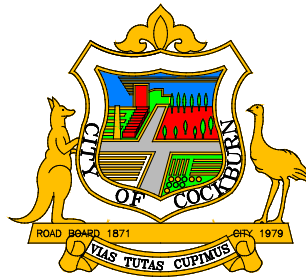


CITY OF COCKBURN



ORDINARY COUNCIL

AGENDA PAPER

FOR

THURSDAY, 13 AUGUST 2015

CITY OF COCKBURN

SUMMARY OF AGENDA TO BE PRESENTED TO THE ORDINARY COUNCIL MEETING TO BE HELD ON THURSDAY, 13 AUGUST 2015 AT 7:00 PM

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CITY OF COCKBURN**AGENDA TO BE PRESENTED TO THE ORDINARY
COUNCIL MEETING TO BE HELD ON
THURSDAY, 13 AUGUST 2015 AT 7:00 PM**

- 1. DECLARATION OF MEETING**
- 2. APPOINTMENT OF PRESIDING MEMBER (If required)**
- 3. DISCLAIMER (To be read aloud by Presiding Member)**

Members of the public, who attend Council Meetings, should not act immediately on anything they hear at the Meetings, without first seeking clarification of Council's position. Persons are advised to wait for written advice from the Council prior to taking action on any matter that they may have before Council.

- 4. ACKNOWLEDGEMENT OF RECEIPT OF WRITTEN DECLARATIONS OF FINANCIAL INTERESTS AND CONFLICT OF INTEREST (by Presiding Member)**
- 5. APOLOGIES AND LEAVE OF ABSENCE**
- 6. ACTION TAKEN ON PREVIOUS PUBLIC QUESTIONS TAKEN ON NOTICE**
- 7. PUBLIC QUESTION TIME**
- 8. CONFIRMATION OF MINUTES**
 - 8.1 (OCM 13/8/2015) - MINUTES OF THE SPECIAL COUNCIL MEETING - 29 JUNE 2015**

RECOMMENDATION

That Council adopt the Minutes of the Special Council Meeting held on Monday, 29 June 2015, as a true and accurate record.

COUNCIL DECISION

8.2 (OCM 13/8/2015) - MINUTES OF THE ORDINARY COUNCIL MEETING - 9 JULY 2015

RECOMMENDATION

That Council adopt the Minutes of the Ordinary Council Meeting held on 9 July 2015, as a true and accurate record.

COUNCIL DECISION

8.3 (OCM 13/8/2015) - MINUTES OF THE SPECIAL COUNCIL MEETING - 16 JULY 2015

RECOMMENDATION

That Council adopt the Minutes of the Special Council Meeting held on 16 July 2015, as a true and accurate record.

COUNCIL DECISION

9. WRITTEN REQUESTS FOR LEAVE OF ABSENCE

10. DEPUTATIONS AND PETITIONS

11. BUSINESS LEFT OVER FROM THE PREVIOUS MEETING (If adjourned)

12. DECLARATION OF COUNCILLORS WHO HAVE NOT GIVEN DUE CONSIDERATION TO MATTERS IN THE BUSINESS PAPER

13. COUNCIL MATTERS

13.1 (OCM 13/8/2015) - MINUTES OF THE AUDIT & STRATEGIC FINANCE COMMITTEE MEETING - 16 JULY 2015 (026/007) (S DOWNING) (ATTACH)

RECOMMENDATION

That Council receive the Minutes of the Audit and Strategic Finance Committee Meeting held on Thursday, 16 July 2015, and adopt the recommendations contained therein.

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COUNCIL DECISION

Background

A meeting of the Audit and Strategic Finance Committee was conducted on 16 July 2015.

Submission

N/A

Report

The Audit and Strategic Finance Committee received and considered the following items:

1. Risk Management Program Report
2. Internal Audit – Employee Time-Keeping
3. Annual Debt Write-Off
4. Internal Audit – Procurement Improvement
5. Interim External Audit

Strategic Plan/Policy Implications

Leading & Listening

- A responsive, accountable and sustainable organisation.

- Manage our financial and infrastructure assets to provide a sustainable future.
- A culture of risk management and compliance with relevant legislation, policy and guidelines

Budget/Financial Implications

As contained in the Minutes.

Legal Implications

As contained in the Minutes.

Community Consultation

N/A

Attachment(s)

Minutes of the Audit and Strategic Finance Committee Meeting – 16 July 2015.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

13.2 (OCM 13/8/2015) - MATTER FOR INVESTIGATION - COUNCIL MEETINGS IN THE COMMUNITY (086/003; 182/006) (D GREEN) (ATTACH)

RECOMMENDATION

That Council refers this matter to the Delegated Authority, Policies and Position Statements (DAPPS) Committee for consideration.

COUNCIL DECISION

Background

At the July 2015 Ordinary Council Meeting, Mayor Howlett requested the following matter be investigated:

Council Meetings in the Community

A report be presented to the August 2015 meeting of Council outlining a plan to introduce Ordinary Council Meetings in each ward of the district, i.e. 3 meetings to be held each year outside of the City's Administration Building.

Submission

N/A

Report

The place for the holding of Ordinary Council Meetings is governed by section 5.5(1) of the Local Government Act 1995. Further, Council Policy SC3 "Council Meetings" (copy attached) provides that all Council Meetings will be held in the Council Chamber.

Accordingly, a change of Policy will be required for Council to amend this process. It is therefore recommended that the matter be referred to the DAPPS Committee for consideration and recommendation back to Council.

Strategic Plan/Policy Implications

Policy SC3 'Council Meetings' refers.

Leading & Listening

- A responsive, accountable and sustainable organisation.

Budget/Financial Implications

N/A

Legal Implications

Sec. 5.5 of the Local Government Act 1995 refers.

Community Consultation

N/A

Attachment(s)

Council Policy SC3 'Council Meetings'.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

13.3 (OCM 13/8/2015) - MATTER FOR INVESTIGATION - ELECTRONIC EQUIPMENT USAGE POLICY (086/003; 193/001) (D GREEN)

RECOMMENDATION

That Council refers this matter to the Delegated Authorities, Policies and Position Statements Committee for consideration.

COUNCIL DECISION

Background

At the July 2015 Ordinary Council Meeting Cllr Smith raised the following matter for investigation:

That an Electronic Equipment Usage Policy be prepared and presented to a future Council Meeting.

Submission

N/A

Report

Council issues electronic equipment to all elected members and executive staff for use at Council and Committee meetings. These devices are primarily provided for the purposes of accessing documentation forwarded to them by the City's administration. One important document is the Council Meeting Agenda, which is downloaded for viewing purposes and referral during the Council

Meeting. However, Council does not have mechanism in place to regulate the usage of such devices (i.e. iPads and iPhones) during formal meetings, either by way of Policy, or by reference in the Standing Orders Local Law.

Accordingly, it is recommended that a Policy be considered for this purpose. As Council's DAPPS Committee has oversight of Policy matters, the matter should be referred to that Committee for consideration in the first instance.

Strategic Plan/Policy Implications

Leading & Listening

- A responsive, accountable and sustainable organisation.

Budget/Financial Implications

N/A

Legal Implications

N/A

Community Consultation

N/A

Attachment(s)

N/A

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

13.4 (OCM 13/8/2015) - MINUTES OF THE GRANTS AND DONATIONS COMMITTEE MEETING 29 JULY 2015 (162/003) (R AVARD) (ATTACH)

RECOMMENDATION

That Council receive the Minutes of the Grants and Donations Committee Meeting held on 29 July 2015 and adopt the recommendations contained therein.

COUNCIL DECISION

Background

Council established the Grants and Donations Committee to recommend on the level and nature of grants and donations provided to external organisations and individuals. The Committee is also empowered to recommend to Council on donations and sponsorships to specific groups and individuals.

Submission

To receive the Minutes of the Grants and Donations Committee and adopt the recommendations of the Committee.

Report

Council approved a budget for Grants and Donations for 2015/16 of \$1,200,000 to be distributed as grants, donations and sponsorship. The Grants and Donations Committee is empowered to recommend to Council how these funds should be distributed.

Strategic Plan/Policy Implications

Community & Lifestyle

- Community environments that are socially cohesive and embrace diversity.
- Communities that take pride and aspire to a greater sense of community.
- Promotion of active and healthy communities.

Leading & Listening

- A responsive, accountable and sustainable organisation.

Budget/Financial Implications

Council approved a budget for grants and donations for 2015/16 of \$1,200,000 to be distributed as grants, donations and sponsorship.

Following is a summary of the grants, donations and sponsorship allocations proposed by the Committee.

Committed/Contractual Donations	\$496,000
Specific Grant Programs	\$439,000
Donations	\$210,000
<u>Sponsorship</u>	<u>\$55,000</u>
Total	\$1,200,000

Total Funds Available	\$1,200,000
<u>Less Total of Proposed Allocations</u>	<u>\$1,200,000</u>
Balance	\$0

These allocated funds are available to be drawn upon in response to grants, donations and sponsorship applications from organisations and individuals.

The next round of grants, donations and sponsorship funding will open in mid-August and close on 30 September 2015.

Legal Implications

Nil

Community Consultation

The position of Council is for the availability of grants and donations to be advertised through the City's website, local media, Cockburn Soundings, Council networks and related means.

It is recommended that advertising commence immediately following the Council decision to ensure a wider representation of applications.

Attachment(s)

1. Minutes of the Grants and Donations Committee Meeting on 29 July 2015.

Advice to Proponent(s)/Submissioners

Applicants have been advised that this matter is to be considered at the 13 August 2015 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil

14. PLANNING AND DEVELOPMENT DIVISION ISSUES

14.1 (OCM 13/8/2015) - LEASE OF LOT 100 RIVERS STREET, BIBRA LAKE - LOCATION: RIVERS STREET, BIBRA LAKE - OWNER: CITY OF COCKBURN - APPLICANT: ARCH ENGINEERING PTY LTD (4412345) (L GATT) (ATTACH)

RECOMMENDATION

That Council enter into a lease agreement with Arch Engineering Pty Ltd for Lot 100 Rivers Street, Bibra Lake (Lot 100) subject to:

- (1) there being no objections to the proposal following advertising in accordance with Section 3.58 of the Local Government Act 1995;
- (2) the term being for a period of one (1) year at an initial annual rental of \$69,000 ex GST with CPI increases to the lease fee on each anniversary pursuant to option periods thereafter;
- (3) the lease including the option of a further one year extension;
- (4) Arch Engineering being responsible for any costs associated with the valuation, advertising and any costs associated with the preparation of the lease agreement; and
- (5) the 2015/16 Budget be amended accordingly to account for the new lease fee.

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COUNCIL DECISION

Background

Lot 100 is owned in freehold by the City of Cockburn and has an area of 3636m². It has been leased since 2005, as evidenced through the following Council decisions:

On 11 August 2005 Council resolved to lease Lot 100 Rivers Street, Bibra Lake to PIV Engineering Pty Ltd.

On 9 October 2008; 14 October 2010; and 26 October 2012 Council resolved to lease the property to Arch Engineering.

Each of these decisions was subject to the normal statutory requirements, including compliance with s3.58 and the lease terms and rental fee.

Submission

Arch Engineering Pty Ltd has written to the City requesting a new lease with a lower lease fee based on a valuation they have received and the downturn in the mining and related industries which has affected their business and cash flow. A copy of the letter and email detailing the recommended lease fee can be viewed at Attachment 1.

Report

Lot 100 is a vacant land parcel zoned industry with an area of 3636m². Rivers Street comprises of nine developed industry lots with the subject lot currently undeveloped. An extract of the aerial detailing the location and size of the lot can be viewed at Attachment 2.

The City has leased Lot 100 to Arch Engineering for storage of raw materials and hardstand since 2005. They have maintained a good relationship with the City over this time and when their lease came to an end in November 2014, the City chose to leave them on the holding over clause until such time as the City's Engineering Department had assessed the City's requirements for the new Operations Centre.

The Operations Centre adjoins the southern boundary of Lot 100 and the City's Engineering Department had been unable to recommend a new lease due the possibility of the land being required for the new operations building. The City's Engineering Department have now confirmed that the land will not be required for the new operations centre therefore the City is able to offer a new lease to Arch Engineering.

Arch Engineering is located at 9 Rivers Street adjoining Lot 100 and would like to continue leasing Lot 100 but they have requested a reduction in their lease fee based on a valuation they received and the downturn in the mining and related industries to which their organisation is tied.

Arch Engineering requested the lease fee be reduced in line with their valuation which detailed the current lease rates for industrial land in that area was between \$15.00per m2 to \$20.00per m2. Arch initially was prepared to offer \$15.00per m2 (\$54,540.00pa ex GST). The

current rent was \$25.45 per m² (92,552.73pa ex GST) therefore their offer appeared to be a considerable reduction.

The City is required to obtain a written valuation for the purposes of the advertising of this disposal under Section 3.58 of the *Local Government Act 1995*. Discussions with the City's Valuer confirmed that the market had come back and this is reflected in the written valuation confirming that a recommended lease fee of \$19.00 per m² rounded to \$69,000pa ex GST would be applicable. An extract from the written valuation can be viewed at Attachment 3.

Based on this valuation the City has offered, subject to Council decision and no objections from advertising, to enter into a lease for one year with a one year option at a lease fee of \$69,000pa ex GST with a CPI increase at each anniversary date. Arch have given their in principle support to the new lease fee of \$69,000pa ex GST commencing on 1 July 2015. The proposed lease being a standard commercial lease means that outgoings including Council rates will be paid by the lessee.

The lease was advertised for public comment on 15 July 2015 and there were no submissions received.

It is therefore recommended that Council consent to enter into a lease agreement with Arch Engineering Pty Ltd for a term of one year with a one year option at an annual lease fee of \$69,000pa ex GST be offered to Arch Engineering Pty Ltd.

Strategic Plan/Policy Implications

Growing City

- Investment in industrial and commercial areas, provide employment, careers and increase economic capacity in the City.

Leading & Listening

- A responsive, accountable and sustainable organisation.
- Manage our financial and infrastructure assets to provide a sustainable future.

Budget/Financial Implications

The budget revenue for the property will need to be decreased to \$69,000, and the budget amended accordingly.

Legal Implications

Section 3.58 Local Government Act 1995

Community Consultation

N/A

Attachment(s)

- 1 Copy of letter and email
- 2 Aerial view of Lot 100
- 3 Extract from Valuation

Advice to Proponent(s)/Submissioners

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 13 August 2015 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

14.2 (OCM 13/8/2015) - CONSIDER ADOPTION OF PROPOSED TOWN PLANNING SCHEME AMENDMENT NO. 108 EXTENSION OF DEVELOPMENT AREA 33 TO INCLUDE THE POWER STATION (109/044) (C CATHERWOOD) (ATTACH)

RECOMMENDATION

That Council

- (1) note no submissions were received in respect of Amendment 108 to City of Cockburn Town Planning Scheme No. 3 ("Scheme");
- (2) adopt Scheme Amendment No. 108 for final approval for the purposes of:
 1. Including Lot 2 and a portion of Lot 3 Robb Road, North Coogee, as shown on the 'proposed zoning' plan within the boundaries of Development Area No. 33.
 2. Amending the scheme map accordingly.
- (3) ensure the amendment documentation, be signed and sealed and then submitted to the Western Australian Planning Commission with a request for the endorsement of final

approval by the Hon. Minister for Planning.

COUNCIL DECISION

Background

For a number of years the state government has been working toward realising the vision for the Cockburn Coast development. The project is intended to see the redevelopment of the former Robb Jetty industrial area and the South Fremantle Power Station.

The Cockburn Coast District Structure Plan 2009 ("CCDSP 2009") was prepared to guide future land use and transport initiatives within the area stretching between South Beach and the Port Coogee marina.

In 2012, this was supplemented and in part refined by the Cockburn Coast District Structure Plan Part 2 ("CCDSP Part 2") prepared on behalf of LandCorp.

The Metropolitan Region Scheme ("MRS") Amendment No. 1180/41 was made effective on 16 August 2011 to rezone the majority of the Cockburn Coast industrial area from 'Industry' to 'Urban' to reflect the outcomes of the CCDSP Part 2. The South Fremantle Power Station site was rezoned to 'Urban Deferred'.

The Western Australian Planning Commission ("WAPC") identified that this site has a number of unique characteristics that would require further consideration prior to being rezoned to 'Urban'. The WAPC identified that for the Urban Deferment to be lifted a detailed Master Plan would need to be prepared for Lots 2, 3 and 2167 Robb Road, North Coogee.

To progress the planning for the Power Station site Landcorp engaged HASSELL to prepare a Master Plan on behalf of Synergy, the landowners of Lot 2 and 3 Robb Road, North Coogee. Following a public comment period, and subject to modifications to the Master Plan, Council have supported the lifting of Urban Deferred. A request to include the land within the 'Development' zone concurrent with the MRS amendment was also made.

Under Part 9 of the Planning and Development Act 2005, there are obligations on the local government to bring their town planning scheme into line with the MRS. The next amendment needed to the City's Town Planning Scheme No. 3 ("TPS3") is to extend the existing Development Area 33 ("DA33") over the power station lots.

Submission

N/A

Report

The purpose of this report is to consider submissions and final adoption of Scheme Amendment No. 108 to extend the existing Development Area 33 over the power station lots.

Council resolved to initiate the Amendment for the purposes of advertising at the Ordinary Meeting of 11 December 2014. It was advertised for public comment for a period of 42 days from 2 June to 14 July 2015. No submissions were received. This is not considered unusual given the administrative nature of this amendment.

Lots 2 and 3 Robb Road are owned by Synergy and include the Power Station structure. DA33 would relate to the whole of Lot 2 and a portion of Lot 3, that is only the portion to have the 'Urban Deferred' lifted, not the 'Parks and Recreation' reserved portion.

Including the land within the development area will enable Part 6 of TPS3 to be evoked, setting out the need and process for structure planning. It is the local structure plan that will guide subdivision and development of the land.

Conclusion

It is recommended that Council note there were no submissions received and adopt the amendment for final approval.

Strategic Plan/Policy Implications

Growing City

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.

Infrastructure

- Community facilities that meet the diverse needs of the community now and into the future.
- Facilities that promote the identity of Cockburn and its communities.

Community & Lifestyle

- Conservation of our heritage and areas of cultural significance.

A Prosperous City

- Creation and promotion of opportunities for destination based leisure and tourism facilities.

Environment & Sustainability

- To protect, manage and enhance our natural environment, open spaces and coastal landscapes.

Budget/Financial Implications

N/A

Legal Implications

Planning and Development Act 2005

Community Consultation

The Amendment was advertised for public comment for a period of 42 days from 2 June to 14 July 2015. No submissions were received.

Attachment(s)

1. Existing zoning plan (indicating surrounding zonings)
2. Proposed zoning plan (only shows land subject to rezoning)

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

14.3 (OCM 13/8/2015) - PROPOSED VARIATION TO PORT COOGEE STRUCTURE PLAN: OTHELLO QUAYS, NORTH COOGEE APPLICANT: MW URBAN (110/023) (C HOSSEN) (ATTACH)

RECOMMENDATION

That Council

- (1) pursuant to Clause 6.2.9.1(a) of City of Cockburn Town Planning Scheme No. 3 ("Scheme") adopt the Proposed Structure Plan for Lots 891, 892 and 893 Othello Quay, North

Coogee subject to the following modifications:

1. The Structure Plan Map (Plan 1) be relocated to the end of the Statutory Section (Part 1) of the Structure Plan documentation.
 2. Part 1 Section 6.1 being amended to include a mandated requirement for on-site visitor parking to be provided at a rate of 0.5 parking bays per dwelling.
- (2) endorse the Schedule of Submissions prepared in respect of the Proposed Structure Plan for Lots 891, 892 and 893 Othello Quay, North Coogee; and
- (3) advise the proponent, the Western Australian Planning Commission and those persons who made a submission of Council's decision.

COUNCIL DECISION

Background

The Port Coogee Structure Plan was originally adopted by Council in March 2004 in conjunction with the Scheme Amendment introducing Development Area 22 ("DA 22"). The Amendment was gazetted in June 2005.

There have been a number of modifications to the Structure Plan since its initial adoption.

The Port Coogee Structure Plan area is zoned 'Urban' under the Metropolitan Region Scheme ("MRS") and 'Development' under City of Cockburn Town Planning Scheme No. 3 ("Scheme"). The subject land is also located within Development Area 22 ("DA 22") and Development Contribution Area No. 13 ("DCA 13").

The purpose of this report is to consider a Proposed Structure Plan variation to Lots 891, 892 and 893 Othello Quay, North Coogee. The proposal seeks to provide a split coding on the land of Residential R25/40, compared to the current coding of R25.

Submission

The variation to the Port Coogee Structure Plan has been submitted by MW Urban on behalf of the landowners.

Report

The purpose of this report is for Council to consider whether it is prepared to adopt the proposed Structure Plan, in light of the assessment undertaken by officers and the advertising process that has taken place.

Proposed Variation to Structure Plan

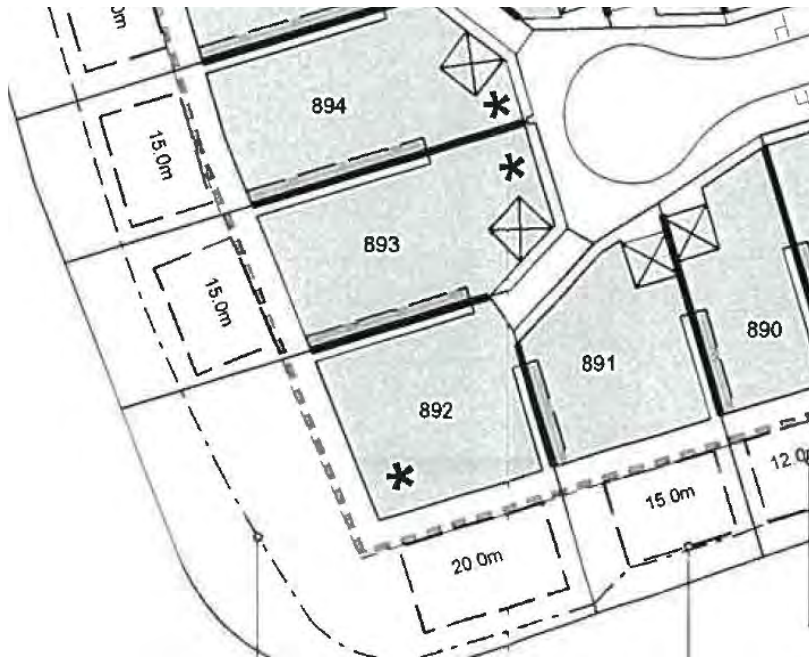
The Structure Plan proposal before Council proposes to introduce a split coding of Residential R25/R40 over the subject lots. The current density coding that applies to the land and the entirety of Othello Quay is R25.

The subject site would therefore retain an underlying density coding of R25, consistent with the remainder of Othello Quays, with a higher coding possible should a landowner or proponent meet certain requirements. These requirements are the preparation, submission and approval by the City of Cockburn of a Detailed Area Plan, demonstrating how a suitable comprehensive development outcome based on an R40 density could occur. If a Detailed Area Plan is not lodged and approved by the City, all development on the subject site would be required to conform to the requirements of the R25 coding.

Built Form/Massing

All built form massing and building design within the Port Coogee Structure Plan area are controlled by the Port Coogee Design Guidelines and Detailed Area Plans.

Currently the Port Coogee – Othello Quays Detailed Area Plan applies to the subject site. The Detailed Area Plan outlines building bulk, mandatory garage locations, setbacks to ground and upper floors and height, among other elements. An excerpt of the relevant Detailed Area Plan below is included for reference.



The image depicts the building envelope and designated garage locations

The applicant has provided current and potential massing diagrams to elaborate on any prospective issues that would arise from the proposed change in density (Attachment 3).

The applicant has also submitted a draft Detailed Area Plan that would prospectively be lodged with the City should the Council consent to the zoning change. This draft Detailed Area plan proposed a number of changes to the current built form massing, designed to reduce the bulk and scale of any proposed multiple dwelling (Attachment 4).

The draft Detailed Area Plan proposes to increase the side setbacks to 2m as a minimum, reduce the allowable building height to 8.75 m (1.25m below the current allowance) and reduce the number of crossovers to Othello Quays. The draft Detailed Area Plan also proposes to increase the allowable front setbacks to a setback consistent with the existing single residential dwellings on Othello Quays, negating the reduced setback allowable on land coded R40.

The building built form was raised by a number of the individuals making submissions.

Parking and Access

It is expected that any proposal for multiple dwellings on the subject site will result in a small increase in vehicle numbers. The endorsed

Transport Report for the Port Coogee Structure Plan classifies Othello Quays as a residential access street. Stating that:

“These streets are intended to provide access to abutting properties and service local trips within the development. Traffic volumes are estimated to be less than 1,000 vpd for the majority of these streets with a target speed environment of 40 km/hr.”

The same report states that the number of vehicle trips per day from single residential dwellings can be expected to be seven. Othello Quays based on current density can therefore be expected to generate 196 vehicle movements per day. A proposal for multiple dwellings is expected to yield a maximum 12 dwellings on the existing three residential lots. Utilising the same assumptions, the proposed change in density would generate approximately 259 vehicle movements per day on Othello Quays. It should be noted that standard traffic engineering principles state that the higher the density a lesser number of trips per day from a dwelling are to be expected.

Therefore the local road network, regardless of the built form outcome on the subject site, will be well below the reasonably expected traffic volumes for a local access street.

Secondary to matters related to traffic volumes is the provision of off-street parking on the subject site and assumed additional demand for on street parking should multiple dwellings be proposed.

The draft Detailed Area Plan for the site proposes visitor parking for any multiple dwelling to be provided at a rate of 0.5 per dwelling. This is double the amount prescribed under the Residential Design Codes. The Officers' Recommendation proposes to establish this as an enforceable statutory provision under the Part 1 of the Structure Plan. Therefore should 12 multiple dwellings be established on site, a total of 6 visitor parking bays will be established on top of the required parking for each individual dwelling.

Community Consultation

The proposed Structure Plan was advertised in writing to all landowners in Othello Quay. Landowners were given a period of 21 days to respond, with letters being posted on 28 May 2015 and response's being required by 22 June 2015. A total of three submissions were received and all those submissions are set out and addressed in the Schedule of Submissions (Attachment 5).

Two submissions were received from individual landowners on Othello Quays and one submission was from the Coogee Progress Association. All three submissions objected to the proposal. The

matters raised in the submissions are discussed in detail below and within the Schedule of Submissions.

The matters raised can be broadly grouped into three categories:

1. Concerns relating to Traffic Volumes and Parking.
2. Building Bulk and Scale – Changes to the Amenity of the Street and Area.
3. Change occurring post residents' expectations for the estate.

A number of other matters were also raised. These are systematically addressed in the Schedule of Submissions.

Traffic Volumes and Parking:

The applicant has outlined, by way of a traffic impact assessment, the likely impact of the proposed zoning change. The traffic impact assessment was carried out in line with standard professional expectations. Assumptions made around the expected trip generation has found that with the additional 9 dwellings the traffic volumes on Othello Quays would only be approximately $\frac{1}{4}$ of the maximum traffic such a road is capable of accommodating.

The applicant has noted in the draft Detailed Area Plan that they intend to provide visitor parking at a rate of 0.5 per dwelling; this is double the requirement of the Residential Design Codes. The officers' recommendation proposes to mandate this through Part 1 of the Structure Plan to provide certainty. This matter is also addressed in the Report portion of this piece. These concerns are therefore considered to be able to be adequately addressed.

Building Bulk and Scale:

The suggested built form controls to be included for any R40 development proposes additional restrictions placed on any such proposals. Building height, side setbacks and front setbacks have been proposed to be increased beyond the current Detailed Area Plan to reduce building bulk, maintain important sight lines and integrate the development into the surrounding urban fabric. Building mass models have been included for reference.

This matter is also addressed extensively in the preceding officers report and in the Schedule of Submissions. This concern is therefore able to be adequately addressed.

Structure Plan changes:

All landowners are able to lodge a request to modify the existing approved Structure Plan for the Council's consideration. The proposal is site specific. Any further proposals for changes to residential density in Port Coogee, or any other part of Cockburn, would be individually judged on their respective planning merits. This concern is therefore not relevant.

This proposal as judged against planning related matters and decisions should be determined on the basis of proper and orderly planning.

Conclusion

It is recommended that Council adopt the Proposed Structure Plan, Lots 891, 892 and 893 Othello Quay, North Coogee subject to modifications.

Strategic Plan/Policy Implications

Growing City

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.
- Development that is soundly balanced between new and existing areas.

Leading & Listening

- A culture of risk management and compliance with relevant legislation, policy and guidelines.

Budget/Financial Implications

Nil.

Legal Implications

N/A

Community Consultation

The Proposed Structure Plan was advertised in writing to all landowners in Othello Quay. Landowners were given a period of 21 days to respond, with letters being posted on 28 May 2015 and response's being required by 22 June 2015. All submissions that were received are set out and addressed in the Schedule of Submissions (Attachment 5). A total of three submissions were received. The issues raised in the submissions have been addressed in the report.

Attachment(s)

1. Location Plan
2. Structure Plan Map
3. Building Massing Plans
4. Draft Detailed Area Plan
5. Schedule of Submissions

Advice to Proponent(s)/Submissioners

The proponent and those who have lodged a submission on the proposal have been advised that this matter is to be considered at the 13 August 2015 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

14.4 (OCM 13/8/2015) - REVISED DETAILED AREA PLAN STAGE 4C SEASPRAY - LOCATION: OTHELLO QUAYS, NORTH COOGEE - OWNERS: MULTIPLE - APPLICANT: TAYLOR BURRELL BARNETT. (6011666) (D BOTHWELL) (ATTACH)

RECOMMENDATION

That Council

- (1) approve the subject revised Detailed Area Plan for Stage 4C "Seaspray" at Othello Quays, North Coogee in accordance with Clause 6.2.15 of Town Planning Scheme No. 3;
- (2) delete the existing Detailed Area Plan for Stage 4C "Seaspray" at Othello Quays, North Coogee; and
- (3) advise those who own land within the area covered by the Detailed Area Plan and those who made submissions of Council's decision.

COUNCIL DECISION

Background

At its meeting on 11 December 2008, Council approved a Detailed Area Plan (DAP) and Jetty Design Guidelines (JDG's) for Stage 4C within the Port Coogee development area. The lots within the DAP area were zoned R25 under the Port Coogee Local Structure Plan. The intent of the DAP was primarily aimed at guiding built form outcomes for the 28 single residential waterside lots whilst the JDG's dealt with the control and development of jetties and related structures in the mooring envelopes.

At its meeting on 14 November 2013, Council approved a revised version of the subject DAP. The proposed changes related to Lots 24-27 which resulted from the subdivision of existing lots 300, 301, 880 and 881. Conditional subdivision approval to create revised lot areas but no additional lots was issued by the Western Australian Planning Commission (WAPC) (Ref Nos. 147286 and 147334) and included conditions requiring that the existing DAP and JDG's to be modified to reflect the new lot areas.

On 13 March 2015, the WAPC conditionally approved a subdivision of existing lots 987-994 Othello Quays. The proposal involved the subdivision of the 5 existing lots into 8 new lots. It is to be noted that all of the proposed new lots meet the minimum and average lot size as well as the minimum frontage requirement as required under the R25 zoning. One of the conditions of the subdivision was for a revised DAP to be submitted to and approved by the City. The subject DAP was lodged with the City for determination on 9 April 2015.

Submission

The attached DAP addresses the following matters as required in the condition of subdivision approval;

- Mandated garage setback;
- Garage integration into the dwelling;
- Upper storey to overhang the majority of the garage below;
- Street and side setbacks of dwellings;
- Reduced garage and crossover widths;
- Garage and crossover location and pattern;
- Interface with setback requirements of existing DAP;
- Minimum two storey height requirement;
- Dry lot boundaries, fencing and mooring envelopes; and
- Amended jetty design guidelines.

Where the DAP does not refer to an alternate standard, the applicable standard is that prescribed in the Residential Design Codes (R-Codes)

or the City's Town Planning Scheme No.3 and/or policies where the R-Codes do not apply.

Report

Approval is required in accordance with the provisions of section 6.2.15.5 of Town Planning Scheme No. 3. TPS No. 3 Clause 6.2.18.8 provides for a DAP to be amended.

The proposed DAP provides a site specific layer of planning information to be considered in the design and development of the subject lots. The information is to be considered within the framework of the Structure Plan adopted by Council for Port Coogee, as well as the R-Codes and the City's Planning Scheme and/or Policies.

Consultation

The proposed DAP was advertised to those who own lots within the DAP area. A total of eight (8) submissions were received including six (6) objections, one (1) support and one (1) submission of general comment. The objections raised the following issues:

1. Concern over impact of garages negatively impacting on streetscape.
2. Concern over smaller lots and lot frontages with permitted nil side setbacks.
3. Concern that footpath will be obstructed with vehicles.
4. Street setbacks should be increased to allow for visitor parking.
5. Concern car parking will spill into streets.
6. Concern that their property values will decrease.
7. Concern over issues associated with Jetties/Canal Walls.
8. Concern over intensification of waterways.
9. Seeking clarification over the provision of NBN.
10. Concern over issues associated with rubbish collection.

Provisions

The following provides an assessment of the key provisions of the amended DAP and the points of difference from the previous DAP to facilitate the proposed new lots 987-994.

Mandated 4.5m Garage Setback

In the previously approved DAP, a garage setback of 4.5m from the Primary Street is encouraged to provide off-street visitor car parking. It was considered that mandating this 4.5m garage setback was particularly important for the proposed new lots 987-994. The provision of a mandated garage 4.5m setback will allow for visitor car parking to

be provided off-street and reduce the impact of visitors parking within the Othello Quays road reserve.

The mandated 4.5m garage setback is also considered to reduce the impact of building bulk from the garages as seen from the street. The mandated setback of 4.5m from the front lot boundary is considered to be a sufficient distance in ameliorating any potential adverse impacts on the streetscape as a result of the additional lots and associated garages.

Maximum Garage Door Width

The existing DAP requires garage openings to be limited to a total maximum of 6.0m. For the proposed new lots 987-994, it was considered that a reduced maximum width of the garage would be a required provision given the proposed narrower lots proposed. As such, the revised DAP requires a maximum width of 5.5m for the garage and its supporting structures which would mean that the garage door opening would be approximately 5.0m, the minimum width for a double garage. The reduced garage width for the proposed new lots 987-994 is considered to assist in reducing any adverse impacts from the garages on the streetscape.

Designated Garage Locations and Dwelling Setbacks

The revised DAP has specific designated garage locations and dwelling setbacks which ensure continuity to the existing DAP and consistency in the Othello Quays streetscape. The garage locations have also been mandated to provide for suitably located lot servicing connections. Garage locations have been designated in pairs on the previously approved DAP to emphasise development massing and symmetry to the street. The designated garage locations for lots 987-994 continue this pattern of pairing the garage locations together ensuring consistency and continuity within the streetscape.

The existing DAP reflects the intended streetscape character for Othello Quays, that of an urban street tightly framed with built form. This is achieved through reduced building setbacks from the street as well as side boundaries. The proposed street setback requirements as set out in the revised DAP for the dwellings associated with lots 987-994 are consistent with those of the other lots on the Island. The setbacks to the respective side boundaries of the proposed new lots are also consistent with the setback requirements of the other existing lots. Examples of this are the permitted nil side setback 15m from the front lot boundary and the minimum 4m rear setback from the dwelling to the dry lot boundary.

Upper Floor Overhang

One of the main objectives of the amended DAP was to ensure that the amenity of the desired Othello Quays streetscape is protected. To alleviate the potential undue impact in terms of garage dominance, the City was keen to impose a requirement on the DAP for the upper level to overhang the garage below to provide articulation as viewed from the street and integration into the front elevation.

Accordingly, a provision has been included in the revised DAP for a mandatory requirement for the upper floor of the dwelling associated with the new proposed lots 987-994 to overhang the majority (at least 50%) of the garage by a minimum of 0.5m. Mandating the upper floor overhang above the garage by a minimum of 0.5m ensures that the garage is integrated into the front façade, reducing the impact of building bulk from the garage and provides visual articulation as viewed from the street.

Garage Door Translucent/Similar Colour to Dwelling

The potential for the garage door itself to have a negative visual impact on the streetscape for the lots 987-994 has also been considered for the subject revised DAP. A garage door has the potential to stand out and have an undue impact on the streetscape and surrounding amenity if it is not translucent or of a similar colour to the main dwelling. In conducting a site visit to Port Coogee and observing similar front loaded dwellings, the garages which stood out were the garages that did not have these treatments to their garage doors.

Given the relatively narrow frontages of some of the proposed new lots, the City wanted to include a provision in the DAP for the proposed dwellings associated with the new lots 987-994 to have garage doors which are either translucent or a colour which is a similar colour to the dwelling. These mandated treatments of the garage door are considered to further ameliorate the potential negative impact of building bulk on the streetscape as a result of the garage and effectively integrate into the front elevation of the dwelling.

Retention of Street Trees

The existing Norfolk Island Pine Trees within the Othello Quays road reserve are considered to be an important and valued feature of the streetscape. Accordingly, the City wanted to ensure that the creation of lots 987-994 would not impact on these street trees with a provision for the retention of these trees reflected in the DAP.

As such, the DAP includes a provision for the final driveway width to be determined by the location of the existing verge tree and planter, which

must be retained. In addition, there is an additional provision for all existing street/verge trees to be retained. Given that these trees are relatively mature, there is no scope for them to be relocated. The provision in the DAP ensuring the retention of the existing street trees is considered to be essential with the Norfolk Island Pine Trees considered to be a long term asset of the Othello Quays streetscape contributing to the overall amenity of the Island and the immediate locality.

Dry Lot Boundaries, Fencing, Mooring Envelopes and Jetty Guidelines

As per the subdivision conditionally approved by the WAPC, only lots 987, 989, 990, 992 and 994 are proposed as marina waterfront lots. The DAP clarifies that for lots 988, 991 & 993, the dry lot boundary is the cadastral boundary. The DAP plan also identifies that access to mooring envelopes is not permitted for these 'dry lots'.

To ensure that access to the mooring envelopes is restricted, the provision of fencing for these dry lots was a required provision to be incorporated into the revised DAP. As such, a clause has been included pertaining to lots 988, 990, 991 and 993, for the rear common boundary fence being a maximum of 1.2m and suitably finished in materials to complement the external walls of the adjacent dwellings. In addition, any fencing to the rear common boundary shall not obstruct the drainage openings in the sea wall.

In addition to the revised DAP, the Port Coogee Jetty Design Guidelines – Stage 4C has also been revised. While there is no increase in the number of mooring envelopes as a result of the proposed lots 987-993, the Jetty Guidelines were required to be amended as they refer to the specifics of each lot. As such, the required minor amendments have been made to the guidelines to reflect the subject subdivision.

Although not relevant to the DAP, it is to be noted that a condition of subdivision approval was for an easement for the benefit of the respective dry lots over the wet lots for the purpose of access and drainage as required.

An additional provision relating to drainage has also been included in the revised DAP applicable to all lots. The provision emphasises to potential purchasers on the lots on Seaspray Island that soak wells are not permitted in the building exclusion area (rear 4m setback).

The submissions received during the consultation period have been duly noted. It is considered that the concerns that were raised relating to streetscape issues and the potential undue impact from the garages

have been sufficiently addressed above through the provisions incorporated into the DAP as outlined in detail above.

The mandated 4.5m setback to the garages to allow for visitor parking within the lot is considered to have addressed the concerns raised relating to visitor car parking spilling into the street. Concern raised during the consultation period relating to the Jetties is considered to have been dealt with under the above section of this report titled *Dry Lot Boundaries, Fencing, Mooring Envelopes and Jetty Guidelines*.

Concern expressed over the intensification of the waterways is not supported as there will be the same number of mooring's as there was in the previously approved DAP. Similarly, the concerns relating to rubbish collection not supported with the City's Waste Management confirming that there would be no issue with the collection of bins to the subject lots. The issue raised in relation to the provision of NBN is not a planning matter, however, clarification was sought from the applicant on this matter with a response received that this issue was not relevant to the DAP.

The concern expressed that the additional lots will devalue their properties has been noted. Although this is not a valid planning matter, it is considered that the development of the subject lots will contribute to the vitality of the area with the majority of the lots on the Island sitting vacant and undeveloped.

It is to be noted that if Council resolved the revised DAP, the WAPC has the ability to clear the condition relating to the DAP being approved by the City without an amended DAP in place. This would mean that any development on lots 987-994 would only have to satisfy the provisions of the R-Codes and would not have the same requirements as the other existing lots on Othello Quays. This would result in the development of these lots which is not in a consistent manner to the existing lots in the DAP area which would have a detrimental impact on the streetscape and surrounding amenity.

Conclusion

It is considered that the revised DAP adequately maintains the original development principles and lot interface requirements as set out in the previously approved DAP. The provisions that are incorporated into the revised DAP such as the mandatory 0.5m upper floor overhang above the garage; reduced garage width and treatments of the garage door are all considered to ameliorate the impacts of building bulk from the garage on the streetscape. The required 4.5m setback to the garage is considered to allow for the provision of on-site visitor car parking. The designated garage locations and dwelling setback ensure continuity to the other existing lots within the DAP area and the requirement for no

existing street tree to be removed is considered to protect the amenity of the streetscape for the long term. The matters associated with the dry lot boundaries and associated fencing, moorings and amended jetty guidelines have also been sufficiently addressed as outlined above. For these reasons, the proposed revised DAP should be approved.

Strategic Plan/Policy Implications

Infrastructure

- Development that is soundly balanced between new and existing areas.
- Diversity of housing to respond to changing needs and expectations.

Community & Lifestyle

- Communities that are connected, inclusive and promote intergenerational opportunities.

Budget/Financial Implications

N/A

Legal Implications

Town Planning Scheme No. 3
Planning and Development Act 2005
State Administrative Tribunal Regulations

Community Consultation

Community consultation was undertaken; please refer to the Consultation section of the report above.

Attachment(s)

Proposed Revised Detailed Area Plan.

Advice to Proponent(s)/Submissioners

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 13 August 2015 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

14.5 (OCM 13/8/2015) - RECONSIDERATION OF PLANNING APPLICATION PETROL FILLING STATION & SIGNAGE 224 (LOT 55) CLONTARF ROAD HAMILTON HILL (2206189) (A LEFORT) (ATTACH)

RECOMMENDATION

That Council pursuant to S31 of the State Administrative Tribunal Act 2004 (WA), reconsider its previous decision of refusal and grant planning approval for a petrol filling station and associated signage at 224 (Lot 55) Clontarf Road Hamilton Hill, in accordance with the attached plans and subject to the following conditions and footnotes:

Conditions

1. Prior to the lodgement of a building permit application, a Noise Management Plan shall be submitted to and approved by the City. Recommendations and measures contained in the approved noise management plan shall be implemented at all times to the satisfaction of the City.
2. Prior to the lodgement of a building permit application, a Traffic Management Plan shall be submitted to and approved by the City. Recommendations and measures contained in the approved Traffic Management Plan shall be implemented at all times to the satisfaction of the City.
3. Prior to the lodgement of a building permit application, a Pollution and Drainage Plan shall be submitted to and approved by the City. Measures contained in the approved plan shall be implemented at all times to the satisfaction of the City.
4. Prior to the lodgement of a building permit application, a Lighting Management Plan shall be submitted to and approved by the City. Measures contained in the approved Lighting Management Plan shall be implemented at all times.
5. Prior to the lodgement of a building permit application, a Safety and Risk Management Plan shall be submitted to and approved by the City. Measures contained in the approved Safety and Risk Management Plan shall be implemented at all times.

6. Prior to the lodgement of a building permit application, a detailed colour and material schedule shall be submitted to and approved by the City for approval. The approved colour and materials schedule shall be implemented thereafter.
7. Prior to the lodgement of a building permit application, a detailed landscaping plan shall be submitted to and approved by the City, and shall include the following:-
 - (1) the location, number, size and species type of existing and proposed trees and shrubs, including calculations for the landscaping area;
 - (2) any lawns to be established;
 - (3) any existing landscape areas to be retained;
 - (4) those areas to be reticulated or irrigated; and
 - (5) verge treatments.
8. Landscaping (including verge planting) shall be installed, reticulated and/or irrigated in accordance with the approved landscaping plan and maintained thereafter to the satisfaction of the City. The landscaping shall be implemented during the first available planting season post completion of development and any species which fail to establish within a period of 12 months from planting shall be replaced to the satisfaction of the City.
9. Prior to the lodgement of a building permit application, a design audit shall be undertaken demonstrating safe manoeuvring for vehicles exiting the site via Carrington Street to the satisfaction of the City.
10. All stormwater shall be contained and disposed of on-site to the satisfaction of the City.
11. Walls, fences and landscape areas are to be truncated within 1.5m of where they adjoin vehicle access points where a driveway and/or parking bay meets a public street or limited in height to 0.75m.
12. All plant and equipment (such as air conditioning condenser units and communications hardware etc) shall be screened from public view to the satisfaction of the City.
13. The vehicle crossovers must be designed and constructed in accordance with the City's requirements.
14. Prior to use of the development hereby approved, vehicle

parking bays, vehicle maneuvering areas, driveways and points of ingress and egress shall be sealed, kerbed, drained, line marked and made available for use to the satisfaction of the City.

15. The premises shall be kept in a neat and tidy condition at all times by the owner/occupier to the satisfaction of the City.
16. The development site must be connected to the reticulated sewerage system of the Water Corporation before commencement of any use
17. Earthworks over the site and batters must be stabilised to prevent sand or dust blowing, and appropriate measures shall be implemented within the time and in the manner directed by the City in the event that sand or dust is blown from the site.
18. No washdown of plant, vehicles or equipment is permitted on the premises.
19. No vacuum services are to be provided for customer vehicles on-site.
20. The supply, storage or sale of LPG gas is not permitted.
21. Prior to commencement of the use of the site for petrol filling (post construction), a report from the builder/developer confirming compliance with the requirements of the acoustic report and that any structural recommendations of the report shall be incorporated into the development, to the satisfaction of the City.
22. During the construction phase, no activities causing noise and/or inconvenience to neighbours being carried out after 7.00pm or before 7.00am, Monday to Saturday, and not at all on Sunday or Public Holidays.
23. No bunting is to be erected on the site. (Bunting includes streamers, streamer strips, banner strips or decorations of similar kind).
24. No person shall install or cause or permit the installation of outdoor lighting otherwise than in accordance with the requirements of Australian Standard AS 4282 - 1997 "Control of the Obtrusive Effects of Outdoor Lighting".

25. A minimum of ten (10) bicycle stands/racks that conform to Australian Standard 2890.3 shall be provided in close proximity to the entrance of the building prior to occupation of the building.
26. The hours of operation of the approved petrol filling station (including convenience store component) are restricted to between 6:00am to 10:00pm seven days per week.
27. Prior to the commencement of the use of the site for petrol filling, the existing fence along the western boundary of the subject site (abutting No. 222 (Lot 3) Clontarf Road) shall be replaced with a 2.4m high masonry wall and associated crash barriers as marked in red on the approved plans to the satisfaction of the City.

Footnotes

1. This is a Planning Approval only and does not remove the responsibility of the applicant/owner to comply with all relevant building, health and engineering requirements of the City, with any requirements of the City of Cockburn Town Planning Scheme No. 3, or the requirements of any other external agency.
2. A sign licence is required to be submitted to the City's Building Services Department in accordance with the City of Cockburn Local Laws, Section 8.5 of Part viii; *Signs, Hoardings and Bill Posting Local Laws*.
3. The primary use of the development hereby approved is 'Petrol Filling Station' defined in the City of Cockburn Town Planning Scheme No. 3 as *"land and buildings used for the retailing of fuel and petroleum products and may include a convenience store with a floor area not exceeding 300 square metres, but does not include a workshop for mechanical repairs or the servicing of vehicles or machinery"*.
4. With regards to Condition 1, the Noise Management Plan shall confirm that all recommendations made in the Environmental Acoustic Assessment submitted by Herring Storer Acoustics dated 10 October 2014 (Ref 18380-1-14211) and Additional Information dated 28 May 2015 have been incorporated into the proposed development and the design and location of all mechanical plant within the development will not result in noise emissions exceeding those set out in the *Environmental Protection (Noise)*

Regulations 1997 (as amended).

5. With reference to Condition No. 10, all stormwater drainage shall be designed in accordance with the document entitled "Australian Rainfall and Runoff" 1987 (where amended) produced by the Institute of Engineers, Australia, and the design is to be certified by a suitably qualified practicing Engineer or the like, to the satisfaction of the City, and to be designed on the basis of a 1:100 year storm event. This is to be provided at the time of applying for a building permit.
6. All food businesses must comply with the *Food Act 2008* and Chapter 3 of the Australia New Zealand Food Standard Code (Australia Only). Under the *Food Act 2008* the applicant must obtain prior approval for the construction or amendment of the food business premises.
7. An application to Construct or Alter a Food Premises must be accompanied by detailed plans and specifications of the kitchen, dry storerooms, coolrooms, bar and liquor facilities, staff change rooms, patron and staff sanitary conveniences and garbage room, demonstrating compliance with Chapter 3 of the Australia New Zealand Food Standard Code (Australia Only).
The plans are to include details of:
 - (i) the structural finishes of all floors, walls and ceilings;
 - (ii) the position, type and construction of all fixtures, fittings and equipment (including cross-sectional drawings of benches, shelving, cupboards, stoves, tables, cabinets, counters, display refrigeration, freezers etc); and
 - (iii) all kitchen exhaust hoods and mechanical ventilating systems over cooking ranges, sanitary conveniences, exhaust ventilation systems, mechanical services, hydraulic services, drains, grease traps and provisions for waste disposal.
8. The development is to comply with the noise pollution provisions of the *Environmental Protection Act 1986*, and more particularly with the requirements of the *Environmental Protection (Noise) Regulations 1997*.
9. The waste storage areas must be of an adequate size to contain all waste bins. Each waste area must be provided with a hose cock, a concrete wash-down pad graded to a 100mm diameter industrial floor waste, and connected to an approved waste water disposal system. If external, the

bin storage area can be centrally located within the development but must be appropriately screened to a height of 1.8m.

10. You are advised that Department of Mines and Petroleum (Resources Safety) approval is required for the storage of some of the materials included in this approval. Please provide documents confirming the plans have been assessed by the Department of Mines and Petroleum prior to the lodgement of a Building Permit Application for this development. Guidance on the use, storage, disposal and special ventilation requirements for hazardous, toxic, ionising or non-ionising material or equipment should be obtained from the Resources Safety Section of the Department of Mines and Petroleum.

COUNCIL DECISION

Background

The subject site is located at the intersection of Clontarf Road and Carrington Street in Hamilton Hill. The site is bounded by Carrington Road to the east, Clontarf Road to the south, a single storey residential dwelling to the west and commercial building to the north. Current vehicle access to the site is from Clontarf Road and via an easement through the adjacent site (Lot 41 Carrington Street) to the north. The site is commercially zoned and contains a single level disused fast food outlet building (KFC), associated car parking and landscaping.

A planning application for a Petrol Filling Station and Signage at the subject site was determined by Council at its ordinary meeting held on 12 February 2015 with the following resolution made:

‘...that Council

- (1) *refuse to grant planning approval for a petrol filling station and signage at No. 224 (Lot 55) Clontarf Road Hamilton Hill based on the following reasons:*

1. *The separation distance between the proposed petrol filling station and the existing residential dwellings is considered insufficient and is likely to negatively impact on the amenity of nearby residents with regards to noise, odour and other*

emissions which is inconsistent with the aims of Town Planning Scheme No.3 as outlined in Clause 1.6.1.

2. *The proposal is in close proximity to an existing petrol filling station (within 200m) and therefore this proposal is considered unnecessary.*

(2) notify the applicant and those who made a submission of Council's decision.'

Subsequent to Council's decision, the applicant exercised their right to apply for a review of the decision by the State Administrative Tribunal (SAT). In response, Council engaged an external Planning Consultant to represent them in SAT where three mediation sessions have been held between the applicant, external planning consultant, several Elected Members and staff. Neighbour representatives attended two of the three mediation sessions and were invited by SAT to ensure the views of nearby residents were taken into account throughout the mediation process. As part of those mediation sessions, the neighbours were provided with copies of the Applicants materials provided to Council and were given the opportunity to make comment on those at the mediation. Copies of the neighbour representative's written submissions preparing during the mediation process are included in Attachment 9.

After the third mediation session, the SAT made the following orders:

'On the application heard before Member Marie Connor on 17 June 2015, it is ordered that:

1. *The applicant is to provide additional information as discussed at the mediation to the respondent by 26 June 2015.*
2. *Pursuant to s 31 of the State Administrative Tribunal Act 2004 (WA) the respondent is invited to reconsider its decision at its meeting of 13 August 2015.*
3. *The matter is listed to directions hearing on 21 August 2015 at 10:30am.*
4. *The respondent is to provide a copy of the Tribunal's orders to the two representatives of the residents' group by 22 June 2015.'*

Therefore, based on the above SAT orders, Council is requested to reconsider its previous decision of refusal, based on a revised proposal.

Submission

The application proposes to develop the site for use as a petrol filling station and associated convenience store, specifically:

1. Conversion of existing fast food outlet (KFC) building of approximately 200m² in area into a convenience store.
2. A forecourt consisting of eight refuelling bays and canopy.
3. Three underground fuel tanks storing an approximate volume of 55,000 litres per tank.
4. Tanker fill points located on the eastern side of tanker filling area (line marked), set back 5.78m from the western boundary.
5. Vent stacks within a landscaped area fronting Carrington Street.
6. Primary customer vehicle access/egress to the site from the existing crossover to Clontarf Road with secondary access/egress from Carrington Street via the existing easement through the adjacent land to the north of the site (Lot 41 Carrington Street).
7. Fuel tankers and service delivery vehicles entering via Carrington Street and exiting via Clontarf Road.
8. Removal of some of the rear additions to the building to facilitate access.
9. Associated signage.

As a consequence of the mediation process, further information and/or a number of changes have been effected by the Applicant to respond to the concerns expressed by the Council and neighbouring residents during the mediation process. The key changes and further information include:

1. Operating hours restricted to between 6:00am to 10:00pm seven days per week. It should be noted that the hours of operation contained in the original proposal (that Council refused) were 24 hours;
2. New 2.4m high colorbond fence for a 20m length along the western boundary within the site. It should be noted that this proposed fence did not form part of the original proposal;
3. Additional information regarding treatment of lighting and application of Australian Standards;
4. In addition to use of Stage 1 Vapour Recovery, agreement to use Stage 2 Vapour Recovery for all fuel dispensers to limit emissions during the refuelling process and mitigate odour concerns;
5. Confirmation that LPG gas will not be supplied on site to mitigate odour concerns;
6. Further information regarding traffic and truck movements which have subsequently been assessed by Council's independent traffic consultant;
7. Updated acoustic assessment for noise associated with vehicle movement, truck refuelling and mechanical plant and associated updated recommendations that have been assessed by Council's independent acoustic consultant;
8. Puma Energy's Health Safety and Environment Management procedures in relation to spill cleaning, emergency evacuation,

- environmental and waste management guidelines and dangerous goods management; and
9. Updated site plans in response to the change in the Applicant's proposal as a consequence of the mediation process.

Consideration of these matters and issues raised during the mediation process, including by local residents, are discussed in this report.

Consultation

As part of the mediation process, local residents were invited to attend the initial site visit and then subsequently two representatives were invited to attend the two following mediation sessions. The further information provided by the Applicant to Council was also provided to the resident representatives for their consideration and comment and the following mediation sessions. Copies of the resident's submissions are included as part of Attachment 9. The resident's concerns can be categorised as follows:

- General amenity concerns
- Lack of compatibility with adjoining residential use
- Noise concerns
- Vibration concerns
- Traffic & Access
- Odour
- Lighting pollution
- Spills and safety
- Business hours and lack of need
- Refuelling tanker route and filling point
- Health concerns

The above concerns, along with those expressed by Council itself, have been considered in this report.

Report

This section shall address in detail, the matters and issues discussed throughout the mediation process which were seen by Council as most important.

Hours of Operation

The original application refused by Council proposed 24 hour operations including retail fuel sale and the convenience store with fuel and goods deliveries restricted to between 7:00am and 7:00pm. The 24 hour operation of the petrol station presented a concern to nearby residents and Council and was discussed at length during mediation. It was considered that the 24 hour nature of the operations would detract from the amenity of neighbours. Noise, lights, vehicles entering and

exiting the site and other activities associated with the petrol filling station were cited as sources of impact – even if they could be demonstrated to comply with the relevant statutory controls. In response to these concerns, the applicant then proposed reduced hours of 5:00am to 12:00am arguing that these hours would serve the early morning commuters and evening business consistent with other commercial operators in the area including Red Rooster, Chinese Restaurant, Tavern and some other businesses. Council and residents remained concerned about potential impacts of the trading hours which were still generally beyond operating hours of the surrounding local centre.

In order to address Council's concerns, the applicant amended their proposal with operating hours restricted to between 6:00am to 10:00pm, seven days per week. The reduced operating hours are considered to be more in line with other commercial businesses in the immediate vicinity which tend to close around 10:00pm and will mean that the impact of the proposal on neighbours living close to the site will be reduced considerably. It also responds to the question of need associated with the original proposal to operate a 24 hour petrol filling station which wasn't justified by the Applicant and not supported by Council in the original determination.

Noise

As part of the consultation process associated with the original proposal nearby residents raised concern about potential noise and queried some of the information contained in the applicant's original noise assessment reporting. In response, Council engaged its own expert noise consultant to undertake a peer review of the applicant's report. The initial peer review dated 6 May 2015 (Appendix 5) identified several areas where further technical information and/or assessment were required including:

- Locations of receivers provided in the noise model;
- Additional information regarding noise emissions from the tyre air fill point;
- Confirmation regarding noise emissions from the mechanical services;
- Location of air compressor; and
- Noise emissions from tanker engine start and air brake air release.

The above further information was provided and Council's noise consultant has considered it to be satisfactory to address the initial concerns. It should be noted that in order to comply with the Noise Regulations in relation to air brake release from the tanker engines, the applicant has revised the proposal to include the construction of a new 2.4m high colorbond fence along the western boundary of the property

adjacent to the existing house for a length of 20m. Whilst this may be suitable in order to achieve compliance with the Noise Regulations, should Council consider approving the proposal, it is suggested that a condition be imposed requiring the fence be masonry (not colorbond) and be extended for the length of the boundary of the property. A masonry wall is considered to be a more visually appropriate and robust interface between a residential and commercial property of this nature.

In addition, should Council approve the proposal, a condition should be imposed requiring the preparation and implementation of a Noise Management Plan to ensure that measures and recommendations outlined in the Acoustic report are implemented.

Traffic & Access

Impacts of the proposal on the surrounding traffic network and also vehicle access in and around the site were a major cause for concern shared by nearby residents and Council. Similar to the noise report, the validity of the applicant's traffic report was queried and some of the assumptions on which the report was based were challenged. Council therefore engaged an independent Traffic Engineer to undertake a peer review of the traffic report with the purpose of either confirming or alleviating its traffic and access concerns. The completed peer review is in Appendix 6.

Based on the peer review which raised a need for additional information, the applicant undertook further analysis and assessment including further traffic counts, analysis regarding tanker movements and comparison with traffic generated from the previous fast food use.

In summary, the Traffic Engineer engaged by Council to undertake the peer review concurs with the assumptions and information provided by the applicant and agrees that traffic generated by the proposal can be satisfactorily accommodated by the existing road network. Notwithstanding this, confirmation of safe manoeuvring of eastbound outbound vehicles from the Carrington Street crossover from Lot 41 to navigate into the northbound-eastbound right-turn pocket at the signalised Carrington Street/Winterfold Road intersection is still required as a design audit of the site and should be imposed as a condition of approval should Council approve the proposal.

Should Council approve the proposal, a condition should be imposed requiring the preparation and implementation of a Traffic Management Plan to ensure that all recommendations and measures outlined in the Traffic Impact Assessment Report are implemented.

Site Access and Manoeuvrability

With regards to fuel tanker and service delivery vehicle access and egress to the site, the previous officer report (OCM 12 February 2015) stated that the proposal for fuel tankers to enter the site via the easement across Lot 41 from Carrington Street and exit via Clontarf Road was not supported. This was due to the impact on pedestrian amenity that would be caused by the widened crossover in this location. However as part of the peer review, both Traffic Engineers disagreed with this recommendation and believe that the proposed fuel tanker access via Carrington remains the safest and best option. There is still concern by nearby residents that fuel tankers accessing the site from Carrington Street via the adjoining site is problematic. Concerns include:

- Traffic congestion caused for Carrington Street traffic northbound due to the turning movement required for a 19m tanker.
- Concerns that two-way vehicle movement will not be able to occur within the right-of-way when the tanker is traversing the site.
- Concerns that the site is simply too small to accommodate the manoeuvrability of the tanker.

However, the applicant's Traffic Engineer and the peer review suggest that the site access, layout and manoeuvrability of vehicles in and around the site are satisfactory. A Swept Path drawing has been undertaken which demonstrates that there is sufficient vehicle manoeuvrability in and around the site to accommodate a 19m tanker.

Odour

As discussed in the previous report, control of odour in relation to the fuel delivery and sale is legislated by the Department of Mines and Petroleum (DMP) and the applicant will require a Dangerous Goods Licence to operate. The vapour recovery system used by the applicant was discussed during mediation and the applicant provided information about Stage 1 (during tanker refuelling) and Stage 2 (during vehicle refuelling) recovery systems being used. The Stage 1 vapour recovery system returns displaced vapour back to the delivery tanker through a vapour tight connection line. The Stage 2 vapour recovery system will be installed in the fuel dispensers and will connect with the underground fuel storage tanks to limit emissions during the regular vehicle refuelling process. The vapour recovery systems being installed as part of this proposal are expected to remove gaseous odour from the site and shall not pose an unreasonable amenity impact for nearby residents.

It should also be noted that the proposal does not include sale or storage of LPG which can be a source of odour emissions on petrol station filling sites. In addition, should Council approve the proposal, it is suggested that conditions be imposed requiring the applicant to prepare further management plans for the treatment of spills and control of drainage and pollution management to further assist in the mitigation of odour impacts and also a condition restricting the sale, storage or supply of LPG from the site.

Safety

Nearby residents and Council expressed concerns about public safety in relation to the proximity of the proposal to residential dwellings. Recent examples of other petrol stations in the metropolitan area experiencing explosions were raised. Concerns were also raised regarding potential spillage of fuel in the site.

As discussed in the Odour section above, the applicants will be required to obtain a Dangerous Goods and Handling Licence to store and sell petrol in accordance with Dangerous Goods legislation. The legislation specifies separation distances to adjoining properties and setbacks, site accessibility for fuel delivery tankers, spill containment, emergency procedures, maintenance, operator training and equipment to be installed. Whilst a Dangerous Goods and Handling Licence has not yet been obtained from the DMP, the applicant will be required to obtain this prior to operation of the proposal as a separate application process.

In addition to the DMP requirements, the applicant has a number of company procedures and guidelines in relation to safety including:

- Safe Work Procedure – Cleaning Spills (over 5 litres).
- Safe Work Procedure – Cleaning Spills (under 5 litres).
- Emergency Evacuation Procedure.
- Environmental and Waste Management Guidelines.
- HS&E Dangerous Goods Management Guidelines.

It is considered that through compliance with the DMP legislative process and the applicant's own internal processes and procedures, that the safety of those visiting and working at the site and importantly, nearby residents is being adequately considered and managed. In addition, to ensure that the planning approval appropriately contemplates risk and safety management measures including communication and evacuation protocols, a condition could be imposed for the preparation and implementation of a Safety and Risk Management Plan to the City's satisfaction on advice from DMP.

Lighting

The potential impacts of lighting spill associated with the proposal caused some concern from adjacent and nearby residents and Council. In response, the applicant has provided a Light Spill Elevation and Lighting Design plan (Attachment 8) prepared by their lighting consultant. The Light Spill Elevation clearly indicates that all external lighting emitted will be contained on site and will not extend into adjacent properties. The lighting has been designed to adhere to the *Australian Standard AS4282 – Control of obtrusive effects of outdoor lighting*. Should Council approve the proposal, a condition can be imposed requiring compliance with this which is a standard condition imposed on most commercial developments along with a lighting management plan to address the operational aspects of minimising the use and treatment of lights after hours on adjacent residential properties.

Health Impacts

Nearby residents raised concerns regarding their health, in relation to potential exposure to benzene. However, health impacts are not noted as an environmental factor that is relevant for consideration of petrol filling stations adjacent to residential developments or sensitive premises and no evidence has been presented to Council to suggest that the proposal is likely to impact the health of nearby residents.

Conclusion

The proposed petrol filling station is a use than can be considered for approval within a Local Centre. The proposal, if approved is likely to service both local residents and passing traffic and is reasonable well located to do so. However, due to the site's close proximity to existing residential dwellings, a number of issues and concerns were raised by residents and Council resulting in Council's initial decision to refuse the proposal. As part of the SAT mediation process, Council engaged independent noise and traffic consultants and the applicant provided further information in relation to a number of matters and concerns.

The recommendation of officers is that subject to the modifications undertaken by the Applicant and the introduction of additional conditions as part of the approval process to address the issues raised, the proposal is suitable for approval subject to conditions. It is important to note that the Applicant will also require separate licensing to operate the petrol filling station through the Department of Minerals and Petroleum as well as operate within other Environmental Protection Act regulations. Notwithstanding the above, the recommendation conditions has sought to ensure that the proposal will operate in accordance with appropriate management controls addressing the specific issues outlined in this report.

Strategic Plan/Policy Implications

Growing City

- Development that is soundly balanced between new and existing areas.
- Investment in industrial and commercial areas, provide employment, careers and increase economic capacity in the City.

Leading & Listening

- Effective and constructive dialogue with all City stakeholders.
- A responsive, accountable and sustainable organisation.
- A culture of risk management and compliance with relevant legislation, policy and guidelines.

Budget/Financial Implications

Further budget implications should the matter proceed to a full hearing in the State Administrative Tribunal.

Legal Implications

The requirement to defend Council's decision should the matter proceed to a full hearing in the State Administrative Tribunal.

Community Consultation

The original proposal (refused by Council at the 12 February 2015 OCM) was advertised to nearby residents for comment and five submissions were received and discussed in the previous report.

As discussed in this report, two people representing the adjacent and nearby residents were involved in the mediation process through SAT in the capacity of providing their views on all information presented. The resident representatives were provided with additional information supplied by the applicant throughout the mediation process.

Attachment(s)

1. Aerial View Plan
2. Site Plan, Floor Plan & Elevation Plan
3. Certificate of Title/Right of Carriageway
4. Applicant's Acoustic Report and Additional Information
5. City of Cockburn's Peer Review of Acoustic Report
6. Applicant's Traffic Report and Additional Information

7. City of Cockburn's Peer Review of Traffic Report
8. Lighting Plan and Light Spill Elevation Plan
9. Neighbour Representative Concerns (17/6/15)

Advice to Proponent(s)/Submissioners

The Proponent(s) and resident representatives have been advised that this matter is to be considered at the 13 August 2015 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

15. FINANCE AND CORPORATE SERVICES DIVISION ISSUES

15.1 (OCM 13/8/2015) - LIST OF CREDITORS PAID - JUNE 2015 (076/001) (N MAURICIO) (ATTACH)

RECOMMENDATION

That Council adopt the List of Creditors Paid for June 2015, as attached to the Agenda.

COUNCIL DECISION

Background

It is a requirement of the Local Government (Financial Management) Regulations 1996, that a List of Creditors be compiled each month and provided to Council.

Submission

N/A

Report

The list of accounts for June 2015 is attached to the Agenda for consideration. The list contains details of payments made by the City in relation to goods and services received by the City.

Strategic Plan/Policy Implications

Leading & Listening

- Effective and constructive dialogue with all City stakeholders.
- A responsive, accountable and sustainable organisation.

Budget/Financial Implications

N/A

Legal Implications

N/A

Community Consultation

N/A

Attachment(s)

List of Creditors Paid – June 2015.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

15.2 (OCM 13/8/2015) - STATEMENT OF FINANCIAL ACTIVITY AND ASSOCIATED REPORTS - JUNE 2015 (071/001) (N MAURICIO) (ATTACH)

RECOMMENDATION

That Council

- (1) adopt the Statement of Financial Activity and associated Reports for June 2015, as attached to the Agenda;
- (2) continue to apply a materiality threshold of \$200,000 variance from the appropriate base amount for the 2015/16 financial year in accordance with Financial Management Regulation 34(5); and
- (3) amend the 2014/15 Municipal Budget by adjusting the following projects and activities:

CW 449-6200	CCW-RPAEC- Construction Cost	LESS	\$13,900,000
CW 4449-5114	CCW-RPAEC – State Govt. Capital Grant Funding	ADD	\$1,500,000
CW 449-0008	CW-RPAEC- Loan Funding	LESS	\$20,000,000
CW 4449-4863	CCW-RPAEC – TF from CCW Development Fund Reserve	ADD	\$4,600,000
CW 1598-6200	Lot 9003 Beeliar Drive – Development Costs	ADD	\$600,000
CW 1598-4153	Lot 9003 Beeliar Drive – RF from Land Development & Investment Reserve	ADD	\$600,000
GL 116-6229	Executive Group – Consultancy Expenses	ADD	\$35,000
OP 9710-6200	LG Reform Provision Expenses	LESS	\$35,000
CW 4514-6200	Western Suburbs Skate Parks – Contract Expenses	ADD	\$11,888
CW 4514-4653	Western Suburbs – Skate Parks – TF from Community Infrastructure DCP13 Reserve	ADD	\$11,888

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COUNCIL DECISION

Background

Regulations 1996 prescribes that a local government is to prepare each month a Statement of Financial Activity.

Regulation 34(2) requires the Statement of Financial Activity to be accompanied by documents containing:–

- (a) details of the composition of the closing net current assets (less restricted and committed assets);
- (b) explanation for each material variance identified between YTD budgets and actuals; and

- (c) any other supporting information considered relevant by the local government.

Regulation 34(4)(a) prescribes that the Statement of Financial Activity and accompanying documents be presented to Council within 2 months after the end of the month to which the statement relates.

The regulations require the information reported in the statement to be shown either by nature and type, statutory program or business unit. The City chooses to report the information according to its organisational business structure, as well as by nature and type.

Local Government (Financial Management) Regulations - Regulation 34 (5) states:

- (5) *Each financial year, a local government is to adopt a percentage or value, calculated in accordance with the AAS, to be used in statements of financial activity for reporting material variances.*

This Regulation requires Council to annually set a materiality threshold for the purpose of disclosing budget variance details within monthly reporting. Council adopted a materiality threshold of \$200,000 for the 2014/15 financial year at its August meeting.

The accounting standard provides a guide of 5% to 10% as the base for establishing a materiality threshold. However, if applied, this could lead to the reporting of variances ranging from those under \$100 to those over \$1 million (depending on size of the relative budget). Therefore, it is recommended that Council continue with the flat value of \$200,000 for 2015/16 as this has worked well during the past financial year.

Whilst this level of variance reporting helps inform the formal mid-year budget review and informal monthly budget reviews, detailed analysis of all budget variances is carried out and put to Council for amendment where necessary.

Submission

N/A

Report

Due to ongoing end of financial year (EOFY) processing, the June Financial Statement being presented to Council is in draft form and subject to final audit (as occurs in June of each financial year).

The final budget position for 2014/15 will be reported to the October 2015 Council meeting, along with the associated list of carried forward projects and a finalised June statement of financial activity.

Opening Funds

The opening funds actuals of \$13.17M represents the audited closing municipal position for 2013/14 and the revised budget was updated to this figure in the mid-year budget review.

The opening funds cover the \$3M uncommitted end of financial year closing Municipal Fund forecast in the adopted budget, \$8.9M of municipal funding attached to carried forward works and projects and a residual balance of \$1.3M in uncommitted funds that was applied to the CCW Development Fund Reserve in accordance with Council's budget policy.

Closing Funds

The budgeted closing funds fluctuate throughout the year, due to the ongoing impact of Council decisions and budget recognition of additional revenue and costs. Details on the composition of the budgeted closing funds are outlined in Note 3 to the financial summaries attached to this report.

Whilst the current closing budget position is showing a surplus of \$16.3M, this includes the municipal funded portion for carried forward projects (currently estimated at \$11.2M) and surplus grant funding (estimated at \$1.5M) yet to be quarantined by transfer to the Restricted Grants and Contributions reserve account.

Further EOFY processing will impact this closing fund's position, but an uncommitted end of financial year closing Municipal Fund of approximately \$3.0M is expected due to favourable variances across the City's operating activities (detailed further within this report). In contrast, the revised budget is showing end of year closing funds of \$40k. The EOFY uncommitted funds will be transferred into one or more of Council's reserve accounts in accordance with the City's budget management policy. This will be reported to Council in October 2015 along with the final June monthly financial activity statement and carried forwards listing.

Operating Revenue

Consolidated operating revenue of \$127.0M was ahead of the budget target by \$2.9M. The significant variances in this result were:

- Rates revenue \$0.9M ahead of budget due to high part year rating from continued strong property development and building levels across the City.
- Operating grants and subsidies were over YTD budget by \$2.3M, mainly due to the Federal Government's very late decision to advance half of next year's FAGS grant of \$1.9M. This will be held within the restricted grants & contributions reserve at EOFY in order to transfer funding into the 2015/16 budget year.
- Operating grants and subsidies also included child care subsidies that were \$0.4M more than budgeted.
- Interest on monies invested came in \$0.2M below the full year budget of \$5.9M, as falling yields impacted returns over the year.
- Parking infringements revenue was \$0.3M more than the full year budget as a result of dedicating more resources to compliance monitoring.
- Lease income from commercial property is \$0.2M ahead of the cash flow budget, but remains on track to the full year budget.
- Commercial landfill fees at \$8.5M ended up \$1.0M behind the adjusted full year budget.

Further details of budget variances are disclosed in the Agenda attachment.

Operating Expenditure

Total operating expenditure of \$115.7M came in \$1.4M lower than full year budget. Cash operating expenditure (excludes asset depreciation) of \$93.7M was under the full year budget by \$1.6M.

The following significant items were identified:

- Material and Contract expenses were overall, \$0.2M under the full year budget. However, within this result were the following significant variances:
 - Waste collection services shows a \$1.0M underspend, although EOFY accruals for \$380k are still to be processed.
 - Waste Disposal contract expenses were \$0.3M underspent, consistent with the lower tonnage through the gate.
 - Contract spending under the Information Services business unit was \$0.3M under the full year budget comprising of software, IT leasing, archiving and scanning expenses.
 - The City's contribution to variable outgoings and landlord expenses at the Cockburn Community & Health facility added a budget variance of \$0.2M. However, this is offset by additional revenue collected from lessees.
 - Roads Maintenance contract spending came in \$0.3M over budget.

- Payments to in-home care and family day care givers were \$0.6M greater than budgeted. However this is cost neutral as these are reimbursed by the federal government.
- Parks maintenance contract costs were \$0.5M over the full year budget of \$4.1M due to extra costs for bore/pump maintenance, contract watering and landscaping services.
- Facilities maintenance contract costs were \$0.7M more than the full year budget of \$2.1M.
- Utility expenses were \$0.4M under the full year budget with Power expenses comprising \$0.3M of this variance. However, EOFY accruals for street lighting and other electricity accounts will erode this current saving.
- Insurance costs were \$0.3M below budget due to savings in premiums for property and workers compensation insurance.
- Other Expenses were nearly \$0.6M under their full year budget due to the landfill levy liability falling \$0.5M below budget. This is a by-product of the reduced tonnage at the Henderson Waste Recovery Park.
- The internal recharging of operating costs to the capital works program was \$0.3M lower than full year budget. This position is attributable to the budget underspend within the infrastructure assets capital works program.

It is worth highlighting that Employee Costs came in \$0.15M under the full year budget of \$45.0M (a 0.3% variance), which is a good budget result. Depreciation also came in very close to budget at \$25.0M (versus a budget of \$25.1M).

A more detailed explanation of the variances within each business unit is included in the attached financial report.

The following table shows the operating expenditure budget performance at the consolidated nature and type level. The internal recharging credits reflect the amount of internal costs capitalised against the City's assets:

Nature or Type Classification	Actual Expenses \$M	FY Revised Budget \$M	Variance to Budget \$M
.	43.81	43.86	0.05
Employee Costs - Indirect	1.22	1.33	0.10
Materials and Contracts	35.39	35.65	0.25
Utilities	4.22	4.62	0.41
Interest Expenses	0.12	0.12	0.00
Insurances	1.95	2.22	0.26

Nature or Type Classification	Actual Expenses \$M	FY Revised Budget \$M	Variance to Budget \$M
Other Expenses	6.98	7.54	0.56
Depreciation (non-cash)	24.96	25.10	0.14
Internal Recharging-CAPEX	(2.91)	(3.25)	(0.34)
Total	115.75	117.19	1.44

Capital Expenditure

The City's total capital spend at year's end was \$38.1M, representing an under spend of \$35.3M against the full year budget of \$73.4M.

The following table shows the budget variance analysis by asset class:

Asset Class	Actual Expenses \$M	FY Revised Budget \$M	Variance to Budget \$M
Roads Infrastructure	9.79	17.12	7.33
Drainage	0.71	1.43	0.72
Footpaths	0.90	1.03	0.13
Parks Hard Infrastructure	4.13	8.45	4.33
Parks Soft Infrastructure	0.86	0.92	0.06
Landfill Infrastructure	0.39	0.85	0.46
Freehold Land	1.41	2.98	1.57
Buildings	15.85	33.97	18.12
Furniture & Equipment	0.00	0.00	0.00
Computers	0.64	1.14	0.50
Plant & Machinery	3.43	5.52	2.09
Total	38.10	73.41	35.31

The CCW project is responsible for \$14.9M of the net \$18.1M underspend variance under Buildings. Other significant delayed building projects include Atwell Clubrooms (\$0.7M), Naval Base toilet block (\$0.5M), Coleville Crescent carpark extension (\$0.5M), Coogee toilet sewerage upgrade (\$0.5M) and Coogee Holiday Park electricity supply upgrade (\$0.4M).

Parks infrastructure projects were collectively \$4.3M underspent against the full year budget of \$8.4M. This included the delayed Bibra Lake Adventure Playground (\$2.9M remaining from \$3.3M), underspending on the Coogee Beach Master Plan (\$0.46M remaining from \$0.54M) and the Manning Park lookout stairs (\$0.27M remaining from \$0.3M).

The roads construction program was \$7.3M under the full year budget mainly due to Beeliar Drive (Spearwood – Stock) under by \$3.2M,

Berrigan Drive (Kwinana Fwy to Jandakot Rd) under by \$1.5M and North Lake Road (Hammond to Kentucky) under by \$1.1M.

The City's drainage capital works program ended \$0.7M behind the full year budget of \$1.4M with several significant projects yet to commence and to be carried forward into 2015/16.

Subdivision costs for the City's land development projects were collectively \$1.5M behind the full year budget of \$3.0M. These included Lot 9003 Beeliar Drive (under by \$0.6M) and Lots 702 Bellier Place/65 Erpingham Road (under by \$0.5M).

Spending on major plant items was \$2.1M below the full year budget, of \$5.5M, with \$1.2M of this amount on order and awaiting delivery in the new FY.

Software related capital projects are collectively \$0.5M under the full year budget, with many having started and requiring to be carried forward.

Further details on these variances are disclosed in the attached CW Variance analysis report.

Capital Funding

Capital funding sources are highly correlated to capital spending, the sale of assets and the rate of development within the City (developer contributions received).

Significant variances for the month included:

- Transfers from financial reserves were \$2.9M below full year budget due to the capital budget under spends.
- Developer contributions received under the Community Infrastructure plan were \$1.3M over the full year budget, even though the budget was significantly increased through the mid-year review. This reflects ongoing strong levels of land development activity across the City.
- Developer contribution plans revenue for roads infrastructure was \$0.3M ahead of the full year budget setting.
- Development partner contributions for the CCW RPAEC project were \$0.4M behind the full year budget forecast.

- The state government paid another \$1.5M of their contribution towards the CCW PPAEC project ahead of budget, due to an adjusted funding milestone.
- Road grant funding was collectively \$1.8M behind the full year budget target, consistent with the delay in completing roads capital projects.
- Sale of land revenue from various sub-divisions came in \$14.9M behind the full year budget target. This included:
 - \$9.6M for Lot 9003 Beeliar Drive (due to settle in October 2015),
 - \$1.4M for Lot 702 Bellier Place and Lot 65 Erpingham Road (due to settle November 2015),
 - \$1.3M for Lot 33 Davilak Avenue (delayed – October 2015),
 - \$1.0M for Lot 23 Russell Road (did not proceed to sell),
 - \$0.9M for Lot 40 Cervantes Loop (did not proceed to sell); and
 - \$0.7M for Lot 1, 4218 and 4219 Quarimor Road (did not proceed to sell).
- Proceeds from the trade-in and sale of plant were also collectively \$0.5M behind the full year budget.

Cash & Investments

The closing cash and financial investment holding at month's end totalled \$133.5M, down from \$141.5M the previous month. \$103.2M of this balance represented the amount held for the City's cash backed financial reserves. Another \$6.2M represented restricted funds held to cover deposit and bond liabilities. The remaining \$24.1M represented the cash and financial investment component of the City's working capital, available to fund current operations, capital projects, financial liabilities and other financial commitments (e.g. end of year reconciling transfers to financial reserves).

The City's investment portfolio made a weighted annualised return of 3.36% for the month, marginally down from 3.41% the previous month and 3.47% in April. Whilst this result compares favourably against the UBS Bank Bill Index and the various short term BBSW indices, it continues to trend downwards. This is due to lower rates being offered for new or renewed investments than those on currently held investments. The cash rate currently sits at 2.00% following the 0.25% cut by the Reserve Bank at its May board meeting. Industry expectations of a possible further cut later this calendar year will put pressure on the City achieving its interest earnings budget of \$5.4M for the 2015/16 financial year.

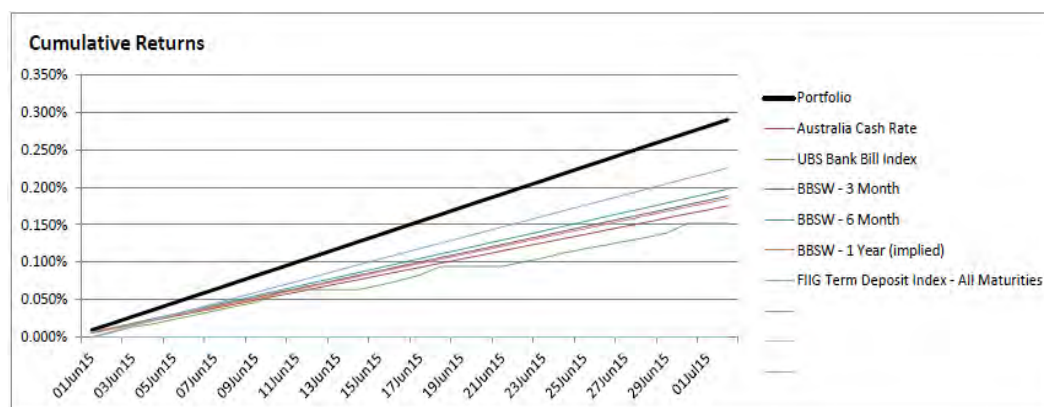


Figure 1: COC Portfolio Returns vs. Benchmarks

The majority of investments are held in term deposit (TD) products placed with highly rated APRA (Australian Prudential Regulation Authority) regulated Australian banks. These are invested for terms ranging from three to twelve months. All investments comply with the Council's Investment Policy and fall within the following risk rating categories:

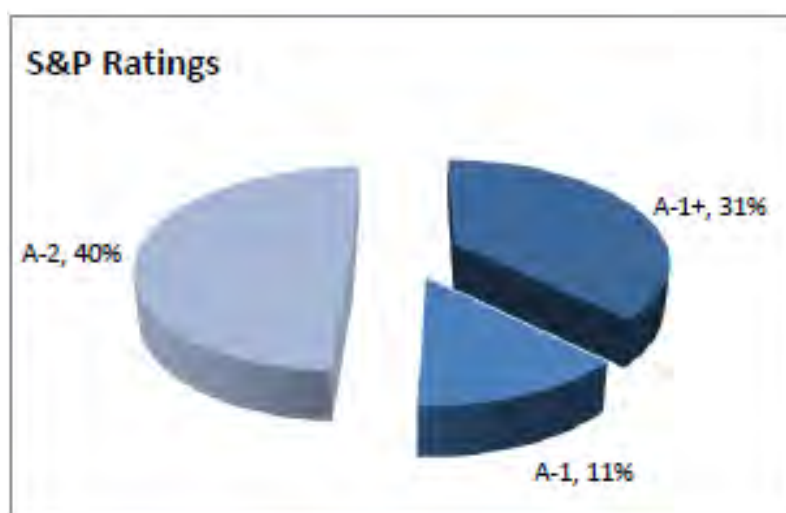


Figure 2: Council Investment Ratings Mix

The current investment strategy seeks to secure the best possible rate on offer over the longer duration terms allowed under legislation and policy (6 to 12 months for term deposits), subject to cash flow planning requirements. The City's investment portfolio currently has an average duration of 115 days (down from 132 days last month) as graphically depicted below:

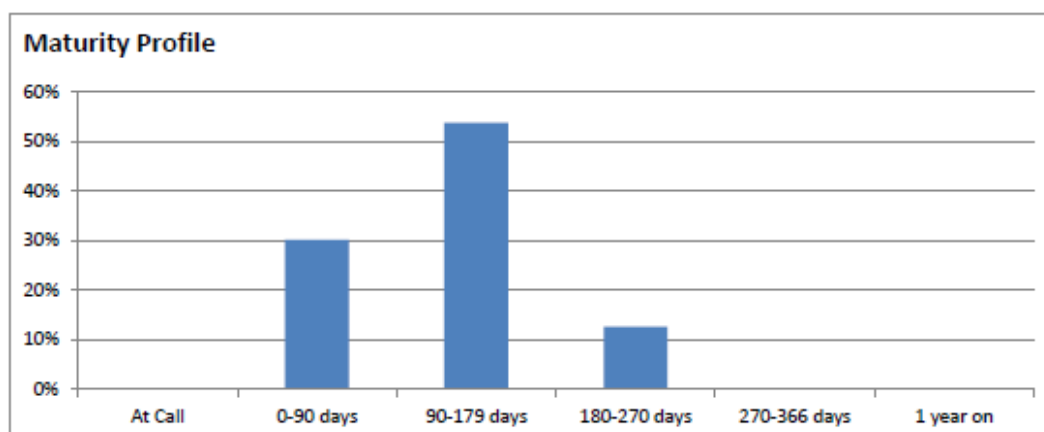


Figure 3: Council Investment Maturity Profile

Budget Revisions

Several budget amendments are recommended to deal with the following matters:

- The City has received a further \$1.5M of the state government's \$10M capital grant towards the RPAEC project at CCW (total received to date of \$3M).
- The budget currently shows loan funding of \$20M for the RPAEC project at CCW. As the City had no need to borrow these funds during 2014/15, in part due to establishing the CCW Development Fund financial reserve, this budget needs to be eliminated.
- A transfer from the CCW Development Fund Reserve for \$4.6M is needed to balance off the funding requirement for the \$8.7M spent on the RPAEC project in 2014/15.
- A reduction of \$13.9M in the RPAEC expenditure budget is needed to offset not borrowing (in 2014/15) \$20M funding from the loan netted off against the additional \$1.5M state government grant payment and the \$4.6M transfer from the CCW Development Fund Reserve.
- Include development costs of \$600,000 (funded from the Land Development & Investment Fund Reserve) for lot 9003 Beeliar Drive, previously flagged as up to \$1M in Minute No. 5426 at the December 2014 Council meeting.
- A transfer of \$35,000 from the LG Reform budget is necessary to fund additional design work required to support the application for grant funds from the Federal Government for the proposed Cockburn bowling club and other sporting facilities at Visko Park.

- Due to an increased funding requirement for the Western Suburbs skate park, the amount approved to be drawn from the Community Infrastructure Developer Contributions Plan (DCP13) was increased by \$11,888 to \$136,678. The income and expenditure budget for this project needs to be adjusted to reflect this increase.

Description of Graphs and Charts

There is a bar graph tracking Business Unit operating expenditure against budget. This provides a very quick view of how the different units are tracking and the comparative size of their budgets.

The Capital Expenditure graph tracks the YTD capital spends against the budget. It also includes an additional trend line for the total of YTD actual expenditure and committed orders. This gives a better indication of how the capital budget is being exhausted, rather than just purely actual cost alone.

A liquidity graph shows the level of Council's net current position (adjusted for restricted assets) and trends this against previous years. This gives a good indication of Council's capacity to meet its financial commitments over the course of the year. Council's overall cash and investments position is provided in a line graph with a comparison against the YTD budget and the previous year's position at the same time.

Pie charts included show the break-up of actual operating income and expenditure by nature and type and the make-up of Council's current assets and liabilities (comprising the net current position)

Strategic Plan/Policy Implications

Leading & Listening

- Effective and constructive dialogue with all City stakeholders.
- Manage our financial and infrastructure assets to provide a sustainable future.
- A culture of risk management and compliance with relevant legislation, policy and guidelines

Budget/Financial Implications

The budget amendments included in the recommendation are self-funding and do not impact the budget surplus position.

Legal Implications

N/A

Community Consultation

N/A

Attachment(s)

Statement of Financial Activity and associated reports – June 2015.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

16. ENGINEERING AND WORKS DIVISION ISSUES

16.1 (OCM 13/8/2015) - FORECAST TRAFFIC VOLUMES ON COCKBURN ROADS (163/009) (J MCDONALD) (ATTACH)

RECOMMENDATION

That Council receive the information within the report.

COUNCIL DECISION

Background

At the ordinary Council Meeting in July, Cr Steven Portelli raised the following matter for investigation:

“That a report be prepared and presented to the August Meeting of Council containing two maps noting the net differences in traffic flow in the Cockburn district between "with" and "without" Roe 8, on the data reported in the 2013 District Traffic Study forecasts for 2020 and 2031.

As exemplified in figure 9 of the 2006 DTS, officers to provide the new maps, their summation in the same format and subject matter as 5.3 summation in the 2006 DTS updated with the newer information as above.

Noting the information is on hand and merely needs transcribing the net differences on traffic flows on the districts roads shown."

Submission

NA

Report

The City's 2013 District Traffic Study (2013 DTS), that contains traffic forecasts for the distributor road network in the City of Cockburn for the years 2020 and 2031, was prepared for the City by consulting engineers Arup Pty Ltd.

As requested by Cr Portelli, forecast traffic volumes from the 2013 DTS have been collated into a table presented similar to Tables 4, 5 and 6 in the 2006 District Traffic Study (2006 DTS). In this instance, the data was collated into a single table with the roads listed in alphabetical order for simplicity. More columns have been included in the table, than were used in the 2006 DTS, to include the forecast traffic volumes both with and with-out Roe Highway Stage 8 (Roe 8) and the difference between those figures. That table is included as Attachment 1.

It is very important that any reader of this report and associated attachments understand that any traffic volume forecasts are indicative only and must not be considered as absolute figures. Forecast traffic volumes are developed from a base year when actual traffic volumes are available, and then calculated taking into account assumptions about population and traffic growth rates; future road capacity and/or development of the road network; and, changes to land use.

As a result, actual traffic volumes that are experienced in the years that traffic has been forecast for will be influenced, to varying degrees, by any of those assumptions not being realised. For example, if population growth and land development occur at a slower rate than assumed, then actual traffic volumes should theoretically be lower than forecast.

Also, because many factors can influence traffic volumes on individual roads any conclusions drawn from the traffic forecasts should be made with care. Adding capacity into the road network by building a new road such as Roe Highway should result in traffic reductions on existing roads that form parallel routes to the new road. But there may be other factors also contributing to that reduction, and the influence of that

additional capacity reduces as the distance away from the new road increases. At the same time, there will also be roads that experience increases in traffic volume because of the way that a new road affects traffic distribution.

For example, if Roe Highway is extended to Stock Road, the 2013 report predicts traffic reductions on roads such as Berrigan Drive, Bibra Drive, Farrington Road, and part of North Lake Road. But its influence would result in lesser benefits for major parallel roads such as Beeliar Drive, and lesser again for Russell Road.

In the 2013 DTS certain assumptions relating to road network upgrades were used in the traffic model, based on anticipated future road improvements and additions anticipated by the City and Main Roads Western Australia at the time that the DTS was commenced in 2012. Those assumptions are listed in Table 1 below.

Table 1: 2013 District Traffic Study - adopted timing of network upgrades for base network

Road Network upgrade	2020	2031
North Lake Road bridge over Kwinana Freeway	✓	✓
Rowley Road extended to Postans Road		✓
Stock Road upgraded to three lanes each direction between Winterfold Road and Phoenix Road		✓
Kwinana Freeway upgraded to three lanes each direction from the north of the model to Beeliar Drive/Armadale Road	✓	✓
Roe Highway upgraded to three lanes each direction between Kwinana Freeway and Karel Avenue	✓	✓
Beeliar Drive upgraded to two lanes each direction between Kwinana Freeway and Stock Road	✓	✓
North Lake Road upgraded to two lanes each direction between Beeliar Drive and Hammond Road	✓	✓
Hammond Road/Frankland Avenue upgraded to two lanes each direction between Beeliar Drive and Russell Road	✓	✓
Grade separation of Stock Road with Spearwood Avenue, Barrington Street, Beeliar Drive and Russell Road		✓
Stock Road/Rockingham Road upgraded to three lanes each direction between Phoenix Road and Wattleup Road		✓
Karel Avenue upgraded to two lanes each direction between Berrigan Drive and Roe Highway		✓
Spearwood Avenue upgraded to two lanes each way between Barrington Street to Beeliar Drive	✓	✓
Beeliar Drive upgraded to 3 lanes each way between Kwinana Freeway and Wentworth Parade	✓	✓
Wentworth Parade four way with Poletti Road	✓	✓
Armadale Road upgrade to two lanes each way between Tapper Road and Nicholson Road	✓	✓

Road Network upgrade	2020	2031
Berrigan Drive upgraded to two lanes each way between Kwinana Freeway and Jandakot Road	✓	✓
Jandakot Road upgraded to 2 lanes each direction		✓
New four way formed with Hammond Road and Beeliar Drive - Hammond Road North of Beeliar Drive to North Lake Road is two lanes each way		✓

The following conclusions are drawn from the traffic forecast information in the attachments:

1. Armadale Road, between the Kwinana Freeway and Tapper Road, will be required to be widened to 6-lanes by 2020.
2. Armadale Road, east of Tapper Road will be required to be widened to 4-lanes by 2020.
3. Beeliar Drive, between Rockingham Road and Spearwood Avenue, will need to be widened to 4-lanes by 2020 without Roe 8.
4. Berrigan Drive, east of the Kwinana Freeway will be required to be widened to 4-lanes by 2020.
5. Bibra Drive will be required to be widened to 4-lanes by 2031 without Roe 8.
6. Cockburn Road will be required to be widened to 4-lanes by 2020;
7. Farrington Road will be required to be widened to 4-lanes by 2020 and 6-lanes by 2031 without Roe 8.
8. Jandakot Road will be required to be widened to 4-lanes by 2031 with/with-out Roe 8.
9. Karel Avenue, between Farrington Road and Berrigan Drive, will be required to be widened to 4-lanes by 2020 with/with-out Roe 8.
10. North Lake Road, north of Berrigan Drive, will be required to be widened to 6-lanes by 2020 without Roe 8.
11. Russell Road, west of Hammond Road, will be required to be widened to 4-lanes by 2031 without Roe 8.
12. Spearwood Avenue, south of Barrington Street, will be required to be widened to 4-lanes by 2020 with/with-out Roe 8.
13. Stock Road will need to be widened to 6-lanes by 2031 with Roe 8.
14. Sudlow Road, south of Phoenix Road, will be required to be widened to 4-lanes by 2020 with Roe 8.

Based on the information contained in the 2013 DTS, the construction of Roe Highway Stage 8 would reduce east-west traffic volumes along various major roads and have most influence on:

1. Berrigan Drive (up to 46% reduction by 2020).
2. Bibra Drive (up to 67% reduction by 2020).
3. Discovery Drive (up to 69% reduction by 2020).

4. Farrington Road (up to 54% reduction by 2020).
5. Karel Avenue (up to 41% reduction by 2020).
6. North Lake Road (up to 59% reduction just north of Berrigan Drive by 2020).
7. North Lake Road (37% reduction by 2031 between Farrington Road and the Roe 8).
8. Stock Road (up to 59% increase by 2020 and 61% increase by 2031).
9. Sudlow Road (100% increase by 2020)

Strategic Plan/Policy Implications

Moving Around

- An integrated transport system which balances environmental impacts and community needs.
- Facilitate and promote healthy transport opportunities.
- A safe and efficient transport system.
- A defined freight transport network.

Budget/Financial Implications

The City's long term financial plan and regional and major roadworks 2013-2030 includes some of the projects. All major projects are dependent on external funding.

Legal Implications

N/A

Community Consultation

N/A

Attachment(s)

1. Table of 2020 and 2031 traffic forecasts from the 2013 District Traffic Study.
2. Maps of differences in traffic forecasts with Roe Highway Stage 8.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

16.2 (OCM 13/8/2015) - MATTERS TO BE NOTED FOR INVESTIGATION, WITHOUT DEBATE CHIVALRY WAY COMMUNITY CONSULTATION (L JAKOVCEVIC) (082/002) (ATTACH)

RECOMMENDATION

That Council receives the report.

COUNCIL DECISION

Background

At the Annual General Meeting of Electors on 3 February 2015, a motion was moved that Council stops the installation of the footpath along Chivalry Way Atwell. Council requested a report be prepared and presented to the 12 March 2015 Ordinary Council Meeting detailing the process of the Chivalry Way footpath project in 2014/15 work program.

At this meeting Council resolved to:

- (1) *support the installation of the proposed footpath in Chivalry Way Atwell; and*
- (2) *advise the residents of Chivalry Way of Council's decision.*

At the Ordinary Council Meeting 12 March 2015 Cr Steve Portelli requested the following matter to be noted for investigation without debate:

"That a report be prepared and presented to Council on Chivalry Way – Community consultation. A report on what specific contact and community engagement was undertaken? What steps can be made so this does not repeat itself?"

Submission

N/A

Report

The footpath in Chivalry Way project was listed on Council 2014/15 budget in response to a petition received from the community in August 2013 for a footpath in Chivalry Way, Atwell. An information letter was sent to all residents impacted by the proposal on Monday 12th January 2015 with information that the works for constructing the footpath will commence at the beginning of February 2015.

Following the Council resolution at the Ordinary Council Meeting of 12 March 2015 regarding the report mentioned above, a second letter drop was then issued to all residents on both sides of the street seeking any objection to the construction of this footpath. The results of the letter drop indicated a high level of support for the construction of the footpath and the project proceeded to completion.

To provide the residents and stakeholders with the opportunity to engage with Council on policies, plans, events or issues that impact their lives, the Council developed the City of Cockburn's Community Engagement Policy SC2 'Community Engagement' which was last reviewed on the 13 March 2014.

The purpose of this policy is:

- to adopt a Community Engagement Model based on three strategies information, consultation and active participation;
- to implement the Community Engagement Framework to ensure a consistent approach to community engagement across the City;
- to ensure the endorsed approach is widely practiced by City staff;
- to ensure that all legislative and statutory regulations are met and, where possible, exceeded, and
- to regularly survey the community on their needs and the performance of the City in meeting their needs

The Community Engagement Policy SC2 policy applies to all employees and to consultants engaged by the City of Cockburn, and all officers have been instructed to comply with that Policy whenever there is community engagement.

All future projects are to be carried out in accordance with this Policy.

Strategic Plan/Policy Implications

Infrastructure

- Community facilities that meet the diverse needs of the community now and into the future.

- Community infrastructure that is well planned, managed, safe, functional, sustainable and aesthetically pleasing.

Community & Lifestyle

- Community environments that are socially cohesive and embrace diversity.
- Communities that take pride and aspire to a greater sense of community.
- Safe communities and to improve the community's sense of safety.

Budget/Financial Implications

N/A

Legal Implications

N/A

Community Consultation

N/A

Attachment(s)

Policy SC2 'Community Engagement'

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

17. COMMUNITY SERVICES DIVISION ISSUES

17.1 (OCM 13/8/2015) - PROPOSED CHANGE TO THE GAZETTED FIRE DISTRICT (ESL AREA 3 - ESL AREA 1) (150/014; 027/007) (R AVARD) (ATTACH)

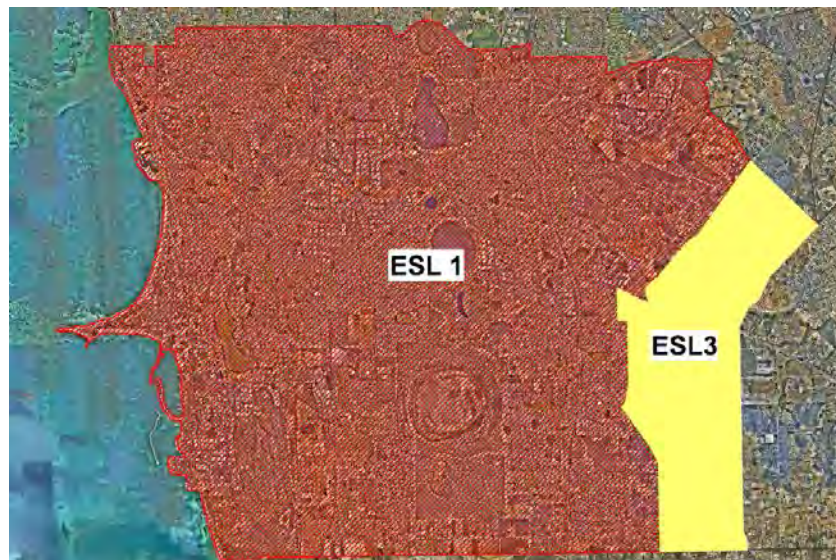
RECOMMENDATION

That Council advise the Department of Fire and Emergency Services that it does not support the proposed amendment to the gazetted Fire District (ESL1) to include the locality of Banjup.

COUNCIL DECISION

Background

The City of Cockburn received correspondence from the Department of Fire and Emergency Services (DFES) (attachment 1), advising on the re-examination of the current Gazetted Fire District. The review by DFES has determined the risk profile and services available within the area of Banjup are adequate to extend the current ESL 1 area across the ESL 3 (see map 1 below) area to form part of the gazetted fire district.



Map 1: Current Emergency Service Levy status within the City of Cockburn

In accordance with the Fire and Emergency Services Act 1998, the Minister can determine the type of category an area is subject to under sub-section 2 of the Act. Before the change in category is gazetted, the Minister is to consult with the affected Local Government. Under the Fire Brigades Act 1942 the minister has the ability to gazette areas of land to a fire district, for the purposes of servicing them from a career Fire and Rescue Service.

At the City of Cockburn Bushfire Advisory Reference Group (BFARG) meeting held 25 March 2015, strong opposition to the proposed change was conveyed by the Reference Group's members. The BFARG unanimously do not to support the current proposal.

Submission

N/A

Report

The Department of Fire and Emergency Services wrote to the City in March 2014 seeking comment on the proposed changes to include Banjup as part of the Gazetted Fire District and in turn alter the category of the Emergency Services Levy. Council at its April 2014 Ordinary Council Meeting resolved to seek detailed justification from DFES and recommended the commencement of a public consultation period for residents within the area affected by the change.

In April 2015, DFES responded to Council's request providing a succinct summary to justify the change in category. The DFES response highlighted a number of reasons for the boundary and category change. This includes a change in risk profile of Banjup and nearby resourcing available from DFES, to support the proposed gazetted fire district.

Members of the BFARG reviewed the justification provided by DFES, and do not support the substantiation to alter the category of Banjup from ESL 3 to ESL 1. BFARG members insisted that without a change in the current services offered to the Banjup area by the Fire and Rescue Service, an effective response is not possible.

The proposed change will ultimately lead to two changes that affect the City of Cockburn and residents in Banjup. The increase in levy charged to landowners through Council rates, and the change in response by the City's Bushfire Brigades.

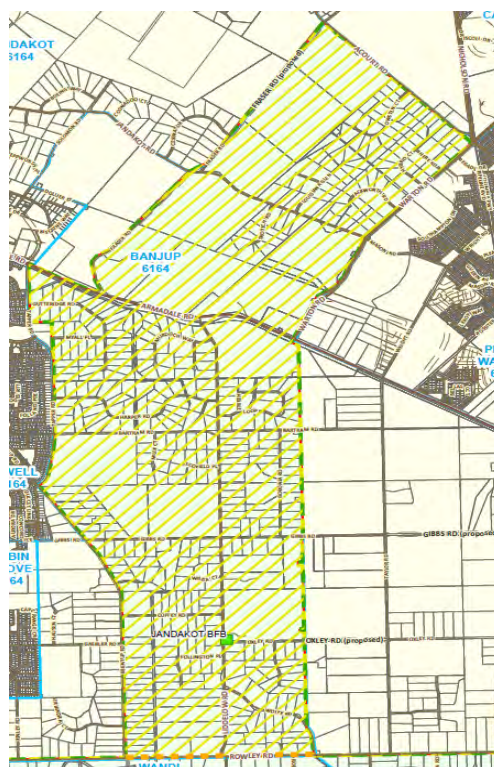
The ESL levy is determined by the Gross Rental Value of a property, and collected by Local Governments on behalf of DFES. Table 1 (below) provides indicative figures for the proposed increase in levy paid by landowners.

Table 1: Indicative charge in levy paid by resident

HECTARES	GRV	ESL3	ESL1
3.153ha	26000	\$156.00	\$319.80
2.00ha	19500	\$117.00	\$239.85
2.04ha	18300	\$109.80	\$225.09

During the 2014/15 financial year, the City of Cockburn collected a total \$12,388,000 in ESL rates for DFES and received \$229,000 in return to support the City's two bushfire brigades and Cockburn State Emergency Services unit.

Currently, when a fire is reported within Banjup/ESL 3 area (map 2), the first response and suppression of fires falls to the Jandakot Volunteer Bushfire Brigade. Should the area be changed to ESL 1, the first call out and response will most likely be serviced from the Success Fire and Rescue Station (FRS). Jandakot and South Coogee Bushfire Brigades will be utilised during larger fires or when their all-terrain and bulk water delivery vehicles are required.



Map 2: Current ESL 3 area

Arrangements can be put into place where the DFES Communications Centre can call out both a volunteer brigade and career units when required. Dual call outs will reduce the time it will take to activate the unique capabilities of the City's volunteer Bush Fire Brigades.

Table 2 (below) highlights the number of calls received by the DFES Communications Centre. The table shows the number of incidents requiring the City's Bushfire Brigades within the Banjup ESL 3 area.

Table 2: BFB Callouts in the ESL 3 Area

Year	Bushfire Incidents
2010/11	16
2011/12	11
2012/13	13
2013/14	21
2014/15	15
Total	76

**Statistics provided by DFES, Information is believed to be accurate at time of writing this report. DFES were not able to supply statistical data relating to the Career Fire and Rescue callouts for the ESL 3 area.*

To date DFES has confirmed the need to maintain and support South Coogee and Jandakot Volunteer Bush Fire Brigades in their current locations, irrespective of the proposed changes to the ESL and gazetted fire district.

The City has received correspondence from the Local Emergency Management Committee and Jandakot Volunteer Bush Fire Brigade, outlining specific reasons for why the proposal should not be supported.

DFES Assistant Commissioner Darren Klemm provided a presentation to the Banjup Residents Group at its 21 June 2015 meeting. The Banjup Residents Group resolved to unanimously not support the proposed changes to the ESL category to Banjup.

Strategic Plan/Policy Implications

Community & Lifestyle

- Safe communities and to improve the community's sense of safety.

Leading & Listening

- A culture of risk management and compliance with relevant legislation, policy and guidelines

Environment & Sustainability

- To protect, manage and enhance our natural environment, open spaces and coastal landscapes.

Budget/Financial Implications

There will be a limited impact on the Council budget as any additional Emergency Services Levy funds raised as a result of the area being transferred from ESL 3 to ESL1 are forwarded directly to the State Government.

Legal Implications

Fire Brigades Act 1942
Fire and Emergency Services Act 1998
Bush Fires Act 1954

Community Consultation

The Banjup Residents Association have advised that they do not support the inclusion of the suburb of Banjup in the ESL 1 area.

Attachment(s)

1. April 2015 correspondence from DFES
2. Correspondence from City of Cockburn Local Emergency Management Committee
3. Correspondence from Jandakot Volunteer Bushfire Brigade
4. Current Emergency Services Levy category 1 & 3 Map
5. Current Emergency Services Levy category 3 Map
6. Proposed Emergency Services Levy area for City of Cockburn

Advice to Proponent(s)/Submissioners

The Department of Fire and Emergency Services have been advised that this matter is to be considered at the 13 August 2015 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

17.2 (OCM 13/8/2015) - PROPOSED AMENDMENT TO BANJUP SUBURB BOUNDARY AND NEW LOCALITY NAME (159/008) (D GREEN) (ATTACH)

RECOMMENDATION

That Council

- (1) advises the Geographic Names Committee (GNC) that it supports:
 1. The creation of a new locality to be named "Quendalup" for the area of Banjup bounded by Armadale Road, Warton Road, Jandakot Road, Solomon Road, Dollier Road and the current Jandakot (north / south) location boundary between Dollier Road (to the north) and Armadale Road (to the south), as shown in the Attachment to the Agenda.
 2. The inclusion of the area of Banjup bounded by Jandakot Road, Warton Road, Acourt Road and Fraser Road into the existing locality of Jandakot.
- (2) informs those who lodged a submission of Council's decision.

COUNCIL DECISION

Background

At the Ordinary Meeting of Council held on 9 July, 2015 it was resolved, as follows:

MOVED Clr S Portelli SECONDED Clr Y Mubarakai that Council:

- (1) propose to amend the boundary of the locality of Banjup and create a new suburb bounded by Armadale Road to the south, Jandakot Road to the north, Warton Road to the east and Solomon Road to the west, excluding the landholdings within the Jandakot Industrial Area (as shown in the attachment to the Minutes), to be called 'Quendalup' or 'Kwentalup';*
- (2) proposed that the remaining area of the locality of Banjup to the north of Jandakot Road and west of Warton Road be included within the existing locality of Jandakot;*
- (3) advise those affected landowners within the current locality of Banjup, together with the developer of the Calleya estate (Stockland), by letter of the proposal to create a new locality, to be called 'Quendalup' or 'Kwentalup' and of the proposed boundary adjustment to the locality of Jandakot to include part of Banjup;*
- (4) invite affected landowners to make a written submission on the proposal, the recommended names and any alternate names which conform to Geographic Names Committee (GNC) Guidelines within a 21 day period;*
- (5) refer the submissions regarding the proposal, the recommended names, and any alternate names with Officers recommendations to Council for consideration; and*
- (6) advise GNC of Council's decision.*

Submission

N/A

Report

In accordance with sub – recommendation (4), letters were sent to all landowners subject to the potential locality name changes, together with a short survey form to indicate whether they supported or opposed the proposed change, in addition to submitting any alternatives which could be considered by Council as suburb names. This entailed two separate letters being forwarded to landholders in the areas affected by Council's decision.

One letter was sent to around 400 landowners in the area proposed to be renamed "Quendalup" or "Kwentalup" (known as Survey 1). This included 7 owner / occupier residential properties in the area branded "Calleya Estate" and 10 rural (2 ha) property owner / occupiers. The remaining landholders are either absent owners, property developers, Government Agencies or operating business premises. At the closing date for submissions, 131 responses have been received. Of these, 74 supported the proposal and 57 opposed. Of the 74 who responded in favour, 54 specified Quendalup and only 5 noted Kwentalup as the preference. Significantly, only 7 of the 17 owner / occupiers responded. Of these 2 supported the proposal and 5 opposed the proposal.

Of the opponents there were a number who offered an alternative for Council's consideration. These are listed separately on the Attachment, together with officer comment on their suitability. The most favoured alternatives were "Treeby" (5), "Oxley" (5) and a merger with adjoining Jandakot (5). The suggestion of merging this part of Banjup with Jandakot is not supported as it would create an abnormally large location which would contain a number of vastly differing land uses and would forego the obvious opportunity to rebrand a developing residential suburb.

Both "Treeby" and "Oxley" could be acceptable, as they are generally in compliance with GNC Guideline 7.1.1, given the relativity of these names to early settlers. Treeby is historically the more significant, as it refers to Joseph Treeby and his wife, Emma, who settled in the area and developed a vegetable garden in the 1890's. In addition, 3 of the Treeby sons served in World War 1, with one killed in action. Oxley refers to the surveyor of the area, James Oxley, who first recorded Lake Bangup, (later altered to Banjup Lake) in 1889.

Both of these names are commemorated in the area already, with Emma Treeby Reserve and Oxley Road, existing in the Banjup locality, to the south side of Armadale Road.

Given this information, it is considered that the name “Quendalup” is widely favoured by the majority of respondents and it is recommended for Council endorsement accordingly.

The other letter (Survey 2) was sent to 98 owners of land in the area of Banjup proposed to be ceded into Jandakot. The majority of these (71) are owner / occupiers with the remainder being either absent owners or Government Agencies. Responses have been received from 39 landowners with 15 in favour of the suggestion (to amalgamate into Jandakot) and 24 in opposition. 2 respondents who opposed the proposal indicated a preference for the area to be integrated with the proposed new locality area. All responses are summarised in the attachments, together with the officer’s commentary.

While there are more opponents to the proposal (to merge with Jandakot) than supporters, the statistical fact is that less than 25% of the landowners in the area have formally registered this opposition, while the remainder are either in favour (16%), or are ambivalent (59%), by not expressing a particular preference.

In addition to the formal responses received, two petitions have been submitted (one on each proposal) offering alternative names for Council to consider on each proposal. However, in both instances, the majority of signatures are from people not directly affected by the proposals (i.e. are not landowners in those areas). For the sake of consistency, the intent of the petitions have been described in the attachments, indicating the wishes of the ‘eligible’ respondents who would not have otherwise provided comment as identified in the statistics above.

Overall, it is considered that, while the level of support for the proposals has not been overwhelming, neither has opposition to them. Accordingly, it is now timely for Council to resolve its preferred nomenclature outcomes for these areas, because the opportunity to introduce a new locality name is limited, as the rate of development (and associated home occupancy) increases in the area.

Strategic Plan/Policy Implications

Community & Lifestyle

- The significance and richness of our local Indigenous people and diverse multicultural community will be recognised and celebrated.
- Conservation of our heritage and areas of cultural significance.

Leading & Listening

- Effective and constructive dialogue with all City stakeholders.

Budget/Financial Implications

N/A

Legal Implications

The Geographic Names Committee (GNC) is the branch of the relevant State Government agency ultimately responsible for allocating names to localities across the State. The GNC has comprehensive guidelines which it uses to support its decisions and these must be adhered to by local governments wishing to name / rename localities.

Community Consultation

The landowners in the areas of Banjup directly affected by these proposals (approximately 500 in total) have been contacted and provided an opportunity to support, oppose or offer alternative suggestions to be applied. Respondents had until 3 August 2015 to reply.

Attachment(s)

1. Item previously presented to July 2015 Council Meeting.
2. Map identifying affected areas.
3. Schedule of Submissions (Survey 1).
4. Schedule of Submissions (Survey 2).

Advice to Proponent(s)/Submissioners

Those who lodged a submission on the proposal have been advised that this matter is to be considered at the 13 August 2015 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

18. EXECUTIVE DIVISION ISSUES**19. MOTIONS OF WHICH PREVIOUS NOTICE HAS BEEN GIVEN****20. NOTICES OF MOTION GIVEN AT THE MEETING FOR CONSIDERATION AT NEXT MEETING**

21. **NEW BUSINESS OF AN URGENT NATURE INTRODUCED BY COUNCILLORS OR OFFICERS**
22. **MATTERS TO BE NOTED FOR INVESTIGATION, WITHOUT DEBATE**
23. **CONFIDENTIAL BUSINESS**
 - 23.1 **(OCM 13/8/2015) - MINUTES OF THE CHIEF EXECUTIVE OFFICER PERFORMANCE AND SENIOR STAFF KEY PROJECTS APPRAISAL COMMITTEE MEETING - 16 JULY 2015 (027/002) (S CAIN) (ATTACH)**

RECOMMENDATION

That Council receive the Minutes of the Chief Executive Officer Performance and Senior Staff Key Projects Appraisal Committee Meeting held on 16 July 2015, as attached to the Agenda, and adopt the recommendations therein.

COUNCIL DECISION

Background

The Chief Executive Officer's Performance and Senior Staff Key Projects Appraisal Committee met on 16 July 2015. The minutes of that meeting are required to be presented to Council and its recommendations considered by Council.

Submission

The Minutes of the Committee meeting are attached to the Agenda. Items dealt with at the Committee meeting form the basis of the Minutes.

Report

The Committee recommendations are now presented for consideration by Council and, if accepted, are endorsed as the decisions of Council. Any Elected Member may withdraw any item from the Committee meeting for discussion and propose an alternative recommendation for

Council's consideration. Any such items will be dealt with separately, as provided for in Council's Standing Orders.

Strategic Plan/Policy Implications

Leading & Listening

- A responsive, accountable and sustainable organisation.
- A skilled and engaged workforce.

Budget/Financial Implications

Committee Minutes refer.

Legal Implications

Committee Minutes refer.

Community Consultation

N/A

Attachment(s)

Minutes of the Chief Executive Officer Performance and Senior Staff Key Projects Appraisal Committee 16 July 2015 are provided to the Elected Members as confidential attachments.

Advice to Proponent(s)/Submissioners

The CEO and Senior Staff have been advised that this item will be considered at the August 2015 OCM.

Implications of Section 3.18(3) Local Government Act, 1995

Committee Minutes refer.

24 (OCM 13/8/2015) - RESOLUTION OF COMPLIANCE (SECTION 3.18(3), LOCAL GOVERNMENT ACT 1995)

RECOMMENDATION

That Council is satisfied that resolutions carried at this Meeting and applicable to items concerning Council provided services and facilities, are:-

- (1) integrated and co-ordinated, so far as practicable, with any provided by the Commonwealth, the State or any public body;
- (2) not duplicated, to an extent Council considers inappropriate, services

or facilities as provided by the Commonwealth, the State or any other body or person, whether public or private; and

(3) managed efficiently and effectively.

COUNCIL DECISION

25. CLOSURE OF MEETING

CITY OF COCKBURN

SUMMARY OF MINUTES OF AUDIT & STRATEGIC FINANCE COMMITTEE MEETING HELD ON THURSDAY, 16 JULY 2015 AT 6:00 PM

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CITY OF COCKBURN

MINUTES OF AUDIT & STRATEGIC FINANCE COMMITTEE MEETING HELD ON THURSDAY, 16 JULY 2015 AT 6:00 PM

PRESENT:

ELECTED MEMBERS

Mr Y Mubarakai	-	Councillor (Presiding Member)
Mrs C Reeve-Fowkes	-	Deputy Mayor
Ms L Smith	-	Councillor
Mr P. Eva	-	Councillor

IN ATTENDANCE

Mr S. Cain	-	Chief Executive Officer
Mr D. Green	-	Director, Governance & Community Services
Mr S. Downing	-	Director, Finance & Corporate Services
Mr D. Arndt	-	Director, Planning & Development
Mr C. Sullivan	-	Director, Engineering & Works
Mr N. Mauricio	-	Manager, Financial Services
Mr J. Ngoroyemoto	-	Governance & Risk Co-ordinator
Mrs B. Pinto	-	PA to Directors – Fin. & Corp. Services & Governance & Comm. Services
Ms Claudine Murphy	-	Media & Communications Officer

1. DECLARATION OF MEETING

The Presiding Member declared the meeting open at 6.01 pm.

2. APPOINTMENT OF PRESIDING MEMBER (If required)

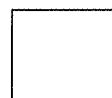
Nil.

3 (ASFC 16/07/2015) - ACKNOWLEDGEMENT OF RECEIPT OF WRITTEN DECLARATION

The Presiding Member advised the meeting he had received a declaration of interest in Item 12.2 which will be read at the appropriate time.

4 (ASFC 16/07/2015) - APOLOGIES & LEAVE OF ABSENCE

Clr Kevin Allen - Apology



5. PUBLIC QUESTION TIME

Nil

6. DEPUTATIONS & PETITIONS

Nil

7. CONFIRMATION OF MINUTES

7.1 (MINUTE NO 152) (ASFC 16/07/2015) - MINUTES OF THE AUDIT AND STRATEGIC FINANCE COMMITTEE MEETING - 19/03/2015

RECOMMENDATION

That Council adopt the Minutes of the Audit and Strategic Finance Committee Meeting held on Thursday, 19 March 2015, as a true and accurate record.

COMMITTEE RECOMMENDATION

MOVED Cllr P Eva SECONDED Deputy Mayor C Reeve-Fowkes that the recommendation be adopted.

CARRIED 4/0

8. BUSINESS LEFT OVER FROM THE PREVIOUS MEETING (IF ADJOURNED)

Nil

9. DECLARATION OF COUNCILLORS WHO HAVE NOT GIVEN DUE CONSIDERATION TO MATTERS IN THE BUSINESS PAPER

Nil

10. COUNCIL MATTERS

10.1 (MINUTE NO 153) (ASFC 16/07/2015) - RISK MANAGEMENT PROGRAM REPORT (021/012) (J NGOROYEMOTO)

RECOMMENDATION

That Council receive the quarterly report on the Risk Management Program.



COMMITTEE RECOMMENDATION

MOVED Deputy Mayor C Reeve-Fowkes SECONDED Cllr P Eva that the recommendation be adopted.

CARRIED 4/0

COUNCIL DECISION**Background**

At the Ordinary Council Meeting of 13 June 2013, Council endorsed the City's Risk Management Policy and associated roll-out program. Subsequently at the Ordinary Council Meeting of 11 December 2014, via the Audit and Strategic Finance Committee, Council endorsed the Risk Management Strategy. The City is progressing in implementing the Risk Program, and this report provides an update on the key milestones achieved over the past 4 months since the last information report was submitted to the Audit Committee.

The City's Risk Program, through adopting the guidelines and principles of the Australian Risk Standard, AS/NZ ISO 31000:2009 is committed to a culture of risk management. City's Policy SC51 'Enterprise Risk Management' (the policy) is a commitment by the City to ensuring that sound risk management practices and procedures are fully integrated into its strategic and operational processes and day to day business practices. With reform now off the agenda, the City will re-commence rolling out the Risk Program in line with the Risk Management Strategy 2013-2015.

Submission

N/A

Report

Risk Management Program:

1. The City's Risk Monitoring and Reporting System - All of the City's operational risks are housed on an Excel spreadsheet. Investigations are continuing to identify an appropriate Risk Management Software. A Business case has been developed and approved. Officers are currently developing the requirements for analysis of short listed suppliers. Software demonstrations and analysis anticipated to be finalised in July.



2. Risk Review Group - A Risk Review Group has been created to co-ordinate the implementation of risk management. This Steering Group will meet bi-monthly, and is charged with integrating risk throughout the organisation. It is a multi-disciplinary team and representative of the 4 divisions of Council, consisting of Governance & Risk Co-ordinator, Manager – Recreation and Community Safety, Manager - Parks and Environment, Manager - Statutory Planning, and Manager - Financial Services. The team will:
 - a. Review Internal Audit Planning.
 - b. Review and monitor the contents of the policy to ensure it remains contemporary and meets the needs of the organisation.
 - c. Communicate and consult on the Policy within each division.
 - d. Engage with appropriate staff members in the identification of risk and development of risk registers.
 - e. Identify, analyse, measure & treat risk.
 - f. Formulate policy relating to the risk management function.

The first two items on the Risk Review Group agenda are to review Position Statement PSES7 – Reports to Council, to include Risk Management on the Agenda, and to implement a formal Risk Management Training program.

The City's Business Continuity Program - The City's Business Continuity Program underwent a testing exercise in May 2015. The Local Government Insurance Services (LGIS) facilitated a Business Continuity Scenario with the Crisis Team lead by the Chief Executive Officer to test the City's Business Continuity Plan (BCP) and Crisis Management capabilities. This exercise achieved the following key outcomes:

1. Validation of current Business Continuity information, plans, timeframes and strategies, and identification of any potential gaps.
2. Familiarisation of the Crisis Team and support staff on roles and responsibilities.
3. Instilling of confidence in using plans and building a common understanding of Business Continuity.

This exercise was facilitated through a workshop with the scenario incorporating a major disruption to Council operations. Key outcomes from the exercise will drive the following initiatives in the coming financial year;



1. Development of Communications Crisis Management Plan.
2. Development of Emergency Traffic Control Procedure.
3. Media training for Crisis Team Members.
4. Fire Extinguisher Training.
5. Evacuation Drill Procedure Review.
6. Review of the City's IT Disaster Recovery Plan capabilities.

Following the Business Continuity Management exercise, the Business Continuity Plan was reviewed. Business Impact Analysis (BIA) discussions were held to confirm, update and review the critical business functions, maximum acceptable outage and recovery strategies. The Business Continuity Plan is currently under review, and the final draft version will be presented to the November Audit and Strategic Finance Committee Meeting.

Insights and moving forward

As the City continues to implement and embed risk management through its Risk Program, it will continue to focus on the following key areas and current initiatives:

1. Continue working with all business units to implement robust processes for embedding risk management, reviewing the City's risk assessment tables and risk profiling into the City's corporate and business planning.
2. Review of the Business Continuity Plan recovery locations suitability.
3. Provision of risk management training for officers to improve the City's risk culture and commitment.
4. A key focus in the next 3 months is to review the City's operational risk registers, systems, and conduct risk control reviews.
5. Strategic Risks Review - once agreed by Council, the CEO's top goals for 2015/16 will provide the strategic outcomes upon which the Strategic Risks will be based.
6. Further investigations into risk software database options, acquire, train implement and rollout in 2015/16.

Strategic Plan/Policy Implications

Leading & Listening

- A responsive, accountable and sustainable organisation.
- A culture of risk management and compliance with relevant legislation, policy and guidelines



Budget/Financial Implications

The City's 2015/16 Annual Budget makes provision for the continued development and further introduction of the Risk Management Framework.

Legal Implications

Local Government (Audit) Regulations 2013, Reg 17.

Community Consultation

N/A

Attachment(s)

N/A

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

11. PLANNING & DEVELOPMENT DIVISION ISSUES

Nil

12. FINANCE & CORPORATE SERVICES DIVISION ISSUES

12.1 (MINUTE NO 154) (ASFC 16/07/2015) - INTERNAL AUDIT - EMPLOYEE TIMEKEEPING (067/004) (M TOBIN) (ATTACH)

RECOMMENDATION

That Council note the Employee Time-Keeping Report, as attached to the Agenda.

COMMITTEE RECOMMENDATION

MOVED Deputy Mayor C Reeve-Fowkes SECONDED Cllr P Eva that the recommendation be adopted.

CARRIED 4/0



COUNCIL DECISION**Background**

Following an audit of salaried employee time-keeping, a recommendation was made and endorsed by the Audit and Strategic Finance Committee that the requirement for certain permanent administrative staff (those on Autopay) to complete timesheets on a daily basis be ended and replaced with a centrally managed, 'exception' reporting system process. It was agreed that this would provide the most beneficial outcome for the City through operational efficiency.

Submission

N/A

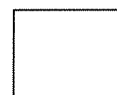
Report

A system of Exception Reporting for flexible hours provisions of the Enterprise Agreement be implemented in appropriate areas across the City. These areas are the professional, salaried and administrative teams which are currently paid under autopay arrangements but who keep an Excel timesheet to manage their flexible hours including days off and time off in lieu of payment.

The City has very few disciplinary issues with its salaried employees and it is rare that instances of attendance or accurate time-keeping are an issue. This would suggest that for many employees, Exception Reporting will be a suitable method of recording time off within the flexible work-hours provisions of the Enterprise Agreement. Other time-off is recorded in the My Leave and then Payroll System.

A two month pilot across four business units in two Divisions was successful and resulted in the guidelines being improved. It led to a renewed focus on obtaining prior approval to work hours above the contracted employment hours. The Exception Reporting demonstrated that the employer is more focussed on outcomes achieved or service delivered rather than time spent sitting at a desk and showed a high degree of trust in a professional employee which can lead to higher engagement from employees.

Whilst there was some resistance to change, this would be overcome as time passed and the system was implemented across more teams. Eventually the culture would change to one of productivity rather than onerous time recording.



Strategic Plan/Policy Implications

Leading & Listening

- A responsive, accountable and sustainable organisation.
- A skilled and engaged workforce.
- A culture of risk management and compliance with relevant legislation, policy and guidelines

Budget/Financial Implications

N/A

Legal Implications

N/A

Community Consultation

N/A

Attachment(s)

Report on Employee Time-Keeping.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

DECLARATION OF INTEREST

The Presiding Member advised the meeting he had received a declaration of Conflict of Interest from Clr Smith in relation to Item 12.2. The nature of the interest is that she was an employee of Halo, which is the subject of a debt write-off.

CLR SMITH LEFT THE MEETING THE TIME BEING 6.05 PM

12.2 (MINUTE NO 155) (ASFC 16/07/2015) - ANNUAL DEBTS WRITE-OFF (069/002) (N MAURICIO) (ATTACH)

RECOMMENDATION

That Council write-off the following bad debts and infringements:

- (1) commercial debt for Quarry Investments Pty Ltd (atf Beeliar

Investment Trust) - \$159,132.73 (ex-GST), being for road work construction services;

(2) sundry commercial and community debts totalling \$13,922.05 (ex-GST); and

(3) infringements plus legal costs totalling \$636.00.

as detailed in the attachments to the Agenda

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COMMITTEE RECOMMENDATION

MOVED Deputy Mayor C Reeve-Fowkes SECONDED Cllr P Eva that the recommendation be adopted.

CARRIED 3/0

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COUNCIL DECISION

Background

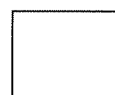
Section 6.12 (1)(c) of the Local Government Act allows local governments to write off any amount of money owing to it (other than rates and service charges). This action is required where debts become delinquent.

Council adopted the Debtors Management Policy AFCS9 at its meeting in June 2012. This states that bad debt write offs should only occur where all avenues for recovery have been exhausted or it is unviable to keep pursuing the debt.

The policy provides for unrecoverable debts (other than rates and service charges) up to the individual value of \$200 to be written off under Council delegation. However, those over \$200 are to be brought to Council for write off on an annual basis.

Submission

N/A



Report

The Audit and Strategic Finance Committee previously considered bad debts in July 2014 with Council subsequently writing off sundry debts and infringements totalling \$39,618.13. Whilst the City has an excellent track record in managing and collecting its outstanding debts, there are always some that become uncollectible for a variety of reasons.

The typical debtors mix for the City comprises commercial debtors (mainly landfill trade debtors) and community based debtors (hall and reserve hire and other community service provision). Common causes of bad debts in both these areas are failing businesses or organisations and untraceable companies and individuals. Some debts are also not worth pursuing due to it being uneconomical to apply formal debt recovery procedures.

Attached to the Agenda is a detailed listing of the uncollectible debts and infringements recommended for write-off by Council this year. These have been categorised by their debt type and include relevant commentary on their status and the recovery efforts made where applicable. A consolidated summary of the write-offs requested for a total of \$173,690.78 (ex-GST) is provided below:

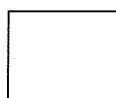
Debt category	Amount to be written-off \$ (ex gst)	Amount to be written-off \$ (inc gst)
Commercial - HWRP landfill	6,649.95	7,314.96
Commercial - construction works	159,132.73	175,046.00
Commercial - other	980.00	1,078.00
Community (hall/reserve hire, services)	6,292.10	6,860.73
Infringements	636.00	636.00
Total	173,690.78	190,935.69

It should be noted that the impact on Council's financial position is \$173,690.78 being the GST exclusive value of the debts to be written off.

Commercial Debtors

This category includes a debt of \$159,133 (ex GST), which is the remaining balance on an original debt of \$259,133. This related to an agreement in Sep 2012 between the City and a property developer for the City to construct a left turn slip and a u-turn facility on Beeliar Drive, in conjunction with road widening works the City was undertaking at the time in the vicinity. Planning conditions required the developer to construct the left turn slip, whilst the developer requested the u-turn facility as it provided a direct benefit to the commercial development at 640 Beeliar Drive.

The City agreed to construct the u-turn and left turn slip on the basis that it met the real costs of the work and this required a \$100,000 advance payment before any works were commenced.



In between the agreement being struck and the works being completed in early 2013, the developer (company) had administrators appointed and eventually was wound up in May 2013. The insolvent financial position of the company did not allow for any dividend payments to unsecured creditors (including the City). Subsequent to this, the Director Engineering & Works unsuccessfully endeavoured to negotiate a settlement with a representative and previous director of the developer.

A strategy to raise a specified area rate for the ten strata properties located at 640 Beeliar Drive was explored during deliberations for the 2015/16 budget, but ultimately abandoned. Whilst four of the units within the development are still owned by related parties of the original company, it was considered unreasonable to burden the owners of the other six lots with the specified area rate.

There is also \$6,649 in landfill debts needing to be written off. Given the high turnover for the HWRP landfill operation, this is considered with normal business parameters (i.e. write-off represents about 0.05% of the annual revenue).

Community Group Debtors

Community based debts totalling \$6,292 are recommended for write-off. This includes \$3,294 for the HALO organisation, representing unpaid accommodation fees from their use of the Joe Cooper recreation centre. The balance of \$2,998 represents a number of hall and reserve hire debts that have proven too difficult to collect and are now considered delinquent.

Infringements

There are two bushfire infringements recommended for write off this year, each for \$250 plus \$68 in legal costs (total write-off \$636). This reflects improved governance and administration over infringement management processes and the effect of the delegation allowing write offs up to \$100 by the Director Finance & Corporate Services.

Debt Write-Offs under Delegation

The City's Debt Management Policy AFCS9 allows for small debts owing to the City to be written off under delegation up to a maximum value of \$200, other than for levied rates and prescribed service charges. Write offs under delegation are usually reported to Council through the monthly financial report as and when they are made.

For the 2014/15 financial year, \$3,319 worth of infringements and \$956 of sundry debts were written off under delegation. Also, \$226 in small balance write-offs comprising penalty interest on rates and service



charges (up to \$5 individually), was waived under delegation in accordance with the Debt Management Policy.

Strategic Plan/Policy Implications

Leading & Listening

- A responsive, accountable and sustainable organisation.
- Manage our financial and infrastructure assets to provide a sustainable future.
- A culture of risk management and compliance with relevant legislation, policy and guidelines

Policy AFCS9 'Debt Management' refers.

Budget/Financial Implications

A write off of bad debts totalling \$173,690.78 will be taken directly to the Income Statement in the 2014/15 financial year, reducing any EOFY surplus position.

Legal Implications

Debts which are irrecoverable require Council authorisation in order to be written off under the provisions of the Local Government Act, Section 6.12 (1)(c).

Community Consultation

N/A

Attachment(s)

List of Outstanding Debts to be written off – July 2015.

Advice to Proponent(s)/Submissioners

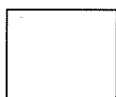
N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

CLR SMITH RETURNED TO THE MEETING THE TIME BEING 6.11 PM.

THE PRESIDING MEMBER ADVISED CLR SMITH THE DECISION OF COMMITTEE.



12.3 (MINUTE NO 156) (ASFC 16/07/2015) - INTERNAL AUDIT - PROCUREMENT IMPROVEMENT (067/004) (N MAURICIO) (ATTACH)

RECOMMENDATION

That Council

- (1) endorse the Procurement Improvement Audit in accordance with the agreed Terms of Reference; and
- (2) require the audit findings to be presented to the Audit and Strategic Finance Committee for consideration at the November 2015 Meeting.

COMMITTEE RECOMMENDATION

MOVED Deputy Mayor C Reeve-Fowkes SECONDED Cllr P Eva that the recommendation be adopted.

CARRIED 4/0

COUNCIL DECISION

Background

Each financial year, the Audit and Strategic Finance Committee needs to review internal audit requirements and prioritise audit projects to be undertaken that year.

Potential projects are identified through various means including Executive Staff, Strategic Business Managers, Internal Auditors and risk reviews.

The Council has a supply contract with Deloitte for the provision of internal auditing services. This is for a period of four years ending 30 June 2015. A report will be brought to the November meeting dealing with the provision of internal audit services beyond 30 June 2015 and the development of a new internal audit plan.

Submission

N/A



Report

The following table indicates the progress in completing previous internal audit plans:

Auditable Issues/areas	Originally Proposed Timetable	Status
Fraud Control Assessment	2011/12	Completed 2011/12
Procurement/Supply Chain Management Process	2011/12	Completed 2012/13
IT Security	2011/12	Risk Management
Contract Management	2012/13	Reschedule to 2014/15
Revenue Recognition (rates and other sources)	2012/13	Completed 2013/14
Employee Health and Safety	2012/13	Risk Management
Exercise of Delegated Authorities	2012/13	Completed 2012/13
Assurance of financial processes at FESA funded services	2012/13	Reschedule 2014/15
Audit compliance with new Building Act requirements effectiveness of business processes	2012/13	Risk Management
Cash handling (all areas)	2013/14	Risk Management
Confidential Information (awareness and protection)	2013/14	Risk Management
Payroll Function	2013/14	Risk Management
Audit salaried employee timekeeping practices	2013/14	Completed 2013/14
Cash handling procedures across the City	2013/14	Risk Management
Review implementation of fraud risk management	2013/14	Completed 2013/14
Payment processing	2014/15	Risk Management
Long term financial planning	2014/15	Not completed
Major land transactions	2014/15	Not completed

Due to the pervasive nature of local government reform during 2014/15, the internal audit projects previously planned were put on hold. Management determined the best use for the audit resource was to assist the City with reform related activities. Preliminary discussions and planning took place to scope the internal audit of the divestment of assets and liabilities to the Cities of Fremantle and Melville. This would serve to provide a level of support and assurance to the City, as well as some independence and equity to the negotiations that had already commenced between the three local governments.



With the sudden collapse of the State Government's local government reform agenda, a substitute area was sought for internal audit. Management settled on an audit of the City's progress in improving its procurement related activities and addressing the many recommendations arising from several previous year audits and risk reviews as outlined below.

In 2012/13, Deloitte completed a comprehensive review into the overall effectiveness of the procurement function and the adequacy of internal controls and made key recommendations around procurement methods, strategy and supplier relationship management. Also during 2012/13, the City voluntarily participated in a CCC commissioned audit into local government procurement (along with 4 other large metropolitan Councils). This audit specifically evaluated the City's capacity to prevent, identify and respond to misconduct in procurement practices and also contained several key recommendations. This audit activity, coupled with a Fraud Control and Risk Assessment (carried out in 2012 and then reviewed in 2014) and the implementation of the City's Risk Management Framework throughout 2013 and 2014 put a heavy focus on the procurement activities of the City.

Contract related procurement risk was identified as one of the City's seven strategic risk areas in a high level risk report prepared in 2013. Specifically, the risk was defined as *an inconsistent method of managing contracts throughout the City of Cockburn (including performance management, consistent documentation, transparent process, etc).*

The following definition of a strategic risk extracted from the report highlights the importance of its proper mitigation:

A strategic risk is deemed as a risk that could possibly impact the whole of the organisation (both from a business interruption point of view, but potentially reducing the capacity of Council to provide services to the community). A strategic risk cannot be managed individually by a Division or a Department. These risks need to be managed and initiated by the Executive, as Council will need to dedicate both expenditure and associated resources to manage these risks.

All of the above serves to demonstrate the importance of ensuring the right level of priority and resourcing is targeted at the City's procurement function and that past effort and work does not get wasted or forgotten. With these imperatives in mind, a set of terms of reference was developed in consultation with the internal auditor (attached to the Agenda), with the following purpose and scope:

The internal audit will assess the City's progress in implementing action plans designed to address the opportunities for improvement and recommendations raised (38 in total) in the



following audits and reviews of the City's procurement function, conducted during 2012 and 2013:

- *Deloitte Fraud Control and Risk Assessment (January 2012)*
- *Deloitte Procurement Internal Audit (November 2012)*
- *CCC Procurement Audit (January 2013)*
- *RMRI High level operational risk review (September 2013).*

The internal audit field work has been completed and the auditors are in the process of formulating a draft report. It is envisaged that the final report will be delivered by the end of July and brought to the next meeting of the Audit and Strategic Finance Committee in November 2015.

Strategic Plan/Policy Implications

Leading & Listening

- A responsive, accountable and sustainable organisation.
- Manage our financial and infrastructure assets to provide a sustainable future.
- A culture of risk management and compliance with relevant legislation, policy and guidelines

Budget/Financial Implications

Funding for internal audit activities is provided for within the City's operational budget.

Legal Implications

N/A

Community Consultation

N/A

Attachment(s)

Procurement Improvement Internal Audit – Terms of Reference.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.



12.4 (MINUTE NO 157) (ASFC 16/07/2015) - INTERIM EXTERNAL AUDIT (067/001) (N MAURICIO) (ATTACH)

RECOMMENDATION

That Council receive the Interim Financial External Audit Report for the year ended 30 June 2015, as attached to the Agenda.

COMMITTEE RECOMMENDATION

MOVED Deputy Mayor C Reeve-Fowkes SECONDED Cllr P Eva that the recommendation be adopted.

CARRIED 4/0

COUNCIL DECISION

Background

Council is required to examine the report prepared by the External Auditor and is to determine if any matter raised in the report requires action to be taken.

Council is also required to meet with the Auditor of the Local Government at least once in each year. This will be for the receipt of the final audit report as occurs each financial year.

The Terms of Reference of the Audit and Strategic Committee were adopted by Council on 8 November 2007.

Submission

N/A

Report

The Interim External Audit Report for the period ending 30 June 2015 was received from Council's Auditors, Macri Partners in July 2015. The Interim Report covered a review of the accounting and internal control procedures in operation, as well as testing of transactions in the following areas:

- Bank Reconciliations
- Investment of Surplus Funds
- Purchases
- Payments and Creditors



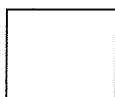
- Rate Receipts and Rate Debtors
- Receipts and Sundry Debtors
- Payroll
- General Accounting (Journals, etc.)
- IT Controls
- Registers (Tenders Register, etc.)
- Fixed Assets
- Review of Council Minutes

The review also included an examination of certain compliance matters required under the Local Government Act and Financial Management Regulations.

Internal controls were examined primarily for the purpose of expressing an opinion on the financial statements of the City of Cockburn. Generally, the controls surrounding purchases, creditors and payments were found to be appropriate to meet the City's requirements.

Below is a summary of the issues and recommendations raised by the Audit and the associated management responses:

<u>Issue</u>	<u>Management Response</u>
Bank Reconciliations	
The listing of un-presented cheques included a number of old cheques totalling \$14,415.64 that were more than 12 months old.	These are required to be dealt with on an annual basis under the City's EOFY procedures, with this timing being considered appropriate given that fewer than 3% of the City's payments are now made by cheque. There is negligible risk to the City from having stale cheques outstanding for up to 12 months.
Investment of Surplus Funds	
Audit noted that whilst the reconciliations of the investment register to the general ledger are performed on a monthly basis, there was no evidence that the reconciliations were signed and dated by the preparer and reviewed by an independent senior officer.	The City will add sign off requirements to the existing investments reconciliation process commencing in July 2015.
Purchasing & Payments	
Audit noted five (5) instances (18 samples tested) whereby the process of raising a purchase requisition, approving the purchase requisition and receiving the goods (which provides approval of the supplier	The City's centre led Procurement model and framework provides an appropriate level of segregation of duties, given the organisation size and structure, systemised Delegated Financial Authority limits and the key separation of the requisitioning and



<u>Issue</u>	<u>Management Response</u>
<p>invoice for payment) was performed by the same officer. Such lack of segregation of duties may increase the risk of misappropriation of Council assets and fraud.</p>	<p>goods receipting duties from the purchasing duties.</p> <p>Internal control includes independent probity checking during the purchasing and invoice processing functions.</p>
<p>Audit noted two (2) instances (18 samples tested) where the minimum quotations as required by the City's Procurement Policy were not obtained to procure goods and services.</p> <p>Lack of following the City's Procurement Policy with respect to obtaining the required number of quotations may fail to provide assurance that the local government is procuring goods and services at best value for money (i.e. at the most competitive prices).</p>	<p>The City's Procurement Policy SC38 states the following:</p> <p>"Public tenders or quotations are not required (regardless of the value of expenditure) if the purchase is under a Contract of WALGA, Commonwealth or State Department of Treasury and Finance..."</p> <p>Given that the 2 identified procurements were both made under WALGA contracts, there is no policy requirement for quotations to be sought on these occasions.</p>
<p>Audit noted one (1) instance (6 samples tested) where a tender contract was signed by a representative of the City, but not signed by the contractor.</p> <p>As auditors, we are concerned about the legal liability to the Council should any dispute arise over the supply of goods and services.</p>	<p>The City accepts this audit finding – the offending Contract acceptance has been sent to the Supplier again for execution.</p> <p>It should be noted that this oversight occurred before the City implemented a fully functioning Contract Management System that has reporting and alerting capability. This will assist to eliminate these types of occurrences in future.</p>
Rates	
<p>Audit noted that the Senior Rates Officer can write off rate debts under \$5. We could not locate any authority in the Delegations Register or Council Policies to support these write offs.</p>	<p>The City's Debt Management Policy AFCS9 allows for small debts owing to the City to be written off under delegation up to a maximum value of \$200 (other than for levied rates and prescribed service charges).</p> <p>A total of \$226 in small balance write-offs (none greater than \$5), comprising penalty interest on rates and service charges, was waived in accordance with the Debt Management Policy and guidelines. The exercise of the delegation will be recorded in future and the Policy will be updated to make this delegation more clear.</p>

<u>Issue</u>	<u>Management Response</u>
Sundry Debtors	
A list of ninety day debtors was identified for review and comment.	Management review indicated that all but one of the debts identified has been paid or is being collected through instalment arrangements. Two of the debts have been referred to audit committee in July to be written off by Council.
<p>The audit review of the procedures relating to raising of credit notes indicated that the credit notes issued were based on the associated debtor invoice number and not sequentially numbered.</p> <p>In the absence of numerical sequence over credit notes, audit believes there is a risk that credit notes raised may not be accounted for within the system and may result in loss of revenue</p>	<p>All credit notes raised within the system are now invoked by an authorised memo from the relevant manager as per the implemented audit recommendation from last year. Credit notes are associated with the relevant invoice as a debt adjustment and hence don't currently have separate numbering.</p> <p>The City will review the credit note raising, approval and processing system methodology to ensure that any loss of revenue risk is appropriately mitigated in future.</p>
Payroll	
Audit noted two (2) instances where calculations were performed by the Payroll Coordinator based on source data obtained from the payroll system. However, the calculation worksheets were not reviewed by an independent senior officer to ensure that the termination calculations were correct.	The City accepts this audit finding – it will amend the Payroll and HR procedures to include compulsory checking of termination payments by the HR Coordinator, or for straightforward termination payments an HR Officer. This will provide an independent check of the calculation worksheets.
During the review of the employee annual leave entitlement balances as at 2 June 2015, audit noted that 41 employees have accrued in excess of 8 weeks to a maximum of 16 weeks.	<p>The current management process for monitoring of excess annual leave balances is resource intensive. A daily status alert is established but does not take into account future leave which has been planned (requested and approved). This reduces the 41 employees significantly.</p> <p>The HR team will work with Business Systems staff to develop a more efficient system that alerts line managers which of their staff are either approaching or have reached excess leave status.</p>



<u>Issue</u>	<u>Management Response</u>
Compliance	
<p>Local Government Regulations require that a copy of the annual budget of a local government is to be submitted to the Departmental CEO within 30 days of its adoption by the local government.</p> <p>Audit noted that the annual budget was adopted on 12 June 2014; however it was forwarded to the department on 14 July 2014.</p>	<p>The City accepts this audit finding - The 30th day after budget adoption being a Saturday (12th July), the City believed submitting this on the following Monday morning (next available business day), was acceptable under "excluded days" provisions under Section 61(1)(e) of the Interpretation Act 1984. However it is now acknowledged that under Section 61(1)(c), the budget would need to be submitted by the 29th day after adoption in order to comply with the regulations. The 2015/16 budget was submitted on the 27th day after adoption.</p>
General Accounting	
<p>During the review of the progress on fair valuation implementation for the 2015 financial year, audit noted that the City had not made provision within the financial accounts for the liability of future costs associated with closing and restoring the HWRP landfill site.</p> <p>The City is required by law to rehabilitate the landfill site and Accounting Standard 137 <i>Provisions, Contingent Assets and Contingent Liabilities</i> requires that a provision be made (where a reliable estimate can be made).</p>	<p>The City is not convinced that this type of calculation will result in an estimate that is sufficiently reliable to use in recognising a provision liability for post closure and rehabilitation costs at the Henderson Waste Recovery Park landfill site. It believes that a contingent liability would be more appropriate under these circumstances.</p> <p>However, the City will seek independent and expert advice to ascertain whether a reliable estimate of its current liability can be prepared. The City will also seek further guidance from the auditor.</p>

The interim audit report attached to the agenda provides more detailed commentary on the findings of the interim audit and the management responses.

Strategic Plan/Policy Implications

Leading & Listening

- A responsive, accountable and sustainable organisation.
- A culture of risk management and compliance with relevant legislation, policy and guidelines

Budget/Financial Implications

The cost for the interim audit is covered by the City's annual budget allocation for external audit activities.



Legal Implications

N/A

Community Consultation

N/A

Attachment(s)

Interim External Audit Report.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) Local Government Act, 1995

Nil.

13. ENGINEERING & WORKS DIVISION ISSUES

Nil

14. COMMUNITY SERVICES DIVISION ISSUES

Nil

15. EXECUTIVE SERVICES DIVISION ISSUES

Nil

16. MOTIONS OF WHICH PREVIOUS NOTICE HAS BEEN GIVEN

Nil

17. NOTICES OF MOTION GIVEN AT THE MEETING FOR CONSIDERATION AT NEXT MEETING

Nil

18. NEW BUSINESS OF AN URGENT NATURE INTRODUCED BY DECISION OF MEETING BY COUNCILLORS OR OFFICERS

Nil



19. MATTERS TO BE NOTED FOR INVESTIGATION, WITHOUT DEBATE

Nil

20. CONFIDENTIAL BUSINESS

Nil

21 (ASFC 16/07/2015) - CLOSURE OF MEETING

6.22 PM

CONFIRMATION OF MINUTES

I, (Presiding Member) declare that these minutes have been confirmed as a true and accurate record of the meeting.

Signed: Date:/...../.....



POL	COUNCIL MEETINGS	SC3
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POLICY CODE:	SC3
DIRECTORATE:	Executive Services
BUSINESS UNIT:	Executive Support
SERVICE UNIT:	Executive Support
RESPONSIBLE OFFICER:	Director, Governance & Community Services
FILE NO.:	086/001
DATE FIRST ADOPTED:	15 April 2007
DATE LAST REVIEWED:	12 December 2013
ATTACHMENTS:	N/A
DELEGATED AUTHORITY REF.:	N/A
VERSION NO.	2

Dates of Amendments / Reviews:	
DAPPS Meeting:	22 March 2012 28 November 2013
OCM:	11 June 2010 12 April 2012

BACKGROUND:

Council Meetings are to be held in accordance with the Local Government Act (Part 5).

PURPOSE:

To establish a uniform time for the holding of Ordinary monthly meetings of Council.

POLICY:

- (1) Full Council will meet on the second Thursday of each month at 7.00pm in the Council Chamber and will be open to the public.
- (2) The Council meeting will consider matters as listed on the Council Agenda paper.
- (3) An Ordinary Council Meeting not be conducted during January of each year and any required business be conducted as a Special Council Meeting during this specific period.
- (4) Any further variation to the date and/or time of Council Meetings will be subject to resolutions of Council.

CITY OF COCKBURN

SUMMARY OF MINUTES OF THE GRANTS & DONATIONS COMMITTEE MEETING HELD ON WEDNESDAY, 29 JULY 2015 AT 6:00 PM

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CITY OF COCKBURN

MINUTES OF THE GRANTS & DONATIONS COMMITTEE MEETING HELD ON WEDNESDAY, 29 JULY 2015 AT 6:00 PM

PRESENT:

Mr L. Howlett	-	Mayor
Mr S. Portelli	-	Councillor (Presiding Member)
Ms L. Wetton	-	Councillor
Mr P. Eva	-	Councillor

IN ATTENDANCE:

Mr R. Avard	-	Manager, Community Services
Ms M. Bolland	-	Grants & Research Officer

1. DECLARATION OF MEETING

The Manager, Community Services declared the meeting open, the time being 6:10pm.

2. APPOINTMENT OF PRESIDING MEMBER (If required)

The Manager, Community Services advised that in the absence of the appointed Presiding Member, and pursuant to Section 5.44 of the Local Government Act, 1995 he had been delegated the power to preside at the Grants and Donations Committee Meeting held on 29 July 2015 and to conduct the election to determine the Presiding Member of the Committee, in accordance with Schedule 2.3 Division 1 of the Act.

The Manager, Community Services called for nominations and received a nomination for Cllr Steve Portelli to be appointed Presiding Member for the meeting, from Cllr Lyndsey Wetton.

There being no further nominations, Cllr Steve Portelli was duly declared Presiding Member.



3. **ACKNOWLEDGEMENT OF RECEIPT OF WRITTEN DECLARATIONS OF FINANCIAL INTERESTS & CONFLICT OF INTEREST (BY PRESIDING MEMBER)**

Nil

4. **(GAD 29/07/2015) - APOLOGIES & LEAVE OF ABSENCE**

Deputy Mayor Carol Reeve-Fowkes	-	Apology
Clr Stephen Pratt	-	Apology
Clr Yaz Mubarakai	-	Apology

5. **CONFIRMATION OF MINUTES**

5.1 **(MINUTE NO 85) (GAD 29/07/2015) - MINUTES OF THE GRANTS AND DONATIONS COMMITTEE MEETING - 22/4/2015 (ATTACH)**

RECOMMENDATION

That the minutes of the Grants and Donations Committee meeting held on 22 April 2015 be adopted as a true and accurate record.

COMMITTEE DECISION

MOVED Clr L Wetton SECONDED Clr P Eva that Council adopt the Minutes of the Grants and Donations Committee meeting held on 22 April 2015 as a true and accurate record.

CARRIED 4/0

6. **DEPUTATIONS & PETITIONS**

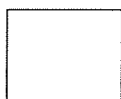
Nil

7. **BUSINESS LEFT OVER FROM THE PREVIOUS MEETING (IF ADJOURNED)**

Nil

8. **DECLARATION OF COUNCILLORS WHO HAVE NOT GIVEN DUE CONSIDERATION TO MATTERS IN THE BUSINESS PAPER**

Nil



9. COUNCIL MATTERS

9.1 (MINUTE NO 86) (GAD 29/07/2015) - INDIAN OCEAN FIREWORKS SPONSORSHIP AGREEMENT WITH CITY OF FREMANTLE (162/003) (S SEYMOUR-EYLES) (ATTACH)

RECOMMENDATION

That Council provide an annual donation of \$25,000 (plus GST), for a three-year period up to and including 2018, from the Grants and Donations Budget to the City of Fremantle, to co-host the Indian Ocean Fireworks event on Australia Day, and agree to the terms and conditions outlined in the Sponsorship Agreement attached to the Agenda.

COMMITTEE RECOMMENDATION

MOVED Mayor L Howlett SECONDED Cllr P Eva that the recommendation be adopted.

CARRIED 4/0

COUNCIL DECISION

Background

Since 2007, the City of Fremantle has held a fireworks display in Fremantle on Australia Day. In 2011, the City of Cockburn agreed to become a Sponsorship Partner to contribute to the fireworks display, which is now known as the Indian Ocean Fireworks.

The latest three-year agreement expired in the 2014/15 financial year and the City of Fremantle has proposed a new three-year Sponsorship Agreement for the event in 2016, 2017 and 2018.

Submission

The proposed Indian Ocean Fireworks Agreement is attached to the Agenda.

Report

The Event

On Australia Day each year, the night sky above Bathers Beach and Fishing Boat Harbour in Fremantle comes alive with a 20-minute display of fabulous fireworks (with simulcast) from 8pm. Prior to the fireworks display from 4pm, the Esplanade Reserve is filled with music, food, roving entertainers and kids' activities.

Attendance

It is anticipated that in excess of 40,000 people will enjoy the atmosphere of the Australia Day Celebrations and the Indian Ocean Fireworks display at Bathers Beach in 2016 with expected attendance numbers to increase each year.

Participation

In 2015, the fireworks were viewed by a large cross section of many age demographics, music tastes and different backgrounds from Perth's coastal suburbs, north and south of the river. The Indian Ocean Fireworks display is seen as a relaxed family orientated fun event.

According to the independent market research and evaluation undertaken by Asset Research for the 2015 event, it shows that the majority of respondents (91.1%) who had visited Fremantle on Australia Day were 'at least' satisfied with the experience they had. 31.1% of these respondents were very satisfied with the experience, with a further 60.0% being satisfied.

A total of 96 survey respondents on the day came from suburbs within the City of Cockburn (up from 89 in 2014). This represented 32.7% of all Perth metropolitan area respondents (31.1% in 2014) and 29.5% of all survey respondents (28.3% in 2015). This represents a continuation in the growth of respondents coming from the Cockburn area in this survey period.

Sponsorship Agreement

For the City's \$25,000 (plus GST) contribution, the Sponsorship Agreement proposes a list of Sponsorship Partner Benefits (Item 8 in the Schedule to the Agreement) including:

- Recognition of the Sponsorship Partner as a partner in the event on all forms of printed advertising and promotional material in relation to the event and published before, during and after the event.



- 30 invitations (free of charge) to the VIP function at Bather's Beach House.
- An evaluation report of the Sponsorship Partnership arrangement by 1 March in each year.

Strategic Plan/Policy Implications

Community & Lifestyle

- Communities that are connected, inclusive and promote intergenerational opportunities.
- Communities that take pride and aspire to a greater sense of community.
- Promotion of active and healthy communities.

Leading & Listening

- A responsive, accountable and sustainable organisation.

Budget/Financial Implications

An allocation of \$25,000 (plus GST) from the 2015/16 Grants and Donations Budget has been recommended.

Legal Implications

If Council agrees to the proposed Sponsorship terms and conditions, as contained in the proposed Indian Ocean Fireworks Agreement attached to the Agenda, it will enter into a legal agreement with the City of Fremantle for a term of three years.

Community Consultation

An independent Asset Research evaluation survey is conducted each year to gauge community satisfaction. For the 2015 event it shows that the majority of respondents (91.1%) who had visited Fremantle on Australia Day were 'at least' satisfied with the experience they had. 31.1% of these respondents were very satisfied with the experience, with a further 60.0% being satisfied.

Attachment(s)

1. Proposed Indian Ocean Fireworks Agreement with the City of Fremantle.



Advice to Proponent(s)/Submissioners

The City of Fremantle has been advised that the proposed Agreement will be presented to the Grants and Donations Committee on Wednesday 29 July 2015 and the Committee's recommendations will go to the August Council Meeting for a decision.

Implications of Section 3.18(3) Local Government Act, 1995

Nil

9.2 (MINUTE NO 87) (GAD 29/07/2015) - SPEARWOOD BOWLING CLUB REQUEST FOR DONATION TO RESURFACE BOWLING GREEN (162/002) (R AVARD) (ATTACH)

RECOMMENDATION

That Council:

- (1) contribute up to one-third of the cost of resurfacing one of the synthetic bowling greens at Spearwood Bowling Club, to a maximum contribution of \$57,300 (plus GST), and
- (2) the works in (1) above be to a standard to allow the Club to retain eligibility for pennant competitions.

COMMITTEE RECOMMENDATION

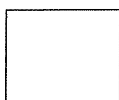
MOVED Mayor L Howlett SECONDED Cllr P Eva that the recommendation be adopted.

CARRIED 4/0

COUNCIL DECISION

Background

Spearwood Bowling Club was established in 1993 and is located on Azelia Road at the Dalmatinac Club in Spearwood. The bowling greens



are on City of Cockburn land leased to the Club. The Club currently has 99 members and the close proximity of the Cockburn Bowling and Recreational Club limits the membership growth of both clubs as they compete for members. The Club has three greens in use and all need replacement as they have had seven years of use. On a weekly basis there are 50 people who regularly use the greens about three times a week.

Submission

The request letter from Spearwood Bowling Club and a quote for the resurfacing works (Option B) is attached to the Agenda.

Report

The Spearwood Bowling Club have advised that they are currently having some major problems with their synthetic bowling greens, which have now deteriorated to a point that they may not pass inspection by Bowls WA. This may result in the Club's exclusion from the next pennant competition which will commence towards the end of October 2015.

The Club must urgently resurface at least one bowling green to satisfy Bowls WA standards to retain eligibility for pennant competition. Funds required for this short-term fix are approximately \$171,900 (plus GST), as indicated in "Option B" in the attached quote. Currently neither the club nor its members are in a financial position to cover this entire cost.

The Club has requested Council support for a grant application to the Department of Sport and Recreation (DSR) Community Sporting and Recreation Facilities Fund (CSRFF) for the funding to be shared between the Club, Council and the State Government on the basis of one-third of the cost, or \$57,300 (plus GST), being contributed by each party.

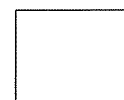
The Club proposes to raise its contribution through a mixture of fundraising, member donations and a loan for the remainder.

It is proposed that the Grants and Donations Committee make a one-off \$57,300 (plus GST) allocation from its 2015/16 Budget on the condition that the other \$114,600 (plus GST) is externally funded.

Strategic Plan/Policy Implications

Infrastructure

- Community facilities that meet the diverse needs of the community now and into the future.



Community & Lifestyle

- Community environments that are socially cohesive and embrace diversity.
- Promotion of active and healthy communities.

Leading & Listening

- A responsive, accountable and sustainable organisation.

Budget/Financial Implications

An allocation of \$57,300 (plus GST) from the 2015/16 Grants and Donations Budget has been recommended on the condition that the other \$114,600 (plus GST) is externally funded.

Legal Implications

Nil

Community Consultation

Nil

Attachment(s)

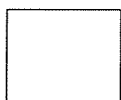
1. Spearwood Bowling Club Request Letter and Quote for the Resurfacing Works (Option B).

Advice to Proponent(s)/Submissioners

The Spearwood Bowling Club has been advised that request will be considered by the Grants and Donations Committee on Wednesday 29 July 2015 and the Committee's recommendations will go to the August Council Meeting for a decision.

Implications of Section 3.18(3) Local Government Act, 1995

Nil



9.3 (MINUTE NO 88) (GAD 29/07/2015) - GRANTS AND DONATIONS COMMITTEE RECOMMENDED ALLOCATIONS 2015/16 (162/003) (R AVARD) (ATTACH)

RECOMMENDATION

That Council:

- (1) adopt the grants, donations, and sponsorship recommended allocations for 2015/16 as attached to the agenda, and
- (2) advertise the availability of the grants, donations and sponsorships in two instalments closing 30 September 2015 and 31 March 2016 respectively.

COMMITTEE RECOMMENDATION

MOVED Mayor L Howlett SECONDED Cllr L Wetton that the recommendation be adopted.

CARRIED 4/0

COUNCIL DECISION

Background

Council approved a budget for grants and donations for 2015/16 of \$1,200,000. The Grants and Donations Committee is empowered to recommend to Council how these funds are to be distributed.

Submission

The City received funding requests from:

- City of Fremantle - Indian Ocean Fireworks Sponsorship Agreement (Agenda Item 9.1 and Attachment)
- Spearwood Bowling Club (Agenda Item 9.2 and Attachment)
- Native ARC (Report Attached)
- Cockburn Wetlands Education Centre (Report Attached)

Report

Committed/Contractual Donations

As can be seen in the Budget attachment, a number of donations are deemed to be committed by legal agreements, such as leases, or by Council Decision.

There is one new proposed commitment for the 2015/16 financial year:

- Spearwood Bowling Club (as to Agenda Item 9.2 and Attachment)

And there are three ongoing commitments that require approval for the 2015/16 financial year:

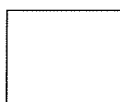
- City of Fremantle - Indian Ocean Fireworks Sponsorship Agreement (as to Agenda Item 9.1 and Attachment)
- A donation to support the administration costs of Native ARC of \$87,921.91.
- A donation to support the administration costs of Cockburn Wetlands Education Centre of \$87,921.91.

At its meeting on 14 August 2014, the following Council Decision was approved:

That Council:

- (1) *approve the contribution sponsorship request from both the Cockburn Wetlands Education Centre and Native ARC for funding towards the annual administration costs for each organisation (Cockburn Wetlands Education Centre \$86,708 and Native ARC \$86,708) for a period of four years indexed annually according to Perth consumer price index, and coinciding with the terms of their leases and also being subject to:*
 1. *The Cockburn Wetlands Precinct members providing an annual report detailing their progress in meeting designated joint Key Performance Indicators as endorsed previously by Council.*
 2. *The Cockburn Wetlands Education Centre and Native ARC each providing a separate annual report which summarises the previous 12 months activities and their progress in meeting designated individual key performance indicators previously endorsed by Council.*

Native ARC and Cockburn Wetlands Education Centre have provided their reports for 2015-16, which are attached to the agenda, and it is



recommended to approve the 2015/16 donation of \$87,921.91 (including CPI of 1.4%) to each organisation.

The total for committed/contractual donations will be \$496,000.

Grants

As can be seen in the Budget attachment, there are a number of grants for which there are established criteria and processes in place.

The 2015/16 Budget includes small increases in the following allocations based on the previous year's expenditure and predicted expenditure for 2015/16:

- Sustainability Grants Program
- Grants General Welfare
- Alcoa Cockburn Community Projects Fund
- Environmental Education Initiatives Program
- Grants to Schools

The 2015/16 Budget includes decreases in the following allocations based on little to no expenditure in the previous year:

- Len Packham Hall Hire Subsidy (Burdiya)
- Safety House/Walk to School Program

There are no other significant changes from last financial year in the new allocations.

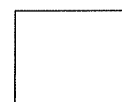
The total proposed for grants is \$439,000.

Donations

It is proposed that Council will seek applications for Donations from not-for-profit organisations in two instalments. It is proposed for 2015/16 to have the first round closing on 30 September 2015 and the second round closing on 31 March 2016.

Applications for Donations will be assessed under policy ACS2, and a report presented to the Committee for its deliberation. The Committee will then consider the requests for Donations and make a recommendation to Council.

Due to an increased demand for Donations in 2014/15, it is proposed to increase the allocation for Donations from \$196,300 to \$210,000 for 2015/16.



Sponsorship

It is proposed to allocate \$55,000 of the 2015/16 Grants and Donations Budget to the Sponsorship program, to reflect the actual expenditure in 2014/15 and allow for a small increase.

It is proposed to seek applications for Sponsorship for Groups in line with the other funding opportunities closing on 30 September 2015 and 31 March 2016, other than Sponsorship for Individuals, where applications are invited all year round.

Strategic Plan/Policy Implications

Community & Lifestyle

- Community environments that are socially cohesive and embrace diversity.
- Communities that take pride and aspire to a greater sense of community.
- Promotion of active and healthy communities.

Leading & Listening

- A responsive, accountable and sustainable organisation.

Budget/Financial Implications

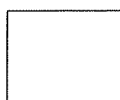
Council approved a Budget for Grants and Donations for 2015/16 of \$1,200,000. Following is a summary of the proposed grants, donations and sponsorship allocations.

Summary of Proposed Allocations

Committed/Contractual Donations	\$496,000
Specific Grant Programs	\$439,000
Donations	\$210,000
Sponsorship	\$55,000
Total	\$1,200,000
 Total Funds Available	 \$1,200,000
<u>Less Total of Proposed Allocations</u>	<u>\$1,200,000</u>
Balance	\$0

Legal Implications

Nil



Community Consultation

Council's grants are advertised widely in the local community through the City's website, local media, Cockburn Soundings, and Council networks. It is recommended that advertising start immediately following the Council decision to ensure a wider representation of applications.

Attachment(s)

1. Grants, Donations and Sponsorship Recommended Allocations Budget for 2015/16.
2. Cockburn Wetlands Precinct Reports from Cockburn Wetlands Education Centre and Native ARC.

Advice to Proponent(s)/Submissioners

Submissioners have been advised that a decision will be made at the Council Meeting on 13 August 2015 and they will be advised of the outcome following this meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil

10. MOTIONS OF WHICH PREVIOUS NOTICE HAS BEEN GIVEN

Nil

11. NOTICES OF MOTION GIVEN AT THE MEETING FOR CONSIDERATION AT NEXT MEETING

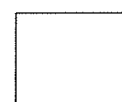
Nil

12. NEW BUSINESS OF AN URGENT NATURE INTRODUCED BY DECISION OF MEETING BY COUNCILLORS OR OFFICERS

Nil

13. MATTERS TO BE NOTED FOR INVESTIGATION, WITHOUT DEBATE

Nil



14. CONFIDENTIAL BUSINESS

Nil

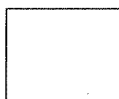
15. (GAD 29/07/2015) - CLOSURE OF MEETING

6:15pm

CONFIRMATION OF MINUTES

I, (Presiding Member) declare that these minutes have been confirmed as a true and accurate record of the meeting.

Signed: Date:/...../.....





ACN 009 116 796 ABN 11 009 116 796

9 Rivers Street, Bibra Lake WA 6163

T: (08) 9418 5088 F: (08) 9418 5979 E: admin@archeng.com.au W: www.archengineering.com.au

4 May 2015

Ms Lee Gatt
Property & Lands Officer
City of Cockburn
PO Box 1215
BIBRA LAKE WA 6965

RE: Lease – 11 Rivers Street, Bibra Lake

Dear Lee,

TO: - OF COCKBURN
DATE: 06 MAY 2015
SUBJECT: 641/008
RETENTION: 7.2 D21
PROPERTY: 4412345
APP: LSE09/0008
ACTION: LEE GATT

We are writing to you to request your consideration for a reduction in the monthly lease costs in relation to the land at 11 Rivers Street, Bibra Lake.

As you are aware, in recent months there has been a significant economic downturn in both mining and related industries. This has had a major impact on the cash flow of a great many businesses that derived the majority of their income from these industries – including Arch Engineering Pty Ltd. In fact in the 40 years that I have been involved with the Company I can honestly say that I have never seen it this bad. We have had to cut our costs across the board in order to just break-even, with the hope that we can “ride out the storm” and return to more profitable times.

It is worth noting that since we took on the lease, we have had, and continue to have, an extremely good track record in relation to meeting the monthly financial commitments of the lease. We have always co-operated with Council whenever issues arose. However, whilst we understand the Council's potential position on this matter, if the current economic climate continues then we may be forced to seek cheaper alternatives.

With this, and the current economic downturn in the mining industry in mind, we would appreciate it if you please consider reducing the monthly lease costs on 11 Rivers Street, Bibra Lake to be more in line with current market rates and to help us through this most challenging time for our business.

Yours Sincerely

Les Archibald
Managing Director

Attachment 1 – Copy of Email supporting the letter from Arch Engineering.

From: Steve Lawty
Sent: Thursday, 30 April 2015 9:48 AM
To: 'Brodie Keay'
Subject: RE: Hardstand rates

Yeah, the block is flat

Thanks for that

Steve Lawty
Financial Controller



T: (08) 9418 5088 | F: (08) 9418 5979 | M: 0498 999 898

E: steve.lawty@archeng.com.au | W: www.archengineering.com.au

From: Brodie Keay [<mailto:BKeay@burgessrawson.com.au>]
Sent: Thursday, 30 April 2015 9:43 AM
To: Steve Lawty
Subject: RE: Hardstand rates

If it's flat and level \$15-\$20sqm, prices have come back recently

Kind Regards,

Brodie Keay
Director

M 0403 552 809
D 08 9288 0214
E bkeay@burgessrawson.com.au
T 08 9288 0288 F 08 9481 5353

Assistant **Neya Perovec** | D 08 9288 0230 | E nperovec@burgessrawson.com.au



Burgess Rawson
Commercial Property Consultants

Level 7, 221 St Georges Terrace, Perth WA 6000

PO Box 7658 Cloisters Square, Perth WA 6850

MARKET APPRAISAL (SALES & LEASING)

A market appraisal of the property is undertaken by Burgess Rawson (WA) Pty Ltd and should only be taken as an estimate of the **rental / sale** price and not a valuation. It takes into consideration market conditions prevailing as at the date of this report which are subject to change.

We do not contemplate or accept responsibility for the whole or any part of the contents of this report to any other third party using the same or to whom the same is communicated without written consent from this Company.

This appraisal is valid for a period of thirty (30) days from the date of this report at which time a further assessment should be made.

From: Steve Lawty [<mailto:Steve.Lawty@archeng.com.au>]

Sent: Thursday, 30 April 2015 9:38 AM

To: Brodie Keay

Subject: RE: Hardstand rates

Block is 3,636 square metres

Cheers

Steve Lawty
Financial Controller



AUSTRALIAN STEEL INSTITUTE



From: Brodie Keay [<mailto:BKeay@burgessrawson.com.au>]

Sent: Thursday, 30 April 2015 9:26 AM

To: Steve Lawty

Subject: RE: Hardstand rates

Hi Steve

How big is the block?

Kind Regards,

Brodie Keay

Director

M 0403 552 809

D 08 9288 0214

E bkeay@burgessrawson.com.au

T 08 9288 0288 **F** 08 9481 5353

Assistant **Neya Perovec** | **D** 08 9288 0230 | **E** nperovec@burgessrawson.com.au



Burgess Rawson

Commercial Property Consultants

Level 7, 221 St Georges Terrace, Perth WA 6000

PO Box 7658 Cloisters Square, Perth WA 6850

From: Steve Lawty [<mailto:Steve.Lawty@archeng.com.au>]

Sent: Thursday, 30 April 2015 9:21 AM

To: Brodie Keay

Subject: Hardstand rates

Hi Brodie,

Just a quick question – are you able to give me an idea of what current hardstand rates are around Bibra Lake. We currently lease a block from the council – not concrete – it is just limestone – we use it for storage and car parking etc.

But the lease is coming up for renewal and so we were just wondering what the dollar rate per square metre was currently around Bibra lake.

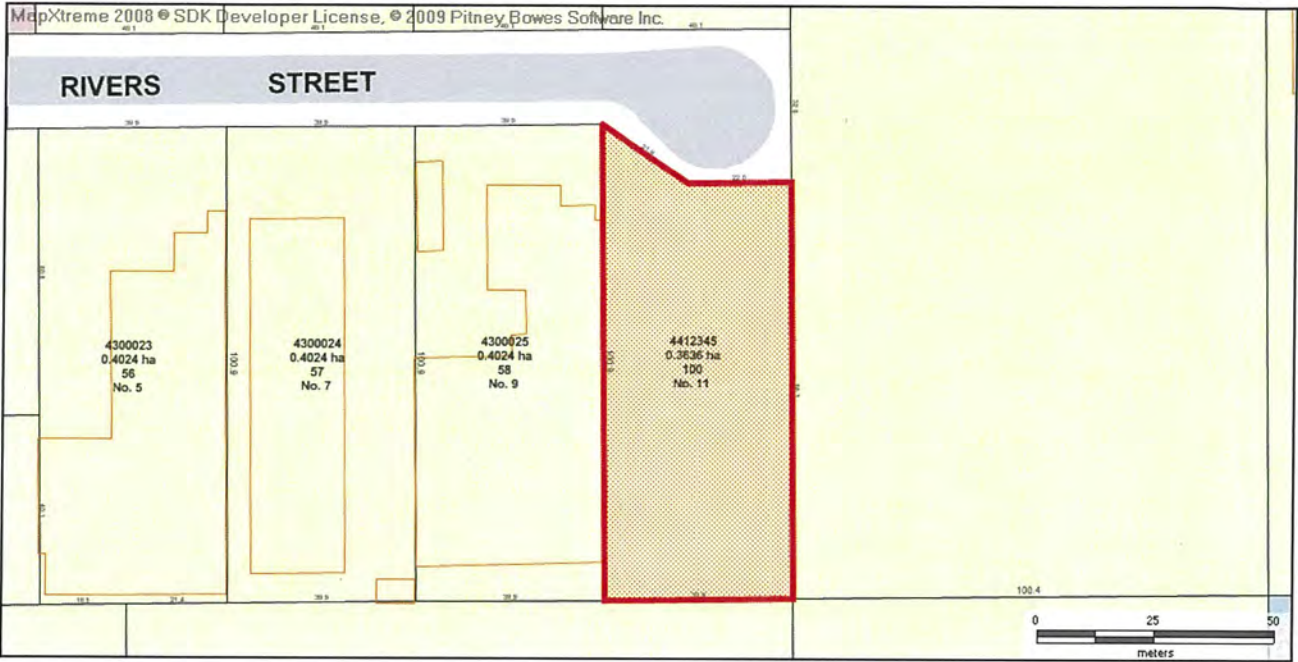
Regards

Steve Lawty
Financial Controller



T: (08) 9418 5088 | F: (08) 9418 5979 | M: 0498 999 898

E: steve.lawty@archeng.com.au | W: www.archengineering.com.au



Direct Comparison Approach

In utilising the direct comparison approach, we have analysed directly comparable ground rents on a rate per square metre basis.

Our adopted ground rent has taken into consideration the subject lot's size, zoning, location and market factors.

In investigating directly comparable evidence, we have looked at both ground rents and current asking rents located within general industrial locations throughout the Perth metropolitan area.

The ground rental evidence and asking rents we have relied upon is set out as follows:

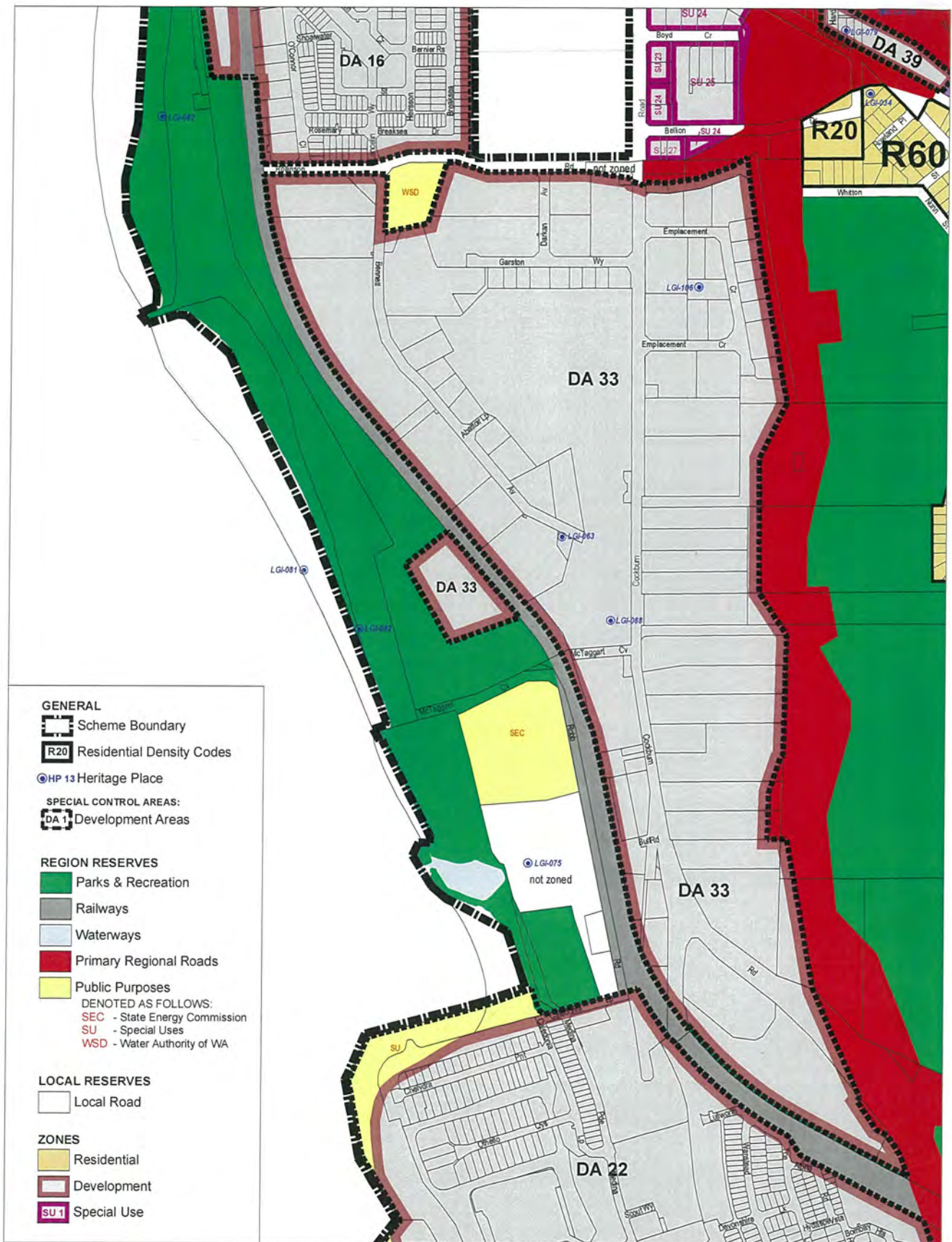
Address:	45 Wellard Street, Bibra Lake
Asking Rent:	\$220,000 net per annum in July 2015
Asking Rental Rate:	\$22.00/m² net per annum exclusive of GST
Area:	1.0 hectare
Comments:	The property comprises a hardstand site with an adjoining bitumen paved car parking area located within the heart of the Bibra Lake Industrial Area. The hardstand area is not sealed but appears to be limestone based. The hardstand area enjoys two street frontage.
Comparability:	<i>A larger hardstand area located in close proximity to the subject. Based on comparable ground rents, we would argue that the current asking rent is above market parameters. We believe the subject would attract a lower ground rental rate.</i>
Address:	11 (Lot 100) Rivers Street, Bibra Lake
Tenant:	Arch Engineering
Area:	3,636m ²
Annual Rent:	\$69,000 net per annum
Rental Rate:	\$19.00/m² net per annum exclusive of GST
Date Set:	June 2015 (Market Rent Review)
Lease Terms:	Not advised.
Comments:	The property comprises a ground lease located in the heart of the Bibra Lake Industrial Area. The ground rent was negotiated between the City of Cockburn and the sitting tenant. The property is considered to be generally level to its street frontage and is not connected to deep sewerage.
Comparability:	<i>The subject property.</i>
Address:	Lot 33 Clarence Beach Road, Henderson
Tenant:	SBF Marine
Area:	1.4636 ha
Annual Rent:	\$265,714.46 net per annum
Rental Rate:	\$18.15/m² net per annum exclusive of GST
Date Set:	May 2015 following CPI review.
Lease Terms:	The land was originally leased at a commencing rent of \$140,000 with annual increments of \$160,000, \$180,000, \$195,000 and \$210,000, with a market review applying in May 2011. Following the market rent review where the rent was increased to \$243,400 per annum, the rent will be annually reviewed to CPI. The current rent was set via CPI review on 1 May 2015. Next market review due 1 May 2016.
Comments:	Oceanfront site improved with administration offices and a metaldeck clad workshop however, the lease is for the land component only.
Comparability:	<i>A larger ground lease situated in a more strategic waterfront location. The subject would attract a slightly higher ground rental rate.</i>

Address: Lot N59B cnr Rudderham Drive/Birksgate Road, North Fremantle
Area: 1.2015 ha
Rental Rate: \$20.15/m² per annum
Date Fixed: 1 June 2012
Method: Market
Review Period: Every 3 years to Market
Comments: Ground lease area at Rous Head having proximity to berths and water but no direct water frontage.
Comparability: A larger property situated in a superior location. The subject would attract a lower ground rental rate per square metre.

Address: Jandakot Airport Ground Leases
Comments: We were verbally advised by Leo Seaward of Ascot Capital in late 2014 that ground leases which adjoin the Jandakot Airport runway generally attract ground rents of approximately \$22.00/m² for sites ranging in land area between 3,000m² to 6,000m².
Comparability: We would argue the subject property occupies a slightly inferior less strategic location. The subject would attract a lower ground rental rate.

Based on the above comments and the identified ground rental evidence, we have adopted the following ground rental rate:

- Lot 100 3,636m² @ \$19.00/m² = \$69,084
Adopt **\$69,000** net per annum exclusive of GST





City of
Cockburn

wetlands to waves



GNA 11/2014

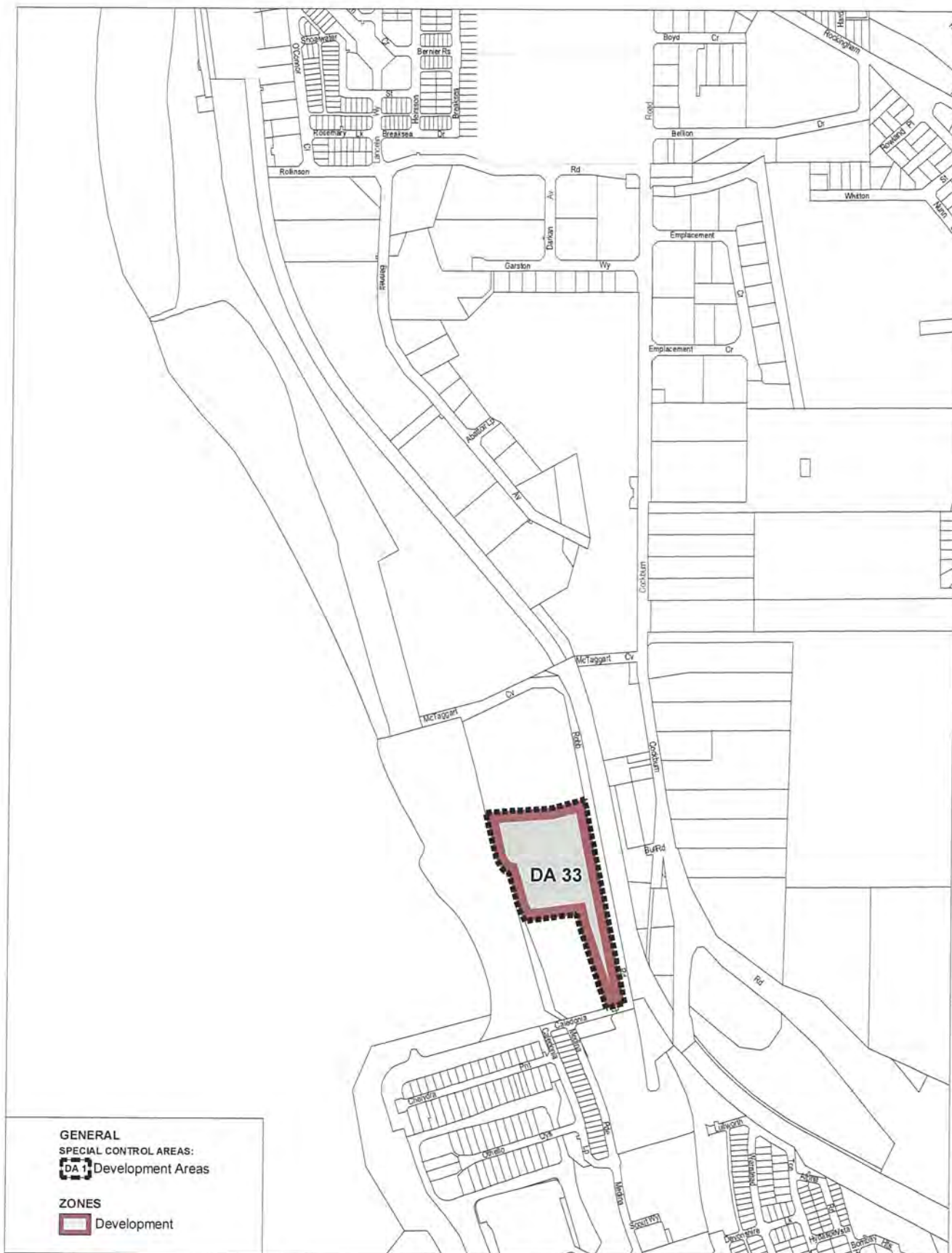
0 200m

Scale

EXISTING ZONING

TOWN PLANNING SCHEME No.3

AMENDMENT No.108



GNA 112014

A horizontal line segment with a tick mark at the left end labeled '0' and a tick mark at the right end labeled '200m'. Below the line segment is the word 'Scale'.

**PROPOSED ZONING
TOWN PLANNING SCHEME No.3
AMENDMENT No.108**



FIGURE 1

MWURBAN
planning and development

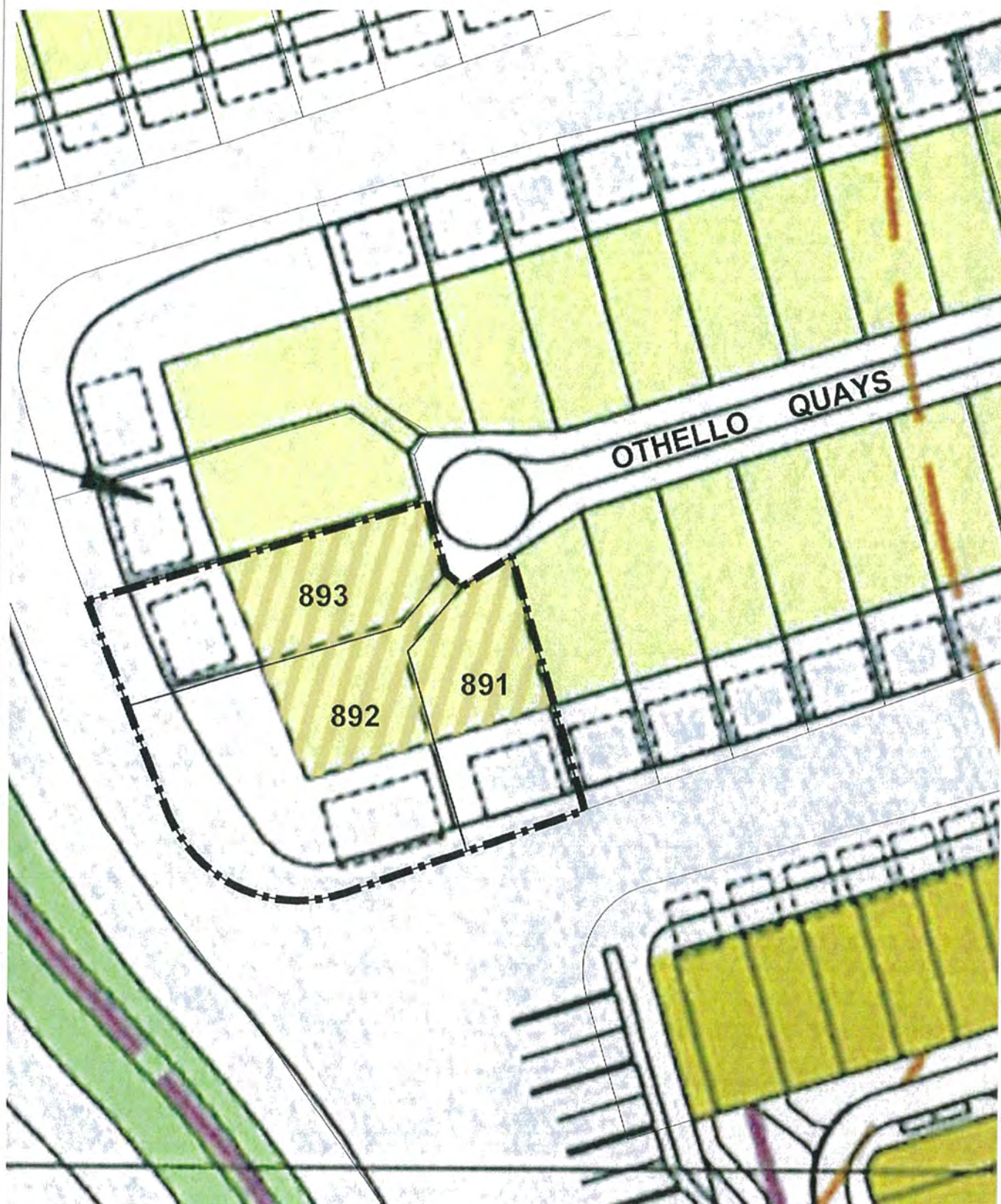
LOCATION PLAN

LOTS 891, 892, 893 Othello Quays, North Coogee
City of Cockburn



PLAN: ROT-1 002A
DATE: 150323
PROJECT: OTHELLO QY
DESIGNED: TW

SCALE: 1:3000 @A4





SOURCE: CITY OF COCKBURN INTRAMAPS/NEARMAPS

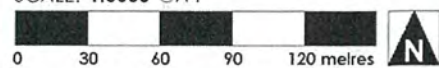
-  Structure Plan Boundary
-  Residential R25/R40

STRUCTURE PLAN MAP

LOTS 891, 892, 893 Othello Quays, North Coogee
City of Cockburn

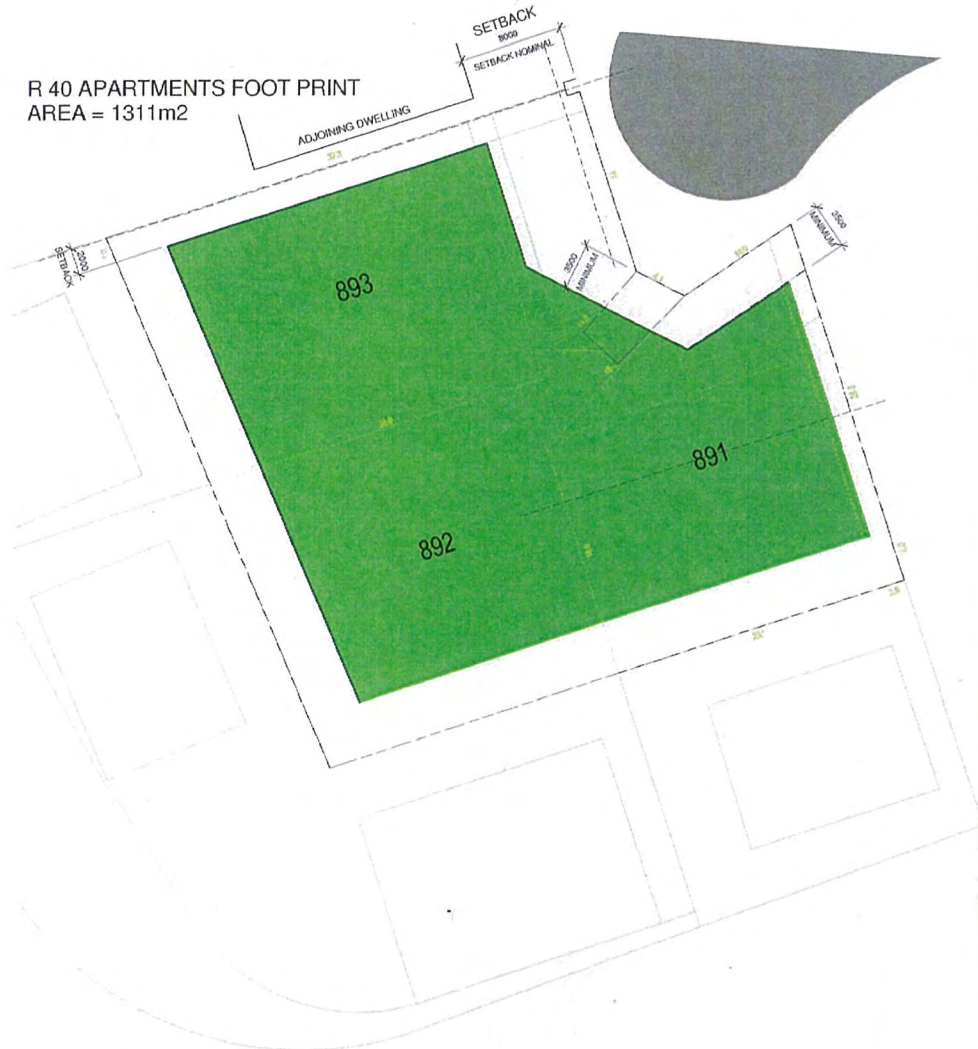
PLAN: ROT-1 007A
DATE: 150418
PROJECT: OTHELLO QY
DESIGNED: TW

SCALE: 1:3000 @A4



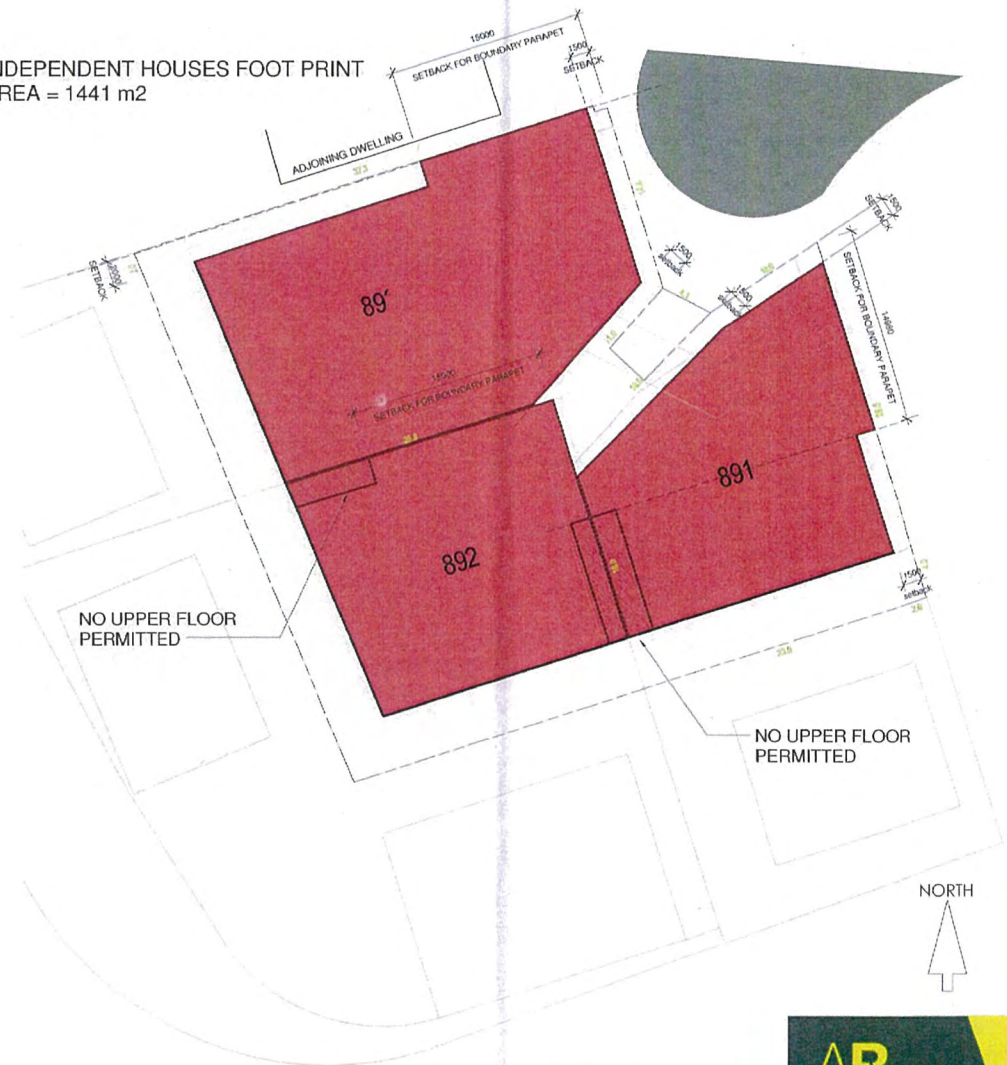
MWURBAN
planning and development

R 40 APARTMENTS FOOT PRINT
AREA = 1311m²



APARTMENT FOOT PRINT PLAN
SCALE = 1:100 @ A3 SHEET SIZE

INDEPENDENT HOUSES FOOT PRINT
AREA = 1441 m²



INDEPENDENT HOUSES FOOT PRINT PLAN
SCALE = 1:100 @ A3 SHEET SIZE

STRUCTURE PLAN - MASSING MODEL

LOT 891, 892 & 893 OTHELLO QUAYS



Level 1/9 Adelaide Street, Fremantle, Western Australia 6160 // PO Box 288, Fremantle, WA 6959
T: +61 8 9433 2221 // E: admin@artemarchitecture.com.au // W: www.artemarchitecture.com.au
Artem Architecture Pty Ltd ABN 35 601 092 817



DETAILED AREA PLAN

Provisions of the City of Cockburn's Town Planning Scheme No.3 and the Residential Design Codes (R-Codes) apply to any multiple dwelling development on the land with the exception of the design guidelines and/or R-Code variations detailed below.

This DAP applies to development taking up the R40 density option, requiring the amalgamation of Lots 891-893. The design and development of single dwellings on individual lots is to be in accordance with the 'Revised Detailed Area Plan - Stage 4C "Seaspray"'.

BUILDING SIZE

Any development is to comply with the plot ratio requirement as it applies to the land (Table 4 of the R-Codes) taking into account maximum building height, setback and finished lot level considerations.

BUILDING HEIGHT

- A maximum building height of two (2) storeys with a top of roof height 8.75m above the finished lot level is permitted.
- A non-habitable level partially or entirely below the finished lot level is not deemed a storey for the purpose of calculating building height.
- Service elements such as stairs and lift over-runs shall be integrated into the roof design where they extend beyond 8.75m.

STREET SETBACK

- Othello Quays (Lot 893). Development is to be setback within 1.0m of the garage setback of the adjoining dwelling on Lot 894 at the western end of Othello Quays.
- Othello Quays (Lot 891). Development is to be setback a minimum 3.5m.
- Balconies are permitted to extend into the above setback areas a maximum of 2.5m.

LOT BOUNDARY SETBACKS

- To Lot 890 and 894. Development is to be setback a minimum 2.0m.
- Rear boundaries. Development is to be setback a minimum 4.0m from the dry lot boundary.

OPEN SPACE

- Development is to comply with a minimum open space requirement of 30% (over the dry lot area).
- Landscaping shall be provided in the front setback area, appropriate in scale to the builtform.

STREETSCAPE AND WATERWAY ADDRESS

- Development is to address Othello Quays with major openings to habitable rooms (living and/or bedrooms).
- Development is to address the waterway with facades that are generally parallel to the dry lot boundary, containing major openings to habitable rooms (living and/or bedrooms) and balconies.

FENCING

- Fencing within the rear setback shall be a maximum 1.2m high above the rear lot level of RL 1.25m AHD and visually permeable.
- Fencing on the side boundaries i.e. within the minimum 4.0m setback shall be a maximum 1.2m high above the rear lot level of RL 1.25m AHD.

BUILDING APPEARANCE

Development is to have due regard to the intent and requirements of the Port Coogee Design Guidelines.

OUTDOOR LIVING AREAS

- Each dwelling is to be provided with at least one (1) balcony addressing the street and/or the waterway depending on where it is positioned on-site.
- Balconies compliant with the requirements of the R-Codes will be included in the calculation of Open Space.

PARKING

A maximum two (2) bays per dwelling and 0.5 visitor bays per dwelling are permitted as part of any development.

VEHICULAR ACCESS

- Vehicle access is limited to one (1) crossover.
- The crossover is to be no greater than 6.0m in width.

SITE WORKS

- Filling shall be no greater than 0.5m above the finished lot level (maximum ground finished floor level 3.8m AHD).
- All habitable rooms to have a minimum floor level of 2.5m AHD.
- Construction beneath 3.3m AHD may require a private sewerage pump station (gravity sewer connection designed to function from a height of 3.3m AHD).
- The finished lot level of 1.25m AHD within the 4.0m rear setback is to be maintained with the exception of a reduction by 0.5m for the specific purpose of access steps. No excavation for this purpose within the rear setback area shall be below 0.75m AHD (to protect the geogrid soil reinforcement).
- All works within the 4.0m rear setback area combined with an allowance for pedestrian loading shall not exceed a total loading of 5kPa (0.5 Tonne/m²). Engineering certification to enable the determination of a final loading is to take into account any proposed light weight structure, landscaping and pedestrian loading.

VISUAL PRIVACY

The provisions of 6.4.1 of the R-Codes dealing with visual privacy applies to major openings facing side boundaries outside the front and rear setbacks.

OUTBUILDINGS

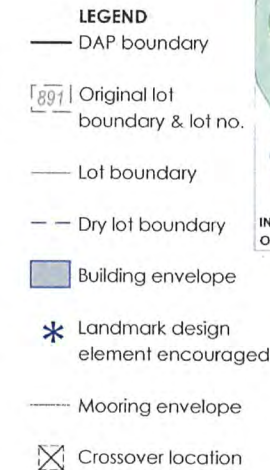
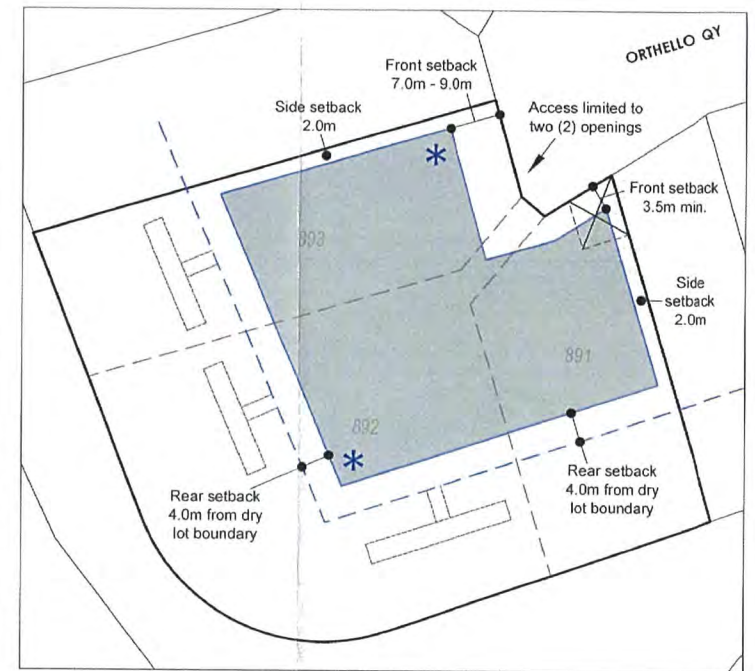
- No outbuildings (significant structure/s) are permitted within the 4.0m rear setback area.
- Light weight shade structures are permitted within the 4.0m rear setback area (up to 3.0m in height above the finished lot level of 1.25m AHD). The design, finishes and colour/s of any light weight shade structure is to appear integral to, and complement the design of the remainder of the development.
- A light weight shade structure is a pergola, gazebo or similar.

UTILITIES AND FACILITIES

- All lockable storage areas are to be contained within the development, not visible from the street or waterway.
- Waste storage and management is to accord with an approved Waste Management Plan prepared in accordance with the requirements of the City's Policy 'Waste Management in Multiple Unit Developments'.
- Clothes drying areas are to be screened from view from the street and waterway.

JETTY AND MOORING ENVELOPE

Refer Jetty and Mooring Design Guidelines adopted by the City of Cockburn.



APPROVED

This Detailed Area Plan has been adopted by Council and signed by the Principal Planner.

Principal Planner: _____

Date: _____

DRAFT

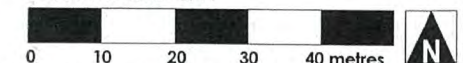
DETAILED AREA PLAN

LOTS 891, 892, 893 Othello Quays, North Coogee
City of Cockburn

MWURBAN
planning and development

PLAN: ROT-1 001B
DATE: 150331
PROJECT: DAP
DESIGNED: TW

SCALE: 1:1000 @A4



DISCLAIMER: ISSUED FOR ILLUSTRATIVE PURPOSES ONLY. ALL AREAS AND DIMENSIONS ARE SUBJECT TO DETAIL DESIGN AND SURVEY.

SCHEDULE OF SUBMISSIONS
PROPOSED STRUCTURE PLAN – LOTS 891-893 OTHELLO QUAYS, NORTH COOGEE

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
1	Leon Verity Hodges 28 Othello Quays North Coogee WA 6163	<p>Objection</p> <p>We live in the only currently constructed home at the end of Othello Quays on what was sold to me as one of four Iconic Entry Statement Blocks in the Development.</p> <p>I am fed up with developers seeking to change the codes to allow for smaller lots such as the application before council currently to further subdivide lots on the south side of Othello Quays, and now this application to potentially build 12 apartments next door, quite frankly this is unacceptable.</p> <p>We purchased this lot to build a dream home by the sea, the addition of apartments will detract from the original town planning approvals, and the inflexible constraints that were imposed upon us during the panning and approvals process to build there were onerous, we wanted to build a much smaller home on this lot for the two of us with the kids having all left home, but this was not allowed then by council, so as a result we had to build that huge house to meet council requirements. This application cannot be granted, it is a disgrace to see the council being bullied by developers or driven by greed to increase rate returns, if this is the case. To add further insult to injury council mounted what felt like a targeted raid on my house in the guise of a compliance check to view the house, and in conclusion eluded to me that there was a suspicion we were running some sort of illegal fishing operation from my home, this left a very sour taste in my mouth I might add, it was unnecessary and very undignified. I am to this day still unsure of what the Councils intent was on that day.</p> <p>The addition of apartments will add a further 24 cars to the street along with parking for visitors, add this to a potential additional 12 cars if the other blocks on Othello Quays are split and we have a massive on going vehicle issue. This development was not approved on the basis of smaller lots and high density living, we purchased here because the land lots and water frontage were a good size not crammed in or built one on top of the other. These two proposed amendments are not in the spirit of the development or in its original concept</p>	<p>Objection Noted.</p> <p>Noted.</p> <p>The City controls residential development within the Port Coogee area through a number of mechanisms including: The endorsed Structure Plan, State Planning Policy 3.1: The Residential Design Codes and various Detailed Area Plans.</p> <p>In general these set minimum built form expectations. The Detailed Area Plan sets in place specific development controls for the Othello Quays area. These primarily go to maximum controls. The City would be able to consider any proposal for development of a residential development within Othello Quays of a smaller scale through a Development Application. Such proposals would then be assessed against performance criteria found in the Residential Design Codes.</p> <p>Minimum building controls are often imposed on purchasers as conditions of sale. This occurs within the Port Coogee development area.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>intent and marketing. It is a disgrace to even consider any further lot size changes R code changes or reconfiguration of the Othello Quays current approved land use and size. Further reduction in lot sizes and addition of multi residential property will devalue my property and my ability to ever sell it if we are crammed into a high residential island, add to this the street traffic and congestion and Cockburn Council will have effectively a slum on its hand, this is not what I have spent 7.5 Million dollars investing into. David Sumich had expressed his desire to sell his land (my southern boundary) because he cannot afford to build on it, his options were to split it sell it or find an alternative, this appears to be the alternative, He is aware he will not get what he originally paid for the land in today's market. Joe Rottendalla seems to be keen to redevelop his two lots adjoining and now including the Sumich block in order to also sell the land and make high returns, both are driven by the need to sell and not what is considered to be in the best interest of the development, the council or the existing residents who have played by the rules and brought into the dream.</p> <p>There is no possible reason for the council to approve a further subdivision or R code change, this island was approved, and constructed under an approved plan and should remain as it was intended, with the current lot sizes and the current approvals process required to construct homes on the island, after all it is the Jewell in the crown of the Cockburn council land portfolio.</p> <p>We strongly oppose and such submissions and changes to Othello Quays its land sizes and zoning and I am currently engaging legal council to deal with both of these issues The further subdivision of land on the south side of Othello Quays The application to vary the structure plan for lots 891-m 893 Leon and Verity Hodges 28 Othello Quays North Coogee</p>	<p>With regard to car parking and access. The applicant has outlined by way of a traffic impact assumptions the likely impact of the proposed zoning change. The impact assessment was carried out in line with standard professional expectations. Assumptions made around the expected trip generation has found that with the additional 9 dwellings traffic volumes on Othello Quays would only be approximately ¼ of the maximum allowable traffic on such a road.</p> <p>The applicant has noted in the draft Detailed Area Plan that they intend to provide visitor parking at a rate of 0.5 per dwelling. This being twice the requirement of the Residential Design Codes. The officers' recommendation proposes to mandate this through the Part 1 of the Structure Plan to provide certainty.</p> <p>Private financial matters are not planning considerations and can form no part in the determination of such proposals.</p>
2	<p>Coogee Beach Progress Association PO Box 422 SOUTH FREMANTLE WA 6162</p>	<p>Objection</p> <p>The Coogee Beach Progress Association at its 11 June 2015 General Meeting has resolved to:</p> <p>1) Object to the "Revised Detailed Area Plan - Stage 4C "Seaspray", Port Coogee", 2) Object to the Proposed Structure Plan Variation for Lots 891-893 Othello Quays, Port Coogee.</p> <p>The Coogee Beach Progress Association is appalled that these development</p>	<p>Objection Noted</p> <p>The proposal is for a rezoning via a</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>proposals are planned in direct contradiction of the approved Development and Structure Plans, as well as being inconsistent with the original public intent of these Port Coogee "Island" developments, and with total disregard for the ability of the existing Island roads and infrastructure, including services, to support such increased densities, with public parking areas already being totally inadequate for Port Coogee.</p> <p>The proposed developments do not appear to meet Council requirements and guidelines for Port Coogee, are inconsistent with the covenants applied to all existing land titles at Port Coogee, and negatively impact the aesthetics and community amenity of the original design, to make way for high residential style views and congestion in a high profile area which is not suited in any way for this type of proposal.</p> <p>These developments are not in the interests of Port Coogee residents, or the broader community who would look to participate in current or planned facilities at Port Coogee.</p> <p>The Association would also note that the planned Retail and Commercial Centre of Port Coogee and associated public amenities, promoted by the Developer with much fanfare and community support, have not as yet been significantly provided. This delayed provision of these facilities is in direct contradiction of the advice provided to the Developer by the "Port Coogee People and Places Working Group", the Community consultation group initiated by the Developers, in which this Association participated, and the Developers own Consultants "Creating Communities".</p> <p>If the Developer was to proceed with the early provision of the long awaited Port Coogee Retail and Commercial Centre, it is highly likely that these proposals, at Item 1 & 2 above, would not be required to assist in the sale of the "Seaspray" and "Othello Quays" properties.</p>	<p>structure plan modification to alter the zoning from Residential R25 to Residential R25/40. As such the proposal would contravene the existing zoning of the land. However all landowners reserve the right to request a structure plan modification and the City assesses all proposals put to it. Should the Council support the proposed changes the new zoning would become the prevailing guidelines for that portion of Port Coogee.</p> <p>The City is not party to any covenants relating to development standards on Othello Quays and as such is not bound to consider these in making a determination of the Structure Plan.</p> <p>The Structure Plan outlines and addresses in detail matters relating to traffic, infrastructure, servicing and built form aesthetics.</p> <p>See response to submission 1 for comments on visitor parking.</p> <p>The built form controls proposed to be included for the proposed R40 development see's further restrictions placed on such proposals. Building height, side setbacks and front setbacks have been proposed to be increased beyond the current Detailed Area Plan to reduce building bulks, maintain important sight lines and to integrating the development into the existing urban fabric. Building mass models have been included in the Council report for reference.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
			The City has received and approved various commercial developments within the Marina Village area of Port Coogee. Timing of the construction of these proposals is a matter for the respective applicants.
3	Robin & David Gray 14 Othello Quays North Coogee WA 6163	<p>Objection</p> <p>The Proposed subdivision of the lots 891-893 is considered by the residents and land owners on Othello Quays and Cheldrya Point as un-necessary and not in keeping with the ideal sold to each and every one of us at the sale point.</p> <p>These two iconic islands were to be the centre piece of this development with generous block sizes, good parking access and direct access to the water front and your own Private jetty.</p> <p>The proposed structure plan variation of these lots will destroy that ideal and the street view and create major traffic issues with just this development it will increase the traffic by a minimum of 24 extra cars introduced to the street, parking and congestion issues and foot path access as it currently stands.</p> <p>The island westward of the Dome is rated R60 which would suit this type of proposed development proposal and this is at a stage of nothing being prepared by Australand.</p> <p>All of us have paid very high prices to buy a piece of this dream, there were covenants in place stipulated by council governing land use, boundary setbacks and house sizes, all of which appear to be disregarded in an eff approved. In essence I could have built a smaller house rather than going to the expense I did to meet these covenants which were mandated by Council.</p> <p>The main reason behind this development amendment we feel is due to pricing of the land without doubt.</p> <p>These lots came to market during a real estate boom, since then pricing has fallen as potential buyers and owners see their cash reserves diminish as a result of the GFC followed by the mining down turn.</p> <p>This proposal is not what we, as owners purchased when we individually brought a vacant lot on these islands.</p>	<p>Objection Noted.</p> <p>Noted.</p> <p>Comments relating to parking and access see Submission 1</p> <p>Comments relating to restricted covenants see Submission 2</p> <p>The City does not mandate housing size minimums. For comments relating to these matters see Submission 1.</p> <p>Comments Noted</p> <p>Comments Noted</p> <p>Comments Noted.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>Precedence</p> <p>If this is allowed it will set precedence for anyone within the Port Coogee Estate to join in with a neighbour and put a proposal to council and change the R coding to sell of the lad and put apartments in.</p> <p>If you consider this application and add to it the previous on lots 884 to lot 888 and the following on the North side of Othello Lots 903 to lot 90S we can potentially have up to and above a potential 34 additional cars introduced to this Othello Quay street.</p> <p>Issues not addressed in the proposal.</p> <ul style="list-style-type: none"> • When we purchased these lots, it was indicated on the NBN map, that these lots would all be wired ready for the NBN. Upon moving into our properties it was discovered that Australand refused to wire the islands to meet the criteria for NBN stating that the development was completed pre 2010 and that there was no obligation on them to provide this service. So will Australand be requested to retrospectively wire NBN to the street and all lots if the application is successful or will they avoid this and make it apply to only their proposed new sub divided lots. • A more detailed plan showing water usage lot dimensions and how they plan to deal with public footpath access and street parking. • Any proposed additional jetties • Environmental impact statement • NBN provisions • Provide Street scape artists impression showing how the development will look once completed • Traffic management plan • Waste management plan • Hydraulic plan and power demand plan needs to be reviewed to ensure the infrastructure will meet the proposed new demand <p>Concerns</p> <p>What is even more concerning is that this approval, should it be successful, then precedence will be set, and before you know it, the remaining three lots at the west end of the Island will be divided into smaller lots as well, further</p>	<p>All landowners reserve the right to request via the Council for a minor modification to a Structure Plan to occur. The proposal is site specific. Any further proposals for changes to residential density in Port Coogee, or any other part of Cockburn, as individually judged on their merits.</p> <p>Comments relating to parking and access see Submission 1</p> <p>Point 1: Noted. This matter is not relevant to the proposal to vary a structure plan.</p> <p>Point 2: The proposed Council resolution includes a requirement to have additional off street visitor parking on this site. This being twice the rate required under the Residential Design Codes. Moreover, the proposal is likely to have less points of egress (crossovers) onto Othello Quays that the current three residential lots, therefore pedestrian access and on street parking will be more manageable than the current outcome.</p> <p>Point 3: Noted – These would be subject to relevant approvals and also the City's Local Planning Policy relating to private jetties.</p> <p>Point 4: Not supported – proposals such as this are not traditionally subject to an Environmental Impact Assessment.</p> <p>Point 5: Noted above</p>

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		<p>congesting the street, changing the intent and devaluing our homes.</p> <p>This will make it difficult to ever recover the costs we have born in the construction of these homes. When we purchased our lot, it was deemed an "iconic entry statement block" we were made to comply with the building codes and there was no allowance for change considered. So effectively we had to build what the DAP suggested no variation. The house had to be a certain size with the prescribed setbacks etc.</p> <p>It is impossible to understand how this can be considered good for the area, the first totally residential water way project in WA in 40 years will end up being like the Mandurah canals, I would have thought lessons would have been learnt from this.</p> <p>The original plans and development are what we all purchased our blocks from, and to have this change take effect now is not in the spirit or the intent of the development.</p> <p>We are going to be forced to consider our legal options in this matter should this approval, be granted.</p> <p>There is absolutely no reason other than greed for this to be approved. The Island behind the Dome coffee shop is the Island set aside for high density residential development, zoned R60, there is no need to create this atmosphere on Othello Quays Seaspray Island when it was never originally approved for this.</p> <p>It is unfair to change the rule half way through a development such as this and expect it to be received well by those of us whom have taken the steps to build and live here. People here have invested anywhere between 1.3 and 3.7 million dollars to own this dream and have by virtue of acquisition entrusted the local government to protect that investment and to administer the development as it was originally presented and sold to us.</p> <p>As a business man we all roll the dice on the ventures we get involved in and economic downturns are not a licence to change the rules.</p> <p>This objection is the view of the attached current land owners and residents of the area, please consider our concerns carefully in relation to this matter as it is important to us that the life style that was sold to us and which we have brought</p>	<p>Point 6: The applicant has provided a number of built form massing models and also a proposed Detailed Area Plan for the site. Beyond specific built form features a sound understanding of the mass and nature of any R40 proposal can be ascertained by these items.</p> <p>Point 7: Traffic assumptions have been included by the applicant utilising the Port Coogee Structure Plan modelling and also standard traffic engineering standards. This shows that Othello Quays will continue to function well within the maximum expected traffic volumes for such a street.</p> <p>Point 8: A waste management plan will be required to accompany any proposal for development in accordance with the City's relevant Local Planning Policy. The City's Waste Manager has viewed the proposal and is satisfied with such an approach.</p> <p>Point 9: Noted</p> <p>Concerns: Matter to do with precedence is outlined above.</p> <p>Private financial matters are not relevant planning considerations.</p> <p>The Council report outlines the planning rationale and provides officers comment.</p> <p>The planning system by its very nature is not design to protect one person's</p>

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		into and heavily invested in is kept whole.	investment from another. It is private finance blind to ensure probity in decision making.

Detailed Area Plan R-Code Variation

The District Town Planning Scheme and R-Codes are varied as described below.

R-CODING

Density Coding is R25

SCHEME AND RESIDENTIAL DESIGN CODE VARIATIONS

The requirements to consult with adjoining or other land owners to achieve a variation to the R Codes is not required where the design complies with the following standards.

DESIGN ELEMENTS

- All dwellings must be located within the building envelope depicted on the DAP.
- Only non-habitable lightweight shed/cover and gazebos structures are permitted within the Rear Setback (refer Dwelling Setbacks section to determine required setbacks). Structures shall not exceed a maximum height of 3m above finished lot level. Building materials form and colour palette shall be complementary to the design of the dwelling.
- No sheds or storage structures are permitted in the rear setback. All storage areas shall be within the designated building envelope and incorporated into the main dwelling.
- Dwellings shall address the Primary Street (Road 35) and the waterway by way of design, fenestration, entry and must contain major opening(s) to a living area and/or bedroom.
- All dwellings overlooking POS shall be suitably designed and orientated to ensure passive surveillance (namely Lots 24 and 27). Dwellings shall have one or more major opening(s) to a habitable room (being a lounge or living room and not to include bedrooms) and an outdoor living area facing the Public Open Space.
- Any exposed parapet wall on a common boundary shall be suitably finished to match the external walls of the dwelling, unless otherwise agreed with the adjoining property owner.
- At least one balcony greater than 10 m² is to be provided to the Primary Street elevation of each dwelling.
- Balconies with an area of 10 m² or greater may be included as part of the minimum open space requirement equal to the area of that balcony.
- A maximum area cover measuring 70% of the dry lot area is permitted.
- Part 5.4.1 of the R Codes applies to major openings facing side boundaries outside the rear setback area only. Rear setback areas (abutting the waterway) are exempted from the requirements of Part 5.4.1 of the R-Codes, which relates to visual privacy.

DWELLING SETBACKS

- Setbacks for development shall be in accordance with the following:
 - (Except where specified below all other setbacks shall be in accordance with the R-Codes)
 - The dwelling setback from the Primary Street shall be a minimum of 1.5m and maximum of 3m.
 - A balcony (complying with the specified Design Elements criteria) is permitted to project over the dwelling/garage to within 0.5m of the front lot boundary.
 - The minimum rear dwelling setback shall be 4m from the dry lot boundary (as defined on the DAP). Any lightweight structure (as described under Design Elements) shall be setback a minimum of 2m from the rear dry lot boundary. For lots 900, 901 & 903 the dry lot boundary is the cadastral lot boundary.
 - On lots 24, 27, 902 and 905 the minimum side dwelling setback to the waterway shall be 4m from the dry lot boundary (as defined on the DAP). Any lightweight structure (as described under Design Elements) shall be setback a minimum of 2m from the dry lot boundary.
- North - South generally orientated lots (Lots 887-894, 24-27, 882-881 & 886-885)
 - Dwelling upper levels shall be setback 2.0m from the western lot boundary beyond 15m of the front boundary (where indicated on the DAP - this excludes Lots 908-904).
 - For Lots 25 and 26 only, dwelling upper levels shall be setback 2.0m from one side boundary only (where indicated on the DAP) for a length of 15m from the front boundary less the required setback.
 - For Lots 907-904 only, dwelling upper levels shall be setback 1.5m from one side boundary only (where indicated on the DAP) for a length of 15m from the front boundary less the required setback.
 - A nil setback is permitted for walls on the western lot side boundary (where indicated on the DAP) for a maximum length determined by the required front and rear setbacks.
 - A nil setback is permitted for walls on the eastern lot side boundary (where indicated on the DAP) for a length of 15m from the front boundary less the required front setback.
 - For Lots 907-903 only, a nil setback is permitted for walls on the eastern lot side boundary (where indicated on the DAP) for a maximum length determined by the required front and rear setbacks. Note Nil setbacks are permitted to upper levels beyond 15m of the front boundary on both side boundaries.
- East - West generally orientated lots (Lots 882-883)
 - Dwelling upper levels shall be setback 2.0m from the southern lot boundary beyond 15m of the front boundary (where indicated on the DAP).
 - A nil setback is permitted for walls on the southern lot side boundary (where indicated on the DAP) for a maximum length determined by the required front and rear setbacks.
 - A nil setback is permitted for walls on the northern lot side boundary (where indicated on the DAP) for a length of 15m from the front boundary less the required front setback.
- A nil setback will be permitted for one side boundary only, this may be nominated from the options displayed on the DAP, and described below. No combination of nil setbacks will be permitted except for lots 907-904, 25-26, 892 and 895 (as defined on the DAP).

DWELLING HEIGHT

- A maximum building height of 2 storeys (+1m) with top of roof pitch at 10m above the finished lot level is permitted. Roof terraces and all lightweight structures (highest point) are excluded from the height calculation.
- Non-habitable levels, partially or wholly below finished lot level shall not be deemed storeys for the purpose of the height calculation.

GARAGES AND VEHICULAR ACCESS

- Designated garage locations on the DAP are mandatory.
- The garage shall not dominate the front elevation of the dwelling. To comply with this requirement, the garage shall be setback a minimum of 0.5m behind the proposed dwelling setback. Consideration may be given to other means by which the requirement for garages not to dominate the front elevation can be achieved.
- Garages for lots 893 and 894 shall be located and integrated into the dwelling design to minimise dominance of their appearance at the end of Othello Quay.
- Designated garage location for lot 893 may be permitted to relocate along the south-western / southern lot boundary provided it remains a minimum 0.5m behind the proposed dwelling setback.
- Designated garage location for lot 894 may be permitted to relocate along the north-western / northern lot boundary provided it remains a minimum 0.5m behind the proposed dwelling setback.
- For lots 907-904, a garage setback of 4.5m from the Primary Street is required to provide off street visitor parking. For lots 25 & 26, a garage setback of 4.5m from the Primary Street is encouraged to provide off street visitor parking.
- Development above garages may overhang the garage front setback requirements and extend out to the dwelling setback.
- Garage openings shall be limited to total maximum width of 6m as total. For lots 907-904 the maximum garage door width shall be 5.5m.
- Carports are not permitted.
- For lots 907-904, the total driveway crossover width will be determined by the location of the existing verge tree and planter, which must be retained. All existing street verge trees are to be retained.
- For lots 907-904, the upper level shall overhang at least 50% of the garage width by a minimum of 0.5m.
- For lots 907-904, garage doors shall either be translucent or of a similar colour to the dwelling.

FENCING

- Fence height to the dry lot boundary (as defined on the DAP) shall be a maximum of 1.2m above the rear side setback lot level (RL+1.25m). The fence must be visually permeable and incorporate elements such as glass panels, screens, shutters or louvers.
- The side boundary fence within the rear side setback (4m) shall be a maximum height of 1.2m above the rear setback lot level (RL+1.25m).
- All other fencing to lot boundaries shall be in accordance with specifications detailed in Port Coogee Guidelines - Appendix C.
- For lots 908, 900, 901 & 903, the rear common boundary fence (when located adjacent the dry lot boundary) shall be a maximum of 1.2m and suitably finished in materials to complement the external walls of adjacent dwellings, unless alternative materials are otherwise agreed with the adjoining property owner.
- Any fencing to the rear common boundary (when located adjacent the dry lot boundary) shall not obstruct the drainage openings in the sea wall.

FINISHED LOT LEVELS

- Finished ground floor levels shall be no greater than 0.5m above the finished lot level, no brick build-up or additional fill is permitted for dwelling construction.
- All habitable rooms shall have a minimum floor level of RL2.5m AHD.
- The sewer system is designed for a gravity sewer connection from a lot level of RL3.3m AHD. Construction below this service level may require a private sewerage pump station, which includes the lower water edge level.
- The finished lot level of RL1.25m AHD within the 4m rear building setback exclusion area shall be maintained, except for the specific purpose of access steps where earthworks shall be within 0.5m of the finished lot level (RL1.25m AHD).
- All works proposed within the 4m rear building setback area (building exclusion zone) combined with potential pedestrian loading allowance, shall not exceed a total loading of 5kPa (0.5 Tonne/m²). As such, the total combined outcome (including, but not limited to, any proposed lightweight structure, landscaping works and pedestrian loading) within this area must be considered to enable engineering certification of final loading.
- No excavation (including for the purposes of access steps or landscaping) within the 4m rear building setback area shall be below RL0.75m AHD to preserve the geogrid soil reinforcement.
- All applications to Council are to include a site feature survey with levels shown relative to AHD.

RETAINING WALLS

- Construction involving load bearing walls on a nil setback must comply with engineering requirements associated with any retaining/feature walls present.
- Dwellings are generally to be setback 1.0m from retaining/feature walls for single storey dwellings and 1.5m to double storey dwellings. Certification for load bearing walls must be obtained from an independent practising Structural Engineer in relation to final dwelling proximity to retaining/feature wall.

DRAINAGE

- The stormwater drainage system design requires that all stormwater shall be managed by soakage within the lot. Discharge to the waterway is not permitted.
- Soakwells are not permitted in the building exclusion area (rear 4m setback).

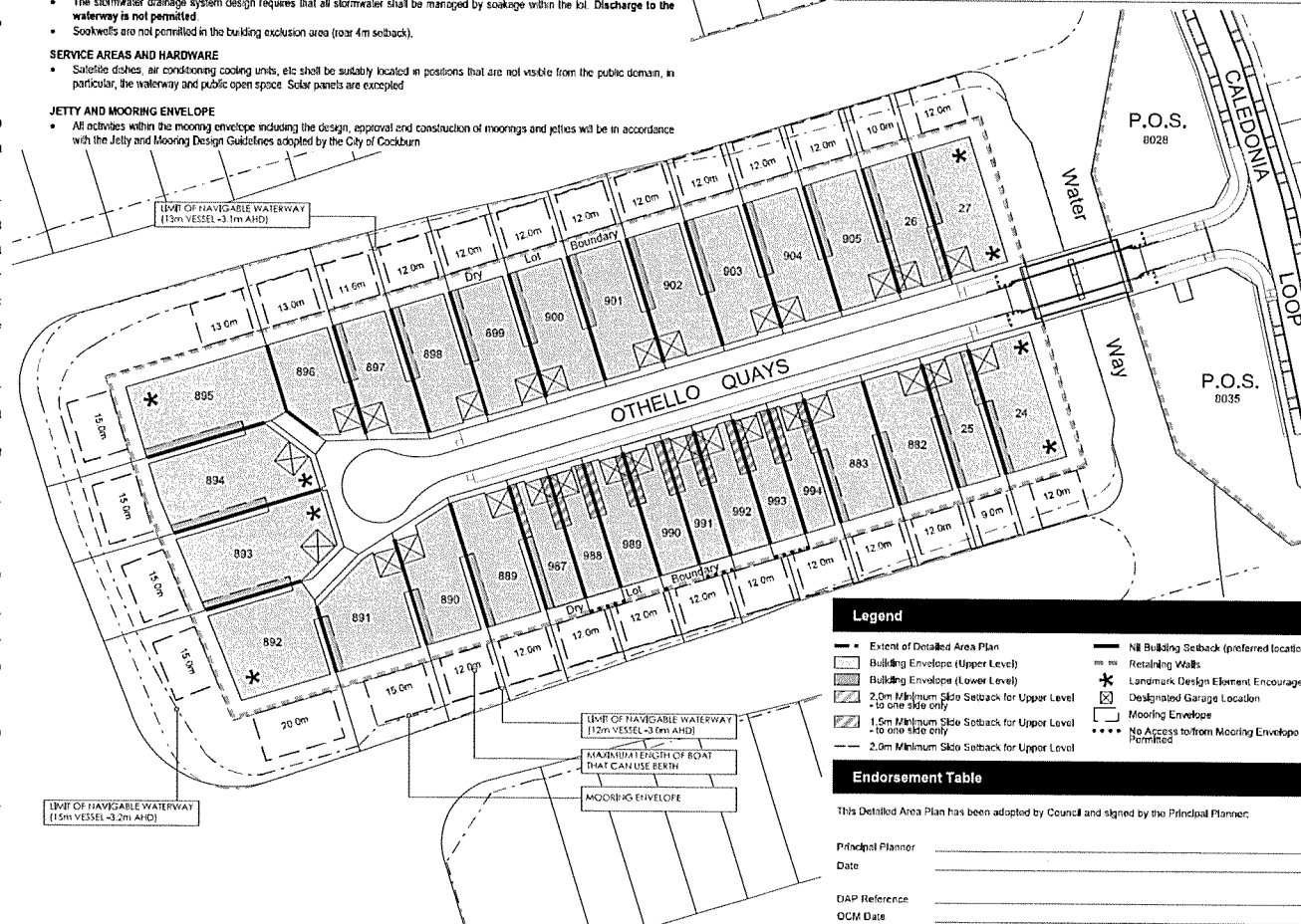
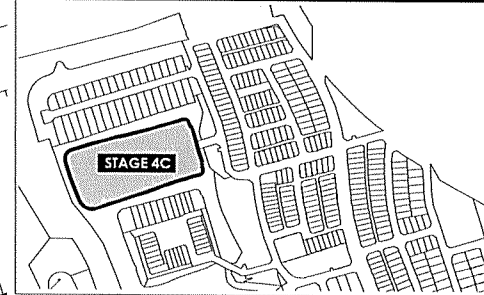
SERVICE AREAS AND HARDWARE

- Satellite dishes, air conditioning cooling units, etc shall be suitably located in positions that are not visible from the public domain, in particular, the waterway and public open space. Solar panels are exempted.

JETTY AND MOORING ENVELOPE

- All activities within the mooring envelope including the design, approval and construction of moorings and jetties will be in accordance with the Jetty and Mooring Design Guidelines adopted by the City of Cockburn.

Location Plan



Legend

- Extent of Detailed Area Plan
- Building Envelope (Upper Level)
- Building Envelope (Lower Level)
- 2.0m Minimum Side Setback for Upper Level - to one side only
- 1.5m Minimum Side Setback for Upper Level - to one side only
- 2.0m Minimum Side Setback for Upper Level
- Nil Building Setback (preferred location)
- Retaining Walls
- Landmark Design Element Encouraged
- Designated Garage Location
- Mooring Envelope
- No Access to/from Mooring Envelope Permitted

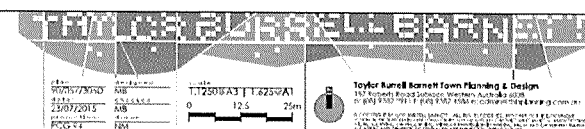
Endorsement Table

This Detailed Area Plan has been adopted by Council and signed by the Principal Planner:

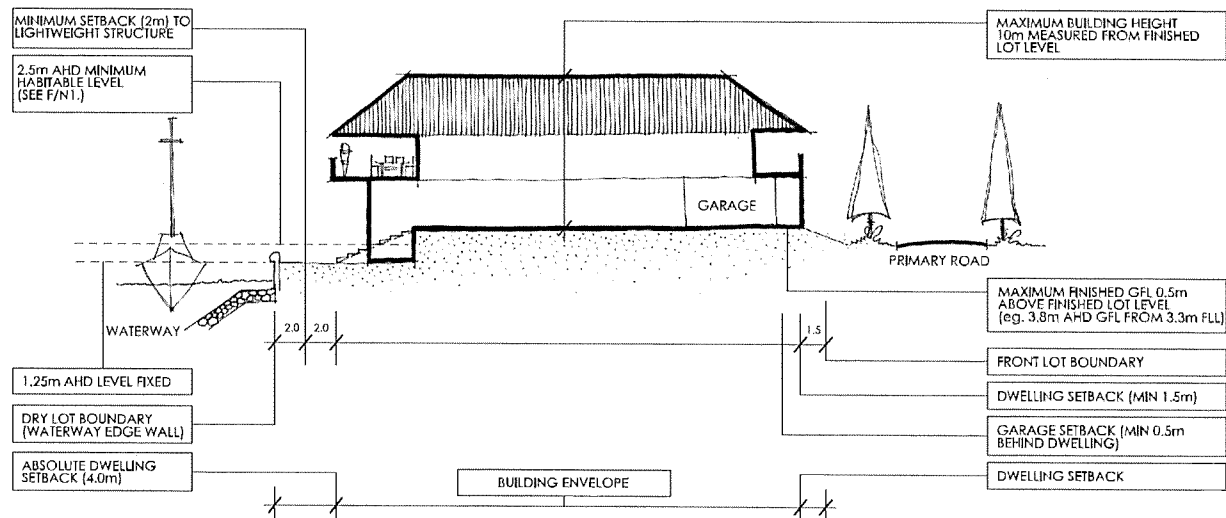
Principal Planner	
Date	
DAP Reference	
OCM Date	

Revised Detailed Area Plan - Stage 4C "Seaspray" - (Sheet 1 of 2)

PORT COOGEE



Indicative Section - Canal Lot Setbacks



FOOTNOTE 1: CONSTRUCTION BELOW RL3.3mAHD MAY REQUIRE A PRIVATE SEWERAGE PUMP STATION.

NOT TO SCALE

Endorsement Table

This Detailed Area Plan has been adopted by Council and signed by the Principal Planner:

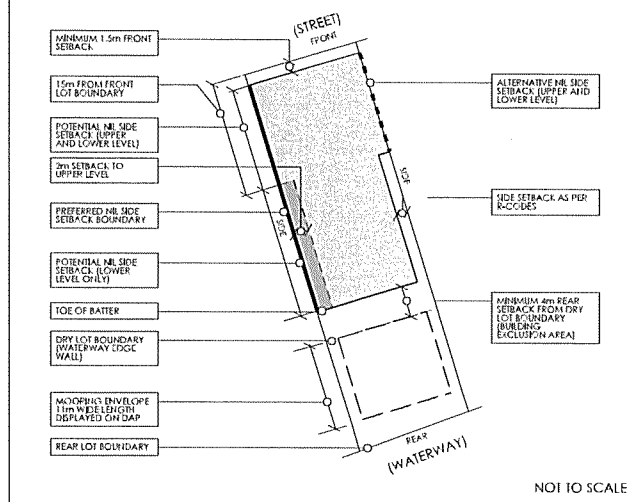
Principal Planner _____

Date _____

DAP Reference _____

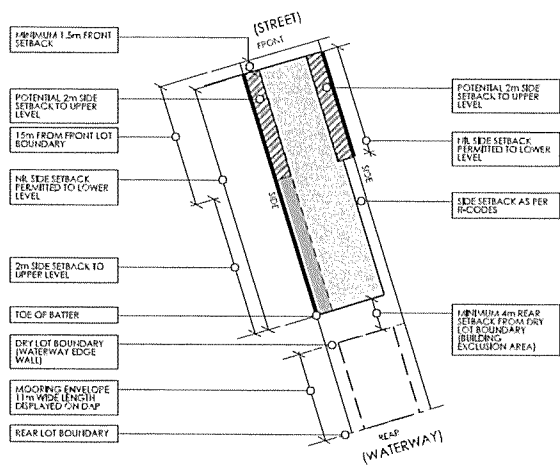
OCM Date _____

Typical Setback Requirements Lots 24, 27, 882-905



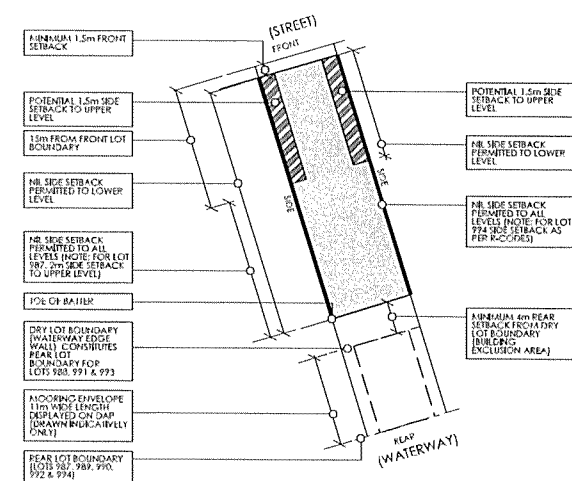
NOT TO SCALE

Typical Setback Requirements Lots 25 & 26



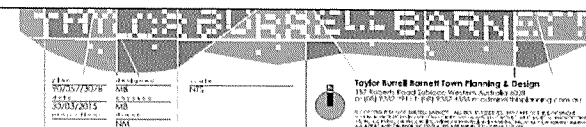
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Typical Setback Requirements Lots 987-994

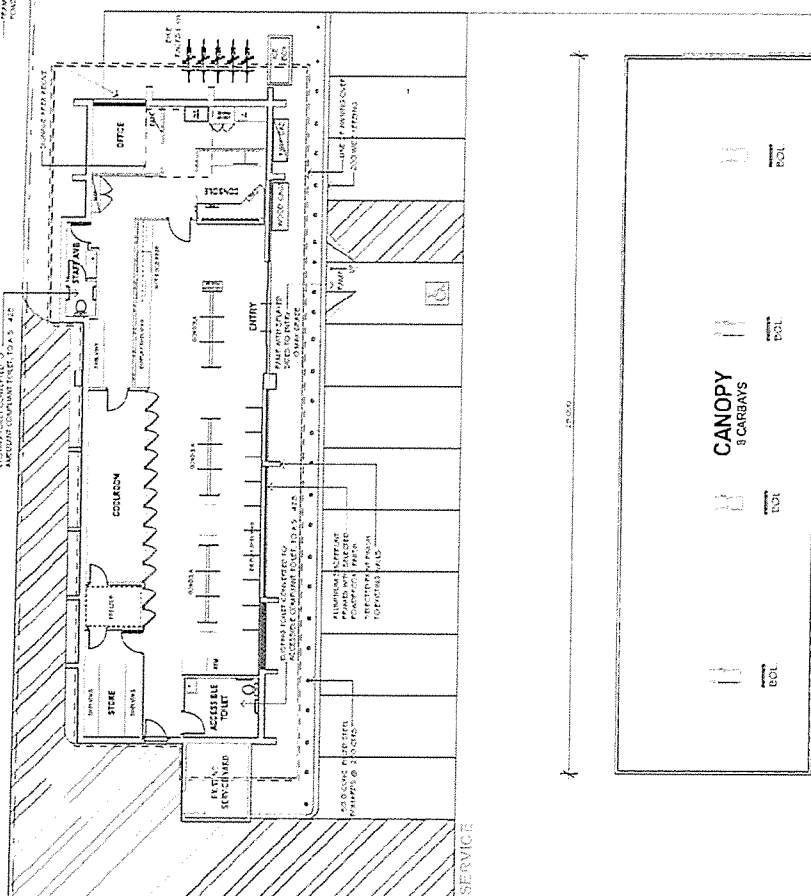


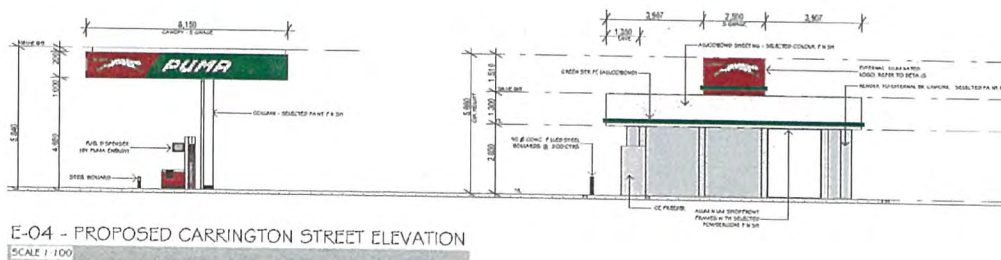
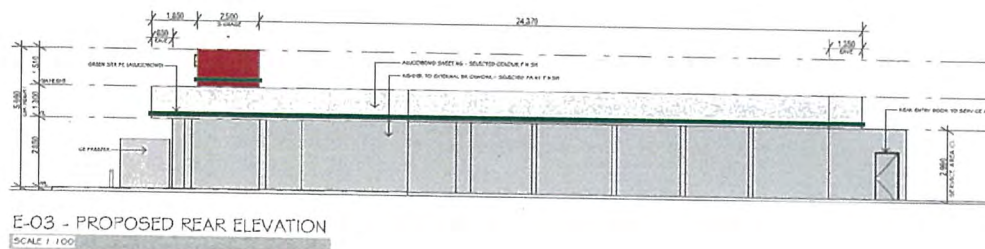
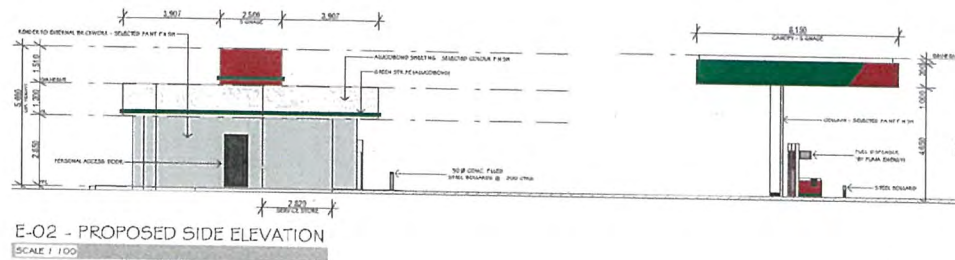
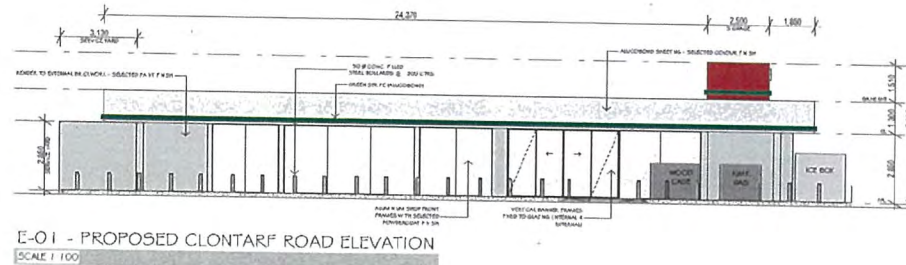
NOT TO SCALE

Revised Detailed Area Plan Stage 4C "Seaspray" - (Sheet 2 of 2)
PORT COOGEE









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PROPOSED ELEVATIONS
SCALE 1:100

PROPOSED COMMERCIAL DEVELOPMENT
LOT 55 No.224 CLONTARF ROAD
HAMILTON HILL WA 6163
for PUMA ENERGY (AUSTRALIA)

HA
HINDLEY & ASSOCIATES
PTY LTD
BUILDING DESIGNERS
166 STirling Highway
NEDLANDS WA 6909
PO BOX 199
NEDLANDS WA 6909
PHONE +61 8 9338 6666
FAX +61 8 9338 6700
admin@hindley.com.au

DEVELOPMENT APPLICATION

Attach 2

Drawn by
Checked by
Scale
App No.
Date
Rev. B



THIS DRAWING IS A PRELIMINARY DESIGN AND MUST NOT BE USED FOR ANY OTHER PURPOSES WITHOUT THE WRITTEN AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.	HINDLEY & ASSOCIATES PTY LTD AS TRUSTEES FOR HINDLEY TRUST ACT 1998	NO: SATS: REVISED: A 06.11.14 ISSUE FOR CLIENT COMMENT	DRAWN: CHEN CHKD: AM
DO NOT SCALE FROM THIS DRAWING.			
THE CONTRACTOR AND HIS SUBCONTRACTORS SHALL VERIFY ALL DIMENSIONS ON SITE BEFORE ANY CONSTRUCTION WORK COMMENCES.			
THIS IS A PRELIMINARY DESIGN AND MUST NOT BE USED FOR ANY OTHER PURPOSES WITHOUT THE WRITTEN AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.			

PROPOSED PERSPECTIVES

N.T.S

A1 SHEET

PROPOSED COMMERCIAL DEVELOPMENT
LOT 55 No.224 CLONTARF ROAD
HAMILTON HILL WA 6163
for PUMA ENERGY (AUSTRALIA)

HIA
HINDLEY & ASSOCIATES
PTY LTD
BUILDING DESIGNERS

166 STirling Highway
WEDLANDS WA 6009

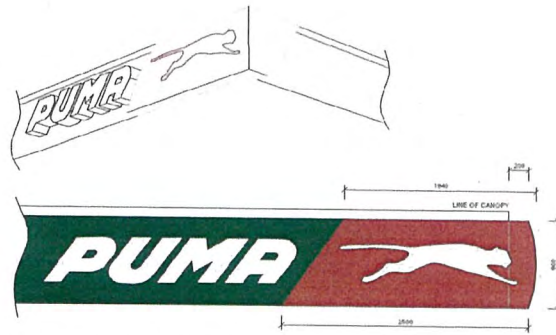
PO BOX 188
WEDLANDS WA 6009

PHONE +61 8 9446 0400
FAX +61 8 9446 0700
info@hindley.com.au

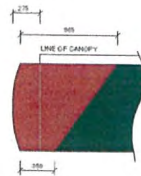
DEVELOPMENT APPLICATION

Attach 2

Date: 06.11.14
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Rev: A



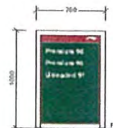
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SCALE 1:25



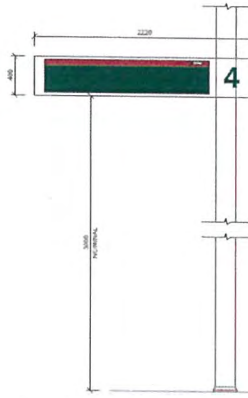
SUPER 7 SHOP ID - INTERNALLY ILLUMINATED
SCALE 1:25



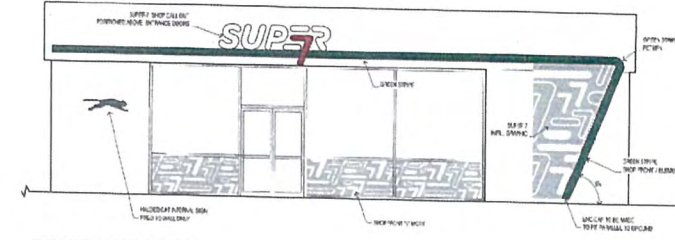
AIR & WATER
SCALE 1:25



LEADER BOARD
SCALE 1:25



SPREADER - INTERNALLY ILLUMINATED
SCALE 1:25



SHOP FRONT DECAL SIGNAGE
SCALE 1:25



Gm MID - INTERNALLY ILLUMINATED
SCALE 1:25

HIA
HINDLEY & ASSOCIATES
PTY LTD
BULKHEAD DESIGNS
166 STirling Highway
NEDLANDS WA 6009
PO BOX 198
NEDLANDS WA 6009
PHONE 9446 6666
FAX 9446 6788
admin@hindley.com.au

DEVELOPMENT APPLICATION

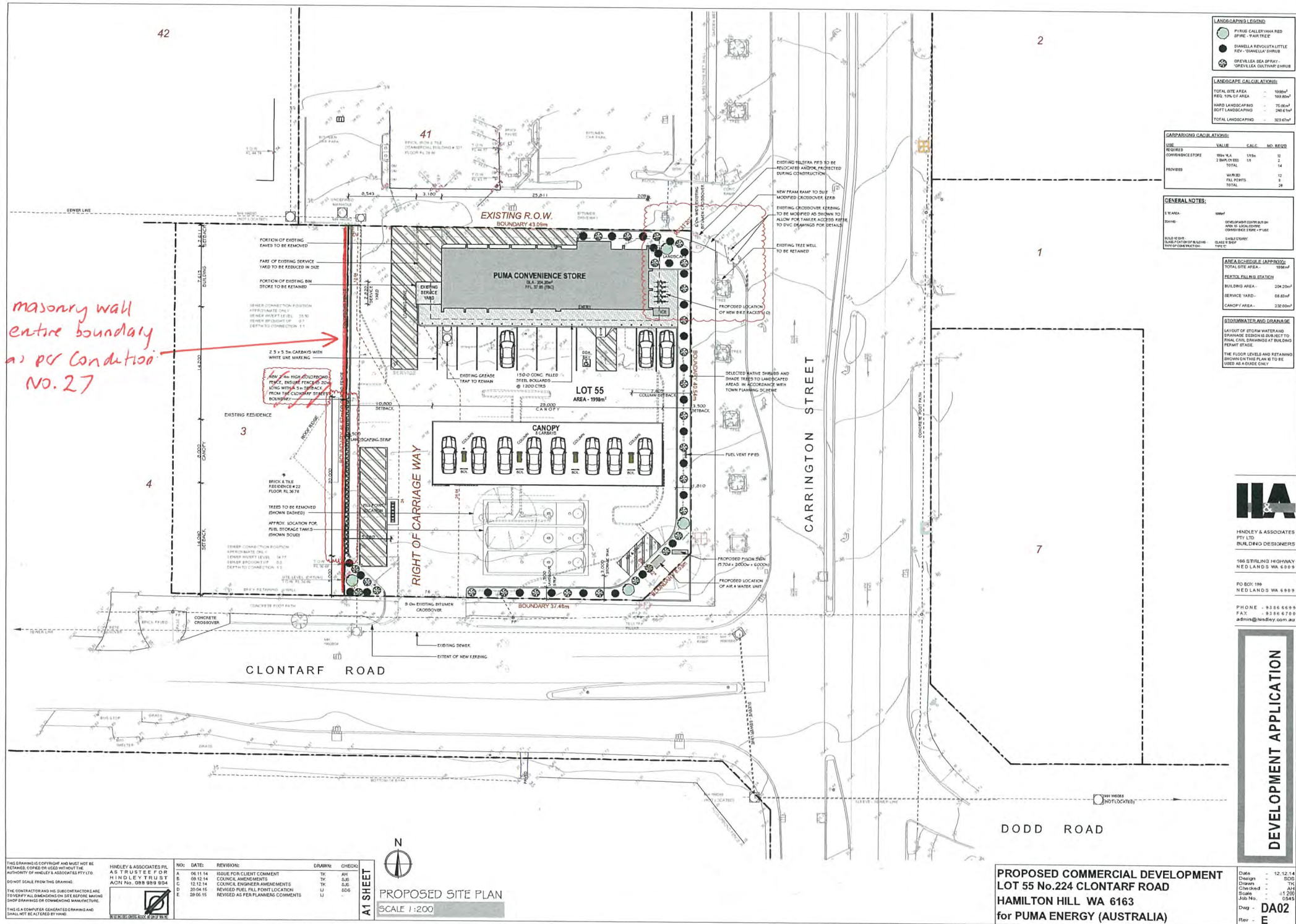
Attach 2

PROPOSED COMMERCIAL DEVELOPMENT
LOT 55 (#224) CLONTARF ROAD
HAMILTON HILL WA 6163
for PUMA ENERGY (AUSTRALIA)

Date: 08/08/2015
Design: DA0
Drawn: DA0
Checked: DA0
Scale: DA0
Job No: DA0
Rev: DA0

THIS DRAWING IS A PRELIMINARY DESIGN AND NOT BE RENDERED FOR CONSTRUCTION WITHOUT THE AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.	HINDLEY & ASSOCIATES PTY LTD AS TRUSTEES FOR HINDLEY TRUST ACN 008 988 984	NO: 001 DATE: 08/08/2015 REVISION: 01 DESIGNER: DA0 CHECKED: DA0	DATE: 08/08/2015 REVISION: 01 DESIGNER: DA0 CHECKED: DA0
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A1 SHEET



WESTERN



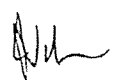
AUSTRALIA

REGISTER NUMBER 41/D59319	
Duplicate Edition N/A	DATE DUPLICATE ISSUED N/A

RECORD OF CERTIFICATE OF TITLE UNDER THE TRANSFER OF LAND ACT 1893

VOLUME 1653 FOLIO 515

The person described in the first schedule is the registered proprietor of an estate in fee simple in the land described below subject to the reservations, conditions and depth limit contained in the original grant (if a grant issued) and to the limitations, interests, encumbrances and notifications shown in the second schedule.


REGISTRAR OF TITLES



LAND DESCRIPTION:

LOT 41 ON DIAGRAM 59319

REGISTERED PROPRIETOR: (FIRST SCHEDULE)

KUPAL INVESTMENTS PTY LTD
IN 4/12 SHARE
SNOWDONIA NOMINEES PTY LTD
IN 4/12 SHARE
MARIA SOMAS
IN 2/12 SHARE
CHRISTOPHER LUKE SOMAS
IN 1/12 SHARE
MARCIA MANOLAS
IN 1/12 SHARE
ALL OF 193 MILL POINT ROAD, SOUTH PERTH
AS TENANTS IN COMMON

(T F960178) REGISTERED 21 AUGUST 1995

LIMITATIONS, INTERESTS, ENCUMBRANCES AND NOTIFICATIONS: (SECOND SCHEDULE)

- C638260 EASEMENT BENEFIT SEE SKETCH ON VOL 1653 FOL 515. REGISTERED 18.10.1983.
- C638261 EASEMENT BURDEN SEE SKETCH ON VOL 1653 FOL 515. REGISTERED 18.10.1983.

Warning: A current search of the sketch of the land should be obtained where detail of position, dimensions or area of the lot is required.
* Any entries preceded by an asterisk may not appear on the current edition of the duplicate certificate of title.
Let as described in the land description may be a lot or location.

-----END OF CERTIFICATE OF TITLE-----

STATEMENTS:

The statements set out below are not intended to be nor should they be relied on as substitutes for inspection of the land and the relevant documents or for local government, legal, surveying or other professional advice.

SKETCH OF LAND: 1653-515 (41/D59319).
PREVIOUS TITLE: 1606-581.
PROPERTY STREET ADDRESS: 337 CARRINGTON ST. HAMILTON HILL.

END OF PAGE 1 - CONTINUED OVER

RECORD OF CERTIFICATE OF TITLE

REGISTER NUMBER: 41/D59319

VOLUME/FOLIO: 1653-515

PAGE 2

LOCAL GOVERNMENT AREA: CITY OF COCKBURN.

LANDGATE COPY OF ORIGINAL NOT TO SCALE Tue Dec 16 12:01:14 2014 JOB 46386723



Application C638263

WESTERN

AUSTRALIA

Volume 1653 Folio 516

VOL.
1653FOL.
516

CERTIFICATE OF TITLE

UNDER THE "TRANSFER OF LAND ACT, 1893" AS AMENDED



I certify that the person described in the First Schedule hereto is the registered proprietor of the undermentioned estate in the undermentioned land subject to the easements and encumbrances shown in the Second Schedule hereto.

Dated 18th October, 1983

REGISTRAR OF TITLES


ESTATE AND LAND REFERRED TO

Estate in fee simple in portion of Cockburn Sound Location 10 and being Lot 55 the subject of Diagram 43656, delineated and coloured green on the map in the Third Schedule hereto, together with a right of carriageway over the portion of Lot 41 on Diagram 59319 coloured brown on the said map hereon as set out in Transfer C638261.

FIRST SCHEDULE (continued overleaf)

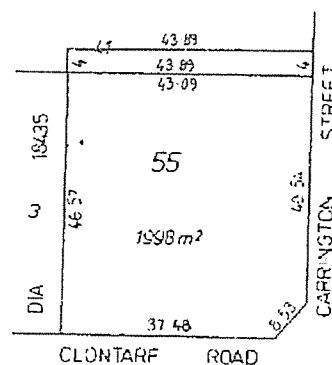
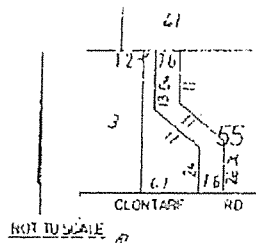
~~Paul William Gilbert Cook, Married, Newagent, Dorothy Cook, his wife, Geoffrey Paul Cook, Chemist and Paul Thomas Cook, Architectural Student, all of 16 Shipping Road, City Beach and Paul, son, of 137 Grand Promenade, Dianella, as tenants in common in equal shares~~

SECOND SCHEDULE (continued overleaf)

1. TRANSFER C638260. A right of carriageway over the portion of the within land coloured brown on the map in the margin as set out in the said Transfer is granted to the proprietor or proprietors for the time being of Lot 41 on Diagram 59319. Registered 18.10.83 at 2.51 o/c.
2. C/VEAT C638264. Lodged 18.10.83 at 2.53 o/c.

REGISTRAR OF TITLES

THIRD SCHEDULE



SCALE 1:1000
1/5

NOTE: RULING THROUGH AND SEALING WITH THE OFFICE SEAL INDICATES THAT AN ENTRY NO LONGER HAS EFFECT
ENTRIES NOT RULED THROUGH MAY BE AFFECTED BY SUBSEQUENT ENDORSEMENTS.

720029/12/77-45M-S/2080

PERSONS ARE CAUTIONED AGAINST ALTERING OR ADDING TO THIS CERTIFICATE OR ANY NOTIFICATION HEREON

WESTERN



AUSTRALIA


REGISTER NUMBER 55/D43656	
DUPLICATE EDITION 1	DATE DUPLICATE ISSUED 23/12/2005

RECORD OF CERTIFICATE OF TITLE UNDER THE TRANSFER OF LAND ACT 1893

VOLUME
1653

FOLIO
516

The person described in the first schedule is the registered proprietor of an estate in fee simple in the land described below subject to the reservations, conditions and depth limit contained in the original grant (if a grant issued) and to the limitations, interests, encumbrances and notifications shown in the second schedule.


REGISTRAR OF TITLES



LAND DESCRIPTION:

LOT 55 ON DIAGRAM 43656

REGISTERED PROPRIETOR: (FIRST SCHEDULE)

PRECIOUS HOLDINGS PTY LTD OF 3RD FLOOR, 1 HAVELOCK STREET, WEST PERTH
(T E231039) REGISTERED 9 NOVEMBER 1989

LIMITATIONS, INTERESTS, ENCUMBRANCES AND NOTIFICATIONS: (SECOND SCHEDULE)

1. C638261 EASEMENT BENEFIT SEE SKETCH ON VOL 1653 FOL 516. REGISTERED 18.10.1983.
2. C638260 EASEMENT BURDEN SEE SKETCH ON VOL 1653 FOL 516. REGISTERED 18.10.1983.
3. *C638264 CAVEAT BY SELDEN PTY LTD LODGED 18.10.1983.
4. *F036918 CAVEAT BY SELDEN PTY LTD LODGED 12.11.1992.
5. *K875958 CAVEAT BY SELDEN PTY LTD LODGED 11.3.2009.

Warning: A current search of the sketch of the land should be obtained where detail of position, dimensions or area of the lot is required.
* Any entries preceded by an asterisk may not appear on the current edition of the duplicate certificate of title.
Lot as described in the land description may be a lot or location.


-----END OF CERTIFICATE OF TITLE-----

STATEMENTS:

The statements set out below are not intended to be nor should they be relied on as substitutes for inspection of the land and the relevant documents or for local government, legal, surveying or other professional advice.

SKETCH OF LAND: 1653-516 (55/D43656).
PREVIOUS TITLE: 1653-514.
PROPERTY STREET ADDRESS: 224 CLONTARF RD. HAMILTON HILL.
LOCAL GOVERNMENT AREA: CITY OF COCKBURN.

LANDGATE COPY OF ORIGINAL NOT TO SCALE Wed Sep 17 17:27:37 2014 JOB 45683747


Landgate
www.landgate.wa.gov.au



WESTERN



AUSTRALIA

Volume 1653 Folio 514

POL.
1653

FOL.
516

⑤

UNDER THE "TRANSFER OF LAND ACT, 1893" AS AMENDED.

I certify that the person described in the First Schedule hereto is the registered proprietor of the undermentioned estate in the undermentioned land subject to the easements and encumbrances shown in the Second Schedule hereto.

Dated 18th October, 1983

REGISTRAR OF TITLES

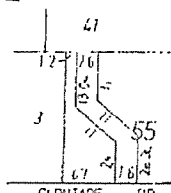


ESTATE AND LAND REFERRED TO

Estate in fee simple in portion of Cockburn Sound Location 10 and being Lot 55 the subject of Diagram 43656, delineated and coloured green on the map in the Third Schedule hereto, together with a right of carriageway over the portion of Lot 41 on Diagram 59319 coloured brown on the said map hereon as set out in Transfer C638261.

FIRST SCHEDULE (continued overleaf)

Green and Irwin Stone, Architects, 1115 G. Chipping Wood, Greyhound
Green and Irwin Stone, Architects, 1115 G. Chipping Wood, Greyhound



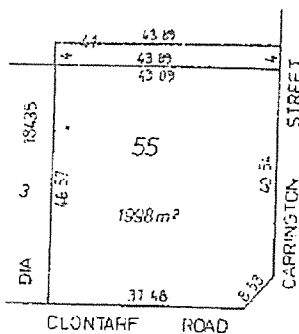
SECOND SCHEDULE (continued overleaf)

1. TRANSFER C638260. A right of carriageway over the portion of the within land coloured brown on the map in the margin as set out in the said Transfer is granted to the proprietor or proprietors for the time being of Lot 41 on Diagram 59319. Registered 18.10.83 at 2.51 o/c.

2. ONVSTAT 0632264. Lodged 16.10.83 at 2.53 o'clock.

REGISTRAR OF TITLES

THIRD SCHEDULE

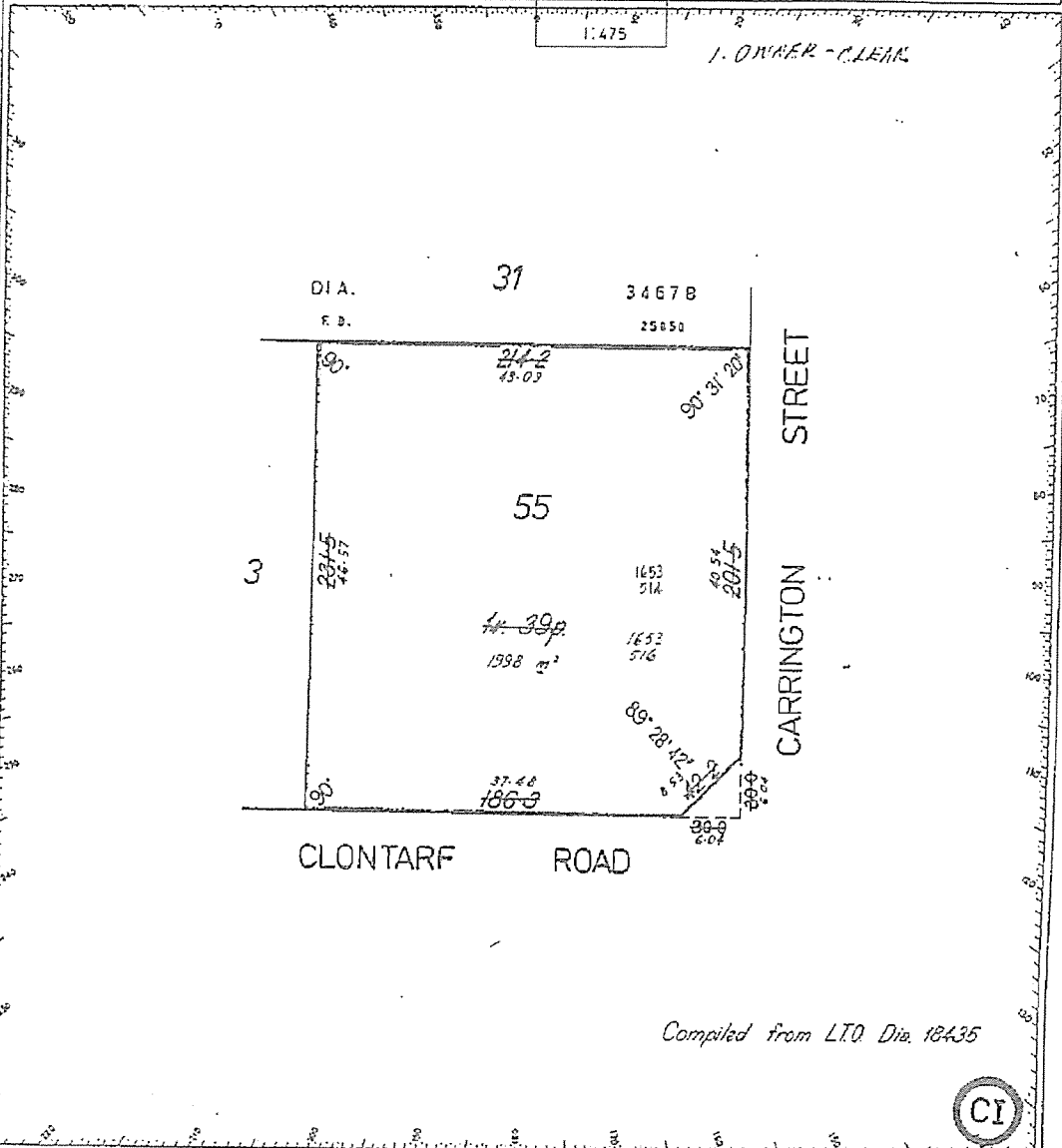


NOTE: RULING THROUGH AND SEALING WITH THE OFFICE SEAL INDICATES THAT AN ENTRY NO LONGER HAS EFFECT
ENTRIES NOT RULED THROUGH MAY BE AFFECTED BY SUBSEQUENT ENDORSEMENTS

72003/1277-45M-5/7362

PERSONS ARE CAUTIONED AGAINST ALTERING OR ADDING TO THIS CERTIFICATE OR ANY NOTIFICATION HEREON

Town or District.	Number of Lot or Location.	Field Book.	Scale	Certificate in which Land is Vested.	Area
COCKBURN SOUND	LOTS 1 & 2 of LOC. 10		60 Links equals to one inch 1:475	Vol. 1173.. Fol. 478.. 1173 479	A. R. R.



CERTIFICATE I hereby certify that this survey was performed by me personally (or under my own personal supervision, inspection and field check) in strict accordance with the Licensed Surveyors (Guidance of Surveyors) Regulations, 1961.		Approved by Town Planning Board DIA 43656 	
Compiled Licensed Surveyor. Date _____		Chairman Date _____	
Approved <i>D. E. Campbell</i> Inspected of Plans and Surveys Examined <i>M. J. WILSON</i> Date <i>14.1.72</i> <i>CE. BK. 2/109 12.7.72</i>	On Plan Diagram 18435 Index Plan 207744 PERTH 2000 08-11	Registered L.S. FEE MAIN 5.50 17777 27.1.72	Diagram No. 43656

No. C 638261 E



FEES (office use)	\$	c
Oct 19 2 51 PM 1983	23	✓

Parties MORADA PTY. LTD.
AND
MESSRS. COOK & PEDO PTY. LTD.

Lodged by MUIR WILLIAMS NICHOLSON
AUSTMARK CENTRE
Address 15-17 WILLIAM STREET
PERTH. W.A. 6000
Phone No. 327 5777
Ref: BGI:POVE9880-003

Use this space for instructions if any documents are to issue to other than lodging party.

Titles, Crown Leases, Declarations, etc., lodged with this document. (To be filled in by person lodging.)

1. *dup. deed*
2. *1606-581*
3. _____
4. _____
5. _____
6. _____

Received items,

No's *1-2*Rec. Clerk. *[Signature]*

BELOW THIS LINE FOR OFFICE USE ONLY

Encumbrances
not notified
on face.

Registered

*18th October 1983*at *2 51*

o'clock and

particulars entered in the Register Book.

New Titles
to issue or
Endorsing
instruction.

*DR to instr.*Initials of
Signing
Officer*[Signature]**[Signature]*

REGISTRAR OF TITLES

EXAMINED

24/10/83

SIGNED by the said PAUL WILLIAM GILBERT COOK in the presence of:

Paul Cook

WITNESS:

ADDRESS: 79 EDWARDS ST BEAFORD W.A.

OCCUPATION: BANK OFFICER

SIGNED by the said DOROTHY COOK in the presence of:

Dorothy Cook

WITNESS:

ADDRESS: 79 EDWARDS ST BEAFORD W.A.

OCCUPATION: BANK OFFICER

SIGNED by the said GEOFFREY PAUL COOK in the presence of:

Geoff Cook

WITNESS:

ADDRESS: 79 EDWARDS ST BEAFORD W.A.

OCCUPATION: BANK OFFICER

SIGNED by the said PAUL THOMAS COOK in the presence of:

Paul Cook

WITNESS:

ADDRESS: 79 EDWARDS ST BEAFORD W.A.

OCCUPATION: BANK OFFICER

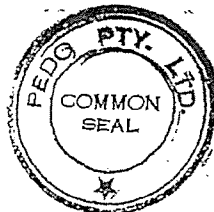
THE COMMON SEAL of PEDO PTY. LTD. was hereunto affixed by authority of the Directors in the presence of:

Director:

D. A. Leach

Secretary:

Paul Cook



ENDORSE 1ST SCHED. (COUNT) 1653-514 (only only) 2nd Sched. 1/14/83.
TRANSFER C63826/ A right of carriageway over the portion of lot 41 on Diagram 59319
coloured red on the map in Transfer C 63826/ is granted to the proprietor
or proprietors for the time being of the within land as set out in the
said Transfer. Registered 18-10-83 D 2-51 01C.

2562d - 3 - 22.8.83

WESTERN AUSTRALIA STAMP DUTY
 21SEP83 20326243 5/D \$5.00

WESTERN AUSTRALIA.
 Transfer of Land Act 1893 as amended

No. 0638261

Blank Instrument Form (see footnote)

1. Insert type of document here.

THIS DEED OF EASEMENT made

BETWEEN

MORADA PTY. LTD. of 108 Brighton Road Scarborough
 (hereinafter together called "the Grantor" which
 expression shall include all persons deriving a title
 under the Grantor) of the one part

AND

PAUL WILLIAM GILBERT COOK Retired Newsagent and DOROTHY
 COOK his wife and GEOFFREY PAUL COOK Chemist and PAUL
 THOMAS COOK Architectural Student all of 18 Chipping Road
 City Beach and PEDO PTY. LTD. of 337 Grand Promenade
 Dianella (hereinafter together called "the Grantee" which
 expression shall include all persons deriving a title
 under the Grantor) of the other part

WHEREAS

- A. The Grantor is registered as the proprietor for the time being of an estate in fee simple in all that piece of land being Portion of Cockburn Sound Location 10 and being Lot 41 on Diagram 59319 being the whole of the land comprised in Certificate of Title Volume 1606 Folio 581 ("Lot 41").
- B. The Grantee is registered as the proprietor for the time being of an estate in fee simple in all the land being Portion of Cockburn Sound Location 10 and formerly being Lots 1 and 2 on Diagram 18435 and being formerly the whole of the land comprised respectively in Certificates of Title Volume 1173 Folio 479 and Volume 1173 Folio 478 and now being Lot 55 on Diagram 43656 and being the whole of the land comprised in Certificate of Title Volume 1653 Folio 514 ("Lot 55").

FORM APPROVAL
 No. 046
 REGISTRAR OF TITLES

NOTE: This Form may be used only when the "Box Type" Form is not suitable. It may be completed in narrative style.

 Sands & McDougall
 STOCK FORM 324

C. The Grantor has agreed to grant to the Grantee an easement over that portion of Lot 41 as is coloured red on the Plan hereto annexed ("the subject land") upon the terms conditions covenants hereinafter set forth.

NOW THIS DEED WITNESSES as follows:-

1. The Grantor hereby grants and transfers to the Grantee as appurtenant to Lot 55 full and free right and liberty to the Grantee and the Grantee's tenants, servants, agents, workmen and visitors on foot or in vehicles for all lawful purposes connected with the use of Lot 55 at all times hereafter to pass and repass over the subject land for the purposes of gaining access to and egress from Lot 55 and to and from the roadways which adjoin Lot 55 and Lot 41.
2. The Grantor and the Grantee hereby covenant and agree with each other that save with the written consent of each none of the rights hereby granted shall be modified surrendered released or abandoned either wholly or partially and no party shall make application to any Court or to any competent authority for the said rights or any of them to be modified or extinguished either wholly or partially or removed from the Register Book.
3. The Grantor and the Grantee hereby covenant and agree that all costs duties and fees incurred in relation to the instructions for stamping and registration of this Deed of Easement shall be borne equally by the parties.

EXECUTED by the parties as a Deed this 19th day of September 1983.

THE COMMON SEAL of MORADA PTY. LTD. was hereunto affixed by authority of the Directors in the presence of:

Director:

Geoff Cook

Secretary:

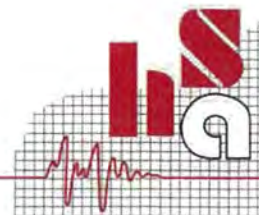
Paul Cook



2562d - 3 - 22.8.83

HERRING STORER ACOUSTICS

Suite 34, 11 Preston Street, Como, W.A. 6152
 P.O. Box 219, Como, W.A. 6952
 Telephone: (08) 9367 6200
 Facsimile: (08) 9474 2579
 Email: hsa@hsacoustics.com.au



Our ref: 19220-2-14211-02

28 May 2015

Planning Solutions
 296 Fitzgerald Street
 PERTH WA 6000

Attention : Marc Re
 Email : marcr@planningsolutions.com.au

Dear Marc,

**PROPOSED SERVICE STATION
 LOT 55 CLONTARF ROAD, HAMILTON HILL
 ADDITIONAL ACOUSTICAL INFORMATION**

Following the State Administrative Tribunal (SAT) mediation undertaken for the above development on Friday 8th May 2015, we provide the following information. From the mediation, we understand that additional information was requested by council for the following :

- Locations of receivers used in noise model;
- Additional information regarding noise emissions from the tyre air fill point;
- Confirmation regarding noise emissions from the mechanical services;
- Location of air compressor; and
- Noise emissions from tanker engine start and air brake air release.

ASSIGNED NOISE LEVELS

For information, the assigned noise levels at the neighbouring residence are listed below in Table 1.

TABLE 1 - ASSIGNED OUTDOOR NOISE LEVEL

Noise sensitive premises		Assigned Noise Level (LA10)		
		LA10	LA1	LAmax
Noise sensitive premises	0700 - 1900 hours Monday to Saturday	52	62	73
	0900 - 1900 hours Sunday and Public Holidays	47	57	67
	1900 - 2200 hours all days	47	57	62
	2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and Public Holidays	42	52	62

Note: LA10 is the noise level exceeded for 10% of the time.
 LA1 is the noise level exceeded for 1% of the time.
 LAmax is the maximum noise level.



RECEIVER LOCATIONS

The receiver locations are shown on Figure 1 attached.

TYRE AIR FILL POINT

The air and water point is located at the south west corner of the site. We understand that there will be no "beep" indications, and as stated in the report will not be noisy, hence compliance will be achieved. However, we have undertaken a noise level measurement of the air release associated with the air fill point at a similar facility. A measurement of 78 dB(A) at 1metre was recorded. Thus at the neighbouring residence, noise received would be 46 dB(A). Thus noise received at the neighbouring residence would comply with the regulatory criteria.

MECHANICAL SERVICES

We note that the mechanical services have not been designed at this stage. However, from another similar project (ie proposed petrol station located at Lot 216 Rockingham Road, Hamilton Hill) the mechanical services would comprise :

- 6 off air conditioning condensing units; 52 dB(A) @ 1m;
- 3 exhaust systems; 1 at 47 dB(A) @ 3m and 2 at 57 dB(A) @ 3m; and
- 3 refrigeration units at 63 dB(A) @ 3m.

In terms of noise emissions, as single noise emissions, the equivalent single noise level would be :

Air conditioning condensing unit	-	60 dB(A) @ 1m;
Exhaust systems	-	60 dB(A) @ 3m; and
Refrigeration units	-	77 dB(A) @ 1m.

The noise levels used in the assessment report, as stated in Table 4.1 of the report were :

TABLE 4.1 – SUMMARY OF MECHANICAL SERVICES NOISE LEVELS

Air conditioning condensing units	74 dB(A)
Exhaust fans	61 dB(A)
Refrigeration units	78 dB(A)

The overall noise emissions from the 2 sets of data are equivalent. Thus the acoustic assessment as outlined in the assessment report still holds and compliance will be achieved.

Notes :

- 1 The above does not consider any diversification that would occur during the night / evening period.
- 2 For the mechanical services, the critical location is the first storey of residence E.
- 3 The assessment is based on the mechanical services being located on the roof.

AIR COMPRESSOR LOCATION

At this stage, the exact location of the air compressor has not been determined; however, we understand that the preferred location is within the service yard. From information received we understand that the air compressor will be an Royce RC20/100 which has a rated noise level of 79 dB(A) at 1 metre.

Based on the above noise level, the noise received at the worst case location (first floor of receiver E) has been calculated to be 38 dB(A).

The air compressor would operate for less than 10% of the time, however, it could run during the night period. Hence noise received at the neighbouring needs to comply with the assigned night period L_{A1} noise level of 52 dB(A).

Noise received at the neighbouring residence from the air compressor could be tonal, in which case an +5 dB(A) penalty would be applied. Thus the assessable noise level would be 43 dB(A).

Thus, noise emissions from the air compressor would comply with the assigned noise level at all times.

TANKER START AND AIR BRAKE AIR RELEASE

As requested as part of the mediation, we have undertaken an assessment of noise emissions from the refuelling tanker starting and air brake air release. To determine the noise emissions from these items, noise level measurement were undertaken of a tanker. The noise level measurements recorded were :

MEASURED NOISE LEVELS AT 1METRE FROM TANKER

Engine Start	80
Air Brake Air Release	75

Noise received at the worst case location (being location D) are as listed in the table below :

RESULTANT NOISE LEVELS

Engine Start	74
Air Brake Air Release	70

The measurements indicated that noise emissions from the above items would be considered as impulsive and a +10 dB(A) penalty would be applied to the calculated noise level.

The tanker refuelling is a day time operation, thus noise received at the neighbouring residence needs to comply with the assigned day period $L_{A\text{Max}}$ noise level of 72 dB.

With the addition of the penalty, noise received at the neighbouring residence from the engine start would comply with the required assigned noise level, however, noise received from the air brake would exceed the assigned noise level by 6 dB(A).

For the noise emissions from the air brake to comply with the assigned noise level, a 2.4 metre high barrier is required to be installed for a proportion of the western boundary. To achieve compliance it is recommended a second colourbond type fence be installed inside (ie petrol station side) the existing fence. The extent of 2.4 metre high barrier to commence at 5 metre from the southern boundary (ie 5 metre setback from southern boundary) and extend to the rear of the neighbouring residence or 20 metres in length.

We believe the above provides the additional information requested and shows that noise emissions from the proposed development will comply with the requirements of the *Environmental Protection (Noise) Regulations 1997*.

Yours faithfully,
For **HERRING STORER ACOUSTICS**

Tim Reynolds

Att.



FIGURE 1 - NOISE MODEL RESIDENTIAL RECIEVER LOCATIONS



GENERAL NOTES:	
1 TO AREA	10000'
CONTOUR	CONTOUR INTERVAL 5 FT OR AREA TO LOCAL CONTAINMENT CONTOUR TO STORE - 1" USE
BUILDING SHIR	5 HOLE STORY
CLASSIFICATION OF BUILDING	CLASS 1-1000
TYPE OF CONSTRUCTION	TYPE 1

STORMWATER AND DRAINAGE

LAYOUT OF STORM WATER AND DRAINAGE DESIGN IS SUBJECT TO FINAL CIVIL DRAWINGS AT BUILDING PERMIT STAGE.

THE FLOOR LEVELS AND RETAINING SHOWN ON THIS PLAN IS TO BE USED AS A GUIDE ONLY.

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Date - 12.12.14
Design - SDS
Drawn - TK
Checked - AH
Scale - 1:200
Job No. - 0545
Dwg - **DA02**
Rev - **E**

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AS TRUSTEE FOR
HINDLEY TRUST
AON No. 088 989 904

PROPOSED SITE PLAN
SCALE 1:200



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To:	City of Cockburn	From:	Daniel Lloyd
Attention:	Andrew Lefort	Date:	17 June 2015
Email:	alefort@cockburn.wa.gov.au	Pages:	2
Our Ref:	15043178-02		
Re:	Acoustic assessment of the proposed Service Station, to be located at 224 Clontarf Road, Hamilton Hill - Peer Review of Additional Information		

This report presents a peer review of the additional information, dated 28 May 2015, provided by Herring Storer Acoustics in relation to noise modelling undertaken for a proposed Service Station, to be located at 224 Clontarf Road, Hamilton Hill.

The additional information provided included:

- Locations of receivers provided in the noise model;
- Additional information regarding noise emissions from the tyre air fill point;
- Confirmation regarding noise emissions from the mechanical services;
- Location of air compressor; and
- Noise emissions from tanker engine start and air brake air release.

Location of Receivers

We do not agree on the location of the noise assessment at Residence D. The regulations require the assigned levels to be met at any location within 15m of a noise sensitive premise in a highly sensitive area (residential). The location used in the HSA report focuses on an area outside of the side window only. While this would be appropriate for the noise from cars at the fuel bowsers, the area in front of the house would be appropriate to assess the noise from the refueling tanker and the rear of the house to assess the noise from the mechanical services.

All other receiver locations are considered to be appropriate.

Tyre Air Fill Point

The assessment of the tyre air fill point is appropriate and we agree with the findings.

Mechanical services

We agree that the revised plant provided is appropriate in assessing the noise from a service station. However, the predicted noise levels to sensitive receivers appear to be under estimated. Assuming a distance of 27m from the convenience store roof (western end) to Residence E, and a sound pressure level of 63 dB(A) at 3m for one of the refrigeration units, we would expect a predicted noise level of approximately 44 dB(A). In addition, we would

expect a penalty for tonality, being +5 dB, which would result in an adjusted noise level of L_{A10} 49 dB. This level is likely to increase if all of the refrigeration units were operating during a warm summer night. We therefore seek clarification on the assumed location of the refrigeration units and the methodology used in the noise level predictions.

Air Compressor

We agree with the sound pressure levels assumed in the calculations. We also agree that the air compressor could run at night and that the L_{A1} assigned level would be appropriate. However, the distance from the service yard at the western end of the convenience store to Residence E is approximately 24m and if there is a clear view of the compressor from this location, the predicted noise level of 38 dB(A) appears low. Based purely on a reduction in noise as a result of the distance, we would expect a noise level of 51 dB(A). We therefore seek clarification on the methodology used in the noise level predictions. It should be noted that as part of the noise management plan, the air compressor could be turned off during the night period.

Tanker Engine Start and Air Brake Air Release

The measured noise levels for this plant appear reasonable, however, we would question the effectiveness of the noise wall if the receiver location for Residence D were within 15m of the front of the house rather than at the side of the house adjacent to the window. We believe that the noise at this location should be provided to ensure compliance with the *Environmental Protection (Noise) Regulations 1997*.



Daniel Lloyd
Director



May 2015

FINAL Rev 2

224 Clontarf Road Hamilton Hill,
Service Station Proposal

Prepared For:
Puma Energy

Transport Statement
Report



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DOCUMENT ISSUE AUTHORISATION

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0	0	30/10/2014	Draft Report	JH		
1	0	17/11/2014	Final Report	JH	DNV	DNV
1	1	24/04/2015	Additional Information	JH	DNV	DNV
1	2	28/05/2015	Additional Information	JH	DNV	DNV

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1 INTRODUCTION

1.1 BACKGROUND

This Transport Impact Statement has been prepared by Donald Veal Consultants on behalf of Puma Energy, with regard to the proposed service station and convenience store to be located at 224 Clontarf Road, Hamilton Hill. The site is located on the northwest corner of the intersection of Clontarf Road and Carrington Street, within the City of Cockburn.

The site is located approximately 22kms to the southwest of the Perth CBD and only 5kms to the southwest of the Fremantle Town Centre. Major arterial roads within close proximity include Carrington Street, Stock Road and Hampton Road, all orientated in a north-south direction and South Street and Winterfold Road, both orientated in an east west direction. The site location is shown in a regional context in **Figure 1.1** and in a local context in **Figure 1.2**.

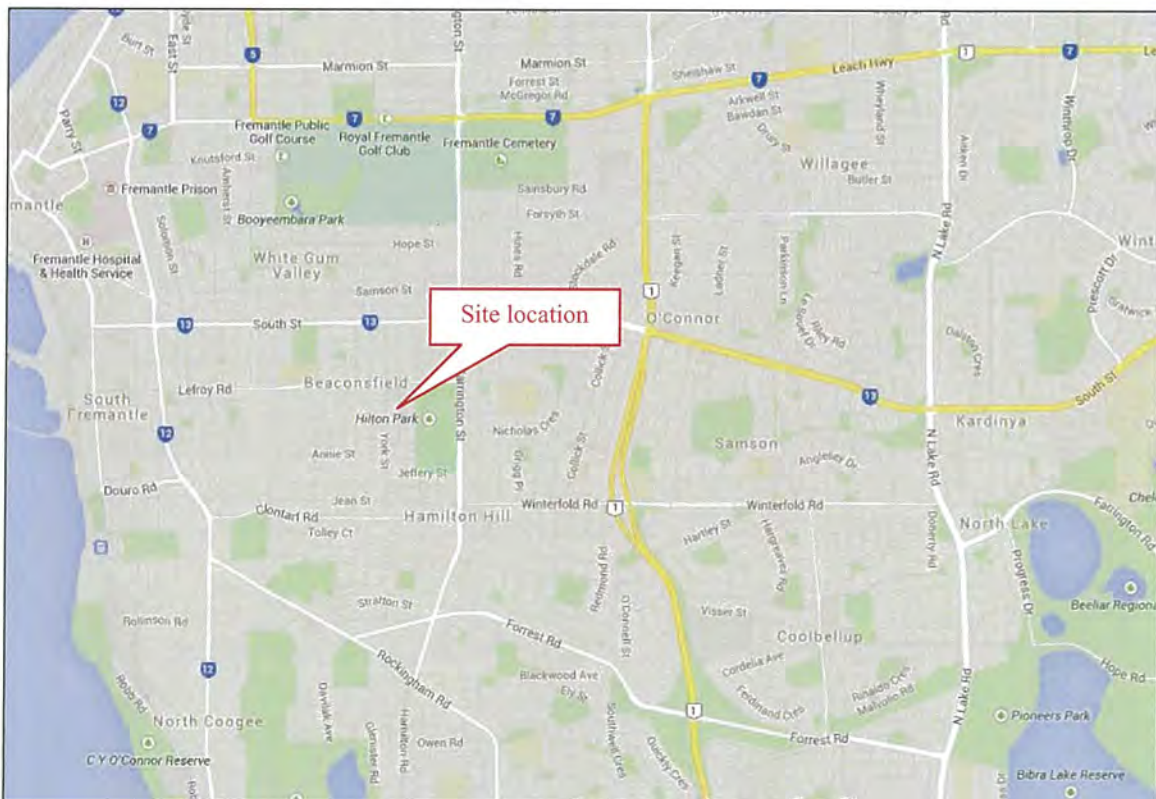


Figure 1.1: Site location – Regional context

Source: Google Maps



The level of transport assessment required is considered to be that of a 'moderate impact' development. A "moderate impact" development is one that generates between 10 -100 vehicle trips in the development's peak hour. While the overall development will attract slightly over 100 vehicle trips in the peak hour a significant percentage of trips are passing trade hence are not considered additional trips to the road network thus the development is to be assessed as a "moderate impact".

2 DEVELOPMENT PROPOSAL

2.1 PROPOSED LAND USES

The proposed Puma development on Clontarf Road is for a service station and convenience store. The development is proposed to operate from 5am to midnight, seven (7) days a week. The site will contain eight (8) fuelling bays.

The primary access to the site is via an existing crossover on Clontarf Road however secondary access is possible via an existing crossover located on Carrington Street that services a number of other businesses abutting Carrington Street.

Figure 2.1 outlines the site plan of the development.



Figure 2.1: Site Plan Layout

Source: Hindley and Associates

2.2 CONTEXT WITH SURROUNDS

The site previously operated as a fast food outlet (KFC). Fast food outlets (say 50 seats) and service station (8 pumps) have similar traffic generation.

The existing building will remain on site and be refurbished to suit the proposed development. Adjacent to the northern boundary of the site are a number of businesses such as take-away food/restaurant outlets and a tattoo parlour.

3 EXISTING ROAD NETWORK SITUATION

3.1 ROAD INFRASTRUCTURE

The southern boundary of the site runs along Clontarf Road. Clontarf Road is typically constructed to a two lane single carriageway standard. Immediately adjacent to the site the Clontarf Road eastbound lane is widened to two lanes at its approach to Carrington Street.

Carrington Street abuts the eastern boundary of the site. Adjacent to the site Carrington Street is constructed to a four-lane divided carriageway standard. Carrington Street to the south of Clontarf Road is typically constructed to a two-lane undivided standard.

Clontarf Road intersects with Carrington Street forming a t-junction under stop control. The Clontarf Road approach at Carrington Street is locally widened to accommodate separate left and right turning lanes. A right turning lane is also provided on Carrington Street at its intersection with Clontarf Road.

Figure 3.1 outlines the existing standard of Clontarf Road and Carrington Street in the vicinity of the site.



Figure 3.1: Road standard of Clontarf Road and Carrington Street in the vicinity of the site

Source: Nearmap

Immediately north (approximately 90m) of the intersection of Carrington Street and Clontarf Road is the signalised t- intersection of Carrington Street and Winterfold Road. Immediately to the south of the intersection of Carrington Street and Clontarf Road is the t-junction of Dodd Street and Carrington Street. Vehicles movements into and out of Dodd Street are restricted to left turn movement only.

Clontarf Road and Carrington Street have speed limits of 50km/h and 60km/h, respectively.

3.2 ROAD HIERARCHY CLASSIFICATION

Clontarf Road is classified as a local distributor road under Main Roads WA's *Functional Road Hierarchy*. Local distributor roads "carry traffic within a cell and link District Distributors or Primary Distributors at the boundary, to access roads. The route of Local Distributors should discourage through traffic so that the cell formed by the grid of higher order distributor roads, only carries traffic belonging to, or serving the area. Local Distributors should accommodate buses, but discourage trucks." This road is managed by the City of Cockburn.

Carrington Street is classified as a District Distributor A road under Main Roads WA's *Functional Road Hierarchy*. This classification is applied to roads which are to: "carry traffic between industrial, commercial and residential areas and generally connect to Primary Distributors." This road is managed by the City of Cockburn. **Figure 3.2** outlines the road hierarchy classification of the surrounding road network.



Figure 3.2: Road hierarchy of surrounding road network

Source: Main Roads Functional Road Hierarchy

3.3 TRAFFIC VOLUMES

The latest available traffic volumes for Clontarf Road and Carrington Street were sourced from Main Roads WA and the City of Cockburn. **Table 3.1** summarise the traffic data available.

Table 3.1: Existing Traffic Data Summary from Main Roads and City of Cockburn

Location	Date	Average Weekday Traffic				
		Daily	Am peak		Pm Peak	
			Volume	Hour	Volume	Hour
Clontarf Road, west of Carrington Street	5/2015	-	612	8-9am	574	3.30-4.30pm
Clontarf Road, west of Carrington Street	8/2011	5,639	520	8-9am	516	3-4pm
Clontarf Road, west of York Street	9/2012	5,500	498	8-9am	497	5-6pm
Carrington Street, north of Clontarf Street	5/2015	20,000*	1,802**	7.45-8.45am	1,821**	4-5pm
Carrington Street, south of Clontarf Street	5/2015	-	1,585**	7.45-8.45am	1,507**	3.45-4.45pm
Carrington Street, south of Winterfold Road	4/2013	19,316	1,243	9-10am	1,748	4-5pm

*Data derived from SCATS data on Thursday May 14 2015

** Data from traffic turn counts at Carrington St and Clontarf St on Thursday May 14 2015

A four hour traffic turn count was undertaken at the intersection of Carrington Street and Clontarf Road on May 14th, 2015. These counts were undertaken during the am and pm peak hours of 7.30-9.30am and 3.00-5.00pm. The overall peak for the intersection was 8-9am and 4-5pm however, the individual intersection legs experienced peaks as shown in **Table 3.1**.

3.4 OTHER TRAFFIC SURVEYS

To fully understand the existing traffic operation of the surrounding intersections and existing crossover, the traffic turn counts were supplemented with various queuing and gap surveys, also undertaken over the 4 hour period on May 14th, 2015. These queuing and gap surveys were used to verify the SIDRA analysis to ensure that the model was properly calibrated in the base case and therefore suitable for forecasting the situation with the development traffic. These surveys included:

- Queuing survey along the southern leg on Carrington Street at its intersection with Winterfold Road;
- Gap survey on Clontarf Road at the site crossover; and
- Gap survey on Carrington Street at its intersection with Clontarf Road.

A copy of all the traffic data is included as **Appendix A**.

4 VEHICULAR ACCESS AND PARKING

4.1 ACCESS ARRANGEMENTS

The primary access to the site is via an existing crossover on Clontarf Road located approximately 40m west from Carrington Street. All turning movements to/from the site are permitted from this primary access.

Secondary access to the site is via an existing crossover on Carrington Street that services a number of businesses abutting Carrington Street. This access permits left turn in and left turn out movements only. A left turn pocket is provided along Carrington Street.

Due to the internal layout of the development it is likely that the majority of vehicles will use the Clontarf Road access.



Figure 4.1: Access to/from the site

Source: Nearmap

4.2 SERVICE VEHICLES

4.2.1 *Service Deliveries*

Service deliveries to the site will include fuel for the service station and various goods for the convenience store.

It is proposed that refuelling tankers access the site via the secondary access on Carrington Street and exit via the primary access on Clontarf Road. This access arrangement is supported on traffic grounds for the following reasons:

- Involves left turn movements into and out of the site which is preferred over right turn movements on safety grounds;
- Clontarf Road is a lower speed road (50km/h) compared to Carrington Street (60km/h) hence the gap distance required in the traffic for a tanker to enter is less. Additionally, all vehicles along Clontarf Road will be slowing down as all vehicles need to stop and either turn left or right onto Carrington Street (i.e. there is no through movement occurring at 50km/h).

The swept path for a 19m semi trailer entering from Carrington Street and exiting Clontarf Street is shown in **Figure 4.2**. The refuelling tanker will be able to stop along the western boundary of the site to refuel without blocking the access on Clontarf Road or disrupting general access to the refuelling bowsers used by the public.

The swept path of the tanker demonstrates that the existing kerbline will need to be modified to accommodate the turning tanker. The existing southern radius is approximately 5m with a driveway width of 6m. Typically, for a driveway to cater for a single unit truck (12.5m) i.e. a standard delivery vehicle, its design typically comprises of 7m turnout radii and a 9m wide crossing. Refer Main Roads WA driveway drawing for single units 200431 0200-1. The existing driveway provided does not meet these typical standards hence the need for it to be modified.

The Main Roads WA driveway policy states the following:

"Service stations on a corner lot may have one driveway up to 11.0m wide to a State road and another to the minor road. Those not on a corner may have two driveways, each up to 11.0m wide."

The proposed geometric driveway modifications involve an increased radius and driveway width. The driveway width varies but between the centreline of the pedestrian ramps it measures approximately 9m. This driveway is therefore not considered to be excessive as it is less than the standard 11m permitted for service stations.

A review of the surrounding driveways along this length of Carrington Street indicates that there are a number of driveways wider than the 9m driveway proposed. These driveways are shown in **Figure 4.5**.

The modified pedestrian ramps will need to be constructed to comply with appropriate standards to meet grade and width requirements. The proposed crossing width of 8m is not considered to be excessive on this low volume access driveway.

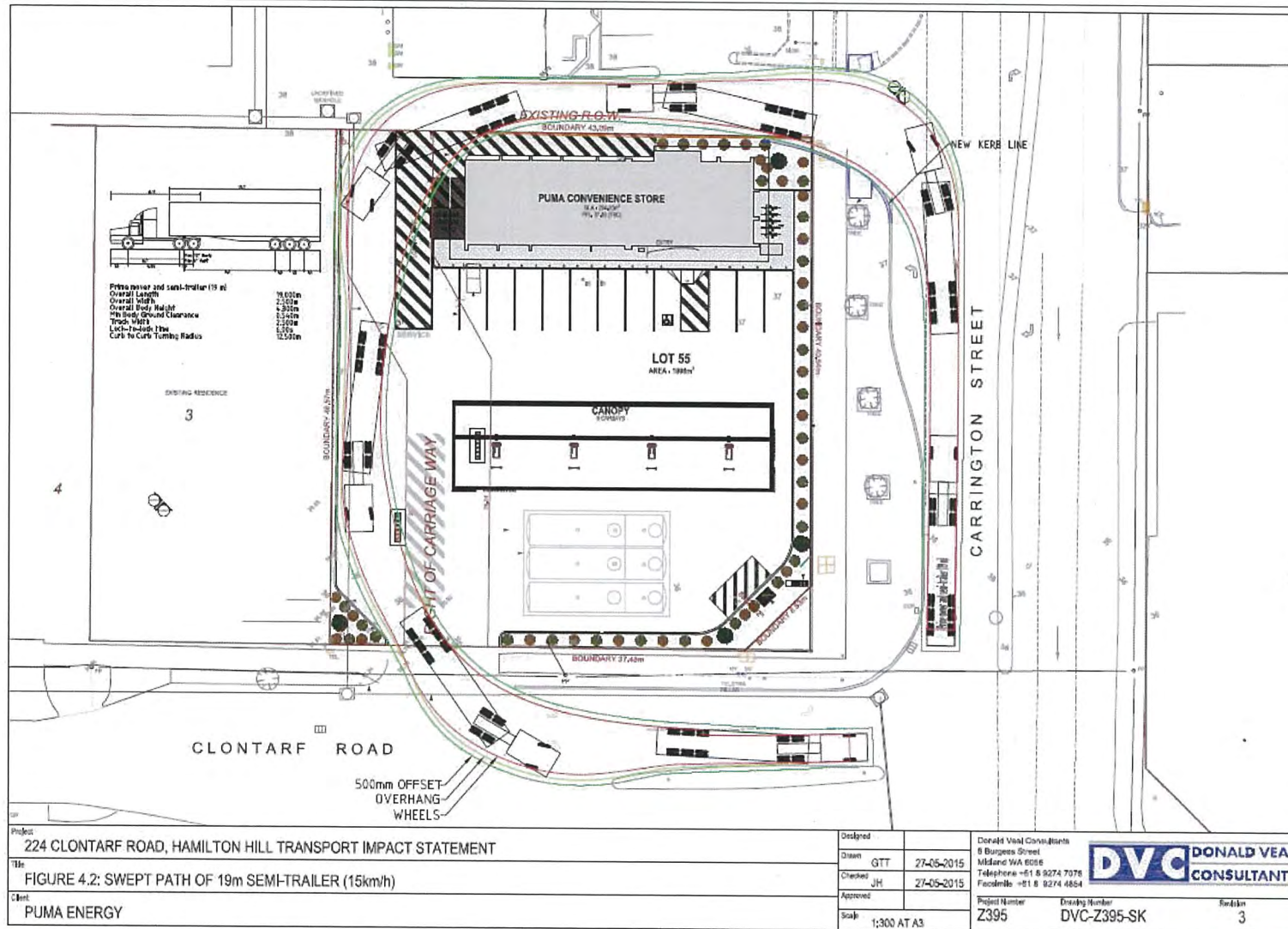


Figure 4.2: Swept path of refuelling vehicle entering Carrington Street and exiting Clontarf Road



Figure 4.5: Various Driveway Widths along Carrington Street

Source: Nearmap

Reversing the tanker access to enter from Clontarf Road and exiting Carrington Street is not the preferred access arrangement due to the following additional reasons:

- Tankers are required to undertake a right hand turn from Clontarf Road into the site. Whilst there is a painted median which allows for the storage of 1-2 typical vehicles it is unlikely that a 19m tanker would be able to manoeuvre into this space and could disrupt Clontarf Road through traffic.
- To avoid tankers turning right into the Clontarf Road crossover would mean that tankers would need to approach from the west along Clontarf Road. Using Clontarf Road, a local distributor road as an approach route is less desirable with respect to amenity issues compared to using Carrington Street, a Distributor A road.

A loading zone is provided adjacent to the site to allow for deliveries of goods as required by the convenience store. The swept path of a delivery vehicle is shown in **Figure 4.4**.

4.2.2 Rubbish Collection

Rubbish collection would be undertaken within the site and not along the road network. Rubbish bins would be located adjacent to the loading zone near the deliveries. The generous loading zone area would be adequate for a garbage truck to manoeuvre to collect rubbish and exit the site. The swept path of a delivery vehicle is shown in **Figure 4.3**.



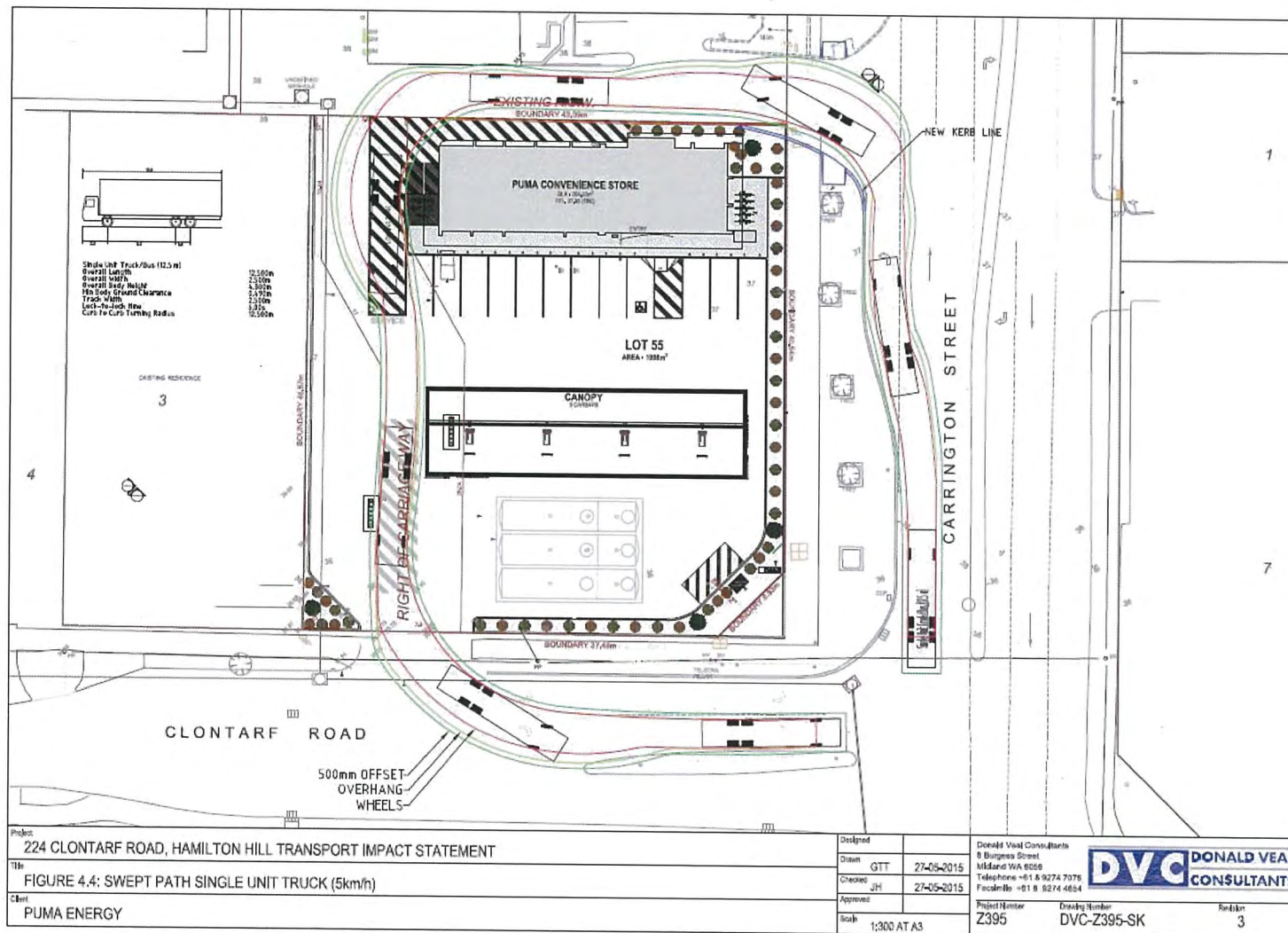


Figure 4.4: Swept path of service delivery vehicle entering Clontarf Road and exiting Carrington Street

4.3 SIGHT DISTANCE

Sight lines from the existing Clontarf Road crossover were inspected. Sight lines from this crossover primarily servicing the site are good.

Photos 1 and 2 show the typical sight lines from the existing site access along Clontarf Road.



Photo 1: Sight lines from existing Clontarf Road access to the west along Clontarf Road



Photo 2: Sight lines from existing Clontarf Road access to the east along Clontarf Road

Three different sight distances are generally reviewed for property accesses. These are:

- Approach Sight Distance (ASD);
- Safe Intersection Sight Distance (SISD); and
- Minimum Gap Sight Distance (MGSD).

Table 4.1 summarises the calculations of these various sight distances as determined from Austroads guidelines "*Guide to Road Design, Part 4A Unsignalised and Signalised Intersections*". A design speed of 60km/h (posted + 10km/h) was used for the ASD and SISD calculations with a reaction time of between 1.5 -2.0 seconds. For the MGSD calculation the 85th percentile speed was assumed to be the posted speed i.e. 50km/h. The gap acceptance for a left and right turn into traffic is typically 5 seconds. For the sight lines to the east a speed of 25km/h is used as approaching vehicles will have just turned onto Clontarf Road. (Refer **photo 2**).

Table 4.1: Calculation of various Sight Distances

	Speed	Reaction Time (sec) / Gap Acceptance (sec)	Sight Distance (m)
Approach Sight Distance	60km/h	1.5-2.0	64-73
Safe Intersection Sight Distance	60km/h	1.5-2.0	114-123
Minimum Gap Sight Distance	50km/h	5	69
Approach Sight Distance	25km/h	1.5-2.0	17-20
Safe Intersection Sight Distance	25km/h	1.5-2.0	38-42
Minimum Gap Sight Distance	25km/h	5	41

Figure 4.6 theoretically demonstrates that there are clear sight lines along Clontarf Road towards Tuart Street. The Austroads guidelines "*Guide to Road Design, Part 4A Unsignalised and Signalised Intersections*" state that sight lines should be measured 3-5m back from the intersection. Using a 4m setback, the safe intersection sight distance (SISD) of 123m as a minimum can be achieved as shown on **Figure 4.5**. This is also supported by the site visit as shown in **Photo 1**. Accordingly, the available sight distance can be considered adequate for this situation.



Figure 4.6: Sight lines to the west from crossover along Clontarf Road

It is noted that there is a bus stop on the northern side of the eastbound carriageway located approximately 64m from the driveway exit. A stopped bus will temporarily restrict the sight lines from the driveway exit. Due to the road geometry upstream of the bus and the approaching intersection with Carrington Street a vehicle overtaking a bus is likely to be travelling at 40km/h or less. The corresponding approach sight distance is 34-40m meaning that a vehicle overtaking the bus has adequate distance (bus stop approximately 64m away) to observe a vehicle pulling out of the driveway and stop in time. (Refer **Figure 4.7**) Similarly, the minimum gap sight distance for a vehicle approaching at 40km/h is 55m (bus stop approximately 64m away) suggesting that vehicles from the driveway will have adequate distance and time to enter Clontarf Road when a bus is temporarily stopped at the bus stop.



Figure 4.7: Sight lines between overtaking vehicle and vehicle exiting driveway

5 TRAFFIC ASSESSMENT

In order to assess the potential traffic impacts associated with the service station and convenience store, a traffic generation and distribution exercise was undertaken. This exercise established the level of traffic that would be generated from the proposed development and enabled the effect that the additional traffic has on the adjacent road network to be assessed.

5.1 TRAFFIC GENERATION

In order to determine traffic generation for the proposed development, documented trip generation rates were sourced from "*Trip Generation Manual, 8th Edition, Institution of Transportation Engineers (2003).*" The documented rates are as follows:

- Weekday Trip Rate = 163 trips/ refuelling position
- AM peak hour Trip Rate = 10.2 trips/ refuelling position
- PM peak hour Trip Rate = 13.4 trips/ refuelling position

Based on the above data the trip generation for the proposed service station and convenience store is estimated to be as follows:

- Daily Trips = 1,304 trips /day
- AM Peak Hour Trips = 82 trips /hour
- PM peak Hour Trips = 107 trips /hour

Service stations typically attract a significant percentage of passing through trade. This traffic is already on the road network hence it is not considered as additional traffic. Data suggests that approximately 56% of service station trips are typically passing trade.

Based on the likelihood of passing trade, the additional trips generated by the development are expected to be considerably less, being:

- Daily Trips = 574 trips /day
- AM Peak Hour Trips = 36 trips /hour
- PM peak Hour Trips = 47 trips /hour

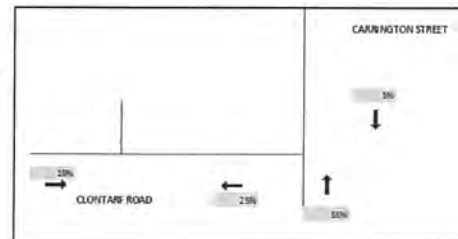
Data also indicates that the inbound and outbound trip breakdown during the peak hours is even with 50% inbound and 50% outbound.

5.2 TRAFFIC DISTRIBUTION

For the purpose of the traffic distribution two trip types are used i.e. passing trade trips and additional trips. The traffic was distributed onto the service station access and road network for these two trip types.

The passing trade traffic is already on the road network passing the site and elects to stop on the way past. The majority of passing trade was considered to come from Carrington Street as this road carries higher volumes of traffic than Clontarf Road. The percentages have been based on the passing traffic flows with only a small percentage allocated to Carrington Street southbound as passing trade on a divided carriageway is likely to look for a service station on the same side of the road. The resulting distribution of trips has therefore been assumed to be as follows:

- 55% northbound along Carrington Street;
- 25% westbound along Clontarf Road from Carrington Street (north or south);
- 15% eastbound along Clontarf Road and
- 5% southbound along Carrington Street.



The additional traffic is those trips that are made specifically to the site. Subsequently, their approach route and departure route is considered to be the same. For example a trip approaching from Clontarf Road from the west of the site would depart along Clontarf Road towards the west. Based on the residential catchment, similar land uses and the road network classification the distribution patterns adopted for the additional trips are as follows:

- 25% to/from Clontarf Road west of the site;
- 30% to/from Carrington Street north of the site; and
- 45% to/from Carrington Street south of the site.

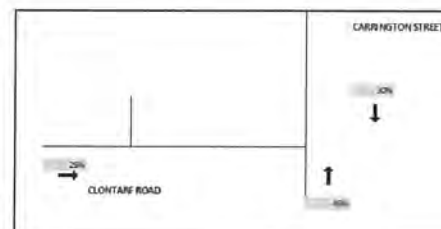


Figure 5.1 to 5.3 show the likely traffic distribution for the AM peak hour, PM peak hour and daily scenarios respectively.

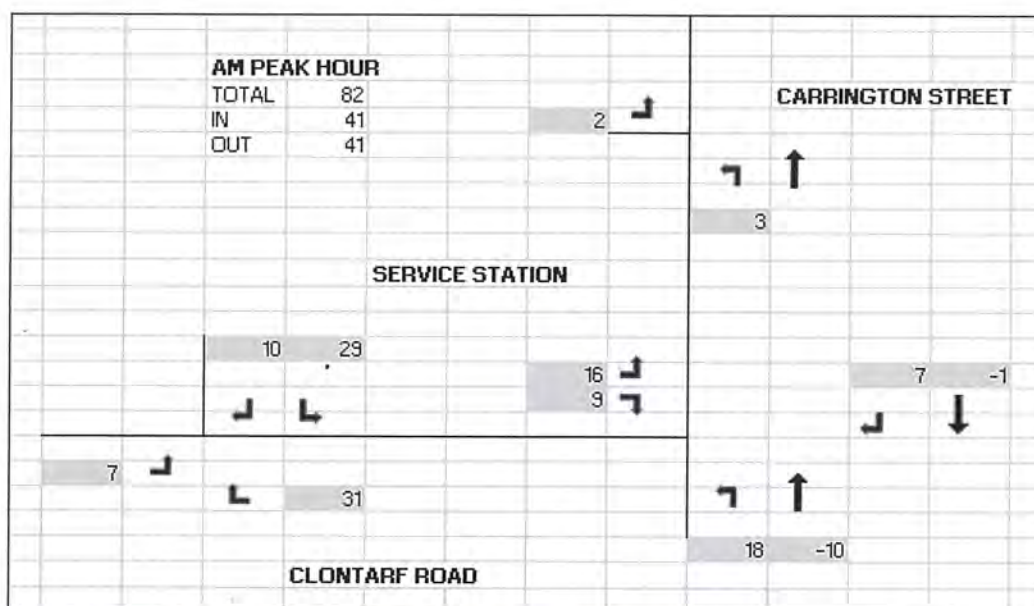


Figure 5.1: Am peak hour distribution of development traffic

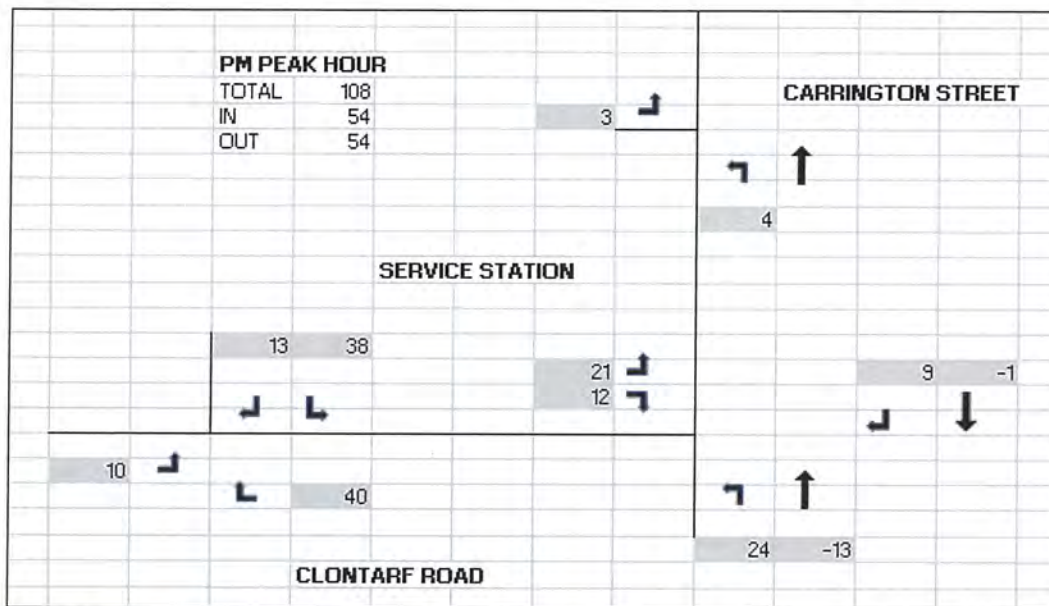


Figure 5.2: Pm peak hour distribution of development traffic

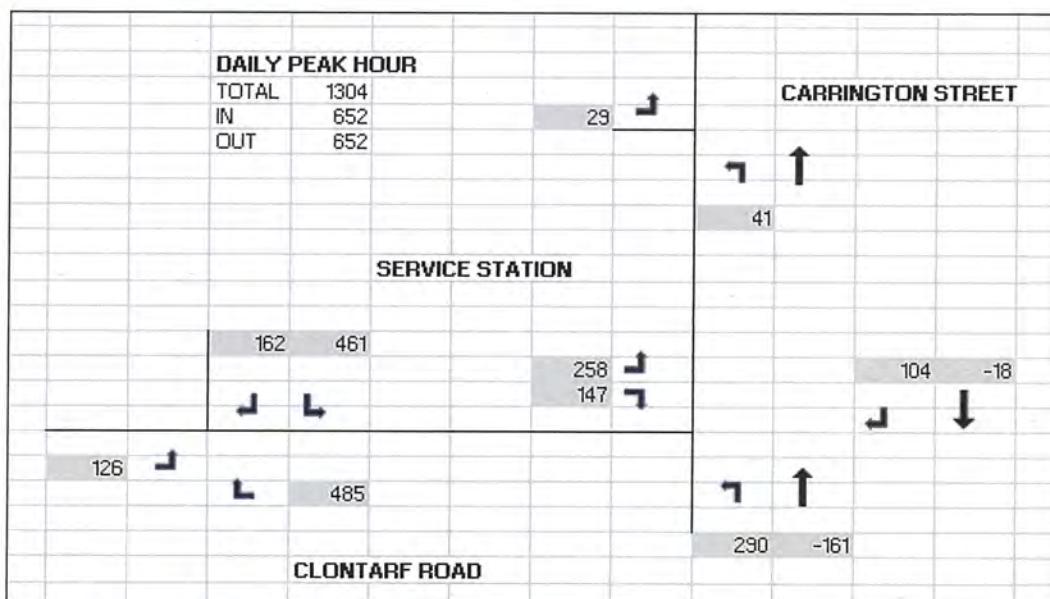


Figure 5.3: Daily distribution of development traffic

5.3 IMPACT ON LOCAL ROAD NETWORK

Clontarf Road is classified as a *Local Distributor Road* and operates under a 50 km/h speed zone. The road has been constructed to a two lane single carriageway standard, (although locally widened at its intersection with Carrington Street) and carries in the order of 5,600 vehicles per day (August 2011).

The expected increases in traffic from the proposed development of approximately 142 vehicles per day can be readily accommodated within the practical capacity of Clontarf Road, in the context of its current standard, with no significant impact on existing traffic operations.

Carrington Street currently carries in the order of 20,000 vehicles per day (May 2015). The likely increase of 258 and 172 per day along Carrington Street both south and north of Clontarf Road respectively can be readily accommodated within the practical capacity of Carrington Street, in the context of its current standard, with no significant impact on existing traffic operations.

5.4 SIDRA ANALYSIS OF ACCESS

Intersection capacity analysis using the SIDRA computer package (*version 6.1*) was undertaken at the intersection of Clontarf Road and the development access to assess the intersection's traffic operations and in particular model the likely traffic queues.

SIDRA is a commonly used intersection-modelling tool by traffic engineers for all types of intersections. SIDRA outputs are presented in the form of Degree of Saturation, Level of Service, Average Delay and 95% Queue. These characteristics are defined as follows:

Degree of Saturation: is the ratio of the arrival traffic flow to the capacity of the approach during the same period. The Degree of Saturation ranges from close to zero for varied traffic flow up to one for saturated flow or capacity.

Level of Service (LOS): is the qualitative measure describing operational conditions within a traffic stream and the perception by motorists and/or passengers. In general, there are 6 levels of services, designated from A to F, with Level of Service A representing the best operating condition (i.e. free flow) and Level of Service F the worst (i.e. forced or breakdown flow).

Average Delay: is the average of all travel time delays for vehicles through the intersection.

95% Queue: is the queue length below which 95% of all observed queue lengths fall.

The analysis was undertaken for the am and pm peak period of the road network.

5.4.1 Clontarf Road and Development Access

The results of the SIDRA analysis for the peak hour conditions for the intersection are detailed in **Tables 5.1 and 5.2**. These SIDRA results indicate that proposed access will operate satisfactorily. The 95% queue length within the westbound lane on Clontarf Road during the peak periods is 1 vehicle. There is adequate length for this vehicle without blocking the intersection of Clontarf Road and Carrington Street. The provision of a painted median island along Clontarf Road near the access would allow storage of right turning traffic and allow through traffic to pass.

Table 5.1: SIDRA Output: Clontarf Road and Development Access Am Peak

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
East: Clontarf Road											
5	T1	419	5.0	0.264	0.0	LOS A	0.2	1.5	0.05	0.02	43.9
6	R2	33	0.0	0.264	2.9	LOS A	0.2	1.5	0.05	0.02	30.0
Approach		452	4.6	0.264	0.2	NA	0.2	1.5	0.05	0.02	43.4
North: Site Access											
7	L2	31	0.0	0.027	0.5	LOS A	0.1	0.7	0.28	0.18	25.3
9	R2	11	0.0	0.027	1.8	LOS A	0.1	0.7	0.28	0.18	40.4
Approach		41	0.0	0.027	0.8	LOS A	0.1	0.7	0.28	0.18	31.8
West: Clontarf Road											
10	L2	7	0.0	0.132	4.6	LOS A	0.0	0.0	0.00	0.02	40.7
11	T1	216	5.0	0.132	0.0	LOS A	0.0	0.0	0.00	0.02	49.8
Approach		223	4.8	0.132	0.2	NA	0.0	0.0	0.00	0.02	49.5
All Vehicles		716	4.4	0.264	0.3	NA	0.2	1.5	0.05	0.03	44.8

Table 5.2: SIDRA Output: Clontarf Road and Development Access, Pm Peak

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
East: Clontarf Road											
5	T1	336	5.0	0.220	0.1	LOS A	0.3	1.9	0.08	0.03	43.5
6	R2	42	0.0	0.220	2.9	LOS A	0.3	1.9	0.08	0.03	29.4
Approach		378	4.4	0.220	0.4	NA	0.3	1.9	0.08	0.03	42.9
North: Site Access											
7	L2	40	0.0	0.035	0.6	LOS A	0.1	0.9	0.30	0.20	25.2
9	R2	14	0.0	0.035	1.7	LOS A	0.1	0.9	0.30	0.20	40.3
Approach		54	0.0	0.035	0.9	LOS A	0.1	0.9	0.30	0.20	31.6
West: Clontarf Road											
10	L2	11	0.0	0.150	4.6	LOS A	0.0	0.0	0.00	0.02	40.7
11	T1	243	5.0	0.150	0.0	LOS A	0.0	0.0	0.00	0.02	49.8
Approach		254	4.8	0.150	0.2	NA	0.0	0.0	0.00	0.02	49.3
All Vehicles		685	4.2	0.220	0.4	NA	0.3	1.9	0.07	0.04	44.7

The site gap surveys undertaken during the am and pm peak hours were used to verify that the SIDRA results accurately model vehicle movements in and out of the site driveway. It is acknowledged that during the peak hours the queue along Clontarf Street from its intersection with Carrington Street does at times block the driveway. The gap surveys took into account any queuing that blocked vehicle movements from the crossover. While the Clontarf Road queue extended to the crossover, temporarily restricting access, the queue does clear quickly and provides numerous gaps to allow for traffic to exit/enter the site.

During the am peak hour the site gap surveys indicated that there were sufficient gaps in the eastbound traffic at the driveway to allow 715 vehicles to turn left from the driveway onto Clontarf Road and/or to turn right from Clontarf Road into the site. The observed gaps in the traffic in both directions are sufficient to allow 417 vehicles to turn right out of the site onto Clontarf Road. All these estimates use an acceptance gap of 6 seconds and a follow up headway of 3 seconds for all movements. This is considered to be conservative. The 715 vehicles per hour gap capacity within the eastbound traffic stream along Clontarf Road is more than adequate to

allow for the 29 vehicles (left turn from site) and 31 vehicles (right turn into site). Similarly, the 417 vehicle per hour capacity within the westbound and eastbound traffic streams is more than adequate to allow for the 10 vehicles within the peak hour to exit right onto Clontarf Road even on the basis that the available capacity (417) will be reduced slightly by the right turning vehicles into the site (31).

During the pm peak hour the site gap surveys indicated that there were sufficient gaps in the eastbound traffic at the driveway to allow 758 vehicles to turn left from the driveway onto Clontarf Road and/or to turn right from Clontarf Road into the site. The observed gaps in the traffic in both directions are sufficient to allow 415 vehicles to turn right out of the site onto Clontarf Road. All these estimates use an acceptance gap of 6 seconds and a follow up headway of 3 seconds for all movements. This is considered to be conservative. The 758 vehicles per hour gap capacity within the eastbound traffic stream along Clontarf Road is more than adequate to allow for the 38 vehicles (left turn from site) and 40 vehicles (right turn into site). Similarly, the 415 vehicle per hour capacity within the westbound and eastbound traffic streams is more than adequate to allow for the 13 vehicles within the peak hour to exit right onto Clontarf Road even on the basis that the available capacity (415) will be reduced slightly by the right turning vehicles into the site (40).

In general, and considering the limitation in both observed data and the SIDRA model, the measured gap analysis demonstrates that the driveway will operate satisfactorily as shown in the SIDRA results.

5.4.2 Clontarf Road and Carrington Street

The results of the SIDRA analysis for the peak hour conditions for the intersection of Clontarf Road and Carrington Street under the existing conditions are detailed in **Table 5.3** and **Table 5.4**. The site geometric layout is such that it does allow right turning vehicles from Clontarf Road to cross Carrington Street in two stages if required. The SIDRA analysis has been undertaken on this basis.

During the am peak hour the highest degree of saturation is 0.719 associated with the right turn from Carrington Street into Clontarf Road. This movement also experiences the highest average delay of 25 seconds. During the pm peak hour the highest degree of saturation is 0.395 associated with the left turn into Clontarf Road. The highest average delay is 15 seconds associated with the right turn from Clontarf Road onto Carrington Street.

The gap analysis was used to confirm the validity of these SIDRA results. Over the peak hour the observed gap time between vehicles travelling northbound and southbound along Carrington Street was recorded. An estimate of the number of vehicles that could enter these gaps was then estimated. **Table 5.5** summarises the number of gaps, the number of cars that could enter the traffic, and a comparison of the degree of saturation based on the gap assessment and that from the SIDRA analysis. The results demonstrate that the degree of saturation using the observed gaps and those calculated in SIDRA are comparable confirming the validity of the SIDRA results with respect to the existing situation.

Table 5.3: SIDRA Output: Clontarf Road and Carrington Street, Am Peak Existing Traffic

Movement Performance - Vehicles													
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Arrival Flows Total veh/h	HV %	DoS veh	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
Clontarf Road (Stage 1)													
1	L2	186	2.0	186	2.0	0.508	19.4	LOS C	2.4	16.9	0.85	1.05	44.5
2	T1	33	5.0	33	5.0	0.153	20.6	LOS C	0.5	3.4	0.86	0.93	36.0
Approach		219	2.4	219	2.4	0.508	19.6	LOS C	2.4	16.9	0.85	1.03	43.7
Carrington Street (south)													
4	L2	165	2.0	165	2.0	0.586	5.5	LOS A	0.0	0.0	0.00	0.09	57.3
5	T1	937	5.0	937	5.0	0.586	0.0	LOS A	0.0	0.0	0.00	0.09	58.9
Approach		1102	4.6	1102	4.6	0.586	1.0	NA	0.0	0.0	0.00	0.09	58.7
Carrington St (north)													
12	R2	260	2.0	260	2.0	0.719	24.6	LOS C	3.5	24.9	0.93	1.16	41.9
Approach		260	2.0	260	2.0	0.719	24.6	NA	3.5	24.9	0.93	1.16	41.9
All Vehicles		1581	3.8	1581	3.8	0.719	7.3	NA	3.5	24.9	0.27	0.40	52.8

Movement Performance - Vehicles													
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Arrival Flows Total veh/h	HV %	DoS veh	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
Median Storage Area													
3	R2	33	2.0	33	2.0	0.033	1.4	LOS A	0.1	0.6	0.39	0.27	50.8
Approach		33	2.0	33	2.0	0.033	1.4	LOS A	0.1	0.6	0.39	0.27	50.8
Carrington Street (North)													
11	T1	508	5.0	508	5.0	0.135	0.0	LOS A	0.0	0.0	0.00	0.00	60.0
Approach		508	5.0	508	5.0	0.135	0.0	NA	0.0	0.0	0.00	0.00	60.0
All Vehicles		541	4.8	541	4.8	0.135	0.1	NA	0.1	0.6	0.02	0.02	59.8

Note: Right turn from Clontarf Road onto Carrington Street: DoS = 0.153, Average Delay = 22seconds, LoS = C

Table 5.4: SIDRA Output: Clontarf Road and Carrington Street, Pm Peak Existing Traffic

Movement Performance - Vehicles													
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Arrival Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
Clontarf Road (Stage 1)													
1	L2	193	2.0	193	2.0	0.310	11.2	LOS B	1.4	10.0	0.65	0.89	49.5
2	T1	55	5.0	55	5.0	0.134	12.2	LOS B	0.5	3.3	0.72	0.86	43.0
Approach		247	2.7	247	2.7	0.310	11.4	LOS B	1.4	10.0	0.67	0.89	48.5
Carrington Street (south)													
4	L2	73	2.0	73	2.0	0.395	5.6	LOS A	0.0	0.0	0.00	0.06	57.7
5	T1	671	5.0	671	5.0	0.395	0.0	LOS A	0.0	0.0	0.00	0.06	59.3
Approach		743	4.7	743	4.7	0.395	0.6	NA	0.0	0.0	0.00	0.06	59.2
Carrington St (north)													
12	R2	272	2.0	272	2.0	0.362	10.9	LOS B	1.6	11.4	0.68	0.91	49.7
Approach		272	2.0	272	2.0	0.362	10.9	NA	1.6	11.4	0.68	0.91	49.7
All Vehicles		1262	3.7	1262	3.7	0.395	4.9	NA	1.6	11.4	0.26	0.40	54.7

Movement Performance - Vehicles													
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Arrival Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
Median Storage Area													
3	R2	55	2.0	55	2.0	0.068	2.6	LOS A	0.2	1.3	0.49	0.43	49.1
Approach		55	2.0	55	2.0	0.068	2.6	LOS A	0.2	1.3	0.49	0.43	49.1
Carrington Street (North)													
11	T1	782	5.0	782	5.0	0.207	0.0	LOS A	0.0	0.0	0.00	0.00	60.0
Approach		782	5.0	782	5.0	0.207	0.0	NA	0.0	0.0	0.00	0.00	60.0
All Vehicles		837	4.8	837	4.8	0.207	0.2	NA	0.2	1.3	0.03	0.03	59.5

Note: Right turn from Clontarf Road onto Carrington Street: DoS = 0.134, Average Delay = 15seconds, LoS = B

Table 5.5: Comparison of Degree of Saturation using measured gaps and SIDRA

	AM Peak Hour		PM Peak Hour	
	Left from Clontarf	Right from Median	Left from Clontarf	Right from Median
No. of Gaps	261	147	264	168
No. Cars Out	355	941	571	721
Turning Flow	177	31	183	52
Gap DoS	0.499	0.033	0.320	0.072
SIDRA DoS	0.508	0.033	0.310	0.068

The results of the SIDRA analysis for the peak hour conditions for the intersection of Clontarf Road and Carrington Street under the proposed conditions with the development traffic are detailed in **Table 5.6 and Table 5.7**. There is minimal difference in the operation of the intersection with the additional traffic from the development. The additional traffic represents approximately a 3% and 4% increase in traffic volumes already passing through the intersection in the am and pm peak hours, hence the minimal difference in the results.

During and am peak hour the highest degree of saturation increases from 0.719 under the existing conditions to 0.755 with the development traffic. This right turn from Carrington Street into Clontarf

Road still experiences the highest average delay of 26 seconds (a 1 second increase from the existing situation) within the am peak hour. During the pm peak hour the highest degree of saturation increases from 0.395 under the existing conditions to 0.403 with the development traffic associated with the left turn into Clontarf Road. The highest average delay remains unaltered with 15 seconds associated with the right turn from Clontarf Road onto Carrington Street. The calculated increases in queue lengths are negligible (0.4 vehicles and 0.1 vehicles in the am and pm peak) hence the existing queues are expected to remain the same. Based on this the observed gaps at the site crossover taking into account the existing queues would still be appropriate hence confirming the satisfactory operation of the crossover.

Table 5.6: SIDRA Output: Clontarf Road and Carrington Street, Am Peak with Development Traffic

Movement Performance - Vehicles													
Mov ID	OD Mov	Demand Flow Total veh/h	Flow HV %	Arrival Flow Total veh/h	Flow HV %	Deg. Satn veh	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
Clontarf Road (Stage 1)													
1	L2	293	2.0	293	2.0	0.541	19.7	LOS C	2.6	18.7	0.86	1.07	44.4
2	T1	42	5.0	42	5.0	0.199	21.6	LOS C	0.6	4.6	0.87	0.95	35.3
Approach		245	2.5	245	2.5	0.541	20.0	LOS C	2.6	18.7	0.86	1.05	43.3
Carrington Street (south)													
4	L2	184	2.0	184	2.0	0.591	5.5	LOS A	0.0	0.0	0.00	0.10	57.2
5	T1	926	5.0	926	5.0	0.591	0.0	LOS A	0.0	0.0	0.00	0.10	58.9
Approach		1111	4.5	1111	4.5	0.591	1.0	NA	0.0	0.0	0.00	0.10	58.6
Carrington St (north)													
12	R2	267	2.0	267	2.0	0.755	26.4	LOS D	3.9	27.5	0.94	1.20	41.0
Approach		267	2.0	267	2.0	0.755	26.4	NA	3.9	27.5	0.94	1.20	41.0
All Vehicles		1623	3.8	1623	3.8	0.755	8.0	NA	3.9	27.5	0.29	0.42	52.2

Movement Performance - Vehicles													
Mov ID	OD Mov	Demand Flow Total veh/h	Flow HV %	Arrival Flow Total veh/h	Flow HV %	Deg. Satn veh	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
Median Storage Area													
3	R2	42	2.0	42	2.0	0.042	1.4	LOS A	0.1	0.8	0.39	0.27	50.8
Approach		42	2.0	42	2.0	0.042	1.4	LOS A	0.1	0.8	0.39	0.27	50.8
Carrington Street (North)													
11	T1	507	5.0	507	5.0	0.134	0.0	LOS A	0.0	0.0	0.00	0.00	60.0
Approach		507	5.0	507	5.0	0.134	0.0	NA	0.0	0.0	0.00	0.00	60.0
All Vehicles		549	4.8	549	4.8	0.134	0.1	NA	0.1	0.8	0.03	0.02	59.5

Note: Right turn from Clontarf Road onto Carrington Street: DoS = 0.199, Average Delay = 23seconds, LoS = C

Table 5.7: SIDRA Output: Clontarf Road and Carrington Street, Pm Peak with Development Traffic

Movement Performance - Vehicles													
Mov ID	OD Mov	Demand Flows Total veh/s	Flows HV %	Arrival Flows Total veh/s	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queue	Effective Stop Rate per veh	Average Speed km/h
Clontarf Road (Stage 1)													
1	L2	215	2.0	215	2.0	0.340	11.3	LOS B	1.6	11.5	0.66	0.91	49.4
2	T1	67	5.0	67	5.0	0.168	12.5	LOS B	0.6	4.2	0.73	0.87	42.7
Approach		282	2.7	282	2.7	0.340	11.6	LOS B	1.6	11.5	0.67	0.90	48.3
Carrington Street (south)													
4	L2	98	2.0	98	2.0	0.403	5.6	LOS A	0.0	0.0	0.00	0.00	57.5
5	T1	881	5.0	881	5.0	0.403	0.0	LOS A	0.0	0.0	0.00	0.00	59.2
Approach		759	4.6	759	4.6	0.403	0.8	NA	0.0	0.0	0.00	0.00	59.0
Carrington St (north)													
12	R2	281	2.0	281	2.0	0.385	11.2	LOS B	1.7	12.4	0.70	0.93	49.4
Approach		281	2.0	281	2.0	0.385	11.2	NA	1.7	12.4	0.70	0.93	49.4
All Vehicles		1322	3.7	1322	3.7	0.403	5.3	NA	1.7	12.4	0.29	0.43	54.4

Movement Performance - Vehicles													
Mov ID	OD Mov	Demand Flows Total veh/s	Flows HV %	Arrival Flows Total veh/s	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queue	Effective Stop Rate per veh	Average Speed km/h
Median Storage Area													
3	R2	67	2.0	67	2.0	0.084	2.7	LOS A	0.3	1.6	0.50	0.44	10.7
Approach		67	2.0	67	2.0	0.084	2.7	LOS A	0.3	1.6	0.50	0.44	10.7
Carrington Street (North)													
11	T1	781	5.0	781	5.0	0.207	0.0	LOS A	0.0	0.0	0.00	0.00	60.0
Approach		781	5.0	781	5.0	0.207	0.0	NA	0.0	0.0	0.00	0.00	60.0
All Vehicles		848	4.8	848	4.8	0.207	0.2	NA	0.3	1.6	0.04	0.04	59.6

Note: Right turn from Clontarf Road onto Carrington Street: DoS = 0.168, Average Delay = 15seconds, LoS = C

5.4.3 Carrington Street and Winterfold Road

SIDRA analysis under the peak hour conditions for the intersection with the existing traffic volumes and those with the proposed development are detailed in **Table 5.8 to 5.11**. SCATS data from Thursday, May 14th was used as the base data for the existing intersection analysis. There is negligible difference in the operation of the signalised intersection with the additional traffic from the development. The additional traffic represents less than 1% increase in traffic volumes already passing through the intersection, hence the negligible difference in the results.

In summary, the signalised intersection operates at a satisfactory level of service C for both the am and pm peak hour. The highest degree of saturation is in the am peak remains 0.851 associated with the right turn from Winterfold Road. During the pm peak the highest degree of saturation alters slightly from 0.884 to and 0.894 associated with the right turn into Winterfold Road. The average delay is 20 and 24 seconds during the am and pm peak periods.

Table 5.8: SIDRA Output: Carrington Street and Winterfold Road, Am Peak, Existing

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Carrington St											
2	T1	782	5.0	0.383	7.3	LOS A	5.6	40.9	0.62	0.53	53.6
3	R2	315	5.0	0.798	30.3	LOS C	8.5	62.4	1.00	0.96	39.2
Approach		1097	5.0	0.798	13.9	LOS B	8.5	62.4	0.73	0.66	48.5
East: Winterfold Road											
4	L2	265	5.0	0.264	11.8	LOS B	3.4	24.7	0.55	0.72	49.0
6	R2	336	5.0	0.851	33.2	LOS C	9.8	71.4	1.00	1.01	37.9
Approach		601	5.0	0.851	23.8	LOS C	9.8	71.4	0.80	0.88	42.1
North: Carrington St											
7	L2	123	5.0	0.838	32.8	LOS C	8.8	64.5	1.00	1.03	39.6
8	T1	503	5.0	0.838	27.1	LOS C	9.0	65.7	1.00	1.03	41.1
Approach		626	5.0	0.838	28.3	LOS C	9.0	65.7	1.00	1.03	40.8
All Vehicles		2324	5.0	0.851	20.3	LOS C	9.8	71.4	0.82	0.81	44.5

Table 5.9: SIDRA Output: Carrington Street and Winterfold Road, Pm Peak, Existing

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Carrington St											
2	T1	563	5.0	0.248	5.1	LOS A	3.3	23.7	0.50	0.42	55.4
3	R2	285	5.0	0.884	37.0	LOS D	8.8	64.1	1.00	1.08	36.6
Approach		848	5.0	0.884	15.8	LOS B	8.8	64.1	0.67	0.64	47.2
East: Winterfold Road											
4	L2	276	5.0	0.334	15.1	LOS B	4.4	32.0	0.68	0.76	46.9
6	R2	245	5.0	0.855	35.1	LOS D	7.2	52.6	1.00	1.02	37.2
Approach		521	5.0	0.855	24.5	LOS C	7.2	52.6	0.83	0.88	41.8
North: Carrington St											
7	L2	212	5.0	0.883	34.0	LOS C	15.1	110.2	1.00	1.12	39.0
8	T1	778	5.0	0.883	28.3	LOS C	15.4	112.4	1.00	1.12	40.5
Approach		989	5.0	0.883	29.5	LOS C	15.4	112.4	1.00	1.12	40.2
All Vehicles		2359	5.0	0.884	23.5	LOS C	15.4	112.4	0.84	0.90	42.9

Table 5.10: SIDRA Output: Carrington Street and Winterfold Road, Am Peak Proposed

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Carrington St											
2	T1	785	5.0	0.385	7.3	LOS A	5.6	41.2	0.62	0.53	53.6
3	R2	317	5.0	0.803	30.5	LOS C	8.7	63.2	1.00	0.96	39.1
Approach		1102	5.0	0.803	14.0	LOS B	8.7	63.2	0.73	0.66	48.4
East: Winterfold Road											
4	L2	267	5.0	0.266	11.8	LOS B	3.4	24.9	0.55	0.72	49.0
6	R2	336	5.0	0.851	33.2	LOS C	9.8	71.4	1.00	1.01	37.9
Approach		603	5.0	0.851	23.7	LOS C	9.8	71.4	0.80	0.88	42.1
North: Carrington St											
7	L2	123	5.0	0.842	33.1	LOS C	8.9	65.2	1.00	1.03	39.5
8	T1	506	5.0	0.842	27.4	LOS C	9.1	66.4	1.00	1.03	41.0
Approach		629	5.0	0.842	28.5	LOS C	9.1	66.4	1.00	1.03	40.7
All Vehicles		2335	5.0	0.851	20.4	LOS C	9.8	71.4	0.82	0.82	44.4

Table 5.11: SIDRA Output: Carrington Street and Winterfold Road, Pm Peak Proposed

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Carrington St											
2	T1	567	5.0	0.250	5.1	LOS A	3.3	23.9	0.50	0.42	55.4
3	R2	288	5.0	0.894	38.0	LOS D	9.0	66.1	1.00	1.10	36.2
Approach		856	5.0	0.894	16.2	LOS B	9.0	66.1	0.67	0.65	47.0
East: Winterfold Road											
4	L2	279	5.0	0.338	15.1	LOS B	4.4	32.4	0.68	0.76	46.9
6	R2	245	5.0	0.855	35.1	LOS D	7.2	52.6	1.00	1.02	37.2
Approach		524	5.0	0.855	24.4	LOS C	7.2	52.6	0.83	0.88	41.8
North: Carrington St											
7	L2	212	5.0	0.887	34.5	LOS C	15.3	111.7	1.00	1.13	38.8
8	T1	782	5.0	0.887	28.8	LOS C	15.6	113.9	1.00	1.13	40.3
Approach		994	5.0	0.887	30.0	LOS C	15.6	113.9	1.00	1.13	40.0
All Vehicles		2374	5.0	0.894	23.8	LOS C	15.6	113.9	0.84	0.90	42.7

During the am peak hour the SIDRA estimated 95% queue lengths along Carrington Street south of Winterfold Road are 6 vehicles for the through movement and 9 vehicles for the right turn movement. The corresponding observed maximum queue lengths are 9 vehicles and 13 vehicles. During the pm peak hour the SIDRA estimated 95% queue lengths along Carrington Street south of Winterfold Road are 4 vehicles for the through movement and 9 vehicles for the right turn movement. The corresponding observed maximum queue lengths are 6 vehicles and 9 vehicles. Therefore the estimated queues by SIDRA may be slightly underestimated for the am peak however, are more comparable for the pm peak. In any instant, SIDRA does not anticipate that the existing queue lengths along the southern approach of Carrington Street to Winterfold Road would increase (max of 0.2 of a vehicle) with the additional development traffic. Accordingly, the existing observed queues are reflective of the likely queues with the development.

During the am and pm peak hour the Carrington Street queue from the signals extended to the Clontarf Road intersection on 3 and 1 occasions respectively, however it did not extend beyond Clontarf Road and subsequently the intersection of Clontarf Road and Carrington Street did not become blocked. With the negligible alterations in traffic operations with the development traffic as demonstrated by SIDRA it is unlikely that the increases in traffic from the development would cause the intersection to become blocked.

5.5 PREVIOUS LANDUSE (KFC) ON SITE

In order to determine traffic generation for the previous land use on site i.e. KFC, data was sourced from "Guide to Traffic Generating Developments, RTA, (2002)". The RTA guidelines state that "Surveys in 1990 of McDonalds and Kentucky Fried Chicken outlets found substantially different generation rates. Gross floor area did not provide a good indication of the generation rates." Subsequently, the RTA document recommends the following for assessment of KFC sites:

"Evening peak hour vehicle trips:

_ assume 100 veh/hr for average development (mean of survey results),

_ for sensitivity test, assess effect of 120 veh/hr (maximum of survey results)."

The RTA document also states that *"the proportion of passing trade is typically at least 50%"*

The proposed service station development is estimated to generate 107 trips per hour in the pm peak hour which is similar to the mean of traffic generation survey results of KFC sites of 100 vehicles per hour. Data suggests that service station sites typically comprise of 56% of passing trade. This is slightly more than suggested minimum for KFC sites of 50%.

By comparison the additional traffic generated by the proposed service station is 47 trips / hour (107 x 44%) in the pm peak compared to previous KFC site with 50 trips/ hour (100 x 50%).

6 CONCLUSION

The proposed Puma development on Clontarf Road is for a service station and convenience store. The site will contain eight (8) fuelling bays. The development is proposed to operate from 5am to midnight, seven days per week.

The primary access to the site is via an existing crossover on Clontarf Road located approximately 40m west from Carrington Street. Secondary access to the site is via an existing crossover on Carrington Street that services a number of businesses abutting Carrington Street.

Service deliveries by refuelling tankers, rubbish collection trucks and other goods delivery vehicles are catered for adequately by the proposed design. Refuelling tankers are proposed to enter via the Carrington Street secondary access and exit via the primary access to the site located on Clontarf Road.

The site is estimated to generate approximately 1,304 vehicle trips daily or 652 customers or 652 trips inbound and 652 trips outbound. Approximately 82 vehicle trips and 107 vehicle trips during the am and pm peak hours respectively are estimated. Research data suggests that approximately 56% of service station trips are typically passing trade hence already on the road network. Based on this data the additional trips occurring on the road network is considerably less with 574 vehicles daily, 36 trips in the am peak hour and 47 trips in the pm peak hour.

Clontarf Road and the surrounding road network can readily accommodate this additional traffic with no significant impact to existing operation and traffic conditions.

A SIDRA analysis of the Clontarf Road access indicates that the access will operate satisfactorily during the am and pm peak hours. The likely queue along the westbound traffic lane is only expected to be 1 vehicle in the peak periods; hence it will not block the intersection of Clontarf Road and Carrington Street. The provision of a painted median island along Clontarf Road near the access would allow storage of right turning traffic and allow through traffic to pass.

SIDRA analyses of the Clontarf Road and Carrington Street indicates there is minimal difference in the operation of the intersection with the additional traffic from the development. The additional traffic represents approximately a 3% and 4% increase in traffic volumes already passing through the intersection in the am and pm peak hours, hence the minimal difference in the results.

Similarly, the SIDRA analyses of the Carrington Street and Winterfold Road signalised intersection indicates there is negligible difference in the operation of the intersection with the additional traffic from the development. The additional traffic represents approximately less than 1% increase in traffic volumes already passing through the intersection in the peak hours, hence the negligible difference in the results.

The previous development on the site was a KFC restaurant. By comparison the additional traffic generated by the proposed service station is 47 trips / hour (107 x 44%) in the pm peak compared to previous KFC site with 50 trips/ hour (100 x 50%).

The preferred access arrangement for tankers to/from the site is to enter via the secondary access on Carrington Street and exit via the primary access on Clontarf Road. This access arrangement means that tanker only need to undertake left hand movements into and out of the site which is preferred on safety grounds.

In conclusion, the proposed service station and convenience store is forecast to operate satisfactorily from a traffic and road safety perspective. The low impact of the traffic from the proposed development on the surrounding road network, good access/egress and circulation system within the development all indicate a well planned development proposal which is supported by this Transport Impact Statement report.

APPENDIX A: TRAFFIC DATA

Weekly Volume by Hour

Traffic Flow: **Both Directions** Road Name: **Clontarf Rd (1030003)**
Site No: **4640** Location Description: **W of Carrington St (SLK 1.07)**
Date Range: **02 Aug 2011 to 06 Aug 2011** Count Type: **Axle Pairs**

Average Vehicle Volume									
Hour	Mon	Tue	Wed	Thu	Fri	Sat	Sun	Mon - Fri	Mon - Sun
0000		11	21	19	13	29		16	16
0100		2	7	11	6	26		7	7
0200		4	6	6	10	17		7	7
0300		3	6	9	4	8		6	6
0400		27	25	20	26	16		25	25
0500		57	57	50	51	14		54	54
0600		164	165	158	147	30		159	159
0700		284	288	312	306	112		297	297
0800		530	514	518	518	237		520	520
0900		327	366	431	378	351		376	376
1000		347	311	324	365	399		337	337
1100		336	370	330	347	416		346	346
1200		344	407	341	378	407		368	368
1300		329	313	332	322	378		324	324
1400		392	410	461	440	394		426	426
1500		533	512	467	553	398		516	516
1600		435	456	489	446	331		457	457
1700		459	496	474	466	387		474	474
1800		319	346	362	366	308		356	356
1900		175	231	232	227	192		216	216
2000		106	132	154	175	146		142	142
2100		85	103	108	120	102		104	104
2200		48	61	50	98	77		64	64
2300		26	46	34	61	69		42	42
Total		5343	5647	5892	5853	4833		5639	5639

		Peak Statistics								
		Mon	Tue	Wed	Thu	Fri	Sat	Sun	Mon - Fri	Mon - Sun
AM	1/4 Hour		0830	0815	0815	0845	1045		0815	0845
	1/4 Hr Vol		144	133	152	137	117		137	122
	1/2 Hour		0830	0815	0815	0830	1145		0815	0830
	1/2 Hr Vol		282	262	282	267	229		270	241
	1 Hour		0800	0800	0815	0800	1115		0800	0800
	1 Hr Vol		530	514	540	518	439		520	463
	1 Hr Fact		920.1	966.2	988.2	945.3	892.3		950.6	947.2
	2 Hour		0745	0800	0745	0745	1015		0745	0745
PM	2 Hr Vol		886	880	954	923	849		911	834
	1/4 Hour		1500	1500	1500	1515	1200		1500	1500
	1/4 Hr Vol		167	166	142	148	123		155	142
	1/2 Hour		1445	1445	1445	1500	1200		1445	1445
	1/2 Hr Vol		306	308	279	291	209		294	270
	1 Hour		1445	1445	1415	1445	1345		1445	1445
	1 Hr Vol		555	543	513	568	410		543	508
	1 Hr Fact		830.8	817.8	803.2	859.5	823.4		878.6	893.1
Peak	2 Hour		1445	1445	1415	1445	1400		1445	1430
	2 Hr Vol		993	997	971	1020	792		991	943
	1/2 Hour		0645	0730	0700	0730	0745		0730	0730
	1/2 Hr Vol		4642	4800	4841	4938	4199		4795	4674

Weekly Volume by Hour

Traffic Flow: **Directional** Road Name: **Clontarf Rd (1030003)**
Site No: **4640** Location Description: **W of Carrington St (SLK 1.07)**
Date Range: **02 Aug 2011 to 06 Aug 2011** Count Type: **Axle Pairs**

Average Vehicle Volume																		
Hour	Mon		Tue		Wed		Thu		Fri		Sat		Sun		Mon - Fri		Mon - Sun	
	E	W	E	W	E	W	E	W	E	W	E	W	E	W	E	W	E	W
0000			6	5	10	11	9	10	7	6	11	18			8	8	8	8
0100			0	2	5	2	3	8	2	4	10	15			3	4	3	4
0200			2	2	2	4	1	5	3	7	12	5			2	5	2	5
0300			3	0	1	5	4	5	1	3	3	5			2	3	2	3
0400			9	18	12	13	8	12	9	17	5	11			10	15	10	15
0500			18	39	15	42	12	38	13	38	6	8			15	39	15	39
0600			76	88	64	101	66	92	64	83	18	12			68	91	68	91
0700			103	181	106	180	113	199	96	210	33	79			105	193	105	193
0800			162	368	155	359	152	366	172	346	75	162			160	360	160	360
0900			130	197	154	212	186	245	183	215	154	197			158	217	158	217
1000			174	173	131	180	165	159	169	196	178	221			160	177	160	177
1100			138	198	175	195	158	172	175	172	206	210			162	184	162	184
1200			177	167	193	214	170	171	168	210	191	216			177	191	177	191
1300			164	165	148	165	168	164	157	165	174	204			159	165	159	165
1400			177	215	200	210	227	234	222	218	179	215			207	219	207	219
1500			282	251	271	241	240	227	276	277	169	229			267	249	267	249
1600			205	230	215	241	242	247	201	245	168	183			216	241	216	241
1700			217	242	256	240	253	221	237	229	198	189			241	233	241	233
1800			149	170	168	178	165	197	189	207	145	183			168	188	168	188
1900			89	86	107	124	118	114	102	125	87	95			104	112	104	112
2000			54	52	59	73	77	77	74	101	71	75			66	76	66	76
2100			48	37	47	56	42	66	70	50	57	45			52	52	52	52
2200			22	26	31	30	24	26	43	55	40	37			30	34	30	34
2300			15	11	27	19	17	17	31	30	31	38			23	19	23	19
Total			2420	2923	2552	3095	2620	3072	2644	3209	2221	2612			2563	3075	2563	3075

Peak Statistics																			
		Mon		Tue		Wed		Thu		Fri		Sat		Sun		Mon - Fri		Mon - Sun	
		E	W	E	W	E	W	E	W	E	W	E	W	E	W	E	W	E	W
AM	1st Hour			1030	0830	1130	0815	0900	0815	1115	0815	1130	0945			1030	0815	1030	0815
	1st Hr Vol			58	96	50	98	52	105	56	92	58	83			52	97	51	84
	1/2 Hour			1030	0830	1130	0815	0930	0815	1115	0800	1145	1045			1030	0815	1030	0815
	1/2 Hr Vol			101	188	95	190	94	196	104	180	116	116			91	187	93	163
	1 Hour			1015	0800	1145	0800	0900	0800	0830	0800	1115	1030			1145	0800	1145	0800
	1 Hr Vol			178	368	195	359	186	386	194	346	221	231			177	360	183	320
	1 Hr Fact			79.46	95.83	76.17	91.58	89.42	87.14	89.81	9.402	9.057	9.315			92.67	93.26	9.492	95.24
	2 Hour			1145	0730	1100	0730	0900	0745	1115	0730	1045	0945			1145	0730	1030	0745
	2 Hr Vol			331	593	368	592	351	632	353	592	412	443			340	600	353	543
	1st Hour			1500	1445	1500	1500	1500	1600	1515	1530	1200	1545			1500	1445	1500	1445
PM	1st Hr Vol			97	84	87	79	88	70	80	80	61	72			88	71	78	66
	1/2 Hour			1500	1445	1445	1445	1445	1430	1500	1515	1700	1530			1445	1445	1445	1445
	1/2 Hr Vol			155	154	156	152	158	131	158	148	107	116			155	138	139	130
	1 Hour			1445	1445	1445	1430	1415	1600	1445	1500	1700	1500			1445	1445	1445	1430
	1 Hr Vol			284	271	285	267	270	247	294	277	198	229			283	261	259	251
	1 Hr Fact			7.32	8.065	8.19	8.449	7.67	8.821	9.188	8.658	8.839	7.951			8.086	9.158	8.344	9.536
	2 Hour			1500	1430	1445	1445	1600	1415	1415	1500	1600	1400			1445	1430	1445	1430
	2 Hr Vol			487	514	503	494	495	488	510	522	366	444			494	500	484	481
	12 Hour			0745	0645	0730	0715	0745	0700	0745	0730	0830	0730			0745	0700	0745	0700
	12 Hr Vol			2088	2563	2185	2621	2246	2602	2250	2698	1939	2272			2191	2616	2139	2542

Public Holidays → School Holidays →

Run on 20-Oct-2014 11:47 by Jennie Hoptmiller

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Weekly Volume by Hour

Traffic Flow: **Both Directions** Road Name: **Carrington St (1030001)**
Site No: **3315** Location Description: **S of Winterfold Rd (SLK 0.07)**
Date Range: **24 Apr 2013 to 28 Apr 2013** Count Type: **Axle Pairs**

Average Vehicle Volume									
Hour	Mon	Tue	Wed	Thu	Fri	Sat	Sun	Mon-Fri	Mon-Sun
0000			69	161	75	153	175	72	118
0100			33	82	40	118	107	41	77
0200			23	57	20	52	66	22	40
0300			35	57	46	44	66	41	48
0400			81	125	73	64	39	77	64
0500			326	279	268	124	86	307	206
0600			729	343	616	274	173	673	448
0700			1091	401	967	430	372	1029	715
0800			1241	465	1187	734	525	1214	922
0900			1195	639	1291	1132	763	1243	1095
1000			1166	798	1277	1250	1015	1222	1177
1100			1215	987	1308	1354	1021	1262	1225
1200			1202	1005	1275	1295	1142	1239	1229
1300			1220	925	1336	1121	988	1278	1166
1400			1321	938	1405	1113	963	1363	1201
1500			1618	907	1483	1115	1024	1551	1310
1600			1884	970	1612	1032	1039	1748	1392
1700			1703	916	1521	985	985	1612	1289
1800			1253	755	1011	825	720	1132	952
1900			792	491	589	513	425	691	580
2000			534	410	433	400	311	484	420
2100			467	305	378	318	288	423	363
2200			301	194	435	288	188	368	303
2300			186	124	262	248	86	224	196
Total			19685	12324	18937	14982	12567	19316	16546

Peak Statistics										
		Mon	Tue	Wed	Thu	Fri	Sat	Sun	Mon - Fri	Mon - Sun
AM	1/4 Hour			0845		1030	1130	1045	1115	1115
	1/4 Hr Vol			325		367	354	279	335	314
	1/2 Hour			1145		1030	1130	1145	1100	1145
	1/2 Hr Vol			639		694	693	573	650	633
	1 Hour			1115		1030	1130	1145	1030	1115
	1 Hr Vol			1254		1373	1393	1124	1292	1253
	1 Hr Fact			.9358		.9353	.962	.9123	.9656	.9467
	2 Hour			1030		1030	1030	1115	1030	1030
PM	2 Hr Vol			2461		2642	2692	2189	2552	2486
	1/4 Hour			1615		1700	1215	1200	1700	1700
	1/4 Hr Vol			507		421	362	308	455	356
	1/2 Hour			1600		1700	1200	1200	1600	1600
	1/2 Hr Vol			983		830	700	575	895	704
	1 Hour			1615		1630	1200	1200	1615	1615
	1 Hr Vol			1897		1636	1295	1142	1763	1393
	1 Hr Fact			.9354		.9715	.8943	.9269	.9687	.9782
Peak	2 Hour			1530		1530	1200	1200	1530	1530
	2 Hr Vol			3647		3194	2416	2130	3421	2745
	12 Hour			0715		0645	0730	0730	0700	0715
	12 Hr Vol			16122		16714	12516	10620	15891	13709

Public Holidays → School Holidays →

Run on 16-Oct-2014 11:00 by Jennie Hopmüller

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Weekly Volume by Hour

Traffic Flow: **Directional** Road Name: **Carrington St (1030001)**
Site No: **3315** Location Description: **S of Winterfold Rd (SLK 0.07)**
Date Range: **24 Apr 2013 to 28 Apr 2013** Count Type: **Axle Pairs**

Hour	Average Vehicle Volume															
	Mon		Tue		Wed		Thu		Fri		Sat		Sun		Mon - Fri	
	N	S	N	S	N	S	N	S	N	S	N	S	N	S	N	S
0000					31	38	73	78	41	34	84	69	88	87	36	38
0100					17	16	34	48	20	20	62	56	42	65	23	18
0200					13	10	28	31	9	11	26	26	31	35	11	11
0300					24	11	28	29	20	17	21	23	36	30	27	14
0400					52	29	93	32	50	23	45	19	24	15	51	26
0500					215	111	188	111	188	100	72	52	52	34	202	106
0600					523	206	183	180	452	164	173	101	120	53	488	185
0700					806	285	237	164	693	274	290	140	273	99	750	260
0800					900	341	272	193	813	374	514	220	364	161	857	358
0900					781	414	363	276	852	439	740	392	478	285	817	427
1000					726	440	430	368	773	504	807	443	590	425	750	472
1100					722	493	613	374	798	510	831	523	606	415	760	502
1200					733	469	630	375	757	518	752	543	647	495	745	494
1300					700	520	564	361	759	577	664	457	578	410	730	549
1400					718	603	542	396	776	628	635	478	589	374	747	616
1500					869	749	552	355	834	649	651	464	575	449	852	699
1600					1081	803	583	387	902	710	559	473	624	415	992	757
1700					875	828	552	364	843	678	590	395	568	417	859	753
1800					714	539	441	314	589	422	493	332	408	312	652	481
1900					449	343	280	211	335	254	285	228	225	200	392	299
2000					265	269	220	190	223	210	210	190	169	142	244	240
2100					217	250	157	148	194	184	154	164	158	130	208	217
2200					154	147	91	103	165	270	153	135	95	93	160	209
2300					100	86	57	67	108	154	137	111	45	41	104	120
Total					11685	8000	7189	5136	11212	7725	8948	6034	7385	5182	11455	7899

Peak Statistics																
	Mon		Tue		Wed		Thu		Fri		Sat		Sun		Mon - Fri	
	N	S	N	S	N	S	N	S	N	S	N	S	N	S	N	S
AM	1/4 Hour				0800	1145			0845	1115	1000	1145	1030	1045	0845	1115
	1/4 Hr Vol				241	135			228	145	227	138	170	127	224	137
	1/2 Hour				0745	1145			0915	1030	1130	1145	1030	1145	0830	1145
	1/2 Hr Vol				471	261			438	283	425	289	322	258	439	262
	1 Hour				0745	1130			0845	1030	1045	1130	1115	1145	0800	1115
	1 Hr Vol				910	515			889	549	835	578	632	498	857	521
	1 Hr Fact				944	9398			9529	9466	9319	9088	9294	9022	9565	9542
	2 Hour				0730	1030			0800	1145	1000	1030	1115	1115	0745	1145
	2 Hr Vol				1735	987			1685	1072	1638	1066	1268	931	1678	1029
	1/4 Hour				1615	1700			1700	1715	1215	1215	1630	1200	1600	1700
PM	1/4 Hr Vol				295	248			245	205	203	159	174	138	259	212
	1/2 Hour				1600	1700			1645	1700	1200	1200	1445	1200	1600	1700
	1/2 Hr Vol				578	460			473	381	390	310	337	248	514	421
	1 Hour				1600	1615			1615	1630	1200	1200	1200	1200	1600	1630
	1 Hr Vol				1081	858			912	734	752	543	647	495	992	796
	1 Hr Fact				9161	8649			9306	8951	9261	8538	8515	8967	9575	9387
	2 Hour				1530	1530			1530	1530	1200	1200	1200	1200	1530	1530
	2 Hr Vol				1977	1670			1773	1421	1416	1000	1225	905	1875	1546
	12 Hour				0700	0745			0630	0715	0730	0815	0715	0815	0645	0730
	12 Hr Vol				9625	6555			9453	6301	7577	4960	6311	4370	9530	6418

Weekly Vehicle Counts (Virtual Week)

VirtWeeklyVehicle-259

Site: Clontarf-01.0.1EW
Description: Between Parnell and York Street
Filter time: 9:00 Sunday, 16 September 2012 => 14:00 Monday, 24 September 2012
Scheme: Vehicle classification (AustRoads94)
Filter: CIs(1 2 3 4 5 6 7 8 9 10 11 12) Dir(EW) Sp(10,160) GapX(>0) Span(0 - 100)

	Mon	Tue	Wed	Thu	Fri	Sat	Sun	Averages	
								1 - 5	1 - 7
Hour									
0000-0100	19.0	9.0	12.0	20.0	14.0	37.0	23.0	13.5	15.9
0100-0200	5.5	10.0	8.0	10.0	9.0	16.0	26.0	8.0	12.5
0200-0300	9.5	4.0	4.0	12.0	12.0	12.0	32.0	6.5	10.4
0300-0400	1.5	9.0	5.0	6.0	6.0	10.0	26.0	3.8	7.4
0400-0500	20.0	20.0	25.0	32.0	29.0	15.0	14.0	24.3	21.9
0500-0600	86.0	79.0	92.0	78.0	82.0	20.0	21.0	89.8	68.0
0600-0700	170.0	194.0	194.0	198.0	172.0	71.0	40.0	183.0	151.1
0700-0800	305.5	294.0	300.0	291.0	325.0	139.0	82.0	303.5	255.3
0800-0900	508.5	525.0	484.0	503.0	456.0	273.0	169.0	497.5	428.4
0900-1000	365.5	347.0	325.0	330.0	402.0	340.0	275.0	355.8	336.8
1000-1100	341.5	345.0	301.0	346.0	361.0	403.0	236.5	339.5	351.1
1100-1200	341.0	295.0	319.0	321.0	350.0	431.0	394.0	327.8	354.0
1200-1300	357.0	325.0	349.0	326.0	382.0	493.0	409.5	349.3	375.7
1300-1400	290.5	290.0	335.0	290.0	293.0	394.0	372.5	312.0	324.6
1400-1500	425.0	357.0	402.0	414.0	381.0	387.0	322.5	395.8	376.4
1500-1600	497.0	450.0	492.0	510.0	496.0	391.0	331.0	477.0	429.8
1600-1700	459.0	495.0	504.0	525.0	459.0	334.0	365.0	458.0	435.0
1700-1800	486.0	479.0	520.0	521.0	475.0	349.0	245.5	496.6	440.4
1800-1900	229.0	256.0	364.0	324.0	295.0	260.0	251.0	335.6	305.0
1900-2000	156.0	184.0	220.0	200.0	206.0	171.0	159.5	199.2	181.8
2000-2100	108.0	139.0	159.0	130.0	121.0	116.0	101.5	131.4	122.0
2100-2200	81.0	59.0	90.0	91.0	92.0	116.0	75.0	85.6	85.6
2200-2300	46.0	48.0	56.0	50.0	72.0	80.0	42.5	58.4	54.0
2300-2400	22.0	26.0	22.0	49.0	48.0	76.0	23.0	22.2	35.4
Totals									
0700-1900	4656.5	4561.0	4698.0	4711.0	4674.0	4199.0	2676.5	4675.5	4423.2
0600-2200	5201.5	5167.0	5361.0	5330.0	5265.0	4673.0	4051.5	5275.0	4971.7
0600-0000	5269.5	5236.0	5439.0	5423.0	5355.0	4829.0	4117.0	5360.6	5061.1
0000-0000	5399.0	5361.0	5525.0	5581.0	5527.0	4939.0	4279.0	5500.6	5200.1
AM Peak	0800	0800	0800	0800	0800	1100	1100		
	505.5	525.0	484.0	503.0	456.0	431.0	394.0		
PM Peak	1700	1600	1700	1600	1500	1200	1200		
	485.0	495.0	520.0	525.0	496.0	493.0	409.5		

* - No data.

City of Cockburn traffic data

Weekly Vehicle Counts (Virtual Week)

VirtWeeklyVehicle-260

Site: Clontarf-01.0.1EW
Description: Between Parnell and York Street
Filter time: 9:00 Sunday, 16 September 2012 => 14:00 Monday, 24 September 2012
Scheme: Vehicle classification (AustRoads94)
Filter: Cls(1 2 3 4 5 6 7 8 9 10 11 12) Dir(E) Sp(10,160) GapX(>0) Span(0 - 100)

	Mon	Tue	Wed	Thu	Fri	Sat	Sun	Averages	
								1 - 5	1 - 7
Hour									
0000-0100	7.0	5.0	6.0	8.0	4.0	13.0	9.0	6.2	7.4
0100-0200	3.0	4.0	3.0	6.0	6.0	9.0	17.0	4.2	6.3
0200-0300	1.5	2.0	1.0	5.0	6.0	4.0	13.0	2.8	4.3
0300-0400	0.5	1.0	1.0	3.0	2.0	6.0	12.0	1.2	3.4
0400-0500	3.0	6.0	5.0	7.0	9.0	6.0	7.0	5.5	5.8
0500-0600	23.5	28.0	27.0	28.0	24.0	6.0	9.0	28.7	21.1
0600-0700	62.0	67.0	74.0	73.0	68.0	26.0	17.0	68.0	56.4
0700-0800	114.5	113.0	91.0	105.0	110.0	41.0	34.0	108.0	90.4
0800-0900	194.5	213.0	199.0	215.0	184.0	104.0	61.0	200.0	170.6
0900-1000	144.0	140.0	126.0	134.0	162.0	131.0	110.5	141.7	138.8
1000-1100	160.5	161.0	127.0	153.0	154.0	168.0	140.5	152.5	151.6
1100-1200	168.5	116.0	158.0	149.0	165.0	186.0	152.0	154.2	157.2
1200-1300	160.5	141.0	168.0	145.0	172.0	214.0	183.5	157.8	169.8
1300-1400	149.5	150.0	158.0	151.0	146.0	194.0	160.5	150.7	157.7
1400-1500	222.0	171.0	211.0	209.0	202.0	165.0	149.0	203.0	184.8
1500-1600	194.0	200.0	206.0	236.0	220.0	193.0	156.5	211.2	195.3
1600-1700	217.0	231.0	232.0	262.0	202.0	166.0	132.0	228.8	209.5
1700-1800	235.0	222.0	255.0	238.0	220.0	168.0	173.0	234.0	210.5
1800-1900	160.0	171.0	187.0	155.0	140.0	116.0	132.5	162.6	149.3
1900-2000	51.0	96.0	111.0	89.0	101.0	74.0	76.5	95.6	88.1
2000-2100	56.0	71.0	75.0	64.0	51.0	52.0	50.0	63.4	55.6
2100-2200	21.0	39.0	45.0	42.0	53.0	60.0	39.5	42.0	43.6
2200-2300	19.0	19.0	23.0	19.0	31.0	47.0	19.5	22.2	24.6
2300-2400	10.0	14.0	10.0	22.0	20.0	36.0	9.5	15.2	16.4
Totals									
0700-1900	2120.0	2029.0	2118.0	2151.0	2077.0	1866.0	1636.0	2104.4	1982.3
0600-2200	2351.0	2302.0	2423.0	2419.0	2350.0	2078.0	1819.0	2373.4	2329.0
0600-0000	2380.0	2335.0	2456.0	2460.0	2401.0	2161.0	1948.0	2410.8	2270.0
0000-0000	2413.5	2381.0	2499.0	2517.0	2432.0	2204.0	1916.0	2456.5	2313.1
AM Peak	0800	0800	0800	0800	0800	1100	1100		
	194.5	213.0	199.0	215.0	184.0	186.0	152.0		
PM Peak	1700	1600	1700	1600	1700	1200	1200		
	235.0	231.0	255.0	262.0	220.0	214.0	183.5		

* - No data.

City of Cockburn traffic data

Weekly Vehicle Counts (Virtual Week)

VirtWeeklyVehicle-261

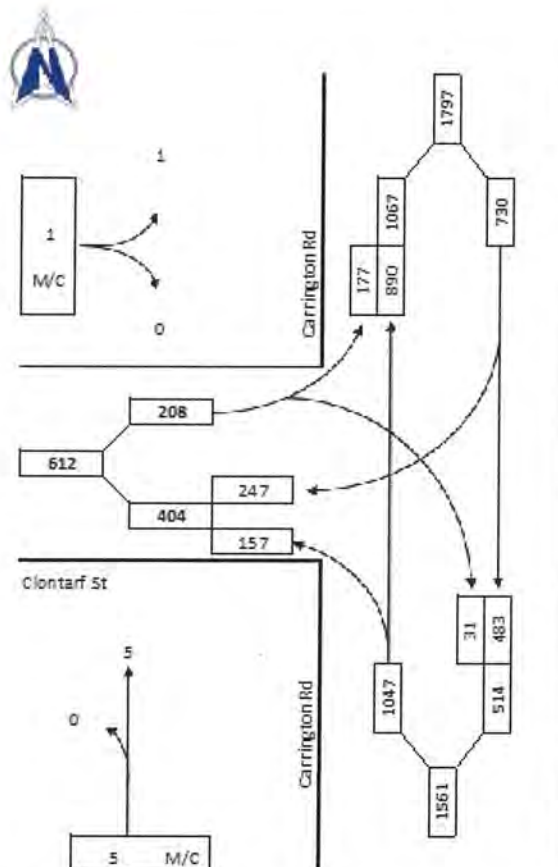
Site: Clontarf-01.0.1EW
Description: Between Parnell and York Street
Filter time: 9:00 Sunday, 16 September 2012 => 14:00 Monday, 24 September 2012
Scheme: Vehicle classification (AustRoads94)
Filter: Cls(1 2 3 4 5 6 7 8 9 10 11 12) Dir(W) Sp(10,160) GapX(>0) Span(0 - 100)

	Mon	Tue	Wed	Thu	Fri	Sat	Sun	Averages	
								1 - 5	1 - 7
Hour									
0000-0100	6.0	4.0	6.0	12.0	10.0	24.0	24.0	7.3	11.5
0100-0200	2.5	6.0	5.0	4.0	3.0	6.0	19.0	3.6	6.3
0200-0300	2.0	2.0	3.0	7.0	6.0	8.0	19.0	2.7	6.1
0300-0400	1.0	2.0	4.0	3.0	4.0	4.0	13.0	2.5	4.0
0400-0500	17.0	14.0	20.0	25.0	20.0	9.0	7.0	18.8	16.1
0500-0600	62.5	51.0	65.0	50.0	58.0	14.0	12.0	58.2	46.9
0600-0700	107.0	127.0	120.0	125.0	104.0	48.0	28.0	115.0	94.8
0700-0800	191.0	181.0	209.0	186.0	215.0	98.0	48.0	195.5	164.9
0800-0900	314.0	312.0	285.0	288.0	272.0	169.0	108.0	297.5	257.6
0900-1000	221.5	207.0	199.0	196.0	240.0	189.0	167.5	214.2	201.0
1000-1100	181.0	187.0	174.0	194.0	207.0	240.0	216.0	187.3	199.6
1100-1200	172.5	179.0	161.0	172.0	185.0	245.0	242.0	173.7	196.8
1200-1300	196.5	184.0	181.0	181.0	210.0	279.0	226.0	191.5	208.9
1300-1400	181.0	140.0	180.0	199.0	147.0	200.0	212.0	161.3	176.9
1400-1500	203.0	186.0	191.0	205.0	179.0	222.0	173.5	192.8	191.6
1500-1600	243.0	250.0	286.0	274.0	276.0	198.0	174.5	265.8	234.5
1600-1700	241.0	264.0	272.0	263.0	256.0	168.0	182.0	259.2	228.5
1700-1800	253.0	257.0	265.0	283.0	255.0	181.0	172.5	262.6	229.9
1800-1900	169.0	185.0	177.0	179.0	155.0	144.0	118.5	173.0	155.8
1900-2000	75.0	88.0	109.0	111.0	105.0	97.0	82.0	97.6	93.6
2000-2100	52.0	68.0	84.0	66.0	70.0	64.0	51.5	69.0	63.4
2100-2200	50.0	50.0	45.0	49.0	39.0	56.0	35.5	46.6	45.0
2200-2300	27.0	24.0	33.0	31.0	41.0	33.0	28.0	31.2	29.4
2300-2400	12.0	12.0	12.0	21.0	28.0	40.0	13.5	17.0	19.0
Totals									
0700-1900	2566.5	2532.0	2580.0	2560.0	2597.0	2333.0	2040.5	2574.4	2446.0
0600-2200	2850.5	2865.0	2938.0	2911.0	2915.0	2395.0	2232.5	2901.6	2742.7
0600-0000	2889.5	2901.0	2983.0	2963.0	2984.0	2665.0	2269.0	2949.5	2791.1
0000-0000	2980.5	2980.0	3086.0	3064.0	3055.0	2735.0	2363.0	3044.1	2882.0
AM Peak	0800	0800	0800	0800	0800	1100	1100		
	314.0	312.0	285.0	288.0	272.0	245.0	242.0		
PM Peak	1700	1600	1500	1700	1500	1200	1200		
	253.0	264.0	256.0	283.0	276.0	279.0	226.0		

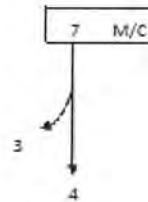
* - No data.

City of Cockburn traffic data

Puma	Day Thurs day	Date 14/05/2015	Weather Sunny
Vehicle Volumes	Location Carrington Rd & Clontarf St		Suburb Hamilton Hill
Puma Clontarf Rd TIA			
Site 1	Job No. 2395	Intersection Type: 3 - Way	
Donald Veal Consultants	Duration: 0800 - 0900		



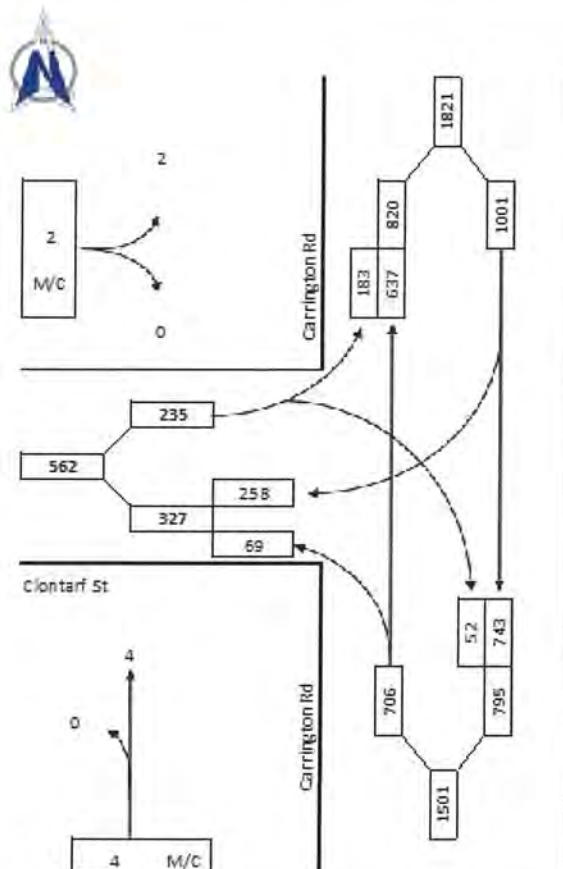
All Vehicles AM Peak Hour



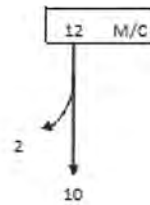
Vehicles Entering Intersection					
Count	To N	To E	To S	To W	Total
From N	0	0	483	247	730
From E	0	0	0	0	0
From S	890	0	0	157	1047
From W	177	0	31	0	208
Total	1067	0	514	404	1985

M/C Entering Intersection					
Count	To N	To E	To S	To W	Total
From N	0	0	4	3	7
From E	0	0	0	0	0
From S	5	0	0	0	5
From W	1	0	0	0	1
Total	6	0	4	3	13

Puma	Day Thurs day	Date 14/05/2015	Weather Sunny
Vehicle Volumes	Location Carrington Rd & Clontarf St		Suburb Hamilton Hill
Puma Clontarf Rd TIA			
Site 1	Job No. Z395	Intersection Type: 3 - Way	
Donald Veal Consultants	Duration: 1600 - 1700		



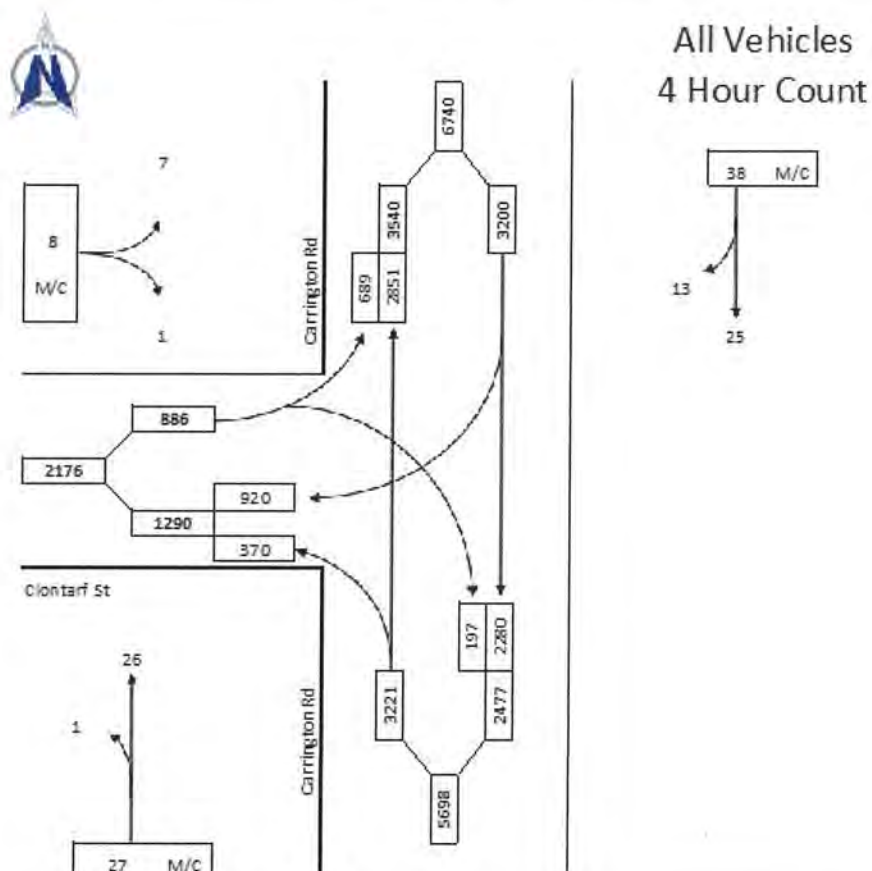
All Vehicles PM Peak Hour



Vehicles Entering Intersection					
Count	To N	To E	To S	To W	Total
From N	0	0	743	258	1001
From E	0	0	0	0	0
From S	637	0	0	69	706
From W	183	0	52	0	235
Total	820	0	795	327	1942

M/C Entering Intersection					
Count	To N	To E	To S	To W	Total
From N	0	0	10	2	12
From E	0	0	0	0	0
From S	4	0	0	0	4
From W	2	0	0	0	2
Total	6	0	10	2	18

Puma	Day Thurs day	Date 14/05/2015	Weather Sunny
Vehicle Volumes	Location Carrington Rd & Clontarf St		Suburb Hamilton Hill
Puma Clontarf Rd TIA			
Site 1	Job No. Z395	Intersection Type: 3 - Way	
Donald Veal Consultants	Duration:	0800 - 0900 & 1500 - 1700	

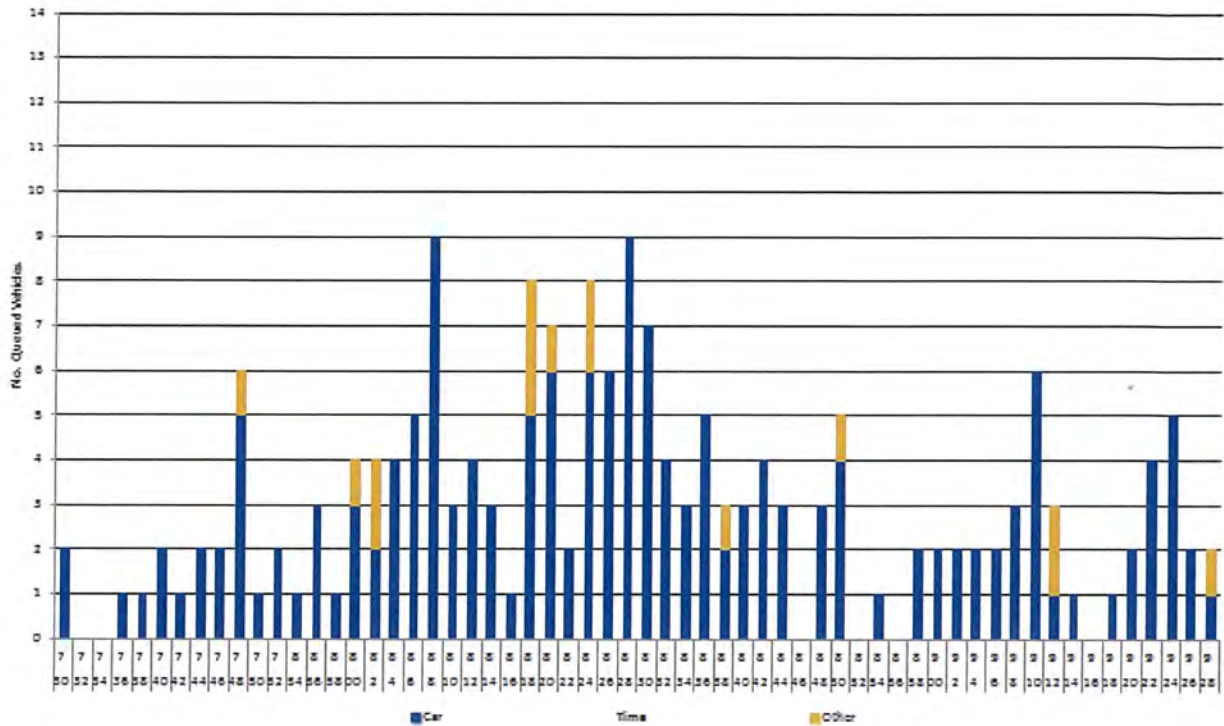


All Vehicles 4 Hour Count

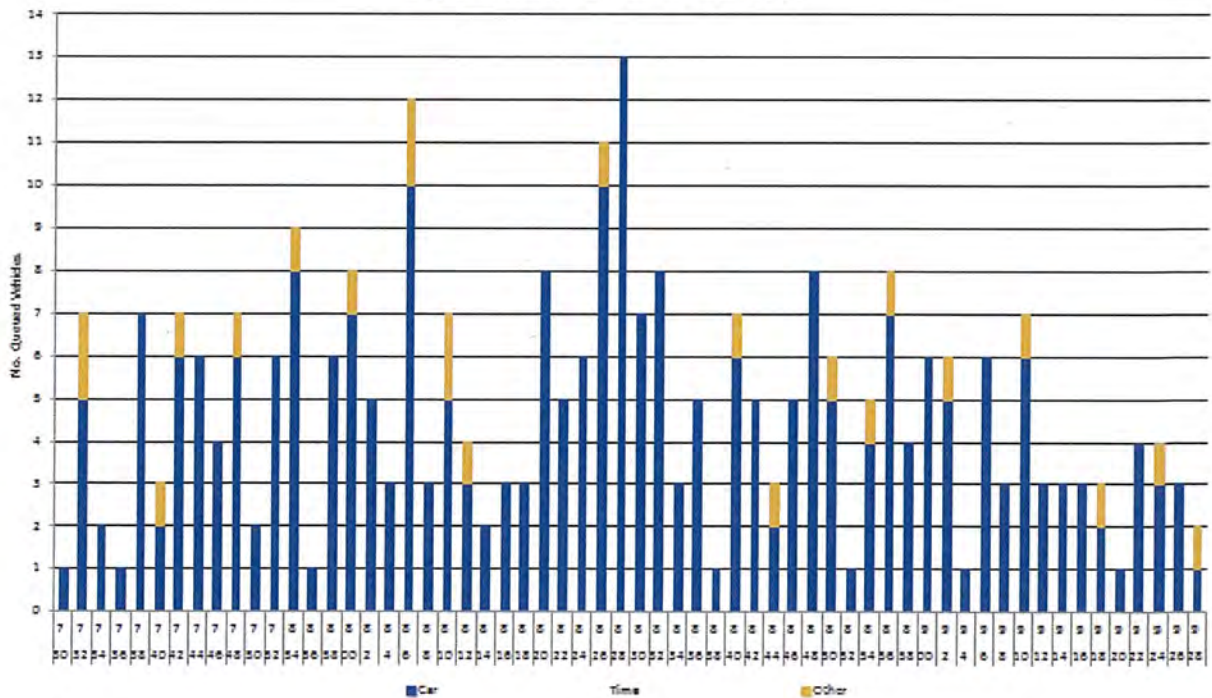
Vehicles Entering Intersection					
Count	To N	To E	To S	To W	Total
From N	0	0	2280	920	3200
From E	0	0	0	0	0
From S	2851	0	0	370	3221
From W	689	0	197	0	886
Total	3540	0	2477	1290	7307

M/C Entering Intersection					
Count	To N	To E	To S	To W	Total
From N	0	0	25	13	38
From E	0	0	0	0	0
From S	26	0	0	1	27
From W	7	0	1	0	8
Total	33	0	26	14	73

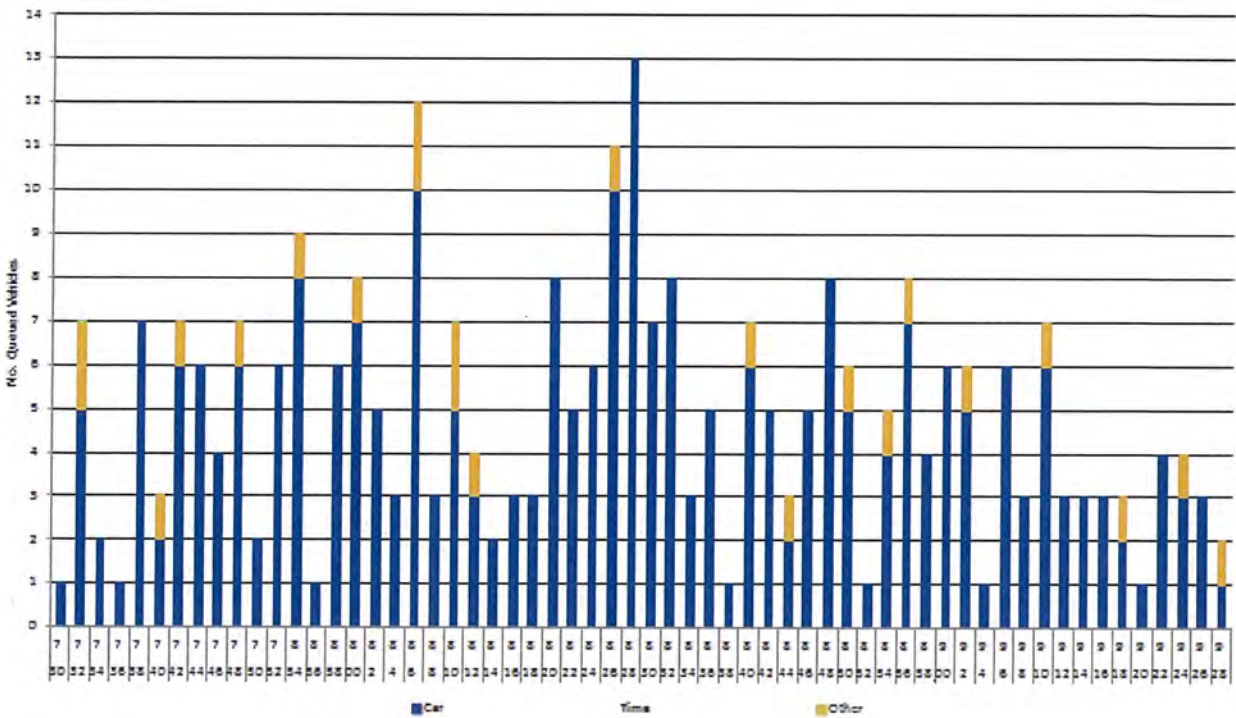
Carrington St Queue Lengths 14th May 2015
Right Lane AM 0730 - 0930



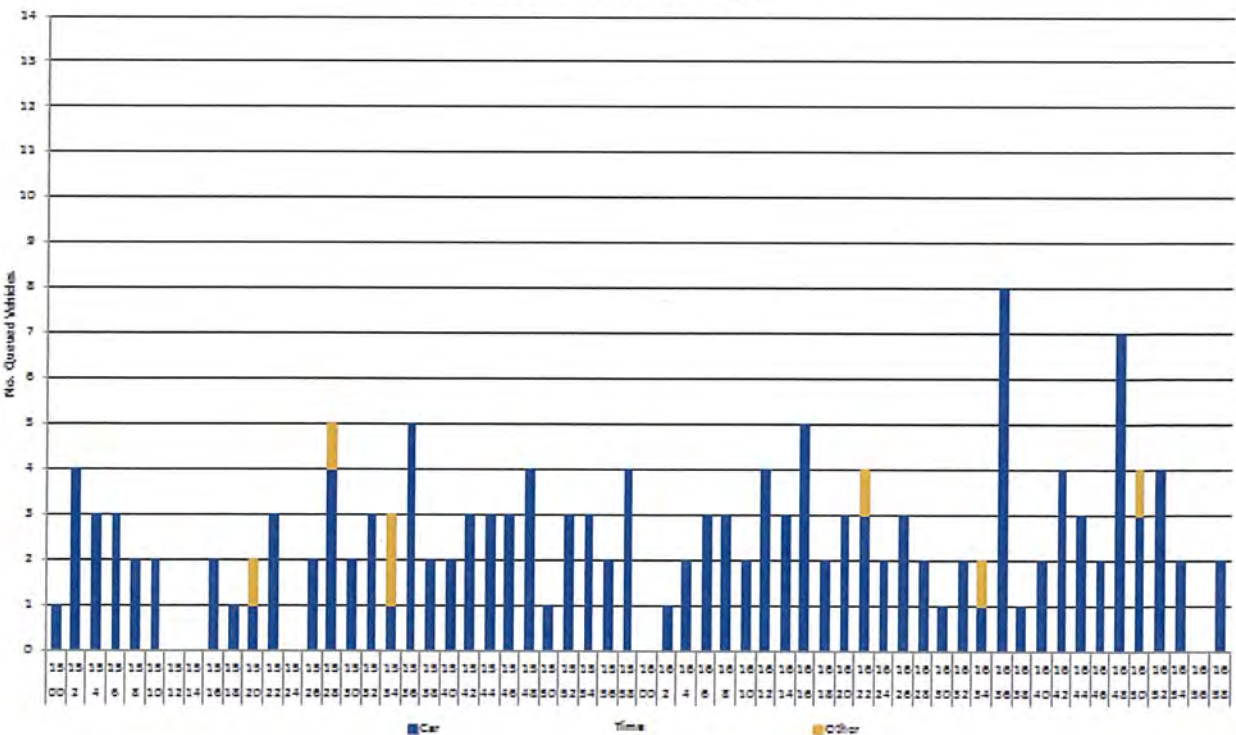
Carrington St Queue Lengths 14th May 2015
Right Turning Lane AM 0730 - 0930



Carrington St Queue Lengths 14th May 2015
Right Turning Lane AM 0730 - 0930



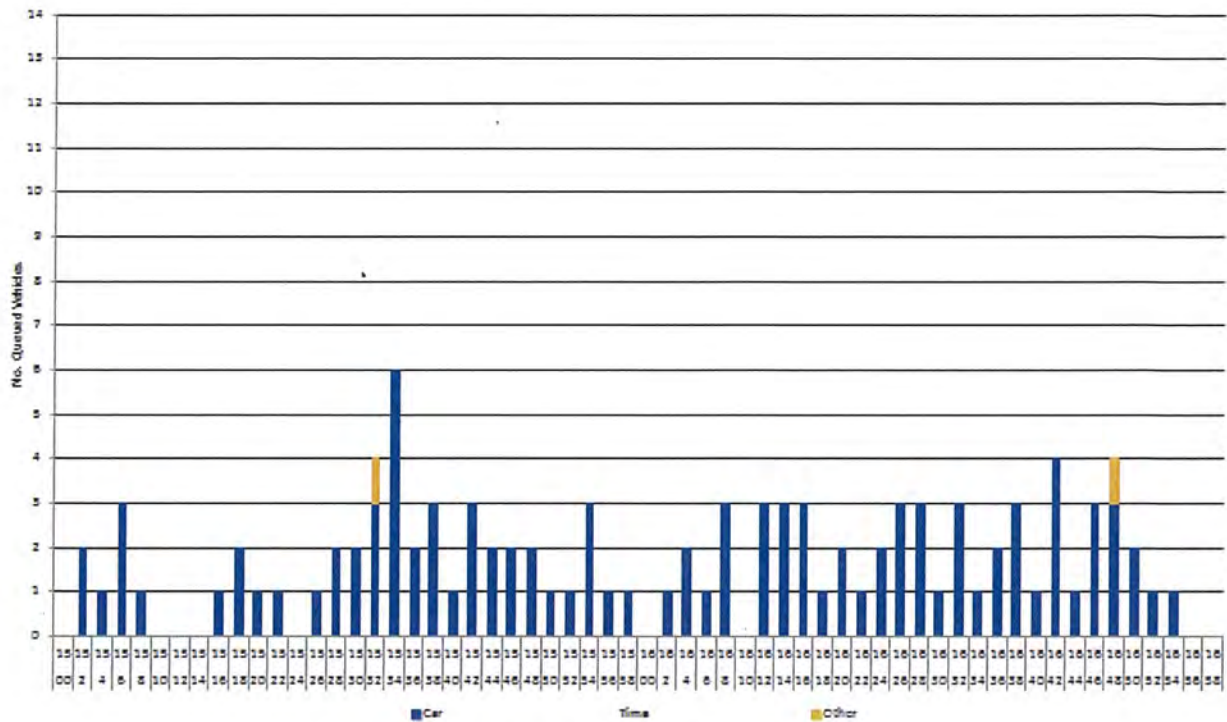
Carrington St Queue Lengths 14th May 2015
Left Lane PM 1500 - 1700



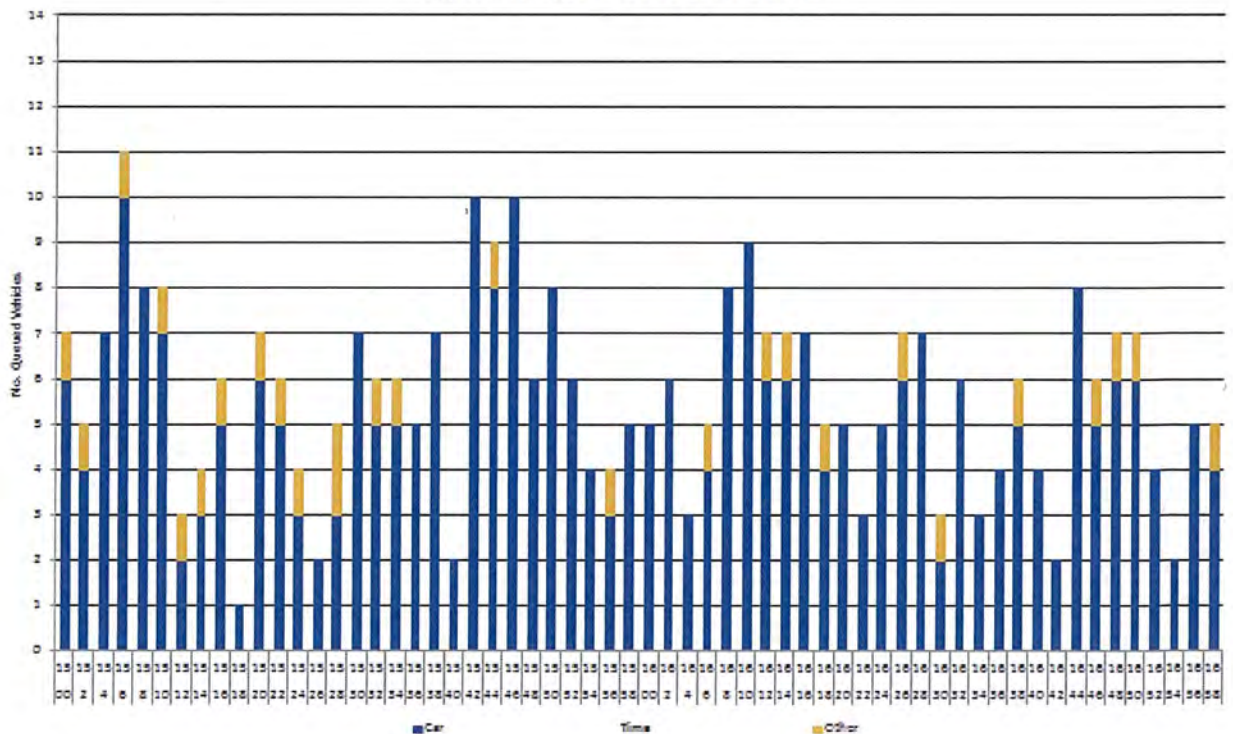
Client Name: Puma Energy

Project Name: 224 Clontarf Rd, Hamilton Hill TIS

Carrington St Queue Lengths 14th May 2015
Right Lane PM 1500 - 1700



Carrington St Queue Lengths 14th May 2015
Right Turning Lane PM 1500 - 1700



Client Name: Puma Energy
Project Name: 224 Clontarf Rd, Hamilton Hill TIS

Carrington Street, Gaps for northbound traffic (i.e. cars turning left from Clontarf and right from Carrington Street – Site 1

No. Gaps / No. Cars Out	Start Time	No. Gaps / No. Cars Out	Start Time
68	0730	57	1500
97		165	
66	0745	59	1515
62		154	
65	0800	69	1530
77		130	
61	0815	62	1545
57		149	
59	0830	66	1600
109		132	
76	0845	67	1615
112		160	
69	0900	63	1630
140		114	
71	0915	69	1645
148		134	

Carrington Street, Gaps for southbound traffic (i.e. cars turning right from median on Carrington Street – Site 2

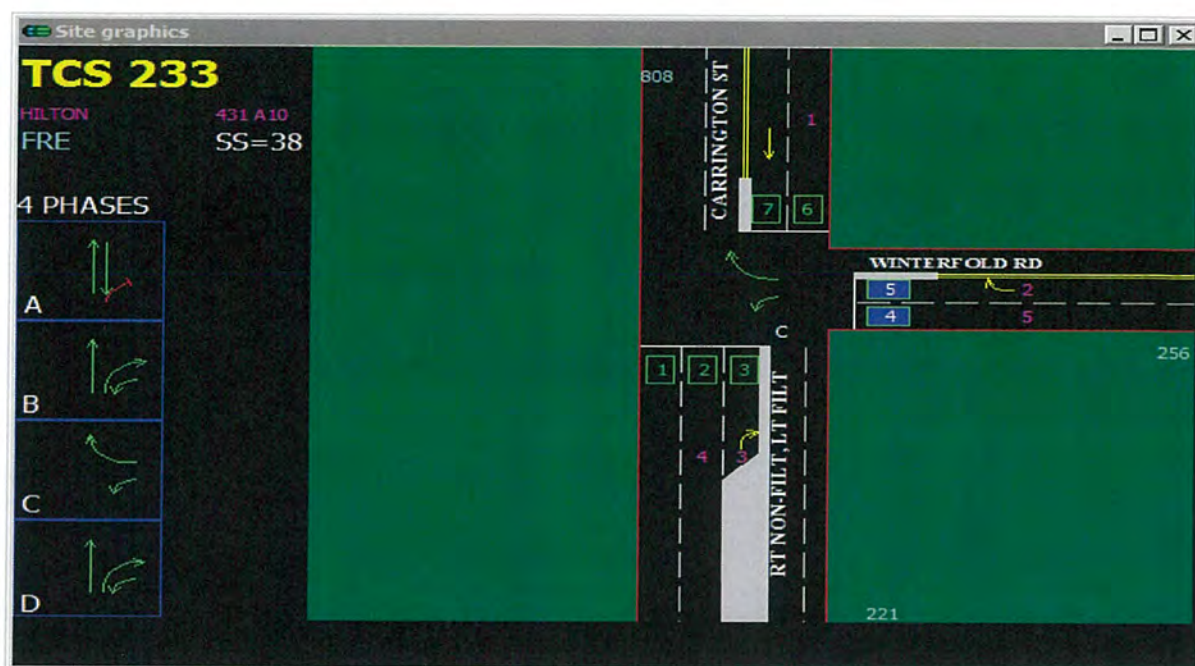
No. Gaps / No. Cars Out	Start Time	No. Gaps / No. Cars Out	Start Time
35	0730	52	1500
253		189	
34	0745	40	1515
242		186	
33	0800	47	1530
270		179	
48	0815	43	1545
214		186	
31	0830	36	1600
226		178	
35	0845	42	1615
231		178	
38	0900	35	1630
238		199	
45	0915	41	1645
234		181	

Clontarf Street, Gaps for eastbound traffic (i.e. cars turning left from crossover or right into crossover

No. Gaps / No. Cars Out	Start Time	No. Gaps / No. Cars Out	Start Time
31	0730	58	1500
228		152	
29	0745	42	1515
214		177	
33	0800	37	1530
155		190	
35	0815	38	1545
166		186	
33	0830	37	1600
204		189	
35	0845	37	1615
190		195	
35	0900	46	1630
207		168	
30	0915	38	1645
206		206	

Clontarf Street, Gaps for westbound and eastbound traffic (i.e. cars turning left from crossover or right into crossover

No. Gaps / No. Cars Out	Start Time	No. Gaps / No. Cars Out	Start Time
3	0730	71	1500
167		83	
12	0745	60	1515
139		108	
14	0800	63	1530
83		101	
3	0815	64	1545
73		119	
25	0830	67	1600
135		97	
16	0845	69	1615
126		109	
20	0900	65	1630
147		108	
10	0915	68	1645
133		101	



	Fri	Sat	Sun	Mon	Tues	Wed	Thurs
Loops	8-May	9-May	10-May	11-May	12-May	13-May	14-May
1,2	7055	6027	5305	6195	6565	6611	6721
3	3449	2725	2569	3087	3109	3195	3275
4	3070	2477	2294	2724	2738	2906	2976
5	2728	2339	2176	2436	2666	2640	2763
6,7	9511	7991	7336	8208	8868	8767	8955
	25813	21559	19680	22650	23946	24119	24690
	25813	21559	19680	22650	23946	24119	24690

% turning left from Carrington Street north into Winterfold Rd = 21%

Carrington St, south of Winterfold Rd = 20018

	Thurs		Thurs
Loops	Am 8-9	Loops	Pm 4-5
1,2	743	1,2	535
3	299	3	271
4	252	4	262
5	319	5	233
6,7	595	6,7	940



Consulting Civil & Traffic
Engineers, Risk Managers.

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E | admin@shawmac.com.au

16th June 2015

Mr. Andrew Lefort
City of Cockburn
9 Coleville Crescent
SPEARWOOD WA 6163

Dear Andrew:

Re: Peer Review of Transport Impact Statement– Proposed Service Station and Kiosk, 224 Clontarf Road, Hamilton Hill of Review – Lot 9500, Thomas Road, Byford Local Structure Plan: Transport Assessment (Prepared by TARSC, 29/08/13) – Response to Updated Transport Assessment (prepared by Applicant's Consultant DVC – dated May 2015)

Shawmac has undertaken a detailed peer review of the updated Transport Impact Statement prepared by Donald Veal Consultants for the proposed Puma Service Station and Kiosk, to be located at 224 Clontarf Road, Hamilton Hill, in the City of Cockburn, following the SAT mediation which occurred on Friday 8th May 2015. This updated review has been prepared on behalf of the City in the current State Administrative Tribunal Proceedings for the application and is to be presented at the SAT mediation scheduled for 16th June 2015.

This detailed review has been based upon the applicant's updated Transport Statement Report dated May 2015 with the following a summary of the key issues which have been identified in the context of this review:

Background and Existing Conditions:

- The proposal consists of a petrol station and kiosk to consist of 8 fuelling bays plus a kiosk.
- The previous land uses on the site consisted of a KFC fast food outlet; however, the site has been effectively vacant for several years with existing ambient traffic volumes on the adjacent road network reflective of the existing lack of activity on the subject site. The updated assessment is reflective of no reduction in site-generated traffic as a result of the previous uses on the site.

- The existing traffic volumes documented for the site were sourced from MRWA and the City of Cockburn for 2011, 2012 and 2013 operating conditions. These traffic volumes cannot be considered current as more up-to-date data was sourced from MRWA for March 2015 conditions. Based upon a review of the existing traffic volumes, the DVC assessment did not consider the traffic volumes north of Winterfold Road or on Winterfold Road, east of Carrington Street with a focus only on the adjacent roads. It is not clear if the existing volumes documented in the assessment are reflective of traffic conditions prior to the KFC outlet being decommissioned on the site.
- Additional traffic surveys undertaken by DVC in April 2015 on Clontarf Road are not reflective of typical weekday traffic demands as some counts were undertaken on 21st April 2015 when two of the major local schools were not operating to full capacity and therefore the traffic volumes would not be representative of base traffic demands.
- Current traffic volumes for Carrington Street and Winterfold Road have not been used as a basis for the assessment (i.e. 2015).
- SCATS data obtained by Shawmac for the signalised intersection of Carrington Street/Winterfold Road is reflective of current traffic demands as the data used for the basis of this review was distilled from the week of 23rd March 2015. It should be noted that the DVC updated assessment has been based on sourcing up-to-day and current ambient boundary road network volumes.

Review of Updated DVC Traffic Assessment

- The estimated traffic generation for the proposal is reasonable and consistent with standard traffic engineering practice with the absolute impact associated with the proposal considered in terms of potential impacts on the boundary road network as it was conveyed in previous communication and at the 8th May 2015 mediation that the KFC fast food outlet had been decommissioned several years prior and hence existing road network volumes were reflective of this.
- The proposed distribution of traffic with 50% of the site-generated activity originating from and destined to the west of the site via Clontarf Road is not reasonable or reflective of spatial land use distribution patterns in the area, existing road traffic patterns or proposed access arrangements to and from the site as noted in the April 2015 DVC assessment. The proposal will clearly generate a significant amount of passing trade, as noted in the report, in the order of 60% which is typically drawn from the major road frontage along the site, namely Carrington Street. It is unlikely therefore that such a significant proportion of custom associated with the proposal would draw from the local area to the west of the site and a trip distribution pattern of 20 to 25% would be far more reasonable and reflective of existing custom on Clontarf Road. Therefore, the impacts on Carrington Street are not correctly reflected in the report with the proposed traffic generated at the Carrington Street crossover and at the unsignalised intersection of Carrington Street/Clontarf Street to accommodate inbound and outbound right-turns would be significantly increased from that documented in the April 2015 DVC report. The updated site traffic distribution and assignment documented in the May 2015 assessment is now reflective of expected desire lines for customers including passing trade.
- Based upon a detailed SIDRA assessment undertaken for Shawmac for the signalised Carrington Street/Winterfold Road intersection using typical midweek March 2015 traffic data, it can be concluded that this intersection is currently operating close to capacity during the existing weekday a.m. and p.m. peak hours with the 95th percentile queue estimated on all approaches to the intersection, including a downstream queue past the proposed

crossover to Carrington Street during both peak hours and past the Carrington Street/Clontarf Street during the a.m. peak hour. The updated DVC assessment has included consideration of the impacts of the site under a future weekday a.m. and p.m. peak hour scenario and the results are reasonable with a limited impact associated with the proposal on the traffic operations at this location.

- A review of the queuing and gap surveys at the Carrington Street/Clontarf Road intersection are reasonable and reflective of existing conditions and the results of the adjusted SIDRA assessment are considered to be acceptable.
- The absolute increase in traffic associated with the proposal should be considered in the context of existing 2015 ambient base traffic volumes with the net difference between the former KFC outlet and the proposal irrelevant as the previous activity on the site ceased some time ago. The updated assessment still compares the traffic generated by the previous decommissioned uses on the site with the proposed uses and this is not relevant in this assessment. However, the increase in site-generated traffic in the context of the boundary roads can be accommodated within the practical capacity of the higher order road network, namely Carrington Street and Winterfold Road; however, the increase in traffic on Clontarf Road is significant west of Carrington Street and has been noted incorrectly in the updated assessment in Section 5.3 with an increase of 142 vehicles per day associated with the proposal as this represents the previous assumptions of 50% to 60% of site-generated traffic would be passing trade and would apply to traffic on Clontarf Road. It is more reasonable to assume that due to the proposed development crossover to Clontarf Road functioning as the primary site crossover that there will be significant increase in traffic on a short section of Clontarf Road immediately west of Carrington Street, regardless of the quantum of passing trade which would reduce through volumes on Carrington Street. The quantum of site-generated traffic west of the development crossover to Clontarf Road can be accommodated within the practical capacity of the road.
- The proximity of the proposed crossover to Carrington Street approximately 30m south of stop line to the northbound approach to the signalised Carrington Street/Winterfold Road signalised intersection was identified as potentially an issue with regard to the potential unsafe weaving manoeuvres for vehicles attempting to cross two through lanes to gain access to the northbound right-turn lane. A weave assessment has not been provided as part of the updated assessment and should be undertaken to confirm if this manoeuvre is safe and efficient and does not present any impacts to the existing risk profile for Carrington Street along the frontage of the site.
- The proposal to facilitate tanker movements on the site inbound at Carrington Street and outbound at Clontarf Road is preferable than the converse (inbound at Clontarf Road and outbound at Carrington Street); however, issues associated with this manoeuvre have been addressed in the updated assessment with a detailed series of AutoTurn schematics provided. The manoeuvring into the site at Carrington Street and out via the Clontarf Road crossover is considered to be acceptable with the required modifications to the site plan with regard to the width of the Carrington Street crossover satisfactory to accommodate this movement. A site management plan for the proposal should confirm that that delivery protocols will be restricted to entering via Carrington Street and exiting via Clontarf Road.
- Rubbish and service/delivery entry and exit manoeuvring is acceptable as noted in the updated report.
- A design audit should be undertaken to identify the any roadside and on-site safety and conflict points and any modifications to existing median islands and road seal, line marking and signage which may be required to accommodate site-generated traffic.

In summary, a review of the updated DVC Transport Assessment for the proposed Puma Service Station and kiosk indicates that the additional information documented therein and revised assumptions have confirmed that the site-generated traffic associated with the proposal can generally be accommodated within the practical capacity of the boundary road network. The proposed entry/exit protocols for fuel tankers and service/delivery and rubbish collection vehicles via entry at Carrington Street and exit at Clontarf Road are deemed a preferable and safer option than the original proposal. Confirmation of safe manoeuvring of eastbound/outbound vehicles from the Carrington Street crossover to navigate into the northbound-eastbound right-turn pocket at the signalised Carrington Street/Winterfold Road intersection is still required as a design audit of the site as noted previously in this advice. My recommendation to Council would be that subject to confirmation of the manoeuvring issue, delivery/service/fuel tanker entry/exit protocols and satisfactory signage, line marking and way finding elements included in the detailed design of the site, the proposal's layout is satisfactory and consistent with acceptable traffic engineering standards and guidelines and that the boundary road network can accommodate the site-generated traffic with minimal impacts to queuing, delay or efficient operations.

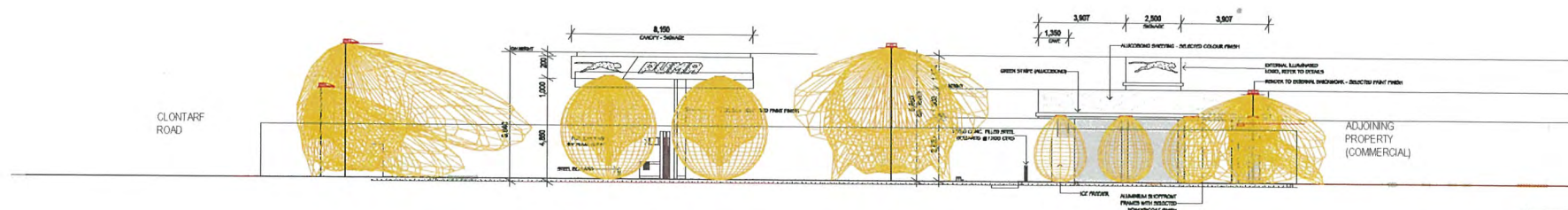
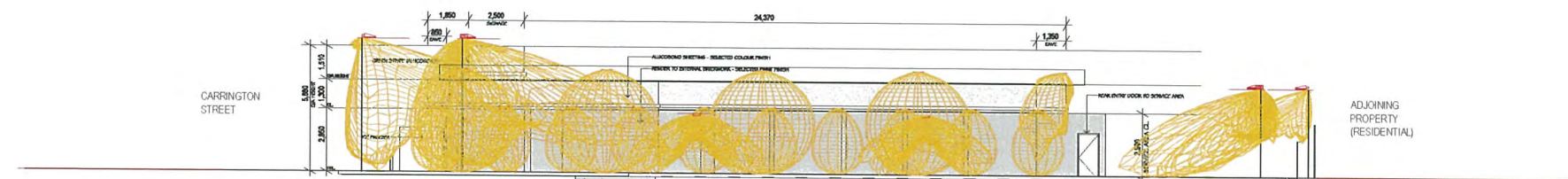
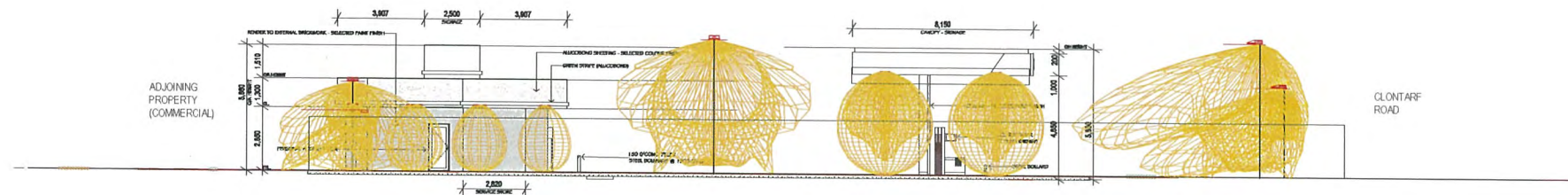
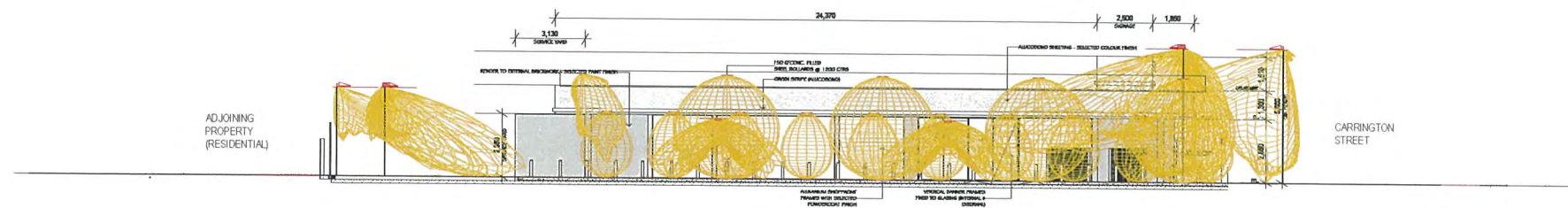
Should you have any queries, please do not hesitate to contact me at 9355 1300 or on my mobile 0434 189 788.

Yours sincerely,



Heidi Lansdell, B.ASc. (Civil), MUDIA, MAITPM, MCILT, MPIA (Assoc.)

Principal Transport Consultant

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NINON ST Pty Ltd

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4	PROJECT No. 2227	
	DATE	2-26-75
	BY/APP	SAP

	PROPOSED PUMA DEVELOPMENT
	LOT 55 No 224 CLONTARF ROAD
	HAMILTON HILL, WA 6163
	ELECTRICAL SERVICES
	LIGHTING

SCALE	NOT TO SCALE
DRAWING NUMBER	
2227-SK01	
SHEET 1 OF 1	REVISED

Attach 8

Summary of the Residents ongoing concerns SAT mediation 17 June 2015

We acknowledge the receipt of the additional information gained by Puma Energy Australia from Planning Solutions (dated 29 May 2015), DVC Consultants (Traffic Report) and Herring Storer Acoustics (Noise report).

This is Puma's third occasion on which it has provided expert evidence. This confirms the residents' concerns that Puma's expert reports lacked the necessary detail to support the proposed development on this site. Even with this third attempt to amend the expert evidence there are still significant gaps and errors in the information which will be highlighted in this response.

Residents do not want the petrol station built on the proposed site, regardless of restricted operating hours or other conditions that might be imposed. Residents have raised on numerous occasions their concerns in respect to the amenity of life and the significant impact on their health and safety should this proposal be approved. These areas have not been address satisfactorily with no additional information being provided on these core issues therefore the residents' views have not changed; their concerns have not been allayed. The proposed development is not supported with the only key limitations being made to operating hours.

At the previous mediation session we raised a number of concerns that have not been addressed in the additional information provided. Issues raised that still need to be addressed include:

1. When considering an application *Part 10 Procedure for dealing with Applications* of the City of Cockburn Town Planning Scheme No 3, needs to be considered, specifically:
 - **Compatibility** - need to be able to coexist with a proposed 24 hour business where 50% of boundaries are residential. Resident within 2 metres of site plus multistorey dwellings including over 55yrs
 - **Amenities of life** - for local residences many of whom are retired and home for most of the day and night. Living in close proximity of a 19 hours plus (when including set up and pack up) business. Impact of odour/emissions, noise - including potential for anti social behaviour late at night, lighting and traffic.
 - **Relationship** - to adjoining land and the impact on local residents and local restaurants - odour and traffic flow including right of way
 - **Access** - the updated report does not take into account the two way right of way and the impact on the existing traffic who use this route
 - **Traffic** - Puma has determined the level of transport assessment as '**moderate impact**' i.e. 10-100 vehicle trips (Western Australian Planning Commission's *Transport Assessment Guidelines for Developments Volume 4 Individual Developments 2006*) although it is expected that **over 100 vehicles trips** are expected in peak hour it was determined that a significant number are passing traffic and therefore **not likely to impact on overall traffic**. We previously asked the question as to whether this determination was confirmed by the approving authority and as yet we have not received a response.

The additional information provided does not address any of the above concerns.

2. **Odour, emissions and lighting pollution** - this is a significant concern for residents. At the previous mediation session we outlined specific concerns with the report provided by Puma however these have not been addressed:
 - no mention of spillage - which is a regular occurrence at a petrol filling station
 - that report noted that odour and/or vapour omitted is unlikely to extend much past 4m from the bowser
 - we raised a question as to whether Vapour Recovery System 2 was to be installed however this has not been addressed
 - the proposed fill box location required confirmation that no ignition source would be within 3m however the neighbouring residence is within this clearance. This point has not been addressed
 - the impact on the health of residents - as discussed at the last mediation there are many research articles that link benzene to health issues - again no information has been provided to address this major concern.

3. **Spills and Safety** - as discussed at mediation, there have been three metropolitan petrol filling incidents in recent years. We referred to the explosion at a Maddington site which had this occur on the proposed site, the neighbouring residence would have been totally destroyed and resident's lives put at risk given the resident's issues with mobility, this increases the issue of safety within such a close proximity to the tanker filling box.

4. **Business hours and lack of need**
 - Puma had raised the argument that the proposed extended hours (5-6am and 10pm-12am) are consistent with the hours of other businesses in the area. At the previous mediation we were clearly able to confirm that all local businesses other than the Chinese Restaurant and the United Petroleum petrol station, close by 10pm. Despite this, Puma have again restated this argument as follows:

The proposed trading hours are also consistent with the established 'night time' trade in the local centre, which includes a Red Rooster restaurant, Hamilton Tavern, and other restaurants. Observation of the local centre indicates these businesses trade up to and beyond 10:00pm. (Numbered paragraph 3 on page 4 of Puma's covering letter to the Third Revision).

This blatant attempt on Puma's behalf to continue this point when it has been proven incorrect makes residents concerned that it may be attempting to mislead the Tribunal.
 - Previously, Puma claimed it would lose 9% of total sales if it were not permitted to open from 5-6am and 10pm-12am daily; a total of three hours. Puma now alleges it will lose 61 sale transactions that on average occur in those three hours. These figures were identified through a preliminary investigation which has a number of flawed assumptions. It is unclear if Puma has taken into account the existence of the United Petroleum petrol station which is 100m from the proposed site and would be currently providing a service to these 'potential' customers.
 - Economic loss to Puma is irrelevant - Puma's potential 'lost sales' should not be taken into account in determining the hours of operation. As stated at the previous mediation, if this is

to be considered then so too should the collective economic loss (due to the devaluation of their properties/homes) residents will suffer if the petrol station is approved.

5. Traffic Statement Report

Much of the new traffic data was difficult to interpret and in some areas we feel that the details are incorrect or potentially misleading.

There has been several drastic changes in the updated DVC report. For example the previous traffic report stated a 13m tanker that has now become a 19m tanker.

The DVC data is based on a four-hour survey of traffic at the intersection of Carrington Street and Clontarf Road on 14 May 2015. We continue to believe that this statistical sample is far too small to be meaningful and is not an acceptable basis to make conclusions about the effect of the proposed development upon current traffic conditions. Similarly, the queuing and gap surveys detailed in Section 3.4 of the DVC report is flawed in that it too is based on a statistical sample spanning one four hour period of time on one day.

Incomplete data and hence analysis

The Western Australian Department of Planning and Infrastructure identifies matters to be considered when assessing projected traffic congestion expected to be created by individual developments. These are set out in its report titled *Transport Assessment: Guidelines for Developments, Volume 4 – Individual Developments, Version for Trial & Evaluation August 2006*. Part C of the report discusses the scope of the assessment that should be undertaken in assessing the impact of a proposed development on traffic flow. Section 6.3 looks at the parameters to be considered, identifying them as:

- *trip generation rates*
- *proportions of pass-by, diverted and new trips*
- *directional distribution of trips*
- *future year base flows*
- *traffic growth rates.*

DVC's report is deficient when compared against this measure as it does not consider future year base flows and traffic growth rates. This indicates that the analysis undertaken is limited, thereby making it unreliable.

Dangerous congestion at Carrington Street

(a) Twice daily peak hours

DVC's report analyses the statistics and concludes traffic will not bank up onto busy Carrington Street behind vehicles travelling westbound on Clontarf Road seeking to enter the proposed site for the petrol station. However, this statistical analysis is contrary to the reality of the situation based upon the experience of residents in the area, including residents who live within 50 metres of the relevant intersection.

Local residents know that traffic banks up onto busy Carrington Street at least during morning and afternoon peak hours.

The facts are as follows:

- Clontarf Road, which is the main entrance, is only 30 metres from the intersection with Carrington Street.
- At the point of the entrance to the proposed site, Clontarf Road is one lane wide heading west and two lanes wide heading east.
- Carrington Street is classified as a District Distributor A road under Main Roads WA's *Functional Road Hierarchy*. As such it is a very busy road, as Puma's own expert reports show (20,000 vehicles per day).
- Many large vehicles travel the route from Carrington Street turning onto Clontarf Road past the entrance to the proposed site including buses (two bus routes use this section of road) and trucks as long as buses and even longer. It is important to note that **this is not a fact that DVC takes into account in its report**. If it did, it might significantly change its conclusions thereby bringing its opinion in line with the residents' observed reality.

Traffic congestion is highly likely to be the result if you build a busy petrol station on the proposed site. This was the case some years ago at the height of KFC's popularity when it operated on the site; and this is the situation the residents envisage as being likely to happen again every peak hour if a busy petrol station is in essence substituted for what was once a busy KFC store. In particular:

- Cars travelling east along Clontarf Road already regularly bank up past the Clontarf Road entrance to the proposed site, hence blocking that entrance off to vehicles seeking to access it travelling west along Clontarf Road.
- Vehicles travelling west along Clontarf Road will bank behind any vehicle seeking to turn into the proposed petrol station.
- As there are only 30 metres to Carrington Street, it only takes one bus and two or three cars before traffic banks back onto Carrington Street congesting what is a very busy road and intersection.

Even without a busy KFC store operating on the site, as was once the case, DVC's report makes this concession stating:

It is acknowledged that during the peak hours the queue along Clontarf Street from its intersection with Carrington Street does at times block the driveway. (First paragraph under Table 5.2 on page 22)

The experience of residents suggests that the description "does at times" rather significantly undersells the true position. At peak hour, traffic is almost always banked back past the site's Clontarf Road entrance. This was certainly the case when the KFC store was in its prime.

The report goes on to say:

While the Clontarf Road queue extended to the crossover, temporarily restricting access, the queue does clear quickly and provides numerous gaps to allow for traffic to exit/enter the site. (First paragraph under Table 5.2 on page 22)

Again, residents' experience suggests otherwise. The queue does not clear quickly during peak periods. Further, the suggestion that there will be "numerous gaps" for cars to enter/exit the site is fanciful, which means that DVC's entire congestion analysis is not reflective of reality.

First, gaps sufficient to allow traffic turning right into the site from Clontarf Road have to span two lanes of banked up traffic. This reduces the chances of there being a sufficient gap to drive through. Without a gap this will cause a banking of traffic back into the Carrington Street intersection.

It is very important to realise that at the same time as traffic trying to enter the petrol station will also need to contend with cars attempting to leave the site. These cars will be turning either left onto Clontarf Road to then turn right into Carrington Street or turning right into Clontarf Road which is even more problematic. Traffic trying to find 'gaps' in peak hour traffic at this entry/exit driveway is extremely problematic which is compounded by the close proximity of the Carrington Street intersection.

The DVC report's failure to consider these practical details not only means its gap analysis is flawed, but has broader implications as it is indicative of its overall failure to consider the realities of the situation.

We request that the Tribunal take into serious consideration the real life experience of local residents who live on that intersection, (all of whom were also in residence when KFC was fully functional) to the limited and flawed analysis offered by DVC.

(b) Petrol price wars

In addition to peak hour traffic levels, the situation will be far worse if Puma adopts a competitive pricing strategy, which might be likely to allow its new business to get a foothold in the market by undercutting its closest competitor merely 100 metres away. This might likely result in greater traffic congestion than seen during current peak periods. This has not been considered in any reports provided.

Road safety concerns

Congestion in itself is undesirable and proper planning should seek to minimise it. However, far worse is the dangerous conditions this scenario creates on a District Distributor A road. The road safety implications must be given greater weight.

DVC's unfounded assumptions

We find that the latest version of DVC's report is greatly altered from their previous version with no reason stated as to why it has done so, even though the circumstances previously reported on have not changed. **This has resulted in residents becoming sceptical on the objectiveness of this report.**

The latest version of DVC's report makes assumptions that are glaringly different to those it made in its earlier versions. For instance, DVC looks at the route 'passing trade' customers are likely to take in entering the site. Its:

1. Second revision of its 'Final Report' dated 24 April 2015 makes the following assumptions:
 - 25% westbound along Clontarf Road;
 - 25% eastbound along Clontarf Road;
 - 25% northbound along Carrington Street; and
 - 25% southbound along Carrington Street.
2. Third revision of its 'Final Report' dated 28 May 2015 makes the following assumptions:
 - 55% northbound along Carrington Street;
 - 25% westbound along Clontarf Road from Carrington Street (north or south);
 - 15% eastbound along Clontarf Road and
 - 5% southbound along Carrington Street.

(See section 5.2 of both versions of the report)

The third revision states:

The percentages have been based on the passing traffic flows with only a small percentage allocated to Carrington Street southbound as passing trade on a divided carriageway is likely to look for a service station on the same side of the road. (First paragraph, page 19)

Yet, the second revision states the assumptions it made “to be appropriate” (second paragraph on page 16).

It clearly appears that DVC does not now believe the assumptions it made in the second revision were appropriate, yet at the time it stood behind those figures and would have the Tribunal rely on them.

Another example of a flawed assumption it has made is in its conclusion. Here, the report states:

SIDRA analyses of the [sic] Clontarf Road and Carrington Street indicates there is minimal difference in the operation of the intersection with the additional traffic from the development. The additional traffic represents approximately a 3% and 4% increase in traffic volumes already passing through the intersection in the am and pm peak hours, hence the minimal difference in the results. (Second last paragraph on page 31)

This is based on the assumption that customers who are otherwise passing by should not be included in the congestion analysis, this assumption does not take into consideration that in fact a passing vehicle that actually stops at the site and spends time at the site is actually building to the congestion for a longer period of time. Instead of merely passing through it now needs to enter and exit the site creating more congestion and wait time at this already busy intersection.

The DVC data at pages 44-46 of its latest report only considers the number of vehicles waiting in traffic at the Carrington Street and Clontarf Road intersection, it does not consider the period of time they waited for.

Conclusion: Traffic congestion

We strongly feel that the weight that can be afforded to the DVC report must be diminished as it not only fails to look at the practicalities and the likely reality that the residents will be left to bear but its conclusions and assumptions appear to change whenever a concern is raised by the residents. It continues to rely on out-of-date statistics and large quantities of data that may or may not have any bearing on the core concerns of the local residents who have seen a number of traffic calming initiatives in nearby streets in fact increase the traffic along Clontarf Road.

We feel that Puma has in fact failed to establish the true impact its proposed development will have on the current and projected future traffic flow. They have failed to even mention that two bus routes travel along Clontarf Road with a bus stop a mere 100 metres from the intersection and even less to the entry and exit of the site.

6. Refuelling tanker Route and filling point

Puma is proposing that refuelling tankers access the site from Carrington Street. The following problems have been identified with this route:

1. The refuelling tankers (which are now noted as 19 metres long not 13 metres as in the previous update) will cause traffic congestion heading north along Carrington Street as one approaches the traffic lights at the intersection with Winterfold Road as it will require the

full width of Carrington Street to enable this turn to occur. This impact has not been acknowledged in the traffic report. The third updated DVC report assumes the refuelling tankers are 19 metres in length and states "The refuelling tanker will be able to stop along the western boundary of the site to refuel without blocking the access on Clontarf Road or disrupting general access to the refuelling bowzers used by the public." (Second paragraph on page 8 of the report). Residents are sceptical as to how increasing the length of the tanker by almost 50% results in less disruption when all else remains the same. A 19-metre tanker has the potential to not only cause disruption to the site, but also off site if it blocks the right-of-way Puma propose to use to access the site.

2. The right-of-way takes traffic in two directions, however, this will not be possible when a tanker is present in the right-of-way. Potentially dangerous situations might arise whereby traffic will be required to back-out of the right-of-way to allow the tankers to pass.
3. The site is really just too small for the proposed development. The 'tight fit' of the tanker route, the impact to traffic on Carrington Street and the narrow existing dual lane right of way is problematic. In addition the tanker refuelling point which is located within 2 metres of a residential property is completely inappropriate and undesirable. The proposed refuelling point is in fact located in the worst position possible on the site, but the small size of the site means that relocating it to another location poses a significant challenge.

Widening of the Carrington Street crossover

DVC relies upon Main Roads policy to justify the widening of the Carrington Street crossover to enable a 19m tanker to enter the right of way. The DVC report states:

The Main Roads WA driveway policy states the following: "Service stations on a corner lot may have one driveway up to 11.0m wide to a State road and another to the minor road. Those not on a corner may have two driveways, each up to 11.0m wide." (Fourth last paragraph on page 8)

DVC rely on this policy to justify a widening of the Carrington Street driveway. However, the Carrington Street driveway is not a driveway on the proposed site. The proposed site has two driveways: one exiting onto Clontarf Road and the other exiting onto the existing right of way at its northwest corner. It does not currently have any driveway exiting onto Carrington Street.

It would seem that Puma cannot rely on the Main Roads policy by applying it to the existing Carrington Street driveway on the neighbouring property. Clarity needs to be sought on this issue. If Puma does wish to rely on this policy it seems that it will need to create its own driveway onto Carrington Street and this no doubt will cause the layout of the site to be altered resulting in reduced bowzers and required parking bays potentially making the business unviable.

7. Noise Pollution

No site inspection

We are very concerned that from the report we received it would appear that once again Herring Storer Acoustics have not actually visited the site. This, we feel, is vital for gaining a true understanding of how the residents surrounding the site will be impacted especially given that bedroom windows are a mere metre away from the boundary fence that adjoins the site.

Undisclosed substantive changes

In the Third Revision the Herring Storer report states that the additional acoustic report has been undertaken to 'address and clarify' the matters discussed at mediation yet we feel that this is not the case. In fact it would appear that Herring Storer's letter dated 28 May 2015 does more than merely 'address and clarify' as it has altered the receiver locations without providing an explanation as to why. (Compare figure 1 on page 6 of Herring Storer's (revised) report dated 22 April 2015 ('the HS Report') with figure 1 on page 5 of its letter dated 28 May 2015 (HS's 'Third Revision')). This just raises more questions by residents:

- Have other changes been made?
- How have the changes influenced the results?

Tyre air fill point and petrol bowser noise

Herring Storer (HS) has chosen not to provide any analysis or data on the noise that would be emitted from a tyre filling point based entirely on Puma telling them that there would not be a 'beep' sound. They therefore conclude that noise levels would comply.

We understand from our own anecdotal investigations of similar sites that the beeping noise emitted from the tyre filling point **and** the petrol bowser pumping fuel is very loud and intrusive at night. Given that the neighbouring resident's bedroom window is a mere metre from the boundary fence regardless of the location of the tyre filling point night time noises will be very loud and intrusive.

Mechanical services

As noted above, HS has changed its receiver locations. One change was to omit completely receiver location A. If a site visit had been undertaken it would have been obvious that receiver point A was by far the closest of the receiver points to the mechanical services (air-conditioning units, exhaust fans and refrigeration units). Its omission raises concerns about the veracity of Herring Storer's finding, and arguably Herring Storer's impartiality.

We feel that HS makes statements in its Third Revision are plainly wrong where it states the following:

For the mechanical services, the critical location is the first storey of residence E. (Second note at the foot of page 2)

We feel that the most critical location is receiver location A as shown in the HS Report, which is omitted from HS's Third Revision. A simple site visit noting the extremely close location of the bedroom window of the neighbouring residence would have noted this.

It is clear from HS's Third Revision that its conclusions are all hypothetical. It concedes this by stating:

We note that the mechanical services have not been designed at this stage. However, from another similar project (i.e. proposed petrol station located at Lot 216 Rockingham Road, Hamilton Hill) the mechanical services would comprise:

- 6 off air conditioning condensing units; 52 dB(A) @ 1m;
- 3 exhaust systems; 1 at 47 dB(A) @ 3m and 2 at 57 dB(A) @ 3m; and
- 3 refrigeration units at 63 dB(A) @ 3m.

(First paragraph under 'Mechanical Services' heading on page 2 of HS's Third Revision)

No basis is given for why the proposed petrol station located at Lot 216 Rockingham Road, Hamilton Hill is similar to that proposed for this site. Some of the more obvious questions that arise are:

- What is the size of each of the buildings?
- What are the sizes of the respective refrigeration units in the respective proposed petrol stations?
- How likely is it that the proposed petrol station located at Lot 216 Rockingham Road, Hamilton Hill will actually be built in the manner described here?
- Are there any residential building in such close proximity to the site as is the case here?

Without answers to these questions it is not possible to test Herring Storer's reliance on the plans for the alleged 'similar' proposed site. Did they for example test the noise levels at any neighbouring residences to conclude that all noise levels comply?

Night-time noise

Finally, HS's Third Revision also contains a note stating that its analysis "*does not consider any diversification that would occur during the night / evening period.*" (First note at the foot of page 2)

We are not certain what is meant by diversification in this context, but it is concerning that the report's findings are limited when it comes to evening and especially night-time noise levels. As previously stated we have spoken to residents who live within close proximity of a petrol filling station and they state that noise levels at night are a significant factor that impacts on their amenity of life.

Given that the neighbouring residence has its bedroom windows so close to the site and the fact that both residents are elderly and suffer from various health conditions we feel that the noises coming from the petrol filling station will be significant and distracting.

Air compressor location

In its Third Revision Herring Storer state:

- "the exact location of the air compressor has not been determined"
- "From information received we understand that the air compressor will be an [sic] Royce RC20/100" (First paragraph page 3)

This assessment is speculation upon speculation and proves nothing. Our own investigations have revealed that in fact air compressors are not located at the tyre filling station but rather at the back of the building. HS concede that this may be the case but still insist that the noise emissions would comply at all times. They also identify the 'worse case location' as the first floor of receiver E (Refer to Figure 1 of the Third Revision to identify the location of the receiver points).

We dispute this and feel that receiver A should be considered the 'worse case location' especially given that the neighbouring residence that runs along the common boundary with the proposed site has its bedroom windows located along this boundary.

Tanker start and air brake release

Failure to disclose testing conditions

Herring Storer state “[t]o determine the noise emissions from [the refuelling tanker starting and air brake air release], noise level measurement were undertaken of a tanker.” (First paragraph under heading ‘Tanker Start and Air Brake Air Release’ on page 3)

Herring Storer fail to provide information as to where it chose to take its measurements

Up until its Third Revision of expert evidence, Puma had referred to a 13-metre long refuelling tanker, but in the Third Revision it refers to a 19-metre tanker without mention of the former 13-metre tanker or explanation for the increase in size by almost 50%.

Herring Storer do not disclose whether the tanker it measured was 13 or 19 metres in length, or indeed some other length. This would presumably make a material difference as a larger tanker would have a larger and therefore louder engine, this would be especially crucial at tanker start-up and move-off given that it is sitting right on the boundary fence with the neighbouring property.

Herring Storer has shielded its methodology from scrutiny by not disclosing its testing conditions. With that must come a corresponding diminishing in the weight that can be given to its results.

No explanation for inconsistent results

Herring Storer considered the tanker start noise in the HS Report, where it said “we have calculated the noise level at the closest neighbouring residence based on sound power level of 89 dB(A)” (HD Report, first paragraph in section 4.3 on page 5). It appears that this would exceed the maximum noise level for compliance, which is 72 dB(A) (HD Report, second paragraph in section 4.3 on page 5).

However, in HS’s Third Revision it finds engine start noise at the same location to merely be 59 dB(A), to which 10 dB(A) is added as a penalty due to the impulsive nature of the noise. Herring Storer now concludes that the engine start noise of 69 dB(A) is compliant with relevant standards.

HS’s Third Revision completely ignores its own inconsistent findings in its earlier HS Report. **How Herring Storer went from a noncompliant 89 dB(A) to a compliant 69 dB(A) is not even mentioned, or explained.**

A discrepancy this large should be addressed. Failure to do so seriously detracts from the weight that can be afforded to Herring Storer’s opinion.

Air brake air release

Curiously, air brake air release noise was the one source of noise that Herring Storer did not earlier consider in the HS Report; and as it happens it is the one and only noise it now finds to be noncompliant. But for the mediation process directing it to consider this noise, it would not have done so.

The consequence of this is significant.

- In the HS Report, Herring Storer concludes the existing 1.8 metre high colourbond boundary fence on the western boundary of the proposed site “is adequately [*sic*] and does not need to be upgraded.” (Last paragraph on page 10)
- In HS’s Third Revision, Herring Storer now concede that for the noise emissions from the air brake to comply with the assigned noise level, a 20 metre long 2.4 metre high barrier is required to be installed along the western boundary of the proposed site. (Last paragraph on page 3)

Puma has put Herring Storer forward as expert in noise emissions, yet Herring Storer has completely changed its findings. It has gone from saying everything is fine and there is no need to take any noise abatement measures; to stating the construction of a 20 metre long by 2.4 metre high fence is now required if noise pollution levels are to be met. Worse still, it has only changed its opinion in this regard when directed to do so following concerns raised in the mediation process.

8. Vibrations

Another factor that seems to be completely overlooked in this assessment is the adverse effects of vibrations.

A 19-metre long tanker parallel parked within a metre of your boundary and 3 metres from your bedroom wall, sitting idly potentially with its engine on for several minutes at a time will produce significant vibration and exhaust emissions.

There has been no assessment of the extent to which these vibrations would be felt in the neighbouring property and the potential damage it might cause.

It is also to be noted that while Puma say refilling will be limited to daytime hours, it does not disclose how many times a week refilling will take place, nor how long it will take to refill the underground tanks.

CITY OF COCKBURN
MUNICIPAL BANK ACCOUNT

OCM 13/8/2015 Item 15. 1

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF085817	11741	WATC LOAN REPAYMENTS	4/06/2015	748,327.95
EF085818	11867	KEVIN JOHN ALLEN MONTHLY COUNCILLOR ALLOWANCE	5/06/2015	2,575.00
EF085819	12740	MAYOR LOGAN HOWLETT MONTHLY COUNCILLOR ALLOWANCE	5/06/2015	11,158.33
EF085820	19059	CAROL REEVE-FOWKES MONTHLY COUNCILLOR ALLOWANCE	5/06/2015	4,398.96
EF085821	20634	LEE-ANNE SMITH MONTHLY COUNCILLOR ALLOWANCE	5/06/2015	2,575.00
EF085822	21185	BART HOUWEN MONTHLY COUNCILLOR ALLOWANCE	5/06/2015	2,575.00
EF085823	23338	STEVE PORTELLI MONTHLY COUNCILLOR ALLOWANCE	5/06/2015	2,575.00
EF085824	23339	STEPHEN PRATT MONTHLY COUNCILLOR ALLOWANCE	5/06/2015	2,575.00
EF085825	23340	SHAHYAZ MUBARAKAI MONTHLY COUNCILLOR ALLOWANCE	5/06/2015	2,575.00
EF085826	25352	LYNDSEY WETTON MONTHLY COUNCILLOR ALLOWANCE	5/06/2015	2,575.00
EF085827	25353	PHILIP EVA MONTHLY COUNCILLOR ALLOWANCE	5/06/2015	2,575.00
EF085828	10154	AUST TAXATION DEPT PAYROLL DEDUCTIONS	8/06/2015	312,276.00
EF085829	11789	WALGA ADVERTISING/TRAINING SERVICES	8/06/2015	469.70
EF085830	12025	TELSTRA CORPORATION COMMUNICATIONS SERVICES	8/06/2015	4,606.28
EF085831	12656	COOGEE BEACH SURF LIFESAVING CLUB INC POOR GROVE SLSC DEFECTS LIABILITY RETENTION	8/06/2015	155,280.82
EF085832	18553	SELECTUS PTY LTD PAYROLL DEDUCTIONS	8/06/2015	10,785.07
EF085833	19157	LOMBARD THE PAPER PEOPLE STATIONERY SUPPLIES	8/06/2015	83.41
EF085834	21547	MUCKY DUCK BUSH BAND ENTERTAINMENT	8/06/2015	1,400.00
EF085835	23250	DEPARTMENT OF PLANNING DAP APPLICATIONS & DAP FEES	8/06/2015	12,442.00
EF085836	25987	TOYOTA FLEET MANAGEMENT PAYROLL DEDUCTIONS - NOVATED LEASE	8/06/2015	567.62
EF085837	99997	COC GRANTS, DONATIONS & REFUNDS GRANTS, DONATIONS & REFUNDS	8/06/2015	320.00
EF085838	99997	EMMANUEL CATHOLIC COLLEGE SUSTAINABILITY GRANT	8/06/2015	1,800.10
EF085839	99997	PERTH WALDORF SCHOOL SUSTAINABILITY GRANT	8/06/2015	519.00

CITY OF COCKBURN
MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF085840	99997	HARBEST LAKES RESIDENTS ASSOCIATION SUSTAINABILITY GRANT	8/06/2015	3,936.09
EF085841	99997	EARTH EVENTUS SUSTAINABILITY GRANT	8/06/2015	2,730.82
EF085842	99997	VOLUNTEER TASK FORCE SUSTAINABILITY GRANT	8/06/2015	1,862.96
EF085843	99997	PHOENIX PRIMARY SCHOOL SUSTAINABILITY GRANT	8/06/2015	2,464.00
EF085844	99997	AT THE KITCHEN SUSTAINABILITY GRANT	8/06/2015	3,800.00
EF085845	99997	SOUTH LAKE PRIMARY SCHOOL SUSTAINABILITY GRANT	8/06/2015	2,786.00
EF085846	99997	SUSTAINABLE TRADIES SUSTAINABILITY GRANT	8/06/2015	1,300.00
EF085847	99997	JOEL CHAN SUSTAINABILITY GRANT	8/06/2015	700.00
EF085848	99997	HARMONY PRIMARY SCHOOL SUSTAINABILITY GRANT	8/06/2015	550.00
EF085849	99997	KERRY STREET COMMUNITY SCHOOL SUSTAINABILITY GRANT	8/06/2015	3,718.00
EF085850	99997	GATEWAY COMMUNITY CHURCH COMMUNITY GRANT	8/06/2015	1,650.00
EF085851	99997	DYNAMIC FLAME BADMINTON COMMUNITY GRANT	8/06/2015	2,833.00
EF085852	99997	JANDAKOT VOLUNTEER BUSH FIRE BRIGADE COMMUNITY GRANT	8/06/2015	375.00
EF085853	99997	FRIENDS OF THE COMMUNITY INC. COMMUNITY GRANT	8/06/2015	5,500.00
EF085854	99997	COCKBURN SENIOR CITIZENS ASSOCIATION COMMUNITY GRANT	8/06/2015	1,650.00
EF085855	99997	ATWELL COMMUNITY ASSOCIATION COMMUNITY GRANT	8/06/2015	889.00
EF085856	99997	GIRL GUIDES WA INC COMMUNITY GRANT	8/06/2015	1,080.75
EF085857	99997	YANGEBUP FAMILY CENTRE INC SUSTAINABLE EVENTS GRANT	8/06/2015	2,750.00
EF085858	99997	SECOND HARVEST INC DONATION	8/06/2015	14,000.00
EF085859	99997	BUSINESS FOUNDATIONS INC DONATION	8/06/2015	10,000.00
EF085860	99997	CITY OF COCKBURN PIPE BAND DONATION	8/06/2015	9,000.00
EF085861	99997	CONSTALE CARE CHILD SAFETY FOUNDATION DONATION	8/06/2015	12,000.00
EF085862	99997	THE CHURCHES COMMISSION ON EDUCATION DONATION	8/06/2015	9,000.00

CITY OF COCKBURN
MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF085863	99997	PETS OF OLDERS PERSONS WA INC DONATION	8/06/2015	2,450.00
EF085864	99997	SOUTH LAKE OTTEY FAMILY & NEIGHBOURH DONATION	8/06/2015	10,000.00
EF085865	99997	COCKBURN VOLUNTEER SEA SEARCH & RESC DONATION	8/06/2015	8,500.00
EF085866	99997	PORT SCHOOL DONATION	8/06/2015	15,000.00
EF085867	99997	VOLUNTEER HOME SUPPORT DONATION	8/06/2015	5,000.00
EF085868	99997	AUSTRALIAN ASSOCIATION FOR ENVIRONME CONTRACTUAL DONATION	8/06/2015	27,500.00
EF085869	99997	ROBERTA BUNCE REIMBURSEMENT OF EXPENSES	8/06/2015	105.50
EF085870	99997	JANET WELLS MILEAGE CLAIM REIMBURSEMENT	8/06/2015	21.00
EF085871	99997	DOLLY HOCHKIRCHER MILEAGE CLAIM REIMBURSEMENT	8/06/2015	155.18
EF085872	99997	DOLLY HOCHKIRCHER MILEAGE CLAIM REIMBURSEMENT	8/06/2015	54.45
EF085873	99996	PROPERTY EFT REFUNDS RATES (NOT BONDS PROPERTY EFT REFUNDS	8/06/2015	1,136.84
EF085874	99996	PROPERTY EFT REFUNDS RATES (NOT BONDS PROPERTY EFT REFUNDS	8/06/2015	7,471.00
EF085875	99996	PROPERTY EFT REFUNDS RATES (NOT BONDS PROPERTY EFT REFUNDS	8/06/2015	1,934.28
EF085876	99996	PROPERTY EFT REFUNDS RATES (NOT BONDS PROPERTY EFT REFUNDS	8/06/2015	74.41
EF085877	99996	PROPERTY EFT REFUNDS RATES (NOT BONDS PROPERTY EFT REFUNDS	8/06/2015	2,986.86
EF085878	99996	PROPERTY EFT REFUNDS RATES (NOT BONDS PROPERTY EFT REFUNDS	8/06/2015	426.00
EF085879	99996	PROPERTY EFT REFUNDS RATES (NOT BONDS PROPERTY EFT REFUNDS	8/06/2015	324.06
EF085880	23410	VMWARE INTERNATIONAL LIMITED SOFTWARE	9/06/2015	8,899.00
EF085881	13910	ATO - DEPUTY COMMISSIONER OF TAXATION FBT PAYMENT	10/06/2015	32,979.96
EF085882	99997	BEN TANO EXPENSES REIMBURSEMENT	10/06/2015	220.50
EF085883	10154	AUST TAXATION DEPT PAYROLL DEDUCTIONS	22/06/2015	367,411.80
EF085884	10683	GRONBEK SECURITY LOCKSMITH SERVICES	22/06/2015	868.40
EF085885	10888	LJ CATERERS CATERING SERVICES	22/06/2015	2,355.10

CITY OF COCKBURN
MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF085886	12565	SOUTHERN METRO REGIONAL COUNCIL - LOAN LOAN REPAYMENT	22/06/2015	388,696.04
EF085887	18553	SELECTUS PTY LTD PAYROLL DEDUCTIONS	22/06/2015	12,110.82
EF085888	24950	BERKELIUM CONSULTING CONSULTANCY SERVICES	22/06/2015	1,410.75
EF085889	25987	TOYOTA FLEET MANAGEMENT PAYROLL DEDUCTIONS - NOVATED LEASE	22/06/2015	567.62
EF085890	99997	SUZANNE VALENTE SHOWOFF ARTWORK PAYMENT	22/06/2015	320.00
EF085891	99997	JOHN FARAC SAFETY PRESCRIPTION GLASSES CONTRIBUTION	22/06/2015	224.00
EF085892	99997	ZURICH INSURANCE AUSTRALIA INSURANCE EXCESS CLAIM NO 633370569	22/06/2015	1,000.00
EF085893	99997	SUSAN ARMSTRONG WHOOPING COUGH VACCINATION REIMBURSEMENT	22/06/2015	42.00
EF085894	99997	SANDRA FONSECA WHOOPING COUGH VACCINATION REIMBURSEMENT	22/06/2015	43.00
EF085895	99997	GEORGIA MAE HEAD WHOOPING COUGH VACCINATION REIMBURSEMENT	22/06/2015	43.00
EF085896	99997	EUGENE LYDON SAFETY PRESCRIPTION GLASSES CONTRIBUTION	22/06/2015	199.00
EF085897	99997	JANDAKOT VOLUNTEER BUSH FIRE BRIGADE REIMBURSEMENT OF EXPENSES	22/06/2015	1,080.99
EF085898	99997	GARETH ROZARIO STUDY FEES CONTRIBUTION	22/06/2015	495.00
EF085899	99997	CAVALIERS NETBALL CLUB SPORTING EQUIPMENT GRANT	22/06/2015	200.65
EF085900	99997	JANDAKOT PARK CRICKET CLUB SPORTING EQUIPMENT GRANT	22/06/2015	962.73
EF085901	99997	JANDAKOT VOLUNTEER BUSH FIRE BRIGADE EXPENSES REIMBURSEMENT	22/06/2015	4,514.58
EF085902	99997	JANDAKOT VOLUNTEER BUSH FIRE BRIGADE EXPENSES REIMBURSEMENT	22/06/2015	1,147.87
EF085903	10152	AUST SERVICES UNION PAYROLL DEDUCTIONS	8/06/2015	3,332.82
EF085904	10733	HOSPITAL BENEFIT FUND PAYROLL DEDUCTIONS	8/06/2015	1,054.30
EF085905	11001	MUNICIPAL EMPLOYEES UNION PAYROLL DEDUCTIONS	8/06/2015	698.40
EF085906	11856	WA LOCAL GOVERNMENT SUPER PLAN PAYROLL DEDUCTIONS	8/06/2015	335,280.09

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Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF085907	11857	CHAMPAGNE SOCIAL CLUB PAYROLL DEDUCTIONS	8/06/2015	1,128.80
EF085908	11859	STAFF SOCIAL CLUB PAYROLL DEDUCTIONS	8/06/2015	50.60
EF085909	11860	45S CLUB PAYROLL DEDUCTIONS	8/06/2015	44.00
EF085910	18005	COLONIAL FIRST STATE PAYROLL DEDUCTIONS	8/06/2015	409.95
EF085911	18432	HESTA SUPER FUND PAYROLL DEDUCTIONS	8/06/2015	3,461.35
EF085912	18718	FIRST STATE SUPER PAYROLL DEDUCTIONS	8/06/2015	1,175.02
EF085913	19010	SUMMIT PERSONAL SUPER PLAN PASQUALE C PAYROLL DEDUCTIONS	8/06/2015	459.21
EF085914	19193	REST SUPERANNUATION PAYROLL DEDUCTIONS	8/06/2015	1,120.40
EF085915	19726	HEALTH INSURANCE FUND OF WA PAYROLL DEDUCTIONS	8/06/2015	2,617.00
EF085916	19727	MTAA SUPER FUND PAYROLL DEDUCTIONS	8/06/2015	405.09
EF085917	19997	AUSTRALIANSUPER PAYROLL DEDUCTIONS	8/06/2015	19,736.77
EF085918	20056	CBUS PAYROLL DEDUCTIONS	8/06/2015	1,916.89
EF085919	20300	CATHOLIC SUPER & RETIREMENT FUND PAYROLL DEDUCTIONS	8/06/2015	974.78
EF085920	20406	HOSTPLUS SUPERANNUATION FUND PAYROLL DEDUCTIONS	8/06/2015	1,011.09
EF085921	20755	COLONIAL FIRST STATE - ROBERT GRAEME V PAYROLL DEDUCTIONS	8/06/2015	29.64
EF085922	21365	ING LIFE - ONEANSWER PERSONAL SUPER PAYROLL DEDUCTIONS	8/06/2015	131.90
EF085923	21921	MAURICIO FAMILY PAYROLL DEDUCTIONS	8/06/2015	1,947.14
EF085924	22067	STEPHENS SUPERANNUATION FUND PAYROLL DEDUCTIONS	8/06/2015	724.34
EF085925	22901	FONTANA SUPER PLAN PAYROLL DEDUCTIONS	8/06/2015	1,313.20
EF085926	23695	NETWEALTH INVESTMENT & SUPERANNUATI PAYROLL DEDUCTIONS	8/06/2015	620.36
EF085927	24620	E & B PINTO SUPERANNUATION FUND PAYROLL DEDUCTIONS	8/06/2015	1,165.36
EF085928	24642	TWUSUPER PAYROLL DEDUCTIONS	8/06/2015	749.25
EF085929	24813	KINETIC SUPER PAYROLL DEDUCTIONS	8/06/2015	726.19

CITY OF COCKBURN
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Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF085930	25043	COLONIAL FIRST STATE - KERRY MARGARET PAYROLL DEDUCTIONS	8/06/2015	154.38
EF085931	25051	ANZ SMART CHOICE SUPER R BARRETT PAYROLL DEDUCTIONS	8/06/2015	1,242.22
EF085932	25495	ONEPATH CUSTODI PAYROLL DEDUCTIONS	8/06/2015	508.68
EF085933	25538	NORTH PERSONAL SUPERANNUATION PLAN PAYROLL DEDUCTIONS	8/06/2015	411.90
EF085934	25590	FIRST CHOICE WHOLESALE - M SAPSWORTH PAYROLL DEDUCTIONS	8/06/2015	745.84
EF085935	25649	COMMONWEALTH BANK GROUP SUPER PAYROLL DEDUCTIONS	8/06/2015	449.92
EF085936	25950	BT LIFETIME SUPER - EMPLOYER PLAN PAYROLL DEDUCTIONS	8/06/2015	583.46
EF085937	25956	COLONIAL FIRST STATE - KHRISTINE ANNE F PAYROLL DEDUCTIONS	8/06/2015	165.85
EF085938	25963	ONEPATH SUPER - RACHEL PLEASANT PAYROLL DEDUCTIONS	8/06/2015	620.36
EF085939	26071	NORTH PERSONAL SUPERANNUATION PLAN SUPERANNUATION FUND	8/06/2015	198.68
EF085940	26089	ESSENTIAL SUPPERANNUATION FUND SUPPERANNUATION	8/06/2015	1,242.22
EF085941	26144	COLONIAL FIRST STATE - GLENN PETHICK PAYROLL DEDUCTIONS	8/06/2015	603.66
EF085942	26154	AMP FLEXIBLE LIFETIME SUPER RAYMOND D PAYROLL DEDUCTIONS	8/06/2015	530.30
EF085943	26161	THE TRUSTEE FOR ELDER CLAN SUPERANNU SUPERANNUATION FUND	8/06/2015	518.12
EF085944	26311	SUNSUPER PTY LTD PAYROLL DEDUCTIONS	8/06/2015	1,024.26
EF085945	26373	ONE PATH MASTERFUND - ERIN MAE RUKMIN PAYROLL DEDUCTIONS	8/06/2015	392.16
EF085946	26374	IOOF SUPERANNUATION FUND PAYROLL DEDUCTIONS	8/06/2015	982.97
EF085947	26405	ANZ SMART CHOICE SUPER R GATLAND PAYROLL DEDUCTIONS	8/06/2015	358.91
EF085948	26445	ANZ SMART CHOICE SUPER C NORMAN PAYROLL DEDUCTIONS	8/06/2015	451.64
EF085949	26468	SULLIVAN SUPER FUND PTY LTD PAYROLL DEDUCTIONS	8/06/2015	4,300.74
EF085950	26504	NGS SUPER SHANTELLE TYLOR PAYROLL DEDUCTIONS	8/06/2015	426.13
EF085951	26538	BT RETIREMENT WRAP ASANKA VIDANAGE 3 PAYROLL DEDUCTIONS	8/06/2015	549.99
EF085952	26553	ONE PATH MASTERFUND 3597 LEEANNE DAV PAYROLL DEDUCTIONS	8/06/2015	18.24

CITY OF COCKBURN
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Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF085953	10590	DEPARTMENT OF FIRE AND EMERGENCY SERVICES ESL REMITTANCE	29/06/2015	1,239,214.97
EF085954	10023	ACTIV FOUNDATION INC PACKAGING SERVICES	30/06/2015	4,541.13
EF085955	10058	ALSCO PTY LTD HYGIENE SERVICES/SUPPLIES	30/06/2015	2,233.26
EF085956	10071	AUSTRALASIAN PERFORMING RIGHT ASSOC. LICENCE - PERFORMING RIGHTS	30/06/2015	126.51
EF085957	10091	ASLAB PTY LTD ASPHALTING SERVICES/SUPPLIES	30/06/2015	1,894.24
EF085958	10118	AUSTRALIA POST POSTAGE CHARGES	30/06/2015	31,303.10
EF085959	10160	DORMA AUSTRALIA PTY LTD AUTOMATIC DOOR SERVICES	30/06/2015	220.00
EF085960	10184	BENARA NURSERIES PLANTS	30/06/2015	9,526.98
EF085961	10207	BOC GASES GAS SUPPLIES	30/06/2015	1,236.06
EF085962	10212	BOSS BOLLARDS SECURITY PRODUCTS	30/06/2015	621.50
EF085963	10220	BOYA EQUIPMENT EQUIPMENT SUPPLIES	30/06/2015	146.09
EF085964	10221	BP AUSTRALIA LIMITED DIESEL/PETROL SUPPLIES	30/06/2015	10,079.75
EF085965	10226	BRIDGESTONE AUSTRALIA LTD TYRE SERVICES	30/06/2015	31,861.78
EF085966	10236	BG & E PTY LTD CONSULTANCY SERVICES	30/06/2015	1,617.00
EF085967	10239	BUDGET RENT A CAR - PERTH MOTOR VEHICLE HIRE	30/06/2015	426.88
EF085968	10244	BUILDING & CONST INDUSTRY TRAINING FUN LEVY PAYMENT	30/06/2015	286,333.72
EF085969	10246	BUNNINGS BUILDING SUPPLIES PTY LTD HARDWARE SUPPLIES	30/06/2015	3,069.05
EF085970	10247	BUNZL AUSTRALIA LTD PAPER/PLASTIC/CLEANING SUPPLIES	30/06/2015	195.84
EF085971	10255	CABCHARGE AUSTRALIA PTY LTD CABCHARGES	30/06/2015	596.21
EF085972	10256	CABLE LOCATES & CONSULTING LOCATING SERVICES	30/06/2015	1,015.30
EF085973	10280	CCH AUSTRALIA LIMITED REPORTING SERVICES	30/06/2015	2,872.00
EF085974	10333	CJD EQUIPMENT PTY LTD HARDWARE SUPPLIES	30/06/2015	1,744.42
EF085975	10335	CLASSIC HIRE EQUIPMENT HIRING SERVICES	30/06/2015	577.50

CITY OF COCKBURN
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Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF085976	10346	COATES HIRE OPERATIONS PTY LTD EQUIPMENT HIRING SERVICES	30/06/2015	631.18
EF085977	10348	COCA COLA AMATIL SOFT DRINK SUPPLIES	30/06/2015	1,198.70
EF085978	10349	COCKBURN BASKETBALL ASSOC INC ELECTRICITY REIMBURSEMENTS	30/06/2015	220.00
EF085979	10353	COCKBURN CEMENT LTD RATES REFUND	30/06/2015	710.16
EF085980	10358	COCKBURN LIQUOR CENTRE LIQUOR SUPPLIES	30/06/2015	166.86
EF085981	10368	COCKBURN WETLANDS EDUCATION CENTRE COMMUNITY GRANT	30/06/2015	10,940.50
EF085982	10375	VEOLIA ENVIRONMENTAL SERVICES WASTE SERVICES	30/06/2015	6,108.68
EF085983	10380	COLQUHOUNS FREMANTLE BAG COMPANY WOVEN BAGS	30/06/2015	352.00
EF085984	10384	PROGILITY PTY LTD COMMUNICATION SERVICES	30/06/2015	7,240.53
EF085985	10394	CD'S CONFECTIONERY WHOLESALERS CONFECTIONERY	30/06/2015	551.30
EF085986	10460	DAVID WILLS & ASSOCIATES CONSULTANCY SERVICES	30/06/2015	1,331.50
EF085987	10483	LANDGATE MAPPING/LAND TITLE SEARCHES	30/06/2015	11,254.41
EF085988	10498	DIGITAL MAPPING SOLUTIONS COMPUTER SOFTWARE	30/06/2015	1,716.00
EF085989	10512	DOMUS NURSERY VARIOUS PLANTS	30/06/2015	10,999.67
EF085990	10526	E & MJ ROSHER PTY LTD MOWER PARTS	30/06/2015	1,505.95
EF085991	10527	EAGLE SPORTS SPORTING GOODS	30/06/2015	2,228.33
EF085992	10535	WORKPOWER INCORPORATED EMPLOYMENT SERVICES - PLANTING	30/06/2015	13,791.58
EF085993	10550	EMERALD PEST CONTROL PEST CONTROL SERVICES	30/06/2015	440.00
EF085994	10580	FC COURIERS COURIER SERVICES	30/06/2015	2,018.45
EF085995	10608	FORESHORE REHABILITATION & LANDSCAPE FENCING/LANDSCAPING SERVICES	30/06/2015	2,673.00
EF085996	10611	FORPARK AUSTRALIA PLAYGROUND EQUIPMENT	30/06/2015	73,926.60
EF085997	10636	FUJI XEROX AUSTRALIA PTY LTD PHOTOCOPY CHARGES	30/06/2015	16,632.64
EF085998	10641	GALVINS PLUMBING SUPPLIES PLUMBING SERVICES	30/06/2015	2,711.83

CITY OF COCKBURN
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EF085999	10655	GHD PTY LTD CONSULTANCY SERVICES	30/06/2015	74,349.97
EF086000	10679	GRASSTREES AUSTRALIA PLANTS & PLANTING SERVICES	30/06/2015	18,810.00
EF086001	10681	GREENING AUSTRALIA WA VEGETATION CONSULTANCY SERVICES	30/06/2015	2,750.00
EF086002	10692	AECOM AUSTRALIA PTY LTD CONSULTANCY SERVICES	30/06/2015	123,372.28
EF086003	10709	HECS FIRE FIRE SYSTEM MAINTENANCE	30/06/2015	29,525.10
EF086004	10711	HERALD PUBLISHING COMPANY PTY LTD ADVERTISING SERVICES	30/06/2015	2,633.40
EF086005	10726	HOLTON CONNOR ARCHITECTS & PLANNERS ARCHITECTURAL SERVICES	30/06/2015	9,386.30
EF086006	10739	HYDRAMET PTY LTD POOL PARTS/EQUIPMENT	30/06/2015	722.90
EF086007	10779	J F COVICH & CO PTY LTD ELECTRICAL SERVICES	30/06/2015	45,359.44
EF086008	10781	JANDAKOT EARTHMOVING & RURAL CONTRA FIREBREAK CONSTRUCTION	30/06/2015	13,660.00
EF086009	10783	JANDAKOT METAL INDUSTRIES METAL SUPPLIES	30/06/2015	1,072.50
EF086010	10787	JANDAKOT ACCIDENT REPAIR CENTRE PANEL BEATING SERVICES	30/06/2015	2,902.00
EF086011	10791	JASMAN ENTERPRISES PTY LTD HIGH PRESSURE CLEANING	30/06/2015	676.50
EF086012	10794	JASON SIGNMAKERS SIGNS	30/06/2015	2,055.90
EF086013	10814	JR & A HERSEY PTY LTD SAFETY CLOTHING SUPPLIES	30/06/2015	917.85
EF086014	10836	KERB DOCTOR CONCRETE KERBING - SUPPLY & LAYING	30/06/2015	10,964.25
EF086015	10879	LES MILLS AEROBICS INSTRUCTION/TRAINING SERVICES	30/06/2015	1,105.38
EF086016	10884	WSP BUILDINGS PTY LTD CONSULTANCY SERVICES	30/06/2015	21,120.00
EF086017	10888	LJ CATERERS CATERING SERVICES	30/06/2015	9,482.00
EF086018	10906	AUSTRALIAN PLANT WHOLESALERS VARIOUS PLANTS	30/06/2015	200.20
EF086019	10913	BUCHER MUNICIPAL PTY LTD PURCHASE OF NEW PLANT / REPAIR SERVICES	30/06/2015	37,063.99
EF086020	10918	MAIN ROADS WA REPAIRS/MAINTENANCE SERVICES	30/06/2015	63,788.85
EF086021	10923	MAJOR MOTORS PTY LTD REPAIRS/MAINTENANCE SERVICES	30/06/2015	179.66

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EF086022	10931	MARLBROH BINGO ENTERPRISES BINGO EQUIPMENT	30/06/2015	34.00
EF086023	10939	LINFOX ARMAGUARD BANKING SECURITY SERVICES	30/06/2015	1,843.55
EF086024	10942	MCGEES PROPERTY PROPERTY CONSULTANCY SERVICES	30/06/2015	7,700.00
EF086025	10944	MCLEODS LEGAL SERVICES	30/06/2015	16,052.69
EF086026	10960	METRO FILTERS FILTER SUPPLIES	30/06/2015	704.50
EF086027	10968	MINIQUIP HIRING SERVICES	30/06/2015	19,278.60
EF086028	10981	MOBILE MASTERS COMMUNICATIONS EQUIPMENT/SERVICES	30/06/2015	264.00
EF086029	10990	MOWER CITY SALES & SERVICES PTY LTD LAWN MOWING EQUIPMENT	30/06/2015	1,567.00
EF086030	10991	BEACON EQUIPMENT MOWING EQUIPMENT	30/06/2015	3,735.15
EF086031	11022	NATIVE ARC GRANTS & DONATIONS	30/06/2015	275.00
EF086032	11026	NESTLE FOOD SERVICES CATERING SUPPLIES	30/06/2015	400.20
EF086033	11028	NEVERFAIL SPRINGWATER LTD BOTTLED WATER SUPPLIES	30/06/2015	799.85
EF086034	11036	NORTHLAKE ELECTRICAL ELECTRICAL SERVICES	30/06/2015	12,138.18
EF086035	11070	OTIS ELEVATOR COMPANY ELEVATOR REPAIRS/MAINTENANCE	30/06/2015	1,975.81
EF086036	11077	P & G BODY BUILDERS PTY LTD PLANT BODY BUILDING SERVICES	30/06/2015	5,549.50
EF086037	11136	DONEGAN ENTERPRISES FENCING REPAIRS/MAINTENANCE	30/06/2015	10,995.60
EF086038	11152	FULTON HOGAN INDUSTRIES PTY LTD ROAD MAINTENANCE	30/06/2015	6,415.20
EF086039	11164	PMP PRINT PTY LTD PRINTING SERVICES	30/06/2015	275.00
EF086040	11182	PREMIUM BRAKE & CLUTCH SERVICE BRAKE SERVICES	30/06/2015	5,860.14
EF086041	11205	QUALITY TRAFFIC MANAGEMENT PTY LTD TRAFFIC CONTROL SERVICES	30/06/2015	23,782.49
EF086042	11208	QUICK CORPORATE AUSTRALIA PTY LTD STATIONERY/CONSUMABLES	30/06/2015	8,899.10
EF086043	11235	REINFORCED CONCRETE PIPES PTY LTD CONCRETE PIPE SUPPLIES	30/06/2015	6,260.87
EF086044	11240	INITIAL HYGIENE RENTOKIL INITIAL PRT LTD SANITARY SERVICES	30/06/2015	539.78

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EF086045	11264	ROCLA PIPELINE PRODUCTS CONCRETE LINER SUPPLIES	30/06/2015	5,705.03
EF086046	11268	TASKERS PTY LTD REPAIRS/MAINTENANCE TO SAILS	30/06/2015	297.51
EF086047	11294	SAFEMAN (WA) PTY LTD PROTECTIVE CLOTHING/EQUIPMENT	30/06/2015	476.52
EF086048	11307	SATELLITE SECURITY SERVICES PTY LTD SECURITY SERVICES	30/06/2015	12,719.38
EF086049	11308	SBA SUPPLIES HARDWARE SUPPLIES	30/06/2015	5,059.37
EF086050	11318	SELECT SECURITY WA PTY LTD SECURITY SERVICES	30/06/2015	345.00
EF086051	11331	SHAWMAC PTY LTD CONSULTANCY SERVICES - CIVIL	30/06/2015	2,805.00
EF086052	11333	SHELFORD CONSTRUCTIONS PTY LTD CONSTRUCTION SERVICES	30/06/2015	168,208.70
EF086053	11361	SIGMA CHEMICALS PTY LTD CHEMICAL SUPPLIES	30/06/2015	2,190.16
EF086054	11373	SKIPPER TRUCK PARTS SPARE PARTS & MAINTENANCE SERVICES	30/06/2015	1,971.49
EF086055	11380	SNAP PRINTING FREMANTLE PRINTING SERVICES	30/06/2015	2,728.00
EF086056	11387	BIBRA LAKE SOILS SOIL & LIMESTONE SUPPLIES	30/06/2015	1,025.00
EF086057	11425	SOUTHERN METROPOLITAN REGIONAL COUN WASTE DISPOSAL GATE FEES	30/06/2015	903,940.43
EF086058	11447	SPEARWOOD DALMATINAC CLUB INC COMMUNITY GRANT	30/06/2015	19,123.05
EF086059	11453	SPEARWOOD NEWSROUND NEWSPAPER SUPPLIES	30/06/2015	1,129.60
EF086060	11459	SPEARWOOD VETERINARY HOSPITAL VETERINARY SERVICES	30/06/2015	950.00
EF086061	11469	SPORTS TURF TECHNOLOGY TURF CONSULTANCY SERVICES	30/06/2015	5,016.00
EF086062	11470	SPORTSWORLD OF WA SPORT SUPPLIES	30/06/2015	257.40
EF086063	11483	ST JOHN AMBULANCE AUST WA OPERATIONS FIRST AID COURSES	30/06/2015	1,891.20
EF086064	11488	POSITION PARTNE INSTRUMENT SUPPLIES	30/06/2015	673.09
EF086065	11502	STATE LAW PUBLISHER ADVERTISING SERVICES	30/06/2015	97.30
EF086066	11505	STATE LIBRARY OF WESTERN AUSTRALIA BOOK SUPPLIES	30/06/2015	2,553.10
EF086067	11543	SWIM AUSTRALIA SPORTING EQUIPMENT	30/06/2015	469.00

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EF086068	11546	T FAULKNER & CO INSTALLATIONS/SUPPLY OF HAND RAILS	30/06/2015	28,341.00
EF086069	11557	TECHNOLOGY ONE LTD IT CONSULTANCY SERVICES	30/06/2015	364,760.77
EF086070	11594	THE MEN OF THE TREES SUPPLY OF PLANTS	30/06/2015	5,536.80
EF086071	11609	THOMSON REUTERS (PROFESSIONAL) AUSTR SOFTWARE SUPPORT/LICENCE FEES	30/06/2015	16,064.40
EF086072	11625	TOTAL EDEN PTY LTD RETICULATION SUPPLIES	30/06/2015	8,740.72
EF086073	11642	TRAILER PARTS PTY LTD TRAILER PARTS	30/06/2015	2,071.54
EF086074	11651	TREE WATERING SERVICES TREE WATERING SERVICES	30/06/2015	20,544.00
EF086075	11655	TRISLEYS HYDRAULIC SERVICES PTY LTD POOL EQUIPMENT/REPAIRS	30/06/2015	495.00
EF086076	11657	TRUCKLINE PARTS CENTRES AUTOMOTIVE SPARE PARTS	30/06/2015	1,995.46
EF086077	11665	TUNNEL VISION PLUMBING SERVICES	30/06/2015	561.00
EF086078	11667	TURFMASTER FACILITY MANAGEMENT TURFING SERVICES	30/06/2015	73,601.96
EF086079	11697	VAT MAN-FAT FILTERING SYSTEMS FILTER CLEANING SERVICES	30/06/2015	456.10
EF086080	11699	VERNON DESIGN GROUP ARCHITECTURAL SERVICES	30/06/2015	1,434.40
EF086081	11701	VIBRA INDUSTRIA FILTER SUPPLIES	30/06/2015	639.10
EF086082	11708	VITAL PACKAGING PTY LTD PACKAGING SUPPLIES	30/06/2015	4,675.00
EF086083	11715	WA BLUEMETAL ROADBASE SUPPLIES	30/06/2015	46,769.13
EF086084	11722	WA HINO SALES & SERVICE PURCHASE OF NEW TRUCKS / MAINTENANCE	30/06/2015	5,307.58
EF086085	11726	WA LIMESTONE LIMESTONE SUPPLIES	30/06/2015	18,161.32
EF086086	11739	WA SPIT ROAST COMPANY CATERING SERVICES	30/06/2015	5,116.00
EF086087	11742	WACKER NEUSON PTY LTD HARDWARE/EQUIPMENT SUPPLIES	30/06/2015	1,925.00
EF086088	11743	WA COUNCIL OF SOCIAL SERVICE INC TRAINING SERVICES	30/06/2015	5,940.00
EF086089	11749	WARRENS EARTHMOVING CONTRACTORS EARTHMOVING SERVICES	30/06/2015	2,475.00
EF086090	11773	WESFARMERS LANDMARK LIMITED CHEMICAL SUPPLIES	30/06/2015	12,393.70

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EF086091	11789	WALGA ADVERTISING/TRAINING SERVICES	30/06/2015	4,852.83
EF086092	11793	WESTERN IRRIGATION PTY LTD IRRIGATION SERVICES/SUPPLIES	30/06/2015	29,513.63
EF086093	11806	WESTRAC PTY LTD REPAIRS/MTNCE - EARTHMOVING EQUIPMENT	30/06/2015	1,822.32
EF086094	11810	ABAXA PREVIOUSLY WH LOCATIONS LOCATING SERVICES	30/06/2015	10,433.58
EF086095	11822	WOOD & GRIEVE ENGINEERS ENGINEERING CONSULTANCY SERVICES	30/06/2015	16,082.00
EF086096	11824	WORK CLOBBER SAFETY CLOTHING	30/06/2015	300.00
EF086097	11828	WORLDWIDE ONLINE PRINTING - O'CONNOR PRINTING SERVICES	30/06/2015	2,093.68
EF086098	11854	ZIPFORM PRINTING SERVICES	30/06/2015	8,232.30
EF086099	11972	COBEY MAINTENANCE SERVICES TURF MANAGEMENT	30/06/2015	1,980.00
EF086100	11987	SAFETY ZONE AUSTRALIA PTY LTD SAFETY EQUIPMENT	30/06/2015	1,230.26
EF086101	11990	EARTHCARE (AUSTRALIA) P/L LANDSCAPING SERVICES	30/06/2015	8,404.00
EF086102	11993	BLUE HEELER TRADING CLOTHING SUPPLIES	30/06/2015	1,385.45
EF086103	12007	SHANE MCMASTER SURVEYS SURVEYING SERVICES	30/06/2015	12,485.00
EF086104	12014	TUTT BRYANT EQUIPMENT BT EQUIPMENT P EXCAVATING/EARTHMOVING EQUIPMENT	30/06/2015	5,220.83
EF086105	12018	O'CONNOR LAWMOWER & CHAINSAW CENTRE MOWING EQUIPMENT/PARTS/SERVICES	30/06/2015	264.00
EF086106	12079	CHARTER PLUMBING & GAS PLUMBING SERVICES	30/06/2015	359.70
EF086107	12123	TRANEN PTY LTD PLANT SUPPLIES	30/06/2015	36,532.31
EF086108	12127	ABLE WESTCHEM CHEMICAL/CLEANING SUPPLIES	30/06/2015	344.11
EF086109	12146	BUSWEST TRANSPORT SERVICES - BUS HIRE	30/06/2015	1,562.00
EF086110	12153	HAYS PERSONNEL SERVICES PTY LTD EMPLOYMENT SERVICES	30/06/2015	3,146.63
EF086111	12207	CIVICA PTY LTD SOFTWARE SUPPORT/LICENCE FEES	30/06/2015	90,409.00
EF086112	12415	FACE PAINTING FUN AND GAMES ENTERTAINMENT SERVICES	30/06/2015	1,710.00
EF086113	12458	KITE KINETICS ENTERTAINMENT SERVICES	30/06/2015	550.00

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EF086114	12500	ELLENBY TREE FARM PLANT SUPPLIES	30/06/2015	1,815.00
EF086115	12542	SEALIN GARLETT CEREMONIAL SERVICES	30/06/2015	500.00
EF086116	12560	AUSTSWIM LTD TRAINING SERVICES	30/06/2015	240.00
EF086117	12589	AUSTRALIAN INSTITUTE OF MANAGEMENT TRAINING SERVICES	30/06/2015	6,380.00
EF086118	12672	NORMAN DISNEY & YOUNG CONSULTANCY SERVICES	30/06/2015	38,500.00
EF086119	12779	WESTERN RESOURCE RECOVERY PTY LTD WASTE DISPOSAL SERVICES	30/06/2015	2,361.12
EF086120	12791	ALCHEMY TECHNOLOGY COMPUTER SOFTWARE SERVICES	30/06/2015	1,034.00
EF086121	12796	ISENTIA PTY LIMITED MEDIA MONITORING SERVICES	30/06/2015	805.60
EF086122	12803	ASSUREX ESCROW PTY LTD ANNUAL SOFTWARE FEE	30/06/2015	1,094.50
EF086123	12849	GIUDICE SURVEYS SURVEYING SERVICES	30/06/2015	5,690.00
EF086124	12883	CONSERVATION VOLUNTEERS AUSTRALIA ENVIRONMENTAL SERVICES	30/06/2015	825.00
EF086125	13037	PPCA LTD LICENCE FEE - SOUND & MUSIC	30/06/2015	499.82
EF086126	13102	MICHAEL PAGE INTERNATIONAL EMPLOYMENT SERVICES	30/06/2015	14,797.75
EF086127	13165	SEBEL FURNITURE LTD FURNITURE SUPPLIES	30/06/2015	1,047.20
EF086128	13409	KLEENIT CLEANING SERVICES	30/06/2015	26,954.30
EF086129	13462	ATI-MIRAGE PTY LTD TRAINING SERVICES	30/06/2015	1,967.00
EF086130	13475	BURGESS RAWSON (WA) PTY LTD VALUATION SERVICES	30/06/2015	9,500.00
EF086131	13563	GREEN SKILLS INC EMPLOYMENT SERVICES	30/06/2015	32,631.13
EF086132	13779	PORTER CONSULTING ENGINEERS ENGINEERING CONSULTANCY SERVICES	30/06/2015	21,070.50
EF086133	13825	JACKSON MCDONALD LEGAL SERVICES	30/06/2015	20,067.28
EF086134	13832	INSIGHT CALL CENTRE SERVICES COMMUNICATION SERVICES	30/06/2015	5,077.33
EF086135	14111	POLYTECHNIC WEST EDUCATIONAL SERVICES	30/06/2015	383.80
EF086136	14118	AGED & COMMUNITY SERVICES WA WORKSHOPS	30/06/2015	215.00

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EF086137	14350	BAILEYS FERTILISERS FERTILISER SUPPLIES	30/06/2015	10,072.44
EF086138	14435	LAKES JUNIOR FOOTBALL CLUB YOUTH ACTIVE PRGM REGISTRATION FEES	30/06/2015	1,800.00
EF086139	14459	BIDVEST (WA) FOOD/CATERING SUPPLIES	30/06/2015	876.36
EF086140	14593	AUSTREND INTERNATIONAL PTY LTD ALUMINIUM SUPPLIES	30/06/2015	4,416.72
EF086141	14667	APPEALING SIGNS SIGNS	30/06/2015	2,090.00
EF086142	14777	LGIS INSURANCE BROKING INSURANCE PREMIUMS	30/06/2015	244.11
EF086143	14831	ANGLICARE WA TRAINING SERVICES	30/06/2015	230.00
EF086144	15072	DRUM PRINT & PUBLICATIONS PRINTING SERVICES	30/06/2015	1,338.00
EF086145	15267	CHEMSEARCH AUSTRALIA CHEMICAL SUPPLIES	30/06/2015	8,637.01
EF086146	15326	DATA DICTION PTY LTD COMPUTER SOFTWARE	30/06/2015	4,400.00
EF086147	15393	GREENWAY ENTERPRISES HARDWARE SUPPLIES	30/06/2015	4,262.39
EF086148	15513	NATIONAL IN HOME CHILD CARE ASSOCIATION MEMBERSHIP	30/06/2015	220.00
EF086149	15550	APACE AID PLANTS & LANDSCAPING SERVICES	30/06/2015	26,207.51
EF086150	15587	DAVIDSON TRAHARE CORPSYCH TRAINING SERVICES	30/06/2015	17,050.00
EF086151	15588	NATURAL AREA MANAGEMENT & SERVICES WEED SPRAYING	30/06/2015	9,029.63
EF086152	15609	CATALYSE PTY LTD CONSULTANCY SERVICES	30/06/2015	53,464.40
EF086153	15625	OPUS INTERNATIONAL CONSULTANTS (PCA) LTD CONSULTANCY SERVICES	30/06/2015	29,480.00
EF086154	15678	A2Z PEST CONTROL PEST CONTROL	30/06/2015	6,827.00
EF086155	15746	WESTERN AUSTRALIA POLICE SERVICE POLICE CLEARANCES	30/06/2015	88.20
EF086156	15764	PITNEY BOWES SOFTWARE PTY LTD EQUIPMENT MAINTENANCE / SOFTWARE LICENCE	30/06/2015	10,167.70
EF086157	15786	AD ENGINEERING INTERNATIONAL PTY LTD SIGNS - ELECTRONIC	30/06/2015	132.00
EF086158	15914	T-QUIP MOWING EQUIPMENT	30/06/2015	630.00
EF086159	15916	1SPATIAL AUSTRALIA ANNUAL SOFTWARE SUBSCRIPTION	30/06/2015	2,420.00

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EF086160	16064	CMS ENGINEERING PTY LTD AIRCONDITIONING SERVICES	30/06/2015	36,185.34
EF086161	16291	WA PROFILING ROAD PROFILING SERVICES	30/06/2015	6,468.00
EF086162	16396	MAYDAY EARTHMOVING ROAD CONSTRUCTION MACHINE HIRE	30/06/2015	107,773.12
EF086163	16704	ACCIDENTAL FIRST AID SUPPLIES - PERTH S MEDICAL SUPPLIES	30/06/2015	469.45
EF086164	16706	COUNCIL ON THE AGEING (WA) INC EDUCATION SERVICES	30/06/2015	300.00
EF086165	16985	WA PREMIX CONCRETE SUPPLIES	30/06/2015	25,663.44
EF086166	16997	AUS SECURE SECURITY SERVICES/PRODUCTS	30/06/2015	200.00
EF086167	17092	CENTRAL SCREENS SECURITY SYSTEMS/PRODUCTS	30/06/2015	200.00
EF086168	17097	VALUE TISSUE PAPER PRODUCTS	30/06/2015	545.60
EF086169	17121	UNDERGROUND POWER DEVELOPMENT PTY L ELECTRICAL SERVICES	30/06/2015	3,520.00
EF086170	17214	FLAMES NETBALL CLUB SPORTS CLUB	30/06/2015	200.00
EF086171	17279	AUSSIE COOL SHADES SHADE SAILS & AWNINGS	30/06/2015	25,982.00
EF086172	17297	AUST INSTITUTE OF TRAFFIC PLANNING AND MEMBERSHIP / SEMINARS	30/06/2015	662.85
EF086173	17301	GOOD READING MAGAZINE SUBSCRIPTION	30/06/2015	1,771.00
EF086174	17362	JOHN EARLEY TRAINING	30/06/2015	250.00
EF086175	17471	PIRTEK (FREMANTLE) PTY LTD HOSES & FITTINGS	30/06/2015	2,030.12
EF086176	17587	WEST COAST SHADE SHADE STRUCTURES	30/06/2015	12,386.00
EF086177	17798	WESTERN DIAGNOSTIC PATHOLOGY ANALYTICAL SERVICES	30/06/2015	813.90
EF086178	17887	RED SAND SUPPLIES PTY LTD MACHINERY HIRE	30/06/2015	2,079.00
EF086179	17942	MRS MAC'S FOOD SUPPLIES	30/06/2015	374.95
EF086180	18114	BOLLIG DESIGN GROUP P/L ARCHITECTURAL SERVICES	30/06/2015	14,767.50
EF086181	18122	SIGNMAN SIGNAGE	30/06/2015	1,181.40
EF086182	18147	AURECON AUSTRALASIA PTY LTD CONSULTANCY - CIVIL ENGINEERING	30/06/2015	11,718.85

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EF086183	18203	NATSYNC ENVIRONMENTAL PEST CONTROL	30/06/2015	555.95
EF086184	18249	LISSO MEDIA ADVERTISING	30/06/2015	440.00
EF086185	18265	FREMANTLE CITY DOCKERS FOOTBALL CLUB	30/06/2015	1,400.00
EF086186	18272	AUSTRACLEAR LIMITED INVESTMENT SERVICES	30/06/2015	37.91
EF086187	18508	JOHN TURNER BRICK LAYING SERVICES	30/06/2015	14,023.00
EF086188	18533	FRIENDS OF THE COMMUNITY INC. DONATION	30/06/2015	1,206.00
EF086189	18579	DEPARTMENT OF INDIGENOUS AFFAIRS STREET BANNER SPONSORSHIP	30/06/2015	1,100.00
EF086190	18613	ECO-HIRE EQUIPMENT HIRE	30/06/2015	10,998.33
EF086191	18614	BOWMAN & ASSOCIATES PTY LTD CONSULTANCY SERVICES - PROJECT MGMT	30/06/2015	1,683.00
EF086192	18628	UNILEVER AUSTRALIA LTD BEVERAGES	30/06/2015	43.68
EF086193	18695	MYAREE CRANE HIRE CRANE HIRE	30/06/2015	231.00
EF086194	18731	OCCMEDIC MEDICAL SERVICES	30/06/2015	4,367.40
EF086195	18764	AFFIRMATIVE PAVING BRICK PAVING SERVICES	30/06/2015	5,494.50
EF086196	18801	FREMANTLE BIN HIRE BIN HIRE - SKIP BINS	30/06/2015	550.00
EF086197	18962	SEALANES (1985) P/L CATERING SUPPLIES	30/06/2015	1,475.22
EF086198	19058	VALENTINE'S CAMERA HOUSE FREMANTLE PHOTOGRAPHIC EQUIPMENT	30/06/2015	457.90
EF086199	19107	FOREVER SHINING MONUMENT	30/06/2015	21,997.80
EF086200	19436	WHITCHURCH REFRIGERATION & AIRCONDITIONING AIR CONDITIONING SERVICES	30/06/2015	2,008.99
EF086201	19505	ADVANCED WINDOW SHUTTERS WINDOW SHUTTERS	30/06/2015	2,730.00
EF086202	19533	WOOLWORTHS LTD GROCERIES	30/06/2015	1,700.17
EF086203	19541	TURF CARE WA PTY LTD TURF SERVICES	30/06/2015	5,874.00
EF086204	19546	THE BIG PICTURE FACTORY PRINTING SERVICES	30/06/2015	6,717.70
EF086205	19558	COMPLETE FIRE DESIGN FIRE CONSULTANCY SERVICES	30/06/2015	15,279.00

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Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF086206	19623	ERGOLINK OFFICE FURNITURE	30/06/2015	838.45
EF086207	19657	BIGMATE MONITORING SERVICES PTY LTD COMPUTER HARDWARE/SOFTWARE	30/06/2015	2,459.60
EF086208	19692	CATIA DOLZADELLI ARTWORK - SHOW OFF EXHIBITION	30/06/2015	450.00
EF086209	19718	SIFTING SANDS CLEANING SERVICES - SAND	30/06/2015	30,650.17
EF086210	19731	IPDAT COMMUNICATIONS CONSULTATION CONSULTANCY SERVICES - COMMUNICATIONS	30/06/2015	2,772.00
EF086211	19747	ALLERDING & ASSOCIATES CPLANNING CONSULTANCY SERVICES	30/06/2015	9,191.82
EF086212	19755	EMBROIDME MYAREE EMBROIDERY	30/06/2015	1,248.50
EF086213	19794	THE SOUTHERN LIONS RUGBY UNION CLUB GRANTS & DONATIONS	30/06/2015	3,400.00
EF086214	19795	FREMANTLE RUGBY LEAGUE CLUB INC REGISTRATION FEES	30/06/2015	5,580.00
EF086215	19847	PFD FOOD SERVICES PTY LTD CATERING SERVICES	30/06/2015	1,022.10
EF086216	19885	SAFEGUARD INDUSTRIES SECURITY SCREENS/DOORS	30/06/2015	100.00
EF086217	19938	ECHELON AUSTRALIA PTY LTD INSURANCE SERVICES	30/06/2015	4,950.00
EF086218	19967	FINGER FOOD CATERING CATERING SERVICES	30/06/2015	532.00
EF086219	20000	AUST WEST AUTO ELECTRICAL P/L AUTO ELECTRICAL SERVICES	30/06/2015	23,643.72
EF086220	20146	DATA#3 LIMITED CONTRACT IT PERSONNEL & SOFTWARE	30/06/2015	29,445.76
EF086221	20215	POWERVAC CLEANING EQUIPMENT	30/06/2015	955.90
EF086222	20238	MY DELICIOUS CAKE DECORATING SERVICES	30/06/2015	340.00
EF086223	20321	RIVERJET P/L EDUCTING-CLEANING SERVICES	30/06/2015	1,650.00
EF086224	20322	PLANTRITE PLANT SUPPLIES	30/06/2015	27,024.80
EF086225	20341	WILHELMINA MARIA HOUWEN GARDENING SERVICES	30/06/2015	1,785.00
EF086226	20549	A1 CARPET, TILE & GROUT CLEANING CLEANING SERVICES - TILES/CARPET	30/06/2015	2,596.00
EF086227	20693	RENTOKIL INITIAL PTY LTD PEST CONTROL SERVICES	30/06/2015	594.00
EF086228	20763	JECODA CONCRETE CONCRETE SUPPLY	30/06/2015	1,436.00

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EF086229	20786	THE BUTCHER SHOP ARTISTIC SUPPLIES	30/06/2015	278.40
EF086230	20833	BOOMERS PLUMBING & GAS PLUMBING SERVICES	30/06/2015	189.20
EF086231	20857	DOCKSIDE SIGNS SIGN MAKERS	30/06/2015	2,145.00
EF086232	20882	BELL-VISTA FRUIT & VEGETABLE FRUIT & VEGETABLE	30/06/2015	968.81
EF086233	20885	TACTILE INDICATORS (PERTH) PTY LTD TACTILES	30/06/2015	1,309.00
EF086234	20925	JANDAKOT LAKES JUNIOR CRICKET CLUB JUNIOR CRICKET	30/06/2015	400.00
EF086235	21005	BRAIN TEASERS OZ PTY LTD EDUCATIONAL PRODUCTS	30/06/2015	66.00
EF086236	21010	REDMAN SOLUTIONS PTY LTD COMPUTER SOFTWARE	30/06/2015	6,547.20
EF086237	21127	JOANNA AYCKBOURN INSTRUCTION - SINGING	30/06/2015	600.00
EF086238	21131	STATE WIDE TURF SERVICES TURF RENOVATION	30/06/2015	550.00
EF086239	21139	AUSTRAFFIC WA PTY LTD TRAFFIC SURVEYS	30/06/2015	6,468.00
EF086240	21151	DIGITAL MONOPOLY PERTH WEB DESIGN WEB DESIGN SERVICES	30/06/2015	561.00
EF086241	21177	DESERT SHADOW ARTISTIC SERVICES	30/06/2015	400.00
EF086242	21236	SANDCARDS ENTERTAINMENT SERVICES	30/06/2015	500.00
EF086243	21287	T.J.DEPIAZZI & SONS SOIL & MULCH SUPPLIES	30/06/2015	17,166.60
EF086244	21294	CAT HAVEN ANIMAL SERVICES	30/06/2015	2,409.00
EF086245	21363	TENDERLINK.COM PTY LTD COMPUTER SOFTWARE	30/06/2015	550.00
EF086246	21364	OFFICINO OFFICE FURNITURE OFFICE FURNITURE	30/06/2015	698.50
EF086247	21371	LD TOTAL SANPOINT PTY LTD LANDSCAPING WORKS/SERVICES	30/06/2015	85,930.61
EF086248	21397	THE PLAYROOM O'CONNOR TOYS AND GAMES	30/06/2015	200.00
EF086249	21527	TOUCHWOOD NURSERY PLANT SUPPLIES	30/06/2015	4,376.13
EF086250	21529	BRAND SUCCESS PROMOTIONAL PRODUCTS	30/06/2015	1,684.10
EF086251	21556	VITAL INTERPRETING PERSONNEL CONSULTANCY - EDUCATION	30/06/2015	448.25

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EF086252	21665	MMJ REAL ESTATE (WA) PTY LTD PROPERTY MANAGEMENT SERVICES	30/06/2015	25,105.28
EF086253	21672	MEGA MUSIC AUSTRALIA MUSICAL INSTRUMENTS/SOUND EQUIPMENT	30/06/2015	2,942.00
EF086254	21782	WEST COAST TIMBER FLOORING FLOORING SUPPLIES	30/06/2015	3,410.00
EF086255	21796	GREEN LEAF GARDENS LANDSCAPING SERVICES	30/06/2015	1,800.00
EF086256	21853	NEARMAP PTY LTD COMPUTER SOFTWARE - PHOTOMAPS	30/06/2015	8,250.00
EF086257	21879	SPOTLESS SERVICES AUSTRALIA LTD (CLEAN CLEANING SERVICES	30/06/2015	105,074.94
EF086258	21915	ECOWATER SERVICES PTY LTD MAINTENANCE SERVICES - WASTE SYSTEMS	30/06/2015	501.40
EF086259	21946	RYAN'S QUALITY MEATS MEAT SUPPLIES	30/06/2015	1,326.02
EF086260	21990	MEDIBANK HEALTH SOLUTIONS PTY LTD MEDICAL SERVICES	30/06/2015	3,088.46
EF086261	22012	ELEGANT GLOVES EVENTS AND SERVICES CATERING SERVICES	30/06/2015	8,001.00
EF086262	22109	PUBLIC LIBRARIES WESTERN AUSTRALIA INC PROFESSIONAL ORGANISATION	30/06/2015	2,730.00
EF086263	22129	DURASTEEL PTY LTD ROOF TRUSSES & WALL FRAMES	30/06/2015	1,547.00
EF086264	22133	AIR BORN AMUSEMENTS AMUSEMENT SERVICES	30/06/2015	8,951.51
EF086265	22195	CAFE CORPORATE COFFEE SUPPLIES/MACHINE SERVICES	30/06/2015	483.70
EF086266	22242	ASPHALT SURFACES PTY LTD ASPHALTING SERVICES	30/06/2015	60,267.65
EF086267	22307	CREATIVE SPACES GRAPHIC DESIGN	30/06/2015	5,227.09
EF086268	22332	MACQUARIE EQUIPMENT RENTALS PTY LTD LEASE RENTAL	30/06/2015	14,514.57
EF086269	22388	CARRINGTON'S TRAFFIC SERVICES TRAFFIC MANAGEMENT SERVICES	30/06/2015	13,910.33
EF086270	22448	CAKES WEST PTY LTD CATERING	30/06/2015	150.72
EF086271	22553	BROWNES FOOD OPERATIONS CATERING SUPPLIES	30/06/2015	1,516.93
EF086272	22568	SUCCESS NETBALL ASSOCIATION INCORPOR SPORTS SERVICES - NETBALL	30/06/2015	1,780.00
EF086273	22569	SONIC HEALTH PLUS PTY LTD MEDICAL SERVICES	30/06/2015	1,408.00
EF086274	22589	JB HI FI - COCKBURN ELECTRICAL EQUIPMENT	30/06/2015	808.35

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EF086275	22600	CUBIC PROMOTIONS PTY LTD PROMOTIONAL PRODUCTS	30/06/2015	2,915.00
EF086276	22613	VICKI ROYANS ARTISTIC SERVICES	30/06/2015	600.00
EF086277	22624	AUSSIE EARTHWORKS PTY LTD EARTHWORKS	30/06/2015	5,236.00
EF086278	22639	SHATISH CHAUHAN TRAINING SERVICES - YOGA	30/06/2015	800.00
EF086279	22653	PCYC FREMANTLE SPONSORSHIP	30/06/2015	817.64
EF086280	22682	BEAVER TREE SERVICES PTY LTD TREE PRUNING SERVICES	30/06/2015	35,136.13
EF086281	22720	DIAMONDS NETBALL CLUB WA INC. SPORTS - NETBALL	30/06/2015	600.00
EF086282	22751	WORKFORCE CLOTHING PTY LTD CLOTHING - INDUSTRIAL	30/06/2015	933.35
EF086283	22805	COVS PARTS PTY LTD MOTOR PARTS	30/06/2015	2,662.28
EF086284	22806	AUSTRALIAN FUEL DISTRIBUTORS PTY LTD FUEL SUPPLIES	30/06/2015	110,233.72
EF086285	22903	UNIQUE INTERNATIONAL RECOVERIES LLC DEBT COLLECTORS	30/06/2015	320.00
EF086286	23213	SPOTLESS FACILITY SERVICES PTY LTD (LAU LAUNDRY SERVICES	30/06/2015	240.64
EF086287	23288	ARIANE ROEMMELE AMUSEMENT - CHILDREN'S ACTIVITIES	30/06/2015	145.00
EF086288	23302	BUILDING SERVIC BUILDING SERVICES LEVIES	30/06/2015	113,611.63
EF086289	23309	FUN IN TRAINING AUSTRALIA PTY LTD FITNESS CLASSES-INSTRUCTIONS	30/06/2015	1,045.00
EF086290	23457	TOTALLY WORK WEAR FREMANTLE CLOTHING - UNIFORMS	30/06/2015	666.22
EF086291	23570	A PROUD LANDMARK PTY LTD LANDSCAPE CONTRUCTION SERVICES	30/06/2015	211,740.04
EF086292	23600	IRONBARK SUSTAINABILITY CONSULTANCY SERVICES - ENVIRONMENTAL	30/06/2015	2,950.00
EF086293	23696	JULIE REIDY DRAFTING SERVICES	30/06/2015	2,750.00
EF086294	23765	BOOKERY EDUCATION BOOK SUPPLIES	30/06/2015	1,140.00
EF086295	23815	SOUTH FREMANTLE WOMENS FOOTBALL CLU REGISTRATION FEES - KIDSPORT	30/06/2015	200.00
EF086296	23818	AM & IE MUTCH ENGINEERING CONSULTANTS CONSULTANCY SERVICES	30/06/2015	45,512.50
EF086297	23822	URIMAT AUSTRALIA PLUMBING SUPPLIES	30/06/2015	344.30

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EF086298	23825	PALMYRA REBELS NETBALL CLUB REGISTRATION FEES	30/06/2015	400.00
EF086299	23849	JCB CONSTRUCTION EQUIPMENT AUSTRALIA PLANT/MACHINERY	30/06/2015	1,193.52
EF086300	23858	SPECIALISED SECURITY SHREDDING DOCUMENT DESTRUCTION SERVICES	30/06/2015	10.84
EF086301	23866	TENNANT AUSTRALIA PTY LTD PLANT & MACHINERY	30/06/2015	388.32
EF086302	23929	ANTIQUITEA HIRE-FUNCTION EQUIPMENT	30/06/2015	5,869.00
EF086303	23971	FIND WISE LOCATION SERVICES LOCATING SERVICES - UNDERGROUND	30/06/2015	1,274.35
EF086304	23985	WINNACOTT KATS JUNIOR FOOTBALL CLUB I REGISTRATION FEES-KIDSPORT	30/06/2015	483.00
EF086305	24156	MASTEC AUSTRALIA PTY LTD PURCHASE OF NEW BINS	30/06/2015	930.60
EF086306	24160	WILDTHINGS ANIMAL CONTROL SOLUTIONS FERAL ANIMAL CONTROL SERVICES	30/06/2015	2,475.00
EF086307	24183	WELLARD GLASS GLASS REPAIR SERVICES	30/06/2015	2,097.46
EF086308	24185	HIPPY BELLY DANCE TRAINING SERVICES - DANCE CLASSES	30/06/2015	260.00
EF086309	24186	ELAN ENERGY MANAGEMENT PTY LTD RECYCLING SERVICES - TYRES	30/06/2015	355.04
EF086310	24187	TOTAL GREEN RECYCLING E-WASTE RECYCLING SERVICES	30/06/2015	2,582.25
EF086311	24189	COMMUNITY TRANSFORMATIONS INC TRAINING SERVICES	30/06/2015	1,250.00
EF086312	24195	PAYNE'S WINDOW CLEANING AND SERVICES WINDOW CLEANING SERVICES	30/06/2015	14,685.30
EF086313	24281	ECO LOGICAL AUSTRALIA PTY LTD MAPPING SERVICES	30/06/2015	11,446.94
EF086314	24506	AMARANTI'S PERSONAL TRAINING PERSONAL TRAINING SERVICES	30/06/2015	300.00
EF086315	24524	CALO HEALTH HEARTMOVE CLASSES	30/06/2015	3,250.00
EF086316	24527	AUSTRALIAN ASSOCIATION FOR ENVIRONME COURSE REGISTRATION	30/06/2015	1,100.00
EF086317	24595	CONTEMPORARY IMAGE PHOTOGRAPHY PTY PHOTOGRAPHY SERVICES	30/06/2015	2,380.00
EF086318	24599	POOLWERX SPEARWOOD ANALYTICAL SERVICES	30/06/2015	4,603.81
EF086319	24643	BIBLIOTHECA RFID LIBRARY SYSTEMS AUST PURCHASE OF LIBRARY TAGS	30/06/2015	2,556.32
EF086320	24655	AUTOMASTERS SPEARWOOD VEHICLE SERVICING	30/06/2015	6,595.00

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EF086321	24724	QUALITY MARINE COATING SYSTEMS P/L CLEANING SERVICES - ROAD SURFACES	30/06/2015	2,860.00
EF086322	24736	ZENIEN CCTV CAMERA LICENCES	30/06/2015	4,531.12
EF086323	24748	PEARMANS ELECTRICAL & MECHANICAL SER ELECTRICAL SERVICES	30/06/2015	103,257.28
EF086324	24805	KAREN WOOLHEAD DANCING CLASSES	30/06/2015	960.00
EF086325	24861	MELVILLE CITY FOOTBALL CLUB SPORTS FEES	30/06/2015	440.00
EF086326	24885	PG CREATIVE CONNECTIONS ENTERTAINMENT SERVICES	30/06/2015	120.00
EF086327	24886	A NATURAL SELF ENTERTAINMENT SUPPLIES	30/06/2015	384.00
EF086328	24945	NS PROJECTS PTY LTD PROJECT MANAGEMENT SERVICES	30/06/2015	22,000.00
EF086329	24949	BITUMEN SURFACING BITUMEN SUPPLIES	30/06/2015	12,008.70
EF086330	24974	SCOTT PRINT PRINTING SERVICES	30/06/2015	17,699.00
EF086331	24976	SNAP PRINTING - COCKBURN CENTRAL PRINTING SERVICES	30/06/2015	544.80
EF086332	25060	DFP RECRUTIMENT SERVICES EMPLOYMENT SERVICES	30/06/2015	24,688.01
EF086333	25092	LINKS MODULAR SOLUTIONS PTY LTD COMPUTER SOFTWARE	30/06/2015	10,670.00
EF086334	25102	FREMANTLE MOBILE WELDING WELDING SERVICES	30/06/2015	5,538.50
EF086335	25115	FIIG INVESTMENT MANAGEMENT SERVICES	30/06/2015	2,750.00
EF086336	25121	IMAGESOURCE DIGITAL SOLUTIONS BILLBOARDS	30/06/2015	4,485.80
EF086337	25128	HORIZON WEST LANDSCAPE & IRRIGATION P LANDSCAPING SERVICES	30/06/2015	10,704.65
EF086338	25158	MPIRE SECURITY SECURITY SERVICES	30/06/2015	6,935.73
EF086339	25255	FREMANTLE CBC A REGISTRATION FEES	30/06/2015	200.00
EF086340	25262	SANDOVER PINDER ARCHITECTS ARCHITECTURAL SERVICES	30/06/2015	111,133.00
EF086341	25291	STARWEST PARTY HIRE PARTY HIRE	30/06/2015	320.50
EF086342	25325	NATSALES ADVERTISING PTY LTD PRINTING SERVICES	30/06/2015	1,930.50
EF086343	25333	INTELLIGENZ SOLUTIONS COMPUTER SOFTWARE	30/06/2015	10,670.00

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EF086344	25335	JAXON PTY LTD BUILDING CONSTRUCTION SERVICES	30/06/2015	3,036.00
EF086345	25373	ARCADIAN BEAUTY MASSAGE THERAPY	30/06/2015	120.00
EF086346	25383	KARDINYA JUNIOR CRICKET CLUB REGISTRATION FEES - KIDSPORT	30/06/2015	99.00
EF086347	25406	GRIFFITH GREEN ELECTRICS ELECTRICAL SERVICES	30/06/2015	15,653.00
EF086348	25418	CS LEGAL LEGAL SERVICES	30/06/2015	3,314.19
EF086349	25539	CALIBRE CONSULTING (MELB) PTY LTD CONSULTANCY SERVICES	30/06/2015	55,000.00
EF086350	25588	CIVCO MINING SERVICES PTY LTD PLANT / EQUIPMENT HIRE	30/06/2015	8,088.30
EF086351	25609	NEXXIAL ECOLOGY PTY LTD WEED CONTROL SERVICES	30/06/2015	1,760.00
EF086352	25644	DYMOCKS GARDEN CITY PURCHASE OF BOOKS	30/06/2015	656.18
EF086353	25645	YELAKITJ MOORT NYUNGAR ASSOCIATION INC WELCOME TO THE COUNTRY PERFORMANCES	30/06/2015	1,100.00
EF086354	25646	THE GREEN MO CATERING SERVICES	30/06/2015	800.00
EF086355	25648	NANKIVELL, MATTHEW LEE MN CREATIVE PHOTOGRAPHY SERVICES	30/06/2015	798.00
EF086356	25654	WINDOW SHIELD AUSTRALIA CLEAR STEEL SOLUTIONS GLASS CLEANING SERVICES	30/06/2015	900.00
EF086357	25657	LOCK JOINT AUSTRALIA LOCKSMITH SERVICES	30/06/2015	935.00
EF086358	25658	GUNDI CORPORATION PTY LTD ABORIGINAL REFERENCE GROUPS	30/06/2015	7,150.00
EF086359	25708	AMGROW PTY LTD HERBICIDES	30/06/2015	82.50
EF086360	25713	DISCUS ON DEMAND PRINTING SERVICES	30/06/2015	3,595.68
EF086361	25733	MIRACLE RECREATION EQUIPMENT PLAYGROUND INSTALLATION / REPAIRS	30/06/2015	26,031.50
EF086362	25736	BLUE TANG (WA) PTY LTD EMERGE ASSOCIATES CONSULTANCY SERVICES	30/06/2015	25,421.00
EF086363	25822	FIT2WORK.COM.AU MERCURY SEARCH AND SCREENING EMPLOYEE CHECK	30/06/2015	38.39
EF086364	25832	EXTERIA ENGINEERING & DESIGN SERVICES	30/06/2015	4,328.50
EF086365	25875	COOGEE PLUMBING SERVICES PLUMBING SERVICES	30/06/2015	21,354.14
EF086366	25955	ADECCO INDUSTRIAL PTY LTD EMPLOYMENT SERVICES	30/06/2015	410,423.50

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EF086367	25962	ALL LINES LINEMARKING SERVICES	30/06/2015	440.00
EF086368	26020	GRANT ELEVATORS LIFT MAINTENANCE	30/06/2015	858.00
EF086369	26028	APPLABS TECHNOLOGIES LTD DESIGN & DEVELOPMENT OF APPS	30/06/2015	9,900.00
EF086370	26029	AUTOSWEEP WA SWEEPING SERVICES	30/06/2015	6,094.00
EF086371	26048	DAVID WILLS AND ASSOCIATES CONSULTANCY SERVICES - ENGINEERING	30/06/2015	2,750.00
EF086372	26067	SPRAYKING WA PTY LTD CHEMICAL WEED CONTROL SERVICES	30/06/2015	7,725.03
EF086373	26090	FREMANTLE MILK DISTRIBUTORS MILK DISTRIBUTORS	30/06/2015	216.60
EF086374	26110	DASH CIVIL CONTRACTING CONCRETING SERVICES	30/06/2015	160,839.52
EF086375	26112	BROOKE BOBRIDGE GRAPHIC DESIGN SERVICES	30/06/2015	600.00
EF086376	26114	GRACE RECORDS MANAGEMENT RECORDS MANAGEMENT SERVICES	30/06/2015	332.06
EF086377	26120	ECOBURBIA ENVIRONMENTAL WASTE WORKSHOPS	30/06/2015	600.00
EF086378	26146	TOP GEAR PAINTING PAINTING SERVICES	30/06/2015	8,580.00
EF086379	26162	RANDSTAD PTY LTD EMPLOYMENT SERVICES	30/06/2015	1,506.78
EF086380	26173	SOUTHSIDE PLUMBING PLUMBING SERVICES	30/06/2015	5,458.20
EF086381	26189	J. P. BENNETT PTY LTD CONSULTANCY SERVICES	30/06/2015	4,400.00
EF086382	26195	PLAY CHECK CONSULTING SERVICES	30/06/2015	357.50
EF086383	26211	AMCOM PTY LTD INTERNET/DATA SERVICES	30/06/2015	6,070.00
EF086384	26253	CREATE IT TIME LAPSE CAMERA	30/06/2015	1,826.00
EF086385	26257	PAPERBARK TECHNOLOGIES ARBORICULTURAL CONSULTANCY SERVICES	30/06/2015	2,400.00
EF086386	26261	LEARNING SEAT TRAINING SERVICES	30/06/2015	220.00
EF086387	26275	SHEFA CORPORATION PAINTING SERVICES	30/06/2015	6,875.00
EF086388	26299	IMAGE LAB DIGITAL PRODUCTION EXHIBITION PRINTING SERVICES	30/06/2015	5,852.00
EF086389	26303	GECKO CONTRACTING TURF & LANDSCAPE M TURF & LANDSCAPE MAINTENANCE	30/06/2015	74,595.40

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EF086390	26314	CPE GROUP TEMPORARY EMPLOYMENT SERVICES	30/06/2015	119.92
EF086391	26323	AT THE KITCHEN CATERING SERVICES	30/06/2015	2,766.00
EF086392	26330	KENNARDS HIRE - BIBRA LAKE EQUIPMENT HIRE	30/06/2015	1,033.80
EF086393	26354	ELECTROFEN REPAIR SERVICES - SECURITY FENCES	30/06/2015	396.00
EF086394	26359	WILSON SECURITY SECURITY SERVICES	30/06/2015	159,505.60
EF086395	26370	ESTRAT CONSULTANCY SERVICES - IT	30/06/2015	11,220.00
EF086396	26391	ANDOVER DETAILERS GOLDFINCH NOMINEES DETAILING SERVICES	30/06/2015	1,554.20
EF086397	26413	SEALEY, MARITA WORKSHOP FACILITATION SERVICES	30/06/2015	6,765.00
EF086398	26418	INTEGRANET TECHNOLOGY GROUP PTY LTD ICT CONSULTANCY SERVICES	30/06/2015	24,761.00
EF086399	26419	CORPORATE SCORECARD PTY LTD CREDIT REFERENCE CHECKS	30/06/2015	790.02
EF086400	26423	ALPHA PEST ANIMAL SOLUTIONS INVASIVE S PEST CONTROL SERVICES	30/06/2015	12,210.00
EF086401	26439	ALTERNATIVE TECHNOLOGY ASSOCIATION IN SUSTAINABILITY EDUCATION SERVICES	30/06/2015	6,050.00
EF086402	26442	BULLANT SECURITY PTY LTD KEY WEST LOC LOCKSMITH & SECURITY SERVICES	30/06/2015	3,825.09
EF086403	26460	KISS PHOTOBOOTH PHOTOBOOTH HIRE	30/06/2015	499.00
EF086404	26461	777 MAINTENANCE PTY LTD MAINTENANCE SERVICES	30/06/2015	4,499.90
EF086405	26463	BOLLYGOOD FOODS PTY LTD CATERING SERVICES - FOOD VENDOR	30/06/2015	300.00
EF086406	26467	WREFORD, KATHY HIRE OF MASSAGE CHAIR	30/06/2015	120.00
EF086407	26469	MURFETT LEGAL PTY LTD LEGAL SERVICES	30/06/2015	4,543.44
EF086408	26470	SCP CONSERVATION AND LAND MANAGEMEN FENCING SERVICES	30/06/2015	15,220.00
EF086409	26474	PENNY LANES MUSIC WORKSHOP ENTERTAINMENT - BAND	30/06/2015	200.00
EF086410	26475	QUANTUM BUILDING SERVICES CARPENTRY SERVICES	30/06/2015	13,628.18
EF086411	26477	UTILITY MANAGEMENT SOLUTIONS PTY LTD PLUMBING SERVICES	30/06/2015	3,021.59
EF086412	26480	MATTRESS REMOVAL WA MATRESS REMOVAL SERVICES	30/06/2015	3,840.00

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EF086413	26481	MEN IN HARMONY BARBERSHOP CHORUS INC ENTERTAINMENT SERVICES	30/06/2015	250.00
EF086414	26486	BIBRA LAKE FABRICATORS PTY LTD FABRICATION SERVICES	30/06/2015	2,508.00
EF086415	26500	MINERAL HAULAGE & EARTHMOVING EARTHMOVING SERVICES	30/06/2015	3,752.80
EF086416	26503	UNCLE CHOPS SMOKEHOUSE CATERING SERVICES	30/06/2015	235.00
EF086417	26506	DVG MADDINGTON HYUNDAI PURCHASE OF NEW VEHICLE	30/06/2015	54,953.20
EF086418	26514	US 2 U PLUMBING PLUMBING SERVICES	30/06/2015	800.00
EF086419	26516	ULTIMATE LIMESTONE CONSTRUCTION SERVICES	30/06/2015	27,500.00
EF086420	26519	CAREY BAPTIST COLLEGE INC KIDSPORT REGISTRATIONS	30/06/2015	390.00
EF086421	26525	ENVIRO CONTRACTING PTY LTD BUILDING MAINTENANCE SERVICES	30/06/2015	5,294.74
EF086422	26530	MEPHALENE RUST CONTROL P/L SANDBLASTING SERVICES	30/06/2015	8,712.00
EF086423	26532	HCI PROFESSIONAL SERVICES PTY LTD EMPLOYMENT SERVICES	30/06/2015	2,640.00
EF086424	26536	SKYLINE LANDSCAPE SERVICES (WA) LANDSCAPING SERVICES	30/06/2015	27,316.71
EF086425	26537	CONOR MCGRATH DESIGN GRAPHIC DESIGN SERVICES	30/06/2015	1,800.00
EF086426	26539	VITAL BUILDING COMPANY PTY LTD CONSTRUCTION SERVICES	30/06/2015	132,154.00
EF086427	26540	ROYER, SANDRA ANN (SANDRA ROYER) WORKSHOP	30/06/2015	300.00
EF086428	26541	CORNUEL, JAYNE J (JAYNE JUDITH CORNUEL) WORKSHOP	30/06/2015	300.00
EF086429	26543	PUBLIC OUTDOOR PTY LTD OUTDOOR PING PONG TABLE	30/06/2015	4,592.50
EF086430	26544	DIVERSIFIED FIRE SERVICES BUSHFIRE SPRINKLER SYSTEMS	30/06/2015	20,350.00
EF086431	26545	CANVALE PTY LTD (CORPORATE LIVING) OFFICE CHAIRS	30/06/2015	450.00
EF086432	26547	P. J. WRIGHT & ASSOCIATES PTY LTD CONSULTANCY SERVICES	30/06/2015	1,474.00
EF086433	26549	SHARON GREGORY (KOORT-KADAK CONSULT) CONSULTANCY SERVICES	30/06/2015	3,000.00
EF086434	26554	BRONZBEADZ PURCHASE OF ARTWORK	30/06/2015	1,095.00
EF086435	26561	MOSS AND FERN FLORIST	30/06/2015	100.00

CITY OF COCKBURN
MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF086436	26563	INTO THE MASK THEATRE DRAMA CLASSES	30/06/2015	150.00
EF086437	26566	THE HANGING MAN PICTURE HANGING SERVICES	30/06/2015	4,895.89
EF086438	26568	UNITED DIAMOND TOOLS DIAMOND CUTTING TOOLS	30/06/2015	2,060.00
EF086439	26571	MARK IT PRINTING SERVICES/PROMOTIOANL	30/06/2015	2,750.00
EF086440	26572	ECO BUY LTD SUSTAINABILITY ADVICE	30/06/2015	1,144.00
EF086441	26574	EVA BELLYDANCE ENTERTAINMENT - BELLY DANCING	30/06/2015	260.00
EF086442	26577	S-TECH HOLDINGS PTY LTD SIGN MAKING SERVICES	30/06/2015	17,022.50
EF086443	26582	ROAD SPECIALIST AUSTRALIA HYDRAULIC REPAIRS	30/06/2015	1,465.55
EF086444	10363	COCKBURN SENIOR CITIZENS ASSOCIATION GRANTS/DONATIONS	30/06/2015	180.00
EF086445	11841	YANGEBUP FAMILY CENTRE INC VENUE HIRE / GRANTS & DONATIONS	30/06/2015	576.00
EF086446	99997	MICHAEL PETER GASPAR CROSSOVER CONTRIBUTION	30/06/2015	300.00
EF086447	99997	LINDA M GOUVEIA CROSSOVER CONTRIBUTION	30/06/2015	300.00
EF086448	99997	SHAWN D'SOUZA CROSSOVER CONTRIBUTION	30/06/2015	300.00
EF086449	99997	FENG ZHAN SEN CROSSOVER CONTRIBUTION	30/06/2015	300.00
EF086450	99997	FRANK JAMES AMATO CROSSOVER CONTRIBUTION	30/06/2015	300.00
EF086451	99997	RAQUEL RODRIGUES CROSSOVER CONTRIBUTION	30/06/2015	300.00
EF086452	99997	LAWRENCE KP & SE CROSSOVER CONTRIBUTION	30/06/2015	300.00
EF086453	99997	MATTHEW GOUVEIA CROSSOVER CONTRIBUTION	30/06/2015	300.00
EF086454	99997	GA & M MUNTZ CROSSOVER CONTRIBUTION	30/06/2015	300.00
EF086455	99997	RENE SEEBERGER COMPOST BIN REBATE	30/06/2015	50.00
EF086456	99997	DEAN C & JILLIAN E BOWKER COMPOST BIN REBATE	30/06/2015	50.00
EF086457	99997	BRANKO KUZMANOUIC CROSSOVER CONTRIBUTION	30/06/2015	19.00
EF086458	99997	LIK XIONG SOU TOT (MARK) ANIMAL STERILISATION CONTRIBUTION	30/06/2015	42.50

CITY OF COCKBURN
MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF086459	99997	KA & KE JONES ANIMAL STERILISATION CONTRIBUTION	30/06/2015	42.50
EF086460	99997	KYLIE J MEAD ANIMAL STERILISATION CONTRIBUTION	30/06/2015	25.00
EF086461	99997	J WHEELER ANIMAL STERILISATION CONTRIBUTION	30/06/2015	107.50
EF086462	99997	DOUGLAS R & PAMELA D PATCHING BIRD BATH REBATE	30/06/2015	27.50
EF086463	99997	PAUL A & H L CLOUGH CHANGE OF ANIMAL STATUS REFUND	30/06/2015	30.00
EF086464	99997	BN & SM LAWRENCE CROSSOVER CONTRIBUTION	30/06/2015	300.00
EF086465	99997	TANYA M LINFORD CROSSOVER CONTRIBUTION	30/06/2015	300.00
EF086466	99997	YVONNE PATRICIA GAZZONE CROSSOVER CONTRIBUTION	30/06/2015	300.00
EF086467	99997	KWANG YONG CHUNG CROSSOVER CONTRIBUTION	30/06/2015	300.00
EF086468	99997	DAVID MENTESANA CROSSOVER CONTRIBUTION	30/06/2015	300.00
EF086469	99997	NITIN AGGARWAL CROSSOVER CONTRIBUTION	30/06/2015	300.00
EF086470	99997	RICHARD PARKER COMPOST BIN REBATE	30/06/2015	33.90
EF086471	99997	GLYNIS KING SLLC MEMBERSHIP REFUND	30/06/2015	260.00
EF086472	99997	PAMELA AMMOUN YOUTH ART SCHOLARSHIP	30/06/2015	500.00
EF086473	99997	SARAH MANGAN YOUTH ART SCHOLARSHIP	30/06/2015	500.00
EF086474	99997	SOPHIE ERAKOVIC YOUTH ART SCHOLARSHIP	30/06/2015	500.00
EF086475	99997	SHARON MASON YOUTH ART SCHOLARSHIP	30/06/2015	500.00
EF086476	99997	EMILY ROONER YOUTH ART SCHOLARSHIP	30/06/2015	450.00
EF086477	99997	HISTORICAL SOCIETY OF COCKBURN ANNUAL CONTRIBUTION	30/06/2015	14,000.00
EF086478	99997	JERVIOSE BAY SEA SCOUTS DONATION - PHOEBE HART	30/06/2015	200.00
EF086479	99997	JERVIOSE BAY SEA SCOUTS DONATION - NIKITA ROSE	30/06/2015	200.00
EF086480	99997	HAMMOND PARK PRIMARY SCHOOL DONATION	30/06/2015	200.00
EF086481	99997	CADEJ HORTON BIGGEST MORNING TEA DONATION	30/06/2015	200.00

CITY OF COCKBURN
MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF086482	99997	COCKBURN INTEGRATED HEALTH ALCOA GRANT	30/06/2015	11,000.00
EF086483	99997	COCKBURN CULTURAL COUNCIL FREMANTLE PORTS FUND GRANT	30/06/2015	3,000.00
EF086484	99997	JOHN CHEGWIDDEN JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086485	99997	THOMAS HARVEY JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086486	99997	AMMON KEOGH JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086487	99997	LACHLAN KAMES JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086488	99997	DYLAN BARWICK JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086489	99997	YING XIANG JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086490	99997	LOGAN PRATT JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086491	99997	GERIMAY NICHOLAS JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086492	99997	BELLA YUTONG LIU JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086493	99997	ISOBELLE MCCRACKAN JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086494	99997	CAITLIN WHINCUP JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086495	99997	ZACHARY NEWTON JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086496	99997	LIAM JOHNSON JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086497	99997	ABBEY COWAN JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086498	99997	SHANE TURNER JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086499	99997	JAKE DOUBIKIN JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086500	99997	ISABELLA WALLHEAD JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086501	99997	ALEX FRASER JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086502	99997	JACINTA AINSWORTH JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086503	99997	ELYSE AINSWORTH JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086504	99997	EZRA KAYE JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00

CITY OF COCKBURN
MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF086505	99997	CHLOE WATKINSON JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086506	99997	GENEVIEVE BRINK JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086507	99997	NATHAN GRIFFITHS JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086508	99997	CASSIUS BENN JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086509	99997	JESSE MIRCO JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086510	99997	CONNOR MCCRAE JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086511	99997	JACKSON CARTER JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086512	99997	BRAYDEN HODDER JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086513	99997	TAYA CHIAPPINI JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086514	99997	JOSHUA GRANT JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086515	99997	REGAN SPOONER JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086516	99997	MEGAN BELL JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086517	99997	LEWIS COLLIER JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086518	99997	CONNOR RADCLYFFE-SMITH JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086519	99997	LOGAN FRANICH JUNIOR TRAVEL ASSISTANCE	30/06/2015	400.00
EF086520	99997	FREMANTLE MULTICULTURAL CENTRE COMMUNITY GRANT	30/06/2015	6,600.00
EF086521	99997	MELVILLE COCKBURN CHAMBER OF COMMERCE SPONSORSHIP	30/06/2015	16,830.00
EF086522	11794	SYNERGY ELECTRICITY USAGE/SUPPLIES	30/06/2015	115,671.25
EF086523	12025	TELSTRA CORPORATION COMMUNICATIONS SERVICES	30/06/2015	13,021.44
EF086524	17555	ALLEASING PTY LTD LEASE REPAYMENTS	30/06/2015	97,659.06
EF086525	24558	MACQUARIE BANK LIMITED LEASE REPAYMENT	30/06/2015	1,615.13
EF086526	25823	ENGIN WESTERN AUSTRALIA ELECTRICITY USAGE/SUPPLIES	30/06/2015	11,277.66
EF086527	99996	STOCKLAND DEVELOPMENTS RATES REFUND	30/06/2015	441.70

CITY OF COCKBURN
MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF086528	99996	CADDY FOX PTY LTD RATES REFUND	30/06/2015	685.06
EF086529	99996	ANTHONY GROSE RATES REFUND	30/06/2015	266.09
EF086530	99996	MELINDA HIBBERT RATES REFUND	30/06/2015	1,000.00
EF086531	99996	SHANE JOCOB SETTLEMENTS RATES REFUND	30/06/2015	1,580.00
026324	13932	ARMAGUARD BANKING SERVICES	4/06/2015	1,520.75
026325	13932	ARMAGUARD BANKING SERVICES	4/06/2015	1,520.75
026326	13932	ARMAGUARD BANKING SERVICES	11/06/2015	3,319.00
026327	13932	ARMAGUARD BANKING SERVICES	18/06/2015	3,118.10
026328	10589	FINES ENFORCEMENT REGISTRY FINES ENFORCEMENT FEES	17/06/2015	4,194.00
026329	99999	SUNDRY CREDITOR PLANNING APPLICATION REFUND	24/06/2015	147.00
026330	99999	SUNDRY CREDITOR REFUND	24/06/2015	150.00
026331	99999	MELISSA OLSON PLANNING APPLICATION REFUND	24/06/2015	147.00
026332	99999	GOLDMASTER BOND REFUND - WENTWORTH PARADE	24/06/2015	153,856.12
026333	99999	STUART J COOPER PLANNING APPLICATION REFUND	24/06/2015	480.00
026334	99999	MILUC PTY LTD BOND REFUND - LOT 39 GAEBLER RD	24/06/2015	5,008.75
026335	99999	ALICE MATTAROCCHIA BOND REFUND - CAR PARKING BAYS	24/06/2015	2,326.50
026336	99999	GOLD ESTATES HOLDING PTY LTD BOND REFUND	24/06/2015	447,042.75
026337	99999	GOLD ESTATES HOLDING PTY LTD BOND REFUND	24/06/2015	16,080.93
026338	99999	PINDAN PTY LTD BOND REFUND	24/06/2015	12,500.00
026339	99999	PLUNKETT HOMES BOND REFUND	24/06/2015	3,000.00
026340	99999	URBAN CAPITAL GROUP PTY LTD BOND REFUND	24/06/2015	16,635.43
026341	99999	E & G DEVELOPMENTS BOND REFUND	24/06/2015	4,956.00
026342	99999	GOLDBARREL CORPORATION PTY LTD BOND REFUND	24/06/2015	68,403.90

CITY OF COCKBURN
MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
026343	13932	ARMAGUARD BANKING SERVICES	25/06/2015	2,648.40
026344	13932	ARMAGUARD BANKING SERVICES	26/06/2015	2,000.00
026345	10159	AUTHENTIC UPHOLSTERY REUPHOLSTERY SERVICES	30/06/2015	610.00
026346	10747	IINET LIMITED INTERNET SERVICES	30/06/2015	769.40
026347	11795	WESTERN POWER ELECTRICAL SERVICES	30/06/2015	1,713.65
026348	19268	WESTERN AUSTRALIAN PLANNING COMMISSION RATES REFUND	30/06/2015	2,420.00
026349	20679	OFFICE OF STATE REVENUE RATES REFUND	30/06/2015	253.79
026350	26456	HOUSING AUTHORITY RATES REFUND	30/06/2015	1,051.00
026351	10047	ALINTA ENERGY GAS SUPPLIES	30/06/2015	273.40
026352	11758	WATER CORP WATER USAGE SUPPLIES ADD RETENTION HELD NIL LESS PRIOR PERIOD CANCELLED CHEQUES/EFTS	30/06/2015	36,907.64
026324	13932	ARMAGUARD	4/06/2015	1,520.75-
EF085548	19157	LOMBARD THE PAPER PEOPLE	8/06/2015	83.41-
EF085794	99997	FLORENCE WARD	8/06/2015	150.00-
EF085596	21547	MUCKY DUCK BUSH BAND	8/06/2015	1,400.00-
EF085792	99997	SUZANNE VALENTE	8/06/2015	320.00-
EF085070	24973	BLUESTONE RECRUITMENT	9/06/2015	483.58-
EF085837	99997	SUZANNE VALENTE	19/06/2015	320.00-
026207	99999	KWIKDRAFT	24/06/2015	147.00-
026206	99999	TIEAN KHAN	24/06/2015	150.00-
EF086302	23929	ANTIQUITEA	23/07/2015	5,869.00-
CHEQUE LIST TOTAL				11,974,001.24
TOTAL AS PER AP SOURCE 15GLACT9991000				11,974,001.24
TOTAL AS PER TR SOURCE 15GLACT9991000				11,974,001.24
ADDITIONAL DIRECT PAYMENTS				
BANK FEES				
MERCHANT FEES COC				1,403.11
MERCHANT FEES SLLC				1,775.53
MERCHANT FEES VARIOUS OUT CENTRES				91.16

CITY OF COCKBURN
MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
		NATIONAL BPAY CHARGE		1,249.98
		RTGS/ACLR FEE		5.00
		NAB TRANSACT FEE		275.53
		MERCHANDISE / OTHER FEES		-
				4,800.31
		FAMILY DAY CARE AND IN HOME CARE PAYMENTS		
		FDC PAYMENTS		48,798.20
		IHC PAYMENTS		78,687.24
				127,485.44
		PAYROLL TRANSACTIONS		
		COC 02/06/15 CITY OF COCKBURN 042958		1,025,478.40
		COC 02/06/15 CITY OF COCKBURN 042958		47,284.45
		COC 04/06/15 CITY OF COCKBURN 042958		11,635.89
		COC 04/06/15 CITY OF COCKBURN 042958		312.30
		COC 05/06/15 CITY OF COCKBURN 042958		2,063.23
		COC 08/06/15 CITY OF COCKBURN 042958		262.96
		COC 08/06/15 CITY OF COCKBURN 042958		1,902.07
		COC 14/06/15 CITY OF COCKBURN 042958		4,484.14
		COC 16/06/15 CITY OF COCKBURN 042958		1,086,883.96
		COC 26/06/15 CITY OF COCKBURN 042958		2,023.81
		COC 28/05/15 CITY OF COCKBURN 042958		11,285.05
		COC 29/05/15 CITY OF COCKBURN 042958		1,668.70
		COC 30/06/15 CITY OF COCKBURN 042958		16,581.80
		COC 30/06/15 CITY OF COCKBURN 042958		1,051,682.88
				3,263,549.64
		CREDIT CARD PAYMENTS		
		CBA CREDIT CARD PAYMENT		76,661.11
				76,661.11
		TOTAL PAYMENTS FOR JUNE		15,446,497.74

PAYMENT SUMMARY

CHEQUE PAYMENTS

026324 -026352

CANCELLED PAYMENTS

Nil.

ELECTRONIC FUNDS TRANSFER PAYMENT

EF085817 – EF086531

STATEMENT OF FINANCIAL ACTIVITY

for the period ended 30 June 2015

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
Operating Revenue						
Governance	76,017,251	73,643,427	3%	2,373,824 ✓	73,643,427	74,022,182
Financial Services	867,496	789,050	10%	78,446	789,050	657,050
Information Services	360	1,500	-76%	(1,140)	1,500	1,500
Human Resource Management	433,109	286,000	51%	147,109	286,000	86,000
Library Services	41,669	53,346	-22%	(11,677)	53,346	53,346
Community Services	7,705,239	7,224,112	7%	481,127 ✓	7,224,112	7,338,204
Human Services	6,767,676	6,396,392	6%	371,284 ✓	6,396,392	6,459,383
Corporate Communications	18,769	12,500	50%	6,269	12,500	12,500
Statutory Planning	1,760,464	1,713,015	3%	47,449	1,713,015	1,613,015
Strategic Planning	2,481,672	2,291,943	8%	189,729	2,291,943	1,471,943
Building Services	1,631,516	1,645,700	-1%	(14,184)	1,645,700	1,535,700
Environmental Health	236,875	255,500	-7%	(18,625)	255,500	225,500
Waste Services	28,405,769	29,438,023	-4%	(1,032,254) ✗	29,438,023	29,143,124
Parks & Environmental Services	71,928	49,900	44%	22,028	49,900	1,900
Engineering Services	362,692	303,363	20%	59,329	303,363	293,363
Infrastructure Services	202,060	3,297	6029%	198,763	3,297	3,297
	127,004,545	124,107,068	2%	2,897,477	124,107,068	122,918,007
Total Operating Revenue	127,004,545	124,107,068	2%	2,897,477	124,107,068	122,918,007
Operating Expenditure						
Governance	(4,769,305)	(4,702,844)	1%	(66,460)	(4,702,844)	(4,633,859)
Financial Services	(5,247,705)	(5,420,104)	-3%	172,399	(5,420,104)	(5,464,284)
Information Services	(4,195,993)	(4,600,548)	-9%	404,555 ✓	(4,600,548)	(4,385,908)
Human Resource Management	(2,675,255)	(2,824,945)	-5%	149,691	(2,824,945)	(2,302,028)
Library Services	(3,050,511)	(3,201,689)	-5%	151,178	(3,201,689)	(3,168,305)
Community Services	(9,283,175)	(9,511,299)	-2%	228,123 ✓	(9,511,299)	(9,490,807)
Human Services	(8,219,975)	(7,899,026)	4%	(320,949) ✗	(7,899,026)	(7,729,708)
Corporate Communications	(2,592,635)	(2,791,953)	-7%	199,317	(2,791,953)	(2,682,290)
Statutory Planning	(1,237,586)	(1,286,082)	-4%	48,496	(1,286,082)	(1,286,082)
Strategic Planning	(1,979,448)	(1,683,131)	18%	(296,317) ✗	(1,683,131)	(1,566,420)
Building Services	(1,542,809)	(1,564,494)	-1%	21,685	(1,564,494)	(1,564,494)
Environmental Health	(1,650,167)	(1,887,046)	-13%	236,879 ✓	(1,887,046)	(1,773,929)
Waste Services	(18,554,061)	(20,096,655)	-8%	1,542,594 ✓	(20,096,655)	(20,076,655)
Parks & Environmental Services	(11,946,893)	(11,586,867)	3%	(360,026) ✗	(11,586,867)	(11,330,867)
Engineering Services	(8,034,878)	(7,997,243)	0%	(37,636)	(7,997,243)	(7,985,243)
Infrastructure Services	(8,717,540)	(8,281,370)	5%	(436,170) ✗	(8,281,370)	(8,195,699)
	(93,697,937)	(95,335,295)	-2%	1,637,358	(95,335,295)	(93,636,577)

OCM 13/8/2015 Item 15. 2

STATEMENT OF FINANCIAL ACTIVITY

for the period ended 30 June 2015

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget		Revised Budget	Adopted Budget
	\$	\$	%	\$		\$	\$
Less: Net Internal Recharging	2,907,325	3,246,783	-10%	(339,458) X		3,246,783	3,243,783
Add: Depreciation on Non-Current Assets							
Computer & Electronic Equip	(94,462)	(94,440)	0%	(22)		(94,440)	(94,440)
Furniture & Equipment	(152,833)	(156,413)	-2%	3,580		(156,413)	(156,413)
Plant & Machinery	(2,794,683)	(2,969,268)	-6%	174,585		(2,969,268)	(2,969,268)
Buildings	(4,535,245)	(4,261,656)	6%	(273,589) X		(4,261,656)	(4,261,656)
Roads	(10,137,584)	(10,247,952)	-1%	110,368		(10,247,952)	(9,467,952)
Drainage	(2,288,450)	(2,290,068)	0%	1,618		(2,290,068)	(2,290,068)
Footpaths	(1,089,112)	(1,161,348)	-6%	72,236		(1,161,348)	(1,161,348)
Parks Equipment	(2,737,486)	(2,764,012)	-1%	26,526		(2,764,012)	(3,354,012)
Landfill	(1,130,001)	(1,154,832)	-2%	24,831		(1,154,832)	-
	(24,959,856)	(25,099,989)	-1%	140,133		(25,099,989)	(23,755,157)
Total Operating Expenditure	(115,750,468)	(117,188,501)	-1%	1,438,033		(117,188,501)	(114,147,951)
Change in Net Assets Resulting from Operations	11,254,077	6,918,567	63%	4,335,510		6,918,567	8,770,056
Non-Operating Activities							
Profit/(Loss) on Assets Disposal							
Plant & Machinery	384,266	(217,704)	-277%	601,971 ✓		(217,704)	(582,947)
Freehold Land	2,050,673	17,521,068	-88%	(15,470,395) X		17,521,068	1,150,000
Furniture & Office Equipment	-	-	0%	-		-	-
Buildings	(30,944)	157,795	-120%	(188,739)		157,795	-
	2,403,995	17,461,159	-86%	(15,057,163)		17,461,159	567,053
Less: Underground Power Infrastructure Contribution	-	(574)	-100%	574		(574)	-
Asset Acquisitions							
Land and Buildings	(17,254,453)	(36,944,947)	-53%	19,690,494 ✓		(36,944,947)	(24,387,000)
Infrastructure Assets	(16,779,941)	(29,805,582)	-44%	13,025,641 ✓		(29,805,582)	(17,116,028)
Plant and Machinery	(3,426,802)	(5,518,133)	-38%	2,091,332 ✓		(5,518,133)	(4,007,500)
Furniture and Equipment	-	-	0%	-		-	(206,000)
Computer Equipment	(640,022)	(1,140,572)	-44%	500,550 ✓		(1,140,572)	(434,000)
Note 1.	(38,101,218)	(73,409,235)	-48%	35,308,017		(73,409,235)	(46,150,528)
Add: Transfer to Reserves	(46,454,176)	(59,117,259)	-21%	12,663,084 ✓		(59,117,259)	(35,534,109)
	(70,897,321)	(108,147,343)	-34%	37,250,021		(108,147,343)	(72,347,529)

STATEMENT OF FINANCIAL ACTIVITY

for the period ended 30 June 2015

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget		Revised Budget	Adopted Budget
	\$	\$	%	\$		\$	\$
Add Funding from							
Grants & Contributions - Asset Development	18,339,185	17,582,755	4%	756,430 ✓		17,582,755	6,726,309
Proceeds on Sale of Assets	4,002,046	19,419,231	-79%	(15,417,185) ✗		19,419,231	2,525,125
Reserves	28,815,113	31,744,806	-9%	(2,929,693) ✗		31,744,806	18,281,347
Loan Funds Raised	-	20,000,000	-100%	(20,000,000) ✗		20,000,000	20,000,000
Contributed Developer Assets	1,458,317	-	0%	1,458,317 ✓		-	-
	(18,282,660)	(19,400,551)	-6%	1,117,890		(19,400,551)	(24,814,748)
Non-Cash/Non-Current Item Adjustments							
Depreciation on Assets	24,959,856	25,099,989	-1%	(140,133)		25,099,989	23,755,157
Profit/(Loss) on Assets Disposal	(2,403,995)	(17,461,159)	-86%	15,057,163 ✓		(17,461,159)	(567,053)
Loan Repayments	(1,373,356)	(1,373,356)	0%	(0)		(1,373,356)	(1,373,356)
Non-Current Leave Provisions	246,615	-	0%	246,615 ✓		-	-
	3,146,459	(13,135,076)	-124%	16,281,535		(13,135,076)	(3,000,000)
Opening Funds	13,175,076	13,175,076	0%	(0)		13,175,076	3,000,000
Closing Funds	Note 2, 3.	16,321,500	40,000	40704%	16,281,500	40,000	-
	-	-		-		-	-

Notes to Statement of Financial Activity

Note 1.

Additional information on the capital works program including committed orders at end of month:

	Actuals	Commitments at Month End	Commitments & Actuals YTD	YTD Revised Budget	Full Year Revised Budget	Uncommitted at Month End
	\$	\$			\$	\$
Assets Classification						
Land and Buildings	(17,254,453)	(3,013,708)	(20,268,161)	(36,944,947)	(36,944,947)	16,676,786
Infrastructure Assets	(16,779,941)	(3,343,073)	(20,123,014)	(29,805,582)	(29,805,582)	9,682,568
Plant and Machinery	(3,426,802)	(1,150,180)	(4,576,981)	(5,518,133)	(5,518,133)	941,152
Furniture and Equipment	-	-	-	-	-	-
Computer Equipment	(640,022)	(154,015)	(794,037)	(1,140,572)	(1,140,572)	346,535
	(38,101,218)	(7,660,975)	(45,762,193)	(73,409,235)	(73,409,235)	27,647,042

Note 2.

Closing Funds in the Financial Activity Statement are represented by:

	Actuals	YTD Revised Budget	Full Year Revised Budget	Adopted Budget
	\$	\$	\$	\$
Current Assets				
Cash & Investments	129,048,616	113,947,104	113,947,104	86,462,923
Rates Outstanding	(200,516)	900,000	900,000	900,000
Rubbish Charges Outstanding	230,247	300,000	300,000	300,000
Sundry Debtors	2,314,818	2,700,000	2,700,000	2,700,000
GST Receivable	823,925	-	-	-
Prepayments	606,713	350,000	350,000	350,000
Accrued Debtors	323,074	-	-	-
Stock on Hand	18,634	20,000	20,000	20,000
	133,165,511	118,217,104	118,217,104	90,732,923
Current Liabilities				
Creditors	(7,041,986)	(5,000,496)	(5,000,496)	(5,000,496)
Income Received in Advance	-	52,856	52,856	52,856
GST Payable	(268,357)	-	-	-
Withholding Tax Payable	-	-	-	-
Provision for Annual Leave	(2,766,414)	(2,000,000)	(2,000,000)	(2,000,000)
Provision for Long Service Leave	(1,852,386)	(2,595,980)	(2,595,980)	(2,595,980)
	(11,929,143)	(9,543,620)	(9,543,620)	(9,543,620)
Net Current Assets	121,236,367	108,673,484	108,673,484	81,189,303
Add: Non Current Investments	4,506,731	4,339,420	4,339,420	4,339,420
	125,743,098	113,012,904	113,012,904	85,528,723
Less: Restricted/Committed Assets				
Cash Backed Reserves #	(103,217,443)	(112,972,904)	(112,972,904)	(85,528,723)
Deposits & Bonds Liability *	(6,204,155)	(2,789,342)	(2,789,342)	(2,789,342)
Grants & Contributions Unspent *	-	2,789,343	2,789,343	2,789,343
	16,321,500	40,000	40,000	-
Closing Funds (as per Financial Activity Statement)	16,321,500	40,000	40,000	-

See attached Reserve Fund Statement

* See attached Restricted Funds Analysis

Note 3.

Amendments to original budget since budget adoption. Surplus/(Deficit)

Ledger	Project/ Activity	Description	Council Resolution	Classification	Non Change (Non Cash Items) Adjust.	Increase in Available Cash	Decrease in Available Cash	Amended budget Running Balance
					\$	\$	\$	\$
	Budget Adoption			Closing Funds Surplus(Deficit)				0
GL	830	Increase conference budget	OCM 11/9/14 #5370	Operating Expenditure			2,000	(2,000)
GL	594	Salary reduction due to system error	OCM 11/9/14 #5370	Operating Expenditure		18,369		16,369
GL	105	Increase in FAGS grant	OCM 11/9/14 #5370	Operating Income		86,745		103,114
GL	161, 162, 175	Reallocating FESA grants and expenditure	OCM 11/9/14 #5370	Operating Income		4,498		107,612
GL	137	Allocating telecommunication expenses budget which was missed out during annual budget process	OCM 13/11/14 #5408	Operating Expenditure			65,000	42,612
OP	6245	Carried forward unspent fund in Coastal Monitoring project which was missed out during carry forward process	OCM 13/11/14 #5408	Operating Expenditure			20,687	21,925
GL	310, 350,	Adjustments to Financial Counselling budgets as 2 staff now relocated	OCM 11/12/14 #5429	Operating Expenditure			15,482	6,443
OP	7696	Rent income received from DFES for occupying CVES building		Operating Income		4,000		10,443
Various	Mid-year budget review		OCM 12/02/15 #5456	Various			10,443	0
OP	8291	Allocating internal administration charge	OCM 09/04/15 #5489	Operating Income		8,500		8,500
OP	8276	Coolbellup security guard	OCM May #5504	Operating Expenditure			8,500	0
CW	1359	Transfer Reserve funding back as project is no longer active	OCM June #5519	Operating Income			93,000	(93,000)
OP	8260	Cash in lieu from FY13/14 was not accounted correctly	OCM June #5519	Operating Income			90,000	(183,000)
OP	9710	Increase surplus	OCM 09/07/15	Operating Expenditure		223,000		40,000
				Closing Funds Surplus (Deficit)	0	345,112	305,112	40,000

Statement of Comprehensive Income *by Nature and Type*

for the period ended 30 June 2015

	Actual	Amended YTD Budget	\$ Variance to YTD Budget	Forecast	Amended Budget	Adopted Budget
	\$	\$	\$	\$	\$	\$
OPERATING REVENUE						
01 Rates	63,803,361	62,880,000	923,361	63,803,361	62,880,000	62,880,000
02 Specified Area Rates	312,317	270,000	42,317	312,317	270,000	270,000
05 Fees and Charges	40,295,613	40,950,088	(654,475)	40,295,613	40,950,088	39,708,911
06 Service Charges	4,031,642	4,000,000	31,642	4,031,642	4,000,000	4,000,000
10 Grants and Subsidies	11,726,382	9,417,149	2,309,233	11,726,382	9,417,149	9,325,765
15 Contributions, Donations and Reimbursements	1,133,874	683,340	450,534	1,133,874	683,340	356,840
20 Interest Earnings	5,675,020	5,899,391	(224,371)	5,675,020	5,899,391	6,369,391
25 Other revenue and Income	26,336	7,100	19,236	26,336	7,100	7,100
Total Operating Revenue	127,004,545	124,107,068	2,897,477	127,004,545	124,107,068	122,918,007
OPERATING EXPENDITURE						
50 Employee Costs - Salaries & Direct Oncosts	(43,809,957)	(43,858,909)	48,951	(43,809,957)	(43,858,909)	(42,697,487)
51 Employee Costs - Indirect Oncosts	(1,222,219)	(1,325,346)	103,127	(1,222,219)	(1,325,346)	(898,966)
55 Materials and Contracts	(35,392,409)	(35,645,169)	252,760	(35,392,409)	(35,645,169)	(35,548,621)
65 Utilities	(4,216,418)	(4,622,439)	406,021	(4,216,418)	(4,622,439)	(4,513,005)
70 Interest Expenses	(123,170)	(123,300)	130	(123,170)	(123,300)	(123,300)
75 Insurances	(1,952,591)	(2,217,500)	264,909	(1,952,591)	(2,217,500)	(2,340,500)
80 Other Expenses	(6,981,207)	(7,542,632)	561,425	(6,981,207)	(7,542,632)	(7,514,698)
85 Depreciation on Non Current Assets	(24,959,856)	(25,099,989)	140,133	(24,959,856)	(25,099,989)	(23,755,157)
Add Back: Indirect Costs Allocated to Capital Works	2,907,325	3,246,783	(339,458)	2,907,325	3,246,783	3,243,783
Total Operating Expenditure	(115,750,503)	(117,188,501)	1,437,998	(115,750,503)	(117,188,501)	(114,147,951)
CHANGE IN NET ASSETS RESULTING FROM OPERATING ACTIVITIES	11,254,042	6,918,567	4,335,475	11,254,042	6,918,567	8,770,056
NON-OPERATING ACTIVITIES						
11 Capital Grants & Subsidies	6,550,725	7,031,234	(480,509)	6,550,725	7,031,234	2,361,387
16 Contributions - Asset Development	11,788,460	10,551,521	1,236,939	11,788,460	10,551,521	4,364,922
95 Profit/(Loss) on Sale of Assets	2,403,995	17,461,159	(15,057,163)	2,403,995	17,461,159	567,053
57 Acquisition of Crown Land for Roads	-	-	-	-	-	-
58 Underground Power Scheme	-	(574)	574	-	(574)	-
Total Non-Operating Activities	20,743,181	35,043,340	(14,300,159)	20,743,181	35,043,340	7,293,362
NET RESULT	31,997,223	41,961,907	(9,964,684)	31,997,223	41,961,907	16,063,417

Notes to Statement of Comprehensive Income

Note 1.

Additional information on main sources of revenue in fees & charges.

	Actual	Amended YTD Budget	Amended Budget	Adopted Budget
	\$	\$	\$	\$
<u>Community Services:</u>				
Recreational Services	570,630	534,320	534,320	534,320
South Lake Leisure Centre	2,821,412	2,856,666	2,856,666	2,988,286
Law and Public Safety	815,916	388,496	388,496	388,496
	4,234,092	3,789,482	3,789,482	3,911,102
<u>Waste Services:</u>				
Waste Collection Services	19,041,380	19,050,000	19,050,000	18,695,101
Waste Disposal Services	9,356,598	10,378,023	10,378,023	10,448,023
	28,397,978	29,428,023	29,428,023	29,143,124
	32,632,069	33,217,505	33,217,505	33,054,226

Note 2.

Additional information on Salaries and Direct On-Costs by each Division.

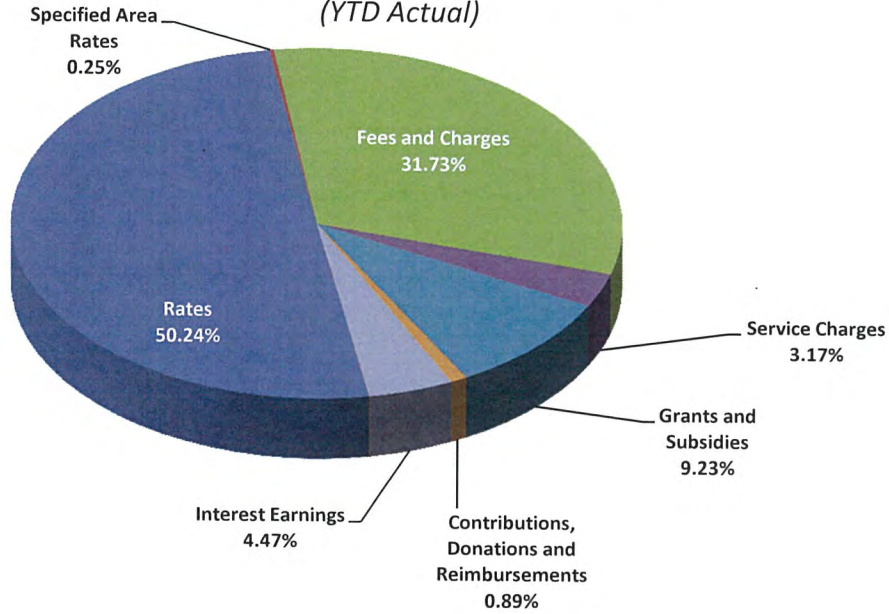
	Actual	Amended YTD Budget	Amended Budget	Adopted Budget
	\$	\$	\$	\$
Executive Division	(2,118,874)	(1,922,751)	(1,922,751)	(1,919,506)
Finance & Corporate Services Division	(7,012,024)	(7,087,510)	(7,087,510)	(6,145,420)
Community Services Division	(13,357,270)	(13,367,568)	(13,367,568)	(13,196,817)
Planning & Development Division	(5,003,456)	(5,036,253)	(5,036,253)	(5,024,097)
Engineering & Works Division	(16,318,334)	(16,444,827)	(16,444,827)	(16,411,647)
	(43,809,957)	(43,858,909)	(43,858,909)	(42,697,487)

Note 3

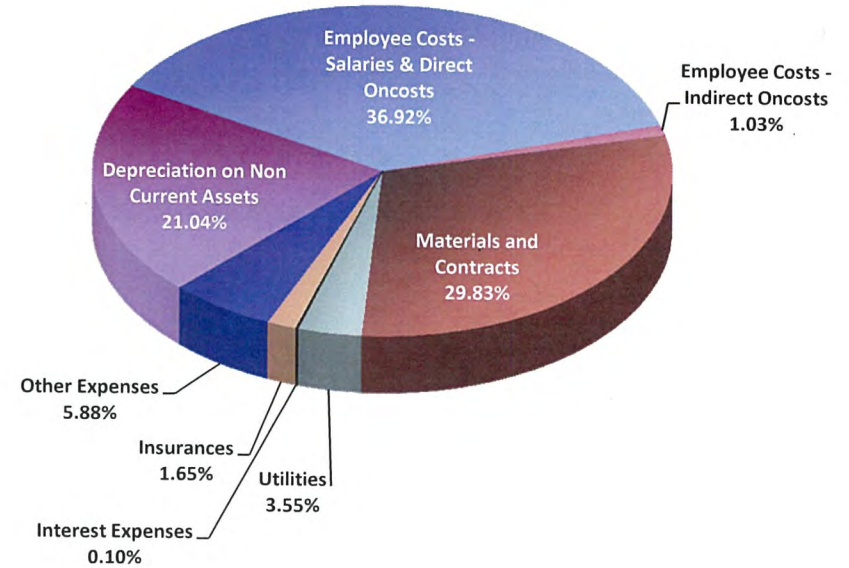
Additional information on Materials and Contracts by each Division.

	Actual	Amended YTD Budget	Amended Budget	Adopted Budget
	\$	\$	\$	\$
Executive Division	(1,799,023)	(2,058,855)	(2,058,855)	(2,087,788)
Finance & Corporate Services Division	(2,408,413)	(2,717,612)	(2,717,612)	(3,269,713)
Community Services Division	(7,745,719)	(7,770,553)	(7,770,553)	(7,649,134)
Planning & Development Division	(1,286,149)	(1,283,567)	(1,283,567)	(1,065,895)
Engineering & Works Division	(22,153,105)	(21,814,582)	(21,814,582)	(21,476,091)
Not Applicable	0	0	0	0
	(35,392,409)	(35,645,169)	(35,645,169)	(35,548,621)

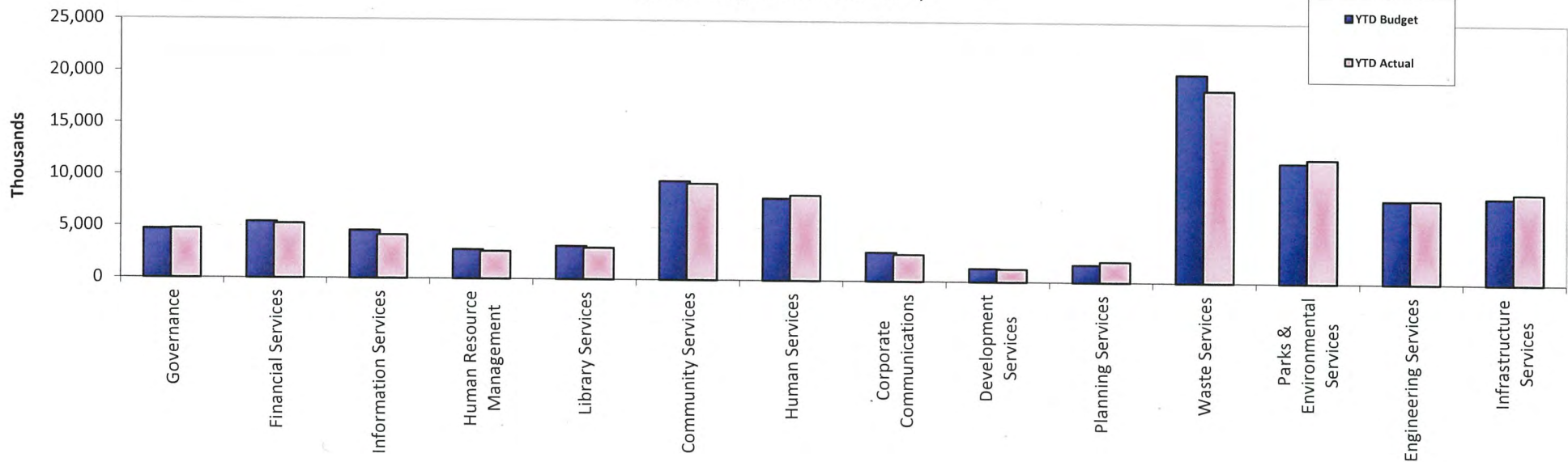
Operating Income by Nature and Type
(YTD Actual)



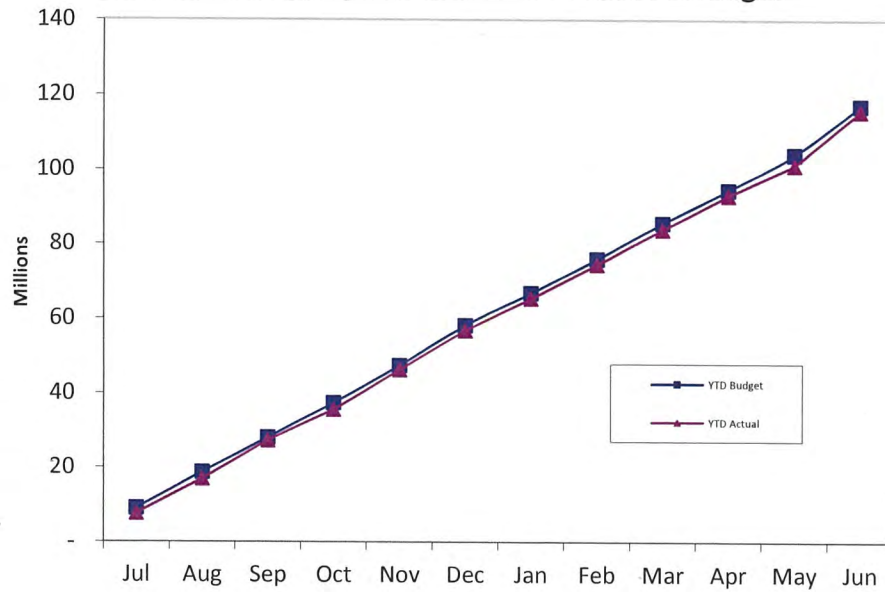
Operating Expenditure by Nature and Type
(YTD Actual)



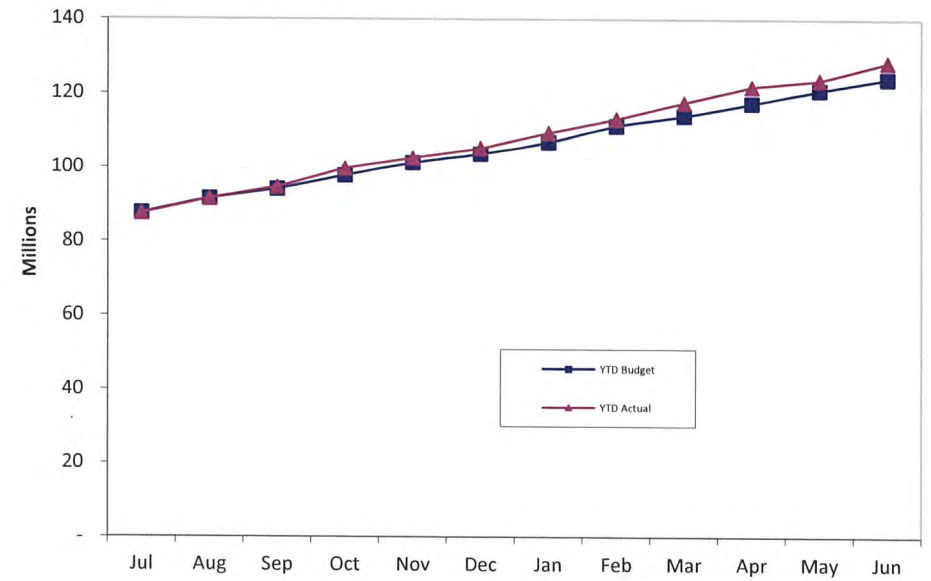
Operating Expenditure by Business Unit
(YTD Budget vs YTD Actual)



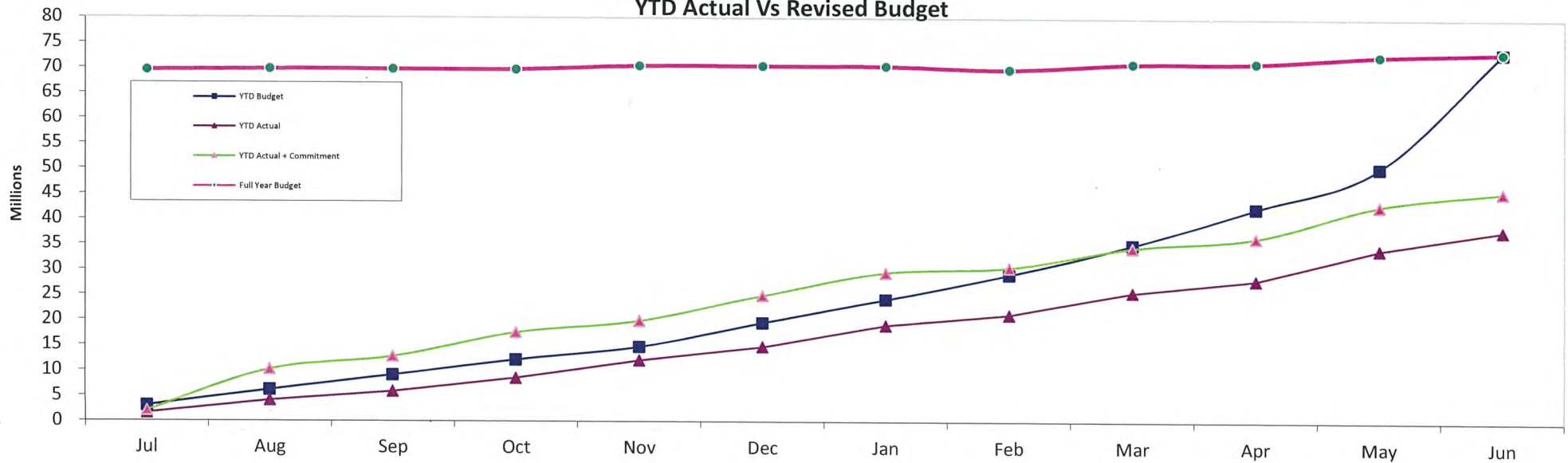
YTD Operating Expenditure Vs YTD Revised Budget



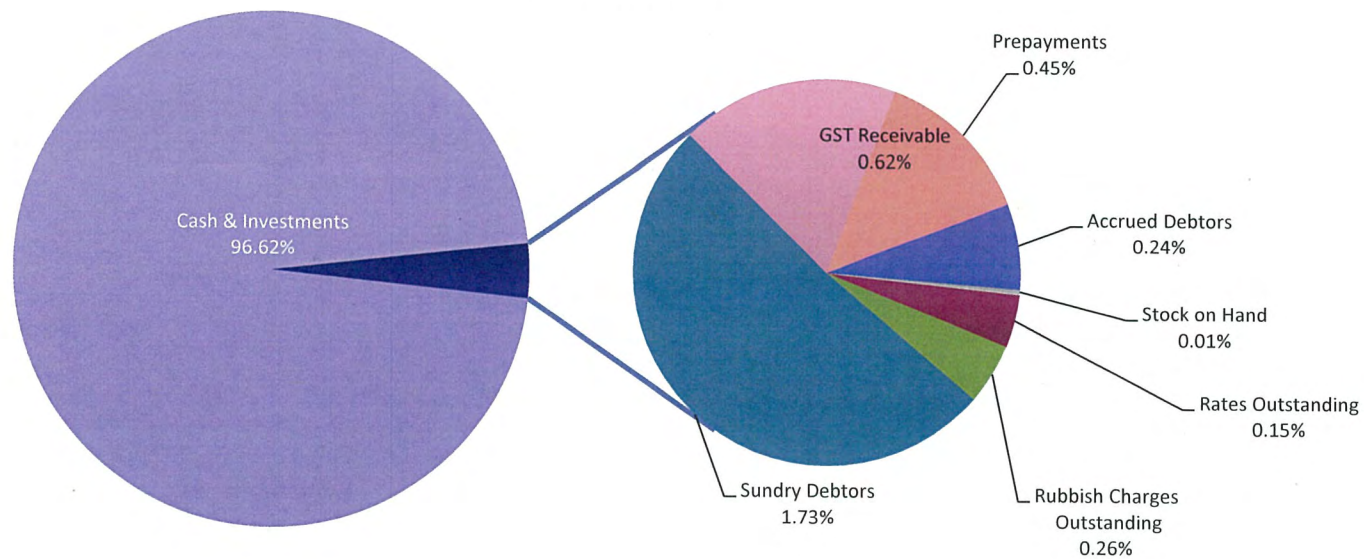
YTD Operating Income Vs YTD Revised Budget



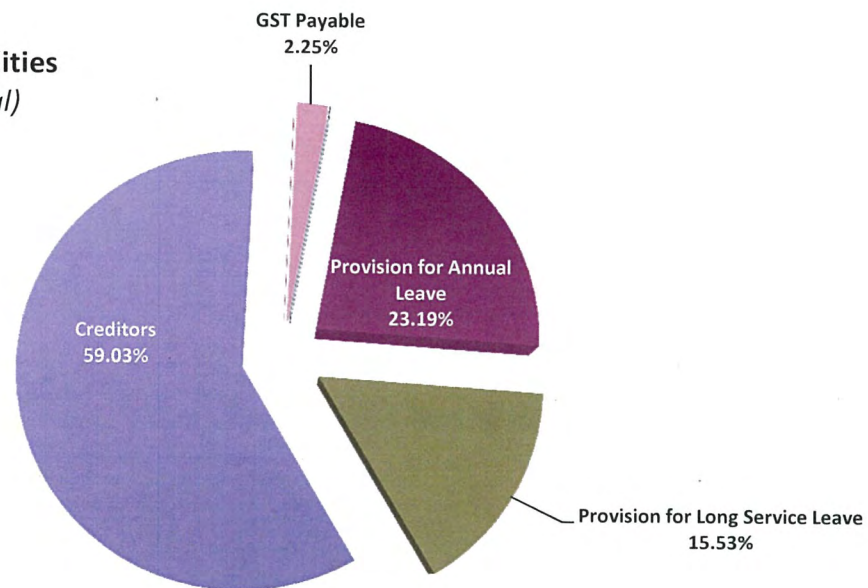
**Capital Expenditure
YTD Actual Vs Revised Budget**



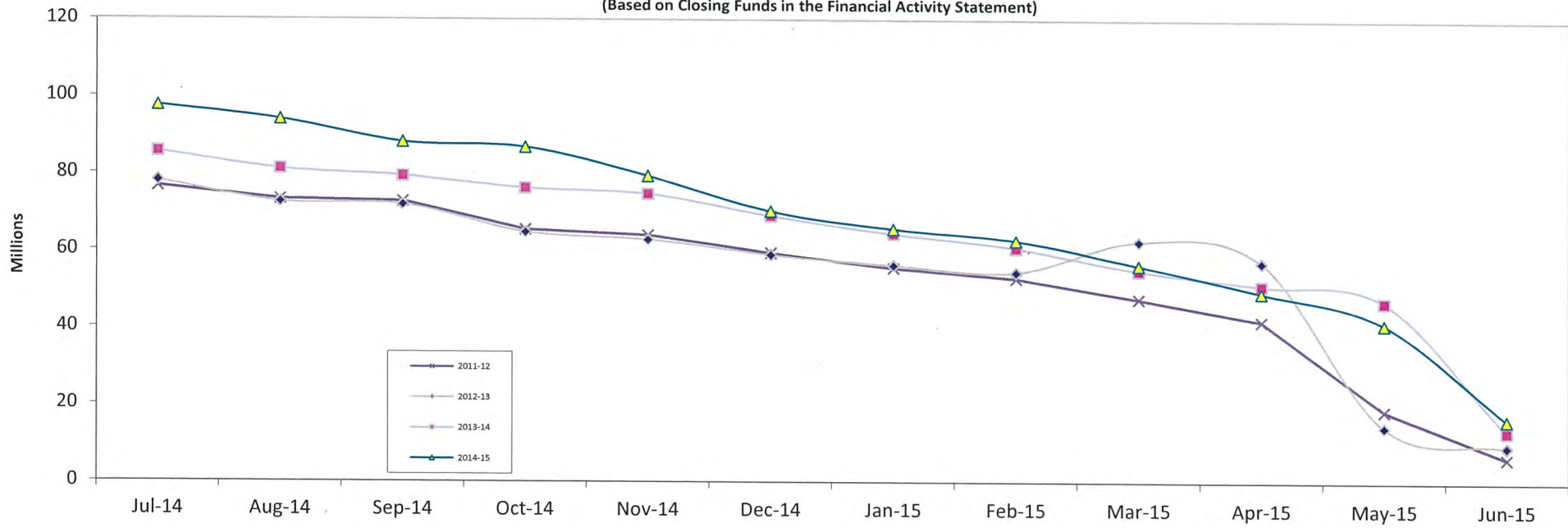
Current Assets (YTD Actual)



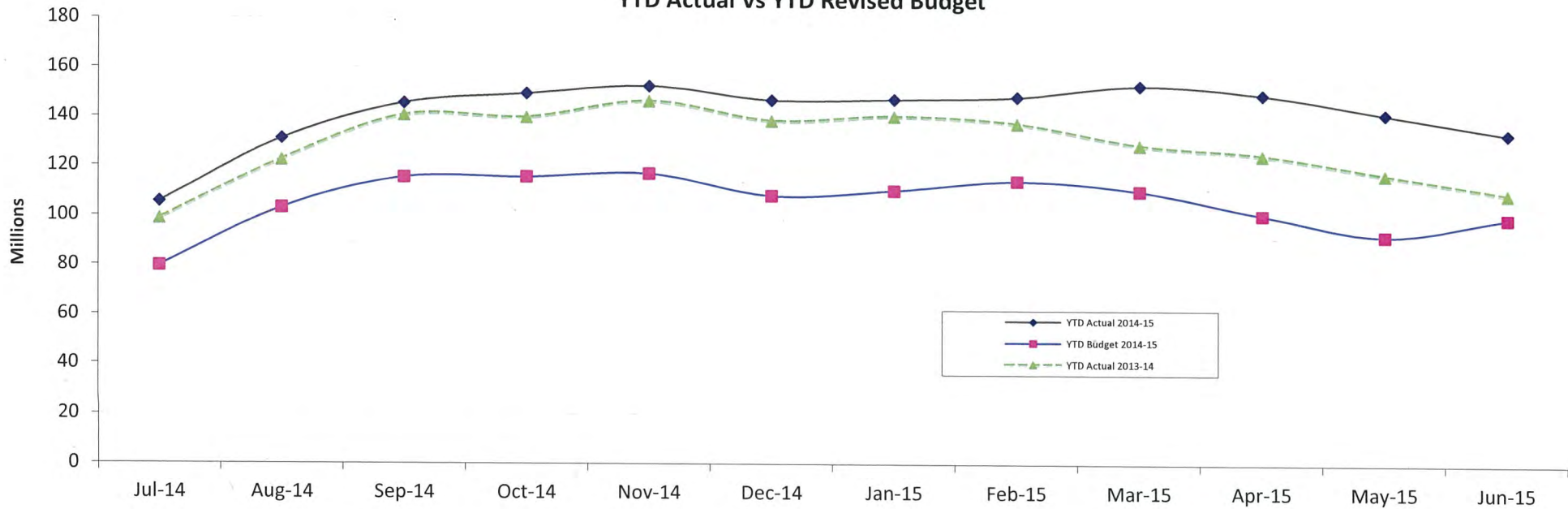
Current Liabilities (YTD Actual)



Municipal Liquidity Over the Year
(Based on Closing Funds in the Financial Activity Statement)



Cash & Investments Positions
YTD Actual Vs YTD Revised Budget



City of Cockburn - Reserve Funds

Financial Statement for Period Ending 30 June 2015

Account Details	Opening Balance		Interest Received		t/f's from Municipal		t/f's to Municipal		Closing Balance	
	Budget	Actual	Budget	Actual	Budget	Actual	Budget	Actual	Budget	Actual
Council Funded										
Bibra Lake Management Plan Reserve	847,819	847,819	(9,120)	21,044	-	-	(120,000)	(120,000)	718,699	748,863
Bibra Lake Nutrient Management	313,447	313,447	12,230	8,117	-	-	-	-	325,677	321,564
CCW Development Fund	-	-	-	-	12,491,292	12,491,292	-	(4,582,685)	12,491,292	7,908,607
Community Infrastructure	9,830,572	9,830,572	200,590	251,723	4,484,735	4,371,414	(8,481,224)	(8,411,024)	6,034,674	6,042,685
Community Surveillance Levy Reserve	653,841	653,841	29,690	13,741	643,918	861,535	(267,898)	(151,049)	1,059,551	1,378,068
Contaminated Sites	2,518,882	2,518,882	51,520	64,181	-	-	(100,000)	(94,144)	2,470,402	2,488,919
DCD Redundancies Reserve	2,991	2,991	-	56	-	39,692	(2,900)	(2,900)	91	39,839
Environmental Offset Reserve	277,367	277,368	(3,110)	7,183	-	-	-	-	274,257	284,550
Green House Emissions Reductions	652,516	652,516	13,880	16,403	200,000	200,000	(762,000)	(232,064)	104,396	636,855
Information Technology	261,600	261,600	37,220	5,369	100,000	100,000	(99,361)	(87,175)	299,459	279,795
Land Development & Investment Fund Reserve	4,719,455	4,719,455	283,600	106,688	18,058,553	3,117,453	(4,647,508)	(2,193,949)	18,414,100	5,749,647
Major Buildings Refurbishment	5,439,366	5,439,366	25,930	140,612	-	-	(252,412)	(81,515)	5,212,884	5,498,643
Mobile Rubbish Bins	63,279	63,279	24,440	(708)	-	-	(62,000)	(62,000)	25,719	571
Municipal Elections	49,722	49,721	13,270	1,288	-	-	-	-	62,992	51,009
Naval Base Shacks	766,920	766,921	16,420	19,776	151,413	146,919	(769,428)	(47,073)	165,325	886,542
Plant & Vehicle Replacement	5,930,546	5,930,546	76,610	126,404	3,527,151	3,450,000	(3,525,233)	(2,090,830)	6,009,074	7,416,121
Port Coogee Special Maintenance Reserve	1,005,467	1,005,468	23,060	24,874	270,000	312,317	(117,925)	(117,925)	1,180,602	1,224,733
Roads & Drainage Infrastructure	2,026,150	2,026,150	101,580	40,652	1,406,584	1,406,584	(1,762,755)	(468,809)	1,771,560	3,004,577
Staff Payments & Entitlements	2,271,100	2,271,100	157,540	54,884	110,000	110,000	(361,917)	(386,942)	2,176,723	2,049,042
Waste & Recycling	18,659,246	18,659,246	626,270	469,267	3,472,157	3,207,741	(1,875,359)	(1,405,066)	20,882,314	20,931,188
Waste Collection Levy	264,697	264,697	1,540	5,434	545,854	1,451,551	(325,500)	(325,500)	486,591	1,396,182
Workers Compensation	379,495	379,495	15,480	9,827	-	-	-	-	394,975	389,322
POS Cash in Lieu (Restricted Funds)	4,240,467	4,240,466	132,710	125,291	1,113,748	1,601,101	(888,000)	(477,227)	4,598,925	5,489,631
	61,174,947	61,174,947	1,831,350	1,512,105	46,575,406	32,867,599	(24,421,420)	(21,337,878)	85,160,283	74,216,773
Grant Funded										
Aged & Disabled Vehicle Expenses	322,162	322,162	9,855	7,710	62,625	104,119	(148,499)	(3,499)	246,144	430,493
CIHF Building Maintenance Resrv	-	-	-	7,650	2,414,548	2,064,509	-	-	2,414,548	2,072,159
Cockburn Super Clinic Reserve	1,936,374	1,936,374	169,220	30,964	-	-	(1,985,154)	(1,970,405)	120,440	(3,067)
Family Day Care Accumulation Fund	22,384	22,383	3,000	54	-	-	(24,314)	(24,314)	1,070	(1,876)
Naval Base Shack Removal Reserve	333,944	333,945	4,270	8,648	54,693	54,693	-	-	392,907	397,285
Restricted Grants & Contributions Reserv	5,923,657	5,923,657	-	156	-	-	(3,113,747)	(3,802,885)	2,809,910	2,120,928
UNDERGROUND POWER	754,224	754,224	(11,570)	19,175	1,200,000	1,146,332	(1,372,637)	(1,372,063)	570,016	547,667
Welfare Projects Employee Entitlements	444,423	444,422	12,452	11,667	10,513	37,479	(72,373)	(61,313)	395,015	432,255
	9,737,168	9,737,168	187,227	86,024	3,742,380	3,407,132	(6,716,724)	(7,234,479)	6,950,050	5,995,845
Development Cont. Plans										
Aubin Grove DCP	170,698	170,698	4,705	4,419	-	-	(887)	(639)	174,516	174,478
Community Infrastructure DCA 13	10,029,345	10,029,345	140,180	337,310	5,000,000	6,249,691	(484,789)	(136,678)	14,684,736	16,479,668
Gaebler Rd Development Cont. Plans	984,238	984,238	18,924	25,486	-	-	(3,474)	(3,156)	999,687	1,006,568
Hammond Park DCP	(14,180)	(14,180)	9,354	2,703	396,000	479,022	(13,595)	(9,721)	377,579	457,825
Munster Development	432,526	432,526	18,147	14,628	443,798	254,733	(17,871)	(9,825)	876,600	692,062
Muriel Court Development Contribution	(48,104)	(48,104)	-	(1,247)	-	-	(19,092)	(30,078)	(67,196)	(79,429)
Packham North - DCP 12	(105,792)	(105,792)	10,529	(1,367)	434,388	121,324	(19,192)	(6,896)	319,933	7,270
Solomon Road DCP	360,190	360,190	8,493	9,902	120,000	78,474	(7,721)	(37,173)	480,962	411,393
Success Lakes Development	887,990	887,991	3,817	22,994	-	-	(3,474)	(1,044)	888,333	909,941
Success Nth Development Cont. Plans	1,185,551	1,185,550	15,311	37,682	11,700	481,749	(5,611)	(4,256)	1,206,951	1,700,726
Thomas St Development Cont. Plans	12,079	12,079	294	313	-	-	-	-	12,373	12,392
Wattleup DCP 10	(9,363)	(9,363)	-	(81)	-	21,468	(12,695)	(9,377)	(22,058)	2,647
Yangebup East Development Cont. Plans	436,865	436,865	6,026	14,341	130,036	402,240	(3,748)	(2,079)	569,179	851,367
Yangebup West Development Cont. Plans	354,406	354,406	9,195	9,201	-	16,330	(2,624)	(2,019)	360,977	377,918
	14,676,448	14,676,448	244,975	476,284	6,535,922	8,105,031	(594,773)	(252,939)	20,862,572	23,004,825
Total Reserves										
	85,588,562	85,588,563	2,263,552	2,074,414	56,853,707	44,379,762	(31,732,917)	(28,825,295)	112,972,904	103,217,443

Total Reserves

Variance Analysis						
Municipal Financial Activity Statement for the period ended 30 June 2015						
	YTD Actuals \$	YTD Revised Budget \$	Full Year Revised Budget \$	YTD Variance \$	v = Favourable x = Unfavourable	Jun-15
OPERATING REVENUE						
Governance	76,017,251	73,643,427	73,643,427	2,373,824	√	GRV part year rates are \$879k above budget. First quarter FAGS grants for 15/16 is received this year, resulting in \$1.86m favourable variance in untied grants.
Community Services	7,705,239	7,224,112	7,224,112	481,127	√	Parking infringement and penalty is \$327k over budget.
Human Services	6,767,676	6,396,392	6,396,392	371,284	√	In-home care subsidies received is \$387k ahead of budget.
Waste Services	28,405,769	29,438,023	29,438,023	(1,032,254)	×	Landfill fees income is under budget by \$966k.
OPERATING EXPENDITURE						
Human Services	8,219,975	7,899,026	7,899,026	(320,949)	×	In-home care caregiver payments are over budget by \$430k.
Waste Services	18,554,061	20,096,655	20,096,655	1,542,594	√	Landfill levy and entry fees for RRRR are under budget by \$1.34m and \$1.25m respectively. However entry fees for recyclables are overspent by \$413k against budget. Waste collection salaries are over budget as well by \$224k.
Infrastructure Services	8,717,540	8,281,370	8,281,370	(436,170)	×	Facilities maintenance projects are over budget by \$458k.
ADDITIONAL FUNDING RECEIVED						
Grants & Contributions - Asset Development	18,339,185	17,582,755	17,582,755	756,430	√	Owners contribution received from DCP13, DCP1 and DCP5 are ahead from budget by \$1.31m, \$470k, and \$272k respectively. DCP12 however, is \$313k under budget. Cockburn Central West received \$1.5m grant ahead from its budget. MRRG funded road rehabilitation projects have not received the grants \$530k. Blackspot grant received for North Lake Road is under budget by \$353k. Regional road grant for North Lake Road received is under budget by \$547k and none received for Beeliar Drv \$440k.
Proceeds on Sale of Assets	4,002,046	19,419,231	19,419,231	(15,417,185)	×	Many land subdivisions have not been sold yet, they are Lot 9003 Beeliar Drv \$9.6m, Lot 33 Davilak Ave \$1.3m, Lot 23 Russell Rd \$1.0m, Lot 702 Beelier Pl & Lot 65 Erpingham Rd \$1.4m, Lot 40 Cervantes Loop \$900k and 4218 & 4219 Quarimor \$720k.

Capital Expenditure

for the period ended 30 June 2015

	Actuals	YTD Revised Budget	Full Year Revised Budget	\$ Variance to YTD Budget	✓ = Favourable ✗ = Unfavourable	Explanation
\$	\$	\$	\$	\$		
SUMMARY						
Purchase of Land and Buildings	16,974,555	36,944,947	36,944,947	19,970,392	✓	
Acquisition & Development of Infrastructure Assets	16,292,441	29,805,582	29,805,582	13,513,141	✓	
Purchase of Plant and Machinery	3,409,902	5,518,133	5,518,133	2,108,232	✓	
Purchase of Furniture and Equipment	0	0	0	0	✗	
Purchase of Computer Equipment	624,589	1,140,572	1,140,572	515,983	✓	
	37,301,487	73,409,235	73,409,235	36,107,748		
Material Variances Identified:						
<u>Works in Progress - Roads Infrastructure</u>						
3545 - Beeliar Drive Hammond Road North and South	1,442,797	1,663,434	1,663,434	220,637	✓	Project completed with a saving. Western Power works completed 8/03/2015.
2473 - North Lake Road Bibra Dr Roundabout	613,632	875,503	875,503	261,871	✓	Design in progress. Carry forward of 261k.
2471 - Beeliar Drive [Wentworth Pde to Kwinana Fwy]	644,757	1,062,997	1,062,997	418,240	✓	Initially delay due to Project CW3545 being given higher priority. Carry forward of 418k.
3544 - North Lake Road (Hammond to Kentucky)	3,173,412	4,308,542	4,308,542	1,135,130	✓	Street Lighting commenced February. \$558k in committed orders.
2989 - Berrigan Dr – [Kwinana Fwy to Jandakot] Construct 2nd cwy	44,237	1,597,301	1,597,301	1,553,064	✓	Design still in progress. Carry forward of \$1.5m.
2475 - Beeliar Drive (Spearwood - Stock)	135,194	3,375,000	3,375,000	3,239,806	✓	Design in progress. Projected carry forward of \$3.2 million.
Sub Total	6,054,029	12,882,777	12,882,777	6,828,748		
<u>Works in Progress - Parks Hard Infrastruc</u>						
009 - Bibra Lake Management Plan	326,310	3,344,748	3,344,748	3,018,438	✓	\$53k in committed orders. Contract has not been finalised. \$3m to be carried forward.
5443 - Formalise path to lookout from Manning Carpark. (Wooden Step	33,885	300,239	300,239	266,354	✓	Materials delivered, project fence installed. Initial delay due to the tender being developed.
5407 - Park Development - Coogee Beach Master Plan	80,558	541,400	541,400	460,842	✓	The project is on hold as it needs Department of Environment Regulation (DER) approval. \$460k to be carried forward.
Sub Total	440,753	4,186,387	4,186,387	3,745,634		
<u>Freehold Land</u>						
1539 - Subdivision Lot 702 Bellier Pl & Lot 65 Erpingham Rd	956,684	1,464,340	1,464,340		✗	Project has not been settled. Expected to be settled October Carry forward of the remaining budget.
1598 - Lot 9003 Beeliar Drive Beeliar	0	600,000	600,000	600,000	✓	The subdivisions is awaiting approval. Carry forward of the entire project.
Sub Total	956,684	2,064,340	2,064,340	600,000		
<u>Works in Progress - Buildings</u>						
007 - Operations Centre Upgrade	78,932	298,469	298,469	219,537	✓	The irrigation bore relocation commenced in May, with the Animal facility at the detailed design stage and the Workshop currently at the developed design stage
005 - Cockburn Integrated Health Facilities	3,444,250	3,794,050	3,794,050	349,800	✓	Committed order of \$580. Initial delay due to change of construction methodology.
026 - Cockburn Central Aquatic Recreation Ctr	8,619,727	23,591,757	23,591,757	14,972,030	✓	Significant redesign happening and the contract has not been signed. Project has a \$20m full year budget with the remaining budget to be carried forward.
4584 - Coogee B Caravan Holiday Units	467,499	0	0	(467,499)	✗	Gifted Asset. The City of Cockburn did not build or maintain assets. The Journal entry was debited against this account to register the buildings value.
4579 - Coogee B Caravan Ablution Block 1	234,232	0	0	(234,232)	✗	Gifted Asset. The City of Cockburn did not build or maintain assets. The Journal entry was debited against this account to register the buildings value.
4541 - CoC Civic Building HVAC Upgrade Project	2,708	252,000	252,000	249,292	✓	New pumps may need be put in to the project. Cost/benefit analysis to be undertaken. Project delayed as attention was given to amalgamation. Carry Forward of the entire project.
4471 - Civic Bldg Energy Reduction Initiative	7,200	262,000	262,000	254,800	✓	Insufficient funds this financial year. Project to begin major works next financial year with the remaining \$254k budget to be carried forward.

Capital Expenditure

for the period ended 30 June 2015

	Actuals	YTD Revised Budget	Full Year Revised Budget	\$ Variance to YTD Budget	✓ = Favourable ✗ = Unfavourable	Explanation
4473 - Coogee Holiday Park Electrical Supply Upgrade	\$ 0	\$ 387,957	\$ 387,957	\$ 387,957	✓	Project CW 4464 (Coogee Toilet Upgrade sewerage from domestic system) took priority. Entire project to be carried forward.
4464 - Coogee Toilet Upgrade sewerage from domestic system	56,374	470,000	470,000	413,626	✓	Recommendation Report approved. To commence meeting with the contractor TRACC Civil in July.
4540 - Coleville Cres Carpark Expansion	20,798	542,000	542,000	521,203	✓	Initial delay due to finalising specifications. \$522k to be carried forward.
4536 - Naval Base Holiday Park northern toilet block relocation & d	2,865	550,000	550,000	547,135	✓	Project agreed by managers to be cancelled as it was assessed as not being needed.
4485 - Atwell Clubrooms & Playing Surfaces - Upgrades	42,936	741,510	741,510	698,574	✓	Tender needs to be prepared by procurement. The design has been completed. Carry Forward of the remaining full year budget \$700k.
Sub Total	12,977,521	30,889,743	30,889,743	17,912,222		
Plant & Machinery						
7778 - Hooklift Truck	0	250,000	250,000	250,000	✓	Truck purchase cancelled.
7749 - Waste Collection Side Loader Compaction Truck PL749	0	380,000	380,000	380,000	✓	Carried forward. To be purchased and delivered next financial year.
7752 - Waste Collection Side Loader Compaction Truck PL752	0	380,000	380,000	380,000	✓	Carried forward. To be purchased and delivered next financial year.
7748 - Heavy Fleet-Waste Iveco F2350G/260 Rubbish Truck PL7481	0	385,000	385,000	385,000	✓	Carried forward. To be purchased and delivered next financial year.
Sub Total	0	1,395,000	1,395,000	1,395,000		

Road section	Existing traffic ¹	Date (Month/Year)	2013 layout	2020 traffic ²			2031 traffic ³			Required improvements and timing to address existing/forecast traffic volumes
				Without Roe 8	With Roe 8	Difference	Without Roe 8	With Roe 8	Difference	
Armada Road										
Kwinana Fwy to Solomon Rd	36,400	09/2014	4L	47,700	46,100	-1,600	55,300	54,900	-400	6L required by 2020 with/without Roe 8
Solomon Rd to Warton Rd	23,400	03/2013	4L/2L	47,800	45,400	-2,400	54,000	53,900	-100	6L required by 2020 with/without Roe 8
East of Warton Rd	15,200	05/2012	2L	34,000	33,000	-1,000	39,600	39,600	0	4L required by 2020 with/without Roe 8
Barrington Street										
West of Stock Rd	7,300	05/2014	2L	17,600	18,400	800	19,900	20,600	700	No improvement required (2L)
East of Stock Rd	7,800	06/2014	2L	13,700	15,100	1,400	17,900	17,800	-100	No improvement required (2L)
Beelair Drive										
West of Rockingham Rd	10,400	03/2012	2L	8,000	13,500	5,500	13,500	12,400	-1,100	No improvement required (2L)
Rockingham Rd to Spearwood Ave	16,400	08/2014	2L	18,000	12,700	-5,300	19,700	16,000	-3,700	4L required by 2020 without Roe 8
Spearwood Ave to Hammond Rd	27,700	03/2012	4L	26,900	19,600	-7,300	31,700	24,600	-7,100	No improvement required (4L)
Hammond Rd to Wentworth Pde	NA	NA	4L	26,500	18,000	-8,500	29,700	22,800	-6,900	No improvement required (4L)
Wentworth Pde to Kwinana Fwy	NA	NA	6L	34,000	21,300	-12,700	45,800	28,300	-17,500	No improvement required (6L)
Berrigan Drive										
North Lake Rd to Semple Ct	20,600	08/2012	4L	26,200	15,500	-10,700	31,600	17,100	-14,500	No improvement required (4L)
West of Kwinana Fwy	25,500	08/2012	4L	45,000	24,300	-20,700	52,400	28,200	-24,200	No improvement required (4L)
Kwinana Fwy to Jandakot Rd	18,900	08/2014	2L	29,300	24,400	-4,900	41,200	41,700	500	4L required by 2020 with/without Roe 8
Jandakot Rd to Karel Ave	14,400	08/2014	2L	24,900	20,800	-4,100	31,800	33,200	1,400	4L required by 2020 with/without Roe 8
Bibra Drive										
South of Farrington Rd	12,000	09/2012	2L	16,800	5,500	-11,300	20,100	7,300	-12,800	No improvement required (2L)
North of North Lake Rd	7,000	04/2015	2L	10,400	5,500	-4,900	19,900	7,300	-12,600	4L required by 2031 without Roe 8
Carrington Street										
North of Rockingham Rd	13,600	09/2014	2L	9,200	11,100	1,900	10,200	13,500	3,300	No improvement required (2L)
Cockburn Road										
Rockingham Rd to Spearwood Ave	21,100	03/2015	2L	24,600	24,700	100	25,500	27,600	2,100	4L required by 2020 with/without Roe 8
Spearwood Ave to Beelair Dve	17,600	06/2015	2L/4L	17,600	17,500	-100	19,600	21,100	1,500	4L required by 2020 with/without Roe 8
Beelair Dve to Russell Rd	14,400	04/2010	2L	28,700	28,700	0	27,800	27,500	-300	4L required by 2020 with/without Roe 8
South of Russell Rd	10,100	05/2014	2L	14,100	14,100	0	14,800	14,800	0	No improvement required (2L)
Discovery Drive										
Spearwood Ave to North Lake Rd	10,700	06/2013	2L	17,700	5,400	-12,300	17,100	5,000	-12,100	No improvement required (2L)
Farrington Road										
North Lake Rd to Bibra Dve	17,600	09/2012	2L	38,300	17,700	-20,600	59,700	19,400	-40,300	4L required by 2020 with/without Roe 8
Bibra Dve to Kwinana Fwy	26,600	09/2012	4L	41,800	25,600	-16,200	64,700	14,500	-50,200	6L required by 2031 without Roe 8
Forrest Road										
West of Stock Rd	7,400	03/2013	2L	7,600	0	-7,600	7,900	1,600	-6,300	No improvement required (2L)
East of Stock Rd	7,200	05/2014	2L	4,500	0	-4,500	14,600	0	-14,600	No improvement required (2L)
Frankland Avenue										
Russell Rd to Rowley Rd	NA	NA	2L	15,100	16,400	1,300	19,600	19,600	0	4L required by 2020 with/without Roe 8
Gibbs Road										
Kwinana Fwy to Lyon Rd	15,700	08/2014	2L	14,000	14,200	200	17,100	16,900	-200	4L required by 2020 with/without Roe 8
Lyon Rd to Liddellow Rd	8,400	08/2014	2L	9,000	9,400	400	10,700	10,500	-200	No improvement required (2L)
Hamilton Road										
Rockingham Rd to Spearwood Ave	7,500	12/2012	2L	6,800	8,600	1,800	8,200	11,600	3,400	No improvement required (2L)
Spearwood Ave to Beelair Dve	9,000	02/2012	2L	14,900	10,400	-4,500	16,700	11,900	-4,800	No improvement required (2L)
Hammond Road										
North Lake Rd to Beelair Dve	9,700	12/2014	2L	6,500	5,700	-800	8,900	6,900	-2,000	No improvement required (2L)
Beelair Dve to Russell Rd	9,000	12/2014	2L/4L	11,500	11,000	-500	15,400	13,900	-1,500	4L required by 2031 without Roe 8
Henderson Road										
Fanstone Ave to Russell Rd	5,500	02/2014	2L	1,900	1,900	0	2,000	2,000	0	No improvement required (2L)
Jandakot Road										
West of Solomon Rd	12,000	08/2013	2L	13,200	14,400	1,200	26,000	26,600	600	4L required by 2031 with/without Roe 8
East of Solomon Rd	9,700	08/2013	2L	10,900	12,100	1,200	18,100	18,300	200	4L required by 2031 with/without Roe 8
Karel Avenue										
Farrington Rd to Roe Hwy	14,400	06/2008	2L	36,000	21,200	-14,800	42,900	26,800	-16,100	4L required by 2020 with/without Roe 8
Roe Hwy to Berrigan Dve	19,700	08/2014	2L	25,100	21,800	-3,300	34,000	33,600	-400	4L required by 2020 with/without Roe 8
East of Berrigan Dve	11,400	08/2014	4L	12,400	12,400	0	20,800	20,800	0	No improvement required (4L)
Kwinana Freeway										
North of Roe Hwy	82,640	09/2011	5L	101,400	109,100	7,700	107,700	121,400	13,700	At least 6L required by 2020 with/without Roe 8
Roe Hwy to Berrigan Dve	NA	NA	4L	158,200	147,800	-10,400	180,600	160,200	-20,400	At least 6L required by 2020 with/without Roe 8
Berrigan Dve to Beelair Dve	99,100	09/2011	4L	140,300	141,000	700	162,300	161,100	-1,200	At least 6L required by 2020 with/without Roe 8
Beelair Drive to Russell Rd	88,900	09/2011	4L	109,200	104,900	-4,300	141,000	133,700	-7,300	At least 6L required by 2020 with/without Roe 8
Russell Rd to Rowley Rd	84,700	01/2013	4L	94,700	93,000	-1,700	125,300	121,500	-3,800	At least 6L required by 2020 with/without Roe 8
Liddellow Road										
Armada Rd to Gibbs Rd	3,900	12/2014	2L	5,200	5,600	400	4,900	4,700	-200	No improvement required (2L)
Gibbs Rd to Rowley Rd	2,500	12/2014	2L	3,600	3,500	-100	3,200	3,100	-100	No improvement required (2L)
Lyon Road										
Gibbs Rd to Rowley Rd	9,200	08/2014	2L	6,100	5,500	-600	6,700	6,600	-100	No improvement required (2L)
North Lake Road										
Farrington Rd to Roe Hwy alignment	NA	NA	5L	38,400	34,900	-3,500	53,600	34,000	-19,600	No improvement required (5L)
Forrest Rd to Phoenix Rd	NA	NA	5L	39,600	45,700	6,100	59,800	50,300	-9,500	No improvement required (5L)
Phoenix Rd to Hammond Rd	35,452	09/2012	4L	59,900	24,700	-35,200	66,000	35,500	-30,500	6L required by 2020 without Roe 8
Hammond Rd to Kwinana Fwy	17,800	06/2014	2L	28,500	21,900	-6,600	35,900	26,400	-9,500	4L required by 2020 with/without Roe 8
East of Kwinana Fwy	NA	NA	NA	13,500	12,600	-900	17,300	17,300	0	4L required by 2031 with/without Roe 8
Osprey Drive										
North Lake Rd to Beelair Dve	8,600	12/2007	2L	9,400	9,300	-100	9,000	8,600	-400	No improvement required (2L)
Phoenix Road										
West of Stock Rd	14,100	05/2012	4L	23,700	26,800	3,100	27,700	23,000	-4,700	No improvement required (4L)
East of Stock Rd	20,400	05/2014	4L	40,000	39,200	-800	46,900	39,600	-7,300	No improvement required (4L)
Poletti Road										
North Lake Rd to Beelair Dve	4,600	06/2014	2L	3,900	4,000	100	5,200	5,000	-200	No improvement required (2L)
Rockingham Road										
Cockburn Rd to Hamilton Rd	12,500	04/2015	4L	16,400	14,800	-1,600	18,100	23,100	5,000	No improvement required (4L)
Hamilton Rd to Spearwood Ave	17,700	02/2013	4L	15,500	14,900	-600	18,100	16,700	-1,400	No improvement required (4L)
Spearwood Ave to Beelair Dve	16,200	02/2012	2L	14,900	15,700	800	16,700	16,600	-100	No improvement required (2L)
Roe Highway										
Stock Rd to North Lake Rd	NA	NA	NA	0	35,800	35,800	0	61,200	61,200	6L required by 2031
North Lake Rd to Kwinana Fwy	NA	NA	NA	0	74,700	74,700	0	97,800	97,800	6L required by 2020
East of Kwinana Fwy	51,500	10/2011	4L	63,000	81,400	18,400	81,800	103,200	21,400	6L required by 2020
Rowley Road										
Rockingham Rd to Power Ave	NA	NA	NA	0	0	0	10,600	6,000	-4,600	No improvement required (2L)
Power Ave to Frankland Ave	NA	NA	NA	0	0	0	15,400	10,700	-4,700	No improvement required (2L)
Frankland Ave to Kwinana Fwy	4,870	06/2013	2L	13,400	11,700	-1,700	18,400	13,600	-4,800	4L required by 2031 without Roe 8
Kwinana Fwy to Liddellow Rd	7,600	06/2013	2L	7,500	8,600	1,100	7,300	7,200	-100	No improvement required (2L)
Russell Road										
West of Rockingham Rd	NA	NA	2L	16,400	16,400	0	19,200	18,900	-300	4L required by 2020 with/without Roe 8
Rockingham Rd to Henderson Rd	9,700	05/2014	2L	12,100	8,700	-3,400	17,900	13,800	-4,100	4L required by 2031 without Roe 8
Henderson Rd to Hammond Rd	13,600	05/2014	2L	14,300	10,200	-4,100	18,600	13,200	-5,400	4L required by 2031 without Roe 8
Hammond Rd to Kwinana Fwy	19,500	04/2015	4L	17,800	14,100	-3,700	25,500	19,900	-5,600	No improvement required (4L)

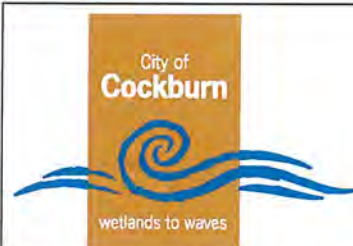
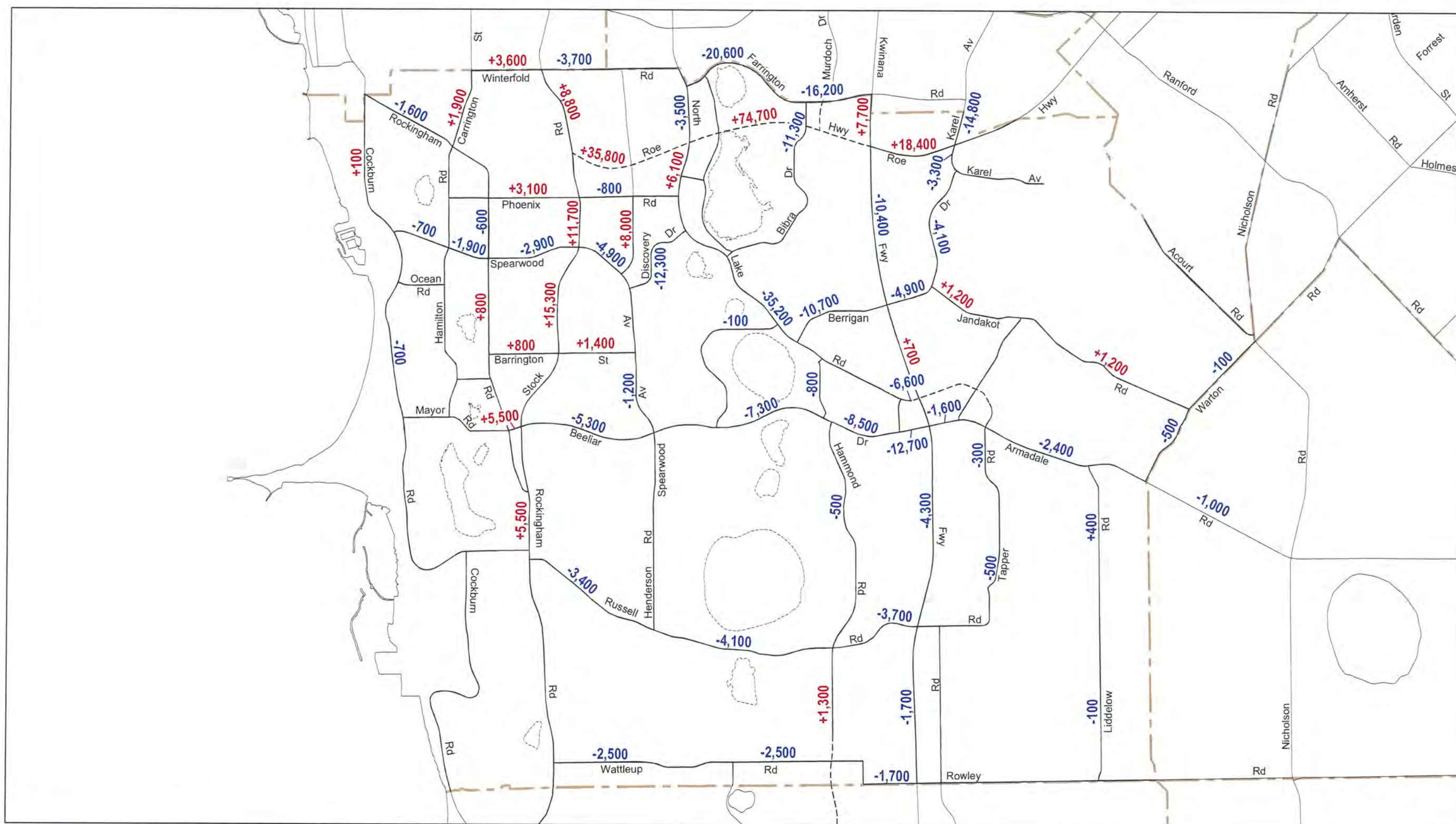
Road section	Existing traffic ¹	Date (Month/Year)	2013 layout	2020 traffic ²			2031 traffic ³			Required improvements and timing to address existing/forecast traffic volumes
				Without Roe 8	With Roe 8	Difference	Without Roe 8	With Roe 8	Difference	
Sample Court										
Berrigan Dve to North Lake Rd	5,300	06/2015	2L	6,300	6,500	200	6,800	7,100	300	No improvement required (2L)
Solomon Road										
South of Jandakot Rd	6,800	03/2012	2L	5,200	5,300	100	9,100	9,100	0	No improvement required (2L)
Spearwood Avenue										
Cockburn Rd to Hamilton Rd	6,700	08/2014	2L	7,500	6,800	-700	8,000	6,800	-1,200	No improvement required (2L)
Hamilton Rd to Rockingham Rd	7,300	05/2012	2L	9,100	7,200	-1,900	10,300	7,800	-2,500	No improvement required (2L)
Rockingham Rd to Stock Rd	11,400	08/2014	2L	11,700	8,800	-2,900	13,600	9,400	-4,200	No improvement required (2L)
Stock Rd to Barrington St	17,111	11/2012	4L	23,800	18,900	-4,900	26,700	21,300	-5,400	No improvement required (4L)
Barrington St to Beellar Dve	22,000	02/2013	2L	21,700	20,500	-1,200	26,100	25,000	-1,100	4L required by 2020 with/without Roe 8
Beellar Dve to Fanstone Ave	8,700	02/2014	2L	1,800	1,800	0	2,000	1,800	-200	No improvement required (2L)
Stock Road (Melville-Mandurah Hwy)										
Winterfold Rd to Forrest Rd	24,900	02/2010	4L	32,100	40,900	8,800	45,000	53,300	8,300	6L required by 2031 with Roe 8
Forrest Rd to Phoenix Rd	NA	NA	4L	35,800	44,500	8,700	37,900	60,900	23,000	6L required by 2031 with Roe 8
Phoenix Rd to Spearwood Ave	NA	NA	4L	35,100	46,800	11,700	45,700	60,200	14,500	6L required by 2031 with Roe 8
Spearwood Ave to Barrington St	21,000	02/2010	4L	26,000	41,300	15,300	40,700	56,200	15,500	6L required by 2031 with Roe 8
Sudlow Road										
Phoenix Rd to Spearwood Ave	7,600	03/2014	2L	8,000	16,000	8,000	13,300	19,300	6,000	4L required by 2020 with Roe 8
Tapper Road										
Armada Rd to Bartram Rd	10,900	03/2015	2L	11,400	11,100	-300	11,600	11,500	-100	No improvement required (2L)
Bartram Rd to Gibbs Rd	4,700	03/2015	2L	8,400	7,900	-500	10,600	8,100	-2,500	No improvement required (2L)
Warton Road										
North of Jandakot Rd	8,900	05/2012	4L	24,900	24,800	-100	32,700	32,800	100	No improvement required (4L)
Jandakot Rd to Armadale Rd	NA	NA	4L	14,000	13,500	-500	14,700	14,700	0	No improvement required (4L)
Wattleup Road										
Rockingham Rd to Power Ave	5,200	03/2014	2L	14,000	11,500	-2,500	0	0	0	No improvement required (2L)
Power Ave to Mandogalup Rd	6,300	03/2014	2L	17,000	14,500	-2,500	0	0	0	No improvement required (2L)
East of Mandogalup Rd	4,400	03/2014	2L	14,400	11,900	-2,500	1,000	1,000	0	No improvement required (2L)
Wentworth Parade										
North of Bartram Rd	11,100	08/2013	2L/4L	13,100	13,100	0	14,400	14,600	200	No improvement required (2L)
Winterfold Road										
West of Stock Rd	10,200	04/2013	2L	14,400	18,000	3,600	17,800	14,100	-3,700	No improvement required (2L)
East of Stock Rd	8,500	08/2011	2L	16,000	12,300	-3,700	21,500	11,900	-9,600	No improvement required (2L)

1. Existing Traffic = most recent available weekday traffic volume from City of Cockburn or Main Roads WA databases

2. 2020 traffic volumes are sourced from the City's 2013 District Traffic Study forecasts

3. 2031 traffic volumes are sourced from the City's 2013 District Traffic Study forecasts

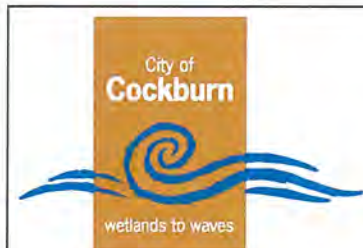
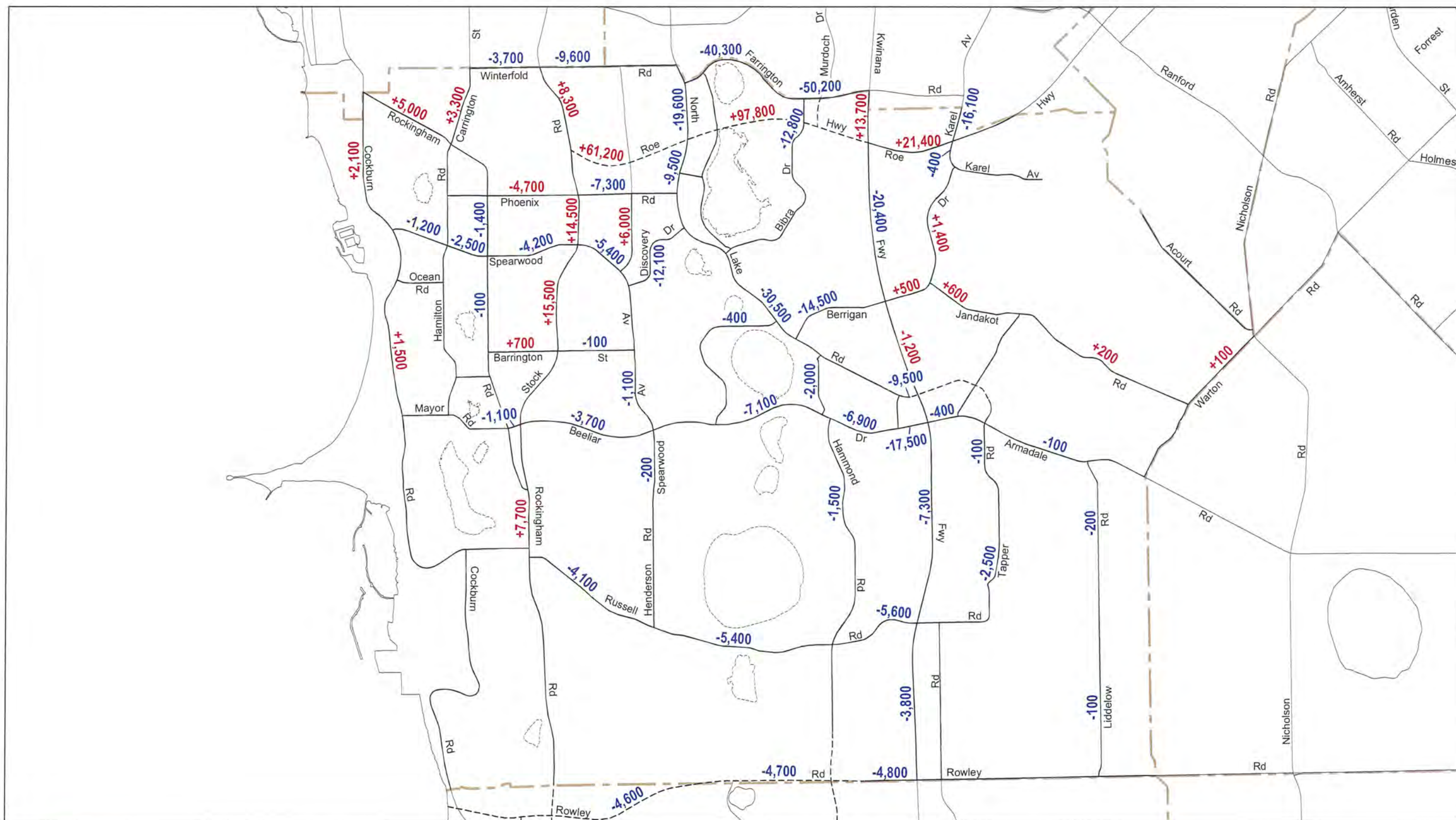
NA = not available/not applicable



GNA 7/2015

2020 WEEKDAY TRAFFIC DIFFERENCES WITH ROE HIGHWAY STAGE 8

(Based on 2013 District Traffic Study Forecasts)



GNA 7/2015

2031 WEEKDAY TRAFFIC DIFFERENCES WITH ROE HIGHWAY STAGE 8

(Based on 2013 District Traffic Study Forecasts)

POL	COMMUNITY ENGAGEMENT	SC2
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POLICY CODE:	SC2
DIRECTORATE:	Executive Services
BUSINESS UNIT:	Executive Support
SERVICE UNIT:	Executive Support
RESPONSIBLE OFFICER:	Manager, Community Services
FILE NO.:	182/001
DATE FIRST ADOPTED:	18 July 2000
DATE LAST REVIEWED:	13 March 2014
ATTACHMENTS:	N/A
DELEGATED AUTHORITY REF.:	N/A
VERSION NO.	3

Dates of Amendments / Reviews:	
DAPPS Meeting:	15 November 2007 28 November 2013 27 February 2014
OCM:	13 December 2007 12 April 2012 12 December 2013

BACKGROUND:

This policy applies to all employees and to consultants engaged by the City of Cockburn. It reflects the values embodied in the City of Cockburn's Strategic Community Plan (2012 to 2022), Cockburn Community Development Strategic Plan (2011 to 2014), Communications Strategy and Action Plan (2012 to 2017) and Disability Access and Inclusion Plan (2012 to 2017)

Definition:

At the City of Cockburn, community engagement refers to the way in which people interact and connect on policies, projects, events and issues to make the City of Cockburn an attractive place to live, work, visit and invest in.

"Community" is broadly defined as any ratepayer, resident, landowner or other person or body, either singularly, jointly or severally, having an economic or personal interest in any land, development, proposal or issue which affects, or potentially impacts, upon the physical, economic, social or environmental fabric of the City of Cockburn.

PURPOSE:

- (1) To adopt a Community Engagement Model based on three strategies – information, consultation and active participation.

POL	COMMUNITY ENGAGEMENT	SC2
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- (2) To implement the Community Engagement Framework to ensure a consistent approach to community engagement across the City.
- (3) To ensure the endorsed approach is widely practiced by City staff.
- (4) To ensure that all legislative and statutory regulations are met and, where possible, exceeded.
- (5) To regularly survey the community on their needs and the performance of the City in meeting their needs.

Principles

Providing stakeholders with the opportunity to engage with Council on policies, plans, events or issues that impact their lives requires:

1. strong organisational commitment;
2. an inclusive approach to accommodate minority and hard to reach groups;
3. adequate financial, human and technical resources;
4. access to information so that stakeholders have the capacity to be informed and understand the impact of policies, projects, events or issues;
5. adequate time, planning and preparation;
6. clear objectives for the consultation;
7. that the City accounts for the use it makes of stakeholders' input through the delivery of feedback;
8. that the City maintains a consultation register and reporting system to ensure that it learns from community engagement activity.

POLICY:

(1) Procedure

In planning and conducting community engagement, Council staff will be guided by this Policy and the Community Engagement Framework.

If Council makes a decision or implements a change that impacts the community, a Community Engagement Plan will need to be prepared. The decision to prepare a Plan will be made considering the size and nature of the change(s) that are being proposed, and the potential impact(s) on the community.

In preparing a Community Engagement Plan, staff will follow, where appropriate, the ten step process outlined in the Framework:

- Step 1 - Decide if community engagement is needed
- Step 2 - Assess the impacts
- Step 3 - Address legal requirements
- Step 4 - Determine the objectives
- Step 5 - Determine the methods of engagement

POL	COMMUNITY ENGAGEMENT	SC2
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- Step 6 - Determine when engagement will occur
- Step 7 - Resource and implement the plan
- Step 8 - Manage risks
- Step 9 - Provide feedback
- Step 10 - Reflect on the lessons learned

(2) Review

The Community Engagement Framework and Policy will be reviewed every two years.

- (3) The City of Cockburn is committed to the ongoing consultation of its Community to ensure that its Plan for the Future of the District and Corporate Strategic Planning processes are cognizant of community opinion.

To ensure community opinion represents a gauge of Councils wider planning processes, the following practices will form the basis of its consultation mechanisms:

1. A comprehensive Community Needs Survey (CNS), undertaken tri-ennially to establish and continually review that Councils facility and service provision is aligned, so far as it is practical to do so, with community priorities.
2. An annual Community Satisfaction Benchmarking (CSB) Survey to monitor the standard of Councils facilities and service provision.
3. Ad-hoc localised surveys undertaken on an as required basis as a means of measuring community opinion on issues identifiable within a clearly defined local area or group of people.



Government of Western Australia
Department of Fire & Emergency Services

Attach 1



Our Ref: FT22245
Your Ref: 027/007

Mr Stephen Cain
Chief Executive Officer
City of Cockburn
PO Box 1215
BIBRA LAKE WA 6965

Dear Stephen

In response to your letter dated 16th April 2014.

The proposed ESL boundary review for the Banjup area in 2014 was postponed on request by the Minister for Emergency Services. Following further consideration, the proposed changes for 2014 have been revisited and are once again under consideration for 1 July 2015. The ESL category reflects the services available to the area, not the risks associated with the area. Ideally these will be closely matched but this is not always the case or necessary. The proposed ESL boundary change for the Banjup area has been part of this review.

DFES conduct an annual review of the ESL boundaries. When conducting the review DFES consider both the risk profile and the resourcing to determine any proposed boundary changes.

Following a review DFES consider the Banjup area meets the following:

Risk profiles

- Detached houses, duplexes, apartments and other buildings formed into suburbs in a Metropolitan Region.
- Risk of uncontrolled bushfire in areas of forest, scrub, grassland or Rural Urban Interface.

Resourcing

- DFES have determined the proposed ESL boundary change in the Banjup area can be covered by the seven minute isochrones from the Success Career Fire and Rescue Service (CFRS). This allows for 90% of calls to be responded to within 12 minutes;
- The area is supported by a network of metropolitan CFRS brigades which includes specialised appliances such as Specialised Equipment Tender for Breathing Apparatus operations, aerial appliances and heavy rescue;
- The area is supported by network of metropolitan volunteer SES and Bushfire brigades; and

- The Banjup area is supported by Metropolitan Bulk Water Tankers including the tanker located at the Jandakot Bush Fire Brigade.

The above response provides a comprehensive capability including both internal and external structural fire fighting, bush fire fighting, rescue and a hazardous materials capability.

The Fire and Emergency Services Regulations of 1988 prescribe different emergency services categories for different kinds of services that are provided. The Fire and Emergency Services Act 1998 declares that following consultation with the Local Government the Minister may declare a prescribed emergency services category. This consultation process is an information gathering process seeking feedback from Local Government. Following the consultation the Fire and Emergency Services Commissioner makes a recommendation to the Minister concerning the appropriate ESL category.

In the case of the Banjup area, the Department can provide an integrated Career service within acceptable time frames and accordingly will recommend the Banjup area is categorised ESL 1.

Yours Sincerely



GREG FEENEY
SUPERINTENDENT SOUTH COASTAL OPERATIONS

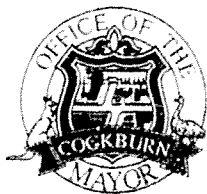
20th April 2015

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Western Australia 6965

9 Coleville Crescent, Spearwood,
Western Australia 6163

Telephone (08) 9411 3444
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www.cockburn.wa.gov.au



City of Cockburn

CITY OF COCKBURN
19 JUN 2015
SUBJECT 150/014
RETENTION 96.3.2 D6
PROPERTY
APP
ACTION 011/010

Logan K. Howlett, JP

16 June 2015

Mr Stephen Cain
Chief Executive Officer
City of Cockburn
PO Box 1215
BIBRA LAKE DC WA 6965

Dear Mr Cain,

RE: PROPOSED ESL BOUNDARY CHANGE TO THE SUBURB OF BANJUP

Recently the City of Cockburn received advice from the Department of Fire and Emergency Services, highlighting the completion of a risk review and the subsequent justification to the change in status of the Emergency Services Levy (ESL) to the landowners of property within Banjup.

At the City of Cockburn Local Emergency Management Committee (LEMC) meeting held on 2 June 2015, the committee spoke on the wide ranging implications this proposal may have to the residents of Banjup, and the potential risk associated with the type of emergency response received.

Although the LEMC does not see the ESL monetary rate as an item that fits within its terms of reference, the committee would like to make the follow observations, highlighting concerns should the potential change come to fruition;

- The Career Fire and Rescue service does not have an all-terrain capability located within the advisable 12 minutes response time of Banjup;
- The Success Fire Station cannot effectively respond to bushfires 30 metres from any sealed road reserve (the majority of 000 Call outs for Banjup) without support from the City's Volunteer Bushfire Brigade; and
- Potential delays in response and appropriate equipment may allow for the spread of a fire during heightened bushfire conditions.

Due to the above concerns and potential wider impact to the community, The City of Cockburn LEMC unanimously decided not to support the propose change of the ESL without a thorough justification from the Department of Fire and Emergency Services.

Yours Sincerely,

Logan Howlett
Mayor
City of Cockburn
Chair – City of Cockburn Local Emergency Management Committee

City Of Cockburn

I would like to confirm that Jandakot Volunteer Bush Fire Brigade do not support the proposed direction to move the Banjup area from the current ESL 3 district to include it into the Metropolitan fire district being ESL 1. The brigade strongly believes there is no supporting evidence for the change as per the proposed position because;

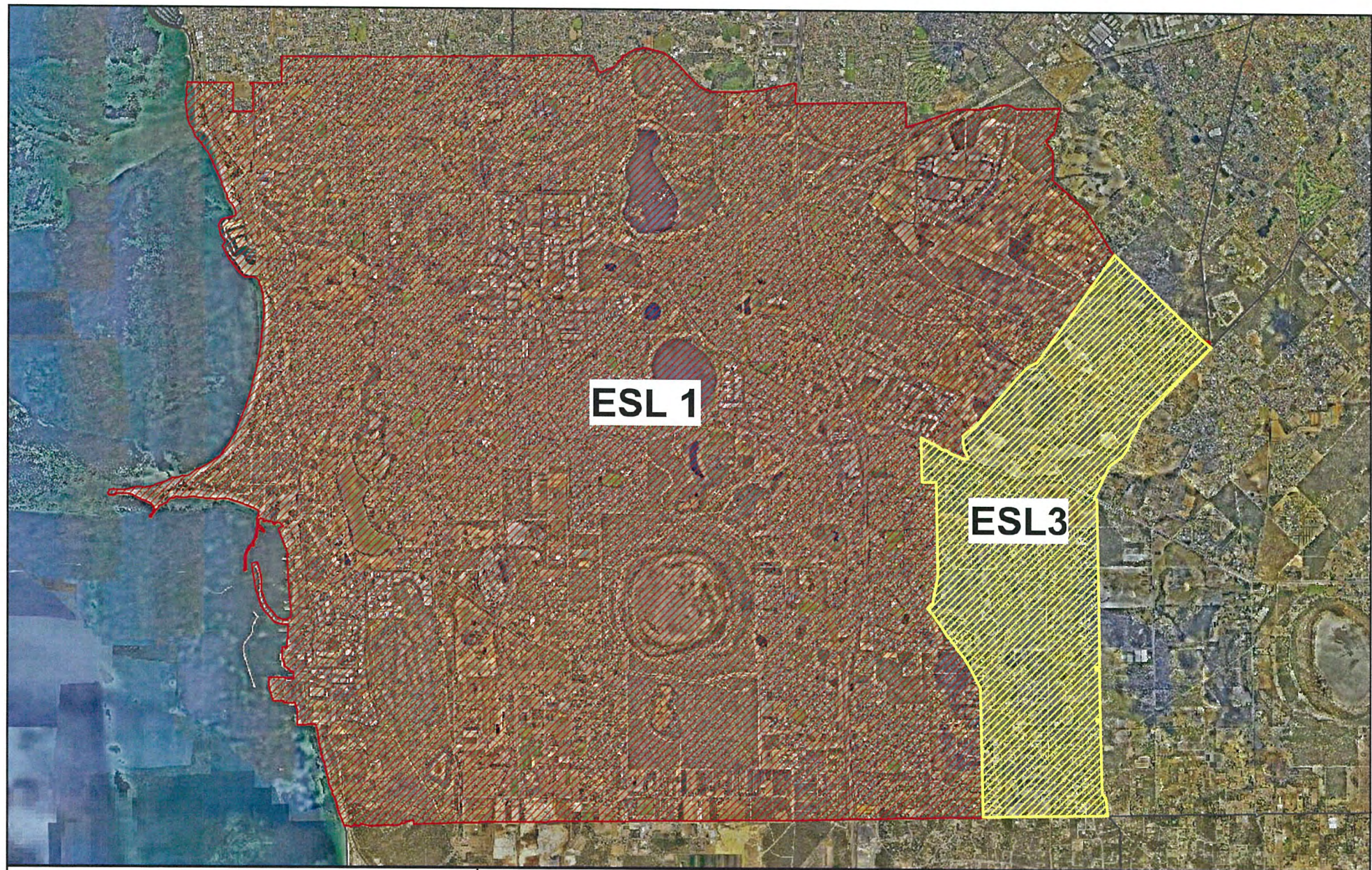
- The area has not been developed into an urban area, nor is it likely to be into the foreseeable future.
- There is no established reticulated water supply throughout Banjup.
- Very low instance of structural fires in the Banjup area
- The brigade continues to remain operationally effective with a strong active member base
- Considerable delayed response times that can occur from traffic in and around the area during business hours, especially during peak hour periods.

It should be observed the definition of ESL as per the DFES website is as follows;

- ESL 1 is '*A network of metropolitan career Fire and Rescue Service stations and the SES.*' – This is not applicable as the Jandakot BFB will still be required for fires within the area and does not reflect the true nature of the response within the area.
- ESL 3 is '*Volunteer Fire and Rescue Service brigade with the availability of a network of career fire stations OR bush fire brigade with the availability of a network of career fire stations AND the state wide SES network.*' - This continues to remain the correct definition for within the Banjup area.

Regards

Shane Harris
Captain/Fire Control Officer
Jandakot Volunteer Bush Fire Brigade



ESL 1

ESL3



CITY OF COCKBURN
G.I.S Services Department

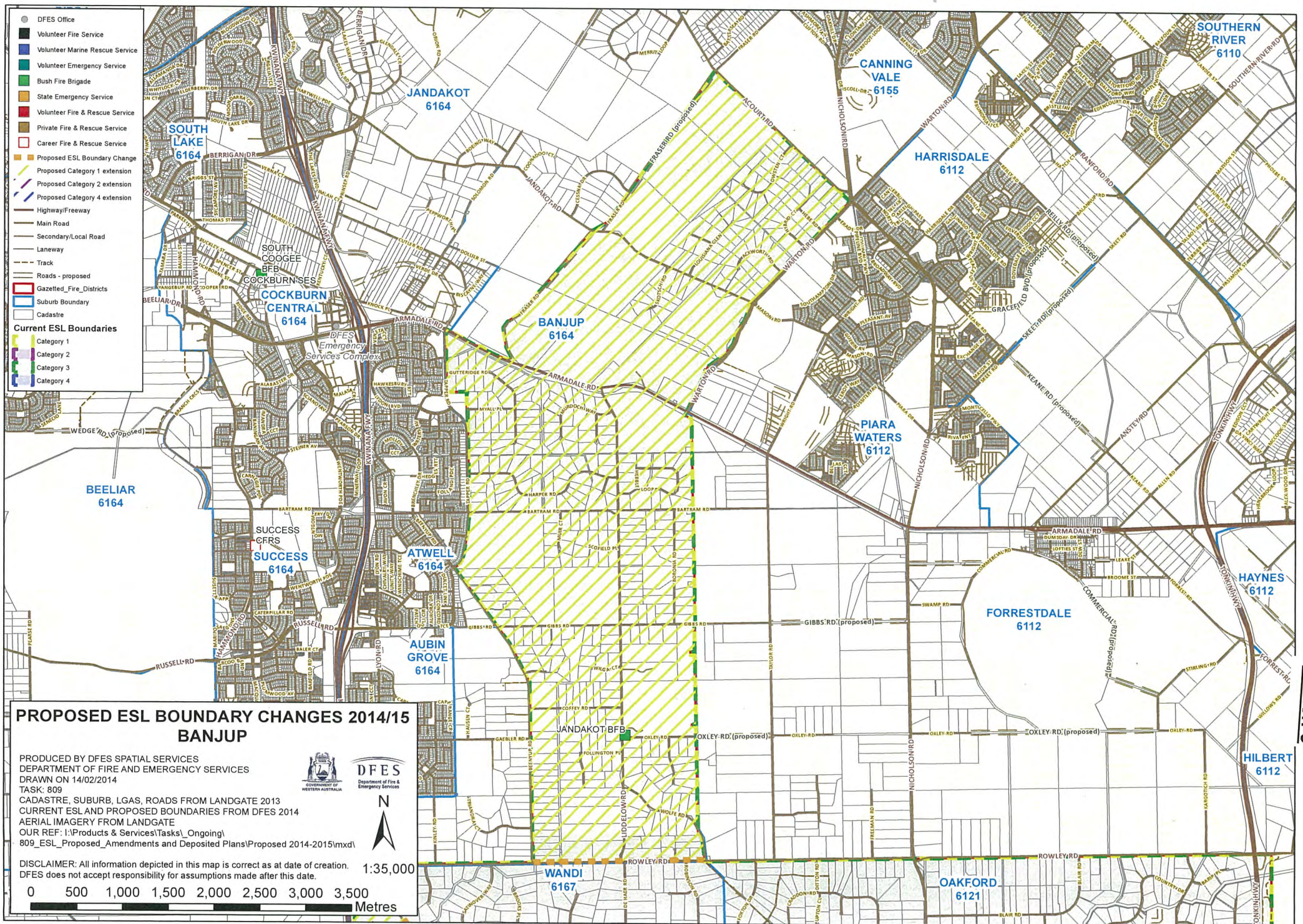
Current ESL Categories

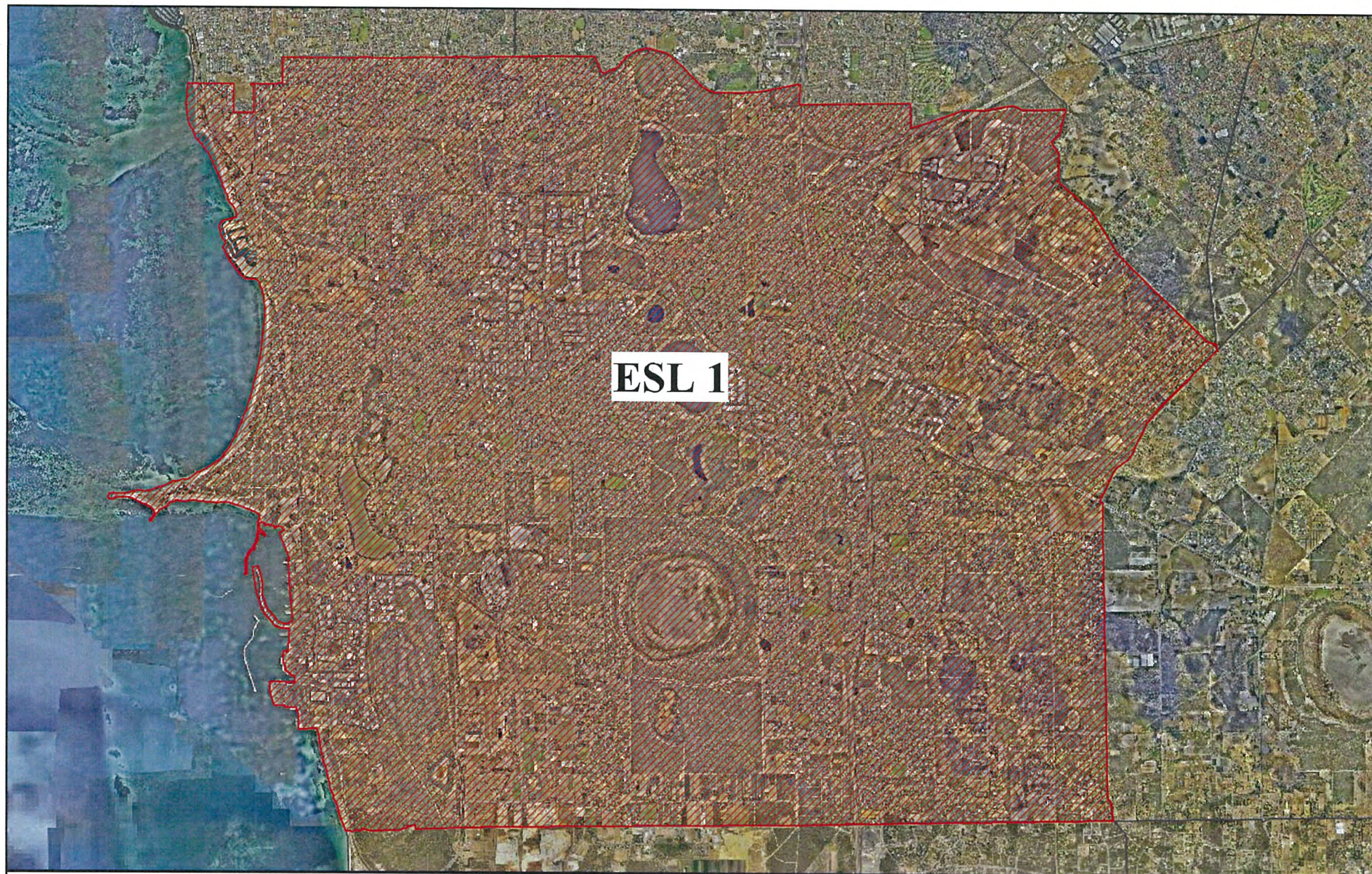
SCALE = 1 : 55,000



DATE: 26/03/2014

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 <p>CITY OF COCKBURN G.I.S Services Department</p>	<p>Proposed ESL Category</p>	<p>SCALE = 1 : 55,000</p>	<p>DATE: 26/03/2014</p>	
		<p>DISCLAIMER - THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE IT WAS INTENDED. THE CITY OF COCKBURN TAKES NO RESPONSIBILITY FOR THE ACCURACY OF THE DATA CONTAINED HEREIN</p>		

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EXTRACT OF MINUTES OF THE ORDINARY COUNCIL MEETING – 9 JULY 2015**17.2 (MINUTE NO 5550) (OCM 9/7/2015) - PROPOSED AMENDMENT TO BANJUP SUBURB BOUNDARY AND NEW LOCALITY NAME (159/008) (G BOWMAN) (ATTACH)****RECOMMENDATION**

That Council:

- (1) advise the Geographic Names Committee (GNC) that it supports the proposal to amend the boundary of the suburb of Banjup and create a new locality as shown in the attachment to the Agenda;
- (2) advise the Geographic Names Committee (GNC) that it supports the proposal to name the new Locality "Quendalup (first preference)" or "Kwentilup (second preference)";
- (3) advise landowners of the current Banjup locality 'Resource Zone' properties (bounded by Warton Road and Acourt Road), plus the developer of the remaining area (Stockland) of the proposal and invite comment on the proposal and Council's preferred names; and
- (4) subject to not more than 50% of those referred to in (3) above objecting to the proposal, or to Council's preferred locality names, the matter not be required to be reconsidered by Council.

COUNCIL DECISION

MOVED Cllr S Portelli SECONDED Cllr Y Mubarakai that Council:

- (1) propose to amend the boundary of the locality of Banjup and create a new suburb bounded by Armadale Road to the south, Jandakot Road to the north, Warton Road to the east and Solomon Road to the west, excluding the landholdings within the Jandakot Industrial Area (as shown in the attachment to the Minutes), to be called 'Quendalup' or 'Kwentilup';
- (2) proposed that the remaining area of the locality of Banjup to the north of Jandakot Road and west of Warton Road be included within the existing locality of Jandakot;
- (3) advise those affected landowners within the current locality of Banjup, together with the developer of the Calleya estate (Stockland), by letter of the proposal to create a new locality, to be called 'Quendalup' or 'Kwentilup' and of the proposed boundary adjustment to the locality of Jandakot to include part of Banjup;
- (4) invite affected landowners to make a written submission on the

proposal, the recommended names and any alternate names which conform to Geographic Names Committee (GNC) Guidelines within a 21 day period;

- (5) refer the submissions regarding the proposal, the recommended names, and any alternate names with Officers recommendations to Council for consideration; and
- (6) advise GNC of Council's decision.

CARRIED 9/1

Reason for Decision

As per the Officer's report it is appropriate to differentiate the land uses, that is, the existing and proposed residential areas in comparison with the resource/rural type landholdings. The areas north and west of Jandakot Road within the locality of Banjup should then be incorporated within the locality of Jandakot, due to the similar nature of their land uses and their proximity to Jandakot Airport and its surrounds.

It is incumbent upon Council to consult with the community on proposed names and not give a limited selection of names that we see fit. Council should provide some names to stimulate discussion but also allow the community to suggest their own proposals, provided they are consistent with the Guidelines required by GNC. It would be appropriate to inform GNC of Council's position at this time to enable them to prepare for a full Council decision.

Background

Due to the growth and development of the northern portion of the locality of Banjup an investigation has taken place to determine whether there is an opportunity to create a new suburb by amending the boundary of Banjup. Banjup is a Nyungar name and given the Reconciliation Action Plan and the Geographic Names Committee preference for Nyungar names research and consultation regarding an appropriate Nyungar name has taken place for the proposed new locality.

The City of Cockburn's Reconciliation Action Plan 2013-16 includes an Action regarding Signage and Naming" Create a List of appropriate Nyungar Names to be used in naming Cockburn sites, roads and trails".

Submission

N/A

Report

Due to the growth and development of the Northern portion of the locality of Banjup an investigation has taken place to determine whether there is an opportunity to create a new locality by amending the boundary of Banjup.

Currently, the Banjup area extends from Acourt and Fraser Roads in the north, to Rowley Road in the south. It is bounded by Tapper, Beenyup and Lyon Roads to the west and the Local Government boundary with the City of Armadale to the east. The suburb of Banjup in its current form covers an area of 2247ha and consists of a range of different land uses.

Compliance with GNC guidelines

The GNC guidelines relating to locality names and boundaries are specific and any new/modified localities will not be approved unless they adhere to these guidelines. The two key compliance matters for modification or creation of suburb boundaries are locality size and lot numbers. The guidelines note that “a locality should not be so small that it is unable to be distinguished from the surrounding area, however they should be of a reasonable size for practical purposes”. Localities with urban development shall be a minimum size of 100ha, with the minimum number of Lots to be 1,000.

The recommended option proposes a site area of 827ha. This option once fully developed would cover a land area of 827ha with approximately 3100 lots and a projected population of 8350 by 2031.

Result on current suburbs of Jandakot and Banjup (south)

Should the City seek to restructure the existing suburb of Banjup to create a new suburb, the remaining areas of Banjup (south) would still continue to experience growth in both population and dwelling numbers. The population forecasts (forecast i.d) predict that the new Banjup (south) area will continue to grow in population over the next 20 year period by an average 1.37% per annum.

The recommended option would not modify the existing suburb of Jandakot, but would decrease the current suburb of Banjup by approximately 1/3 of its current size. Much of the Jandakot area is already built out due to its location within the Resource zone. A reduced Banjup would still be of a size sufficient under the GNC guidelines for rural areas. Population levels will continue to rise in this suburb over the next 20 years, despite the loss of all areas to the north of Armadale Road. Most areas of Banjup (south) are already subdivided into approximately 2ha blocks as per the requirements of Resource zone.

Naming of new Suburb

The requirements for a new suburb locality name are administered through the guidelines and necessitate the need for a new locality name to have some specific connection to Western Australia, or the local area in question. Banjup is a Nyungar name and following consultation a Traditional Owner Trevor Walley states that Banjup refers to the plants referred to as banjine especially the Pimelea Rosea (Rose Banjine) from which Nyungar people would make rope by twining the stems together for uses such as snares.

It is recommended that Council should seek to create a new locality and name it an Indigenous Nyungar name in accordance with the Reconciliation Action Plan and the Geographic Names Committee (GNC) preference.

In respect of the Nyungar names, these are particularly encouraged by the GNC guidelines under Section 4 as follows:

4. *Recognition and Use of Indigenous Names*
The GNC is committed to the promotion, preservation and restoration of Indigenous culture within Western Australia. This is acknowledged by a preference being given to Indigenous names where possible.

The use of Indigenous names is encouraged and the collection and compilation of recorded Indigenous topographic names is supported.

At the City of Cockburn Aboriginal Reference Group meeting held on the 18 May 2015 the Reference Group recommended that Council support one of the three preferred names that were put forward by the Elders and that were recommended by the consultants Danny Ford and Rose Ford from Kambarang consultancy. Kwentilup meaning place of the Bandicoot, Moodjarlup meaning place of the Christmas Tree; or Mangatj meaning Banksia.

Conclusions

Should the recommended option be considered, the Jandakot location would not be affected in any way. Banjup (south) would retain a land area sufficient to meet the 'rural area' GNC guidelines, with an area of land approximately 1420ha in size. This proposed new locality would meet the requirements of the GNC guidelines and would allow for the future growth of the Banjup north area to be in a new suburb that has a more urbanised character in contrast to the more rural suburb of Banjup to the south.

Each of the Nyungar names is considered a feasible option for Council to consider. The name Quendalup or Kwentilup which means place of the Bandicoot is considered easier to pronounce which is one factor

that needs to be considered. However, the name Moodjarlup meaning place of the Christmas tree was identified by three elders as appropriate for the area. The Geographic Names Committee will be able to consider whether either of the names meets the other criteria.

Strategic Plan/Policy Implications

Community & Lifestyle

- Community environments that are socially cohesive and embrace diversity.
- The significance and richness of our local Indigenous people and diverse multicultural community will be recognised and celebrated.

Budget/Financial Implications

Advertising of the proposed locality change can be funded from an existing budget.

Legal Implications

There are no legal implications however if the recommendation is adopted, the proposal is required to be submitted to and approved by the Geographic Names Committee (GNC), which operates under the auspice of the Department of Planning.

Community Consultation

The City of Cockburn Aboriginal Reference Group requested Kambarang Services consult with six Traditional Owners identifying possible Nyungar names that could be considered for the naming of a new suburb to be created from the Banjup North area.

Each of the Elders/Traditional Owners (TO's) who were consulted are recognised as not only being TO's but also having a long association with the area.

<u>Males</u>	<u>Females</u>
Sealin Garlett	Theresa Walley
Trevor Walley	Marie Taylor
Len Collard	Gladys Yarran

Kambarang Services consulted with Elders/TO's on names they felt could be appropriate for the Banjup area.

Elders/TO's were informed of the project and why they were being approached. The Elders/TO's were advised that any names suggested may or may not be submitted and the final decision on names was to

decided be decided by the Council and then referred to the Geographic Names Committee. As part of the discussion, Elders/TO's were shown a map of the Banjup North site that is proposed to be renamed (Map showing Banjup North area attached).

Elders comments and suggestions were collated and then provided in report form to the City of Cockburn and the Aboriginal Reference Group (ARG) who were to decide on which name(s) they were to submit for Council consideration. The report also contained verification, background and information on meanings or possible stories behind each suggested name.

It is also understood from the provided Department of Aboriginal Affairs (DAA) site register information that there are no significant Aboriginal heritage sites within the proposed area apart from artefact scatter in four places (see below documentation from DAA).

Trevor Walley favoured the Christmas tree name, 'moodjar' and then changed it to 'Moodjarlup' meaning place of the Christmas tree.

Theresa Walley is a Wadjuk Elder who has lived and associated in the Kwinana and Fremantle area for the past 50 plus years. Theresa was keen on the word for the Christmas tree, 'moodjar' For some Nyungar people the moodjar tree has been referred to as a spirit tree where the spirit of a deceased person passes through it. The flowers of the moodjar can be dipped into water to make a sweet drink. Moodjar as a possible word could be coupled with the affix 'up' to make Moodjalup to mean "the place where the moodjar tree is or can be found".

Sealin Garlett is a Wadjuk Elder who has lived and associated with the Coolbellup and Jandakot areas for more than 20 years. Sealin had been thinking about a word for some time and offered the word "mangatj" meaning banksia.

Marie Taylor is a Wadjuk Elder who has lived and associated in the Fremantle areas most of her life. The name favoured by Marie was bandicoot, spelt "Quenda" or "Kwenta" 'Kwinder' recommended spelling 'Kwentlup',

Gladys Yarran is of the Ballardong clan and claims her Wadjuk connection through marriage. However Gladys has lived in the Fremantle and Jandakot area since the mid 1960's. Gladys is listed as a site informant on the DAA list of Wadjuk consultants. Gladys acknowledged the closeness of the Jandakot airport and maybe a word relating to flight or flying could be considered. The word for fly or to fly away is 'bardang'. This could be altered to 'Bardangup'.

Len Collard is a Wadjuk Elder with a lifelong association to the Fremantle and Jandakot areas. Len favoured the name 'moojarlup', the place where a Christmas tree can be.

All of the words, could easily be an appropriate name for new proposed locality.

However, it was clear that of the six people consulted, three preferred the name 'moodjar or moojar(lup)'. One person favoured 'mangatj', one favoured 'Quenda(lup), Kwenta(lup), Kwindar(up)', and one favoured 'bardang(up).

This report was presented at the Aboriginal Reference Group meeting on the 18 May 2015 and the Reference group recommended that:

"Council support one of the three most liked names that were put forward by the Elders and that were recommended by the consultants Danny Ford and Rose Ford. These were Kwentalup meaning place of the Bandicoot, Moodjarlup meaning place of the Christmas Tree; or Mangatj meaning Banksia

While the preceding process represents a full and proper engagement procedure with the TOs, consideration should also be afforded to those property owners who will be directly affected by this proposal. Accordingly, it is recommended that those landowners who have a direct interest in the land be provided an opportunity to comment on the proposal. While adequate time will be allocated to this process to enable feedback, it is also recommended that unless there is strong objection from these stakeholders (i.e. exceeding 50%), that it not be necessary for Council to formally reconsider this matter for final endorsement.

Attachment(s)

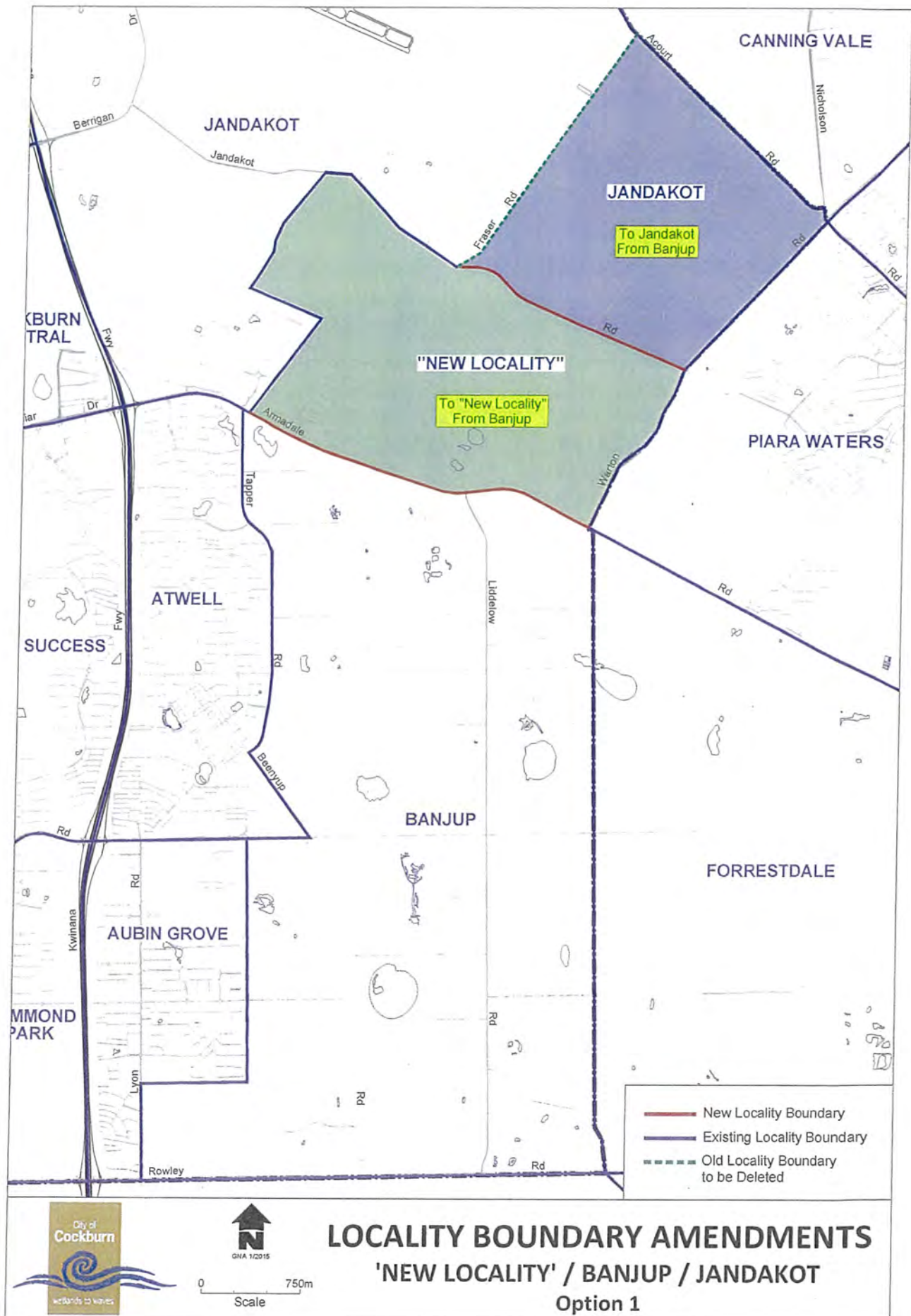
1. Site map of proposed new locality.
2. Consultation Report from Kambarang Services.
3. Minutes form City of Cockburn Aboriginal Reference Group Meeting – 18 May 2015.

Advice to Proponent(s)/Submissioners

The Proponent(s) have been advised that this matter is to be considered at the 9 July 2015 Council Meeting.

Implications of Section 3.18(3) Local Government Act, 1995

Nil.



RESULTS OF SURVEY - PROPOSED NEW LOCALITY NAME

Name	Property Address1	Property Address2	Support	Oppose	Propose	Officer Comment
Adriano Federici	4 Chicory Street	BANJUP WA 6164	x			
Adriano Federici & Christina Federici	15 Yarrow Street	BANJUP WA 6164	x			
Alastair Smillie & Lisa J Drew	57 Skotsch Road	BANJUP WA 6164		x	Treeby	Complies with GNC Guidelines
Alison C Waller	10 Lican Street	BANJUP WA 6164	x		Quendalup	
Alliance Holdings (Aust) Pty Ltd	10 Yarrow Street	BANJUP WA 6164	x		Quendalup	
Alliance Holdings (Aust) Pty Ltd	18 Lovage Street	BANJUP WA 6164	x		Quendalup	
Alliance Holdings (Aust) Pty Ltd	4 Wintergreen Crescent	BANJUP WA 6164	x		Quendalup	
Amanda L Ellis & James E Ellis	23 Wattlesed Avenue	BANJUP WA 6164		x	Maundan	Complies with GNC Guidelines
Amir Tabrizi	25 Lovage Street	BANJUP WA 6164	x		Quendalup	
Antionetta L Landro	20 Yarrow Street	BANJUP WA 6164		x		
Ashmy Pty Ltd	4 Wattlesed Avenue	BANJUP WA 6164	x		Quendalup	
Ashton Mutasa & Plaxedes Mutasa	47 Clementine Boulevard	BANJUP WA 6164	x			
Atri J Acharya & Neeta A Acharya	21 Clementine Boulevard	BANJUP WA 6164	x		Kwentalup	
Aveling Properties Pty Ltd	5 Wattlesed Avenue	BANJUP WA 6164	x			
Berly Baby Mathew & Binesh John	5 Savory Way	BANJUP WA 6164	x			
Booy K Ng & Kai S Jee	49 Wattlesed Avenue	BANJUP WA 6164	x			
Bree J Brown & Gregory M Brown	19 Wattlesed Avenue	BANJUP WA 6164	x		Quendalup	
Carlin Investments (WA) Pty Ltd	12 Elderflower Street	BANJUP WA 6164	x		Quendalup	
Chad R Strother & Tynille L Strother	6 Chicory Street	BANJUP WA 6164		x	Carnaby / North Banjup / Oxley Park	Carnaby may, North Banjup does not, Oxley complies with GNC Guidelines
Cheik T Thong	41 Clementine Boulevard	BANJUP WA 6164	x		Quendalup	
Collin K L Chin	27 Wattlesed Avenue	BANJUP WA 6164		x	Jandakot	Not supported, as this would create a location too large
Conrad B M Petit	37 Elderflower Street	BANJUP WA 6164		x	Either name	
Craig L Woolley	7 Caraway Street	BANJUP WA 6164		x		
Daniel J Law-Davis & Kiara H Dougan	458 Jandakot Road	BANJUP WA 6164		x	Treeby	Complies with GNC Guidelines
Daniel P Trevisiol	24 Clementine Boulevard	BANJUP WA 6164		x		
Daniel W Pianta & Jessica D Pianta	4 Yarrow Street	BANJUP WA 6164	x		Quendalup	
Daphne P Voon	29 Elderflower Street	BANJUP WA 6164		x		
Dean A Fitzgerald & Tracey A Fitzgerald	65L Jandakot Road	BANJUP WA 6164		x		
Deblina Sen & Amit S Sen	19 Clementine Boulevard	BANJUP WA 6164		x	Treeby	Complies with GNC Guidelines
Deborah DelCaro & Rodney DelCaro	51 Skotsch Road	BANJUP WA 6164	x			
Dennis G W Kwok & Hilda V L Ng	51 Wattlesed Avenue	BANJUP WA 6164	x			
Devang Hemendrakumar	7 Chicory Street	BANJUP WA 6164		x	Satellite	Does not comply with GNC Guidelines
Diane E Lanoue & Sarah A Lanoue	27 Caraway Street	BANJUP WA 6164		x		
Emma L McLay & Liam W Gretgrix	6 Lican Street	BANJUP WA 6164	x		Quendalup	
Goldburst Pty Ltd (Caroline Griffiths)	21 Ginger Loop	BANJUP WA 6164	x		Quendalup	
Grant P Stevens & Karen L Stevens	64L Jandakot Road	BANJUP WA 6164		x	Jandakot	Not supported. Too large
Gurpreet Singh	23 Elderflower Street	BANJUP WA 6164	x			
Hemachand Pamulapati & Susmitha Pamulapati	33 Wattlesed Avenue	BANJUP WA 6164	x		Quendalup	
Henryk F Jaskola & Lisa J Jaskola	15 Basil Loop	BANJUP WA 6164	x			
Hyejune Park & Anha Kim	62 Wintergreen Crescent	BANJUP WA 6164	x		Quendalup	
Ian D Johnson & Norma Johnson	68 Wintergreen Crescent	BANJUP WA 6164		x		
James A Cook & Shannon R Cook	29 Caraway Street	BANJUP WA 6164		x		

RESULTS OF SURVEY - PROPOSED NEW LOCALITY NAME

Name	Property Address1	Property Address2	Support	Oppose	Propose	Officer Comment
James L Holland & Katrina D Isaacs	17 Caraway Street	BANJUP WA 6164		x	Quendalup Park or Gardens/Leisure Park/Gateway Beginnings / Cockburn Kent/Buckingham Rise/Bandicoot Bushlands	Quendalup Park may, others do not comply with GNC Guidelines.
Jamie M De Palma	70 Wintergreen Crescent	BANJUP WA 6164		x	East Cockburn	Does not comply with GNC Guidelines
Jeremy P Chittenden & Rupal H Chittenden	10 Lovage Street	BANJUP WA 6164	x		Quendalup	
Justin B Hamer & Bernadette M Hamer	24 Bluegrass Street	BANJUP WA 6164		x		
Justin D Cato	4 Elderflower Street	BANJUP WA 6164		x		
Kalpeshkumar V Charuhan & Sheetal K Chauhan	33 Cilantro Parkway	BANJUP WA 6164	x		Quendalup	
Kap S Lee & Seng F Shek	7 Dill Way	BANJUP WA 6164		x	Calleya	Not acceptable to GNC
Karoline L Jamieson	23 Ginger Loop	BANJUP WA 6164		x	Moondak	Complies with GNC Guidelines
Kay H Soon & Soo K Chan & An W Soon	21 Lovage Street	BANJUP WA 6164	x		Quendalup	
Kevin I Taylor & Rebecca J Lynn	10 Perilla Way	BANJUP WA 6164		x	Calleya or Cockburn	Not acceptable to GNC
Kim D Tarzan & Yousley B Tarzan	17 Wattleseed Avenue	BANJUP WA 6164		x		
Kwang Y Chung & Kyung I Kim	5 Perilla Way	BANJUP WA 6164		x	Prefer name which is easy to spell & pronounce	
Laura A Robinson	9 Lican Street	BANJUP WA 6164		x	Jandakot	Not supported. Too large.
Lisa M Campbell & Anthony J Dennis	3 Yarrow Street	BANJUP WA 6164		x		
Lyndon C Reitsema & Neesha R Reitsema	37 Clementine Boulevard	BANJUP WA 6164		x	Calleya	Not acceptable to GNC
Lyndon Greenway & Renae L Black	31 Skotsch Road	BANJUP WA 6164		x	Remain as Banjup	
Maria J Stutley & Michael J Stutley	30 Elderflower Street	BANJUP WA 6164		x	Oxley	
Mark A Sheppard	24 Skotsch Road	BANJUP WA 6164		x	Park/Middleton/Middleton Park	Acceptable to GNC Guidelines
Mark G Rheinberger & Leah C Rheinberger	16 Caraway Street	BANJUP WA 6164		x	Treeby	Complies with GNC Guidelines
Mark L Chamberlain & Kelly M Chamberlain	6 Caraway Street	BANJUP WA 6164	x		Quendalup	
Mark R Adams	28 Clementine Boulevard	BANJUP WA 6164		x	Banjup	
Martin Lovatt & Ivka-Anna A Lovatt	16 Aniseed Road	BANJUP WA 6164	x		Quendalup	
Martin S Kitching	12 Lovage Street	BANJUP WA 6164		x		
Mathew R Moyle	41 Wattleseed Avenue	BANJUP WA 6164		x	Beelara	Too close to "Beellar" to be acceptable.
Meng Chen	20 Wintergreen Crescent	BANJUP WA 6164		x	Kwentallup	
Mirko Hessel & Uvielane A Capilitan	10 Chicory Street	BANJUP WA 6164	x		Fraser Parkland	Complies with GNC Guidelines
Nareesh K H Patel	8 Lovage Street	BANJUP WA 6164	x		Kwentallup	
Nathan W Beard	25 Elderflower Street	BANJUP WA 6164		x	Moodjalup	Complies with GNC Guidelines
Nigel J Hurley & Katarzyna A Belka	22 Yarrow Street	BANJUP WA 6164		x	Banjup	
Nishant Bajaj & Richa Bajaj	3 Lican Street	BANJUP WA 6164	x		Quendalup	
Noel Baker & Mary D Baker	49 Skotsch Road	BANJUP WA 6164		x	Treeby	Complies with GNC Guidelines
Peng A Lim & Hui L Lim	8 Wintergreen Crescent	BANJUP WA 6164		x	Jandakot	Not supported. Jandakot would be too large.
Peter J Hardie & Jacqueline M Young	74L Skotsch Road	BANJUP WA 6164	x		Quendalup	
Portia Jonasi	32 Wintergreen Crescent	BANJUP WA 6164	x			
Regan M Dalwood	20 Elderflower Street	BANJUP WA 6164		x	Calleya	Unacceptable to GNC

RESULTS OF SURVEY - PROPOSED NEW LOCALITY NAME

Name	Property Address1	Property Address2	Support	Oppose	Propose	Officer Comment
Roan J Barrett & Kerwin E Paulus	4 Lovage Street	BANJUP WA 6164		x		
Robert Muir & Sally E Muir	25 Watleseed Avenue	BANJUP WA 6164	x			
Rohan A Deshpande & Prachi A Joshi	13 Yarrow Street	BANJUP WA 6164		x	Oxley Park	Complies with GNC Guidelines
Sandhya Khadka & Anukraman Neupane	25 Ginger Loop	BANJUP WA 6164		x		
Sarah L Brennan & Luke T Juniper	32 Bluegrass Street	BANJUP WA 6164		x		
Sharad Poudel & Ranjana Sapkota	74 Wintergreen Crescent	BANJUP WA 6164		x		
Sophia A McLay & Neil L McLay	13 Ginger Loop	BANJUP WA 6164	x		Quendalup	
Steven A De Rosa	5 Dill Way	BANJUP WA 6164			Sunnyvale	Does not comply with GNC Guidelines
Steven Price & Cynthia G Price	5 Ginger Loop	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	9008L Bluegrass Street	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	9009L Jandakot Road	BANJUP WA 6164	X		Quendalup	
Stockland WA Development Pty Ltd	9 Basil Loop	BANJUP WA 6164		x	Fraser or Oxley	Both comply with GNC Guidelines
Stockland WA Development Pty Ltd	53 Clementine Boulevard	BANJUP WA 6164		x	Rosea Grove/Hill / Field / lup / other suffix / Pimelia (with or without a suffix) / Bardangup / Moondak	All comply with GNC Guidelines. Pimelia not supported, as it is too close to "Parnelia"
Stockland WA Development Pty Ltd	9514L Clementine Boulevard	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	8 Caraway Street	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	64 Wintergreen Crescent	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	9012L Fraser Road	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	9509L Wintergreen Crescent	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	25 Caraway Street	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd (Paige Palfrey)	8 Ginger Loop	BANJUP WA 6164		x	Proposed names too difficult to pronounce	
Stockland WA Development Pty Ltd (Donna-Lee Tani)	6 Ginger Loop	BANJUP WA 6164		x		
Stockland WA Development Pty Ltd (Candice Gorma)	10 Ginger Loop	BANJUP WA 6164		x		
Stockland WA Development Pty Ltd (Kelly & Anthony)	4 Ginger Loop	BANJUP WA 6164		x		
Stockland WA Development Pty Ltd	2 Ginger Loop	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	9006L Armadale Road	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	9510L Bluegrass Street	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	27 Clementine Boulevard	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	23 Clementine Boulevard	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	29 Clementine Boulevard	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	25 Clementine Boulevard	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	9012L Fraser Road	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	14 Ginger Loop	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	16 Ginger Loop	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	12 Ginger Loop	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	29 Ginger Loop	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	10 Caraway Street	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	137 Wintergreen Crescent	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	147 Wintergreen Crescent	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	3 Caraway Street	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	66 Wintergreen Crescent	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	129 Wintergreen Crescent	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	9512L Wintergreen Crescent	BANJUP WA 6164	x		Quendalup	

RESULTS OF SURVEY - PROPOSED NEW LOCALITY NAME

Name	Property Address1	Property Address2	Support	Oppose	Propose	Officer Comment
Stockland WA Development Pty Ltd	9503L Wintergreen Crescent	BANJUP WA 6164	x		Quendalup	
Stockland WA Development Pty Ltd	9002L Jandakot Road	BANJUP WA 6164	x		Quendalup	
Tangent Nominees Pty Ltd	6 Wattlesseed Avenue	BANJUP WA 6164	x			
Tangent Nominees Pty Ltd	8 Wattlesseed Avenue	BANJUP WA 6164	x			
Tonino Berlingieri & Vincenza Berlingieri	46 Skolsch Road	BANJUP WA 6164	x		Quendalup	
Tracey P Clarke & Liam Groom	141 Wintergreen Crescent	BANJUP WA 6164		x	Oxley Park	Complies with GNC Guidelines
Troy R Cowin	8 Perilla Way	BANJUP WA 6164		x	Prefer non-aboriginal	
Ung S Wong & Hie Ting Yu	22 Clementine Boulevard	BANJUP WA 6164	x		Kwentallup	
Vijaya K Madhivanan	60 Wintergreen Crescent	BANJUP WA 6164	x		Kwentallup	
William De Jesus & Mylene De Jesus	5 Chicory Street	BANJUP WA 6164	x		Quendalup	
WOW Group (WA) Pty Ltd	14 Wattlesseed Avenue	BANJUP WA 6164		x	Jandakot	Not supported. Jandakot would be too large.
Yew M Lim & Toon Y Goh	39 Clementine Boulevard	BANJUP WA 6164	x			
Young-Sok Choi & Yun H Shim	23 Lican Street	BANJUP WA 6164		x	North Banjup	Does not comply with GNC Guidelines
Yung-Chun E Chi	139 Wintergreen Crescent	BANJUP WA 6164	x			

RESULTS OF SURVEY - AMALGAMATE PART BANJUP INTO JANDAKOT

Name	Property Address1	Property Address2	Support	Oppose	Propose	Officer Comment
Ainslie E Hughes	9 Owsten Court	BANJUP WA 6164	x			
Antonio R Napolitano	624 Warton Road	BANJUP WA 6164		x		
Baiggio A Gatani & Kelly D Gatani	57L Spuria Way	BANJUP WA 6164	x			
Barry R M Armstrong & Karen-Lee Armstrong	30 Louisiana Glen	BANJUP WA 6164	x			
Bruce W Clark & Paula L Clark	6 Owsten Court	BANJUP WA 6164		x		
Carol A Howe & Robert T Howe & Terence A Howe & Kylie M Wynn	77 Hybrid Court	BANJUP WA 6164		x	Banjup	
Dianne & John Carmody & John F Carmody	47 Hybrid Court	BANJUP WA 6164		x		
Domenico A Lombardo & Josephine Lombardo	27 Acourt Road	BANJUP WA 6164	x			
Gail M Humphries & Simon J Humphries	50 Louisiana Glen	BANJUP WA 6164	x			
Gordon D Davies & Bristal J Davies	55 Louisiana Glen	BANJUP WA 6164				
Grace Turner	60L Louisiana Glen	BANJUP WA 6164	x		Banjup	
Grazia Maiolo	16 Hebe Road	BANJUP WA 6164		x		
Ian G Ellis & Kerry A Ellis	16 Louisiana Glen	BANJUP WA 6164		x	Banjup	
Ian L Nicholls & Yvonne Nicholls	393 Jandakot Road	BANJUP WA 6164	x			
Jennifer M Robinson & Yvonne Nicholls	15 Hybrid Court	BANJUP WA 6164		x		
Jo-Ann E Kruger & Kimberley R Kruger	377 Jandakot Road	BANJUP WA 6164		x	Banjup	
Kathleen F Brown & Peter J Brown	67 Louisiana Glen	BANJUP WA 6164	x			
Katie J Palmer & Stuart L Palmer	79 Hybrid Court	BANJUP WA 6164		x	Banjup	
Kay L Von Dietze & Erich C Von Dietze	7 Owsten Court	BANJUP WA 6164		x	Quendalup	Complies with GNC Guidelines
Kevin D J Bowles & Julie L Bowles	78 Hybrid Court	BANJUP WA 6164		x	Treeby (in memory of Treeby Family)	Complies with GNC Guidelines
Kimberley I Nalder & Thirza M Nalder	4 Ackworth Road	BANJUP WA 6164		x	Banjup	
Leonard G Baker & Ineke Baker	385 Jandakot Road	BANJUP WA 6164		x		
Linda Hicks & Rodney I Hicks	4 Owsten Court	BANJUP WA 6164		x		
Mark S Blight & Birgit Blight	425 Jandakot Road	BANJUP WA 6164		x	Banjup	
Mirko Simunovic & Snjezana Simunovic	17 Owsten Court	BANJUP WA 6164	x			
Nathan S Ferguson & Lisa M Ferguson	8 Ackworth Road	BANJUP WA 6164		x		
Neil L McLay & Sophia A McLay	17 Louisiana Glen	BANJUP WA 6164	x			
Peter A Van Der Wedden & Tarresa V Van Der Wedden	18 Owsten Court	BANJUP WA 6164		x		
Peter J O'Loughlin & Michelle O'Loughlin	459 Jandakot Road	BANJUP WA 6164		x	Banjup	
Pietro Cinanni & Sharon L Cinanni	1 Owsten Court	BANJUP WA 6164		x		
Roblea Investment Trust	26 Ackworth Road	BANJUP WA 6164	x			
Scott R Haddrell	13 Owsten Court	BANJUP WA 6164		x		
Stephen Marwood & Claire M Marwood	5 Ackworth Road	BANJUP WA 6164	x			

RESULTS OF SURVEY - AMALGAMATE PART BANJUP INTO JANDAKOT

<u>Name</u>	<u>Property Address1</u>	<u>Property Address2</u>	<u>Support</u>	<u>Oppose</u>	<u>Propose</u>	<u>Officer Comment</u>
Steven C Marnick	56L Spuria Way	BANJUP WA 6164	x		North & South should be named the same	
Steven J Gadeke & Brunetta Gadeke Tillbrook Nominees Pty Ltd Tome A Correia & Elsa M Correia Vincent Nominees Pty Ltd	21 Owsten Court	BANJUP WA 6164		x		
	25L Acourt Road	BANJUP WA 6164		x		
	10 Ackworth Road	BANJUP WA 6164	x			
	367 Jandakot Road	BANJUP WA 6164		x	Banjup	
William H Lunt & Hendrika Lunt	11 Ackworth Road	BANJUP WA 6164	x		Relates more to Jandakot than Banjup	
Yodi Pattula	18 Hybrid Court	BANJUP WA 6164				