUG TESTING EQUIPMENT	28/04/2017	3,019.50
EF102261	26929	ELAN ENERGY MATRIX PTY LTD RECYCLING SERVICES	28/04/2017	518.63
EF102262	26935	IRRIGATION WA PTY LTD IRRIGATION SERVICES	28/04/2017	755.70
EF102263	26946	AV TRUCK SERVICES PTY LTD TRUCK SERVICING	28/04/2017	5,414.39
EF102264	26953	ROCK AND ROLL MOUNTAIN BIKING MOUNTAIN BIKE TOURS	28/04/2017	765.00
EF102265	26957	JBS & G AUSTRALIA PTY LTD CONSULTANCY - ENVIRONMENTAL	28/04/2017	8,666.46
EF102266	26964	SOUTH METROPOLITAN TAFE EDUCATION	28/04/2017	357.50
EF102267	26965	END OF LIFE CARE ARTISTIC	28/04/2017	730.00
EF102268	26968	THE DISTRIBUTORS PERTH CONFECTIONARY WHOLESALER	28/04/2017	741.05
EF102269	26970	FARHANA AZ CLEAK COMM. CAMPAIGN MNGMT/WEBSITE DEV.	28/04/2017	8,400.00
EF102270	26983	HITECH SPORTS PTY LTD SPORTING EQUIPMENT	28/04/2017	8,453.50
EF102271	26985	ACCESS ICON PTY LTD DRAINAGE PRODUCTS	28/04/2017	500.50
EF102272	26987	CTI RISK MANAGEMENT SECURITY - CASH COLLECTION	28/04/2017	3,861.00
EF102273	26988	BLADON WA PTY LTD PROMOTIONAL PRODUCTS	28/04/2017	1,969.00
EF102274	26998	PEROCIN PTY LTD (T/AS LANGE CONSULTING) CONSULTANCY - SOFTWARE	28/04/2017	13,924.00
EF102275	27006	BIBRA LAKE IGA XPRESS LIQUOR SUPPLIES	28/04/2017	1,299.22
EF102276	27010	QUANTUM BUILDING SERVICES PTY LTD BUILDING MAINTENANCE	28/04/2017	12,870.04
EF102277	27012	SPECIALTY CURTAINS & BLINDS CURTAINS/BLINDS - COCKBURN ARC	28/04/2017	52,213.70
EF102278	27015	INTELLI TRAC GPS TRACKING	28/04/2017	132.00

CITY OF COCKBURN
MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF102279	27020	TENDERLINK.COM INTERNET ACESS SERVICES	28/04/2017	330.00
EF102280	27023	SOLARGAIN PV PTY LTD SOLAR PV - COCKBURN ARC	28/04/2017	543,351.60
EF102281	27024	KYOKO YOSHIDA SHIKISAI JAPANESE COOK TRAINING/INSTRUCTION - COOKING	28/04/2017	480.00
EF102282	27026	FIRST ACTION PTY LTD EMERGENCY MANAGEMENT SOLUTIONS	28/04/2017	4,853.75
EF102283	27027	FRIG TECH WA REFRIDGERATION SERVICES	28/04/2017	3,340.98
EF102284	27030	WAGNERS CFT MANUFACTURING PTY LTD COOGEE DIVE TRAIL STAIRS-DESIGN & CONSTRUCTION	28/04/2017	55,165.00
EF102285	27031	DOWNER EDI WORKS PTY LTD ASPHALT SERVICES	28/04/2017	1,330.97
EF102286	27034	ADELBY PTY LTD FIREBREAK CONSTRUCTION	28/04/2017	27,920.20
EF102287	27044	GRAFFITI SYSTEMS AUSTRALIA GRAFFITI REMOVAL & ANTI-GRAFFITI COATING	28/04/2017	9,397.12
EF102288	27046	TFH HIRE SERVICES PTY LTD HIRE FENCING	28/04/2017	780.45
EF102289	27050	ROB CARR PTY LTD CONSTRUCTION - SEWER MAIN TUNNELLING	28/04/2017	343,231.09
EF102290	27052	EVENT MARQUEES MARQUEE HIRE	28/04/2017	1,979.00
EF102291	27060	CANTERBURY GROUP PTY LTD OFFICE FURNITURE - COCKBURN ARC	28/04/2017	84,463.50
EF102292	27064	COMMUNITY FIRST INTERNATIONAL DISABILITY SERVICES	28/04/2017	2,497.00
EF102293	27065	WESTBOOKS BOOKS	28/04/2017	284.34
EF102294	27069	HART SPORT SPORTS EQUIPMENT	28/04/2017	11,663.30
EF102295	27082	KULBARDI PTY LTD STATIONERY SUPPLIES	28/04/2017	1,437.70
EF102296	27087	TRANSKARD PTY LTD STORAGE SERVICES	28/04/2017	487.14
EF102297	27093	MAGNETIC AUTOMATION PTY LTD GATES/BARRIERS	28/04/2017	3,168.00
EF102298	27094	ABACUS CASH SYSTEMS PTY LTD CASH HANDLING	28/04/2017	1,507.00
EF102299	27097	GLOBAL TRADE SALES SULLAGE PUMP UNIT - PORT COOGEE MARINA	28/04/2017	37,620.00
EF102300	27098	Q2 (Q-SQUARED) DIGITAL DATA SERVICE	28/04/2017	3,520.00
EF102301	27100	SEA JEWELS SWIMWEAR SALES STOCK - COCKBURN ARC	28/04/2017	2,264.75
EF102302	27102	CAMERA STORY LTD TRAINING PHOTOGRAPHY	28/04/2017	1,280.00
EF102303	27105	KBEST MARINE PTY LTD FABRICATION SERVICES - HANDRAIL COCKBURN ARC	28/04/2017	43,665.05
EF102304	27106	DAVE LANFEAR CONSULTING CONSULTANCY - SPORT & LEISURE	28/04/2017	6,600.00
EF102305	27109	MINC SERVICES WA FACILITIES MANAGEMENT	28/04/2017	5,292.27
EF102306	27110	RISKWEST MANAGEMENT CONSULTANT	28/04/2017	1,879.00
EF102307	27111	METALWORK WA PTY LTD STEEL FABRICATION	28/04/2017	8,591.00

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EF102308	27116	JSP SOLUTIONS PTY LTD AQUATIC EQUIPMENT	28/04/2017	1,348.00
EF102309	27118	GRIFFITHS ENGINEERING PTY LTD ENGINEERING	28/04/2017	2,200.00
EF102310	27123	THE END STOP ARTISTIC	28/04/2017	1,100.00
EF102311	27132	WILMA SCENINI TRAINING & INSTRUCTOR	28/04/2017	450.00
EF102312	27141	ENGINEERED WATER SYSTEMS MARINE CONSTRUCTION SERVICES	28/04/2017	300.00
EF102313	27151	DAVID PIGRAM ENTERTAINMENT - MUSICAL PERFORMANCE	28/04/2017	800.00
EF102314	99996	LANDSCAPES WA PTY LTD RATES REFUND	28/04/2017	147.00
EF102315	99996	IBRAHIM MHANNA RATES REFUND	28/04/2017	295.00
EF102316	99996	DALLA RIVA (AUST) PTY LTD RATES REFUND	28/04/2017	15,783.87
EF102317	99996	PAUL AND TZUDY WEIR RATES REFUND	28/04/2017	295.00
EF102318	99996	ZUBIAS THREADING RATES REFUND	28/04/2017	625.00
EF102319	99996	MICHAEL SPARTALIS RATES REFUND	28/04/2017	30.00
EF102320	99996	JANET ALISON BACKSHALL RATES REFUND	28/04/2017	154.63
EF102321	99996	SEQUIN HOLDINGS & WR DL MORLING RATES REFUND	28/04/2017	1,564.81
EF102322	99996	LINDA HACKETT RATES REFUND	28/04/2017	1,419.15
EF102323	99996	KIM HENNESSY RATES REFUND	28/04/2017	1,380.00
EF102324	99996	COMMUNITY HOUSING LTD RATES REFUND	28/04/2017	2,233.21
EF102325	99996	DAWN HELLOISE JOHNS RATES REFUND	28/04/2017	809.48
EF102326	99996	CARLIN TEAM RATES REFUND	28/04/2017	511.00
EF102327	99996	STRAND LEGAL & CONVEYANCING RATES REFUND	28/04/2017	161.21
EF102328	99996	STOCK ROAD LAND PTY LTD RATES REFUND	28/04/2017	72.86
EF102329	99996	STOCK ROAD LAND PTY LTD RATES REFUND	28/04/2017	72.86
EF102330	99996	STOCK ROAD LAND PTY LTD RATES REFUND	28/04/2017	72.86
EF102331	99996	STOCK ROAD LAND PTY LTD RATES REFUND	28/04/2017	72.86
EF102332	99996	STOCK ROAD LAND PTY LTD RATES REFUND	28/04/2017	72.86
EF102333	99996	DW SPENCER RATES REFUND	28/04/2017	107.39
EF102334	99996	JEREMY LEIGH BROOKS RATES REFUND	28/04/2017	1,079.72
EF102335	99996	STOCKLAND WA DEVELOPMENT PTY LTD RATES REFUND	28/04/2017	17,463.46
EF102336	99996	J CORP PTY LTD T/A IMPRESSIONS RATES REFUND	28/04/2017	872.73

CITY OF COCKBURN
MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF102337	99997	BRUCE BENNETT CROSSOVER CONTRIBUTION - BRUCE BENNETT	28/04/2017	300.00
EF102338	99997	ST JEROME'S NETBALL CLUB KIDSPORT INV KS007909 ALIVOJVODIC	28/04/2017	200.00
EF102339	99997	HAMMOND PARK JUNIOR FOOTBALL CLUB I KIDSPORT KS007983	28/04/2017	1,110.00
EF102340	99997	CANNING COSMOS JUNIOR SOCCER CLUB KIDSPORT KS007625	28/04/2017	400.00
EF102341	99997	CAREY UNITED SOCCER CLUB KIDSPORT KS007987	28/04/2017	220.00
EF102342	99997	WARNBRO STRIKERS JUNIOR SOCCER CLUB KIDSPORT KS007943	28/04/2017	195.00
EF102343	99997	CANNING VALE JUNIOR FOOTBALL CLUB KIDSPORT KS007972	28/04/2017	200.00
EF102344	99997	ROBERTA BUNCE REIMBURSEMENT FOR LUNCH	28/04/2017	92.00
EF102345	99997	ALAN M HANCOCK WRITING COURSE, SPEARWOOD LIBRARY	28/04/2017	1,700.00
EF102346	99997	LINDA METZ EMPLOYEE OF THE YR17 TRAVEL PRIZE - L	28/04/2017	1,847.63
EF102347	99997	L M FRAYNE BIRD BATH REBATS - FRAYNE	28/04/2017	41.50
EF102348	99997	GREGORY KAZMER REIMBURSEMENT SAFETY GLASSES - G KAZMER	28/04/2017	300.00
EF102349	99997	COMMUNITY AND CULTURAL COUNCIL FREMANTLE PORTS COCKBURN COMMUNITY PROJE	28/04/2017	2,000.00
EF102350	99997	COOGEE BEACH PROGRESS ASSOCIATION DELEGATED AUTHORITY LGACS7	28/04/2017	266.50
EF102351	99997	JESSIE JOHNSON COMPOST BIN REBATE - J JOHNSON	28/04/2017	50.00
EF102352	99997	MICHAEL CONNOR COMPOST BIN REBATE - M CONNOR	28/04/2017	50.00
EF102353	99997	LAUGHTER WA INV 45 - HARMONY WEEK YOGA	28/04/2017	200.00
EF102354	99997	SOUTH PERTH JUNIOR RUGBY LEAGUE CLUB KIDSPORT INV KS008052 - SMITH	28/04/2017	320.00
EF102355	99997	PERTH BASKETBALL ASSOCIATION INC. GRANTS, DONATIONS & REFUNDS	28/04/2017	220.00
EF102356	99997	BICTON NETBALL CLUB KIDSPORT INV KS008210 MCSHANE	28/04/2017	400.00
EF102357	99997	JOANNE AND KENNELLY BRIGHT COMPOST BIN REBATE REQUEST FOR PAYMENT	28/04/2017	50.00
EF102358	99997	THEODORE SPRENNELS COMPOST BIN REBATE 4 MORTON ROAD	28/04/2017	50.00
EF102359	99997	AGELINK THEATRE INC. PERFORMANCE OF MEMORIES ARE MADE OF THIS	28/04/2017	700.00
EF102360	99997	WINNACOTT KATS JUNIOR FOOTBALL CLUB KIDSPORT INV KS008361 DELER	28/04/2017	200.00
EF102361	99997	COCKBURN JUNIOR FOOTBALL CLUB KIDSPORT INV KS008294 SPRAGG X 2	28/04/2017	400.00
EF102362	99997	KICKI KITCHEN CAT STERILISATION SUBSIDY - BORIS	28/04/2017	50.00
EF102363	99997	EPILEPSY ASSOCIATION EPILEPSY ASSOCIATION FOR PURPLE DAY	28/04/2017	393.40
EF102364	99997	KUMAR VASINDA COMARA CROSS-OVER CONTRIBUTION: K. VASINDA COMA	28/04/2017	300.00
EF102365	99997	ROBYN ELAINE BOASE CROSS-OVER CONTRIBUTION - R. BOASE	28/04/2017	300.00

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Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF102366	99997	MR & MRS SLAUGHTER CROSS OVER CONTRIBUTION	28/04/2017	300.00
EF102367	99997	MS JENNIFER SMITH CROSS-OVER CONTRIBUTION	28/04/2017	300.00
EF102368	99997	ALLAN JOHN ALDWORTH CROSSOVER CONTRIBUTION	28/04/2017	300.00
EF102369	99997	ANA PEREIRA CROSSOVER CONTRIBUTION	28/04/2017	300.00
EF102370	99997	COCKBURN JUNIOR FOOTBALL CLUB KID SPORT INV:KS008624	28/04/2017	200.00
EF102371	99997	PALMYRA REBELS NETBALL CLUB KID SPORT INV:KS008547	28/04/2017	200.00
EF102372	99997	ATWELL NETBALL CLUB KID SPORT INV:KS008611	28/04/2017	200.00
EF102373	99997	COCKBURN JUNIOR FOOTBALL CLUB INC. KID SPORT INV:KS008622	28/04/2017	1,000.00
EF102374	99997	WINNACOTT KATS JUNIOR FOOTBALL CLUB KID SPORT INV:KS008417	28/04/2017	200.00
EF102375	99997	WINNACOTT KATS JUNIOR FOOTBALL CLUB KID SPORT INV:KS008418	28/04/2017	200.00
EF102376	99997	DIAMONDS NETBALL CLUB WA INC KID SPORT INV:KS008433	28/04/2017	400.00
EF102377	99997	COCKBURN JUNIOR FOOTBALL CLUB INC. KID SPORT INV:KS008455	28/04/2017	200.00
EF102378	99997	DIAMONDS NETBALL CLUB WA INC KID SPORT INV: KS006647	28/04/2017	200.00
EF102379	99997	BICTON NETBALL CLUB INC KID SPORT INV:21844	28/04/2017	200.00
EF102380	99997	SOUTH LAKE DOLPHINS SWIMMING CLUB KID SPORT INV:KS007527	28/04/2017	200.00
EF102381	99997	LIAM THOMPSON FAMILY DAY EDUCATOR - JESSICA LUDVIGSEN	28/04/2017	279.20
EF102382	99997	SOUTH COOGEE VOLUNTEER BUSH FIRE BR REIMBURSEMENT - 07/04/2017	28/04/2017	19,059.51
EF102383	99997	LINDA MCLARNON FAMILY DAY CARE SUBSIDY	28/04/2017	81.98
EF102384	99997	LIAM THOMPSON FAMILY DAY CARE SUBSIDY	28/04/2017	332.88
EF102385	99997	CATHERINE WHITTEN HELLO BABY EXPO MANNING PARK	28/04/2017	175.00
EF102386	99997	NICOLE DELAMOTTE INVOICE 29317	28/04/2017	175.00
EF102387	99997	JOE SIMICH COOLBELLUP SKATEPARK DAY FESTIVAL	28/04/2017	300.00
EF102388	99997	CATIA DOLZADELLI ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	350.00
EF102389	99997	MARIE FOSTER ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	420.00
EF102390	99997	SANDY GASKETT ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	230.00
EF102391	99997	PAT GILBERT ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	375.00
EF102392	99997	CARL HAANAPPEL ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	550.00
EF102393	99997	JANE HITCHCOCK ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	275.00
EF102394	99997	WARREN IANELLO ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	45.00

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Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF102395	99997	MARGARET KENT ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	150.00
EF102396	99997	ALIA LEADEBRAND ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	285.00
EF102397	99997	ELLA MACNISH ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	200.00
EF102398	99997	JANE MARTIN ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	3,000.00
EF102399	99997	JUPITER MCKENZIE ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	85.00
EF102400	99997	ALANA MCNEE ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	600.00
EF102401	99997	PEREE MEHMET ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	170.00
EF102402	99997	TRESA MUNDRABY ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	350.00
EF102403	99997	HAZEL O'CATHAIL ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	300.00
EF102404	99997	ANA POZKANOVIC ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	250.00
EF102405	99997	SANDY ROBERTSON ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	800.00
EF102406	99997	JILL SPARGO ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	75.00
EF102407	99997	NEDA STARAC ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	350.00
EF102408	99997	SUZANNE VALENTE ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	220.00
EF102409	99997	SALLY ANN WIDLAKE ARTIST PAYMENT FROM EXHIBITION SALES	28/04/2017	60.00
EF102410	99997	BENJAMIN BOYD BENJAMIN BOYD DJ SERVICE	28/04/2017	200.00
EF102411	99997	JANDAKOT TROTTER TRAINING CENTRE JO MCWG - REPLACEMENT OF STOLEN GATE MOTOR	28/04/2017	669.99
EF102412	99997	BEELIAR SPIRIT AFC KID SPORT INV-KS008312	28/04/2017	200.00
EF102413	99997	EAST FREMANTLE JUNIOR FOOTBALL CLUB KID SPORT NV-KS008802	28/04/2017	200.00
EF102414	99997	BEELIAR SPIRIT AFC KID SPORT INV-KS008839	28/04/2017	3,730.00
EF102415	99997	COCKBURN BASKETBALL ASSOCIATION INC KIDSPORT INVOICE KS008955	28/04/2017	440.00
EF102416	99997	FREMANTLE RUGBY LEAGUE CLUB KIDSPORT INVOICE KS008957	28/04/2017	2,515.00
EF102417	99997	ALLAN ALDWORTH CAT STERILISATION SUBSIDY - WINSTON	28/04/2017	50.00
EF102418	99997	PHILIP HALL D121 SUB LEASE REFUND	28/04/2017	1,232.88
EF102419	99997	VICTOR TORDESILLAS CROSSOVER VICTOR TORDESILLAS	28/04/2017	300.00
EF102420	99997	F & S FIANNACA CROSSOVER F & S FIANNACA	28/04/2017	300.00
EF102421	99997	JOSE GONCALVES CROSSOVER JOSE GONCALVES	28/04/2017	300.00
EF102422	99997	SHAWN D'SOUZA CROSSOVER SHAWN D'SOUZA	28/04/2017	300.00
EF102423	99997	MANOJKUMAR VAIDHYA CROSSOVER MANOJKUMAR VAIDHYA	28/04/2017	300.00

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Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF102424	99997	NICHOLAS ELLIOTT CROSSOVER NICHOLAS ELLIOTT	28/04/2017	300.00
EF102425	99997	SUSAN DART CROSSOVER SUSAN DART	28/04/2017	300.00
EF102426	99997	KAREN M GANE BIRD BATH REBATE	28/04/2017	36.00
EF102427	99997	SUZANNE CONNORS GRANTS, DONATIONS & REFUNDS	28/04/2017	300.00
EF102428	99997	SOUTHERN FORCE FOOTBALL CLUB INC KIDSPORT INV KS008758 - WONG	28/04/2017	200.00
EF102429	99997	MELVILLE CITY HOCKEY CLUB INC KIDSPORT INV KS008734 - SWINTON	28/04/2017	220.00
EF102430	99997	PALMYRA REBELS NETBALL CLUB KIDSPORT INV KS008739 - GARRETT & MONTOG	28/04/2017	400.00
EF102431	99997	ROSLYN FAIRLESS GRANTS, DONATIONS & REFUNDS	28/04/2017	50.00
EF102432	99997	PAUL EMERY COUNCIL CROSSOVER CONTRIBUTOR	28/04/2017	300.00
EF102433	99997	SPEARWOOD DALMATINAC SPORT KID SPORT INV-KS008865	28/04/2017	880.00
EF102434	99997	SPEARWOOD DALMATINAC SPORT KID SPORT INV-KS008934	28/04/2017	440.00
EF102435	99997	WINNACOTT KATS JUNIOR FOOTBALL CLUB KID SPORT INV-KS008886	28/04/2017	200.00
EF102436	99997	FREMANTLE PCYC KIDS SPORT INV KS005837	28/04/2017	220.00
EF102437	99997	WINNACOTT KATS JUNIOR FOOTBALL CLUB KIDSPORT INV KS006204	28/04/2017	200.00
026947	26987	CTI RISK MANAGEMENT SECURITY - CASH COLLECTION	5/04/2017	2,180.05
026948	99999	JOHN MILLER PLANNING APPLICATION REFUND	6/04/2017	7,402.02
026949	26987	CTI RISK MANAGEMENT SECURITY - CASH COLLECTION	12/04/2017	1,490.15
026950	26987	CTI RISK MANAGEMENT SECURITY - CASH COLLECTION	19/04/2017	1,317.10
026951	26987	CTI RISK MANAGEMENT SECURITY - CASH COLLECTION	27/04/2017	1,332.65
026952	99999	PHILIP HALL BOND REFUND	28/04/2017	500.00
026953	99999	KATHERINE ROWE BOND REFUND	28/04/2017	150.00
026954	99995	JAMES McGOWAN PROPERTY REFUNDS	28/04/2017	450.00
026955	99995	BPS HOLDINGS PROPERTY REFUNDS	28/04/2017	1,478.68
026956	99995	BPS HOLDINGS PROPERTY REFUNDS	28/04/2017	2,932.02
026957	99995	GLORIA MOURVEEN HILZINGER PLANNING APPLICATION REFUND	28/04/2017	147.00
026958	99995	THE PATIO GUYS PLANNING APPLICATION REFUND	28/04/2017	147.00
026959	99995	ONE STOP PATIO SHOP PLANNING APPLICATION REFUND	28/04/2017	147.00
026960	99995	SUMMIT PROJECTS PLANNING APPLICATION REFUND	28/04/2017	640.87
026961	99995	OUTDOOR WORLD WANGARA PLANNING APPLICATION REFUND	28/04/2017	147.00

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Cheque/ EFT	Account No.	Account/Payee	Date	Value
026962	99995	OOFFICE OF STATE REVENUE PROPERTY REFUNDS	28/04/2017	750.00
026963	99995	ROBERT DORSA PEN HOLDER REFUND	28/04/2017	2,489.00
026964	99998	KATE JONES AUSTRALIA DAY AWARD WINNER	28/04/2017	50.00
026965	99998	GEORGE AZAR PURCHASE OF LAND - JANDAKOT RD WIDENING	28/04/2017	35,750.00
026966	99998	JOSEPH AZAR PURCHASE OF LAND - JANDAKOT RD WIDENING	28/04/2017	164,322.40
026967	99998	ALICIA ANNE PUNCH PURCHASE OF LAND - JANDAKOT RD WIDENING	28/04/2017	40,700.00
026968	11758	WATER CORP UTILITY ACCOUNT ONLY - PLE WATER USAGE / SUNDRY CHARGES ADD RETENTION HELD NIL	28/04/2017	64,219.39 0
		LESS PRIOR PERIOD CANCELLED CHEQUES/EFTS		
EF101276		RYAN NUTTALL		-400.00
EF101275		JACK COLLINS		-400.00
EF101277		KEVIN KALEMBER		-400.00
EF101213		INVESTWISE PROPERTY MANAGEMENT		-2,112.83
026931		JOHN MILLER		-7,402.02
EF101852		KEVIN KALEMBER		-400.00
EF101222		BRUCE BENNETT		-300.00
026540		SHIRE OF MURRAY		-375.00
026534		KATE JONES		-50.00
		PAYMENT LIST TOTAL		11,238,169.61
		TOTAL AS PER AP SOURCE 17GLACT9991000		11,238,169.61
		ADDITIONAL DIRECT PAYMENTS		
		BANK FEES		
		MERCHANT FEES COC		2,420.76
		MERCHANT FEES SLLC		966.98
		MERCHANT FEES VARIOUS OUT CENTRES		73.80
		NATIONAL BPAY CHARGE		1,950.72
		RTGS/ACLR FEE		35.50
		NAB TRANSACT FEE		4,341.20
		MERCHANDISE / OTHER FEES		9.00
		CBA CREDIT CARD PAYMENT		64,226.69
				74,024.65
		FAMILY DAY CARE AND IN HOME CARE PAYMENTS		
		FDC PAYMENTS		85,619.28
		IHC PAYMENTS		177,229.51
		PAYROLL TRANSACTIONS		
5/04/2017		COC04/04/17 PYMTID 95027026 City of Cockburn		1210494.47
7/04/2017		COC10/04/17 PYMTID 95196268 City of Cockburn		2078.09
19/04/2017		COC18/04/17 PYMTID 95664291 City of Cockburn		1198510.91
20/04/2017		COC20/04/17 PYMTID 95718790 City of Cockburn		230.38
21/04/2017		COC21/04/17 PYMTID 95817141 City of Cockburn		347.20
24/04/2017		COC24/04/17 PYMTID 95870692 City of Cockburn		2457.38
5/04/2017		COC30/03/17 PYMTID 94981496 City of Cockburn		6913.22
				2421031.65
		TOTAL PAYMENTS FOR APRIL		13,996,074.70

PAYMENT SUMMARY

CHEQUE PAYMENTS

026947 - 026968

ELECTRONIC FUNDS TRANSFER PAYMENT

EF101844 - EF102437

CANCELLED PAYMENTS

EF101276; EF101275; EF101277; EF101213; EF101852; EF101222
026931; 026540; 026534

STATEMENT OF FINANCIAL ACTIVITY

for the period ended 30 April 2017

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
Operating Revenue						
Governance	102,300,220	101,128,924	1%	1,171,296 ✓	105,193,374	104,743,874
Financial Services	757,548	649,908	17%	107,640	690,050	690,050
Information Services	3,250	1,250	160%	2,000	1,500	1,500
Human Resource Management	211,251	243,327	-13%	(32,075)	292,000	292,000
Library Services	40,161	52,866	-24%	(12,705)	55,146	55,146
Recreation & Community Safety	3,692,005	4,334,235	-15%	(642,231) ✗	5,858,503	5,648,025
Community Development & Services	7,666,864	6,949,138	10%	717,727 ✓	8,105,143	6,464,424
Corporate Communications	11,877	21,300	-44%	(9,423)	21,400	13,400
Statutory Planning	1,019,019	1,232,500	-17%	(213,481) ✗	1,497,000	1,587,000
Strategic Planning	2,452,054	2,503,663	-2%	(51,609)	3,029,906	2,955,811
Building Services	1,080,310	1,278,033	-15%	(197,723)	1,505,408	1,605,408
Environmental Health	323,783	317,417	2%	6,366	338,500	260,500
Waste Services	7,530,837	7,415,332	2%	115,506	8,393,846	9,907,593
Parks & Environmental Services	97,246	59,461	64%	37,784	59,778	1,900
Engineering Services	224,603	265,100	-15%	(40,497)	318,120	318,120
Infrastructure Services	1,207,921	1,116,852	8%	91,069	1,147,099	1,072,099
	128,618,949	127,569,306	1%	1,049,643	136,506,773	135,616,850
Total Operating Revenue	128,618,949	127,569,306	1%	1,049,643	136,506,773	135,616,850
Operating Expenditure						
Governance	(2,962,470)	(3,363,607)	-12%	401,136 ✓	(4,564,669)	(4,681,477)
Strategy & Civic Support	(1,029,445)	(1,007,994)	2%	(21,451)	(1,271,647)	(1,197,428)
Financial Services	(5,662,441)	(5,497,866)	3%	(164,575)	(6,648,821)	(6,300,165)
Information Services	(3,967,748)	(4,408,927)	-10%	441,179 ✓	(5,260,395)	(5,044,232)
Human Resource Management	(2,053,143)	(2,290,210)	-10%	237,067 ✓	(2,814,514)	(2,782,444)
Library Services	(2,738,950)	(2,919,203)	-6%	180,254	(3,571,032)	(3,508,075)
Recreation & Community Safety	(8,868,700)	(9,993,226)	-11%	1,124,526 ✓	(12,880,129)	(12,570,826)
Community Development & Services	(8,414,976)	(8,344,580)	1%	(70,396)	(10,356,419)	(8,499,849)
Corporate Communications	(2,727,746)	(2,919,220)	-7%	191,474	(3,532,220)	(3,369,861)
Statutory Planning	(1,095,979)	(1,106,843)	-1%	10,864	(1,402,004)	(1,478,763)
Strategic Planning	(1,522,809)	(1,451,656)	5%	(71,154)	(2,031,230)	(1,846,310)
Building Services	(1,291,679)	(1,250,569)	3%	(41,110)	(1,582,264)	(1,651,768)
Environmental Health	(1,347,540)	(1,469,706)	-8%	122,167	(1,834,027)	(1,807,798)
Waste Services	(16,041,179)	(16,522,046)	-3%	480,867 ✓	(20,315,923)	(20,898,986)
Parks & Environmental Services	(10,834,524)	(10,320,160)	5%	(514,365) ✗	(12,655,169)	(12,596,094)
Engineering Services	(6,383,218)	(6,428,976)	-1%	45,758	(7,943,807)	(7,795,741)
Infrastructure Services	(7,770,049)	(8,133,296)	-4%	363,246 ✓	(9,854,469)	(9,189,712)
	(84,712,597)	(87,428,085)	-3%	2,715,488	(108,518,738)	(105,219,528)

OCM 8/6/2017 - Item No. 16.2

STATEMENT OF FINANCIAL ACTIVITY

for the period ended 30 April 2017

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
Less: Net Internal Recharging	2,036,255	2,063,456	-1%	(27,201)	2,586,019	2,229,821
Add: Depreciation & Amortisation on Non-Current Assets						
Computer Equipment	(177,966)	(214,560)	-17%	36,594	(257,472)	(257,472)
Furniture and Equipment	(119,377)	(153,850)	-22%	34,473	(184,620)	(184,620)
Plant & Machinery	(2,384,274)	(2,362,021)	1%	(22,253)	(2,834,469)	(3,034,473)
Buildings	(3,526,769)	(3,655,270)	-4%	128,501	(4,464,644)	(5,175,945)
Infrastructure - Roads	(8,965,559)	(8,979,090)	0%	13,531	(10,774,908)	(11,029,308)
Infrastructure - Drainage	(2,041,520)	(2,065,740)	-1%	24,220	(2,478,888)	(2,622,888)
Infrastructure - Footpaths	(976,984)	(1,012,220)	-3%	35,236	(1,214,664)	(1,214,664)
Infrastructure - Parks Equipment	(2,993,173)	(3,052,250)	-2%	59,077	(3,662,700)	(2,834,244)
Landfill Infrastructure	(906,269)	(992,610)	-9%	86,341	(1,191,132)	(1,191,132)
Marina Infrastructure	(787,747)	(794,670)	-1%	6,923	(953,601)	-
	(22,879,638)	(23,282,281)	-2%	402,643	(28,017,098)	(27,544,746)
Total Operating Expenditure	(105,555,980)	(108,646,910)	-3%	3,090,930	(133,949,817)	(130,534,453)
Change in Net Assets Resulting from Operations	23,062,969	18,922,395	22%	4,140,573	2,556,956	5,082,396
Non-Operating Activities						
Profit/(Loss) on Assets Disposal						
Plant and Machinery	149,505	1,757,520	-91%	(1,608,015) X	540,270	14,650
Freehold Land	10,329,737	12,550,000	-18%	(2,220,263) X	12,840,000	-
Buildings	(168,330)	-	0%	(168,330)	-	-
	10,310,911	14,307,520	-28%	(3,996,609)	13,380,270	14,650
Capital Expenditure						
Computer Equipment	(545,831)	(957,102)	-43%	411,271 ✓	(1,940,259)	(484,800)
Furniture and Equipment	(915,728)	(1,776,702)	-48%	860,974 ✓	(2,989,618)	(33,808)
Plant & Machinery	(5,219,311)	(7,871,710)	-34%	2,652,399 ✓	(8,345,994)	(5,791,000)
Land	(675,271)	(1,537,973)	-56%	862,702 ✓	(1,902,797)	-
Buildings	(48,391,237)	(52,389,141)	-8%	3,997,904 ✓	(55,325,397)	(58,655,520)
Infrastructure - Roads	(12,168,181)	(16,865,950)	-28%	4,697,769 ✓	(17,949,388)	(10,865,703)
Infrastructure - Drainage	(462,076)	(1,134,097)	-59%	672,021 ✓	(1,611,168)	(1,061,475)
Infrastructure - Footpaths	(681,027)	(974,567)	-30%	293,540 ✓	(1,144,530)	(1,003,360)
Infrastructure - Parks Equipment	(5,718,124)	(7,667,777)	-25%	1,949,652 ✓	(9,456,094)	(5,642,000)
Infrastructure - Parks Landscaping	(543,936)	(1,098,495)	-50%	554,559 ✓	(1,231,086)	(970,000)
Landfill Infrastructure	(190,138)	(546,534)	-65%	356,396 ✓	(1,170,329)	(225,000)
Note 1.	(75,510,862)	(92,820,048)	-19%	17,309,186	(103,066,660)	(84,732,666)
Add: Gifted Subdivision Assets	(54,299,256)	(54,299,256)	0%	-	(54,299,256)	-
Add: Transfer to Reserves	(39,326,148)	(41,310,145)	-5%	1,983,997 ✓	(56,701,114)	(44,411,004)

STATEMENT OF FINANCIAL ACTIVITY

for the period ended 30 April 2017

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
Add Funding from						
Non-Operating Grants, Subsidies and Contributions	11,346,643	17,900,418	-37%	(6,553,775) X	19,562,833	23,245,184
Developers Contributions Plans: Cash	5,093,756	5,957,525	-14%	(863,769) X	7,081,178	11,721,922
Proceeds on Sale of Assets	12,084,083	14,307,520	-16%	(2,223,437) X	14,630,270	1,264,650
Reserves	68,421,272	74,624,664	-8%	(6,203,393) X	81,414,201	52,676,958
Gifted Subdivision Assets	54,299,256	54,299,256	0%	-	54,299,256	-
	151,245,009	167,089,383	-9%	(15,844,374)	176,987,738	88,908,714
Non-Cash/Non-Current Item Adjustments						
Depreciation on Assets	21,973,369	22,289,671	-1%	(316,302) ✓	26,825,966	26,353,614
Amortisation on Assets	906,269	992,610	-9%	(86,341)	1,191,132	1,191,132
Profit/(Loss) on Assets Disposal	(10,310,911)	(14,307,520)	-28%	3,996,609 X	(13,380,270)	(14,650)
Loan Repayments	(1,296,075)	(1,296,075)	0%	0	(2,593,138)	(2,593,138)
Non-Current Leave Provisions	447,616	-	0%	447,616 X	-	-
Deferred Pensioners Adjustment	29,731	-	0%	29,731	-	-
	11,749,999	7,678,686	53%	4,071,313	12,043,690	24,936,958
Opening Funds	9,267,511	9,267,511	0%	0	9,267,511	10,500,000
Closing Funds	36,500,134	28,836,046	27%	7,664,088	169,136	299,049
	-	-	-	-	-	-

Note 2, 3.

Notes to Statement of Financial Activity

Note 1.

Additional information on the capital works program including committed orders at end of month:

Assets Classification	Actuals	Commitments at Month End	Commitments & Actuals YTD	YTD Revised Budget	Full Year Revised Budget	Uncommitted at Month End
	\$	\$			\$	\$
Computer Equipment	(545,831)	(616,835)	(1,162,667)	(957,102)	(1,940,259)	777,592
Furniture and Equipment	(915,728)	(461,641)	(1,377,369)	(1,776,702)	(2,989,618)	1,612,249
Plant & Machinery	(5,219,311)	(2,862,838)	(8,082,149)	(7,871,710)	(8,345,994)	263,845
Land	(675,271)	-	(675,271)	(1,537,973)	(1,902,797)	1,227,526
Buildings	(48,391,237)	(4,809,463)	(53,200,700)	(52,389,141)	(55,325,397)	2,124,697
Infrastructure - Roads	(12,168,181)	(2,641,014)	(14,809,195)	(16,865,950)	(17,949,388)	3,140,193
Infrastructure - Drainage	(462,076)	(74,299)	(536,375)	(1,134,097)	(1,611,168)	1,074,793
Infrastructure - Footpaths	(681,027)	(85,745)	(766,772)	(974,567)	(1,144,530)	377,758
Infrastructure - Parks Equipment	(5,718,124)	(2,082,538)	(7,800,662)	(7,667,777)	(9,456,094)	1,655,432
Infrastructure - Parks Landscaping	(543,936)	(105,059)	(648,995)	(1,098,495)	(1,231,086)	582,091
Landfill Infrastructure	(190,138)	(88,152)	(278,290)	(546,534)	(1,170,329)	892,039
	(75,510,862)	(13,827,583)	(89,338,445)	(92,820,048)	(103,066,660)	13,728,215

Note 2.

Closing Funds in the Financial Activity Statement are represented by:

	Actuals	YTD Revised Budget	Full Year Revised Budget	Adopted Budget
	\$	\$	\$	\$
Current Assets				
Cash & Investments	133,836,040	114,565,914	113,947,104	114,885,061
Rates Outstanding	3,774,369	1,600,000	1,600,000	1,600,000
Rubbish Charges Outstanding	125,228	133,800	133,800	133,800
Sundry Debtors	3,019,496	5,300,000	5,300,000	5,300,000
GST Receivable	335,715	-	-	-
Prepayments	409,576	65,000	65,000	65,000
Accrued Debtors	-	-	-	-
Stock on Hand	14,229	13,700	13,700	13,700
	141,514,655	121,678,414	121,059,604	121,997,561
Current Liabilities				
Creditors	(4,511,784)	(2,225,000)	(2,225,000)	(2,225,000)
Income Received in Advance	(2,110,756)	-	-	-
GST Payable	352,361	-	-	-
Withholding Tax Payable	-	-	-	-
Provision for Annual Leave	(4,008,052)	-	-	-
Provision for Long Service Leave	(2,113,784)	(2,000,000)	(2,000,000)	(2,000,000)
	(12,392,014)	(4,225,000)	(4,225,000)	(4,225,000)
Net Current Assets	129,122,640	117,453,414	116,834,604	117,772,561
Add: Non Current Investments	4,882,327	4,668,070	4,668,070	4,668,070
	134,004,967	122,121,484	121,502,674	122,440,631
Less: Restricted/Committed Assets				
Cash Backed Reserves #	(97,504,833)	(93,285,438)	(101,547,613)	(115,812,070)
Deposits & Bonds Liability *	-	-	-	(6,329,512)
	36,500,134	28,836,046	19,955,061	299,049
Closing Funds (as per Financial Activity Statement)	36,500,134	28,836,046	19,955,061	299,049

See attached Reserve Fund Statement

* See attached Restricted Funds Analysis

Note 3.

Amendments to original budget since budget adoption. Surplus/(Deficit)

Ledger	Project/ Activity	Description	Council Resolution	Classification	Non Change (Non Cash Items) Adjust.	Increase in Available Cash	Decrease in Available Cash	Amended budget Running Balance
					\$	\$	\$	\$
		Budget Adoption		Closing Funds Surplus(Deficit)				299,049
GL	202	Remove transport expenses		Operating Expenditure		2,100		301,149
GL	410	Recovery of administration charged to NDIS		Operating Income		46,181		347,330
			OCM 8/10/15					
GL	378	Council contribution to the Financial Counselling	#5614	Operating Expenditure			8,128	339,202
GL	161	FESA budget reallocation		Operating Expenditure		10,789		349,991
GL	162	FESA budget reallocation		Operating Expenditure		13,619		363,610
GL	175	FESA budget reallocation		Operating Expenditure		5,319		368,929
Various		Mid year budget review	OCM Feb 17	Various		4,326		373,255
OP	8839	Reduce expenditure as invoice was paid in prior year		Operating Expenditure		20,000		393,255
OP	8982	Overflow parking at the Adventure World		Operating Income		21,750		415,005
CW	4673	Cockburn ARC Furniture and Equipment		Operating Expenditure			54,000	361,005
GL	355	Admin charge recovery		Operating Income		5,947		366,952
CW	1089	Balancing funding & expenditure		Operating Income			5,209	361,743
CW	1398	Balancing funding & expenditure		Operating Income			4,341	357,402
CW	1400	Balancing funding & expenditure		Operating Income			14,244	343,158
OP	6825	Correcting signage error		Operating Expenditure			30,000	313,158
OP	8996	Cockburn ARC opening day		Operating Expenditure			39,952	273,206
OP	8997	Cockburn ARC Discover Community day		Operating Expenditure			24,070	249,136
CW	5790	Fence replacement	OCM 09/03/17	Operating Expenditure			80,000	169,136
				Closing Funds Surplus (Deficit)	0	130,031	259,944	169,136

Statement of Comprehensive Income *by Nature and Type*

for the period ended 30 April 2017

	Actual	Amended YTD Budget	\$ Variance to YTD Budget	Forecast	Amended Budget	Adopted Budget
	\$	\$	\$	\$	\$	\$
OPERATING REVENUE						
01 Rates	94,274,120	93,358,863	915,258	96,615,258	95,700,000	95,700,000
02 Specified Area Rates	312,936	330,000	(17,064)	312,936	330,000	330,000
05 Fees and Charges	Note 1 18,618,307	19,588,836	(970,529)	22,296,877	23,267,406	24,368,500
06 Service Charges	440,700	449,000	(8,300)	440,700	450,000	450,000
10 Grants and Subsidies	9,624,172	9,241,243	382,929	11,496,495	11,113,566	9,357,649
15 Contributions, Donations and Reimbursements	870,686	561,334	309,351	1,083,116	773,765	638,665
20 Interest Earnings	4,478,028	4,040,030	437,999	5,310,035	4,872,036	4,772,036
25 Other revenue and Income	-	-	-	-	-	-
Total Operating Revenue	128,618,949	127,569,306	1,049,643	137,555,416	136,506,773	135,616,850
OPERATING EXPENDITURE						
50 Employee Costs - Salaries & Direct Oncosts	Note 2 (40,002,245)	(39,826,006)	(176,239)	(49,960,117)	(49,783,878)	(47,426,397)
51 Employee Costs - Indirect Oncosts	(640,865)	(795,565)	154,699	(1,257,857)	(1,412,557)	(1,395,994)
55 Materials and Contracts	Note 3 (30,999,687)	(33,263,360)	2,263,673	(38,526,601)	(40,790,274)	(41,209,671)
65 Utilities	(3,825,901)	(3,819,164)	(6,737)	(4,702,835)	(4,696,098)	(4,684,525)
70 Interest Expenses	(476,638)	(475,000)	(1,638)	(931,638)	(930,000)	(930,000)
75 Insurances	(2,319,438)	(2,429,031)	109,594	(2,319,454)	(2,429,048)	(2,244,048)
80 Other Expenses	(6,447,823)	(6,819,959)	372,137	(8,104,747)	(8,476,884)	(7,328,893)
85 Depreciation on Non Current Assets	(21,973,369)	(22,289,671)	316,302	(26,509,664)	(26,825,966)	(26,353,614)
86 Amortisation on Non Current Assets	(906,269)	(992,610)	86,341	(906,269)	(1,191,132)	(1,191,132)
Add Back: Indirect Costs Allocated to Capital Works	2,036,255	2,063,456	(27,201)	2,558,818	2,586,019	2,229,821
Total Operating Expenditure	(105,555,980)	(108,646,910)	3,090,930	(130,660,365)	(133,949,817)	(130,534,453)
CHANGE IN NET ASSETS RESULTING FROM OPERATING ACTIVITIES						
	23,062,969	18,922,395	4,140,573	6,895,051	2,556,956	5,082,396
NON-OPERATING ACTIVITIES						
11, 16 Non-Operating Grants, Subsidies and Contributions	11,346,643	17,900,418	(6,553,775)	13,009,059	19,562,833	23,245,184
18 Developers Contributions Plans: Cash	5,093,756	5,957,525	(863,769)	6,217,409	7,081,178	11,721,922
17 Gifted Subdivision Assets	54,299,256	54,299,256	-	54,299,256	54,299,256	-
95 Profit/(Loss) on Sale of Assets	10,310,911	14,307,520	(3,996,609)	10,310,911	13,380,270	14,650
Total Non-Operating Activities	81,050,566	92,464,718	(11,414,152)	83,836,635	94,323,537	34,981,756
NET RESULT	104,113,535	111,387,114	(7,273,579)	90,731,686	96,880,493	40,064,152

Notes to Statement of Comprehensive Income

Note 1.

Additional information on main sources of revenue in fees & charges.

	Actual	Amended YTD Budget	Amended Budget	Adopted Budget
	\$	\$	\$	\$
<u>Community Services:</u>				
Recreational Services	495,768	473,983	568,780	568,780
South Lake Leisure Centre	1,969,928	2,231,074	2,231,074	2,231,074
Law and Public Safety	540,148	503,080	589,496	518,496
	<u>3,177,907</u>	<u>3,815,897</u>	<u>5,212,627</u>	<u>5,141,627</u>
<u>Waste Services:</u>				
Waste Collection Services	2,720,606	2,643,181	2,675,000	2,675,000
Waste Disposal Services	4,788,281	4,754,331	5,701,026	7,232,593
	<u>7,508,887</u>	<u>7,397,512</u>	<u>8,376,026</u>	<u>9,907,593</u>
<u>Infrastructure Services:</u>				
Marina & Coastal Services	1,129,885	1,039,105	1,068,802	1,068,802
	<u>1,129,885</u>	<u>1,039,105</u>	<u>1,068,802</u>	<u>1,068,802</u>
	<u>11,816,679</u>	<u>12,252,513</u>	<u>14,657,455</u>	<u>16,118,022</u>

Note 2.

Additional information on Salaries and Direct On-Costs by each Division.

	Actual	Amended YTD Budget	Amended Budget	Adopted Budget
	\$	\$	\$	\$
Executive Services	(1,939,413)	(2,078,126)	(2,614,193)	(2,578,913)
Finance & Corporate Services Division	(6,157,759)	(5,971,491)	(7,398,456)	(7,232,487)
Governance & Community Services Division	(12,879,701)	(13,164,268)	(16,521,317)	(14,544,002)
Planning & Development Division	(4,441,714)	(4,304,048)	(5,363,017)	(5,488,489)
Engineering & Works Division	(14,583,658)	(14,308,072)	(17,886,895)	(17,582,506)
	<u>(40,002,245)</u>	<u>(39,826,006)</u>	<u>(49,783,878)</u>	<u>(47,426,397)</u>

Note 3

Additional information on Materials and Contracts by each Division.

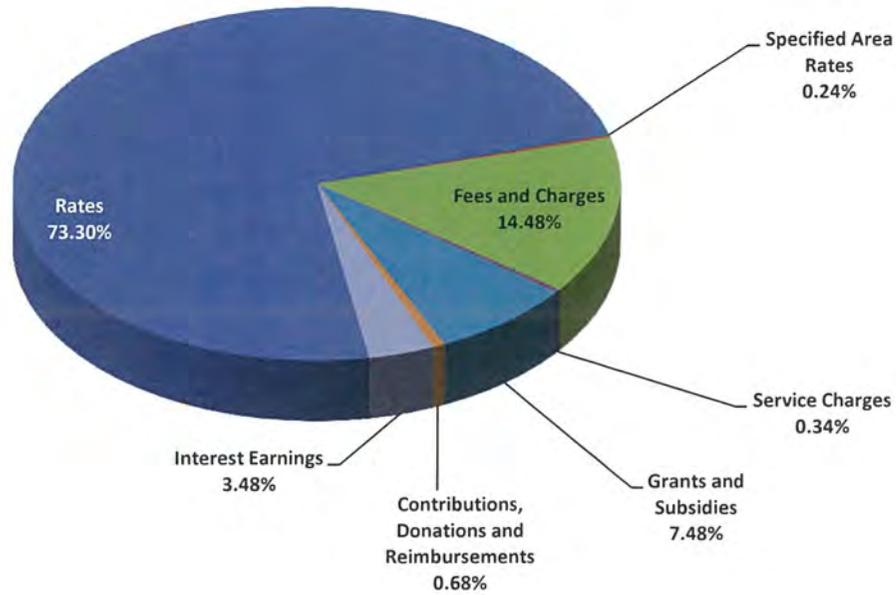
	Actual	Amended YTD Budget	Amended Budget	Adopted Budget
	\$	\$	\$	\$
Executive Services	(1,390,854)	(1,656,325)	(2,035,337)	(2,113,205)
Finance & Corporate Services Division	(2,610,370)	(3,115,938)	(4,036,632)	(3,796,510)
Governance & Community Services Division	(8,058,464)	(8,962,242)	(10,936,979)	(10,577,544)
Planning & Development Division	(726,523)	(912,301)	(1,326,291)	(1,135,933)
Engineering & Works Division	(18,213,476)	(18,616,554)	(22,455,035)	(23,586,480)
Not Applicable	0	0	0	0
	<u>(30,999,687)</u>	<u>(33,263,360)</u>	<u>(40,790,274)</u>	<u>(41,209,671)</u>

City of Cockburn - Reserve Funds

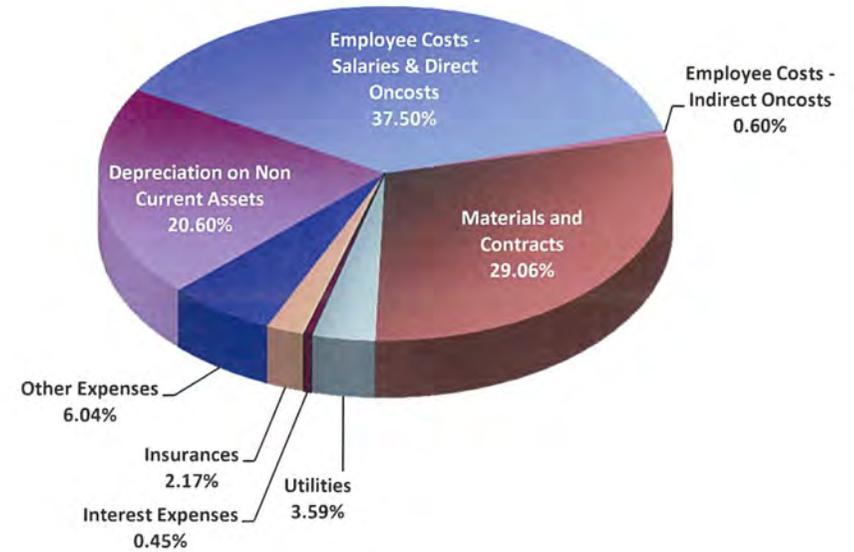
Financial Statement for Period Ending 30 April 2017

Account Details	Opening Balance		Interest Received		t/f's from Municipal		t/f's to Municipal		Closing Balance	
	Budget	Actual	Budget	Actual	Budget	Actual	Budget	Actual	Budget	Actual
Council Funded										
Bibra Lake Management Plan Reserve	985,439	985,439	10,648	14,344	-	-	(441,767)	(391,839)	554,320	607,943
C/FWD Projects Reserve	4,020,698	4,020,698	-	-	6,147,183	6,147,183	(9,457,453)	(6,377,098)	710,429	3,790,784
CCW Development Fund	10,880,762	10,880,762	53,000	120,653	8,300,000	8,300,000	(17,814,000)	(17,814,000)	1,419,762	1,487,415
Community Infrastructure	12,096,036	12,096,036	109,782	222,005	1,304,636	-	(1,232,301)	(871,901)	12,278,153	11,446,140
Community Surveillance Levy Reserve	1,245,490	1,245,490	22,594	23,167	200,000	-	(499,000)	(139,993)	969,084	1,128,664
DCD Redundancies Reserve	40,825	40,825	797	776	-	-	-	-	41,622	41,601
Environmental Offset Reserve	291,595	291,595	7,691	5,540	-	-	-	-	299,286	297,135
Green House Emissions Reductions	901,331	901,331	10,330	14,828	1,450,000	330,000	(2,362,700)	(1,112,550)	(1,039)	133,609
HWRP Post Closure Management	2,322,695	2,322,695	47,780	44,043	-	-	(100,000)	(10,401)	2,270,475	2,356,337
Information Technology	379,658	379,658	8,082	6,931	100,000	-	(496,430)	(205,143)	(8,690)	181,445
Insurance	488,961	488,961	8,090	8,632	-	-	(185,000)	(185,000)	312,051	312,593
Land Development & Investment Fund Reserve	6,348,831	6,348,831	251,777	81,671	13,080,095	11,133,574	(14,885,797)	(11,672,479)	4,794,906	5,891,597
Major Buildings Refurbishment	9,828,567	9,828,567	133,850	186,739	1,620,328	-	(100,000)	-	11,482,745	10,015,306
Municipal Elections	34,213	34,213	2,681	650	120,000	-	-	-	156,894	34,863
Naval Base Shacks	935,871	935,871	22,969	17,781	158,696	-	(2,000)	-	1,115,536	953,652
Plant & Vehicle Replacement	8,252,372	8,252,372	105,975	139,856	3,027,690	-	(4,289,108)	(2,325,501)	7,096,929	6,066,727
Port Coogee Marina Assets Replcmt	-	-	-	-	-	-	(50,000)	-	(50,000)	-
Port Coogee Special Maintenance Reserve	1,400,129	1,400,129	26,794	25,115	274,000	250,453	(375,159)	(364,396)	1,325,764	1,311,301
Port Coogee Waterways Reserve	-	-	8,685	2,952	188,590	188,590	(79,742)	-	117,533	191,542
Port Coogee WEMP	-	-	40,372	27,673	2,100,000	2,163,409	(203,192)	-	1,937,180	2,191,082
Roads & Drainage Infrastructure	8,159,206	8,159,206	64,880	205,081	5,414,846	3,000,400	(192,542)	(132,523)	13,446,390	11,232,163
Staff Payments & Entitlements	2,115,293	2,115,293	45,068	38,530	119,822	-	(162,000)	(162,000)	2,118,183	1,991,823
Waste & Recycling	23,846,752	23,846,752	348,847	287,007	1,271,022	-	(13,100,318)	(12,214,543)	12,366,302	11,919,216
Waste Collection Levy	2,641,530	2,641,530	63,366	43,657	1,321,529	-	(1,483,118)	(674,173)	2,543,307	2,011,014
POS Cash in Lieu (Restricted Funds)	5,471,641	5,471,641	108,937	53,833	96,000	96,000	(5,621,474)	(5,621,474)	55,104	(0)
	102,687,894	102,687,894	1,502,995	1,571,466	46,294,437	31,609,609	(73,133,101)	(60,275,016)	77,352,224	75,593,953
Grant Funded										
Aged & Disabled Vehicle Expenses	326,947	326,947	8,628	4,626	-	-	(162,000)	(145,000)	173,575	186,573
CIHF Building Maintenance Resrv	3,323,192	3,323,192	-	68,665	1,400,000	950,449	(150,000)	(40,891)	4,573,192	4,301,415
Family Day Care Accumulation Fund	8,295	8,295	-	158	-	-	-	-	8,295	8,452
Naval Base Shack Removal Reserve	461,814	461,814	10,217	8,774	54,693	-	(25,000)	-	501,724	470,588
Restricted Grants & Contributions Resrv	2,230,479	2,230,479	-	-	-	-	(1,928,710)	(2,111,888)	301,769	118,591
UNDERGROUND POWER	222,504	222,504	-	4,227	-	-	-	-	222,504	226,731
Welfare Projects Employee Entitlements	479,810	479,810	10,933	9,203	13,000	10,833	-	-	503,743	499,846
	7,053,040	7,053,040	29,778	95,653	1,467,693	961,283	(2,265,710)	(2,297,779)	6,284,801	5,812,197
Development Cont. Plans										
Aubin Grove DCP	500	500	4,705	9	-	-	(656)	(509)	4,549	(0)
Cockburn Coast DCP14	(66,932)	(66,932)	-	(1,272)	-	-	(54,716)	(375)	(121,648)	(68,579)
Community Infrastructure DCA 13	10,361,258	10,361,258	220,238	187,562	5,000,000	3,933,750	(5,865,800)	(5,825,327)	9,715,696	8,657,244
Gaebler Rd Development Cont. Plans	500	500	18,924	9	-	-	(3,385)	(508)	16,039	0
Hammond Park DCP	975,033	975,033	9,354	22,557	396,000	335,552	(15,491)	(375)	1,364,896	1,332,767
Munster Development	1,079,480	1,079,480	18,147	21,000	443,798	39,828	(10,883)	(375)	1,530,542	1,139,932
Muriel Court Development Contribution	(92,248)	(92,248)	-	(1,753)	206,000	-	(32,025)	(375)	81,727	(94,377)
Packham North - DCP 12	25,036	25,036	10,529	607	434,388	18,219	(7,128)	(375)	462,825	43,487
Solomon Road DCP	617,423	617,423	8,493	11,730	120,000	-	(5,166)	(375)	740,750	628,778
Success Lakes Development	500	500	3,817	9	-	-	(1,061)	(509)	3,256	-
Success Nth Development Cont. Plans	2,398,845	2,398,845	15,311	45,729	11,700	242,883	(4,596)	(18,623)	2,421,260	2,668,834
Thomas St Development Cont. Plans	12,699	12,699	294	241	-	-	-	-	12,993	12,940
Wattleup DCP 10	(3,401)	(3,401)	-	974	-	100,304	(10,161)	(375)	(13,562)	97,501
Yangebup East Development Cont. Plans	1,130,859	1,130,859	6,026	22,327	130,036	99,904	(2,161)	-	1,264,760	1,253,090
Yangebup West Development Cont. Plans	419,471	419,471	9,195	7,969	-	-	(2,161)	(375)	426,505	427,065
	16,859,023	16,859,023	325,033	317,698	6,741,922	4,770,439	(6,015,390)	(5,848,477)	17,910,588	16,098,683
Total Reserves	126,599,957	126,599,957	1,857,806	1,984,817	54,504,052	37,341,331	(81,414,201)	(68,421,272)	101,547,613	97,504,833

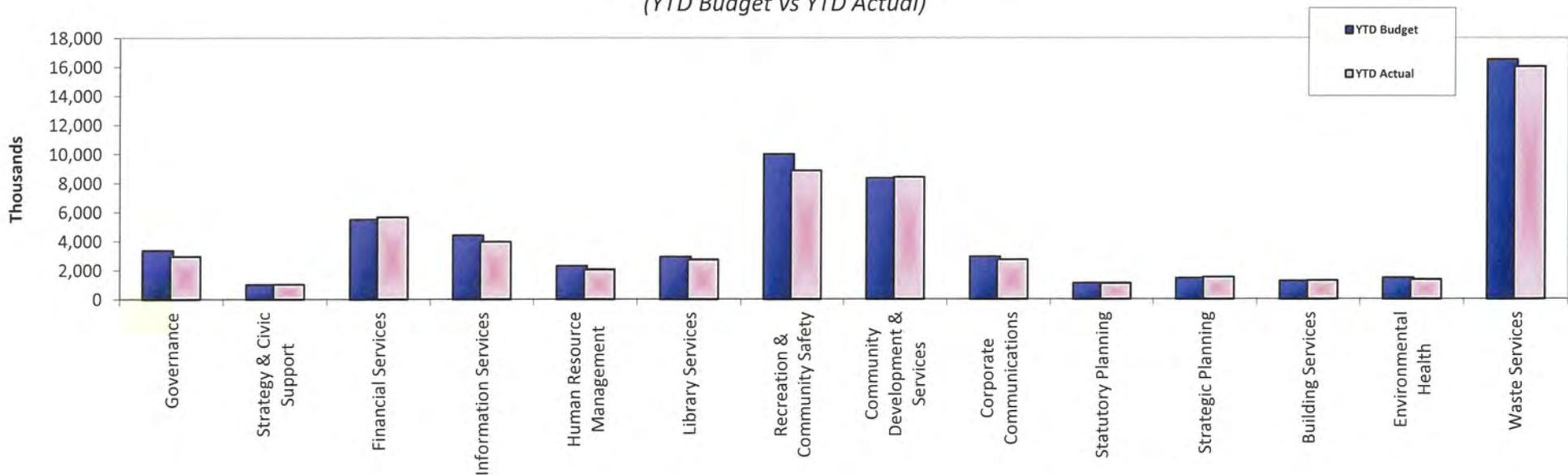
Operating Income by Nature and Type
(YTD Actual)



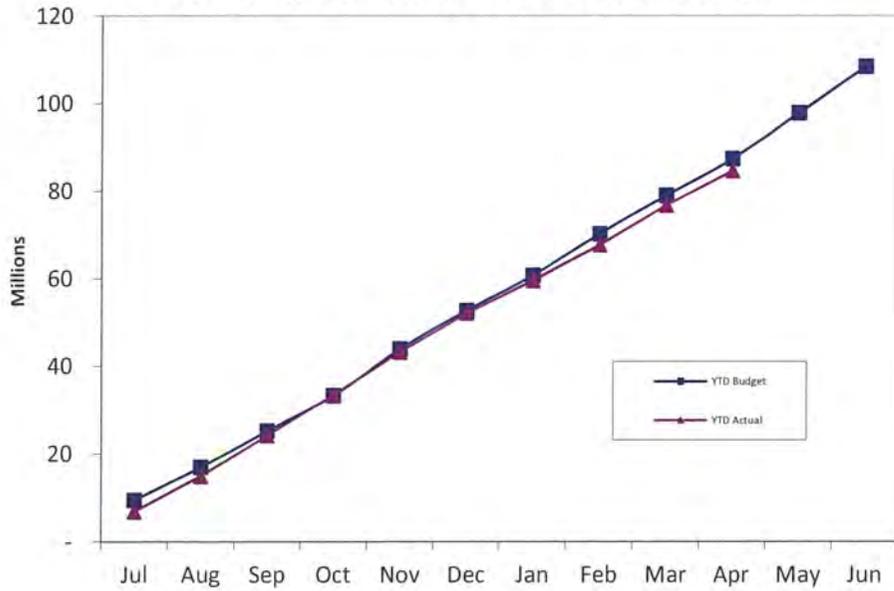
Operating Expenditure by Nature and Type
(YTD Actual)



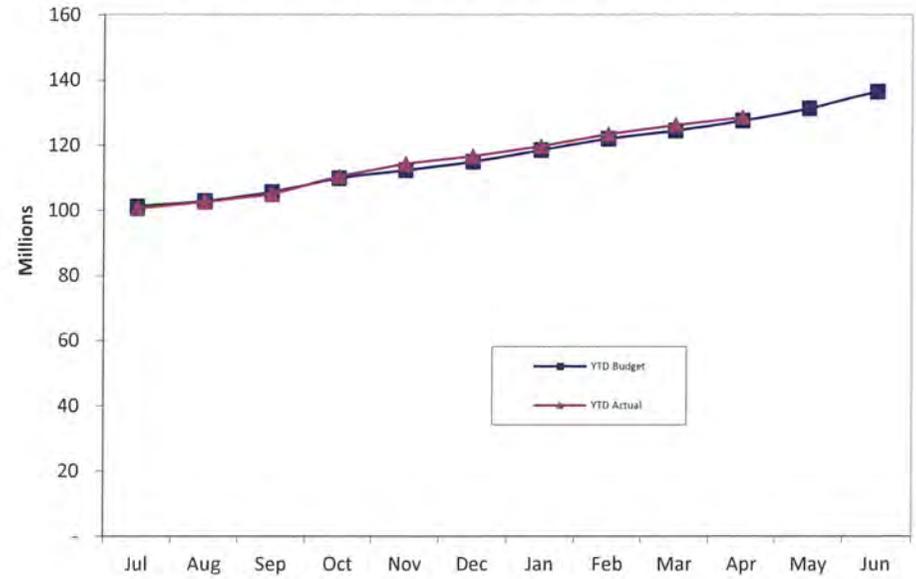
Operating Expenditure by Business Unit
(YTD Budget vs YTD Actual)



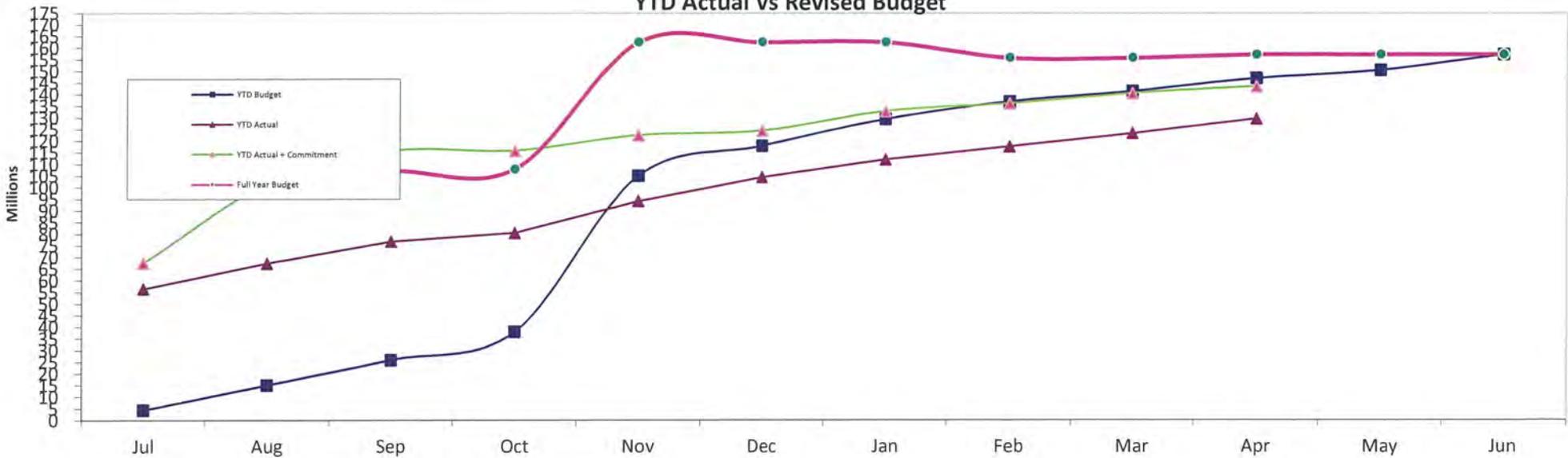
YTD Operating Expenditure Vs YTD Revised Budget



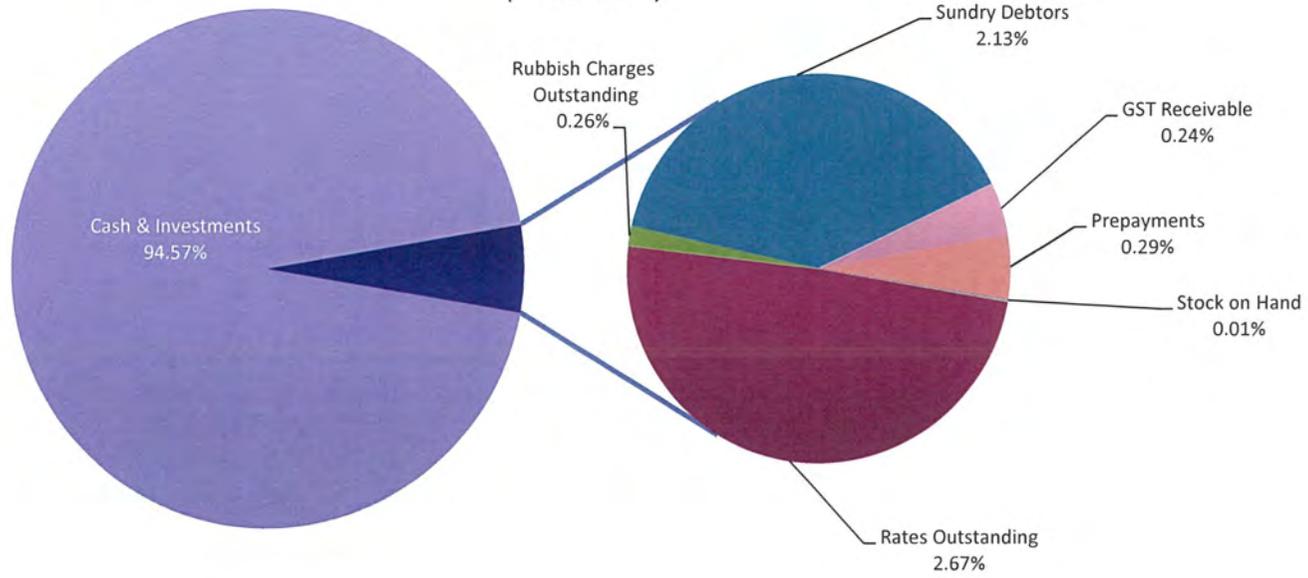
YTD Operating Income Vs YTD Revised Budget



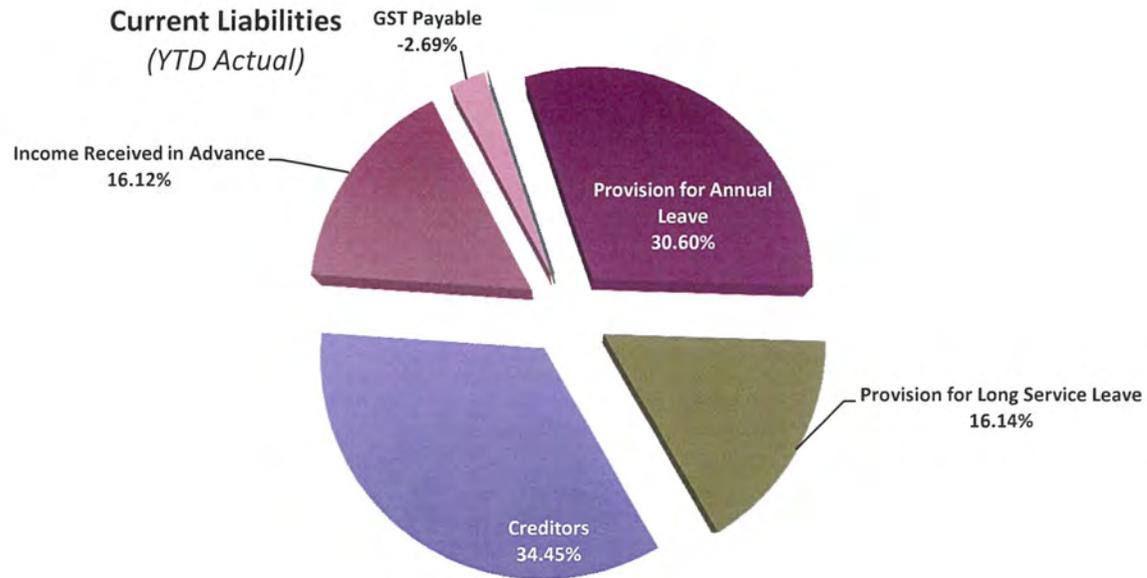
**Capital Expenditure
YTD Actual Vs Revised Budget**



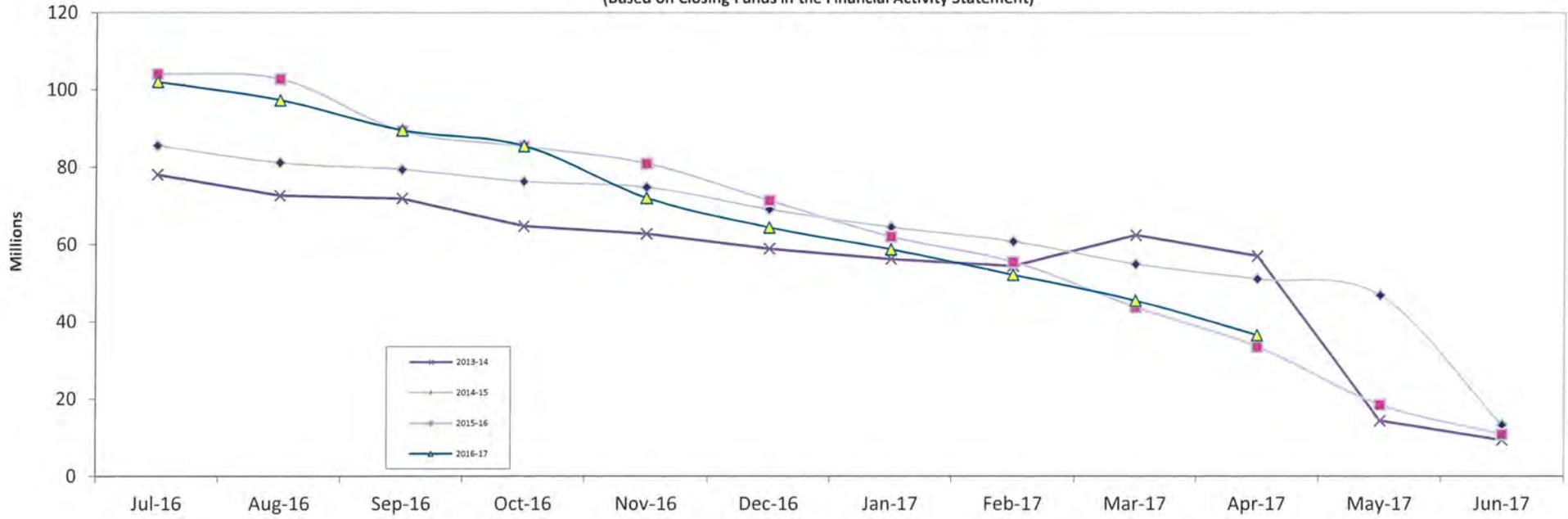
Current Assets (YTD Actual)



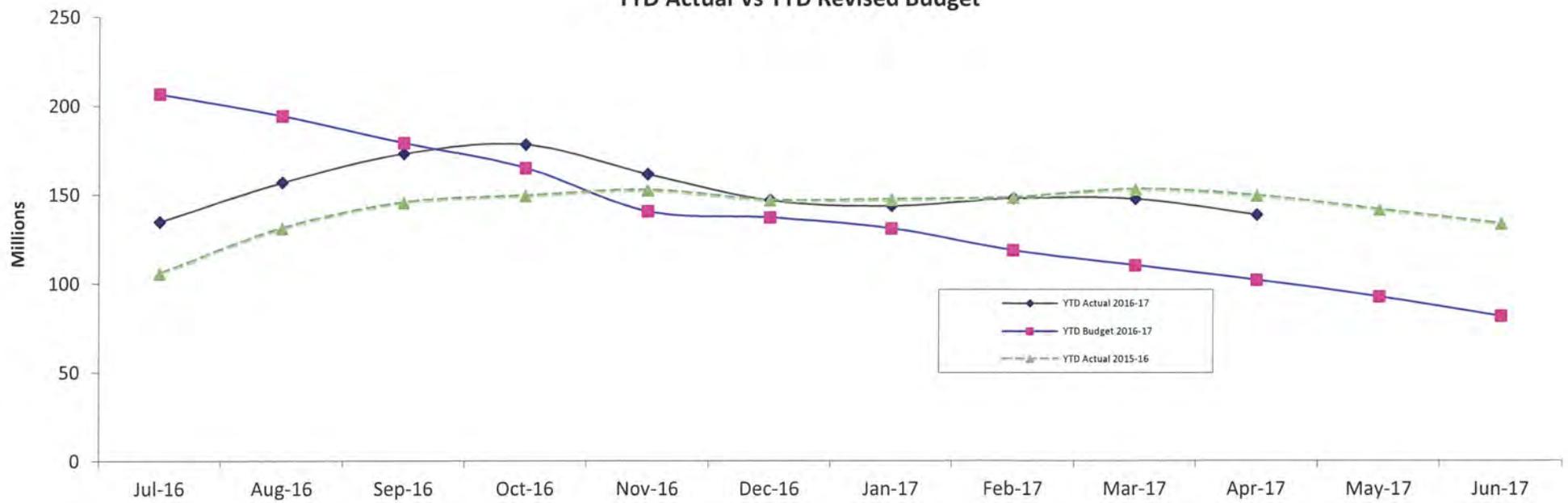
Current Liabilities (YTD Actual)



Municipal Liquidity Over the Year (Based on Closing Funds in the Financial Activity Statement)



Cash & Investments Positions YTD Actual Vs YTD Revised Budget



DETAILED BUDGET AMENDMENTS REPORT

for the period ended 30 April 2017

PROJECT/ACTIVITY LIST		ADD/LESS	EXPENDITURE	TF TO RESERVE	FUNDING SOURCES			
					RESERVE	EXTERNAL	MUNICIPAL	NON-CASH
GL 599-6354	Funds transfer for ARC opening	LESS	(25,000)				25,000	
GL 599-6273	Funds transfer for ARC Style Guide	LESS	(4,539)				4,539	
OP 8996	Increase funding for ARC opening	ADD	72,932			(7,980)	(25,000)	
OP 8997	Increase funding for ARC Discovery	ADD	76,700			(52,630)		
OP 8298	Increase funding for ARC Style Guide	ADD	4,539				(4,539)	
CW 7288	FESA vehicle received	ADD	548,243			(548,243)		
CW 4674	Increase funding for fitout at ARC for Curtin	ADD	193,000		(193,000)			
CW5790	Fence replacement for Friendship Way project (OCM 9/3/17)	ADD	80,000					
CW 3643	Adjustment to Berrigan Dve funding	ADD	435,154	1,504,102		(1,939,256)		
			1,381,029	1,504,102	(193,000)	(2,548,109)	0	0

CITY OF COCKBURN



ORDINARY COUNCIL

CONFIDENTIAL

AGENDA ATTACHMENT

FOR

THURSDAY, 8 JUNE 2017

The content of this document has been determined as CONFIDENTIAL in accordance with Section 5.93 of the Local Government Act 1995.

Section 5.93 of the Local Government Act 1995, provides that:

A person who is a Council member, a Committee member or an employee must not make improper use of any information acquired in the performance by the person of any of his or her functions under this Act or any other written law:

- (a) to gain directly or indirectly an advantage for the person or any other person; or*
- (b) to cause detriment to the local government or any other person;*

Penalty: \$10,000 or imprisonment for 2 years

Accordingly, please ensure that this information is not passed on to any other third party.

CONFIDENTIAL ATTACHMENTS RFP 11/2017 RECOMMENDATION

GREENWASTE DECONTAMINATION PLANT - HWRP

1. Compliance Criteria Assessments
2. Consolidated Evaluation Scores
3. Tendered Prices

NOTE:

The tendered prices are not disclosed at the opening of Tenders nor entered into the Tenders Register.

In accordance with Part 4, Regulation 16-3(c) and 17-3 of the Local Government (Functions and General) Regulations 1996 the Principal is only required to record the price of the winning Tenderer/s in the Tenders Register.

CONSOLIDATED EVALUATION SCORE – RFP 11/2017

Attach 2

City of Cockburn Tender Evaluation Score (Consolidated) - CONFIDENTIAL
RFP11-2017 - Greenwaste Decontamination Plant - HWRP

DATE: 27 April 2017

EVALUATION PANEL: Lyall Davieson (Chair), Margot Tobin (SBMG), Mike Haynes

Qualitative Criteria		Weighting %W	1		2		3		4		5		6		Marked out of / lowest price	Highest Price
Score Transfers from Individual Tenderer's Score Sheet into Yellow Areas			Aust Bale Press		Aust Project Management		EMER		Pipecraft		RDT Engineering		Wastech Engineering		B	C
			mark M	score S	mark M	score S	mark M	score S	mark M	score S	mark M	score S	mark M	score S		
A-1	Provide details (Client & Brief Outline) of similar greenwaste decontamination/handling plants supplied, in particular those provided to other Local Government or Regional Councils;	6.00%	11.00	2.20%	1.00	0.20%	23.50	4.70%	3.00	0.60%	21.00	4.20%	21.00	4.20%	30.00	
A-2	Provide details of issues that arose during the design, installation and initial operating period of the waste processing plant and how these were managed;	3.00%	11.00	1.10%	1.00	0.10%	21.00	2.10%	1.00	0.10%	12.00	1.20%	8.00	0.80%	30.00	
A-3	Provide summary details of other projects (including project name, value, dates started and completed, client's name) and referees details (referee's name and telephone number);	1.00%	6.00	0.20%	1.00	0.03%	24.00	0.80%	2.00	0.07%	18.00	0.60%	18.00	0.60%	30.00	
A-4	Demonstrate competency and proven track record of achieving outcomes;	2.00%	18.00	1.20%	1.00	0.07%	23.00	1.53%	2.00	0.13%	21.00	1.40%	21.00	1.40%	30.00	
A-5	Length of time company has been in continuous existence and has provided waste decontamination/handling plant design, installation and operating services;	1.00%	23.00	0.77%	1.00	0.03%	20.50	0.68%	7.00	0.23%	18.00	0.60%	21.00	0.70%	30.00	
A-6	Provide a copy of your organisation structure and provide background information on your company;	1.00%	20.00	0.67%	3.00	0.10%	21.00	0.70%	3.00	0.10%	22.00	0.73%	21.00	0.70%	30.00	
A-7	Provide details of AS/NZS ISO 9001 Quality Assurance management systems certification and/or company quality system or quality assurance procedures;		No Sub-weighting - Refer to Compliance Criteria													
A-8	Provide details of AS/NZS 4801 Occupational Health and Safety management systems certification and/or company health and safety system or procedures; and		No Sub-weighting - Refer to Compliance Criteria													
A-9	Provide details of your safety record in the installation of greenwaste decontamination/handling plant and equipment.	1.00%	13.00	0.43%	7.00	0.23%	22.50	0.75%	3.00	0.10%	19.50	0.65%	13.00	0.43%	30.00	
A	Demonstrated Experience	15.00%	102.00	6.57%	15.00	0.77%	155.50	11.27%	21.00	1.33%	131.50	9.38%	123.00	8.83%	210.00	
B-1	Provide details of proposed personnel (with an emphasis on their experience in projects of a similar requirement) to be allocated to this project and their role in the performance of the Contract;	4.00%	22.00	2.93%	8.00	1.07%	22.50	3.00%	7.00	0.93%	19.50	2.60%	20.00	2.67%	30.00	
B-2	Provide curriculum vitae of the Principal Contact Person, the Principal Designer and Mechanical Installation/Maintenance Crew Personnel; and	2.00%	21.00	1.40%	11.00	0.73%	21.00	1.40%	10.00	0.67%	18.00	1.20%	16.00	1.07%	30.00	
B-3	Provide details of the back-up/support services that are able to be provided to the Principal, including: i. All relevant warranties (periods and conditions), service support, repairs etc. Including ability to undertake minor warranty/running repairs at the Principal's Henderson Waste Recovery Facility if not included in current warranty support services; and the ability to provide warranty, rectification or modification works outside of the Principal's normal working hours of 7:00am to 4:00pm Monday to Friday. ii. Four (4) year Service and Maintenance Contract (service/major component replacement frequencies/intervals, length of time plant will be unavailable during normal working hours and conditions). iii. Scope of the training to be provided for the operators and the Principal's mechanical maintenance staff on the complete plant; including the number of training sessions. iv. What spare parts are held ex stock in WA and what the turnaround time is required to provide spare parts from the eastern states and/or overseas.	4.00%	15.00	2.00%	5.00	0.67%	25.50	3.40%	16.50	2.20%	21.50	2.87%	20.50	2.73%	30.00	
B	Respondents's Resources	10.00%	58.00	6.33%	24.00	2.47%	69.00	7.80%	33.50	3.80%	59.00	6.67%	56.50	6.47%	90.00	
C-1	Belt feeds that do not break glass or ceramics;	4.00%	17.00	2.27%	11.00	1.47%	24.50	3.27%	6.00	0.80%	13.00	1.73%	6.00	0.80%	30.00	
C-2	Capability for simple and low cost relocation;	1.00%	17.00	0.57%	13.00	0.43%	23.50	0.78%	16.00	0.53%	17.00	0.57%	17.00	0.57%	30.00	
C-3	Provide a design option for a free-standing unit that incorporates an enclosed, air conditioned, air filtered picking station structure. This option shall include details on any weather protection that is necessary for the equipment in the process chain and details on how greenwaste will be prevented from littering the adjacent surroundings in high wind conditions;	2.00%	18.50	1.23%	9.00	0.60%	23.00	1.53%	6.00	0.40%	15.00	1.00%	16.00	1.07%	30.00	
C-4	The picking station enclosure structure must allow a maximum of two (2) staff each side of the inspection belt;	1.00%	19.00	0.63%	17.00	0.57%	23.00	0.77%	16.00	0.53%	21.00	0.70%	21.00	0.70%	30.00	
C-5	Capability to process twenty-five (25) tonnes of greenwaste per hour;	2.00%	20.00	1.33%	17.00	1.13%	24.00	1.60%	16.00	1.07%	22.00	1.47%	23.00	1.53%	30.00	
C-6	Low power consumption – provide power operating costs per hour;	1.00%	14.00	0.47%	15.00	0.50%	21.00	0.70%	15.00	0.50%	16.00	0.53%	20.00	0.67%	30.00	
C-7	A variable speed inspection belt control;	1.00%	20.00	0.67%	19.00	0.63%	21.00	0.70%	20.00	0.67%	21.00	0.70%	21.00	0.70%	30.00	
C-8	An estimate of the total footprint of the entire plant;	1.00%	16.00	0.53%	15.00	0.50%	23.00	0.77%	17.00	0.57%	18.00	0.60%	19.00	0.63%	30.00	
C-9	Occupational safety and health (OSH) features;	6.00%	20.00	4.00%	17.00	3.40%	24.00	4.80%	7.00	1.40%	17.50	3.50%	21.00	4.20%	30.00	
C-10	Any other design features; and	2.00%	19.00	1.27%	11.00	0.73%	23.50	1.57%	11.00	0.73%	0.00	0.00%	16.00	1.07%	30.00	
C-11	Full Proposed Plant Specification with Concept Sketch.	4.00%	14.00	1.87%	0.00	0.00%	24.00	3.20%	14.00	1.87%	23.00	3.07%	23.00	3.07%	30.00	
C	Design Features	25.00%	194.50	14.83%	144.00	9.97%	254.50	19.68%	144.00	9.07%	183.50	13.87%	203.00	15.00%	330.00	

CONSOLIDATED EVALUATION SCORE – RFP 11/2017 CONTINUED

D - 1	Describe in detail the principles and benefits behind the design submitted.	4.00%	15.00	2.00%	6.00	0.80%	24.00	3.20%	13.00	1.73%	20.00	2.67%	14.00	1.87%	30.00	
D - 2	Provide a Gantt chart or similar for the Whole of the Works to indicate that the Works/Services are able to be completed within the period or on the date specified in the Brief;	1.00%	0.00	0.00%	15.00	0.50%	18.00	0.60%	16.00	0.53%	12.00	0.40%	18.00	0.60%	30.00	
D	Methodology	5.00%	15.00	2.00%	21.00	1.30%	42.00	3.80%	29.00	2.27%	32.00	3.07%	32.00	2.47%	60.00	
E - 1	AS/NZS ISO 14001 Environmental Management System certification or other third party, independently assessed, Australian or internationally recognised accreditation covering environmental management systems; and/or The Company's Sustainability /Corporate Social Responsibility Policy (or Procedures) identifying specific environmental, social and financial objectives and initiatives;	1.00%	0.00	0.00%	0.00	0.00%	0.00	0.00%	1.00	0.03%	15.00	0.50%	13.00	0.43%	30.00	
E - 2	Sustainability achievements, including any awards or other supporting information in regards to your policies, management systems or procedures. This may include areas such as waste reduction, resources efficiency (e.g. energy, water etc.) biodiversity, local economy impact, ethical supply (e.g. fair trade, non-child labour etc.), employment equality and diversity (e.g. social enterprises, indigenous etc.);	1.00%	2.00	0.07%	3.00	0.10%	14.00	0.47%	1.00	0.03%	15.00	0.50%	4.00	0.13%	30.00	
E - 3	Previous or current projects and/or contracts incorporating sustainability principles that you have been or are involved in over the past two (2) years;	1.00%	9.00	0.30%	10.00	0.33%	18.00	0.60%	1.00	0.03%	12.00	0.40%	7.00	0.23%	30.00	
E - 4	Involvement with social enterprises (e.g. Australian Disability Enterprises, WA Indigenous controlled businesses etc.) including details of any joint ventures or other business dealings regarding allocation of work, employment opportunities and/or the supply of certain goods and/or services; and	1.00%	0.00	0.00%	0.00	0.00%	2.00	0.07%	1.00	0.03%	15.00	0.50%	0.00	0.00%	30.00	
E - 5	Any other direct involvement regarding your contribution to social benefits and/or community outcomes/impacts.	1.00%	0.00	0.00%	8.00	0.27%	3.00	0.10%	1.00	0.03%	14.00	0.47%	0.00	0.00%	30.00	
E	Sustainability	5.00%	11.00	0.37%	21.00	0.70%	37.00	1.23%	5.00	0.17%	71.00	2.37%	24.00	0.80%	150.00	
			380.50		225.00		558.00		232.50		477.00		438.50		840.00	
TP	Tendered Price (Indicative Lump Sum)	40.00%	\$786,570.00	15.32%	\$328,656.00	38.61%	\$ 689,105.46	20.28%	\$301,301.40	40.00%	\$633,659.79	23.10%	\$510,546.29	29.36%	\$301,301.40	\$786,570.00
QS	Qualitative Score (Non-Cost)	60.00%		30.10%		15.20%		43.78%		16.63%		35.35%		33.57%	GST Exclusive	
TS	TOTAL SCORE	100%		45.42%		53.81%		64.06%		56.63%		58.45%		62.93%		
<p>Relative mark method where submissions are marked relative to each other in regards to the Tendered Price. The lowest price (M) scores the maximum score (S) for the Tendered Price and the other prices/scores are then relatively proportioned. Qualitative formula (where higher is better) $S = W * MB$ Price formula (where lower is better) $S = W + (B - M) * W / C$ The Marked Out of Score (B) = The Mark (M) score for each Qualitative Criteria question is a score out of 10 x the number of evaluation panel members</p>																

TENDERED PRICES – RFP 11/2017

A		GREENWASTE DECONTAMINATION PLANT						RFP 11/2017 - V1	
		DESIGN, FABRICATION, INSTALLATION, TESTING & COMMISSIONING							
		Henderson Waste Recovery Park							
This spreadsheet has been designed to facilitate the Principal's preparation of a comparative price analysis as part of the Tender Evaluation Process.									
Tenderers are advised not to modify the layout of this spreadsheet or submit an alternative format as this will result in them being deemed non-compliant.									
			AUST BALE PRESS	AUST PROJECT MANAGEMENT	EMER (Focus)	PIPECRAFT	RDT ENGINEERING	WASTECH	
LUMP SUM AND BREAKDOWN									
		DESCRIPTION	AMOUNT	AMOUNT	AMOUNT	AMOUNT	AMOUNT	AMOUNT	
A1		General/Preliminaries - Including but not limited to:							
		Insurance Costs							
		Bank Guarantee Costs							
		Building Application							
		Building Permit							
		Project Occupational Safety & Health Plan	\$ 40,000.00	\$ 18,000.00	\$ 20,000.00	\$ 50,000.00	\$ 42,125.00	\$ 14,831.80	
		Dust Management Plan							
		Quality Management Plan							
		Works Programme							
		Mobilisation/Demobilisation							
		Locating Services							
A2		Concept/Preliminary Design	\$ 30,000.00	\$ 19,000.00	\$ 10,000.00	\$ 4,400.00	\$ 5,200.00	supplied	
A3		Design Development and Documentation	\$ 60,000.00	\$ 25,000.00	\$ 10,000.00	\$ 16,500.00	\$ 26,000.00	\$ 7,548.00	
A4		Fabrication	\$ 365,000.00	\$ 124,000.00	\$ 581,730.00	\$ 125,000.00	\$ 326,596.00	\$ 367,397.70	
A5		Siteworks	\$ 65,000.00	\$ 62,000.00	\$ 10,000.00	\$ 25,000.00	\$ 67,600.00	\$ 11,880.00	
A6		Greenwaste Decontamination Plant - Installation	\$ 195,000.00	\$ 61,000.00	\$ 40,000.00	\$ 30,000.00	\$ 142,474.00	\$ 88,718.54	
A7		Testing and Trialling	\$ 10,000.00	\$ 5,000.00	\$ 2,000.00	\$ 10,000.00	\$ 5,000.00	\$ 9,590.40	
A8		Training	\$ 5,000.00	\$ 2,000.00	\$ 2,000.00	\$ 15,000.00	\$ 5,000.00	\$ 1,006.40	
A9		Commissioning	\$ 5,000.00	\$ 8,000.00	\$ 2,000.00	\$ 15,000.00	\$ 10,400.00	\$ 1,006.40	
A10		As Constructed Drawings and Documentation	\$ 10,000.00	\$ 4,000.00	\$ 10,000.00	\$ 9,800.00	\$ 2,000.00	\$ 7,548.00	
		SUB-TOTAL (Ex GST) - A1 to A10:	\$ 785,000.00	\$ 328,000.00	\$ 687,730.00	\$ 300,700.00	\$ 632,395.00	\$ 509,527.24	
A11		BCITF Fee	\$ 1,570.00	\$ 656.00	\$ 1,375.46	\$ 601.40	\$ 1,264.79	\$ 1,019.05	
A12		TOTAL LUMP SUM PRICE (EX GST):	\$ 786,570.00	\$ 328,656.00	\$ 689,105.46	\$ 301,301.40	\$ 633,659.79	\$ 510,546.29	
A13		TOTAL GST COMPONENT:	\$ 78,657.00	\$ 32,865.60	\$ 68,910.55	\$ 30,130.14	\$ 63,365.98	\$ 51,054.63	
A14		TOTAL LUMP SUM PRICE (INC GST):	\$ 865,227.00	\$ 361,521.60	\$ 758,016.01	\$ 331,431.54	\$ 697,025.77	\$ 561,600.92	

CITY OF COCKBURN

WETLANDS TO WAVES

PROPOSAL

1. Introduction
2. Initial Concept
3. Overview
4. What Is An Adventure Race
5. Potential City of Cockburn Event Objectives
6. Current Event Comparisons
7. Analysis of City of Cockburn Areas for Inclusion
8. Course Options
9. Considerations
10. Budget

Appendices

- Permissions / Stakeholders
- Event Checklist
- Event Management
- Sample Map/

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Events Director
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1. INTRODUCTION

Following a meeting with Marie La Frenais, Event and Cultural Coordinator, City of Cockburn, the purpose of this proposal is to establish the potential for a unique Adventure Race Event in the City of Cockburn that profiles the City of Cockburn, its major attributes/ areas and is inclusive to all abilities.

2. INITIAL CONCEPT

Wetlands to Waves (the event) as a concept encompasses starting the event at a wetland area and completing the event on the pristine beach area of the City

The following is an open report on the proposal by the City of Cockburn to establish a "Wetlands to Waves" Adventure race involving swimming, cycling and running. For the purpose of this report we have expanded the concept to encompass the following:

- Addition of a paddle (ski, kayak) to the swim, bike and run sections.
- Reviewed all potential areas considering the advantages of each area and potential impediments. This has includes aspects of acceptance by competitors and organisers, safety as well as cost limitations.

Some 45 hours of actually conducting the activities from various locations over three weeks were used to evaluate all areas. This included actually swimming, paddling, cycling and running each potential area and courses.

3. OVERVIEW

The City of Cockburn contains areas that can include the following disciplines in an event:

- Swim and Paddle - With some 6.5 kilometres of beach stretching from Rollinson Road (North Coogee) to Woodman Point (Jervoise Bay Sailing Club) there is adequate safe areas in which to conduct these sections.
- Run – Run sections involving both path and off road areas are available and can be organised in a safe environment with minimal road traffic management or can incorporate areas that will require significant traffic management or procedures.
- Cycle – This section provides the greatest challenge and could include both on road and off road sections. There are limitations in off road areas depending on the final distance of the event.

A combination of on road and off road will enable a longer course to be designed. This section will also involve traffic management and potentially increased signage. A further alternative is a complete on road course.

4. WHAT IS AN ADVENTURE RACE

An adventure race involves activities in a natural setting using a variety of disciplines over a course of 1 to 8+ hour duration. This report does not recommend an event of longer than 4 hours duration. The recommendation will include two distinct event distance / times (to cater for the general public as well as professional athletes) operating concurrently and involving more than the normal numbers in a team structure in addition to individuals. A team is generally made up of 2 to 4

persons – for this event it could be expanded to 6 to 8 making it a unique event that would include a wider range of entrants.

In addition a preliminary set of activities and course areas can be presented once the event is approved. The course suggested below may not be the final course used, pending approvals from various authorities and land usage. It can also be modified to suit and profile the City of Cockburn requirements

5. CITY OF COCKBURN OBJECTIVES

Establish an adventure event that is unique in concept from other events

- Profile the features of the City in the event course
- Competitive event for all levels of ability
- Social aspect supporting the competition
- Sustainable in the long term
- Incorporate a variety of disciplines including swimming, running, cycling and paddling

Features of the City of Cockburn

- Robb Jetty
- CY O'Connor Beach
- Woodman Point Lighthouse
- Lake Coogee
- Pristine ocean foreshore
- Port Coogee Marina
- Coastal parks
- Manning Park
- Cockburn ARC
- Wetlands – Beellar, Thompsons Lake, Kogolup Lake, Lake Coogee and Bibra lake

6. EVENT COMPARISONS

While many adventure races exist worldwide with multiple variations of disciplines included, two events that are listed as adventure races are analysed below. Both events have similar concepts but include different distances and costs. The designs are relative to the area's constraints and/ or the scenic profile. The distances and areas also cater for different levels of competitive ability.

AUGUSTA ADVENTURE FEST			PADDY PALIN ADVENTURE SERIES		
Augusta, Western Australia			Royal National Park, New South Wales		
Early November			Late May		
DISCIPLINES	DISTANCE	ENTRY FEE	DISCIPLINES	DISTANCE	ENTRY FEE
Long Course			Long Course		
Ocean Paddle	13.8km	Individual \$266	Trail Run	2.0km	Individual / Pairs (per person)
Mountain Bike	28.5km	Junior Individual \$78	Mountain Bike	10km	\$95 -Early
Swim	1.9km	Pair \$441 Team of 2 \$263	Kayak	1km	\$105 – 2 Months

Coastal Run	13.4km	Team of 3 \$325	Mountain Bike	5km	\$115 – 1 Month
Team Run	300m	Team of 4 \$375	Trail Run	5km	
		Junior Team \$162	Mountain Bike	7km	
			Kayak	2km	
Short Course			Short Course		
Ocean Paddle	6km	Individual \$116	Mountain Bike	4km	Individual / Pairs
Mountain Bike	15km	Junior Individual \$78	Kayak	1km	(per person)
Swim	750m	Team of 2 \$164	Trail Run	2km	\$85 - Early
Coastal Run	6km	Team of 3 \$171	Mountain Bike	5km	\$95 – 2 Months
Team Run	300m	Team of 4 \$185	Trail Run	2km	\$105 – 1 Month
		Junior Team \$152			

Note: The Augusta event offers a 4 to 12 years mini kids event

Entry Fees:

Entry fees generally depend on the profile of the event, quality of the event and what is offered. There are a number of inclusions and exclusions for different events. These are based on the cost of staging the events, its profile, requirements of sponsors and the terrain being used for the event

COMPARISON OF POTENTIAL ENTRY FEE INCLUSIONS	
AUGUSTA ADVENTURE FEST	PADDY PALLIN ADVENTURE RACE
Entry to the Race including GST	Race Maps for Your Team Prizes
Professional Medical Team	Prizes
Professional Water Safety Team	A professionally organised, safe and enjoyable adventure event
Electronic Timing and Results	Kayaks, paddles and personal floatation devices ++
Professional Photographers on Course	Qualified First Aiders
Post-Race Fruit	Toilets
Superb Random Draw Prizes	

Note: The Augusta Adventure Fest and most events requires competitors to provide all race equipment - this includes a kayak /ski, paddle and pfd for the paddle leg and mountain bike / bike and helmet for the bike leg.

7. ANALYSIS CITY OF COCBURN AREAS

Course design and timing should take into account potential disruptions and safety:

Major:

The flow of the event, in addition to the course, largely depends on adequate areas for the change from one discipline to another. Each area must be able to have adequate space for equipment,

competitors and areas for spectators supporting event entrants. It must also be designed to allow a flow of competitors in and out and control of spectators included not impeding competitors or putting the safety of competitors at risk.

The four disciplines – swimming, cycling, running and paddling can be more than adequately catered for with large numbers of competitors at the three major transition points and potentially other smaller points.

In selecting the transition points consideration has been given to parking, toilet and support facilities such as cafes and access for competitor equipment.

Potential Transitional Areas - Analysis

AREA	PARKING	TOILETS	AREA COMMENT	ADDITIONAL
Bibra Lake	60 Bays	Female & Male	Start staging area is adequate for Run course	Crossing significant major roads
Cockburn ARC	200+ Bays	Female & Male	Start staging area is adequate for Run course and Bike Start	Crossing significant major roads but has potential
Beeliar Regional Park	None at present	None at present	Dual Use Path – all trails are sand based	Not ideal for Riding or Running
Rollinson Road Car Park	50 Car Bays	None at present	Small. Does have close proximity to beach	Short Course change over
CY O'Connor Reserve	58 Car Bays, 2 Disabled	1 Toilet, Shower	Large grass area, Beach access possible	Potential Change area for Long and Short Course
Marina Beach	56 Car Bays	2 Female, 1 Male +	Small Beach Area	Potential Short Course Change
Coogee Beach	238 Car Bays	4 Female, 3 Male +, Disabled	Major Grass Area and Beach access	Potential Start / Finish area
Coogee SLSC	156 Car Bays	Complete Change Rooms	Major parking area. Café and facilities. Close to beach	Potential Start / Finish area
Woodman Point	221 Car Bays	3 Female, 2 Male +, Disabled	Significant Grass area. Close to Beach	Transition point from Bike to Run, Paddle, or Swim
Manning Park	150 Car Bays	4 Female, 2 Male +, Disabled	Lake and large grass area, Historic buildings, stairs	Mid-point of Run, Not suitable for mountain bike at this stage (mountain bike trail possibly being built 2019)

- + Beeliar Regional Park was surveyed for the mountain bike course – most of the trails are sand based making riding or running impractical.
- ++ Bibra Lake – Is a potential start area for an initial run section to Manning Park. The major impediments are the run would need to be on pedestrian paths and cross major roads requiring Traffic Management.
- +++ The new Cockburn ARC is classified the same as the Bibra Lake start with one significant difference. This has not been investigated thoroughly – Potentially a short early run start can be achieved followed by the Bike section before the normal Sunday increase in traffic

on roads. Potentially uses - Beeliar Road, Kogolup Lake and Thompson's Lake (Beeliar Wetlands), Russell Road, Lake Coogee, Cockburn Road to Coogee SLSC or Woodman Point.

COURSE DESIGN / INCLUSIONS / EXCLUSIONS

Beeliar Regional Park (Currently excluded)



The Park itself is of significant size and could serve as a run course utilizing the dual use path and bush trails. It is some distance (5.2km) from the Coogee SLSC, making a return run of 10 kilometres before incorporating any of the Parks. It is not suitable for a mountain bike course as we found a majority of the time was spent in sand areas – not riding, pushing. Not recommended for inclusion.

Bibra Lake (Potential Inclusion)

Both these facilities have an adequate start point. Bibra Lake via dual use paths along Phoenix Road to Manning Park is 5 kilometres and can be used for a longer course start. The major disadvantages are traffic management, non scenic course and starting the bike sections later creating a potential safety and traffic management issue.

Cockburn ARC

This area as a start point, while not completely investigated, has what we term “left field potential” in that it could be a feasible Cycle Course that is pre-empted by a 2 kilometre run in the ARC precinct. Cockburn ARC, while similar to Bibra Lake, has the advantage of being able to start the Cycle Leg earlier and can encompass the wetlands areas of Kogolup Lake, Thompsons Lake and Lake Coogee. It does involve two different points for the start and finish of the bike section. The distance from the ARC to Coogee SLSC is approximately 16 kilometres but can be expanded to 20+kilometres by using a combination of on road and off road areas. It does involve some traffic management with the use of Beeliar Drive and Russell Road / Rockingham Road but if conducted as the first section after sunrise will have less impact. It may require a separate start for the shorter course at Coogee SLSC; this does bear further extensive investigation.

Rollison Road- new area



Could be used as a minor transition point or section turn. Such as the end of the long course ski leg with paddlers leaving the water and running to the facility and back to their craft to paddle back to the craft start area. It could if required be drop off point for paddle craft if the course was designed as such with the beach in close proximity to the car park area.

CYO'Conor Beach



CYO'Conor reserve and beach lend themselves to a transition area for the run and provide a beach / trail run area that includes the old Robb's Jetty marshalling yards and link via McTaggart to the Manning Park trail run section of 5 kilometres. While parking is adequate it could not sustain very large numbers. With the beach sand dunes it is not ideal for the start finish of a water section.

Port Coogee



Is an ideal area for both the long and short course paddle sections with a course through the canals and under the feature bridges. A high tide would only add to the event with paddlers having to tow paddle craft under bridges. A short swim section is possible from the beach area but not deemed appropriate.

Coogee Beach



Is the desired major finish area having both a beach location for paddle craft and skis and adequate grass area for marquees and finish set up requirements. Could also operate as the start of the event with a short beach run to a bike transition at Woodman Park recreation reserve.

Is only one of two potential finish areas having a café. Only limitation is movement from the caravan park which can be controlled as occurs with the Coogee Beach Triathlon.

Coogee Surf Life Saving Club (SLSC)



Provides an ideal logistic base for swim and ski operations. Either start or finish as the car park is close to the beach. Major advantage of the Coogee SLSC are the facilities and the café. It lacks a large grass finish area like Coogee Beach which is 500metres away.

Woodman Point Reserve



The Woodman Point Reserve has the advantage of large grass areas and car parks. It also has off road trails. While not of great length it could be used for a portion of the cycle section or a small linking run section. This would be an ideal for the transition area for the cycle section to the following section of either swim or paddle. The limitations may be the allowed use by the Department of Parks and Wildlife with respect to non-path use. This needs to be ascertained.

Lake Coogee



The Lake is a focal point of the area. The area surrounding the lake can incorporate a mountain bike course and although not longer than 5 kilometres it can be linked with Cockburn Road to provide a bike course. It appears that some of the area may be under Main Roads control for approvals. Additional distance could be achieved by using the Water Corporation land or extending the road section from Cockburn Road into McGrath Road and Fawcett Roads before entering the Lake Coogee precinct.

Manning Park



Manning Park is an ideal area for a run start, run, turn or section (transition) change for the event. The run would present challenging trails, the Manning Park stairs and can be linked to the CY O'Connor Reserve and the old Robb's Jetty yards. Traffic management crossing Cockburn Road is more than manageable. A 5 kilometre run section can be incorporated into the area.

Kogolup Lake and Thomson's Lake

From an initial investigation both Lakes could be part of a cycle section if the start area was at the new City of Cockburn ARC. While a more thorough survey of the lakes, trails and interconnecting roads is warranted it would only be undertaken if the start was at Cockburn Central. The main advantage would be the promotion of the "Wetlands" in the event concept.

8. COURSE OPTIONS

Two course distances are proposed to enable competitors of different abilities and interests to be involved. The Course design and timing takes into account potential disruptions and safety.

- a. The course design can be changed or modified to encompass the requirements of the City of Cockburn.
- b. They do not currently involve the areas of Cockburn ARC, Cockburn Central or Bibra Lake.

Course design and timing takes into account potential disruptions and safety:

1. Long Course – catering for individuals, pairs and teams who have the capacity to train and significant equipment. Classified as competitive.
2. Short Course – catering for the recreational, family and general participant who has the required equipment to be involved in the event. Classified as competitive/social.

Based on other events this one will be somewhat different in that the long course will have four sections and the shorter course will have four sections using the same transition areas. This can be modified where the longer course can be divided into 8 legs which will enable teams of up to 8 to be involved, increasing the potential number of those attending the event.

Disciplines

- Swim/s- entirely in the ocean at Coogee Beach
- Paddle/s- Includes the ocean and the Coogee Marina
- Run/s- includes coastal path, CY O'Connor area and Manning Park
- Mountain bike - Includes Woodman Point, Cockburn Road and Lake Coogee

Simple Model

Section	Discipline	Start Point	Finish Point	Distance	
1	Run	Coogee Beach	Woodman Point	2.0km	
2	Mountain Bike Trail	Woodman Point	Woodman Point+ Short Course Long Course	16.0km	+ Short Course Long Course
3	SWIM	Coogee SLSC	Coogee Beach	1.5km	+ Short Course Long Course
4	RUN	Coogee Beach	CY O'Connor Beach	5.0km	+ Short Course Long Course
5	RUN	Manning Park	CY O'Connor Beach	5.0km	+ Short Course Long Course
6	Paddle	Coogee SLSC	Coogee SLSC	10.0km	+ Short Course Long Course
6	Final Run	Coogee Beach	Coogee Beach	500m	Teams

Descriptions/Maps

The course outline is an initial course that can (and will) require some modifications/ additions pending.

- Approvals by appropriate regulatory authorities
- A revision of the Cycle Course that currently needs additional distance for the long course

Start

Run 1 – from Coogee Beach a run along the beach to Woodman Point Jetty and Reserve – 2 kilometres

Transition 1 – Woodman Point Grass area – from Run 1 to bike section

Bike section – from Woodman Point, via the Nyerburp Circle, Cockburn Road South, McGrath Road, Fawcett Road, entry to Lake Coogee, Circuit of Lake Coogee using trails, exit onto Cockburn Road south of Mayor, return to Woodman Park.

Run 2: Woodman Point to Coogee SLSC ski transition.

Paddle: from Coogee SLSC to Woodman Point (Jervoise Bay Sailing Club), across the bay to Rollinson Road Car Park, to Coogee Marina, internal Marina Section, return to Coogee SLSC swim transition.

Swim: from Coogee SLSC to the Marina wall and back to Coogee Beach grass transition.

Run 3: From Coogee Beach along the beach, climb Marina wall, along paths of Socrates Parade, Pats the Marina to Caledonia Loop, Climb down limestone wall, beach run to CY O'Connor reserve, trail to Robb Jetty feature, McTaggart Cove, across Cockburn Road into Manning Park for a 5 kilometre trail run including the steps. Return via McTaggart. Cove, CY O'Connor Beach, Coogee Marina and beach run to finish at Coogee Beach Reserve. Teams to form up at the Jetty to cross the line together.

9. CONSIDERATIONS:

- When to hold the event: Is the question that is always asked and there is no perfect answer.
- When are other similar events on? There is no reason not to hold this type of event when similar events are on as our population can sustain it.
- Impact of other events – the Jurien bay Triathlon is on the same day as the Rottneest Channel Swim and is very successful as the competitor group is of a different demographic
- When does it suit the City of Cockburn? The ideal months to consider would be late October through November or April through May. However, consideration should be given to days that are avoided by event organisers for no apparent reason – New Year's Day is an example. The City holds major events in September through to April. While the event management would be outsourced other service support such as marketing would be significant and therefore November would be the preferred month to hold the event.

10. BUDGET

A definitive budget can only be set when the event and it's requirements are finalised. Suffice to say some major items are listed below with potential costs and variations. Cost are based on similar high profile events:

INCOME:

- | | |
|--|----------|
| • Sponsorship – includes cash and offsets
(this may compete with the City's other sponsorship priorities) | \$20,000 |
| • Entry fees – based on 1000 competitors of
200 individuals (\$120 each) and 200 teams
of four (\$200 per team). Entry fees will depend
on what is offered and the event. | \$64,000 |
| • Income based on 50% subsidised by the City | \$32,000 |

COSTS:

- | | |
|--|-----------|
| • Event Director and Event Operations Crew
Would include the event management and sponsorship
management - Race director and 4 section assistant race
directors | \$50,000+ |
| • Medical – St John Ambulance | \$10,000 |
| • Water Safety – SLSC or similar | \$ 5,000 |

• Traffic Management – including plans	\$ 5,000 to \$15,000
• Equipment – including bike racks, marquees	\$ 5,000 +
• Event specific signage	\$ 1,000 to \$3,000
• Public address and commentator	\$ 3,000
• Event timing	\$ 10,000
• Marketing/PR	\$ 10,000
• Competitor race bibs, swim caps, bike numbers	\$ 5,000+
• Community consultation	\$ 2,000

NOTE: There are many other additional costs for which a list can be provided to determine event inclusions and exclusion.

Appendices

PERMISSIONS- STAKEHOLDERS

EVENT CHECKLIST

EVENT MANAGEMENT

SAMPLE MAPS

PERMISSIONS - STAKEHOLDERS

City of Cockburn

9 Coleville Crescent,
Spearwood 6163
08 9411 3444

Parks and Wildlife

John Graham Reserve
C/- Swan Regional Headquarters
Cnr Australia II Drive and Hackett Drive CRAWLEY
Locked Bag 104, Bentley Delivery Centre 6983
(08) 9442 0300

Main Roads Western Australia

enquiries @mainroads.wa.gov.au
Road Panned Interventions Manager
Main Roads Western Australia
Don Aitken Centre
Waterloo Crescent
East Perthv6004
08 9323 5478
0418 827 353 (Michael Priest)

Department of Transport –Marine Division

1 Essex Street
Fremantle 6160
1300 863 308

Coogee Beach Surf Life Saving Club / Surf Life Saving Western Australia

20 Poore Grove,
Munster WA 6166
08 9494 1433

Fremantle Sea Rescue

Tuckfield Street
Mews Road
Fremantle 6160
9335 1332

Water Corporation of Western Australia

629 Newcastle Street
Leederville 6007
131395

West Australian Police

Cockburn Police Office

Linkage Ave,

Cockburn Central WA 6164

State Traffic Intelligence, Planning and Coordination

SMAIL@police.wa.gov.au

Acting Sgt Brock on 6274 8564

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ROAD CLOSURES

The following procedure applies. The application is to be submitted allowing adequate time for assessment.

- Lodge application with Local Authority and obtain approval
- The process may require the applicant to hold a meeting with all stakeholders present
- Obtain Main Roads WA (MRWA) approval and abide by their conditions regarding signage and Traffic Management Plans (TMP) and Traffic Control Plan (TCP)
- Upon receipt of above, lodge the completed application form and other documentation with local police and pay the appropriate fee
- Application package, TMP, TCP and other supporting documentation are to be forwarded to STIPCU by local police for assessment by the Permits and Parades Officer. Successful applications are signed by the delegated commissioned officer at State Traffic Operations

IMPACT

The areas listed below are likely to be impacted upon or experience some disruption or delays to normal movement. The impact that is likely cannot be ascertained until a final course and event times are finalised

- Residents – Port Coogee Area
- Coogee Beach Shopping Centre - Woolworths
- Coogee Beach Café
- Wooman Point Kiosk
- Coogee Beach SLSC Café – Lizard Café
- Cockburn Power Boat Association
- Woodman Point Recreation Camp
- Coogee Beach Holiday Park
- Woodman Point Holiday Park
- There will be others pending final course design

CITY OF COCKBURN
Wetlands to Waves Adventure Race
Event Checklist

even task check list

Pre-Event Decisions

- Concept Finalised
- Race Inclusions Considered
- Budget for Initial Operations
- Appoint Race Director / Organisation

Race Checklist - Initial Tasks

- Race Date and Time
- Course Established
- Develop a Master Plan
- Course Approved by Authorities
- Course Certified (if required)
- Insurance
- Sanctioning (if required)
- Risk Management Plan

Medical Services

- Provider
- Facilities
- Emergency Plan
- Reporting Procedure
- Public Emergency

Communications

- Two Way Radios
- Mobile Phones
- Public Address
- Commentator

Entries / Marketing

- Web Creation
- Web / Online Administration
- Electronic Media
- Online Registration
- Media
- Advertising
- Posters / Entry Forms
- Results system

Event Timing

- Assess Timing Requirements
- Appoint a Timing Group
- Data Inputs
- Race Results
- Race Bibs
- Finish Arch
- Transition Arches

The Course

- Final Course Measure
- Transition areas designed
- Maps developed
- Rules for Each Course Section
- Parking Locations

Officials / Support Services

- Water Safety Groups - SLSC
- Course Marshals
- Accreditation
- Duty Lists

Event Operations / Equipment

- Signage - Course
- Signage - Advisory
- Vehicles / Transport
- Storage
- Lighting
- Marquees
- Tables / Chairs
- Information Boards
- Toilets
- Rubbish Removal
- Security
- Stage

Services

- Public - Café, Mobile Coffee
- Participants
- Parking Areas

Event Inclusions

- T- Shirts / Cap for Competitors
- T-Shirts for Officials
- Key Officials Uniforms
- Rehydration / Refreshments
- Awards / Medals
- Draw Prizes

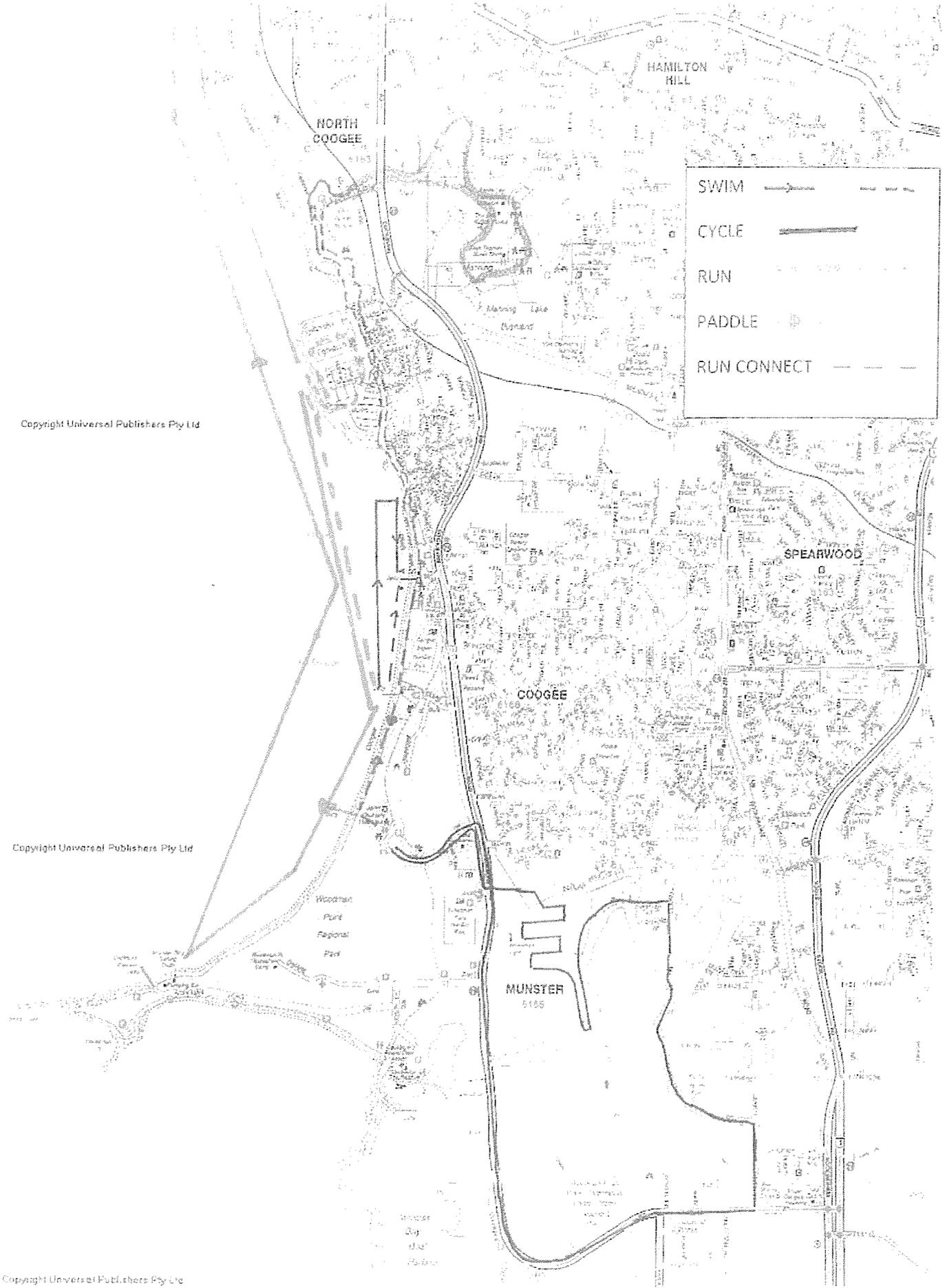
Event Management

The following deliverables should be considered by the event operators (City of Cockburn) in deciding the role of Event Management and the Event Operator.

- Create an event operations plan to include operations pathway, risk management Plan, medical plan and event day run sheet.
- Liaise with all authorities to ensure all permissions and necessary regulations and requirements are met.
- Provide City of Cockburn department managers (engineering, parks, recreation and environmental services) with event operations and risk management plans.
- Finalise the course requirements and design to the City of Cockburn requirements and inclusions.
- Appoint assistant race directors to oversee each event section and inclusion.
- Set up all course sections on the day of the event including transition areas, Start and finish, course signage, registration and athlete recovery areas.
- Contact and appoint timing provider, liaise with City of Cockburn to ensure correct data requirements are provided to timing operators.
- Co-ordinate and appoint relevant officials and organisations involved with the event for Safety (e.g. SLSC) and course control. Provide information and job descriptions for all personnel.
- Organise Medical coverage for the event and a Medical plan.
- Set up participant post recovery area.
- Liaise with sponsors to ensure their event day requirements are met.
- Install venue signage- sponsor and directional.
- Organise all take down and removal of event requirements including refuse removal.

Additional:

- Equipment
- Marketing
- Sponsorship



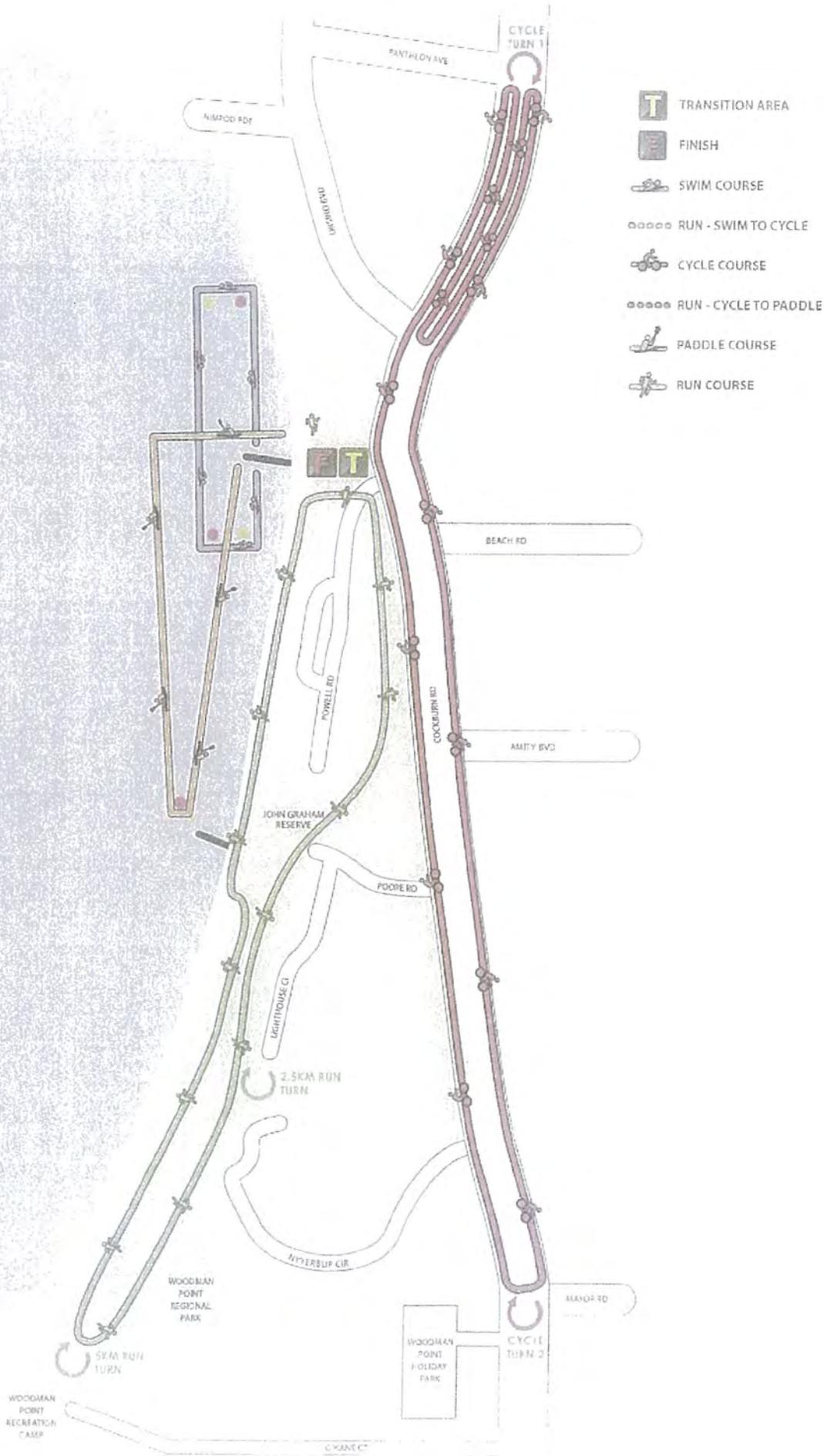
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COOGEE BEACH

COURSE MAP



Design by

0429 927 112

Don Green

Subject: RE: NOTICE OF MOTION

From: Cr Steven Portelli
Sent: Thursday, 11 May 2017 9:05 PM
To: Stephen Cain; Mayor - Logan K Howlett
Subject: NOTICE OF MOTION

RECOMMENDS TO COUNCIL THAT A

Community poll on Roe 8; 3000 with 1000 from each ward and spread evenly over each wards

Council:

- (1) undertake the Poll in relation to ratepayers position on Roe 8; and
- (2) approve an amount of \$14400+GST to conduct the Poll as outlined above.
- (3) submit results of poll to Council as soon as practical for their consideration And action.

Do you support the construction of a Roe 8 from the Kwinana Freeway to Stock Road?

Yes

No

Don't know

Don't care

Reason; because the Cockburn residents should be consulted and asked their opinion on such a critical piece of infrastructure.

At no time in the past has the community been given this opportunity.

It has only been a perceived view of Council and councillors fuelled by vital minority groups.

A previous proposed poll was raised under different circumstances and worded differently; contract was awarded so the discussion was moot. The wording was conducive to a negative response. (refer to motion and reasons by Myself on it)

PS:I thank the Mayor for his advice on standing orders on the question as to whether this motion qualifies as urgent business or whether to list as notice of motion to our next OCM..